

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED
STATE HIGHWAY
FEDERAL AID PROJECT NO. JS-4428(004)
GRADE, DRAIN & BRIDGE
US 81 REALIGNMENT
GRADY COUNTY

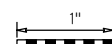
CONTROL SECTION NO. 81-26-54P
STATE JOB NO. 24428(04)

BRIDGE "F" LOCATION NO. 2654P 0181X , NEW NBI NO. XXXX
BRIDGE "H" LOCATION NO. 2654P 0238X , NEW NBI NO. XXXX
BRIDGE "K" LOCATION NO. 2654P 0392X , NEW NBI NO. XXXX

SEE SHEET 2 FOR INDEX OF SHEETS AND STANDARDS

FOR SURVEY CONTROL DATA,
SEE SURVEY DATA SHEETS

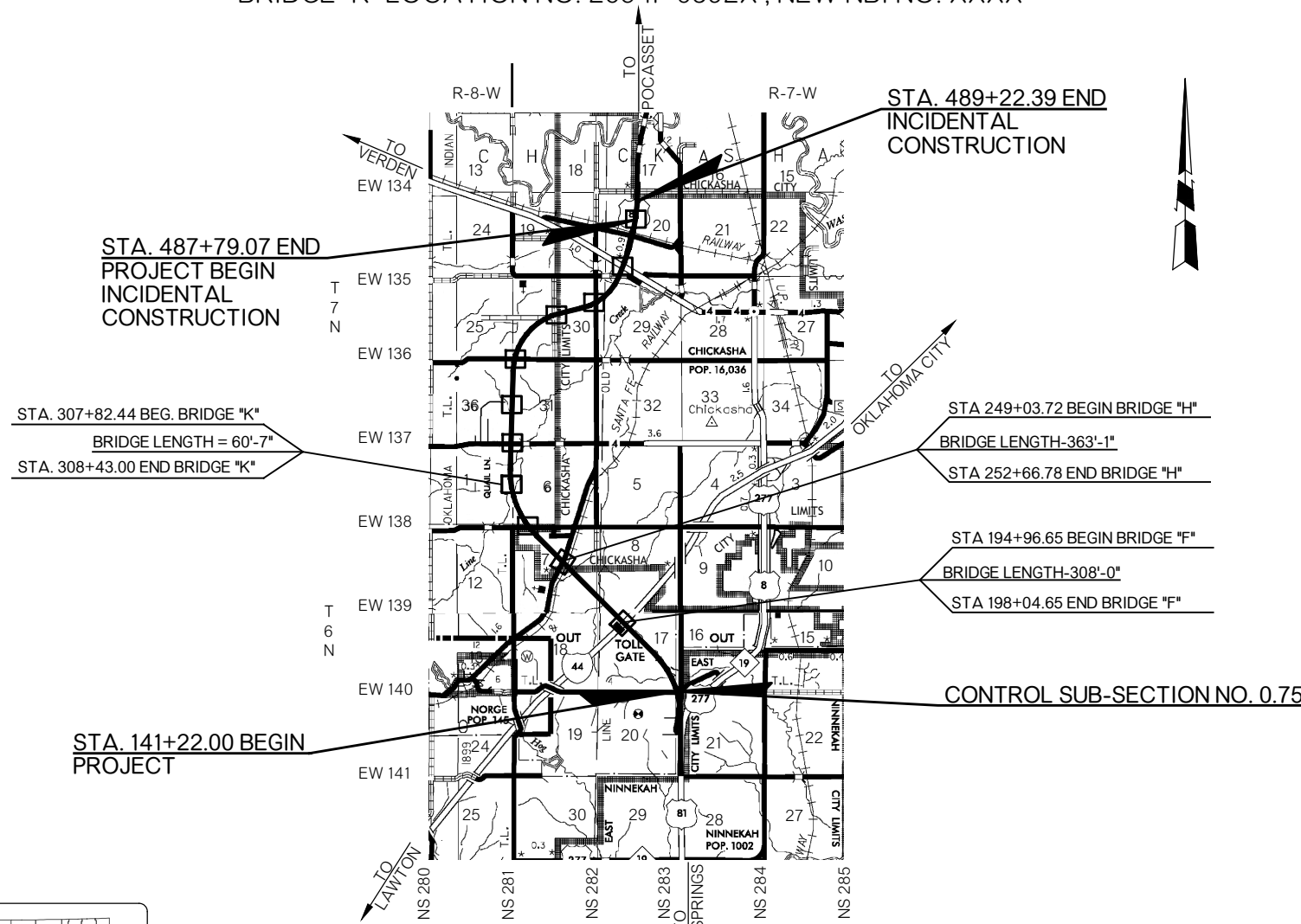
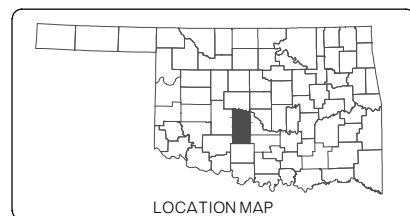
| DESIGN DATA | |
|------------------|----------|
| ADT 20 20 | = 6,220 |
| ADT 20 40 | = 8,400 |
| DHV (ONE-WAY) | = 437 |
| K (DHV/ADT) | = 10% |
| D | = 52% |
| T (% DHV) | = 17% |
| T (% ADT) | = 19% |
| T3 (% ADT) | = 11% |
| V | = 70 MPH |
| 20YR FLEX. ESALS | = 6.2 M |



PLAN 1" = 50'
PROFILE HOR. 1" = 50'
VER. 1" = 5' & 10'
LAYOUT MAP 1" = 5,280'

CONVENTIONAL SYMBOLS

- PROPOSED ROAD
- RAILROADS
- RANGE & TOWNSHIP
- SECTION LINES
- QUARTER SECTION LINES
- FENCES
- GROUND LINE
- EXISTING ROADS
- BASE LINE
- GRADE LINES
- TELEPHONE & TELEGRAPH
- POWER LINES
- BUILDINGS
- OIL WELLS
- DRAINAGE STRUCTURES - IN PLACE
- DRAINAGE STRUCTURES - NEW
- RIGHT-OF-WAY LINES - EXISTING
- RIGHT-OF-WAY LINES - NEW
- CONTROLLED ACCESS
- RIGHT-OF-WAY FENCE



THIS DOCUMENT IS PRELIMINARY
IN NATURE AND IS NOT A FINAL,
SIGNED AND SEALED DOCUMENT.



JOSHUA M. JOHNSTON
LICENSED PROFESSIONAL ENGINEER NO. 26204 DATE

NOTE: PROJECT LENGTH BASED ON CL SURVEY US 81 STATIONING.

| | | |
|----------------|---------------|-----------|
| ROADWAY LENGTH | 33,925.41 FT. | 6.425 MI. |
| BRIDGE LENGTH | 731.66 FT. | 0.138 MI. |
| PROJECT LENGTH | 34,657.07 FT. | 6.563 MI. |

EQUATIONS: NONE
EXCEPTION: NONE

| | | | |
|---------------------------------------|-------------------------------|---|--|
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION | |
| DATE APPROVED | | DATE APPROVED | |
| BY | | BY | |
| CHIEF ENGINEER | | DIVISION ADMINISTRATOR | |
| SWO 4380(1) | F.A. PROJECT NO. JS-4428(004) | | |
| COUNTY GRADY COUNTY | HIGHWAY US 81 REALIGNMENT | SHEET NO. 0001 | |

P.E. NO. XXXX

INDEX OF SHEETS

0001 TITLE SHEET
 0002 INDEX OF SHEETS, STANDARDS, AND BRIDGES
 0003 INDEX OF SEALS
 0004-0016 TYPICAL SECTIONS
 AB01 PAY QUANTITIES AND GENERAL NOTES (BRIDGE "F")
 AB02-AB03 PAY QUANTITIES AND GENERAL NOTES (BRIDGE "H")
 AB04 PAY QUANTITIES AND GENERAL NOTES (BRIDGE "K")
 AE01 ENVIRONMENTAL NOTES
 AR01 PAY QUANTITIES
 AR02 PAY NOTES
 AR03 SUGGESTED SEQUENCE OF CONSTRUCTION
 AR04-AR19 SUMMARIES
 AR20-AR24 SUMMARY OF DRAINAGE STRUCTURES
 AT01 PAY QUANTITIES & NOTES - TRAFFIC CONTROL
 AT02 PAY QUANTITIES & NOTES - TRAFFIC SIGNING & STRIPING
 AT03 SUMMARY OF SHEETS - TRAFFIC
 B001-B003 GENERAL PLAN AND ELEVATION (BRIDGE "F")
 B004-B006 FOUNDATION REPORT (BRIDGE "F")
 B007 SUBSTRUCTURE STAKING DIAGRAM (BRIDGE "F")
 B008-B009 ABUTMENT 1 DETAILS (BRIDGE "F")
 B010 ABUTMENT 1 WINGWALL DETAILS (BRIDGE "F")
 B011-B012 ABUTMENT 2 DETAILS (BRIDGE "F")
 B013 ABUTMENT 2 WINGWALL DETAILS (BRIDGE "F")
 B014 SUBSTRUCTURE EXCAVATION AND PIPE UNDERDRAIN ASSEMBLY DETAILS (BRIDGE "F")
 B015 PIER 1 DETAILS (BRIDGE "F")
 B016 PIER 2 DETAILS (BRIDGE "F")
 B017 PIER 3 DETAILS (BRIDGE "F")
 B018-B019 MISCELLANEOUS PIER DETAILS (BRIDGE "F")
 B020 TYPICAL SECTION (BRIDGE "F")
 B021-B022 LONGITUDINAL SECTION (BRIDGE "F")
 B023 BEARING DETAILS (BRIDGE "F")
 B024 P.C. BEAM AND DIAPHRAGM LAYOUT PLAN (BRIDGE "F")
 B025 DIAPHRAGM DETAILS (BRIDGE "F")
 B026 BOTTOM SLAB REINFORCING PLAN (BRIDGE "F")
 B027 TOP SLAB REINFORCING PLAN AND PARAPET LAYOUT (BRIDGE "F")
 B028 CONCRETE LONGITUDINAL BARRIER DETAILS (BRIDGE "F")
 B029 MISCELLANEOUS SUPERSTRUCTURE DETAILS (BRIDGE "F")
 B030 EXPANSION JOINT DETAILS (BRIDGE "F")
 B031-B032 TYPE IV P.C. BEAM DETAILS (BRIDGE "F")
 B033-B035 APPROACH SLAB DETAILS (BRIDGE "F")
 B036-B037 SLOPE WALL DETAILS (BRIDGE "F")
 B038 DRAIN AT END BRIDGE DETAILS (BRIDGE "F")
 B039-B040 GENERAL PLAN AND ELEVATION (BRIDGE "H")
 B041-B043 FOUNDATION BORING LOGS (BRIDGE "H")
 B044 SUBSTRUCTURE STAKING DIAGRAM (BRIDGE "H")
 B045 DETAILS OF GRADING, SUBSTRUCTURE EXCAVATION & PIPE UNDERDRAIN ASSEMBLY AT ABUTMENTS (BRIDGE "H")
 B046-B047 DETAILS OF ABUTMENT NO. 1 (BRIDGE "H")
 B048-B049 DETAILS OF ABUTMENT NO. 2 (BRIDGE "H")
 B050 DETAILS OF WINGS AT ABUTMENT NO. 1 (BRIDGE "H")
 B051 DETAILS OF WINGS AT ABUTMENT NO. 2 (BRIDGE "H")
 B052-B054 DETAILS OF PIERS (BRIDGE "H")
 B055-B059 DETAILS OF SUPERSTRUCTURE (BRIDGE "H")
 B060 DETAILS OF BEARING ASSEMBLIES (BRIDGE "H")
 B061 DETAILS OF PRESTRESSED CONCRETE BEAMS (TYPE IV) IN SPAN NO. 1 (BRIDGE "H")
 B062 DETAILS OF PRESTRESSED CONCRETE BEAMS (TYPE IV) IN SPAN NO. 2 & 3 (BRIDGE "H")
 B063 DETAILS OF PRESTRESSED CONCRETE BEAMS (TYPE IV) IN SPAN NO. 4 (BRIDGE "H")
 B064 DETAILS OF APPROACH SLABS (BRIDGE "H")
 B065-B066 DETAILS OF LAYOUT OF 42" F-SHAPED PARAPET
 B067 DETAILS OF SLOPE WALLS
 B068 DETAILS OF FENCE ON CONCRETE PARAPETS (BRIDGE "H")
 B069-B070 GENERAL PLAN & ELEVATION (BRIDGE "K")
 B071-B076 DETAILS OF REINFORCED CONCRETE BOX (BRIDGE "K")
 E001 404 PERMIT
 R001 STORMWATER MANAGEMENT PLAN SHEET
 R002-R005 DRAINAGE RECORD
 R006-R011 DRAINAGE AREA MAP
 R012-R064 EROSION CONTROL SHEETS
 R065-R098 GEOMETRIC LAYOUT
 R099-R228 PLAN AND PROFILE SHEETS
 R229-R251 REMOVAL SHEETS
 R252-R310 GRADING AND PAVING LAYOUT SHEETS
 R311-R320 MASS DIAGRAM SHEETS
 R321 STRUCTURE 11 DETAILS
 R322-R326 STRUCTURE 40 DETAILS
 R327 STRUCTURE 69 DETAILS
 S001-S076 SURVEY DATA SHEETS
 T001-T012 SUGGESTED TRAFFIC CONTROL
 T013-T018 SIGNING AND STRIPING
 X001-X607 CROSS SECTIONS

| BRIDGE INDEX | | | | |
|--------------|-----------------|---------------|-------------|---------------|
| BRIDGE | ALIGNMENT | BEGIN STATION | END STATION | BRIDGE LENGTH |
| F | CL SURVEY US 81 | 194+96.61 | 198+04.61 | 308'-0" |
| H | CL SURVEY US 81 | 249+03.72 | 252+66.78 | 363'-0 3/4" |
| K | CL SURVEY US 81 | 307+82.44 | 308+43.00 | 60'-6 3/4" |

THE FOLLOWING STANDARD DRAWINGS SHALL BE REQUIRED FOR THIS PROJECT:

| ROADWAY | BRIDGE | TRAFFIC CONTROL | TRAFFIC SIGNING |
|-----------|------------|--------------------|-----------------------|
| SSS-1-1 | MJB-3-2 | LECS-4-1 | TCS1-1-01 TCS13-1-00 |
| TSC-1-3-2 | MFC-4-1 | LTU-4-0 | TCS2-1-00 TCS14-1-00 |
| TSC-2-3-2 | SPI-4-1 | PED-3-2 | TCS3-1-01 TCS15-1-00 |
| TSD-2-0 | SPB-1-4 | PUD-3-2 | TCS4-1-01 TCS16-1-00 |
| TRFD-1-2 | FHTCP-3-1 | FSHP-42-2-00E | TCS5-1-00 TCS18-1-01 |
| PED-3-2 | SBI-4-2 | EJ-SK-04E | TCS6-1-02 TCS19-1-01 |
| PSE-1-0 | PUD-3-3 | EJDTL-02E | TCS7-1-02 TCS20-1-00 |
| PSMD-1-0 | CLB-1-3 | HP1-2-01E | TCS8-1-00 TCS21-1-02 |
| CET4S-3-2 | MI-3-0 | RCB-E2-H7-0-1-01E | TCS9-1-01 TCS24-1-02 |
| CET6S-3-2 | RDI-3-2 | RCB-C2-8(2-12)-01E | TCS10-1-00 TCS25-1-00 |
| CET4D-3-2 | DC-3-2 | RCB-E2-H7-0-2-01E | TCS11-1-01 |
| CET6D-3-2 | PDT-1-3 | RCB-CW2-D4-0-01E | |
| PCES-4-1 | RWF-1-2-3 | RCB-E2-H8-0-1-01E | |
| SMD-3-2 | SUEL-1-3-2 | RCB-E2-H8-0-2-01E | |
| GPI-4-0 | SUEL-3-3-2 | RCB-E2-H6-0-1-01E | |
| | | RCB-E2-H6-0-2-01E | |

THE FOLLOWING DISCONTINUED STANDARD DRAWINGS SHALL BE REQUIRED FOR THIS PROJECT:

BC-6S3LF
BC-6S3RF

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

INDEX OF SHEETS, STANDARDS AND BRIDGES

OLSSON

THIS SEAL COVERS SHEETS:
AT03, T001-T018



W2M CONSULTING, LLC
2946 N.W. 156TH STREET
EDMOND, OK 73013
PHONE (405) 513-7005
OKLAHOMA C.A. NO. 3919, RENEWAL 06/30/21

W. MIKE MORRISON, P.E.
OKLAHOMA P.E. NO. 16025

DATE

THIS SEAL COVERS SHEETS:
AB01, B1-B38



3020 N.W. 149TH STREET
OKLAHOMA CITY, OK 73134
PH. (405) 752-1122
FAX (405) 752-8855
CA# 1759, RENEWAL 06-30-2021

SHAY V. SMITH
LICENSED PROFESSIONAL ENGINEER NO. 27713

DATE

THIS SEAL COVERS SHEETS:
AB02-AB04, B039-B076



3020 N.W. 149TH STREET
OKLAHOMA CITY, OK 73134
PH. (405) 752-1122
FAX (405) 752-8855
CA# 1759, RENEWAL 06-30-2021

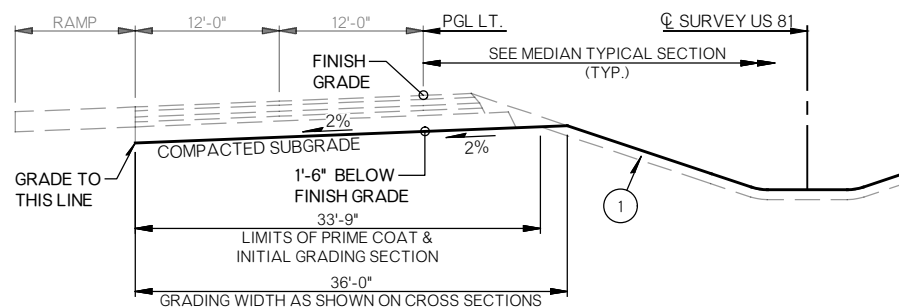
JOSHUA M. JOHNSTON
LICENSED PROFESSIONAL ENGINEER NO. 26204

DATE

THIS SEAL COVERS SHEETS:
001-0016, AE01, AR01-AR24, E001, R001-R327, X001-X607

THIS DOCUMENT IS PRELIMINARY
IN NATURE AND IS NOT A FINAL
SIGNED AND SEALED DOCUMENT.

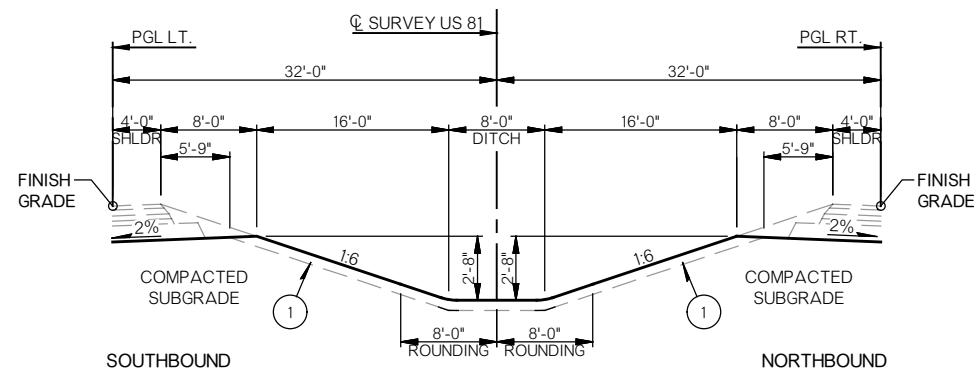
INDEX OF SEALS



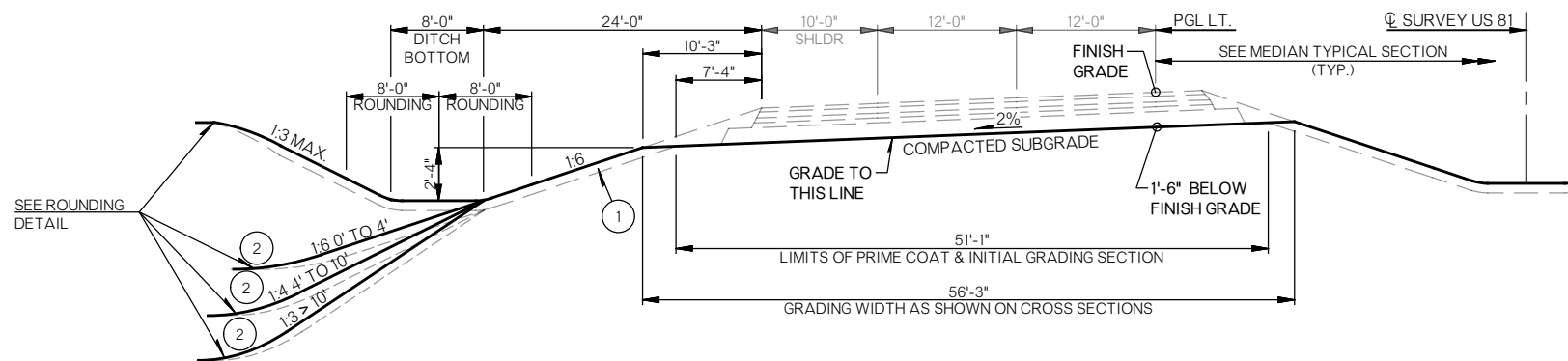
TYPICAL SECTION NO. 1
U.S. 81

STA. 142+29.57 TO STA. 145+68.70 SOUTHBOUND
 STA. 155+05.86 TO STA. 163+93.89 SOUTHBOUND
 STA. 253+68.54 TO STA. 269+80.58 SOUTHBOUND
 STA. 281+30.13 TO STA. 286+64.61 SOUTHBOUND
 STA. 321+78.07 TO STA. 328+97.64 SOUTHBOUND
 STA. 348+38.51 TO STA. 362+20.54 SOUTHBOUND
 STA. 398+96.26 TO STA. 402+84.56 SOUTHBOUND
 STA. 420+41.45 TO STA. 424+59.49 SOUTHBOUND
 STA. 468+70.82 TO STA. 473+87.21 SOUTHBOUND

STA. 148+68.09 TO STA. 164+37.68 NORTHBOUND
 STA. 272+17.98 TO STA. 287+61.68 NORTHBOUND
 STA. 298+53.43 TO STA. 302+07.25 NORTHBOUND
 STA. 321+78.07 TO STA. 329+61.49 NORTHBOUND
 STA. 349+12.36 TO STA. 362+80.39 NORTHBOUND
 STA. 399+37.32 TO STA. 401+96.94 NORTHBOUND
 STA. 422+94.31 TO STA. 425+74.85 NORTHBOUND
 STA. 461+68.17 TO STA. 474+07.46 NORTHBOUND



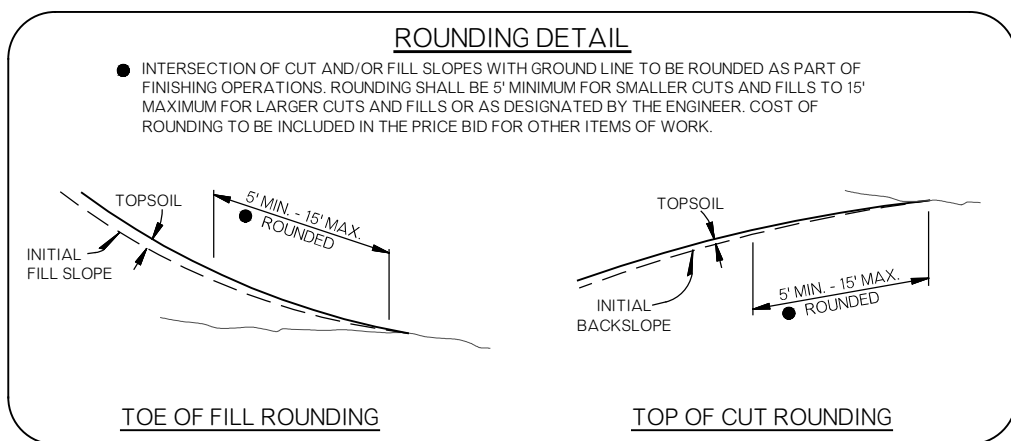
MEDIAN TYPICAL SECTION
U.S. 81



TYPICAL SECTION NO. 2
U.S. 81

STA. 145+68.70 TO STA. 149+37.71 SOUTHBOUND
 STA. 239+00.00 TO STA. 253+68.54 SOUTHBOUND
 STA. 269+80.58 TO STA. 281+30.13 SOUTHBOUND
 STA. 328+97.64 TO STA. 348+38.51 SOUTHBOUND
 STA. 362+20.54 TO STA. 390+90.00 SOUTHBOUND
 STA. 402+84.56 TO STA. 420+41.45 SOUTHBOUND
 STA. 428+65.46 TO STA. 453+80.49 SOUTHBOUND
 STA. 473+87.21 TO STA. 486+37.88 SOUTHBOUND

STA. 141+96.15 TO STA. 148+68.09 NORTHBOUND
 STA. 238+50.00 TO STA. 272+17.98 NORTHBOUND
 STA. 287+61.68 TO STA. 298+53.43 NORTHBOUND
 STA. 329+61.49 TO STA. 349+12.36 NORTHBOUND
 STA. 362+80.39 TO STA. 391+21.64 NORTHBOUND
 STA. 401+96.94 TO STA. 422+94.31 NORTHBOUND
 STA. 436+90.34 TO STA. 461+68.17 NORTHBOUND
 STA. 474+07.46 TO STA. 486+21.99 NORTHBOUND



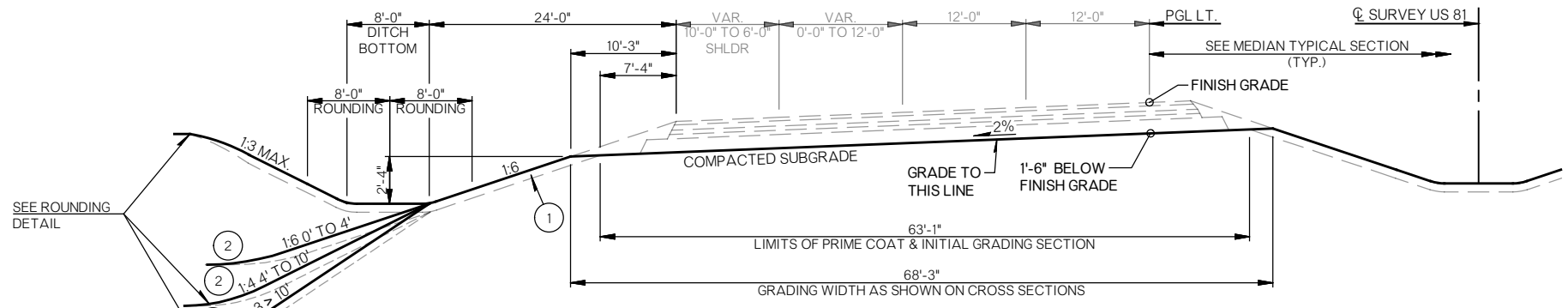
- 1 TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.
- 2 DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.
- PRIME COAT WILL BE APPLIED IMMEDIATELY AFTER SUBGRADE IS COMPACTED TO GRADE.
- SUBGRADE SHALL BE STABILIZED IN FUTURE SURFACING PROJECTS.
- SURFACING SHOWN FOR INFORMATIONAL PURPOSES ONLY. SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS.
- SEE GRADING AND PAVING LAYOUT SHEETS FOR ADDITIONAL TRANSITION AND TAPER INFORMATION AND CROSS SECTIONS FOR ADDITIONAL SUPERELEVATION AND CROSS SLOPE INFORMATION.

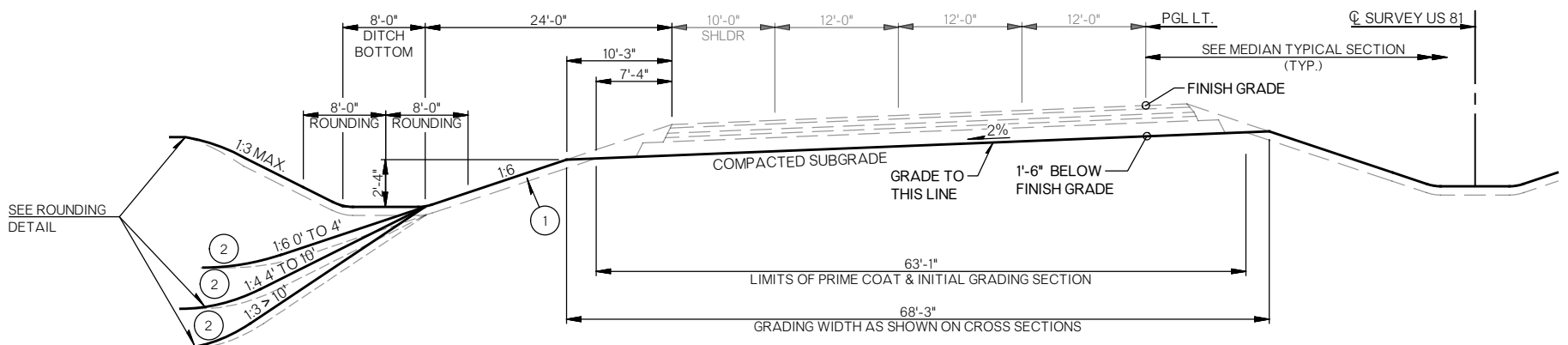
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

TYPICAL SECTIONS

US 81 REALIGNMENT
GRADY COUNTY



TYPICAL SECTION NO. 3
U.S. 81
STA. 149+37.71 TO STA. 155+05.86 SOUTHBOUND



TYPICAL SECTION NO. 4
U.S. 81
STA. 163+93.89 AND TRANSITION TO TYPICAL NO.5 STA. 173+44.37 SOUTHBOUND STA. 164+37.68 AND TRANSITION TO TYPICAL NO. 5 STA. 173+00.00 NORTHBOUND

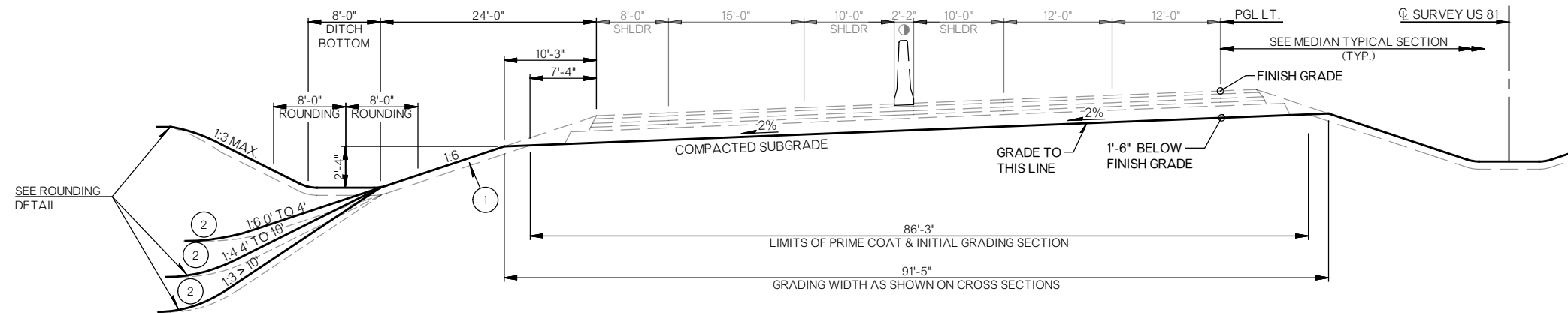
- 1 TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.
- 2 DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.
- PRIME COAT WILL BE APPLIED IMMEDIATELY AFTER SUBGRADE IS COMPACTED TO GRADE.
- SUBGRADE SHALL BE STABILIZED IN FUTURE SURFACING PROJECTS.
- SURFACING SHOWN FOR INFORMATIONAL PURPOSES ONLY. SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS.
- SEE GRADING AND PAVING LAYOUT SHEETS FOR ADDITIONAL TRANSITION AND TAPER INFORMATION AND CROSS SECTIONS FOR ADDITIONAL SUPERELEVATION AND CROSS SLOPE INFORMATION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

TYPICAL SECTIONS

US 81 REALIGNMENT
GRADY COUNTY

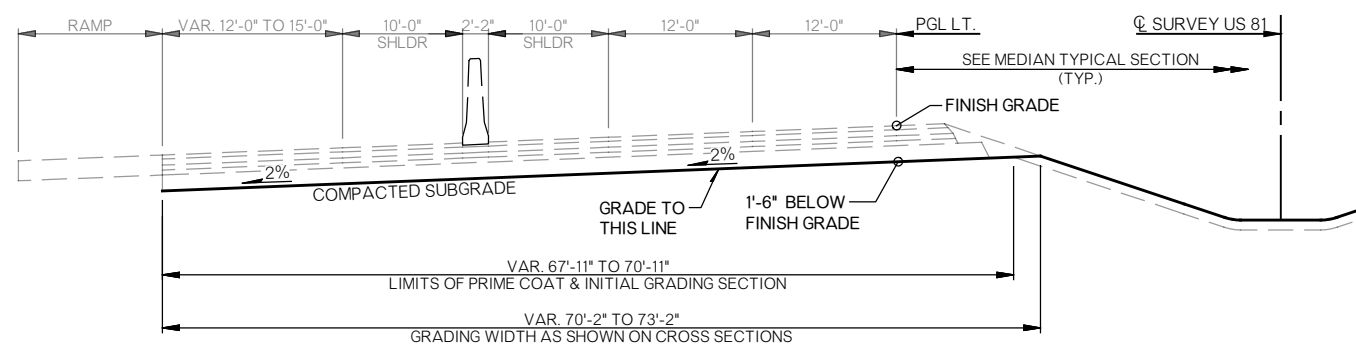


TYPICAL SECTION NO. 5

U.S. 81

STA. 173+44.37 SOUTHBOUND STA. 173+00.00 NORTHBOUND

● BARRIER TO BE PLACED IN FUTURE PROJECT



TYPICAL SECTION NO. 6

U.S. 81

STA. 173+44.37 TO STA. 182+05.92 SOUTHBOUND STA. 173+00.00 TO STA. 180+98.55 NORTHBOUND
 STA. 191+89.12 TO STA. 200+81.92 SOUTHBOUND STA. 192+28.63 TO STA. 201+22.68 NORTHBOUND
 STA. 211+29.79 TO STA. 220+00.00 SOUTHBOUND STA. 212+53.71 TO STA. 221+05.00 NORTHBOUND

1 TOPSOIL NOTE:
 THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.
 THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.

2 DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

PRIME COAT WILL BE APPLIED IMMEDIATELY AFTER SUBGRADE IS COMPACTED TO GRADE.

SUBGRADE SHALL BE STABILIZED IN FUTURE SURFACING PROJECTS.

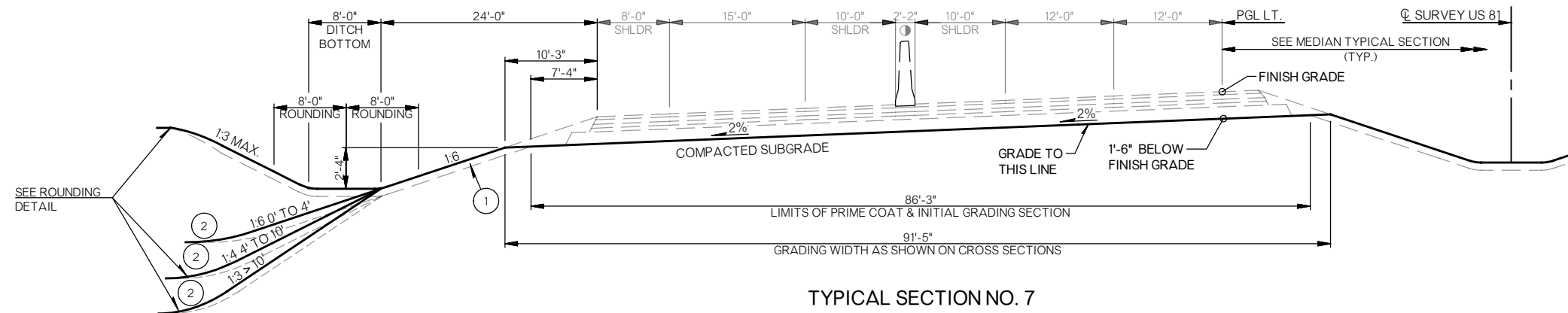
SURFACING SHOWN FOR INFORMATIONAL PURPOSES ONLY. SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS.

SEE GRADING AND PAVING LAYOUT SHEETS FOR ADDITIONAL TRANSITION AND TAPER INFORMATION AND CROSS SECTIONS FOR ADDITIONAL SUPERELEVATION AND CROSS SLOPE INFORMATION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

TYPICAL SECTIONS
 State Job No. 24428(04) Sheet No. 0006

US 81 REALIGNMENT
 GRADY COUNTY

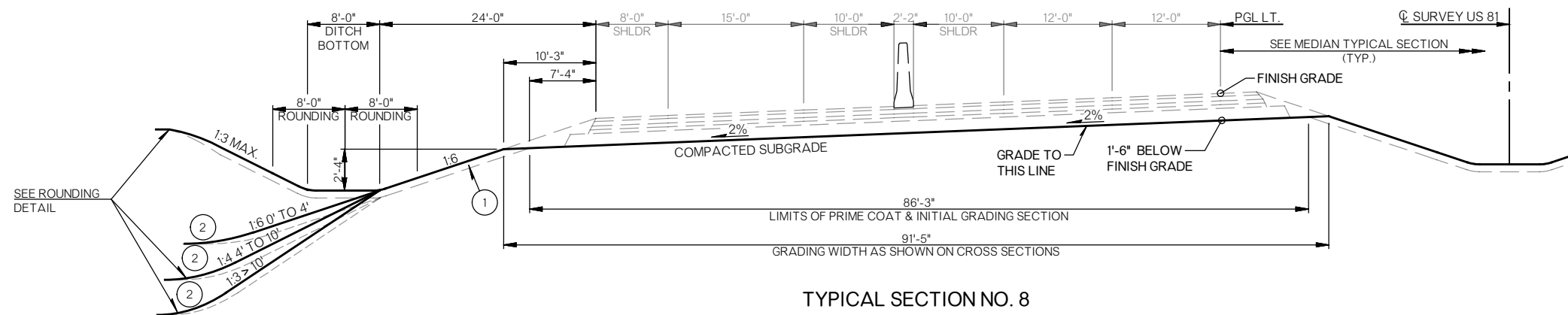


TYPICAL SECTION NO. 7

U.S. 81

STA. 220+00.00 AND TRANSITION TO TYPICAL NO. 2, 239+00.00 SOUTHBOUND STA. 221+05.00 AND TRANSITION TO TYPICAL NO. 2, STA. 238+50.00 NORTHBOUND

● BARRIER TO BE CONSTRUCTED IN FUTURE PROJECT



TYPICAL SECTION NO. 8

U.S. 81

STA. 182+05.92 TO STA. 191+89.12 SOUTHBOUND STA. 180+98.55 TO STA. 192+28.63 NORTHBOUND
STA. 200+81.92 TO STA. 211+29.79 SOUTHBOUND STA. 201+22.68 TO STA. 212+53.71 NORTHBOUND

1 TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.
THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.

2 DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

PRIME COAT WILL BE APPLIED IMMEDIATELY AFTER SUBGRADE IS COMPACTED TO GRADE.

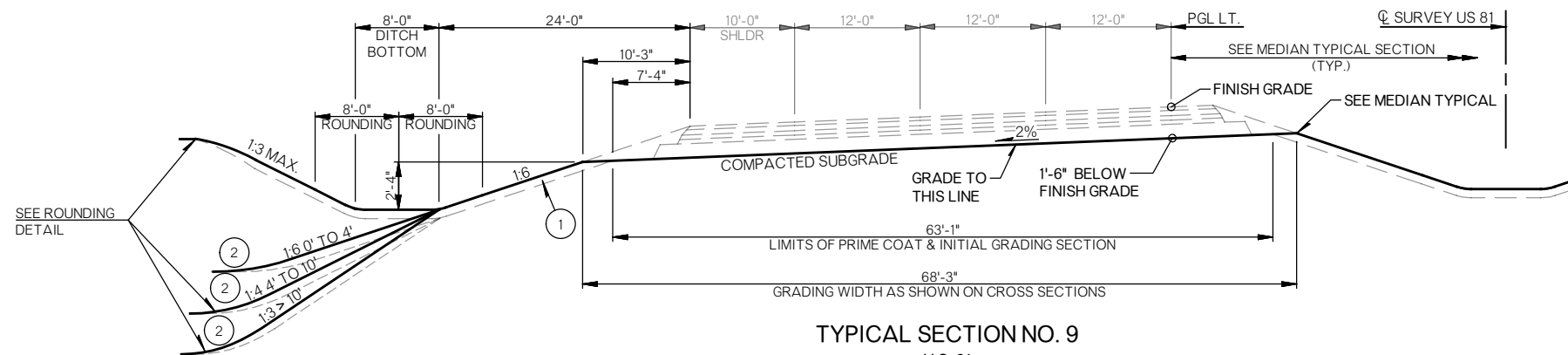
SUBGRADE SHALL BE STABILIZED IN FUTURE SURFACING PROJECTS.

SURFACING SHOWN FOR INFORMATIONAL PURPOSES ONLY. SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS.

SEE GRADING AND PAVING LAYOUT SHEETS FOR ADDITIONAL TRANSITION AND TAPER INFORMATION AND CROSS SECTIONS FOR ADDITIONAL SUPERELEVATION AND CROSS SLOPE INFORMATION.

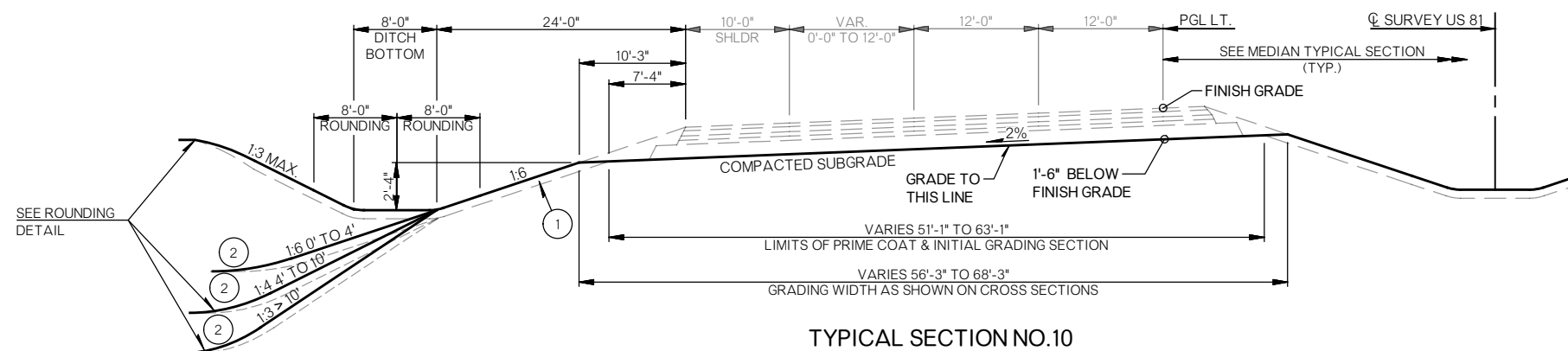
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

TYPICAL SECTIONS



TYPICAL SECTION NO. 9
U.S. 81

| | |
|---|---|
| STA. 286+64.61 TO STA. 321+78.07 SOUTHBOUND | STA. 302+07.25 TO STA. 321+78.07 NORTHBOUND |
| STA. 393+90.00 TO STA. 398+96.26 SOUTHBOUND | STA. 394+28.26 TO STA. 399+37.32 NORTHBOUND |
| STA. 424+59.49 TO STA. 426+68.51 SOUTHBOUND | STA. 425+74.85 TO STA. 433+89.60 NORTHBOUND |
| STA. 456+30.49 TO STA. 468+70.82 SOUTHBOUND | |



TYPICAL SECTION NO. 10
U.S. 81

| | |
|---|---|
| STA. 390+90.00 TO STA. 393+90.00 SOUTHBOUND | STA. 391+21.64 TO STA. 394+28.26 NORTHBOUND |
| STA. 426+68.51 TO STA. 428+65.46 SOUTHBOUND | STA. 433+89.60 TO STA. 436+90.34 NORTHBOUND |
| STA. 453+80.49 TO STA. 456+30.49 SOUTHBOUND | |

- ① TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

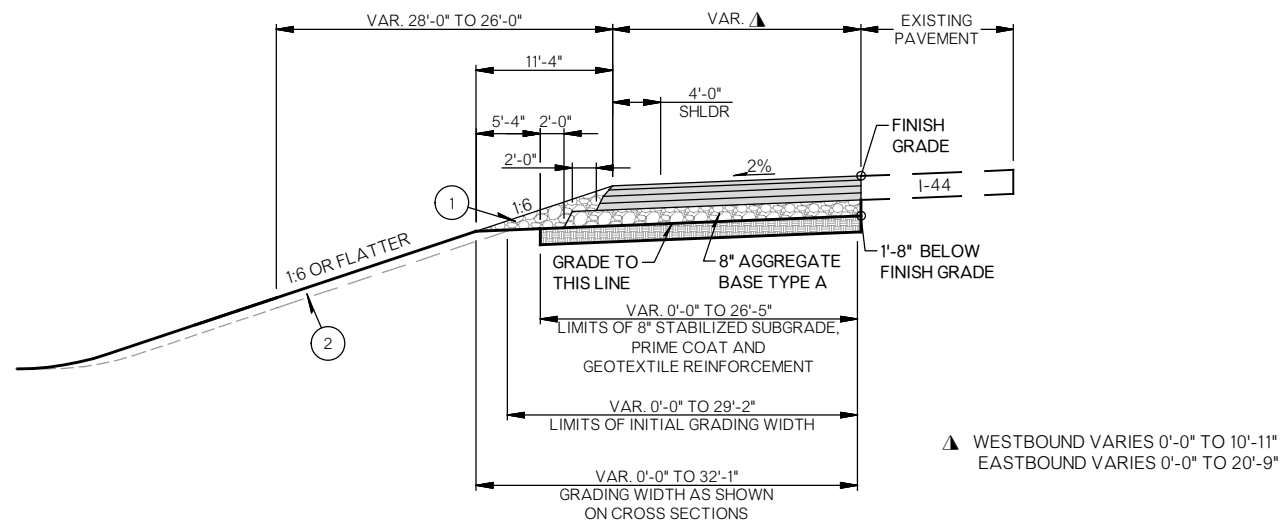
THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.
- ② DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.
- PRIME COAT WILL BE APPLIED IMMEDIATELY AFTER SUBGRADE IS COMPACTED TO GRADE.
- SUBGRADE SHALL BE STABILIZED IN FUTURE SURFACING PROJECTS.
- SURFACING SHOWN FOR INFORMATIONAL PURPOSES ONLY. SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS.
- SEE GRADING AND PAVING LAYOUT SHEETS FOR ADDITIONAL TRANSITION AND TAPER INFORMATION AND CROSS SECTIONS FOR ADDITIONAL SUPERELEVATION AND CROSS SLOPE INFORMATION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

TYPICAL SECTIONS

State Job No. 24428(04) Sheet No. 0008

US 81 REALIGNMENT
GRADY COUNTY

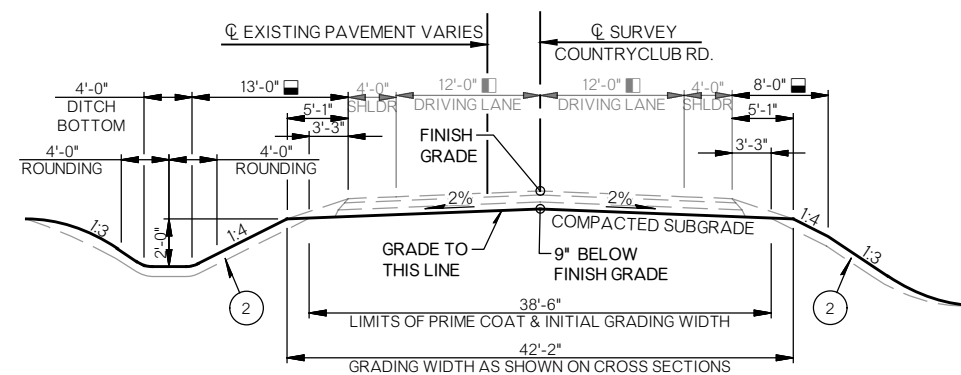


TYPICAL SECTION NO. 11

I-44 WIDENING

STA. 281+59.20 TO STA. 301+70.00 EASTBOUND
STA. 282+52.34 TO STA. 301+70.00 WESTBOUND

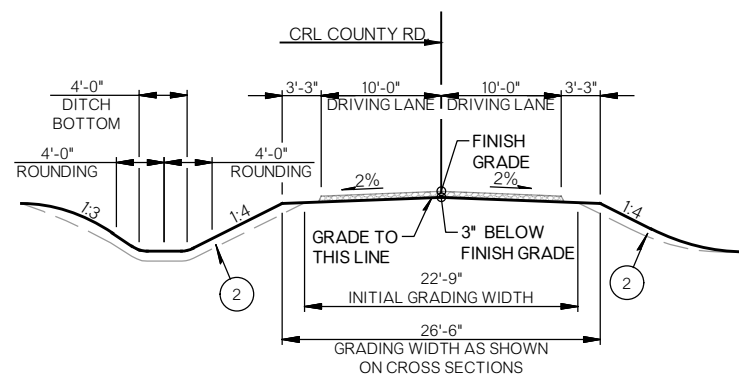
| PAVEMENT REQUIREMENTS | | |
|------------------------|--------------------------------------|--------------------------------------|
| 12" PAVEMENT STRUCTURE | 0'-0" TO 16'-9" DRIVING LANE | 4'-0" SHOULDER |
| SURFACE COURSE | 2" SUPERPAVE TYPE S4 (PG 64-22 OK) | 2" SUPERPAVE TYPE S4 (PG 64-22 OK) |
| BASE COURSE | 3" SUPERPAVE TYPE S3 (PG 64-22 OK) | 3" SUPERPAVE TYPE S3 (PG 64-22 OK) |
| | 3.5" SUPERPAVE TYPE S3 (PG 64-22 OK) | 3.5" SUPERPAVE TYPE S3 (PG 64-22 OK) |
| | 3.5" SUPERPAVE TYPE S3 (PG 64-22 OK) | 3.5" SUPERPAVE TYPE S3 (PG 64-22 OK) |



TYPICAL SECTION NO. 12

COUNTRY CLUB RD.

STA. 101+00.00 TO STA. 123+00.00



TYPICAL SECTION NO. 13

COUNTY RD.

STA. 300+41.76 TO STA. 327+40.20

| PAVEMENT REQUIREMENTS | |
|-----------------------|------------------------------------|
| 3" PAVEMENT STRUCTURE | 10'-0" DRIVING LANE |
| SURFACE COURSE | 3" TRAFFIC SURFACE COURSE TYPE "E" |

1 BACKFILL NOTE:
TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURE IN TBSC TYPE E.

1A BACKFILL NOTE:
TO BE BACKFILLED AS PART OF THE FINISHING OPERATIONS. COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

2 TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.

3 DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISH GRADE SHOULDER.

PRIME COAT WILL BE APPLIED IMMEDIATELY AFTER SUBGRADE IS COMPACTED TO GRADE.

SUBGRADE SHALL BE STABILIZED IN FUTURE SURFACING PROJECTS.

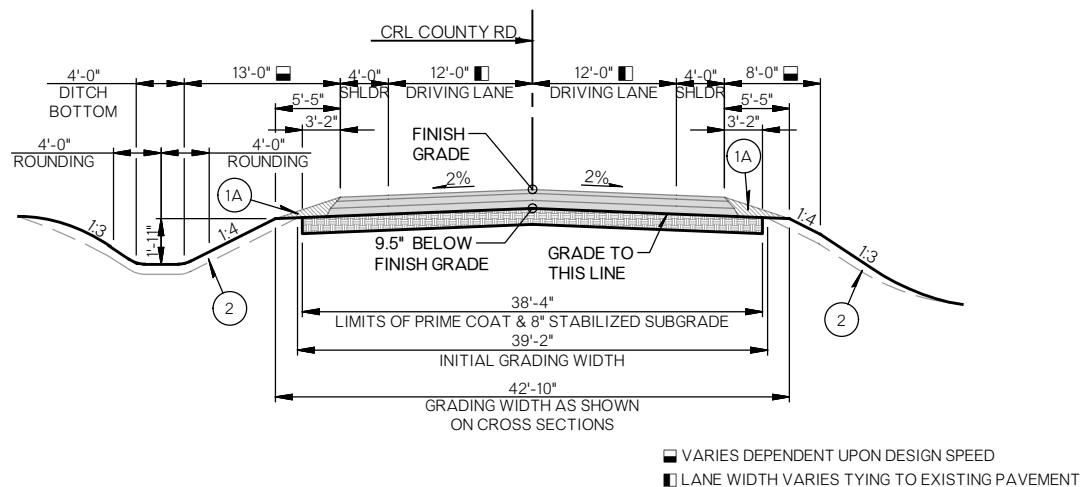
SURFACING SHOWN FOR INFORMATIONAL PURPOSES ONLY. SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS.

SEE GRADING AND PAVING LAYOUT SHEETS FOR ADDITIONAL TRANSITION AND TAPER INFORMATION AND CROSS SECTIONS FOR ADDITIONAL SUPERELEVATION AND CROSS SLOPE INFORMATION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

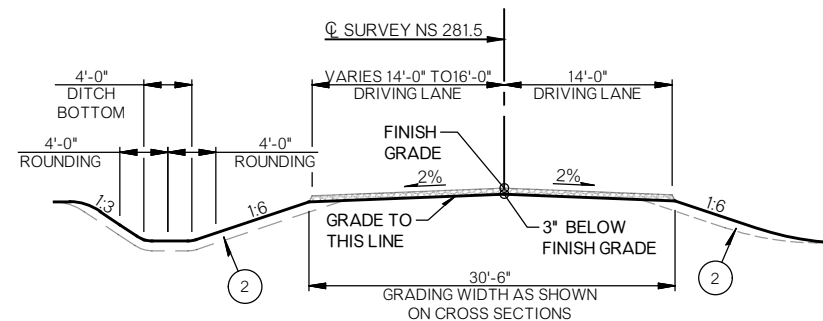
TYPICAL SECTIONS

US 81 REALIGNMENT
GRADY COUNTY



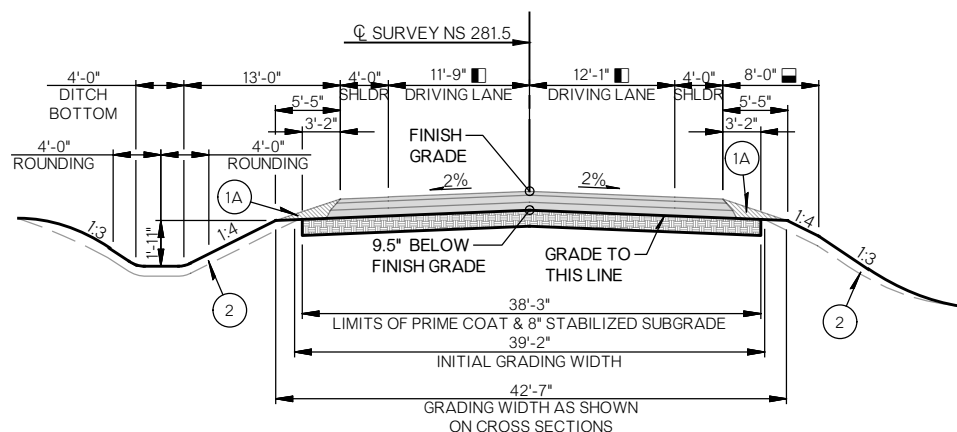
TYPICAL SECTION NO. 14
IOWA
STA. 53+80.00 TO STA. 62+65.00

| PAVEMENT REQUIREMENTS | | |
|-------------------------|--------------------------------------|--------------------------------------|
| 9.5" PAVEMENT STRUCTURE | 12'-0" DRIVING LANE | 4'-0" SHOULDER |
| SURFACE COURSE | 2" SUPERPAVE TYPE S4 (PG 64-22 OK) | 2" SUPERPAVE TYPE S4 (PG 64-22 OK) |
| BASE COURSE | 3.5" SUPERPAVE TYPE S3 (PG 64-22 OK) | 3.5" SUPERPAVE TYPE S3 (PG 64-22 OK) |
| | 4" SUPERPAVE TYPE S3 (PG 64-22 OK) | 4" SUPERPAVE TYPE S3 (PG 64-22 OK) |



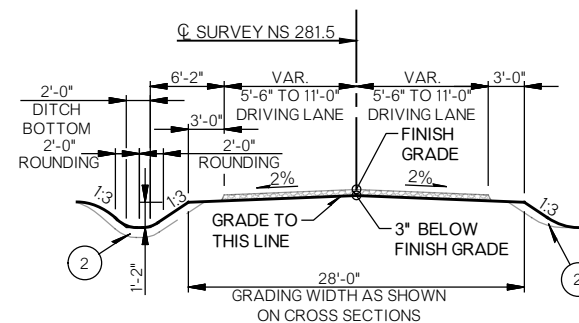
TYPICAL SECTION NO. 15
NS 281.5
STA. 102+00.79 TO STA. 102+58.10
STA. 103+41.62 TO 104+15.00

| PAVEMENT REQUIREMENTS | |
|-----------------------|------------------------------------|
| 3" PAVEMENT STRUCTURE | 14'-0" TO 16'-0" DRIVING LANE |
| SURFACE COURSE | 3" TRAFFIC SURFACE COURSE TYPE "E" |



TYPICAL SECTION NO. 16
NS 281.5
STA. 102+58.10 TO STA. 102+85.79
STA. 103+15.81 TO STA. 103+41.62

| PAVEMENT REQUIREMENTS | | |
|-------------------------|--------------------------------------|--------------------------------------|
| 9.5" PAVEMENT STRUCTURE | 12'-0" DRIVING LANE | 4'-0" SHOULDER |
| SURFACE COURSE | 2" SUPERPAVE TYPE S4 (PG 64-22 OK) | 2" SUPERPAVE TYPE S4 (PG 64-22 OK) |
| BASE COURSE | 3.5" SUPERPAVE TYPE S3 (PG 64-22 OK) | 3.5" SUPERPAVE TYPE S3 (PG 64-22 OK) |
| | 4" SUPERPAVE TYPE S3 (PG 64-22 OK) | 4" SUPERPAVE TYPE S3 (PG 64-22 OK) |



TYPICAL SECTION NO. 17
EW 135.5
STA. 11+96.45 TO STA. 25+40.69

| PAVEMENT REQUIREMENTS | |
|-----------------------|------------------------------------|
| 3" PAVEMENT STRUCTURE | 5'-6" TO 11'-0" DRIVING LANE |
| SURFACE COURSE | 3" TRAFFIC SURFACE COURSE TYPE "E" |

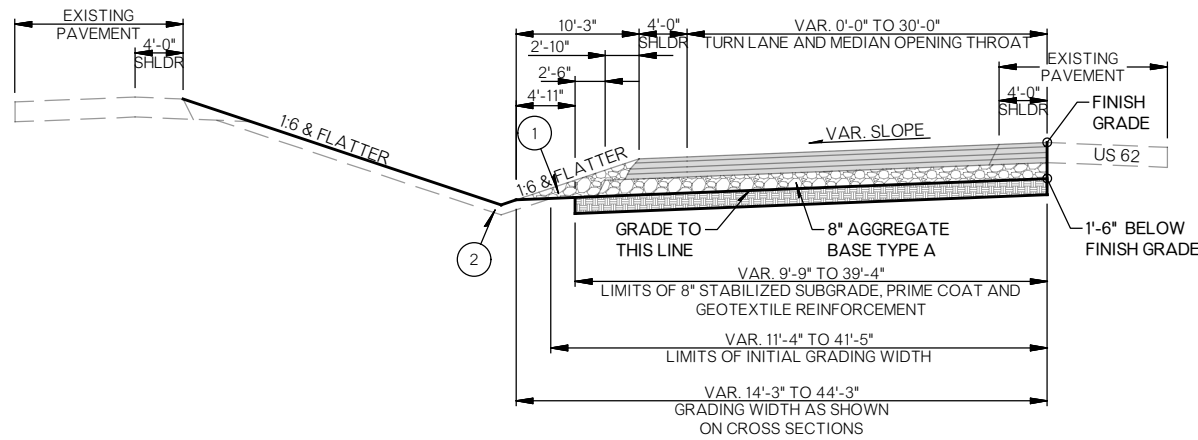
- 1 BACKFILL NOTE:
TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURE IN TBSC TYPE E.
- 1A BACKFILL NOTE:
TO BE BACKFILLED AS PART OF THE FINISHING OPERATIONS. COST TO BE INCLUDED IN OTHER ITEMS OF WORK.
- 2 TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.
- 3 DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISH GRADE SHOULDER.
- PRIME COAT WILL BE APPLIED IMMEDIATELY AFTER SUBGRADE IS COMPACTED TO GRADE.
- SUBGRADE SHALL BE STABILIZED IN FUTURE SURFACING PROJECTS.
- SURFACING SHOWN FOR INFORMATIONAL PURPOSES ONLY. SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS.
- SEE GRADING AND PAVING LAYOUT SHEETS FOR ADDITIONAL TRANSITION AND TAPER INFORMATION AND CROSS SECTIONS FOR ADDITIONAL SUPERELEVATION AND CROSS SLOPE INFORMATION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

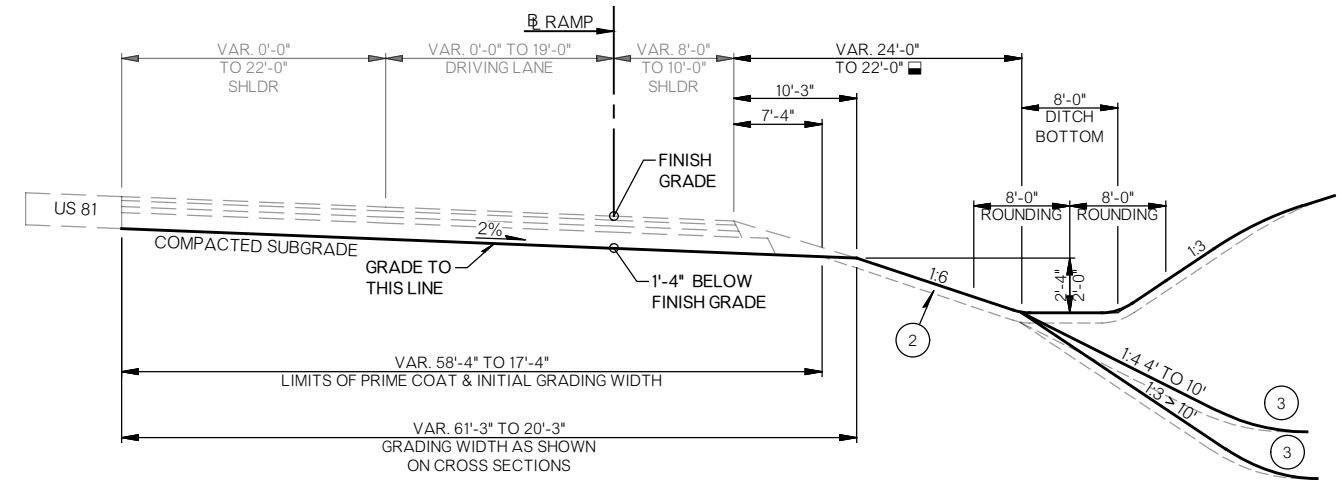
TYPICAL SECTIONS

US 81 REALIGNMENT
GRADY COUNTY



TYPICAL SECTION NO. 18
US 62
STA. 394+05.25 TO STA. 397+01.85

| PAVEMENT REQUIREMENTS | | |
|------------------------|--|--------------------------------------|
| 10" PAVEMENT STRUCTURE | 0'-0" TO 30'-0" TURN LANE & MEDIAN OPENING | 4'-0" SHOULDER |
| SURFACE COURSE | 2" SUPERPAVE TYPE S4 (PG 64-22 OK) | 2" SUPERPAVE TYPE S4 (PG 64-22 OK) |
| BASE COURSE | 3" SUPERPAVE TYPE S3 (PG 64-22 OK) | 3" SUPERPAVE TYPE S3 (PG 64-22 OK) |
| | 2.5" SUPERPAVE TYPE S3 (PG 64-22 OK) | 2.5" SUPERPAVE TYPE S3 (PG 64-22 OK) |
| | 2.5" SUPERPAVE TYPE S3 (PG 64-22 OK) | 2.5" SUPERPAVE TYPE S3 (PG 64-22 OK) |

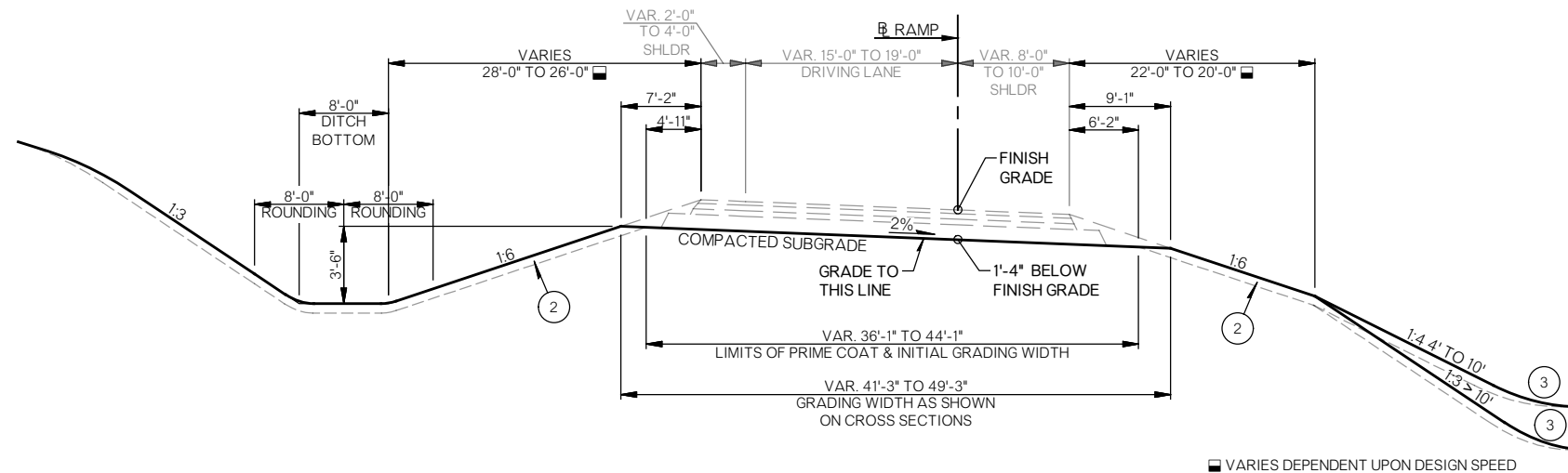


TYPICAL SECTION NO. 19

RAMP A STA. 142+15.57 TO STA. 145+46.83
 RAMP B STA. 254+97.35 TO STA. 263+72.12
 RAMP D STA. 248+61.81 TO STA. 264+50.00
 RAMP G STA. 1173+39.86 TO STA. 1182+00.07
 RAMP H STA. 193+33.21 TO STA. 196+05.98
 RAMP I STA. 329+50.48 TO STA. 331+97.66
 RAMP J STA. 2208+62.99 TO STA. 2217+31.21
 RAMP K STA. 3172+99.80 TO STA. 3180+96.79
 RAMP L STA. 224+38.62 TO STA. 226+79.52
 RAMP M STA. 298+37.48 TO STA. 301+17.77
 RAMP N STA. 4206+87.32 TO STA. 4215+37.31

RAMP O STA. 998+66.78 TO STA. 1014+77.45
 RAMP P STA. 1064+66.70 TO STA. 1070+06.58
 RAMP Q STA. 1090+52.93 TO STA. 1105+77.71
 RAMP R STA. 1169+90.78 TO STA. 1173+38.66
 RAMP S STA. 1207+76.56 TO STA. 1214+94.53
 RAMP T STA. 1259+98.58 TO STA. 1273+78.42
 RAMP U STA. 1298+76.72 TO STA. 1306+58.12
 RAMP V STA. 1360+46.00 TO STA. 1374+12.67
 RAMP CC STA. 461+68.17 TO STA. 473+69.68

VARIES DEPENDENT UPON DESIGN SPEED



TYPICAL SECTION NO. 20

RAMP A STA. 145+46.83 TO STA. 149+90.00
 RAMP A STA. 152+48.18 TO STA. 153+00.00
 RAMP B STA. 246+40.72 TO STA. 254+97.35
 RAMP D STA. 244+00.00 TO STA. 248+61.81
 RAMP G STA. 1182+00.07 TO STA. 1201+48.26
 RAMP H STA. 196+05.98 TO STA. 205+49.88
 RAMP I STA. 319+95.33 TO STA. 329+50.48
 RAMP J STA. 2187+06.96 TO STA. 2208+62.99
 RAMP K STA. 3180+96.79 TO STA. 3203+63.80
 RAMP L STA. 214+72.41 TO STA. 224+38.62
 RAMP M STA. 301+17.77 TO STA. 310+50.38
 RAMP N STA. 4185+10.80 TO STA. 4206+87.32
 RAMP O STA. 1014+77.45 TO STA. 1026+50.26
 RAMP P STA. 1057+00.57 TO STA. 1064+66.70
 RAMP Q STA. 1105+77.71 TO STA. 1111+06.73
 RAMP R STA. 1161+01.00 TO STA. 1169+90.78
 RAMP S STA. 1214+94.53 TO STA. 1225+27.60
 RAMP T STA. 1250+12.00 TO STA. 1259+98.58
 RAMP U STA. 1306+58.12 TO STA. 1316+53.91
 RAMP V STA. 1350+12.00 TO STA. 1360+46.00

VARIES DEPENDENT UPON DESIGN SPEED

1 BACKFILL NOTE:
TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURE IN TBSC TYPE E.

2 TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.
THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.

3 DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

PRIME COAT WILL BE APPLIED IMMEDIATELY AFTER SUBGRADE IS COMPACTED TO GRADE.

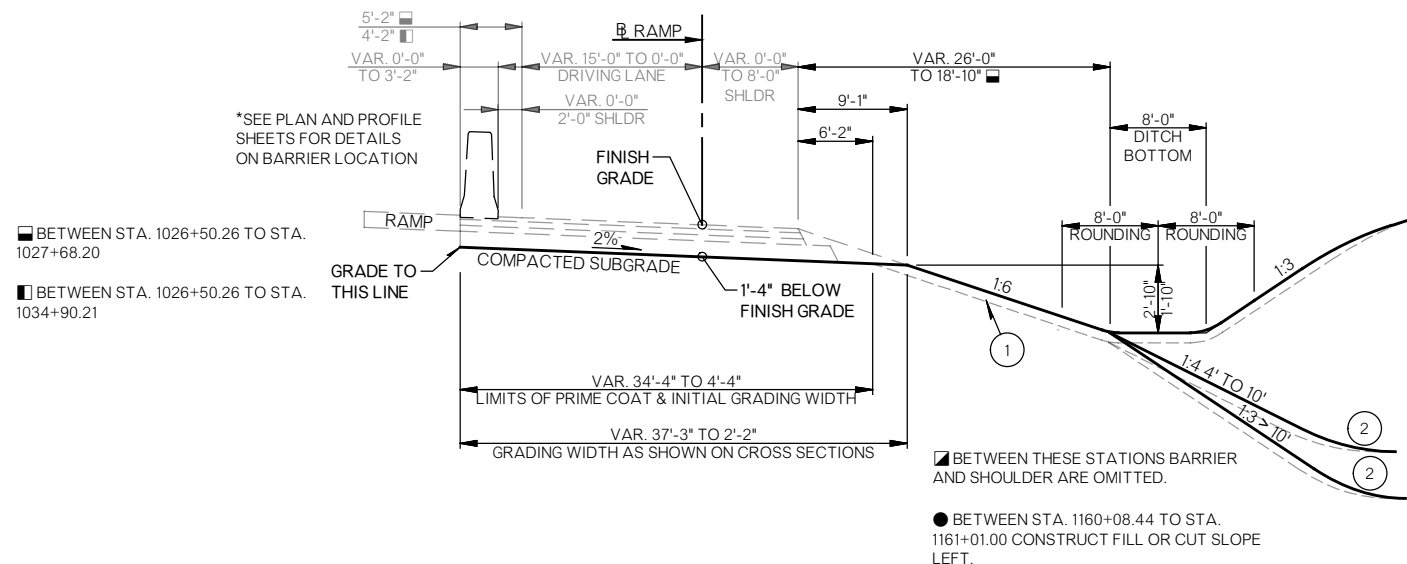
SUBGRADE SHALL BE STABILIZED IN FUTURE SURFACING PROJECTS.

SURFACING SHOWN FOR INFORMATIONAL PURPOSES ONLY. SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS.

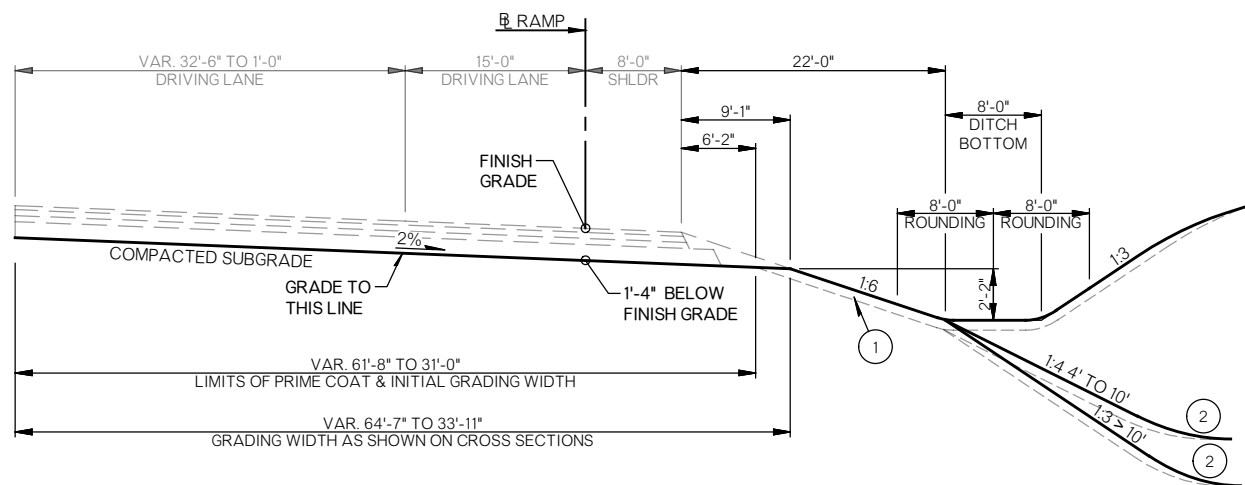
SEE GRADING AND PAVING LAYOUT SHEETS FOR ADDITIONAL TRANSITION AND TAPER INFORMATION AND CROSS SECTIONS FOR ADDITIONAL SUPERELEVATION AND CROSS SLOPE INFORMATION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

TYPICAL SECTIONS



TYPICAL SECTION NO. 21
 RAMP A EXTENSION STA. 243+00.00 TO STA. 244+82.34
 RAMP A STA. 149+90.00 TO STA. 151+33.98
 RAMP A STA. 151+33.98 TO STA. 152.48.18 ■
 RAMP O STA. 1026+50.26 TO STA. 1035+05.04 ■ ■
 RAMP R STA. 1150+16.00 TO STA. 1161+01.00 ●



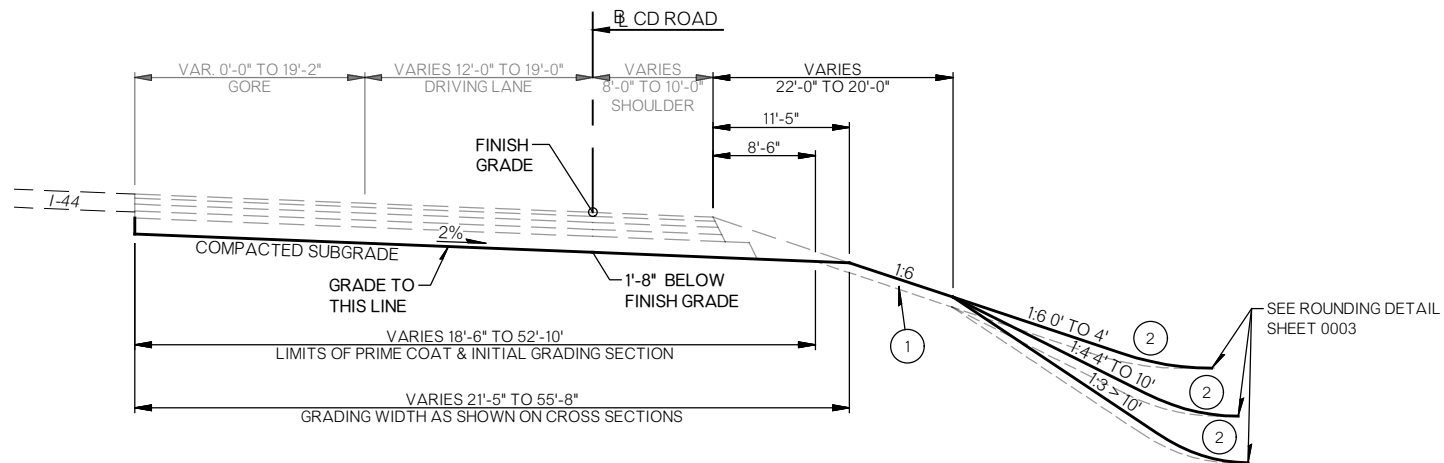
TYPICAL SECTION NO. 22
 RAMP B STA. 243+00.00 TO STA 246+40.72
 RAMP P STA. 1050+16.00 TO STA. 1057+00.57
 RAMP Q STA. 1111+06.73 TO STA. 1120+48.80

- 1 TOPSOIL NOTE:
 THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.
 THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.
- 2 DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.
- PRIME COAT WILL BE APPLIED IMMEDIATELY AFTER SUBGRADE IS COMPACTED TO GRADE.
- SUBGRADE SHALL BE STABILIZED IN FUTURE SURFACING PROJECTS.
- SURFACING SHOWN FOR INFORMATIONAL PURPOSES ONLY. SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS.
- SEE GRADING AND PAVING LAYOUT SHEETS FOR ADDITIONAL TRANSITION AND TAPER INFORMATION AND CROSS SECTIONS FOR ADDITIONAL SUPERELEVATION AND CROSS SLOPE INFORMATION.

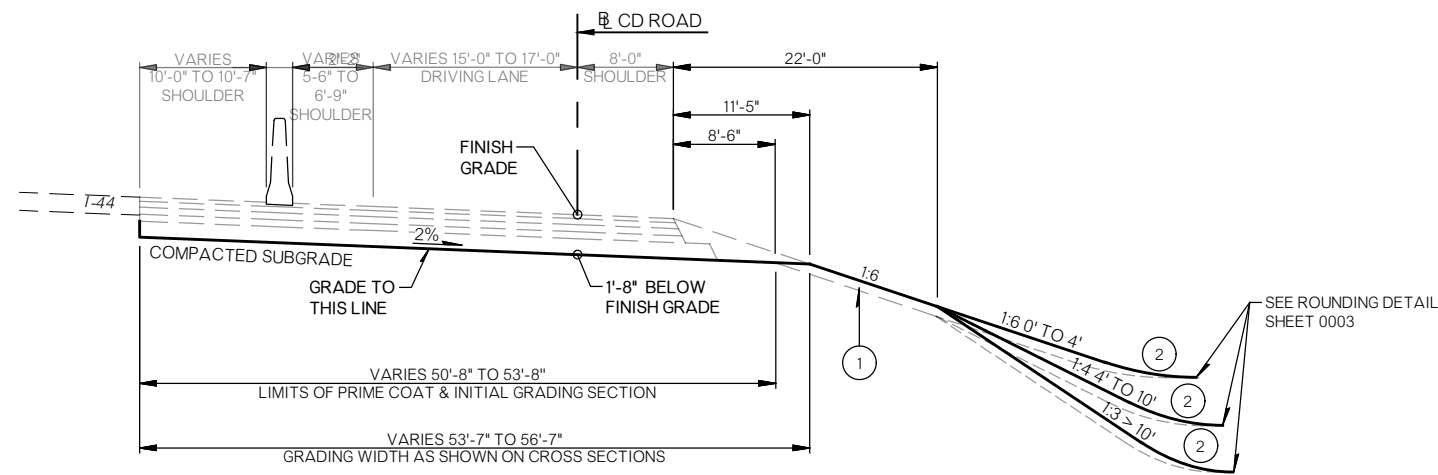
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

TYPICAL SECTIONS
 State Job No. 24428(04) Sheet No. 0012

US 81 REALIGNMENT
 GRADY COUNTY



TYPICAL SECTION NO. 23
COLLECTOR - DISTRIBUTOR ROADS - I44
STA. 251+85.00 TO STA. 265+05.14 EASTBOUND
STA. 316+17.14 TO STA. 332+82.12 EASTBOUND
STA. 245+65.00 TO STA. 262+30.73 WESTBOUND
STA. 315+90.95 TO STA. 328+93.69 WESTBOUND



TYPICAL SECTION NO. 24
COLLECTOR - DISTRIBUTOR ROADS - I44
STA. 265+05.14 TO STA. 266+55.76 EASTBOUND
STA. 314+21.40 TO STA. 316+17.14 EASTBOUND
STA. 262+30.73 TO STA. 264+25.67 WESTBOUND
STA. 314+22.92 TO STA. 315+90.95 WESTBOUND

1 TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.

2 DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

PRIME COAT WILL BE APPLIED IMMEDIATELY AFTER SUBGRADE IS COMPACTED TO GRADE.

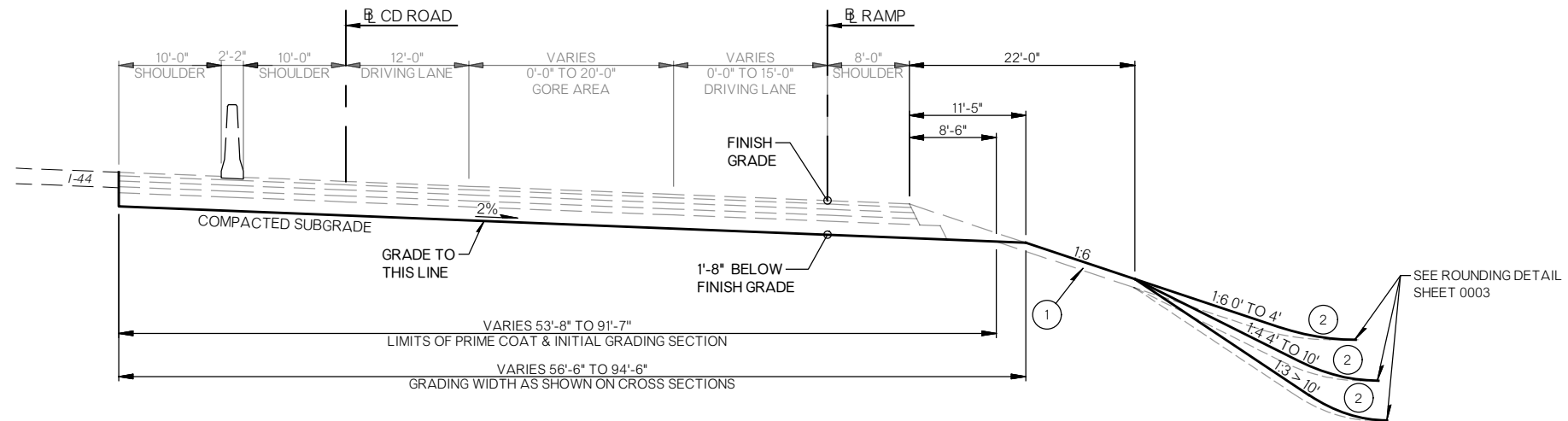
SUBGRADE SHALL BE STABILIZED IN FUTURE SURFACING PROJECTS.

SURFACING SHOWN FOR INFORMATIONAL PURPOSES ONLY. SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS.

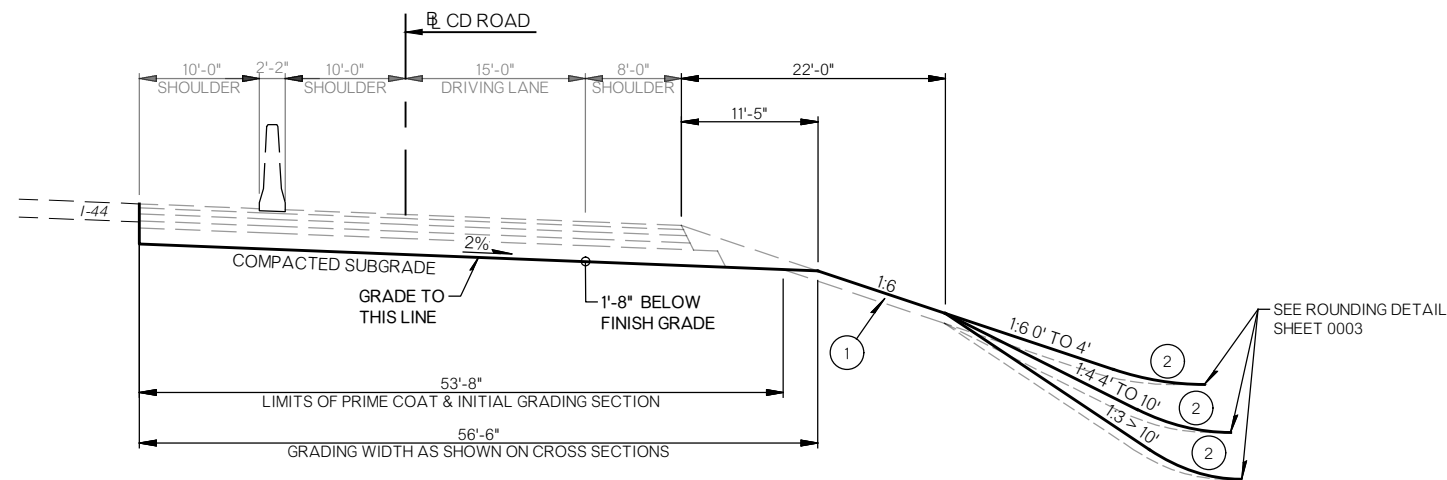
SEE GRADING AND PAVING LAYOUT SHEETS FOR ADDITIONAL TRANSITION AND TAPER INFORMATION AND CROSS SECTIONS FOR ADDITIONAL SUPERELEVATION AND CROSS SLOPE INFORMATION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

TYPICAL SECTIONS



TYPICAL SECTION NO. 25
COLLECTOR - DISTRIBUTOR ROADS - 144
STA. 264+94.02 TO STA. 275+93.08 EASTBOUND
STA. 285+08.56 TO STA. 294+38.49 EASTBOUND
STA. 305+78.09 TO STA. 315+15.89 EASTBOUND
STA. 264+65.23 TO STA. 273+76.07 WESTBOUND
STA. 285+07.96 TO STA. 294+62.68 WESTBOUND
STA. 305+84.85 TO STA. 315+03.36 WESTBOUND



TYPICAL SECTION NO. 26
COLLECTOR - DISTRIBUTOR ROADS - 144
STA. 275+93.08 TO STA. 285+08.56 EASTBOUND
STA. 294+38.49 TO STA. 305+78.09 EASTBOUND
STA. 273+76.07 TO STA. 285+07.96 WESTBOUND
STA. 294+62.68 TO STA. 305+84.85 WESTBOUND

① TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.
THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.

② DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

PRIME COAT WILL BE APPLIED IMMEDIATELY AFTER SUBGRADE IS COMPACTED TO GRADE.

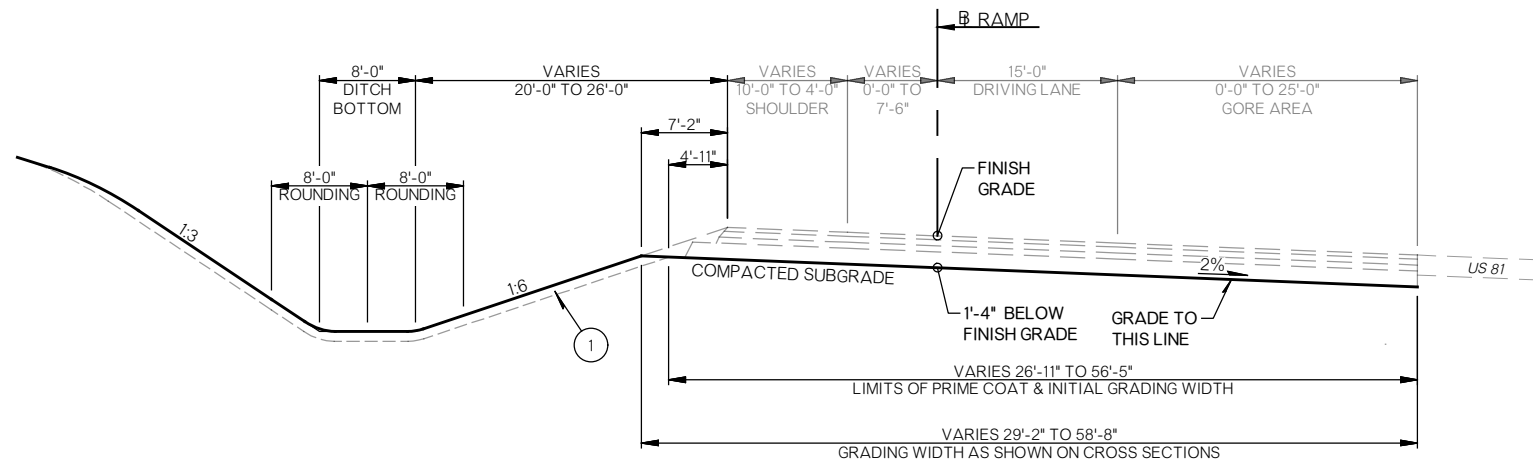
SUBGRADE SHALL BE STABILIZED IN FUTURE SURFACING PROJECTS.

SURFACING SHOWN FOR INFORMATIONAL PURPOSES ONLY. SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS.

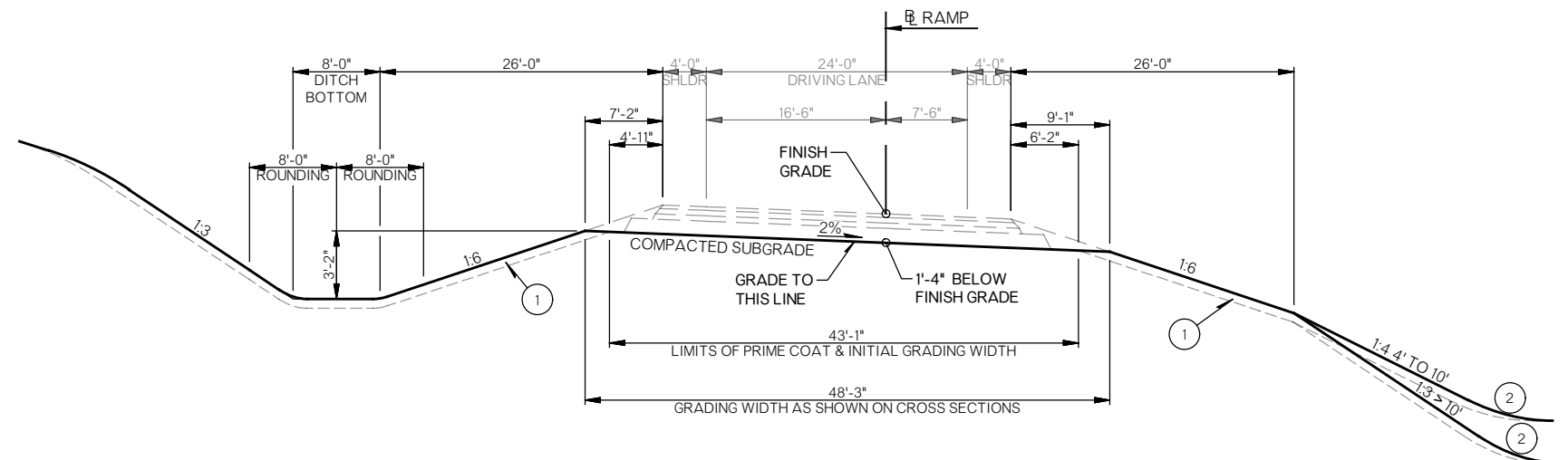
SEE GRADING AND PAVING LAYOUT SHEETS FOR ADDITIONAL TRANSITION AND TAPER INFORMATION AND CROSS SECTIONS FOR ADDITIONAL SUPERELEVATION AND CROSS SLOPE INFORMATION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

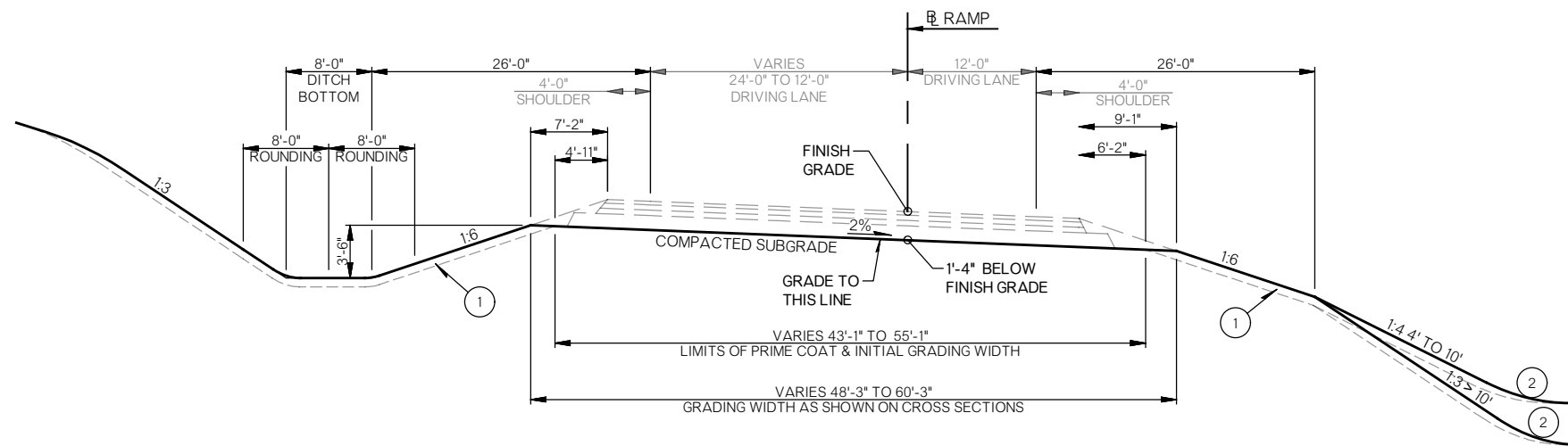
TYPICAL SECTIONS



TYPICAL SECTION NO. 27
RAMP AA STA. 468+70.82 TO STA. 473+73.38
RAMP CC STA. 473+69.68 TO STA. 474+33.00



TYPICAL SECTION NO. 28
RAMP AA STA. 473+74.13 TO STA. 488+06.02
RAMP CC STA. 474+33.00 TO STA. 488+12.03



TYPICAL SECTION NO. 29
US 81 INTERIM CONN. 2 STA. 489+08.00 TO STA. 500+00.00

① TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.

② DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

PRIME COAT WILL BE APPLIED IMMEDIATELY AFTER SUBGRADE IS COMPACTED TO GRADE.

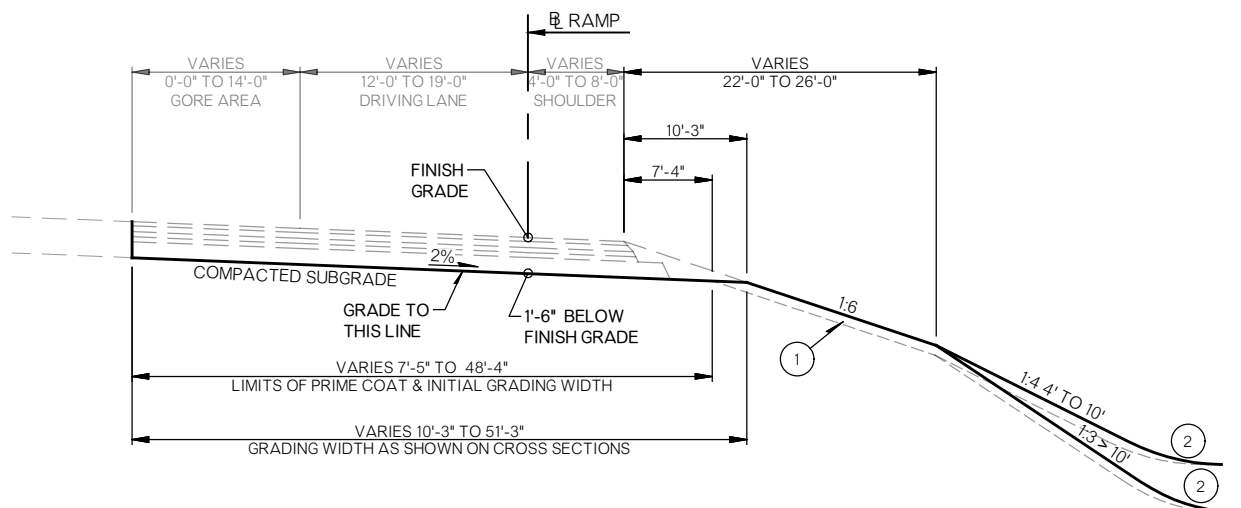
SUBGRADE SHALL BE STABILIZED IN FUTURE SURFACING PROJECTS.

SURFACING SHOWN FOR INFORMATIONAL PURPOSES ONLY. SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS.

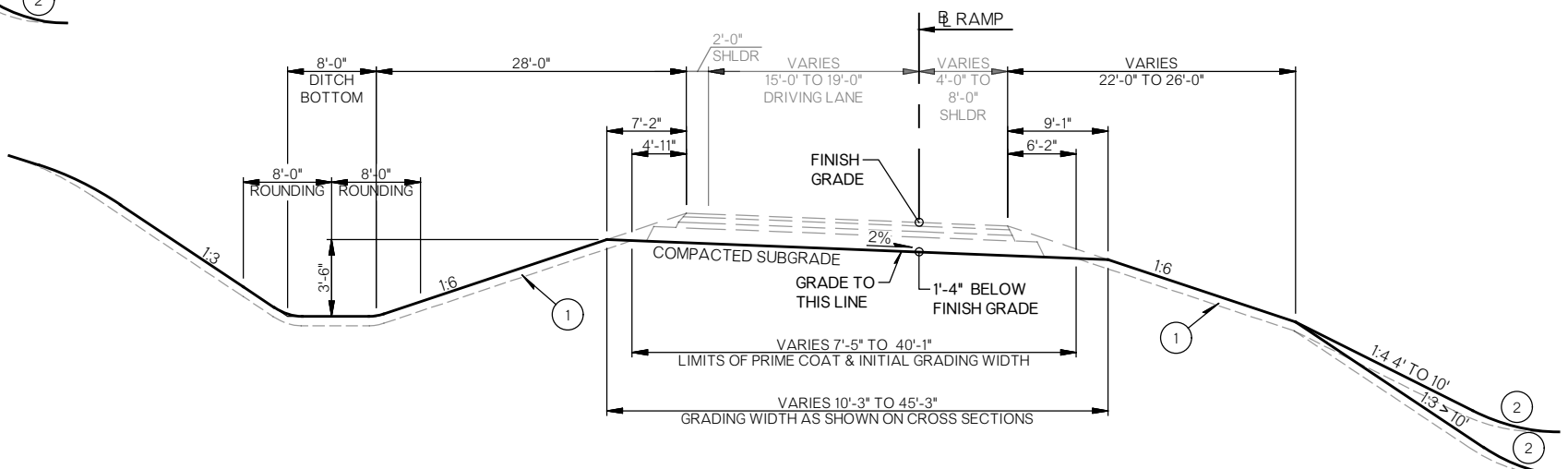
SEE GRADING AND PAVING LAYOUT SHEETS FOR ADDITIONAL TRANSITION AND TAPER INFORMATION AND CROSS SECTIONS FOR ADDITIONAL SUPERELEVATION AND CROSS SLOPE INFORMATION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

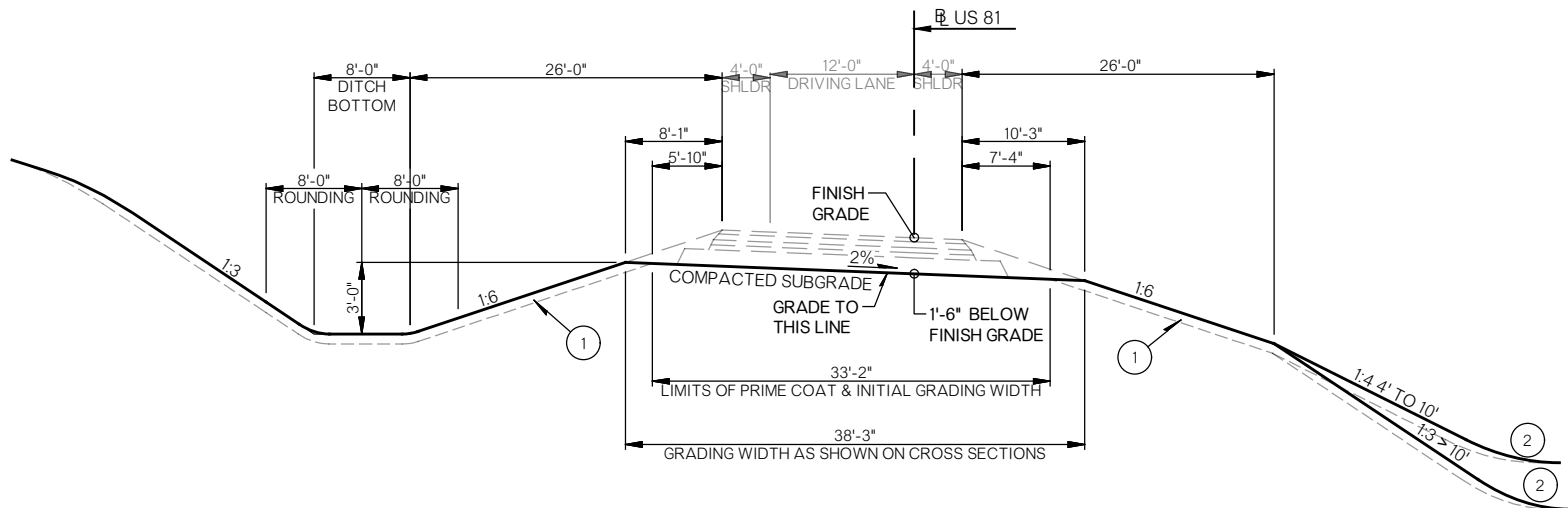
TYPICAL SECTIONS



TYPICAL SECTION NO. 30
 RAMP Q INTERIM STA. 1076+54.10 TO STA. 1091+66.85
 RAMP R INTERIM STA. 1091+66.85 TO STA. 1186+57.08
 RAMP U INTERIM STA. 1291+87.82 TO STA. 1301+75.38
 RAMP V INTERIM STA. 1366+15.83 TO STA. 1379+79.43



TYPICAL SECTION NO. 31
 RAMP Q INTERIM STA. 1091+66.85 TO STA. 1098+98.81
 RAMP R INTERIM STA. 1167+11.10 TO STA. 1172+60.45
 RAMP U INTERIM STA. 1301+75.38 TO STA. 1307+00.00
 RAMP V INTERIM STA. 1360+62.50 TO STA. 1366+15.83



TYPICAL SECTION NO. 32
 US 81 INTERIM CONN. STA. 169+13.00 TO STA. 186+80.91

- ① TOPSOIL NOTE:
 THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.
 THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.
- ② DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.
- PRIME COAT WILL BE APPLIED IMMEDIATELY AFTER SUBGRADE IS COMPACTED TO GRADE.
- SUBGRADE SHALL BE STABILIZED IN FUTURE SURFACING PROJECTS.
- SURFACING SHOWN FOR INFORMATIONAL PURPOSES ONLY. SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS.
- SEE GRADING AND PAVING LAYOUT SHEETS FOR ADDITIONAL TRANSITION AND TAPER INFORMATION AND CROSS SECTIONS FOR ADDITIONAL SUPERELEVATION AND CROSS SLOPE INFORMATION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

TYPICAL SECTIONS

US 81 REALIGNMENT
GRADY COUNTY

M:\W2M Projects\Project Files\M179\Final Design F & G\AB01-2442804-PAY QUANTITIES AND GENERAL NOTES.dwg 3/4/2020 2:47:41 PM

GENERAL NOTES (BRIDGE "F")

| | | |
|-------------|-----------|------|
| DESCRIPTION | REVISIONS | DATE |
| | | |

SPECIFICATIONS:

COMPLY WITH THE REQUIREMENTS OF THE ODOT 2019 STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EXCEPT AS MODIFIED BY THE PLANS AND SPECIAL PROVISIONS.

ABUTMENT PILING CAPACITY:

THE FACTORED REACTION FOR EACH HP12x53 PILE AT EACH ABUTMENT ON BRIDGE "F" IS 76.9 TONS.

THE FOLLOWING FORMULA (GATES EQUATION) SHALL BE USED TO DETERMINE THE AXIAL LOAD RESISTANCE OF THE DRIVEN FOUNDATION PILES:

$$\text{AXIAL LOAD RESISTANCE} = \phi [(0.875 \sqrt{E} \text{ LOG}_{10}(10N)) - 50] \quad (\text{TONS})$$

WHERE:

- ϕ = RESISTANCE FACTOR OF 0.4
- E = ENERGY PRODUCED BY THE HAMMER PER BLOW IN FOOT-POUNDS. FOR GRAVITY AND SINGLE ACTING DIESEL HAMMERS, THE VALUE IS BASED ON THE ACTUAL RAM STROKE OBSERVED IN THE FIELD AND MEASURED IN FEET MULTIPLIED BY THE RAM WEIGHT IN POUNDS.
- N = AVERAGE NUMBER OF HAMMER BLOWS PER INCH OF PILE PENETRATION FOR THE LAST 10 TO 20 BLOWS DELIVERED TO THE PILE HEAD.

THE ABOVE FORMULA IS ONLY APPLICABLE WHEN:

- THE PILE DRIVING HAMMER HAS A FREE FALL (GRAVITY & SINGLE ACTING HAMMERS ONLY).
- THE HEAD OF THE PILE IS NOT BROOMED, CRUSHED OR OTHERWISE DAMAGED.
- THE PENETRATION IS QUICK AND UNIFORM.
- THERE IS NO APPRECIABLE REBOUND OF THE HAMMER, AND
- A FOLLOWER IS NOT USED.

THE NUMBER OF BLOWS PER INCH OF PILE PENETRATION MAY BE MEASURED EITHER DURING INITIAL DRIVING OR BY RE-DRIVING WITH A WARM HAMMER OPERATED AT FULL ENERGY AFTER A PILE SET PERIOD, AS DETERMINED BY THE ENGINEER.

IF WATER JETS ARE USED IN CONNECTION WITH THE DRIVING, DETERMINE THE AXIAL LOAD RESISTANCE BY THE FORMULA SHOWN ONLY AFTER THE JETS HAVE BEEN WITHDRAWN.

PILE DRIVING EQUIPMENT:

USE A PILE DRIVING HAMMER OF THE SIZE AND TYPE CAPABLE OF CONSISTENTLY DELIVERING THE EFFECTIVE DYNAMIC ENERGY SUFFICIENT TO DRIVE THE PILES TO THE REQUIRED TIP ELEVATION AND TO ACHIEVE THE FACTORED PILE REACTION WITHOUT EXCEEDING THE LIMITATIONS SET ON THE ALLOWABLE DRIVING STRESSES IN ACCORDANCE WITH SECTION 514.03.A.2.

SLAB HAUNCHES:

PLAN QUANTITIES FOR CLASS "AA" CONCRETE INCLUDE 38.4 C.Y. FOR HAUNCHES OVER P.C. BEAMS.

BRIDGE SLAB AND CONSTRUCTION JOINTS:

FOR NOTES REGARDING THE BRIDGE SLAB AND CONSTRUCTION JOINTS, SEE SHEET NO. B029.

PENETRATING WATER REPELLENT SURFACE TREATMENT:

A PENETRATING WATER REPELLENT SURFACE TREATMENT SHALL BE APPLIED TO THE FOLLOWING CONCRETE SURFACES OF THE BRIDGE:

- (A) THE TOP AND OUTSIDE EDGE OF SLAB CANTILEVER, OUTSIDE OF PARAPETS, AND THE UNDERSIDE CANTILEVER PORTION OF THE BRIDGE SLAB.
- (B) THE OUTER FACE AND BOTTOM OF THE EXTERIOR P.C. BEAMS.
- (C) THE ROADWAY FACE, TOP AND OUTSIDE FACE OF THE F-SHAPED PARAPETS ON THE BRIDGE SLAB.
- (D) THE ROADWAY FACE AND TOP OF THE F-SHAPED PARAPETS ON THE APPROACH SLABS.
- (E) THE ROADWAY FACES AND TOP OF THE CONCRETE LONGITUDINAL BARRIER ON THE BRIDGE AND APPROACH SLABS.
- (F) THE FRONT FACE OF THE BACKWALL, TOP OF BRIDGE SEAT AND ALL SURFACES OF THE PEDESTALS AT THE ABUTMENTS. THE FRONT FACE AND ENDS OF THE BRIDGE SEAT AND THE OUTSIDE SURFACES OF THE WINGS FROM A LINE EVEN WITH THE BACK FACE OF THE BACKWALL TOWARD THE BRIDGE AND ABOVE THE FINISHED GROUND SURFACE OR TOP OF SLOPE WALL.
- (G) THE TOP OF THE PIER CAPS INCLUDING ALL SURFACES OF THE PEDESTALS, AND ALL VERTICAL SURFACES OF THE PIER CAPS.
- (H) ALL SURFACES OF THE PIER COLUMNS ABOVE THE FINISHED GROUND SURFACE OR TOP OF SLOPE WALL.

APPROACH SLABS:

CLASS AA CONCRETE SHALL BE USED IN THE APPROACH SLABS. THE QUANTITY GIVEN IS BASED ON THE ACTUAL SQUARE YARDS OF THE APPROACH SLABS. THE CONTRACT UNIT PRICE FOR APPROACH SLAB SHALL BE FULL COMPENSATION FOR ALL CONCRETE, REINFORCING STEEL (INCLUDING FS2, FS6 AND CB1 BARS IN PARAPETS AND CONCRETE LONGITUDINAL BARRIERS), BACKER RODS, RAPID CURE JOINT SEALANT, POLYSTYRENE, LABOR, EQUIPMENT AND OTHER INCIDENTALS NECESSARY TO COMPLETE THE WORK AS SPECIFIED IN THE PLANS.

PERFORATED AND NON-PERFORATED PIPE UNDERDRAINS:

EXTENT, LOCATION AND DEPTH OF 6" NON-PERFORATED PIPE UNDERDRAIN MAY BE ADJUSTED BY THE ENGINEER DURING CONSTRUCTION. ALL COSTS OF PIPE UNDERDRAIN COVER MATERIAL (BOTH FINE AND COARSE, FOR ABUTMENTS AND SLOPE WALLS) (EST. AT 367 CY), TRENCH EXCAVATION (EST. AT 116 CY) AND STANDARD BEDDING MATERIAL (EST. AT 115 CY), INCLUDING FILTER FABRIC, EQUIPMENT AND LABOR FOR THEIR INSTALLATION SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE BID FOR 6" PERFORATED PIPE UNDERDRAIN ROUND AND 6" NON-PERF. PIPE UNDERDRAIN RND. INSTALLATION SHALL BE AS SHOWN ON THE PLANS AND STD. PUD-3.

STAY-IN-PLACE SLAB FORMS:

STAY-IN-PLACE STEEL SLAB FORMS MAY BE USED IF THE MINIMUM SLAB THICKNESS OF 8" IS MAINTAINED BY MEASURING FROM THE TOP OF THE SLAB TO THE TOP PORTION OF THE STEEL CORRUGATION. LIMIT THE COMBINED WEIGHT OF THE FORMS AND ANY ADDITIONAL CONCRETE NECESSITATED BY THE USE OF STAY-IN-PLACE FORMS TO A MAXIMUM OF 5 PSF IN EXCESS OF THE ORIGINAL PLAN WEIGHT OF THE BRIDGE FLOOR. PREFORMED CORRUGATION FILLER, COMPOSED OF POLYSTYRENE, OR ANY OTHER FILLER MATERIAL, IF USED, SHALL BE BONDED TO THE STAY-IN-PLACE FORMS.

ALL COSTS ASSOCIATED WITH THE USE OF STAY-IN-PLACE STEEL SLAB FORMS INCLUDING ALL MATERIALS, LABOR, EQUIPMENT AND INCIDENTALS SHALL BE INCLUDED IN THE PRICE BID FOR CLASS AA CONCRETE. PAYMENTS FOR MATERIAL ON HAND WILL NOT BE PROCESSED FOR STAY-IN-PLACE FORMS. FOR ADDITIONAL INFORMATION CONCERNING THE USE OF STAY-IN-PLACE FORMS, SEE SECTION 502 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR MAY SUBSTITUTE STAY-IN-PLACE PRESTRESSED CONCRETE DECK FORMS, AT NO ADDITIONAL COST TO THE DEPARTMENT, IF THE FOLLOWING CONDITIONS ARE MET:

1. THE BRIDGE ENGINEER APPROVES SHOP DRAWINGS AND STRUCTURAL CALCULATIONS FOR THE FORMS SUBMITTED BY THE CONTRACTOR.
2. THE BRIDGE ENGINEER APPROVES NEW STRUCTURAL DESIGN, STRUCTURAL CALCULATIONS, AND A NEW REINFORCING SCHEDULE FOR THE DECK SLAB SUBMITTED BY THE CONTRACTOR.
3. SHOP DRAWINGS, NEW DECK SLAB REINFORCING SCHEDULE, STRUCTURAL DESIGNS, AND CALCULATIONS ARE PREPARED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF OKLAHOMA.

STRUCTURAL STEEL:

ALL STRUCTURAL STEEL SHALL BE AASHTO M270 (GRADE 50W), UNLESS OTHERWISE NOTED.

STAINLESS STEEL ANCHOR PLATES, CONTACT PLATES, ANCHOR BOLTS, NUTS, WASHERS AND WELD MATERIAL SHALL BE PROVIDED IN ACCORDANCE WITH SECTIONS 507, 724.03 AND 724.05 OF THE STANDARD SPECIFICATIONS, AND PER THE ANSI/AWS D1.6 STRUCTURAL WELDING CODE FOR STAINLESS STEEL.

ANCHOR PLATES FOR FIXED AND EXPANSION BEARINGS SHALL BE MATCH MARKED, SHIPPED LOOSE, AND FIELD WELDED IN ORDER THAT ANY NECESSARY MINOR HORIZONTAL ADJUSTMENT OF THE ANCHOR PLATE CAN BE MADE.

ALL SHOP AND FIELD WELDING SHALL BE ARC WELDING AND SHALL BE DONE IN ACCORDANCE WITH THE CURRENT ANSI/AWS D1.5 BRIDGE WELDING CODE AND THE ANSI/AWS D1.6 STRUCTURAL WELDING CODE FOR STAINLESS STEEL. FIELD WELDERS SHALL BE PRE-QUALIFIED BY THE OKLAHOMA DEPARTMENT OF TRANSPORTATION.

METAL USED IN FIELD WELDS WILL NOT BE MEASURED FOR PAYMENT.

DRAINS AT END OF BRIDGE:

ASPHALT SHOULDER WIDENING, CURBS, SLOPE DRAINS AND SPLASH BASINS SHALL BE CONSTRUCTED AS SHOWN ON SHEET NO. B038.

ASPHALT SHOULDER WIDENING ALONG THE BRIDGE GUARDRAIL SHALL BE IN ACCORDANCE WITH STDS. THRI-1 AND GHW1-1, EXCEPT AS SHOWN IN THE PLANS. ALL COSTS OF ASPHALT SHOULDER WIDENING SHALL BE INCLUDED IN THE ROADWAY PAY ITEMS.

THERE IS AN ESTIMATED 13.2 CUBIC YARDS OF CLASS "C" CONCRETE REQUIRED TO CONSTRUCT THE SLOPE DRAINS, SPLASH BASINS AND 6" CONCRETE CURBS AT THE ENDS OF THE BRIDGE. ALL COSTS FOR THE SLOPE DRAINS, SPLASH BASINS, 6" CONCRETE CURBS, CONCRETE, REINFORCING STEEL, EXCAVATIONS AND BACKFILL INCLUDING MATERIALS, LABOR, EQUIPMENT AND INCIDENTALS NECESSARY TO COMPLETE THE WORK AS SHOWN IN THE PLANS SHALL BE INCLUDED IN THE PRICE BID FOR "CLASS C CONCRETE", PER C.Y.

| 24428(04) | | | | |
|--|------|--|-----------|-----------|
| PAY QUANTITIES | | | | |
| 0200 BRIDGE "F" - NBI XXXXX - 55'-99'-99'-55' PRESTRESSED CONCRETE BEAM SPANS; 38'-0" AND 42'-0" CLR. RDWYS. W/ CONC. PARAPETS (FSHP-42) SKEWED 2'39'35.10" RF | | | | |
| ITEM NO. | | DESCRIPTION | UNITS | QUANTITY |
| 501(B) | 1307 | SUBSTRUCTURE EXCAVATION COMMON | (1) CY | 310.00 |
| 501(G) | 6309 | CLSM BACKFILL | (1) CY | 406.70 |
| 503(A) | 1313 | PRESTRESSED CONCRETE BEAMS (TYPE IV) | (1) LF | 2742.00 |
| 504(A) | 1304 | APPROACH SLAB | (1) SY | 532.50 |
| 504(B) | 1305 | SAW-CUT GROOVING | (1) SY | 3238.00 |
| 504(C) | 6250 | SEALED EXPANSION JOINT | (1) LF | 83.26 |
| 504(E) | 6190 | 42" F-SHAPED PARAPET | (1) LF | 728.20 |
| 506(A) | 1322 | STRUCTURAL STEEL | (1) LB | 4060.00 |
| 507(A) | 6170 | STAINLESS STEEL FIXED BEARING ASSEMBLY | (1)(2) EA | 18.00 |
| 507(B) | 6174 | STAINLESS STEEL EXPANSION BEARING ASSEMBLY | (1)(2) EA | 54.00 |
| 509(A) | 1326 | CLASS AA CONCRETE | (1) CY | 801.50 |
| 509(B) | 1328 | CLASS A CONCRETE | (1) CY | 559.20 |
| 509(D) | 1331 | CLASS C CONCRETE | CY | 13.20 |
| 510(C) | 6138 | SLOPE WALL (5") | (1) SY | 1913.00 |
| 511(A) | 1332 | REINFORCING STEEL | (1) LB | 4870.00 |
| 511(B) | 6010 | EPOXY COATED REINFORCING STEEL | (1) LB | 288450.00 |
| 514(A) | 6010 | PILES, FURNISHED (HP12x42) | (3) LF | 173.00 |
| 514(A) | 6011 | PILES, FURNISHED (HP12x53) | (3) LF | 1336.00 |
| 514(B) | 6292 | PILES, DRIVEN (HP10x42) | LF | 173.00 |
| 514(B) | 6294 | PILES, DRIVEN (HP12x53) | LF | 1336.00 |
| 514(L) | 6220 | PILE SPLICE, H-PILE (NON-BIDDABLE) | EA | 1.00 |
| 515(A) | 6013 | WATER REPELLENT (VISUALLY INSPECTED) | (1) SY | 2579.00 |
| 516(A) | 6098 | DRILLED SHAFTS 72" DIAMETER | (4) LF | 392.00 |
| 516(C) | 6200 | CROSSHOLE SONIC LOGGING | (4) EA | 3.00 |
| 523(A) | 6550 | SEALER CRACK PREPARATION | (1) LF | 161.00 |
| 523(B) | 6560 | SEALER RESIN | (1) GAL | 1.30 |
| 613(H) | 6204 | 6" PERFORATED PIPE UNDERDRAIN ROUND | (1) LF | 399.00 |
| 613(I) | 6207 | 6" NON-PERF. PIPE UNDERDRAIN RND. | LF | 460.00 |

PAY QUANTITY NOTES

- (1) PAYMENT FOR THIS ITEM WILL BE BASED ON PLAN QUANTITIES. SEE THE 2019 STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION SECTION 109.01.B "PLAN QUANTITIES".
- (2) PROVIDE AND INSTALL BEARING ASSEMBLIES OF THE SIZE, SHAPE AND LOCATION DETAILED IN THE PLANS AT THE ABUTMENTS AND PIERS. THERE IS AN ESTIMATED 3990 LBS. OF STAINLESS STEEL FOR THE FIXED BEARING ASSEMBLIES AND 12290 LBS. OF STAINLESS STEEL FOR THE EXPANSION BEARING ASSEMBLIES. INCLUDE ALL COSTS ASSOCIATED WITH PROVIDING AND INSTALLING THE ANCHOR PLATES, ANCHOR BOLTS, CONTACT PLATES, NUTS, WASHERS AND ELASTOMERIC BEARING PADS, INCLUDING ALL MATERIALS, LABOR, EQUIPMENT AND INCIDENTALS NECESSARY TO COMPLETE THE WORK AS SHOWN IN THE PLANS, IN THE PRICE BID PER EACH OF "STAINLESS STEEL FIXED BEARING ASSEMBLY" OR "STAINLESS STEEL EXPANSION BEARING ASSEMBLY" AS APPLICABLE.
- (3) ALL ABUTMENT PILING SHALL BE AASHTO M270 GRADE 50 STEEL.
- (4) SEE SPECIAL PROVISIONS.

| | | | | |
|----------|-----|-------|---|----------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 01/19 | SB US-81 OVER I-44 | |
| Checked | . | | | |
| Approved | . | | | |
| Squad | | | | |
| | | | PAY QUANTITIES AND GENERAL NOTES | |
| | | | Job Piece No. 24428(04) | Sheet No. AB01 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

GENERAL NOTES (BRIDGE 'H')

SPECIFICATIONS

COMPLY WITH THE REQUIREMENTS OF THE OKLAHOMA DEPARTMENT OF TRANSPORTATION 2009 STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EXCEPT AS MODIFIED BY THE PLANS AND SPECIAL PROVISIONS.

PILE MATERIAL

ALL STEEL PILING SHALL CONFORM TO AASHTO M 270 GRADE 50 WITH ALL COST INCLUDED IN THE UNIT PRICE BID PER LINEAR FOOT OF "PILES, FURNISHED (HP 10X42)" AND "PILES, FURNISHED (HP 12X53)."

PILE DRIVING EQUIPMENT

USE A PILE DRIVING HAMMER OF THE SIZE AND TYPE CAPABLE OF CONSISTENTLY DELIVERING THE EFFECTIVE DYNAMIC ENERGY SUFFICIENT TO DRIVE THE PILES TO THE REQUIRED TIP ELEVATION AND TO ACHIEVE THE MAXIMUM FACTORED PILE LOAD WITHOUT EXCEEDING THE LIMITATIONS SET ON THE ALLOWABLE DRIVING STRESSES IN ACCORDANCE WITH SECTION 514.03.A.(2) OF THE STANDARD SPECIFICATIONS.

PILE DRIVING METHOD

ALL ABUTMENT PILING SHALL BE DRIVEN THROUGH THE COMPACTED FILL. PILING SHALL BE DRIVEN TO POINT BEARING ON SOLID FOUNDATION MATERIAL AT THE APPROXIMATE ELEVATION SHOWN IN THE PLANS. IF THE MAXIMUM FACTORED PILE LOAD IS NOT OBTAINED AT THIS ELEVATION, DRIVING SHALL CONTINUE UNTIL THE MAXIMUM FACTORED PILE LOAD IS OBTAINED. THE LENGTH OF STEEL PILING SHOWN IN THE PLANS IS FOR ESTIMATING PURPOSES ONLY.

MAXIMUM FACTORED PILE LOAD

THE MAXIMUM FACTORED PILE LOADS FOR THE BRIDGE IS SHOWN IN THE TABLE BELOW:

| MAXIMUM FACTORED PILE LOADS (TONS) | | |
|------------------------------------|-------------------|------------|
| LOCATION | BRIDGE SEAT PILES | WING PILES |
| BRIDGE 'H' | 108 | 78 |

DETERMINATION OF PILE CAPACITY

DRIVE EACH PILE UNTIL THE PILE CAPACITY EXCEEDS THE MAXIMUM FACTORED PILE LOAD. THE PILE CAPACITY OF EACH PILE SHALL BE DETERMINED USING THE GATES EQUATION. THE GATES EQUATION IS DEFINED AS FOLLOWS:

$$CAPACITY \text{ IN TONS} = \phi (0.875 \sqrt{E} \log_{10} (10N) - 50)$$

WHERE:

ϕ = RESISTANCE FACTOR OF 0.4

E = ENERGY PRODUCED BY THE HAMMER PER BLOW IN FOOT-POUNDS. FOR GRAVITY AND SINGLE ACTING DIESEL HAMMERS, THE VALUE IS CALCULATED AS THE ACTUAL RAM STROKE OBSERVED IN THE FIELD AND MEASURED IN FEET MULTIPLIED BY THE RAM WEIGHT IN POUNDS.

N = AVERAGE NUMBER OF HAMMER BLOWS PER INCH OF PILE PENETRATION FOR THE LAST 10 TO 20 BLOWS DELIVERED TO THE PILE HEAD.

THE EQUATION IS APPLICABLE ONLY WHEN:

- THE PILE DRIVING HAMMER HAS A FREE FALL (GRAVITY AND SINGLE ACTING HAMMERS ONLY),
- THE HEAD OF THE PILE IS NOT BROOMED, CRUSHED, OR OTHERWISE DAMAGED,
- THE PENETRATION IS QUICK AND UNIFORM,
- THERE IS NO APPRECIABLE REBOUND OF THE HAMMER, AND
- A FOLLOWER IS NOT USED.

THE NUMBER OF BLOWS PER INCH OF PILE PENETRATION MAY BE MEASURED EITHER DURING INITIAL DRIVING OR BY RE-DRIVING WITH A WARM HAMMER OPERATED AT FULL ENERGY AFTER A PILE SET PERIOD, AS DETERMINED BY THE ENGINEER. IF WATER JETS ARE USED IN CONNECTION WITH THE DRIVING, DETERMINE THE PILE CAPACITY BY THE EQUATION SHOWN ONLY AFTER THE JETS HAVE BEEN WITHDRAWN.

STEEL PILE ENCASEMENTS

AS SHOWN ON BRIDGE STANDARD HP1-2, ALL STEEL PILES SHALL BE ENCASED IN REINFORCED CONCRETE TO A DEPTH OF 4'-0" BELOW THE ABUTMENT BRIDGE SEATS AND ABUTMENT WINGS. THE ENCASEMENT CONSTRUCTION WILL REQUIRE CLASS A CONCRETE AND REINFORCING STEEL (WELDED WIRE FABRIC).

ALL COSTS TO CONSTRUCT THE STEEL PILE ENCASEMENTS INCLUDING THE COST OF EXCAVATION, CLASS A CONCRETE AND REINFORCING STEEL, MATERIAL, LABOR, EQUIPMENT AND INCIDENTALS SHALL BE INCLUDED IN THE UNIT PRICE BID PER LINEAR FOOT OF "PILES, DRIVEN (HP 10X42)" AND "PILES, DRIVEN (HP 12X53)."

PERFORATED PIPE UNDERDRAIN AT ABUTMENTS AND SLOPE WALLS

INSTALLATION OF PERFORATED PIPE UNDERDRAIN AT ABUTMENTS AND SLOPE WALLS WILL REQUIRE PIPE UNDERDRAIN COVER MATERIAL AND FILTER FABRIC. THE INSTALLATION OF THE PERFORATED PIPE, PIPE UNDERDRAIN COVER MATERIAL AND FILTER FABRIC SHALL BE AS SHOWN IN THE PLANS AND ON PED-3 AND PUD-3.

ALL COSTS TO INSTALL THE PERFORATED PIPE UNDERDRAIN INCLUDING THE COST OF ALL PERFORATED PIPE, PIPE FITTINGS INCLUDING END CAPS AND TEES, PIPE UNDERDRAIN COVER MATERIAL, FILTER FABRIC, MATERIAL, LABOR, EQUIPMENT AND INCIDENTALS SHALL BE INCLUDED IN THE UNIT PRICE BID PER LINEAR FOOT OF "6" PERFORATED PIPE UNDERDRAIN ROUND."

NON-PERFORATED PIPE UNDERDRAIN AT ABUTMENTS AND SLOPE WALLS

INSTALLATION OF NON-PERFORATED PIPE UNDERDRAIN AT ABUTMENTS AND SLOPE WALLS WILL REQUIRE TRENCH EXCAVATION AND STANDARD BEDDING MATERIAL. THE INSTALLATION OF THE NON-PERFORATED PIPE SHALL BE AS SHOWN IN THE PLANS AND ON STANDARDS PED-3 AND PUD-3. THE STANDARD BEDDING MATERIAL SHALL BE CLASS B AND SHALL BE PLACED AROUND THE NON-PERFORATED PIPE UNDERDRAIN IN THE BOTTOM 2 FEET OF THE TRENCH. THE TRENCH SHALL BE BACKFILLED WITH COMPACTED NATIVE SOIL ABOVE THE STANDARD BEDDING MATERIAL.

ALL COSTS TO INSTALL THE NON-PERFORATED PIPE UNDERDRAIN INCLUDING THE COST OF ALL NON-PERFORATED PIPE, TRENCH EXCAVATION, STANDARD BEDDING MATERIAL, BACKFILLING, MATERIAL, LABOR, EQUIPMENT AND INCIDENTALS SHALL BE INCLUDED IN THE UNIT PRICE BID PER LINEAR FOOT OF "6" NON-PERF. PIPE UNDERDRAIN RND."

BEARING ASSEMBLIES

MATERIAL REQUIREMENTS FOR ALL BEARING ASSEMBLIES SHALL CONFORM TO THE CURRENT ODOT SPECIAL PROVISIONS FOR "BRIDGE BEARING STRUCTURAL STEEL" AND "ELASTOMERIC BEARING PADS."

ALL COSTS OF PROVIDING AND INSTALLING THE STAINLESS STEEL BEARING ASSEMBLIES AS SPECIFIED OR AS SHOWN IN THE PLANS INCLUDING THE COST OF STEEL REINFORCED ELASTOMERIC BEARING PADS; ALL STAINLESS STEEL PARTS INCLUDING: ANCHOR PLATES, CONTACT PLATES, BUILT-UP CONTACT ANGLES, ANCHOR RODS, NUTS AND WASHERS; AND ALL MATERIAL; LABOR; EQUIPMENT AND INCIDENTALS SHALL BE INCLUDED IN THE UNIT PRICE BID PER EACH OF "STAINLESS STEEL FIXED BEARING ASSEMBLY" AND "STAINLESS STEEL EXPANSION BEARING ASSEMBLY."

STRUCTURAL STEEL

ALL STRUCTURAL STEEL INSTALLED ON ALL BRIDGES SHALL CONFORM TO AASHTO M270 (ASTM A709) GRADE 50W AND SECTION 724.01 OF THE STANDARD SPECIFICATIONS. ALL BOLTED CONNECTIONS SHALL BE MADE WITH HIGH STRENGTH FASTENERS CONFORMING TO SECTION 724.02 OF THE STANDARD SPECIFICATIONS. ALL WELDED STUD SHEAR CONNECTORS SHALL CONFORM TO SECTION 724.04 OF THE STANDARD SPECIFICATIONS.

CONCRETE INTERMEDIATE DIAPHRAGMS

ONCE THE CONCRETE HAS BEEN PLACED FOR THE CONCRETE INTERMEDIATE DIAPHRAGMS, WAIT A MINIMUM OF 24 HOURS BEFORE REMOVING THE SIDE FORMS. DO NOT REMOVE THE BOTTOM FORM FOR A MINIMUM OF 3 DAYS OR AT THE DISCRETION OF THE ENGINEER. THIS TIME MAY BE SHORTENED IF THE CONCRETE HAS ATTAINED 80% OF THE SPECIFIED COMPRESSIVE STRENGTH. DO NOT PLACE THE CONCRETE FOR THE DECK SLAB OR APPLY OTHER MASSIVE LOADS TO THE BEAMS OR DIAPHRAGMS UNTIL THE CONCRETE IN THE DIAPHRAGMS HAS BEEN IN PLACE FOR A MINIMUM OF 10 DAYS OR AT THE DISCRETION OF THE ENGINEER. THIS TIME MAY BE SHORTENED IF THE CONCRETE HAS ATTAINED 80% OF THE SPECIFIED COMPRESSIVE STRENGTH.

STAY-IN-PLACE DECK SLAB FORMS

STAY-IN-PLACE STEEL DECK SLAB FORMS MAY BE USED IF THE MINIMUM DECK SLAB THICKNESS OF 8" IS OBTAINED BY MEASURING FROM THE TOP OF THE DECK SLAB TO THE TOP PORTION OF THE CORRUGATIONS IN THE FORMS. LIMIT THE COMBINED LOAD OF THE FORMS AND ANY ADDITIONAL WEIGHT OF CONCRETE NECESSITATED BY THE USE OF THE FORMS TO A MAXIMUM OF 5 PSF. ANY POLYSTYRENE OR OTHER FILLER MATERIAL PLACED TO FILL THE CORRUGATIONS IN THE FORMS MUST BE BONDED TO THE FORMS.

ALL COSTS ASSOCIATED WITH THE USE OF STAY-IN-PLACE FORMS INCLUDING THE COST OF ALL MATERIAL, LABOR, EQUIPMENT AND INCIDENTALS SHALL BE INCLUDED IN THE UNIT PRICE BID PER CUBIC YARD OF "CLASS AA CONCRETE." FOR ADDITIONAL INFORMATION CONCERNING THE USE OF STAY-IN-PLACE FORMS, SEE SECTION 502 OF THE STANDARD SPECIFICATIONS.

THE CONTRACTOR MAY SUBSTITUTE STAY-IN-PLACE PRESTRESSED CONCRETE DECK SLAB FORMS, AT NO ADDITIONAL COST TO THE DEPARTMENT, IF THE FOLLOWING CONDITIONS ARE MET:

1. SHOP DRAWINGS AND STRUCTURAL CALCULATIONS FOR THE FORMS ARE SUBMITTED TO AND APPROVED BY THE BRIDGE ENGINEER.
2. A NEW STRUCTURAL DESIGN, STRUCTURAL CALCULATIONS AND A NEW REINFORCING SCHEDULE FOR THE DECK SLAB ARE SUBMITTED TO AND APPROVED BY THE BRIDGE ENGINEER.
3. ALL SUBMITTED SHOP DRAWINGS, NEW DECK SLAB REINFORCING SCHEDULE, STRUCTURAL DESIGNS AND CALCULATIONS ARE PREPARED, SIGNED AND SEALED BY AN PROFESSIONAL ENGINEER LICENSED IN THE STATE OF OKLAHOMA.

BRIDGE DECK SLAB CONSTRUCTION METHODS

ANY STEEL USED BY THE CONTRACTOR TO FACILITATE DECK SLAB CONSTRUCTION, SUCH AS INSERT WELD ANCHORS, TY-BAR CLIPS, FORM HANGERS OR OTHER APPURTENANCES, THAT REMAIN IN PLACE IN THE BRIDGE DECK SLAB, MUST BE EPOXY COATED OR GALVANIZED. EPOXY COAT IN ACCORDANCE WITH AASHTO M 284 OR GALVANIZE IN ACCORDANCE WITH AASHTO M 111.

DECK SLAB HAUNCHES

PLAN QUANTITY FOR CLASS AA CONCRETE FOR THE HAUNCHES OVER THE PRESTRESSED CONCRETE BEAMS IS AS SHOWN IN THE TABLE BELOW. NO PAYMENT WILL BE MADE FOR DIFFERENCES BETWEEN PLAN QUANTITY AND THE ACTUAL QUANTITY OF HAUNCH CONCRETE.

| HAUNCH CONCRETE (CUBIC YARDS) | |
|-------------------------------|------|
| BRIDGE 'H' | 13.9 |

SEALING CONSTRUCTION JOINTS IN THE DECK SLAB

THE CONSTRUCTION JOINTS IN THE DECK SLABS SHALL BE SEALED WITH HIGH MOLECULAR WEIGHT METHACRYLATE (HMWM) OR EPOXY RESIN AS SHOWN IN THE PLANS AND IN ACCORDANCE WITH SECTION 523 OF THE STANDARD SPECIFICATIONS.

ALL COSTS TO SEAL THE CONSTRUCTION JOINTS IN THE DECK SLABS INCLUDING THE COST OF CRACK PREPARATION, HMWM, RESIN, MATERIAL, LABOR, EQUIPMENT AND INCIDENTALS SHALL BE INCLUDED IN THE UNIT PRICE BID PER LINEAR FOOT OF "SEALER CRACK PREPARATION" AND THE UNIT PRICE BID PER GALLON OF "SEALER RESIN."

PENETRATING WATER REPELLENT SURFACE TREATMENT

A PENETRATING WATER REPELLENT SURFACE TREATMENT SHALL BE APPLIED TO THE CONCRETE SURFACES OF THE BRIDGES AS SHOWN IN THE PLANS INCLUDING ALL THE FOLLOWING SURFACES:

1. TOP AND EXPOSED FRONT FACE AND ENDS OF ABUTMENT BRIDGE SEATS
2. FRONT FACE AND EXPOSED ENDS OF ABUTMENT BACKWALLS
3. TOPS AND SIDES OF PEDESTALS ON ABUTMENT BRIDGE SEATS
4. TOPS, SIDES AND ENDS OF PIER CAPS
5. TOPS AND SIDES OF PEDESTALS ON PIER CAPS
6. THE OUTER FACE AND BOTTOM OF EXTERIOR PRESTRESSED CONCRETE BEAMS
7. EDGE AND UNDERSIDE OF THE OVERHANGING PORTIONS OF THE BRIDGE DECK SLABS
8. ROADWAY FACE AND TOP OF CONCRETE PARAPETS AND INSIDE ALL DRAIN OPENINGS IN THE CONCRETE PARAPETS

ALL COSTS OF THE PENETRATING WATER REPELLENT SURFACE TREATMENT INCLUDING THE COST OF ALL MATERIAL, LABOR, EQUIPMENT AND INCIDENTALS SHALL BE INCLUDED IN THE UNIT PRICE BID PER SQUARE YARD OF "WATER REPELLENT (VISUALLY INSPECTED)."

APPROACH SLABS

CLASS AA CONCRETE SHALL BE USED IN THE APPROACH SLABS OF THE BRIDGES. THE QUANTITIES GIVEN ARE BASED ON THE ACTUAL SQUARE YARDS OF THE APPROACH SLABS.

ALL COSTS TO CONSTRUCT THE APPROACH SLABS INCLUDING THE COST OF CLASS AA CONCRETE, EPOXY COATED REINFORCING STEEL, SILICONE JOINT SEALANT, RAPID CURE JOINT SEALANT, BACKER ROD, PREFORMED EXPANSION JOINT FILLER, POLYETHYLENE SHEETING, SAW CUTTING, JOINT GRINDING, PREPARATION OF SUBGRADE INCLUDING ANY NECESSARY EMBANKMENT AND EXCAVATION, MATERIAL, LABOR, EQUIPMENT AND INCIDENTALS SHALL BE INCLUDED IN THE UNIT PRICE BID PER SQUARE YARD OF "APPROACH SLAB."

BRIDGE 'H'

PAY QUANTITIES AND GENERAL NOTES

STILLWATER CENTRAL RAILROAD COMPANY NOTES - (BRIDGE 'H')

NOTIFICATION OF WORK

THE CONTRACTOR IS REQUIRED TO GIVE THE STILLWATER CENTRAL RAILROAD COMPANY AT LEAST 10 WORKING DAYS ADVANCE NOTICE, IN WRITING, BEFORE ANY WORK IS STARTED ON THE SITE. TO AVOID HAZARDS, THE STILLWATER CENTRAL RAILROAD COMPANY MAY HAVE A REPRESENTATIVE PRESENT, IF DEEMED NECESSARY, FOR THE PURPOSE OF INSPECTION AND THE ISSUANCE OF ANY APPROPRIATE INSTRUCTIONS FOR RAILROAD OPERATIONS DURING THE CHICKASHA BY-PASS CONSTRUCTION PROJECT IN CHICKASHA, GRADY COUNTY AS IT RELATES TO THE STILLWATER CENTRAL RAILROAD COMPANY'S PROPERTY. (DOT NO. TBD, APPROX. MILEPOST 585.03)

THE CONTRACTOR SHALL NOTIFY:

MR. TOBY WAMPLER
ROADMASTER
STILLWATER CENTRAL RAILROAD COMPANY
900 S. BYERS
OKLAHOMA CITY, OKLAHOMA 73129
PHONE: 405-616-3000
EMAIL: TWAMPLER@WATCOCOMPANIES.COM

MR. JIMMY PATTERSON
GENERAL MANAGER
STILLWATER CENTRAL RAILROAD COMPANY
900 S. BYERS
OKLAHOMA CITY, OKLAHOMA 73129
PHONE: 405-616-3000
EMAIL: JPATTERSON@WATCOCOMPANIES.COM

FLAGGING AND INSURANCE

FLAGGING AND INSURANCE SHALL BE PROVIDED AS SPECIFIED IN SECTION 107 OF THE STANDARD SPECIFICATIONS AND IN THE SPECIAL PROVISIONS FOR RAILROAD FLAGGING (SEE PROPOSAL FOR SPECIAL PROVISIONS) AND WHAT IS STATED IN THE STILLWATER CENTRAL RAILROAD COMPANY'S RIGHT OF ENTRY AGREEMENT. STILLWATER CENTRAL RAILROAD COMPANY, AT THEIR DISCRETION, SHALL PROVIDE FLAGGING FOR THE RAILROAD DURING CONSTRUCTION OPERATIONS.

THE CONTRACTOR IS REQUIRED TO REIMBURSE STILLWATER CENTRAL RAILROAD COMPANY FOR FLAGGING SERVICES PROVIDED.

THE CONTRACTOR SHALL ALSO FURNISH SATISFACTORY EVIDENCE TO THE STATE OF OKLAHOMA THAT THEY HAVE PROVIDED INSURANCE OF THE KINDS AND AMOUNTS AS SPECIFIED IN THE SPECIAL PROVISIONS FOR RAILROAD INSURANCE AND IN THE STILLWATER CENTRAL COMPANY'S RIGHT OF ENTRY AGREEMENT.

THE CONTRACTOR WILL BE REQUIRED TO ENTER INTO A RIGHT OF ENTRY AGREEMENT WITH THE STILLWATER CENTRAL RAILROAD COMPANY BEFORE THEY WILL BE ALLOWED ON THE RAILROAD'S RIGHT-OF-WAY.

PRE-WORK MEETING

PRIOR TO WORKING ON THE STILLWATER CENTRAL RAILROAD COMPANY'S RIGHT-OF-WAY OR IN THE VICINITY OF THEIR TRACKS, YOU **MUST** CONTACT THE LOCAL ROADMASTER FOR THE STILLWATER CENTRAL RAILROAD COMPANY TO COORDINATE YOUR WORK. IT IS **VITAL** THAT YOU HAVE CONTACT WITH THE STILLWATER CENTRAL RAILROAD COMPANY ROADMASTER PRIOR TO GETTING ON THE RAILROAD'S PROPERTY.

COORDINATION WITH RAILROAD

THE CONTRACTOR SHALL CONDUCT CONSTRUCTION OPERATIONS IN A MANNER WHICH WILL NOT DELAY OR INTERFERE WITH TRAIN OPERATIONS. CONSTRUCTION ACTIVITY WITHIN 25 (TWENTY-FIVE) FEET OF ACTIVE TRACKS WILL REQUIRE A FLAGMAN TO BE PROVIDED BY THE STILLWATER CENTRAL RAILROAD COMPANY AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL GIVE WRITTEN NOTICE TO THE STILLWATER CENTRAL RAILROAD COMPANY ROADMASTER, A MINIMUM OF 30 (THIRTY) CALENDAR DAYS IN ADVANCE OF WHEN FLAGGING IS REQUIRED.

SPECIAL PERMISSION MUST BE OBTAINED FROM THE STILLWATER CENTRAL RAILROAD COMPANY BEFORE MOVING ANY EQUIPMENT OR OTHER OBJECT WHICH COULD MAKE THE TRACK IMPASSABLE IF IT FELL WITHIN THE AREA SHOWN ON THE CONSTRUCTION CLEARANCE DIAGRAM.

RAILROAD FLAGGERS, PROTECTIVE SERVICES, AND PROTECTIVE DEVICES WILL BE REQUIRED, BUT NOT LIMITED TO, EVENTS WHEN:

- THE CONTRACTOR WORK ACTIVITIES ARE WITHIN 25 (TWENTY-FIVE) FEET OF THE TRACK, MEASURED FROM THE TRACK CENTERLINE.
- ACTIVITIES ARE OVER OR UNDER THE TRACK.
- CRANES OR SIMILAR EQUIPMENT WILL NOT BE POSITIONED WHERE THEY COULD FOUL THE TRACK IF THEY TIPPED OVER OR EXPERIENCED SOME OTHER CATASTROPHIC EVENT.
- IN THE OPINION OF THE STILLWATER CENTRAL RAILROAD COMPANY REPRESENTATIVE:
 - IT IS NECESSARY TO SAFEGUARD THE STILLWATER CENTRAL RAILROAD COMPANY PROPERTY, EMPLOYEES, TRAINS, ENGINES, AND FACILITIES.
 - WHEN ANY EXCAVATION IS PERFORMED BELOW THE BOTTOM OF THE ELEVATIONS AND TRACK OR OTHER STILLWATER CENTRAL RAILROAD COMPANY FACILITIES MAY BE SUBJECT TO MOVEMENT OR SETTLEMENT.
 - WHEN WORK IN ANY WAY INTERFERES WITH SAFE OPERATION OF TRAINS AND TIMETABLE SPEEDS.
 - WHEN ANY HAZARD IS PRESENTED TO RAILROAD TRACK, SIGNALS, COMMUNICATIONS, ELECTRICAL, OR OTHER FACILITIES EITHER DUE TO PERSON, MATERIAL, EQUIPMENT, OR BLASTING IN THE AREA.

PROTECTION OF RAILROAD UNDER BRIDGE

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING THE RAILROAD TRACK BED DURING ALL CONSTRUCTION OPERATIONS. PRIOR TO ANY WORK BEING STARTED, A PROPOSED METHOD OF PREVENTING DEBRIS FROM FALLING ON THE RAILROAD TRACK BED SHALL BE SUBMITTED TO THE RAILROAD REPRESENTATIVE FOR HIS APPROVAL.

THE CONTRACTOR SHALL NOT BE PERMITTED TO LEAVE ANY WORKER SCAFFOLDING IN PLACE IN WORKING POSITION. AT THE END OF EACH WORKDAY, THE SCAFFOLDING SHALL BE REMOVED AND SET A SAFE DISTANCE FROM ANY OPERATING RAILROAD LINE. SCAFFOLDING SHALL AT ALL TIMES MAINTAIN THE MINIMUM CLEARANCE AS SHOWN ON THE "FALSEWORK DIAGRAM" ON THE PLANS (SHEET NO. B039).

EROSION CONTROL AND DRAINAGE

THE CONTRACTOR WILL INSTALL, MAINTAIN, AND REMOVE ALL EROSION CONTROL MEASURES DEEMED NECESSARY WITHIN THE RAILROAD RIGHT OF WAY.

THE CONTRACTOR WILL MAINTAIN THE RAILROAD DRAINAGE AT ALL TIMES WHEN WORKING WITHIN THE RAILROAD RIGHT OF WAY.

RAIL TRAFFIC

THE STILLWATER CENTRAL RAILROAD COMPANY HAS 6 TRAINS PER DAY AT 20 MPH, ON THE CHICKASHA SUBDIVISION. RAIL TRAFFIC IS FOR INFORMATION PURPOSES ONLY. ACTUAL RAIL TRAFFIC MAY VARY.

| 24428(04) PAY QUANTITIES | | | | |
|--|------|---|------|------------|
| 0201 BRIDGE 'H' - NBI XXXXX - FOUR SPAN (96'-100'-100'-66') P.C. BEAM BRIDGE WITH 16°15'27" SKEW, 32'-0" CLEAR ROADWAY AND 42" F-SHAPED PARAPETS | | | | |
| ITEM | | DESCRIPTION | UNIT | QUANTITY |
| 501(B) | 1307 | SUBSTRUCTURE EXCAVATION COMMON (BR-1) | CY | 190.00 |
| 501(G) | 6309 | CLSM BACKFILL (BR-1) | CY | 287.00 |
| 503(A) | 1313 | PRESTRESSED CONCRETE BEAMS (TYPE IV) (BR-1) | LF | 1,794.17 |
| 504(A) | 1304 | APPROACH SLAB (BR-1) | SY | 321.20 |
| 504(B) | 1305 | SAW-CUT GROOVING (BR-1) | SY | 1,788.00 |
| 504(C) | 6250 | SEALED EXPANSION JOINT (BR-1) | LF | 85.00 |
| 504(E) | 6190 | 42" F-SHAPED PARAPET (BR-1) | LF | 897.20 |
| 506(A) | 1322 | STRUCTURAL STEEL (BR-1) | LB | 1,870.00 |
| 507(A) | 6170 | STAINLESS STEEL FIXED BEARING ASSEMBLY (BR-1)(BR-2) | EA | 20.00 |
| 507(B) | 6174 | STAINLESS STEEL EXPANSION BEARING ASSEMBLY (BR-1)(BR-2) | EA | 20.00 |
| 509(A) | 1326 | CLASS AA CONCRETE (BR-1) | CY | 410.00 |
| 509(B) | 1328 | CLASS A CONCRETE (BR-1) | CY | 378.50 |
| 510(C) | 6135 | SLOPE WALL (5') (BR-1) | SY | 871.00 |
| 511(A) | 1332 | REINFORCING STEEL (BR-1) | LB | 2,230.00 |
| 511(B) | 6010 | EPOXY COATED REINFORCING STEEL (BR-1) | LB | 114,730.00 |
| 514(A) | 6010 | PILES, FURNISHED (HP 10X42) | LF | 202.00 |
| 514(A) | 6011 | PILES, FURNISHED (HP 12X53) | LF | 638.00 |
| 514(B) | 6292 | PILES, DRIVEN (HP 10X42) (BR-3) | LF | 202.00 |
| 514(B) | 6294 | PILES, DRIVEN (HP 12X53) (BR-3) | LF | 638.00 |
| 514(L) | 6220 | PILE SPLICE, H-PILE (NON-BIDDABLE) | EA | 1.00 |
| 515(A) | 6013 | WATER REPELLENT (VISUALLY INSPECTED) (BR-1) | SY | 2,252.00 |
| 516(A) | 6098 | DRILLED SHAFTS 72" DIAMETER | LF | 164.00 |
| 516(C) | 6200 | CROSSHOLE SONIC LOGGING | EA | 1.00 |
| 523(A) | 6550 | SEALER CRACK PREPARATION (BR-1) | LF | 128.00 |
| 523(B) | 6560 | SEALER RESIN (BR-1) | GAL | 1.30 |
| 613(H) | 6204 | 6" PERFORATED PIPE UNDERDRAIN ROUND (BR-1)(BR-4) | LF | 194.00 |
| 613(I) | 6207 | 6" NON-PERF. PIPE UNDERDRAIN RND. (BR-5) | LF | 60.00 |
| 613(Q) | 5946 | OUTLET LATERAL HEADWALL (BR-1) | EA | 2.00 |
| 624(E) | 4294 | FENCE-STYLE CLF (8' HIGH, CLASS A) (BR-1) | LF | 312.00 |

PAY ITEM NOTES

- (BR-1) PAYMENT FOR THIS ITEM WILL BE BASED UPON PLAN QUANTITIES ONLY. SEE SECTION 109.01.B OF THE STANDARD SPECIFICATIONS.
- (BR-2) INCLUDES THE FOLLOWING FOR FABRICATION OF STEEL PARTS OF BEARING ASSEMBLIES:
STAINLESS STEEL: 6,890 POUNDS
- (BR-3) INCLUDES THE FOLLOWING FOR CONSTRUCTION OF STEEL PILE ENCASEMENTS IN ACCORDANCE WITH STANDARD HP1-2:
CLASS A CONCRETE: 7.7 CUBIC YARDS
REINFORCING STEEL: 330 POUNDS
- (BR-4) INCLUDES THE FOLLOWING FOR INSTALLATION OF PERFORATED PIPE UNDERDRAIN:
PIPE UNDERDRAIN COVER MATERIAL: 15 CUBIC YARDS
FILTER FABRIC: 54 SQUARE YARDS
- (BR-5) INCLUDES THE FOLLOWING FOR INSTALLATION OF NON-PERFORATED PIPE UNDERDRAIN:
TRENCH EXCAVATION: 13 CUBIC YARDS
STANDARD BEDDING MATERIAL, CLASS B: 6 CUBIC YARDS

BRIDGE 'H'

PAY QUANTITIES AND GENERAL NOTES

GENERAL NOTES (BRIDGE 'K')

SPECIFICATIONS

COMPLY WITH THE REQUIREMENTS OF THE OKLAHOMA DEPARTMENT OF TRANSPORTATION 2009 STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EXCEPT AS MODIFIED BY THE PLANS AND SPECIAL PROVISIONS.

EXCAVATION

ALL UNSUITABLE (SOFT AND YIELDING) FOUNDATION MATERIAL ENCOUNTERED BELOW THE BOTTOM OF THE NEW BARREL AND APRON SHALL BE REMOVED AND REPLACED WITH SUITABLE MATERIAL IN ACCORDANCE WITH SECTION 501.05 OF THE STANDARD SPECIFICATIONS. THE ENGINEER WILL DETERMINE IF THE FOUNDATION MATERIAL IS UNSUITABLE. PAYMENT FOR EXCAVATION AND REPLACEMENT OF UNSUITABLE MATERIAL ENCOUNTERED BELOW THE BOTTOM OF THE NEW BARREL AND APRON WILL BE MADE AT THE CONTRACT UNIT PRICE FOR STRUCTURAL EXCAVATION UNCLASSIFIED.

EXTENDING CURTAIN WALL INTO ROCK

IF ROCK IS ENCOUNTERED BEFORE THE TOTAL DEPTH OF EXCAVATION REQUIRED TO CONSTRUCT THE CURTAIN WALL AS SHOWN IN THE PLANS IS COMPLETE, THE CURTAIN WALL MAY BE SHORTENED TO EXTEND A MINIMUM OF 6" INTO ROCK. ALL COSTS TO SHORTEN THE CURTAIN WALL INCLUDING THE COST TO ADJUST ANY REINFORCING STEEL BARS SHALL BE INCLUDED IN OTHER ITEMS OF WORK.

PAY ITEM NOTES

(BR-1) PAYMENT FOR THIS ITEM WILL BE BASED UPON PLAN QUANTITIES ONLY. SEE SECTION 109.01.B OF THE STANDARD SPECIFICATIONS.

| 24428(04) PAY QUANTITIES | | | | |
|---|------|------------------------------------|-----------|------------|
| 0201 BRIDGE 'K' - NBI XXXXX - 3 - 18' X 15' X 220' LONG RCB WITH 15° SKEW RIGHT FORWARD AND 8' DEEP CURTAIN WALLS | | | | |
| ITEM | | DESCRIPTION | UNIT | QUANTITY |
| 202(A) | 1301 | UNCLASSIFIED EXCAVATION | CY | 11,460.00 |
| 501(A) | 1306 | STRUCTURAL EXCAVATION UNCLASSIFIED | CY | 1,050.00 |
| 509(A) | 1326 | CLASS AA CONCRETE | (BR-1) CY | 2,670.70 |
| 511(A) | 1332 | REINFORCING STEEL | (BR-1) SY | 392,820.00 |

ENVIRONMENTAL MITIGATION NOTES

| REVISIONS | | |
|-----------|-------------|------|
| REV. NO. | DESCRIPTION | DATE |
| | | |

EARTHWORK NOTE:
 THE CONTRACTOR MUST ENSURE THAT ANY MATERIAL INCORPORATED INTO THE PROJECT IS FREE OF ANY HAZARDOUS, INDUSTRIAL OR CONTAMINATED WASTE, REFER TO SUB-SECTIONS 106.01 AND 202.02 OF THE 2009 STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

IMPORTED MATERIAL (EG. BORROW) - IF MATERIAL IS IMPORTED TO THE PROJECT AND AT ANY POINT THE MATERIAL IS DETERMINED BY THE ENGINEER TO INCLUDE ANY TYPE OF UNACCEPTABLE CONTAMINATION, THE MATERIAL MAY REQUIRE REMOVAL, IN WHOLE, OR IN PART. IF REMOVAL IS REQUIRED, THEN THE INITIAL PLACEMENT, REMOVAL AND PROPER DISPOSAL OF THIS MATERIAL SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE DISPOSAL OF THE UNACCEPTABLE MATERIAL SHALL BE APPROVED BY THE ENGINEER, REFER TO SUB-SECTION 107.15 OF THE 2009 STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

TO ASSIST THE CONTRACTOR, THE "OFF PROJECT FACILITY/ BORROW SITE HAZARDOUS MATERIALS QUESTIONNAIRE" IS PROVIDED ON THE DEPARTMENT'S WEB SITE:

[HTTPS://OK.GOV/ODOT/PROGRAMS AND PROJECTS/ENVIRONMENTAL/INDEX.HTML](https://ok.gov/odot/programs_and_projects/environmental/index.html)

THIS QUESTIONNAIRE IS PROVIDED FOR THE CONVENIENCE OF THE CONTRACTOR SO THAT A CLEARER UNDERSTANDING OF THE CHARACTERISTICS OF THE PROPOSED SITE/ MATERIAL IS ACHIEVED. COMPLETION AND SUBMITTAL OF THIS FORM TO THE ENGINEER DOES NOT EXCUSE THE CONTRACTOR FROM PROVIDING MATERIALS THAT ARE FREE OF HAZARDOUS AND INDUSTRIAL COMPOSITION IN ACCORDANCE WITH SUB-SECTIONS 106.01 AND 202.02 OF THE 2009 STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

MIGRATORY BIRD NOTE:
 MIGRATORY BIRDS ARE PROTECTED BY THE FEDERAL MIGRATORY BIRD TREATY ACT. MANY BIRDS COMMONLY USE BRIDGES AND CULVERTS FOR NESTING. THE NESTING SEASON FOR MOST MIGRATORY BIRD SPECIES EXTENDS FROM MARCH 1 TO AUGUST 31. MIGRATORY BIRD NESTING USE OF THE US-81 RCBS (STA.108+67.8, AT 34.983665,-97.951738), AND EW-140 RCB (34.9859, -97.952847), AND I-44 RCBS (STA.260+24.87, STA.291+29.17, STA.327+99.02) WAS OBSERVED. PAINTING, REPAIR, RETROFIT, REHABILITATION OR DEMOLITION OF THE EXISTING CULVERTS SHALL BE CONDUCTED BETWEEN SEPTEMBER 1, AND FEBRUARY 28, WHEN MIGRATORY BIRD NESTS ARE NOT OCCUPIED. IF PAINTING, REPAIR, RETROFIT, REHABILITATION OR DEMOLITION CANNOT BE COMPLETED BETWEEN SEPTEMBER 1 AND FEBRUARY 28, THE CULVERTS SHALL BE PROTECTED FROM NEW NEST ESTABLISHMENT PRIOR TO MARCH 1, BY MEANS THAT DO NOT RESULT IN BIRD DEATH OR INJURY. OPTIONS INCLUDE THE EXCLUSION OF ADULT BIRDS FROM SUITABLE NEST SITES ON OR WITHIN A STRUCTURE BY THE PLACEMENT OF WEATHER-RESISTANT POLYPROPYLENE NETTING WITH 0.25-INCH OR SMALLER OPENINGS, PRIOR TO MARCH 1. METHODS OTHER THAN NETTING MUST BE PRE-APPROVED BY THE ODOT BIOLOGIST.

ALTHOUGH NO NESTS WERE OBSERVED ON ALL OTHER STRUCTURES, THE BIRDS MAY OCCUPY THE STRUCTURES IN THE FUTURE. THE RESIDENT ENGINEER SHALL CONTACT THE ODOT BIOLOGIST AT 405-210-3671 IF ANY BIRD USE OF THESE STRUCTURES IS OBSERVED. IF BIRDS ARE OBSERVED THEN PAINTING, REPAIR, RETROFIT, REHABILITATION OR DEMOLITION OF THE EXISTING CULVERTS SHALL BE CONDUCTED BETWEEN SEPTEMBER 1, AND FEBRUARY 28 (WHEN MIGRATORY BIRD NESTS ARE NOT OCCUPIED).

| | | | |
|----------------------------|------------------------------|--|--|
| ENVIRONMENTAL NOTES | DETAIL | | |
| | REVIEW | | |
| | APPROVED | | |
| | ENVIRONMENTAL DIVISION | | |
| STATE OF OKLAHOMA | DEPARTMENT OF TRANSPORTATION | | JOB/PIECE NO. 24428(04) SHEET NO. AE01 |

| SUMMARY OF PAY QUANTITIES - ROADWAY | | | | |
|-------------------------------------|------|--------------------------------------|-----------------------|----------|
| ROADWAY 100 | | | | |
| ITEM | | DESCRIPTION | UNIT | QUANTITY |
| 613(L) | 6740 | 54" PREFAB. CULVERT END SEC., ROUND | EA | 1.0 |
| 613(L) | 6744 | 60" PREFAB. CULVERT END SEC., ROUND | EA | 2.0 |
| 613(M) | 6960 | TYPE A4 CULVERT END TREATMENT | EA | 64.0 |
| 613(M) | 6964 | TYPE B4 CULVERT END TREATMENT | EA | 13.0 |
| 613(M) | 6968 | TYPE C4 CULVERT END TREATMENT | EA | 17.0 |
| 613(M) | 6972 | TYPE D4 CULVERT END TREATMENT | EA | 14.0 |
| 613(M) | 6976 | TYPE E4 CULVERT END TREATMENT | EA | 14.0 |
| 613(M) | 6984 | TYPE BB4 CULVERT END TREATMENT | EA | 2.0 |
| 613(M) | 6992 | TYPE DD4 CULVERT END TREATMENT | EA | 6.0 |
| 613(M) | 6996 | TYPE EE4 CULVERT END TREATMENT | EA | 2.0 |
| 613(M) | 7004 | TYPE A6 CULVERT END TREATMENT | EA | 5.0 |
| 613(M) | 7008 | TYPE B6 CULVERT END TREATMENT | EA | 4.0 |
| 613(M) | 7012 | TYPE C6 CULVERT END TREATMENT | EA | 1.0 |
| 613(M) | 7016 | TYPE D6 CULVERT END TREATMENT | EA | 4.0 |
| 613(M) | 7020 | TYPE E6 CULVERT END TREATMENT | EA | 2.0 |
| 613(M) | 7024 | TYPE AA6 CULVERT END TREATMENT | EA | 2.0 |
| 613(Q) | 7500 | OUTLET LATERAL HEADWALL | (14) EA | 5.0 |
| 613(W) | 8135 | 42" JACKED CONDUIT | (11) LF | 196.0 |
| 619(A) | 6200 | REMOVAL OF STRUCTURES & OBSTRUCTIONS | (R-37,38)(22,23) LSUM | 1.0 |
| 619(B) | 6300 | REMOVAL OF HEADWALL | (R-38,39) EA | 5.0 |
| 619(B) | 6364 | REMOVAL OF ASPHALT PAVEMENT | (R-38,39) SY | 7,506.0 |
| 619(B) | 6384 | REMOVAL OF CONCRETE DRIVEWAY | (R-38,39) SY | 51.0 |
| 619(B) | 6416 | REMOVAL OF CONCRETE MEDIAN BARRIER | (R-38,39) LF | 315.0 |
| 619(C) | 6600 | SAWING PAVEMENT | LF | 122.0 |
| 624(A) | 3200 | FENCE-STYLE WWF | (R-41)(4) LF | 5,519.0 |

| SUMMARY OF PAY QUANTITIES (STAKING) | | | | |
|-------------------------------------|------|-------------------------------|----------|----------|
| STAKING 600 | | | | |
| ITEM | | DESCRIPTION | UNIT | QUANTITY |
| 642(B) | 3300 | CONSTRUCTION STAKING LEVEL II | (8) LSUM | 1 |

| SUMMARY OF PAY QUANTITIES (CONSTRUCTION) | | | | |
|--|------|------------------------------------|--------|----------|
| CONSTRUCTION 640 | | | | |
| ITEM | | DESCRIPTION | UNIT | QUANTITY |
| 220 | 1100 | SWPPP DOCUMENTATION AND MANAGEMENT | LSUM | 1 |
| 640(A) | 1210 | FIELD OFFICE | (7) EA | 1 |
| 641 | 2110 | MOBILIZATION | LSUM | 1 |

| SUMMARY OF PAY QUANTITIES - ROADWAY | | | | |
|-------------------------------------|------|--|--------------------|-------------|
| ROADWAY 100 | | | | |
| ITEM | | DESCRIPTION | UNIT | QUANTITY |
| 201(A) | 1200 | CLEARING AND GRUBBING | LSUM | 1.0 |
| 202(A) | 2200 | UNCLASSIFIED EXCAVATION | CY | 5,051,377.0 |
| 202(B) | 2300 | MUCK EXCAVATION | (3) CY | 16,000.0 |
| 202(C) | 2400 | ROCK EXCAVATION | (2) CY | 35,000.0 |
| 202(D) | 2500 | UNCLASSIFIED BORROW | (R-3) CY | 60,000.0 |
| 205(A) | 6200 | TYPE A-SALVAGED TOPSOIL | (R-4,6) LSUM | 1.0 |
| 221(C) | 2300 | TEMPORARY SILT FENCE | (R-8) LF | 35,380.0 |
| 221(D) | 2400 | TEMPORARY SEDIMENT FILTER | (R-8) EA | 65.0 |
| 221(F) | 2600 | TEMPORARY SILT DIKE | (R-8) LF | 13,854.0 |
| 221(G) | 2700 | TEMPORARY ROCK FILTER DAM TYPE 1 | (5)(R-8) CY | 604.0 |
| 221(K) | 2800 | TEMPORARY FIBER LOG | (R-8) LF | 183,349.0 |
| 230(A) | 7200 | SOLID SLAB SODDING | (R-6,7)(19)(15) SY | 1,182,103.0 |
| 233(A) | 0200 | VEGETATIVE MULCHING | (R-11) AC | 245.0 |
| 241 | 3100 | MOWING | (R-15) AC | 490.0 |
| 242 | 4100 | (PL) STABILIZED CONSTRUCTION EXIT | (20) EA | 10.0 |
| 303(A) | 1200 | AGGREGATE BASE TYPE A | CY | 1,169.0 |
| 307(K) | 4300 | STABILIZED SUBGRADE | (9) SY | 10,085.0 |
| 326(A) | 1200 | GEOTEXTILE REINFORCEMENT | (12) SY | 6,233.0 |
| 402(E) | 2600 | TRAFFIC BOUND SURFACE COURSE TYPE E | (R-18)(1) TON | 6,026.0 |
| 407(B) | 7300 | TACK COAT | (R-24) GAL | 1,444.0 |
| 408 | 8100 | PRIME COAT | (R-21) GAL | 231,503.0 |
| 411(B) | 1330 | SUPERPAVE, TYPE S3 (PG 64-22 OK) | (R-25) TON | 3,740.0 |
| 411(C) | 1430 | SUPERPAVE, TYPE S4 (PG 64-22 OK) | (R-25) TON | 821.0 |
| 501(A) | 1200 | STRUCTURAL EXCAVATION UNCLASSIFIED | CY | 1,509.0 |
| 501(G) | 1810 | CLSM BACKFILL | (17) CY | X |
| 509(A) | 0200 | CLASS AA CONCRETE | CY | 4,387.0 |
| 509(B) | 0300 | CLASS A CONCRETE | CY | 138.0 |
| 509(C) | 0400 | CLASS A CONCRETE, SMALL STRUCTURES | CY | 3.0 |
| 509(D) | 0500 | CLASS C CONCRETE | (R-31) CY | 4,801.0 |
| 511(A) | 2200 | REINFORCING STEEL | LB | 974,218.0 |
| 601(B) | 1220 | TYPE I-A PLAIN RIPRAP | (18) TON | 3,531.0 |
| 601(C) | 1300 | TYPE I-A FILTER BLANKET | (18) TON | 1,463.0 |
| 611(A) | 7210 | MANHOLE (4' DIA) | (R-33)(13) EA | 2.0 |
| 611(A) | 7215 | MANHOLE (5' DIA) | (R-33)(13) EA | 2.0 |
| 611(G) | 7934 | INLET GPI TYPE 1 (DES. 2) | (13) EA | 1.0 |
| 611(G) | 7962 | INLET GPI TYPE 2 (DES. 9) | (13) EA | 1.0 |
| 611(G) | 7966 | INLET GPI TYPE 2 (DES. 10) | (13) EA | 2.0 |
| 611(G) | 8155 | INLET - LONG BARRIER - TYPE I, DES. 2 | (10,13) EA | 31.0 |
| 611(G) | 0354 | INLET (SMD-TYPE 2) | (R-33)(13) EA | 55.0 |
| 611(G) | 0358 | INLET (SMD-TYPE 2A) | (R-33)(13) EA | 4.0 |
| 611(G) | 0362 | INLET (SMD-TYPE 2B) | (R-33)(13) EA | 1.0 |
| 611(L) | 1600 | JUNCTION BOXES | (13) CF | 147.0 |
| 613(A) | 5208 | 18" R.C.PIPE CLASS III | (16) LF | 6,646.0 |
| 613(A) | 5216 | 24" R.C.PIPE CLASS III | (16) LF | 2,532.0 |
| 613(A) | 5220 | 30" R.C.PIPE CLASS III | (16) LF | 2,030.0 |
| 613(A) | 5224 | 36" R.C.PIPE CLASS III | (16) LF | 1,354.0 |
| 613(A) | 5228 | 42" R.C.PIPE CLASS III | (16) LF | 710.0 |
| 613(A) | 5232 | 48" R.C.PIPE CLASS III | (16) LF | 1,330.0 |
| 613(A) | 5236 | 54" R.C.PIPE CLASS III | (16) LF | 150.0 |
| 613(A) | 5264 | 18" R.C.PIPE CLASS IV | (16) LF | 1,224.0 |
| 613(A) | 5268 | 24" R.C.PIPE CLASS IV | (16) LF | 72.0 |
| 613(A) | 5284 | 48" R.C.PIPE CLASS IV | (16) LF | 552.0 |
| 613(A) | 5318 | 36" R.C.PIPE CLASS V | (16) LF | 336.0 |
| 613(A) | 5322 | 42" R.C.PIPE CLASS V | (16) LF | 636.0 |
| 613(A) | 5328 | 60" R.C.PIPE CLASS V | (16) LF | 450.0 |
| 613(A) | 5350 | 22" X 13" R.C.PIPE ARCH CLASS A-III | (16) LF | 850.0 |
| 613(A) | 5358 | 28" X 18" R.C.PIPE ARCH CLASS A-III | (16) LF | 128.0 |
| 613(A) | 5378 | 58" X 36" R.C.PIPE ARCH CLASS A-III | (16) LF | 104.0 |
| 613(H) | 6200 | 6" PERFORATED PIPE UNDERDRAIN ROUND | (6) LF | 1,114.0 |
| 613(I) | 6305 | 6" NON-PERF.PIPE UNDERDRAIN RND. | (6) LF | 372.0 |
| 613(L) | 6612 | 22" X 13" PREFAB. CULVERT END SEC., ARCH | EA | 6.0 |
| 613(L) | 6708 | 18" PREFAB. CULVERT END SEC., ROUND | EA | 2.0 |
| 613(L) | 6728 | 36" PREFAB. CULVERT END SEC., ROUND | EA | 1.0 |
| 613(L) | 6732 | 42" PREFAB. CULVERT END SEC., ROUND | EA | 2.0 |

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PAY QUANTITIES

PAY QUANTITY NOTES

- (R-3) INCLUDES 60,000 CU. YDS. FOR DRIVEWAYS, RETURNS, DIKES, AND MISCELLANEOUS EARTHWORK.
- (R-4) AN ESTIMATED QUANTITY OF 282,300 C.Y. TOPSOIL TO BE RESERVED FOR REPLACEMENT OF APPROXIMATELY 5" ON COMPLETED FORESLOPES, DITCHES, AND BACKSLOPES. THIS QUANTITY IS INCLUDED IN THE EARTHWORK BALANCE. ANY ADDITIONAL EXCAVATION REQUIRED IN CUT SECTIONS TO ALLOW FOR PLACEMENT OF TOPSOIL TO FINAL GRADE, SHALL BE INCLUDED IN THE PRICE BID.
- (R-6) FOR TYPE-A SALVAGED TOPSOIL PRICE BID TO INCLUDE COST OF 18-46-0 FERTILIZER, ESTIMATED AT 150 POUNDS PER ACRE.
FOR SOLID SLAB SODDING PRICE BID TO INCLUDE COST OF 10-20-10 FERTILIZER, ESTIMATED AT 200 POUNDS PER 1,000 SY.
- (R-7) FOR SOLID SLAB SODDING PRICE BID TO INCLUDE COST OF WATERING, ESTIMATED AT 40 GALLIONS PER SY.
- (R-11) THE QUANTITIES ESTIMATED FOR TEMPORARY EROSION AND SEDIMENT CONTROL IS 245 ACRES.
- (R-15) QUANTITY BASED ON TWO APPLICATIONS.
- (R-18) ESTIMATED AT 160 LBS. PER CU. FT.
- (R-21) PRIME COAT SHALL BE APPLIED AT AN ESTIMATED RATE OF 0.35 GAL. PER SQ. YD. WHEN APPLIED TO SUBGRADE, AND 0.25 GAL. PER SQ. YD. WHEN APPLIED TO AGGREGATE BASE. THE ACTUAL CUTBACK PRIME COAT REQUIRED FOR PLACEMENT OPERATIONS WILL BE DETERMINED BY THE CONTRACTOR, AND SHALL CONSIDER THE RESIDUE FROM DISTILLATION PERCENTAGE SHOWN IN SECTION 708.03 OF THE STANDARD SPECIFICATIONS.
- (R-24) ESTIMATED AT 0.075 GALLONS PER SQUARE YARD OF ORIGINAL EMULSION OF TACK COAT (BEFORE DILUTION FOR APPLICATION) WITH SECTION 407 OF THE STANDARD SPECIFICATIONS.
- (R-25) ESTIMATED AT 112 LBS. PER SQ. YD. PER 1" THICK.
- (R-31) QUANTITY INCLUDES AN ESTIMATED 350 C.Y. TO BE USED AS DIRECTED BY THE ENGINEER.
- (R-33) THE PRECAST CONCRETE OPTION MAY BE USED INSTEAD, PER DIRECTION OF THE ENGINEER.
- (R-37) INCLUDES REMOVAL OF ALL EXISTING ROADWAY DRAINAGE STRUCTURES, HEADWALLS (UNLESS OTHERWISE SPECIFIED), INLETS, FENCES, AND OTHER STRUCTURES WITHIN THE RIGHT OF WAY.
- (R-38) TO BECOME THE PROPERTY OF AND BE DISPOSED OF BY THE CONTRACTOR IN A MANNER APPROVED BY THE ENGINEER.
- (R-39) MATERIALS REMOVED SHALL NOT BE MEASURED FOR PAYMENT UNDER SECTION 202.06 UNCLASSIFIED EXCAVATION.
- (R-41) INCLUDES 2% FOR GROUND MEASUREMENT.

- (1) PRICE BID INCLUDES 1,500 TONS TO BE USED AS DIRECTED BY THE ENGINEER.
- (2) THE PRICE BID SHALL INCLUDE 35,000 CY TO BE UTILIZED AS NECESSARY. THROUGHOUT THE PROJECT, THERE ARE AREAS OF RELATIVELY SHALLOW BEDROCK, PARTICULARLY IN THE VICINITY OF BRIDGES J (COUNTRY CLUB ROAD), O (IDAHO AVE.), AND P (IOWA AVE.). THE GEOTECHNICAL INVESTIGATIONS INDICATE THAT THE SHALE BEDROCK IN THESE LOCATIONS WILL LIKELY BE RIPPABLE WITH NORMAL EQUIPMENT AS SPECIFIED IN SECTION 202.03 OF THE STANDARD SPECIFICATIONS. HOWEVER, THE CONTRACTOR COULD ENCOUNTER HARDER ROCK SEAMS WHICH MEET THE CLASSIFICATION OF ROCK EXCAVATION. ONLY THAT ROCK MEETING THE SECTION 202 DEFINITION SHALL BE MEASURED AND PAID FOR AS ROCK EXCAVATION. FOR ADDITIONAL INFORMATION, THE CONTRACTOR SHOULD EXAMINE THE AVAILABLE GEOTECHNICAL STUDIES.
- (3) PRICE BID INCLUDES AN ESTIMATED 16,000 C.Y. FOR AREAS THAT MAY/OR MAY NOT BE SUITABLE FOR USE.
- (4) CORNER, STRETCHER, AND END POSTS SHALL BE CONSTRUCTED OF SCH. 40 STEEL USING DIMENSIONS SPECIFIED IN STANDARD RWF1-3-. PIPE WILL BE WELDED AND TENSION WIRES OMITTED. THE CORNER AND STRETCHER POST WILL BE PAINTED WITH ZINC RICH PAINT AFTER WELDING. GALVANIZED POSTS ARE NOT REQUIRED.
- (5) PRICE BID TO INCLUDE THE COST OF SEDIMENT TRAPS.
- (6) PRICE BID TO INCLUDE THE COST OF PIPE UNDERDRAIN COVER MATERIAL AND TRENCH EXCAVATION.
- (7) FIELD OFFICE TO BE EQUIPPED WITH ONE TELECOMMUNICATION PHONE LINE FOR AN OPERATIONAL TELEPHONE. IN ADDITION, THE FIELD OFFICE IS TO BE EQUIPPED WITH A HARDWIRED INTERNET LINE FOR USE IN THE FIELD OFFICE. ONE WIFI FOR USE OUTSIDE THE FIELD OFFICE (NEEDS TO WORK AT PROJECT LOCATION), AND SHALL PROVIDE POTABLE DRINKING WATER FOR INSPECTORS. ALL COSTS ASSOCIATED WITH THESE ITEMS, INCLUDING MONTHLY EXPENSES, SHALL BE INCLUDED IN THE PRICE BID FOR FIELD OFFICE.

PAY QUANTITY NOTES CONTINUED

- (8) IN ADDITION TO SECTION 642.04(B), THE CONTRACTOR IS RESPONSIBLE FOR THE FOLLOWING: SURVEY CONTROL POINTS, REFERENCE POINTS AND BENCHMARKS NOTED ON THE PLANS. THE CONTRACTORS SHALL BE RESPONSIBLE FOR LOCATING AND REFRESHING THE CENTERLINE OF PERMANENT CONSTRUCTION, AND SETTING ALL OTHER CONTROL POINTS AND REFERENCE POINTS REQUIRED FOR CONSTRUCTION AND INSPECTION TO INCLUDE BRIDGE CURVES, CONSTRUCTION REFERENCE LINES (CRL), AND RIGHT-OF-WAY. THE SURVEYOR WILL PROVIDE THE RESIDENT ENGINEER WITH A COMPUTERIZED DISK OF SURVEY DATA. THE SURVEY WILL IDENTIFY AND VERIFY BENCHMARKS SET AND MAINTAIN ADDITIONAL BENCHMARKS WITHIN THE PROJECT UNITS AT A MINIMUM OF 500' AS REQUIRED TO INSURE CONSTRUCTION OF A SMOOTH PROFILE OF MAINLINE TO INSURE SMOOTH TRANSITIONS AT THE BOP, EOP AND BRIDGES AS REQUIRED IN SECTIONS 642.04(C). THE SURVEYOR WILL PROVIDE A COPY OF CHECKED BENCHMARKS TO THE RESIDENT ENGINEER FOR REVIEW AND ACCEPTANCE PRIOR TO BEGINNING ANY EARTHWORK PAY ITEMS. THE CONTRACTOR SHALL PROVIDE FOR THE RESIDENT ENGINEERS USE A ROVING CABLE FREE INTEGRATED GPS & RTK SYSTEM WITH FIELD CONTROLLER. THE SYSTEM SHALL BE COMPATIBLE WITH THE SURVEY BASE STATION USED BY THE CONTRACTOR. THE CONTRACTOR SHALL MAINTAIN THE BASE STATION DURING WORK HOURS FROM THE BEGINNING OF EARTHWORK ACTIVITIES UNTIL SUBSTANTIAL COMPLETION IS ACHIEVED. THE CONTRACTOR SHALL PROVIDE A ONE WEEK TRAINING COURSE FOR THIS EQUIPMENT FOR UP TO FOUR ODOT INSPECTORS. THIS TRAINING WILL BE CONDUCTED PRIOR TO COMMENCING EARTHWORK ACTIVITIES. AT A MINIMUM TRAINING SHALL CONSIST OF UNIT OPERATION, SETUP, TAKEDOWN, STATION, OFFSET, ELEVATION, PROJECT LINE WORK, TOC/TOS, CALCULATE AREA, AND DISTANCE. CONTRACTOR SHALL ALSO SET UP TWO (2) POLES AT EACH BASE LOCATION TO ALLOW INSPECTION AND CONTRACTOR TO OPERATE UNITS SIMULTANEOUSLY.
- (9) "STABILIZED SUBGRADE" SHALL INCLUDE THE COST OF THE CHEMICAL ADDITIVE TO ACHIEVE THE RATE SPECIFIED FOR THE APPROPRIATE SOIL CLASSIFICATION AS SPECIFIED IN THE MOST CURRENT ODOT MATERIALS DIVISION OHD L-50. SOIL CLASSIFICATION SHALL BE DETERMINED BY THE CONTRACTOR.
- (10) PRICE BID TO INCLUDE THE COST OF CONCRETE CAPS, LONGITUDINAL BARRIER INLET FRAMES AND GRATES TO BE EXCLUDED FROM THE PRICE BID AND CONSTRUCTED IN A FUTURE PROJECT.
- (11) STR. 102 REQUIRES AN ESTIMATED 196 L.F. OF 42" JACKED CONDUIT IN ORDER TO CONSTRUCT THE 30" RCP UNDER I-44. THE PRICE BID FOR 42" JACKED CONDUIT SHALL INCLUDE THE COST OF STANDARD SCHEDULE STEEL CONDUIT, CASING SPACERS, LABOR, AND ANY OTHER ITEMS REQUIRED TO CASE AND SUBSEQUENTLY COMPLETE CONSTRUCTION OF THE 30" RCP.
- (12) USE RS380i OR APPROVED EQUAL.
- (13) PRICE BID SHALL INCLUDE THE COST OF ADDITIONAL DEPTH REQUIRED TO CONSTRUCT DRAINAGE STRUCTURES AS SHOWN ON THE PLANS.
- (14) ESTIMATED QUANTITY, TO BE USED AT THE DISCRETION OF THE ENGINEER.
- (15) SOLID SLAB SODDING TO BE USED AS DITCH LINER PROTECTION IN AREAS OF PAVED DITCHES.
- (16) PRICE BID TO INCLUDE COST OF TRENCH EXCAVATION AND STANDARD BEDDING MATERIAL.
- (17) AT THE DISCRETION OF ENGINEER, CLSM, TO USED TO FILL ABANDONED PIPES, UNDERGROUND BOXES, AND VOIDS BETWEEN BORED PIPES AND STEEL CASING.
- (18) INCLUDES 500 TONS TO BE USED AT THE DISCRETION OF THE ENGINEER.
- (19) UN-SODDED AREAS WILL BE LIMITED TO NO GREATER THAN ONE MILE AT ANY GIVEN TIME.
- (20) PRICE BID TO INCLUDE 10 STABILIZED CONSTRUCTION EXITS AT LOCATIONS APPROVED BY THE ENGINEER.
- (21) AGGREGATE BASE TYPE A TO BE USED FOR STABILIZED CONSTRUCTION EXIT. LOCATIONS TO BE DETERMINED IN THE FIELD AND APPROVED BY THE ENGINEER. SEE STABILIZED CONSTRUCTION EXIT DETAIL ON SHEET 0016
- (22) PRICE BID SHALL ALSO INCLUDE, BUT IS NOT LIMITED TO, THE REMOVAL OF CONCRETE DITCH LINER, SAWING PAVEMENT, PAVEMENT MARKINGS, CONCRETE FLUMES, FOOTINGS, STRUCTURES, STORM SEWERS, PIPELINES, FENCES, GATES, CONCRETE CURB & GUTTER, POLES AND SHEET METAL SIGNS TO BE PERMANENTLY REMOVED AND ANY OTHER NON-ORGANIC ITEM NOT SPECIFICALLY LISTED AS A REMOVAL PAY ITEM. ITEMS TO BE REMOVED SHOULD BE FIELD VERIFIED AS TO THE EXISTING CONDITIONS AND POSSIBLE NON-STANDARD REMOVAL OPERATIONS THAT MAY BE REQUIRED.
- (23) ITEMS TO BE REMOVED MAY OR MAY NOT BE PRESENT IN ANY SPECIFIED CONDITION SAID ITEMS TO INCLUDE, BUT NOT LIMITED TO:
CLS US-81 STA. 340+40 205' LT. - SMALL CATTLE PEN
CLS US-81 STA. 394+10 50' LT. - CISTERN
CLS US-81 STA. 431+50 350' LT. - METAL CATTLE GUARD
CLS US-81 STA. 482+49 289' LT. - PRIVATE LIGHT POLE
CLS US-81 STA. 487+21 621' LT. - MISC. PP: METAL FIXTURES, WOOD WORKING BENCHES
CLS COUNTRY CLUB ROAD STA. 121+76 TO 124+83 25'-27' RT. - APPROX. 4LF (2'X2.5' TALL ON EACH SIDE) WOOD BARRIER RETAINING WALL LINING DRIVEWAY
CLS COUNTRY CLUB ROAD STA. 124+00 TO 124+10 25'-35' RT. - 10'X8'X4' STACK OF SMALL BRICKS
CLS COUNTRY CLUB ROAD STA. 124+09 30' RT. - 9LF OF FRENCH DRAIN
CLS COUNTRY CLUB ROAD STA. 124+09 28' RT. - 5LF OF FRENCH DRAIN
CRL IOWA AVE. STA. 61+02 15'-130' LT. - 115LF PRIVATE WATERLINE
CRL IOWA AVE. STA. 61+02 TO 62+60 15' LT - 158LF PRIVATE WATERLINE

GENERAL CONSTRUCTION NOTES

IN ACCORDANCE WITH THE OKLAHOMA UNDERGROUND FACILITIES DAMAGE PREVENTION ACT, THE CONTRACTOR SHALL NOTIFY THE OKLAHOMA ONE-CALL SYSTEM, INC. 48 HOURS PRIOR TO BEGINNING EXCAVATION. OKLAHOMA ONE-CALL SYSTEM, INC. "CALL OKIE" 1-800-522-6543 OR 811.

THIS PROJECT SHALL BE CONSTRUCTED WITHOUT CLOSING THE EXISTING ROAD TO LOCAL AND THROUGH TRAFFIC. SEE STANDARD SPECIFICATIONS FOR MAINTENANCE OF LOCAL AND THROUGH TRAFFIC.

MAINTENANCE OF THROUGH TRAFFIC INCLUDES THE MAINTENANCE OF THE EXISTING ROAD IN CLOSE PROXIMITY TO THE NEW CONSTRUCTION AS SHOWN ON THE PLANS.

FOR PROJECTS THAT INCLUDE WIDENING AND/OR RESURFACING, THE CONTRACTOR SHALL SCHEDULE OPERATIONS TO MINIMIZE POTENTIAL DROP-OFF HAZARDS AND SHALL SUBMIT A SEQUENCE OF CONSTRUCTION OPERATIONS TO THE RESIDENT ENGINEER FOR APPROVAL BEFORE OPERATIONS BEGIN. ANY PORTION OF THE CONSTRUCTION OPERATIONS, SUCH AS SUPERPAVE LAYING OPERATIONS, EXCAVATION FOR PAVEMENT WIDENING, OR EXTENSION OF ROADWAY STRUCTURES, SHALL BE LIMITED TO ONE SIDE AT A TIME, AND THE PROCEDURES OUTLINED IN THE PAVEMENT DROP-OFF TREATMENT STANDARD PDT-2 (LATEST REVISION) SHALL BE IMPLEMENTED. ONLY THAT AMOUNT OF OPEN TRENCH WILL BE ALLOWED THAT CAN BE SURFACED IN 1 (ONE) DAY'S TIME WITHOUT APPROVAL BY THE ENGINEER. LIGHTS, SIGNS AND BARRICADES SHALL BE MOVED AS WORK PROGRESSES.

ALL TREES, BRUSH, AND OTHER DEBRIS THAT MIGHT INTERFERE WITH THE FLOW OF WATER SHALL BE CLEANED OUT TO THE RIGHT-OF-WAY LINE, AT EACH STRUCTURE AND BRIDGE, IN A MANNER APPROVED BY THE ENGINEER. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

THE CONTRACTOR SHALL PROVIDE ALL TEMPORARY RIGHT-OF-WAY FENCE AS REQUIRED. WHEN THE PORTION OF THE PROJECT THAT REQUIRED THIS FENCE IS COMPLETED, THE TEMPORARY FENCE SHALL BE REMOVED, AND PERMANENT RIGHT-OF-WAY FENCING SHALL BE RESTORED OR INSTALLED IN A MANNER APPROVED BY THE ENGINEER. ALL COST FOR TEMPORARY FENCING SHALL BE INCLUDED IN OTHER ITEMS OF WORK.

ALL FLOWLINES THAT ARE TO BE FILLED SHALL BE THOROUGHLY TAMPED BEFORE CONSTRUCTION OR EXTENSION OF DRAINAGE STRUCTURES. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

IN ORDER TO ALLEVIATE DUST CONDITIONS DURING GRADING OPERATIONS AND BEFORE PAVEMENT WORK IS COMPLETED, THE CONTRACTOR SHALL SPRINKLE GRADING AT INTERVALS APPROVED BY THE ENGINEER. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

THIS PROJECT IS LOCATED NEAR KNOWN SOURCES OF GYPSUM (SULFATE) DEPOSITS. SPECIAL ATTENTION SHOULD BE USED TO AVOID BORROW MATERIAL THAT COULD ADVERSELY INTERACT WITH THE CALCIUM BASED ADDITIVES (FLY ASH, PORTLAND CEMENT, CEMENT KILN DUST, AND LIME) USED IN THE STABILIZED SUBGRADE. THE CONTRACTOR MAY BE REQUIRED TO PROVIDE SULFATE TESTING OF BORROW PIT SITES AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL NOT WASTE ANY EXCESS EXCAVATION UNTIL ALL PLANNED EMBANKMENTS AND BACKFILLS ARE COMPLETED. EXCESS UNCLASSIFIED EXCAVATION MATERIAL DETERMINED BY THE ENGINEER TO BE SUITABLE FOR BACKFILL SHALL BE USED TO REDUCE ANY UNCLASSIFIED BORROW NEEDED. COST OF SECOND HANDLING SHALL BE INCLUDED IN OTHER ITEMS OF WORK. ANY REMAINING EXCESS EXCAVATION SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND BE DISPOSED OF IN A MANNER APPROVED BY THE ENGINEER.

PRIME COAT SHALL BE APPLIED TO THE SUBGRADE IMMEDIATELY AFTER FINAL COMPACTION AND SHAPING TO RETAIN MOISTURE FOR PROPER CHEMICAL REACTION OF THE SOIL ADDITIVE.

THE CONTRACTOR SHALL KEEP THE OPEN TRENCH DRAINED. COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

VEGETATIVE MULCHING: THE VEGETATIVE MULCH SHALL BE ANCHORED IN ACCORDANCE WITH THE "MULCHING-TILLER METHOD", AS SPECIFIED IN 233.04B(2) OF THE STANDARD SPECIFICATIONS.

AT THE BEGINNING OF TURFING OPERATIONS, ANY AREAS INCLUDED IN PLANNED QUANTITIES THAT HAVE GROWN A SATISFACTORY VOLUNTEER TURF OF PERENNIAL GRASS, AS DETERMINED BY THE ENGINEER, SHALL BE FERTILIZED AND WATERED AS CALLED FOR ON THE PLANS, BUT SHALL NOT BE SEEDED, SODDED, OR SPRIGGED.

PIPE UNDERDRAIN QUANTITIES ESTIMATED ONLY. LOCATION, IF AND WHERE REQUIRED, TO BE DETERMINED BY THE ENGINEER.

THE CONTRACTOR SHALL REMOVE AND RESET MAILBOXES AS NECESSARY. MAILBOXES ARE TO BE MAINTAINED IN AN UPRIGHT POSITION AND ACCESSIBLE TO MAIL CARRIER'S CAR DURING CONSTRUCTION. ANY DAMAGE TO BOXES OR SUPPORTS SHALL BE REPAIRED BY THE CONTRACTOR. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

SURFACING OF RETURNS, UNLESS OTHERWISE SHOWN ON THE PLANS, SHALL BE OF THE SAME MATERIAL (BASE AND SURFACE) AS THAT OF THE ABUTTING SHOULDER OF THE MAINLINE. BASE AND SURFACE THICKNESS SHALL BE THE THICKNESS SHOWN ON PLANS.

T.B.S.C. SURFACES SHALL BE SPRINKLED WITH WATER AND ROLLED WITH A PNEUMATIC ROLLER IN A MANNER APPROVED BY THE ENGINEER.

THE ENGINEER SHALL CHECK GRADES AT RAMP TERMINALS, AND MAKE ANY ADJUSTMENTS OF THE GRADES AND SUPERELEVATIONS, WHICH ARE REQUIRED TO OBTAIN SMOOTH PROFILES FOR BOTH EDGES OF THE RAMP PAVEMENT. CROSS SLOPE BREAKOVER SHALL NOT EXCEED 5%(FIVE PERCENT).

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PAY NOTES

SUGGESTED SEQUENCE OF CONSTRUCTION

GENERAL NOTES

THE CONTRACTOR SHALL SEQUENCE AND OR PHASE CONSTRUCTION TO ENSURE THAT ACCESS TO EXISTING DRIVES AND LOCAL ROADS ARE MAINTAINED DURING ALL CONSTRUCTION OPERATIONS UNLESS NOTED OTHERWISE. IF AT ANY TIME ACCESS IS TO BE INTERRUPTED THE CONTRACTOR SHALL NOTIFY THE PROPERTY OWNER, THE CITY, AND ALL EMERGENCY AGENCIES A MINIMUM OF FIVE WORKING DAYS BEFORE OPERATIONS BEGIN. DRIVES SHALL BE CONSTRUCTED DURING OFF PEAK HOURS OR AS DIRECTED BY THE ENGINEER.

GRADING PHASES SHOULD BE RUN CONCURRENTLY WITH BRIDGE CONSTRUCTION PHASES WHEN POSSIBLE TO TAKE ADVANTAGE OF EARTHWORK AVAILABILITY AND MINIMIZE CONSTRUCTION TIME.

PLACE SOLID SLAB SOD OR VEGATATIVE MULCH ON COMPLETED COMPLETED SLOPES IN ORDER TO REDUCE THE AMOUNT OF AREA DISTURBED AT ONE TIME.

PHASE 1: GRADE FROM STA 389+00 TO STA 487+44. INCLUDES US 81, BRIDGE EMBANKMENTS, EW 135.5, COUNTY ROAD, & IOWA AVE. FROM STA 53+80 TO STA 62+65.

TRAFFIC ON EXISTING LOCAL STREETS

- A CONSTRUCT IOWA AVE. FROM STA 53+80 TO STA 62+65 TO PROVIDE ACCESS TO A PUMP JACK.
- B CONSTRUCT EW 135.5 INCLUDING PROPOSED DRIVES TO PUMP JACK.
- C CONSTRUCT COUNTY ROAD EAST OF US 81 TO MAINTAIN ACCESS TO ADJACENT PROPERTIES.
- D GRADE AND DRAIN US 81 FROM STA 389+00 TO STA 487+44. POSTPONE GRADING FROM STA 390+90 TO STA 392+90 UNTIL PHASE 1G IN ORDER TO AVOID IMPACTING IDAHO AVE. TRAFFIC. CONSTRUCT TEMPORARY DITCHES SUCH THAT WATER DRAINS AWAY FROM IDAHO AVE. IN BOTH DIRECTIONS. DO NOT DISTURB TRAFFIC ON 29TH STREET DURING GRADING OPERATIONS.
- E GRADE AND DRAIN RAMP AA, RAMP CC, US 81 INTERIM CONNECTION 2, AND DRIVE ON US 62.
- F DEADEND FRISCO AVE. ON THE EAST AND WEST SIDE OF US 81.
- G CLOSE IDAHO AVE. TO TRAFFIC AND COMPLETE US 81 GRADING FROM STA 390+90 TO STA 392+90. COORDINATE IDAHO AVE. CLOSURE AND GRADING WITH THE CONSTRUCTION OF BRIDGE "O" AND IDAHO AVE. SURFACING IN J/P 24428(12).

PHASE 2: GRADE FROM STA 389+00 TO STA 142+00. INCLUDES US 81, BRIDGE EMBANKMENTS, & COUNTRY CLUB ROAD GRADING

TRAFFIC ON EXISTING LOCAL STREETS

- A BEGIN GRADING AND DRAINAGE ON US 81, RAMPS Q, R, S, T, U, & V FROM STA 389+00 TO STA 287+00. POSTPONE GRADING FROM STA 287+00 TO APPROXIMATELY STA 258+25 UNTIL PHASE 4 IN ORDER TO AVOID PONDING OF RUNOFF ADJACENT TO COUNTRY CLUB ROAD. DO NOT DISTURB COUNTRY CLUB ROAD TRAFFIC AT THIS TIME.
- B CONSTRUCT BRIDGE "K".
- C BEGIN GRADING AND DRAINAGE ON US 81 FROM STA 258+25 TO STA 142+00 INCLUDING BRIDGE EMBANKMENTS AND I-44 RAMPS.
- D CONSTRUCT BRIDGE "H" PIERS AND ABUTMENTS CONCURRENTLY WITH GRADING OPERATIONS. DO NOT DISTURB NORGE ROAD TRAFFIC.
- E CONSTRUCT REMAINDER OF BRIDGE "H". SHIFT TRAFFIC ON NORGE ROAD AS NECESSARY TO CONSTRUCT BRIDGE. SHORT TERM CLOSURES OF THROUGH TRAFFIC TO NORGE ROAD DURING BEAM HANGING OPERATIONS ONLY.
- F BEGIN CONSTRUCTION OF BRIDGE "F" ABUTMENTS.
- G GRADE AND DRAIN RAMP A FROM STA 142+15 TO STA 153+00, RAMP A EXT FROM STA 243+00 TO STA 246+26.56, RAMP B FROM STA 243+00 TO STA 263+72.12, RAMP D FROM STA 244+00 TO STA 264+50, AND ANY ADDITIONAL GRADING NECESSARY FOR DRAINAGE WHILE AVOIDING EXISTING US 81 AND QUAIL ROAD TRAFFIC OPERATIONS.

NOTES: STATIONS REPORTED IN PHASE 2G REFER TO THE RESPECTIVE RAMP STATIONING. ALL OTHER STATIONS REPORTED IN PHASE 2 ARE BASED ON US 81 STATIONING.

SUGGESTED SEQUENCE OF CONSTRUCTION CONTINUED

PHASE 3: CONSTRUCT I-44 TEMPORARY WIDENING AND BRIDGE "F"
TRAFFIC ON EXISTING I-44

- A CONSTRUCT STR 102 BY BORING UNDER I-44.
- B SHIFT TRAFFIC TOWARDS THE CENTER MEDIAN OF I-44 APPROXIMATELY 4' FROM STA 277+10 TO STA 297+90 LT & RT.
- C CONSTRUCT I-44 EB AND WB GRADING FROM STA 245+65 TO STA 333+30 AND TEMPORARY WIDENING.
- D CONSTRUCT BRIDGE "F" PIER 1, PIER 3, AND COMPLETE ABUTMENTS.
- E SHIFT I-44 TRAFFIC TO TEMPORARY WIDENING.
- F PLACE PORTABLE LONGITUDINAL BARRIER AND CONSTRUCT I-44 MEDIAN BARRIER, PIER PROTECTION, AND BRIDGE "F" PIER 2.
- G CONSTRUCT REMAINDER OF BRIDGE "F". SHIFT TRAFFIC AS NECESSARY TO CONSTRUCT BRIDGE "F". UTILIZE ROLLING ROADBLOCK DURING BEAM HANGING OPERATIONS.
- H REMOVE PORTABLE LONGITUDINAL BARRIER AND RETURN I-44 TO NORMAL TRAFFIC CONFIGURATION.

NOTES: CONSTRUCT THIS PHASE CONCURRENTLY WITH PHASE 2 TO TAKE ADVANTAGE OF EARTHWORK OPERATIONS REQUIRED FOR BRIDGE "F" ABUTMENTS AND TO REDUCE CONSTRUCTION TIME.

LEAVE I-44 TEMPORARY WIDENING IN PLACE FOR BRIDGE "G" CONSTRUCTION IN J/P 24428(17)
UTILIZE TEMPORARY TRAFFIC TABS RATHER THAN PAINTED STRIPE.

PHASE 4: GRADE FROM STA 258+25 TO STA 287+00, GRADE FOR LOCAL STREET
TRAFFIC ON EXISTING LOCAL STREETS

- A CLOSE COUNTRY CLUB ROAD IN COORDINATION WITH J/P 24428(12) BRIDGE "J" CONSTRUCTION. UTILIZE MOCKINGBIRD ROAD AND NORGE ROAD AS A TEMPORARY DETOUR.
- B GRADE AND DRAIN US 81 FROM STA 287+00 TO STA 258+25.
- C GRADE AND DRAIN COUNTRY CLUB ROAD, RAMP O, AND RAMP P.

NOTES: COUNTRY CLUB ROAD WILL BE OPENED AT THE COMPLETION OF BRIDGE "J" AND COUNTRY CLUB ROAD SURFACING IN J/P 24428(12) PHASE 1.

PHASE 4 GRADING SHOULD BE RUN IN COORDINATION WITH BRIDGE "J" CONSTRUCTION IN J/P 24428(12).

SUGGESTED SEQUENCE OF
CONSTRUCTION

SUMMARY OF SURFACING

| STATION EXTENTS | AGGREGATE BASE TYPE A 303(A) | STABILIZED SUBGRADE 307(K) | GEOTEXTILE REINFORCEMENT 326(A) | 3" TBSC TYPE E 402(E) | TACK COAT 407(B) | PRIME COAT 408 | SUPERPAVE, TYPE S3 (PG 64-22 OK) 411(B) | SUPERPAVE, TYPE S4 (PG 64-22 OK) 411(C) |
|------------------------------------|------------------------------|----------------------------|---------------------------------|-----------------------|------------------|----------------|---|---|
| | CY | SY | SY | TON | GAL | GAL | TON | TON |
| US 81 | | | | | | | | |
| STA. 134+00.00 TO STA. 149+00.00 | | | | | | 2,484.32 | | |
| STA. 149+00.00 TO STA. 164+00.00 | | | | | | 4,400.14 | | |
| STA. 164+00.00 TO STA. 179+00.00 | | | | | | 8,446.38 | | |
| STA. 179+00.00 TO STA. 194+00.00 | | | | | | 9,536.74 | | |
| STA. 194+00.00 TO STA. 209+00.00 | | | | | | 7,217.99 | | |
| STA. 209+00.00 TO STA. 224+00.00 | | | | | | 8,977.54 | | |
| STA. 224+00.00 TO STA. 239+00.00 | | | | | | 7,410.17 | | |
| STA. 239+00.00 TO STA. 246+00.00 | | | | | | 2,783.18 | | |
| STA. 246+00.00 TO STA. 261+00.00 | | | | | | 3,746.05 | | |
| STA. 261+00.00 TO STA. 276+00.00 | | | | | | 5,111.53 | | |
| STA. 276+00.00 TO STA. 291+00.00 | | | | | | 5,029.00 | | |
| STA. 291+00.00 TO STA. 306+00.00 | | | | | | 6,618.10 | | |
| STA. 306+00.00 TO STA. 323+00.00 | | | | | | 8,069.33 | | |
| STA. 323+00.00 TO STA. 338+00.00 | | | | | | 5,021.76 | | |
| STA. 338+00.00 TO STA. 353+00.00 | | | | | | 4,463.99 | | |
| STA. 353+00.00 TO STA. 368+00.00 | | | | | | 4,680.00 | | |
| STA. 368+00.00 TO STA. 383+00.00 | | | | | | 5,963.97 | | |
| STA. 383+00.00 TO STA. 396+00.00 | | | | | | 5,488.84 | | |
| STA. 396+00.00 TO STA. 411+00.00 | | | | | | 5,820.11 | | |
| STA. 411+00.00 TO STA. 426+00.00 | | | | | | 5,569.50 | | |
| STA. 426+00.00 TO STA. 441+00.00 | | | | | | 6,469.76 | | |
| STA. 441+00.00 TO STA. 456+00.00 | | | | | | 6,021.59 | | |
| STA. 456+00.00 TO STA. 471+00.00 | | | | | | 4,387.03 | | |
| STA. 471+00.00 TO STA. 485+00.00 | | | | | | 2,539.39 | | |
| STA. 485+00.00 TO STA. 500+00.00 | | | | | | 516.74 | | |
| US 81 INTERIM | | | | | | | | |
| STA. 169+13.00 TO STA. 180+00.00 | | | | | | 817.77 | | |
| STA. 180+00.00 TO STA. 186+80.91 | | | | | | 186.80 | | |
| RAMP A | | | | | | | | |
| STA. 141+35.10 TO STA. 153+00.00 | | | | | | 1,437.72 | | |
| RAMP A EXT. | | | | | | | | |
| STA. 243+00.00 TO STA. 246+26.56 | | | | | | 130.54 | | |
| RAMP B | | | | | | | | |
| STA. 243+00.00 TO STA. 253+00.00 | | | | | | 1,399.84 | | |
| STA. 253+00.00 TO STA. 263+72.12 | | | | | | 1,430.00 | | |
| RAMP D | | | | | | | | |
| STA. 244+00.00 TO STA. 258+00.00 | | | | | | 1,896.82 | | |
| STA. 258+00.00 TO STA. 264+50.00 | | | | | | 742.74 | | |
| RAMP G | | | | | | | | |
| STA. 1173+39.86 TO STA. 1184+00.00 | | | | | | 1,143.19 | | |
| STA. 1184+00.00 TO STA. 1199+00.00 | | | | | | 2,111.07 | | |
| STA. 1199+00.00 TO STA. 1201+50.16 | | | | | | 1,255.92 | | |
| RAMP H | | | | | | | | |
| STA. 193+36.11 TO STA. 196+00.00 | | | | | | 293.24 | | |
| STA. 196+00.00 TO STA. 205+53.80 | | | | | | 1,359.19 | | |
| RAMP I | | | | | | | | |
| STA. 319+93.28 TO STA. 320+00.00 | | | | | | 7.51 | | |
| STA. 320+00.00 TO STA. 332+95.46 | | | | | | 1,629.37 | | |
| RAMP J | | | | | | | | |
| STA. 2187+05.34 TO STA. 2190+00.00 | | | | | | 1,375.74 | | |
| STA. 2190+00.00 TO STA. 2205+00.00 | | | | | | 2,116.12 | | |
| STA. 2205+00.00 TO STA. 2217+31.21 | | | | | | 1,403.19 | | |
| RAMP K | | | | | | | | |
| STA. 3172+99.80 TO STA. 3179+00.00 | | | | | | 542.02 | | |
| STA. 3179+00.00 TO STA. 3194+00.00 | | | | | | 2,139.79 | | |
| STA. 3194+00.00 TO STA. 3203+65.41 | | | | | | 2,309.04 | | |
| RAMP L | | | | | | | | |
| STA. 214+68.36 TO STA. 215+00.00 | | | | | | 42.34 | | |
| STA. 215+00.00 TO STA. 226+76.90 | | | | | | 1,604.38 | | |

SUMMARY OF SURFACING

| STATION EXTENTS | AGGREGATE BASE TYPE A 303(A) | STABILIZED SUBGRADE 307(K) | GEOTEXTILE REINFORCEMENT 326(A) | 3" TBSC TYPE E 402(E) | TACK COAT 407(B) | PRIME COAT 408 | SUPERPAVE, TYPE S3 (PG 64-22 OK) 411(B) | SUPERPAVE, TYPE S4 (PG 64-22 OK) 411(C) |
|-------------------------------------|------------------------------|----------------------------|---------------------------------|-----------------------|------------------|----------------|---|---|
| | CY | SY | SY | TON | GAL | GAL | TON | TON |
| RAMP M | | | | | | | | |
| STA. 298+40.59 TO STA. 302+00.00 | | | | | | 431.03 | | |
| STA. 302+00.00 TO STA. 310+50.38 | | | | | | 1,212.45 | | |
| RAMP N | | | | | | | | |
| STA. 4185+09.01 TO STA. 4188+00.00 | | | | | | 1,331.59 | | |
| STA. 4188+00.00 TO STA. 4203+00.00 | | | | | | 2,110.96 | | |
| STA. 4203+00.00 TO STA. 4215+37.31 | | | | | | 1,397.23 | | |
| RAMP O | | | | | | | | |
| STA. 998+66.78 TO STA. 1010+00.00 | | | | | | 1,224.07 | | |
| STA. 1010+00.00 TO STA. 1025+00.00 | | | | | | 2,064.66 | | |
| STA. 1025+00.00 TO STA. 1033+21.04 | | | | | | 1,327.64 | | |
| RAMP P | | | | | | | | |
| STA. 1050+00.00 TO STA. 1060+00.00 | | | | | | 1,445.38 | | |
| STA. 1060+00.00 TO STA. 1070+06.58 | | | | | | 1,455.51 | | |
| RAMP Q | | | | | | | | |
| STA. 1090+52.93 TO STA. 1100+00.00 | | | | | | 1,011.27 | | |
| STA. 1100+00.00 TO STA. 1110+00.00 | | | | | | 1,311.62 | | |
| STA. 1110+00.00 TO STA. 1120+64.80 | | | | | | 1,365.88 | | |
| RAMP Q INTERIM | | | | | | | | |
| STA. 1076+54.10 TO STA. 1088+00.00 | | | | | | 514.10 | | |
| STA. 1088+00.00 TO STA. 1100+50.00 | | | | | | 514.74 | | |
| RAMP R | | | | | | | | |
| STA. 1150+00.00 TO STA. 1164+00.00 | | | | | | 1,828.44 | | |
| STA. 1164+00.00 TO STA. 1173+38.66 | | | | | | 1,311.10 | | |
| RAMP R INTERIM | | | | | | | | |
| STA. 1167+25.00 TO STA. 1179+00.00 | | | | | | 743.94 | | |
| STA. 1179+00.00 TO STA. 1186+57.08 | | | | | | 399.73 | | |
| RAMP S | | | | | | | | |
| STA. 1207+76.56 TO STA. 1215+00.00 | | | | | | 910.01 | | |
| STA. 1215+00.00 TO STA. 1225+27.60 | | | | | | 1,441.91 | | |
| RAMP T | | | | | | | | |
| STA. 1250+12.00 TO STA. 1263+00.00 | | | | | | 1,832.61 | | |
| STA. 1263+00.00 TO STA. 1273+78.42 | | | | | | 1,159.74 | | |
| RAMP U | | | | | | | | |
| STA. 1298+76.72 TO STA. 1313+00.00 | | | | | | 1,898.84 | | |
| STA. 1313+00.00 TO STA. 1316+53.90 | | | | | | 494.65 | | |
| RAMP U INTERIM | | | | | | | | |
| STA. 1291+87.82 TO STA. 1304+00.00 | | | | | | 775.94 | | |
| RAMP V | | | | | | | | |
| STA. 1350+12.00 TO STA. 1365+00.00 | | | | | | 1,929.33 | | |
| STA. 1365+00.00 TO STA. 1374+12.67 | | | | | | 972.28 | | |
| RAMP V INTERIM | | | | | | | | |
| STA. 1360+75.00 TO STA. 1369+00.00 | | | | | | 449.17 | | |
| STA. 1369+00.00 TO STA. 1379+79.43 | | | | | | 500.40 | | |
| RAMP AA | | | | | | | | |
| STA. 468+70.82 TO STA. 482+00.00 | | | | | | 1,015.68 | | |
| STA. 482+00.00 TO STA. 488+06.02 | | | | | | 1,013.33 | | |
| RAMP CC | | | | | | | | |
| STA. 461+68.17 TO STA. 475+00.00 | | | | | | 1,645.04 | | |
| STA. 475+00.00 TO STA. 488+12.03 | | | | | | 1,079.02 | | |
| I-44 COLLECTOR ROAD EAST/WEST BOUND | | | | | | | | |
| STA. 245+65.00 TO STA. 252+00.00 | | | | | | 433.33 | | |
| STA. 252+00.00 TO STA. 267+00.00 | | | | | | 3,115.40 | | |
| STA. 267+00.00 TO STA. 282+00.00 | | | | | | 4,028.47 | | |
| STA. 282+00.00 TO STA. 297+00.00 | | | | | | 4,371.79 | | |
| STA. 297+00.00 TO STA. 312+00.00 | | | | | | 4,101.66 | | |
| STA. 312+00.00 TO STA. 327+00.00 | | | | | | 3,178.30 | | |
| STA. 327+00.00 TO STA. 332+82.12 | | | | | | 552.34 | | |
| I-44 WIDENING EASTBOUND | | | | | | | | |
| STA. 281+59.20 TO STA. 290+00.00 | 185.30 | 1,051.86 | 1,051.86 | 421.03 | 99.81 | 576.60 | 320.29 | 56.25 |

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY SHEET

US 81 REALIGNMENT
GRADY COUNTY

SUMMARY OF SURFACING

| STATION EXTENTS | AGGREGATE BASE TYPE A 303(A) | STABILIZED SUBGRADE 307(K) | GEOTEXTILE REINFORCEMENT 326(A) | 3" TBSC TYPE E 402(E) | TACK COAT 407(B) | PRIME COAT 408 | SUPERPAVE, TYPE S3 (PG 64-22 OK) 411(B) | SUPERPAVE, TYPE S4 (PG 64-22 OK) 411(C) |
|---|------------------------------|----------------------------|---------------------------------|-----------------------|------------------|----------------|---|---|
| | CY | SY | SY | TON | GAL | GAL | TON | TON |
| STA. 290+00.00 TO STA. 301+70.00 1-44 WIDENING WESTBOUND | 526.02 | 2,669.74 | 2,669.74 | 849.91 | 356.25 | 1,526.16 | 1,122.01 | 213.57 |
| STA. 282+52.34 TO STA. 290+00.00 | 172.53 | 969.05 | 969.05 | 372.10 | 95.20 | 533.26 | 305.15 | 54.12 |
| STA. 290+00.00 TO STA. 301+70.00 COUNTRY CLUB RD. | 159.56 | 921.04 | 921.04 | 583.64 | 82.31 | 501.86 | 265.52 | 45.83 |
| STA. 101+00.00 TO STA. 115+00.00 | | | | | | 1,328.54 | | |
| STA. 115+00.00 TO STA. 123+00.00 IOWA AVE. | | | | | | 1,178.91 | | |
| STA. 53+80.00 TO STA. 62+65.00 EW 135.5 | | 3,607.18 | | | 368.18 | 1,262.51 | 1,319.08 | 337.56 |
| STA. 11+96.45 TO STA. 25+45.00 NS 281.5 | | | | 582.91 | | | | |
| STA. 101+85.00 TO STA. 104+15.00 COUNTY RD. | | 244.45 | | | 29.72 | 85.56 | 103.98 | 27.75 |
| STA. 300+41.76 TO STA. 315+00.00 | | | | 581.21 | | | | |
| STA. 315+00.00 TO STA. 327+40.20 US 62 | | | | 507.73 | | | | |
| STA. 394+05.25 TO STA. 397+01.85 | 124.99 | 620.87 | 620.87 | 103.10 | 88.69 | 357.91 | 222.36 | 53.53 |
| TOTALS | 1,168.40 | 10,084.19 | 6,232.56 | 4,001.62 | 1,120.16 | 231,399.38 | 3,658.39 | 788.61 |

THESE ITEMS LISTED FOR CONTRACTOR USE. COST FOR THESE ITEMS ARE INCLUDED IN OTHER ITEMS OF WORK

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY SHHET

SUMMARY OF REMOVAL QUANTITIES

| STATION EXTENTS | REMOVAL OF FENCE | REMOVAL OF ASPHALT DRIVEWAY 619(B) | REMOVAL OF ASPHALT PAVEMENT 619(B) | REMOVAL OF CONCRETE MEDIAN BARRIER 619(B) | SAWING PAVEMENT 619(C) |
|----------------------------------|------------------|------------------------------------|------------------------------------|---|------------------------|
| | LF | SY | SY | LF | LF |
| φ US 81 | | | | | |
| STA. 136+00.00 TO STA. 166+00.00 | 7,083.00 | | | | |
| STA. 182+00.00 TO STA. 213+00.00 | 2,677.00 | | | | |
| STA. 213+00.00 TO STA. 243+00.00 | 1,899.00 | | | | |
| STA. 243+00.00 TO STA. 273+00.00 | 783.00 | | | | |
| STA. 273+00.00 TO STA. 303+00.00 | 1,997.00 | | | | |
| STA. 303+00.00 TO STA. 315+00.00 | 958.00 | | | | |
| STA. 315+00.00 TO STA. 323+00.00 | 486.00 | | | | |
| STA. 323+00.00 TO STA. 338+00.00 | 2,033.00 | | | | |
| STA. 338+00.00 TO STA. 353+00.00 | 2,279.00 | | | | |
| STA. 353+00.00 TO STA. 368+00.00 | 2,137.00 | | | | |
| STA. 368+00.00 TO STA. 383+00.00 | 1,501.00 | | | | |
| STA. 383+00.00 TO STA. 398+00.00 | 4,395.00 | | | | |
| STA. 398+00.00 TO STA. 413+00.00 | 2,716.00 | | | | |
| STA. 413+00.00 TO STA. 428+00.00 | 2,558.00 | | | | |
| STA. 428+00.00 TO STA. 443+00.00 | 1,861.00 | | | | |
| STA. 443+00.00 TO STA. 458+00.00 | 653.00 | | | | |
| STA. 458+00.00 TO STA. 473+00.00 | 931.00 | | | | |
| STA. 473+00.00 TO STA. 488+00.00 | 678.00 | | | | |
| I-44 | | | | | |
| STA. 237+00.00 TO STA. 267+00.00 | 1,782.00 | | | | |
| STA. 267+00.00 TO STA. 297+00.00 | 4,933.00 | | 825.96 | 315.00 | |
| STA. 297+00.00 TO STA. 327+00.00 | 2,018.00 | | 963.59 | | 21.00 |
| COUNTRY CLUB RD. | | | | | |
| STA. 100+16.53 TO STA. 123+00.00 | 4,721.00 | 50.49 | 4,909.82 | | 52.00 |
| IOWA AVE. | | | | | |
| STA. 53+00.00 TO STA. 62+65.00 | 415.00 | | 806.20 | | 49.00 |
| TOTALS | 51,494.00 | 50.49 | 7,505.57 | 315.00 | 122.00 |

◆ FOR INFORMATION PURPOSES ONLY. COST TO BE INCLUDED IN PRICE BID FOR REMOVAL OF STRUCTURES AND OBSTRUCTIONS.

NOTE: ITEMS TO BE REMOVED MAY OR MAY NOT BE PRESENT IN ANY SPECIFIED LOCATION.

NOTE: REMOVALS NOT ANTICIPATED FROM STA. 166+00 TO STA. 213+00, STA. 243+00 TO STA. 269+00 OR FROM STA. 299+00 TO STA. 315+00. ANY ITEMS ENCOUNTERED WITH IN THESE EXTENTS WHICH REQUIRE REMOVAL SHALL BE INCLUDED IN THE PRICE BID FOR REMOVAL OF STRUCTURES AND OBSTRUCTIONS.

φ CLEARANCE CONTRACT OCCURED PRIOR TO J/P24428(04). BUILDING FOUNDATIONS, SIDEWALKS, AND OTHER ASSOCIATED IMPROVEMENTS REMOVED BY OTHERS.

SUMMARY OF DITCH TREATMENT

| STATION EXTENTS AND LOCATION | DESIGN NO. | BOTTOM WIDTH | CURTAIN WALL | CLASS C CONCRETE 509(D) |
|------------------------------------|------------|--------------|--------------|-------------------------|
| | | FT | | |
| US-81 | | | | |
| STA. 146+50 TO STA. 147+55 - LT. | 2A | 8 | 3 | 19.25 |
| STA. 149+96 TO STA. 187+33 - RT. █ | 2A | 8 | 40 | 686.80 |
| STA. 222+25 TO STA. 234+00 - RT. | 2A | 8 | 13 | 210.71 |
| STA. 225+55 TO STA. 233+05 - LT. | 2A | 8 | 9 | 134.65 |
| STA. 272+00 TO STA. 276+22 - LT. | 2A | 8 | 6 | 75.98 |
| STA. 285+62 TO STA. 286+92 - LT. █ | 2A | 8 | 3 | 23.67 |
| STA. 302+07 TO STA. 306+54 - RT | 2A | 8 | 6 | 80.40 |
| I-44 | | | | |
| STA. 277+35 TO STA. 279+00 - LT. | 2A | 8 | 3 | 29.86 |
| COUNTRY CLUB RD. | | | | |
| STA. 112+96 TO STA. 116+00 - RT. | 2A | 4 | 5 | 39.62 |
| NS 281.5 | | | | |
| STA. 100+20 TO STA. 102+60 - LT. | 2A | 4 | 4 | 31.29 |
| RAMP B | | | | |
| STA. 243+00 TO STA. 250+21 - LT. | 2A | 8 | 9 | 129.53 |
| STA. 246+84 TO STA. 253+00 - RT. | 2A | 8 | 8 | 110.73 |
| RAMP D | | | | |
| STA. 243+20 TO STA. 246+00 - LT. | 2A | 8 | 4 | 50.42 |
| RAMP H | | | | |
| STA. 196+50 TO STA. 197+20 - LT. | 2A | 8 | 2 | 12.83 |
| RAMP I | | | | |
| STA. 322+00 TO STA. 324+29 - LT. | 2A | 8 | 4 | 41.40 |
| STA. 327+65 TO STA. 329+00 - LT. | 2A | 8 | 3 | 24.55 |
| RAMP J | | | | |
| STA. 2187+50 TO STA. 2196+00 - LT. | 2A | 8 | 10 | 152.56 |
| STA. 2196+96 TO STA. 2200+50 - RT. | 2A | 8 | 5 | 63.73 |
| STA. 2201+50 TO STA. 2205+00 - LT. | 2A | 8 | 5 | 63.02 |
| STA. 2206+40 TO STA. 2208+00 - RT. | 2A | 8 | 3 | 28.97 |
| STA. 2211+88 TO STA. 2212+93 - LT. | 2A | 8 | 3 | 19.25 |
| RAMP K | | | | |
| STA. 3194+00 TO STA. 3213+00 - RT. | 2A | 8 | 20 | 340.48 |
| RAMP L | | | | |
| STA. 222+95 TO STA. 223+50 - LT. | 2A | 8 | 2 | 10.18 |
| RAMP M | | | | |
| STA. 301+64 TO STA. 304+00 - LT. | 2A | 8 | 6 | 80.93 |
| STA. 308+68 TO STA. 309+52 - LT. | 2A | 8 | 2 | 15.31 |
| RAMP N | | | | |
| STA. 4198+80 TO STA. 4205+00 - LT. | 2A | 8 | 8 | 111.44 |
| STA. 4209+58 TO STA. 4211+98 - RT. | 2A | 8 | 4 | 43.34 |
| RAMP O | | | | |
| STA. 1006+00 TO STA. 1022+68 - LT. | 2A | 8 | 18 | 299.01 |
| STA. 1020+00 TO STA. 1022+10 - RT. | 2A | 8 | 4 | 38.04 |
| STA. 1034+16 TO STA. 1304+76 - LT. | 2A | 8 | 3 | 23.14 |
| RAMP Q | | | | |
| STA. 1109+06 TO STA. 1120+00 - RT. | 2A | 8 | 12 | 196.16 |
| STA. 1109+47 TO STA. 1110+16 - LT. | 2A | 8 | 2 | 12.66 |
| RAMP R | | | | |
| STA. 1150+30 TO STA. 1173+38 - RT. | 2A | 8 | 25 | 413.76 |
| RAMP S | | | | |
| STA. 1217+25 TO STA. 1219+72 - RT. | 2A | 8 | 4 | 44.58 |

SUMMARY OF DITCH TREATMENT

| STATION EXTENTS AND LOCATION | DESIGN NO. | BOTTOM WIDTH | CURTAIN WALL | CLASS C CONCRETE 509(D) |
|------------------------------------|------------|--------------|--------------|-------------------------|
| | | FT | | |
| RAMP T | | | | |
| STA. 1250+38 TO STA. 1251+10 - LT. | 2A | 8 | 2 | 13.19 |
| STA. 1250+38 TO STA. 1250+92 - RT. | 2A | 8 | 2 | 10.00 |
| RAMP U | | | | |
| STA. 1308+00 TO STA. 1310+86 - LT. | 2A | 8 | 4 | 51.48 |
| STA. 1308+00 TO STA. 1311+25 - LT. | 2A | 8 | 5 | 58.60 |
| STA. 1313+00 TO STA. 1316+25 - LT. | 2A | 8 | 5 | 58.60 |
| STA. 1313+00 TO STA. 1316+25 - LT. | 2A | 8 | 5 | 58.60 |
| RAMP V | | | | |
| STA. 1350+34 TO STA. 1352+92 - LT. | 2A | 8 | 4 | 46.53 |
| STA. 1350+34 TO STA. 1353+00 - LT. | 2A | 8 | 4 | 47.94 |
| TOTALS | | 326 | | 4,450.10 |

█ QUANTITY INCLUDES RT. DITCHES FOR RAMP D FROM STA. 249+80 TO STA. 264+50 & RAMP K FROM STA. 3173+00 TO STA. 3188+00.

█ QUANTITY INCLUDES LT. DITCH FOR RAMP P FROM STA. 1068+55 TO STA. 1070+06.

SUMMARY OF FENCE

| STATION EXTENTS AND LOCATION | FENCE - STYLE WWF 624(A) |
|--|--------------------------|
| | LF |
| US 81 | |
| STA. 149+00.00 TO STA. 149+50.00 - RT. | 73 |
| STA. 149+92.00 TO STA. 149+99.00 - RT. | 30 |
| STA. 151+08.00 TO STA. 151+25.00 - LT. | 72 |
| STA. 151+72.00 TO STA. 151+95.00 - LT. | 30 |
| STA. 214+55.00 TO STA. 214+97.00 - LT. | 46 |
| STA. 215+61.00 TO STA. 216+20.00 - LT. | 80 |
| STA. 217+76.00 TO STA. 217+77.00 - RT. | 40 |
| STA. 218+43.00 TO STA. 218+95.00 - RT. | 61 |
| STA. 306+14.00 TO STA. 307+09.00 - LT. | 142 |
| STA. 307+74.00 TO STA. 307+97.00 - RT. | 83 |
| STA. 308+26.00 TO STA. 308+45.00 - LT. | 78 |
| STA. 309+20.00 TO STA. 309+92.00 - RT. | 96 |
| STA. 324+50.00 TO STA. 325+19.00 - LT. | 106 |
| STA. 325+81.00 TO STA. 325+85.00 - RT. | 13 |
| STA. 325+68.00 TO STA. 325+84.00 - LT. | 61 |
| STA. 326+33.00 TO STA. 326+57.00 - RT. | 34 |
| STA. 358+47.00 TO STA. 358+60.00 - RT. | 73 |
| STA. 359+11.00 TO STA. 359+22.00 - RT. | 45 |
| STA. 359+19.00 TO STA. 359+41.00 - LT. | 92 |
| STA. 359+93.00 TO STA. 361+04.00 - LT. | 155 |
| STA. 365+52.00 TO STA. 391+83.00 - RT. | 3,006 |
| STA. 422+88.00 TO STA. 432+30.00 - LT. | 994 |
| TOTALS | 5,410 |

SUMMARY OF DRIVES

| STATION AND LOCATION | WIDTH x LENGTH | TYPE | RADI (R1) | RADI (R2) | 3" TBSC TYPE E 402(E) | TACK COAT 407(B) | PRIME COAT 408 | (2-2.5") 5" SUPERPAVE, TYPE S3 (PG 64-22 OK) 411(B) | 2" SUPERPAVE, TYPE S4 (PG 64-22 OK) 411(C) |
|----------------------|------------------|-------|-----------|-----------|-----------------------|------------------|----------------|---|--|
| | | | | | TON | GAL | GAL | TON | TON |
| COUNTY RD. | | | | | | | | | |
| STA. 306+15 - RT. | 18.00' x 89.44' | TBSC. | 15' | 15' | 68.23 | | | | |
| STA. 311+98 - RT. | 16.00' x 94.28' | TBSC. | 15' | 15' | 62.11 | | | | |
| EW 135.5 | | | | | | | | | |
| STA. 24+85 - LT. | 16.00' x 26.17' | TBSC. | 15' | 15' | 20.60 | | | | |
| STA. 25+77 - RT. | 16.00' x 554.85' | TBSC. | 25' | 25' | 372.60 | | | | |
| US-62 | | | | | | | | | |
| STA. 394+77 - RT. | 22.00' x 96.24' | 2 | 30' | 30' | | 42.90 | 102.90 | 80.82 | 31.50 |
| TOTALS | | | | | 523.54 | 42.90 | 102.90 | 80.82 | 31.50 |

● TBSC QUANTITIES ARE FOR TEMPORARY DRIVES - 2 APPLICATIONS

△ QUANTITY INCLUDED IN MAINLINE SURFACING QUANTITIES

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY SHEET

| SUMMARY OF EARTHWORK QUANTITIES | | | | |
|------------------------------------|--------------------------------|-----------|-------------------|----------------------------|
| STATION EXTENTS | UNCLASSIFIED EXCAVATION 202(A) | FILL +15% | EXCESS EXCAVATION | UNCLASSIFIED BORROW 202(D) |
| | CY | CY | CY | CY |
| US 81 | | | | |
| STA. 141+22.00 TO STA. 315+00.00 | 718,140 | 2,329,872 | | 1,611,732 |
| STA. 315+00.00 TO STA. 488+79.07 | 2,467,612 | 919,660 | 1,547,952 | |
| US 81 INTERIM CONN | | | | |
| STA. 169+13.00 TO STA. 186+80.91 | 211 | 2,148 | | 1,937 |
| US 81 INTERIM CONN 2 | | | | |
| STA. 489+14.00 TO STA. 506+88.56 | 1,232 | 14,104 | | 12,872 |
| I-44 | | | | |
| STA. 245+65.00 TO STA. 333+30.00 | 66,476 | 51,681 | 14,795 | |
| COUNTRY CLUB RD | | | | |
| STA. 99+71.46 TO STA. 125+00.00 | 92,171 | 1,342 | 90,829 | |
| COUNTY RD | | | | |
| STA. 300+00.00 TO STA. 327+43.31 | 9,989 | 482 | 9,507 | |
| IOWA AVE | | | | |
| STA. 53+80.00 TO STA. 62+88.51 | 2,488 | 20 | 2,468 | |
| NS 281.5 | | | | |
| STA. 100+00.00 TO STA. 104+15.00 | 823 | 22 | 801 | |
| EW 135.5 | | | | |
| STA. 11+00.00 TO STA. 26+00.00 | 2,467 | 1,536 | 931 | |
| RAMP A | | | | |
| STA. 140+85.40 TO STA. 153+00.00 | 44,067 | 2,521 | 41,546 | |
| RAMP A EXT | | | | |
| STA. 243+00.00 TO STA. 246+26.56 | 1,053 | - | 1,053 | |
| RAMP B | | | | |
| STA. 243+00.00 TO STA. 263+72.32 | 42,426 | 74,802 | | 32,376 |
| RAMP D | | | | |
| STA. 244+00.00 TO STA. 264+50.00 | 58,743 | 85,618 | | 26,875 |
| RAMP G | | | | |
| STA. 1173+39.86 TO STA. 1210+46.26 | 89,670 | 40,290 | 49,380 | |
| RAMP H | | | | |
| STA. 193+03.36 TO STA. 210+19.06 | 10,652 | 31,681 | | 21,029 |
| RAMP I | | | | |
| STA. 315+31.22 TO STA. 332+97.66 | 13,446 | 96,621 | | 83,175 |
| RAMP J | | | | |
| STA. 2177+97.85 TO STA. 2217+31.21 | 20,225 | 175,158 | | 154,933 |
| RAMP K | | | | |
| STA. 3172+99.80 TO STA. 3213+00.38 | 47,866 | 61,132 | | 13,266 |

| SUMMARY OF EARTHWORK QUANTITIES | | | | |
|------------------------------------|--------------------------------|-----------|-------------------|----------------------------|
| STATION EXTENTS | UNCLASSIFIED EXCAVATION 202(A) | FILL +15% | EXCESS EXCAVATION | UNCLASSIFIED BORROW 202(D) |
| | CY | CY | CY | CY |
| RAMP L | | | | |
| STA. 210+19.06 TO STA. 227+77.54 | 20,446 | 41,941 | | 21,495 |
| RAMP M | | | | |
| STA. 298+37.48 TO STA. 315+35.29 | 15,698 | 201,730 | | 186,032 |
| RAMP N | | | | |
| STA. 4175+94.04 TO STA. 4215+37.31 | 49,688 | 302,305 | | 252,617 |
| RAMP O | | | | |
| STA. 998+66.78 TO STA. 1035+09.04 | 89,403 | 9,619 | 79,784 | |
| RAMP P | | | | |
| STA. 1050+12.00 TO STA. 1070+06.58 | 142,104 | 97 | 142,007 | |
| RAMP Q | | | | |
| STA. 1090+52.93 TO STA. 1120+52.80 | 482,197 | - | 482,197 | |
| RAMP Q INTERIM CONN | | | | |
| STA. 1076+54.10 TO STA. 1100+50.00 | 696 | 1,496 | | 800 |
| RAMP R | | | | |
| STA. 1150+12.00 TO STA. 1173+38.66 | 160,795 | 965 | 159,830 | |
| RAMP R INTERIM CONN | | | | |
| STA. 169+13.00 TO STA. 186+80.91 | 1,225 | 698 | 527 | |
| RAMP S | | | | |
| STA. 1207+76.56 TO STA. 1225+27.60 | 21,832 | 21,674 | 158 | |
| RAMP T | | | | |
| STA. 1250+12.00 TO STA. 1273+78.42 | 18,183 | 45,112 | | 26,929 |
| RAMP U | | | | |
| STA. 1298+76.72 TO STA. 1316+53.91 | 6,576 | 39,585 | | 33,009 |
| RAMP U INTERIM CONN | | | | |
| STA. 1291+87.82 TO STA. 1307+00.00 | 254 | 1,096 | | 842 |
| RAMP V | | | | |
| STA. 1350+12.00 TO STA. 1374+12.67 | 14,273 | 62,819 | | 48,546 |
| RAMP V INTERIM CONN | | | | |
| STA. 1360+75.00 TO STA. 1379+79.43 | 484 | 1,630 | | 1,146 |
| RAMP AA | | | | |
| STA. 468+70.82 TO STA. 488+06.02 | 588 | 25,858 | | 25,270 |
| RAMP CC | | | | |
| STA. 461+68.17 TO STA. 488+12.03 | 912 | 74,224 | | 73,312 |
| TOTALS | 4,715,111 | 4,719,539 | 1,484 | 5,912.20 |

• UTILIZE 2,622,281 CY OF EXCESS EXCAVATION TO REDUCE UNCLASSIFIED BORROW

SUMMARY OF EROSION CONTROL

| STATION EXTENTS | LOCATION AND DESCRIPTION | SOLID SLAB SODDING 230(A) | TYPE I-A PLAIN RIPRAP 60(B) | TYPE I-A FILTER BLANKET 60(C) |
|----------------------------------|----------------------------|---------------------------|-----------------------------|-------------------------------|
| | | SY | TON | TON |
| US 81 | | | | |
| STA. 140+77.60 TO STA. 149+00.00 | LEFT, MEDIAN, & RIGHT | 13,402 | | |
| STA. 143+15.00 | STR. 8 AROUND OUTLET RT. | | 28.18 | 8.95 |
| STA. 149+00.00 TO STA. 164+00.00 | LEFT, MEDIAN, & RIGHT | 12,541 | | |
| STA. 150+34.00 | STR. 11 AROUND OUTLET RT. | | 164.25 | 52.18 |
| STA. 164+00.00 TO STA. 179+00.00 | LEFT, MEDIAN, & RIGHT | 31,249 | | |
| STA. 176+00.00 | STR. 18 AROUND OUTLET LT. | | 22.21 | 7.06 |
| STA. 179+00.00 TO STA. 194+00.00 | LEFT, MEDIAN, & RIGHT | 17,329 | | |
| STA. 180+00.00 | STR. 20 AROUND OUTLET LT. | | 21.03 | 6.68 |
| STA. 184+00.00 | STR. 23 AROUND OUTLET RT. | | 11.62 | 3.69 |
| STA. 188+00.00 | STR. 24 AROUND OUTLET LT. | | 2.26 | 0.72 |
| STA. 188+00.00 | STR. 26 AROUND OUTLET RT. | | 5.41 | 1.72 |
| STA. 193+00.00 | STR. 27 AROUND OUTLET LT. | | 12.35 | 3.92 |
| STA. 193+00.00 | STR. 28 AROUND OUTLET RT. | | 11.73 | 3.73 |
| STA. 194+00.00 TO STA. 209+00.00 | LEFT, MEDIAN, & RIGHT | 21,306 | | |
| STA. 195+18.00 | STR. 29 AROUND OUTLET RT | | 30.26 | 9.61 |
| STA. 199+00.00 | STR. 69 AROUND OUTLET RT. | | 30.86 | 9.80 |
| STA. 200+00.00 | STR. 31 AROUND OUTLET RT. | | 25.66 | 8.15 |
| STA. 204+74.00 | STR. 32 AROUND OUTLET LT. | | 5.41 | 1.72 |
| STA. 209+00.00 TO STA. 224+00.00 | LEFT, MEDIAN, & RIGHT | 12,189 | | |
| STA. 213+00.00 | STR. 38 AROUND OUTLET LT. | | 5.41 | 1.72 |
| STA. 217+65.00 | STR. 40 AROUND OUTLET RT. | | 344.60 | 109.47 |
| STA. 220+00.00 | STR. 45 AROUND OUTLET RT. | | 5.41 | 1.72 |
| STA. 224+00.00 TO STA. 239+00.00 | LEFT, MEDIAN, & RIGHT | 31,218 | | |
| STA. 225+60.00 | STR. 51 AROUND OUTLET RT. | | 36.56 | 11.61 |
| STA. 239+00.00 TO STA. 246+00.00 | LEFT, MEDIAN, & RIGHT | 23,128 | | |
| STA. 240+00.00 | STR. 124 AROUND OUTLET RT. | | 5.41 | 1.72 |
| STA. 242+50.00 | STR. 125 AROUND OUTLET RT. | | 37.01 | 11.76 |
| STA. 246+00.00 TO STA. 261+00.00 | LEFT, MEDIAN, & RIGHT | 21,455 | | |
| STA. 261+00.00 TO STA. 276+00.00 | LEFT, MEDIAN, & RIGHT | 22,436 | | |
| STA. 275+65.00 | DITCH LT. | | 23.25 | 7.38 |
| STA. 276+00.00 TO STA. 291+00.00 | LEFT, MEDIAN, & RIGHT | 18,155 | | |
| STA. 282+00.00 | STR. 175 AROUND OUTLET LT. | | 29.34 | 9.32 |
| STA. 286+92.00 | ACROSS DITCH LEFT | | 22.63 | 7.19 |
| STA. 290+00.00 | STR. 178 AROUND OUTLET LT. | | 15.30 | 4.86 |
| STA. 291+00.00 TO STA. 306+00.00 | LEFT, MEDIAN, & RIGHT | 25,629 | | |
| STA. 306+00.00 TO STA. 323+00.00 | LEFT, MEDIAN, & RIGHT | 26,723 | | |
| STA. 314+75.00 | STR. 185 AROUND OUTLET RT. | | 3.17 | 1.01 |
| STA. 321+00 | STR. 133 AROUND OUTLET | | 11.87 | 3.77 |
| STA. 322+00 | STR. 134 AROUND OUTLET | | 15.31 | 4.86 |
| STA. 323+00.00 TO STA. 338+00.00 | LT., RT. AND MEDIAN | 18,464 | | |
| STA. 325+76.00 | STR. 137 AROUND OUTLET RT. | | 95.73 | 30.41 |
| STA. 334+00.00 | STR. 139 AROUND OUTLET RT. | | 49.47 | 15.71 |
| STA. 338+00.00 TO STA. 353+00.00 | LT., RT. AND MEDIAN | 24,554 | | |
| STA. 341+00.00 | STR. 142 AROUND OUTLET RT. | | 26.22 | 8.33 |
| STA. 353+00.00 TO STA. 368+00.00 | LT., RT. AND MEDIAN | 22,703 | | |
| STA. 359+27.00 | STR. 145 AROUND OUTLET RT. | | 110.05 | 34.96 |
| STA. 365+00.00 | STR. 146 AROUND OUTLET RT. | | 16.18 | 5.14 |
| STA. 366+00.00 | STR. 229 AROUND OUTLET RT. | | 16.14 | 5.13 |
| STA. 368+00.00 TO STA. 383+00.00 | LT., RT. AND MEDIAN | 54,645 | | |
| STA. 383+00.00 TO STA. 396+00.00 | LT., RT. AND MEDIAN | 40,461 | | |
| STA. 389+50.00 | STR. 207 AROUND OUTLET LT. | | 17.12 | 5.44 |
| STA. 396+00.00 TO STA. 411+00.00 | LT., RT. AND MEDIAN | 20,728 | | |
| STA. 406+00.00 | STR. 150 AROUND OUTLET RT. | | 17.05 | 5.42 |
| STA. 411+00.00 | STR. 208 AROUND OUTLET RT. | | 12.38 | 3.93 |

SUMMARY OF EROSION CONTROL

| STATION EXTENTS | LOCATION AND DESCRIPTION | SOLID SLAB SODDING 230(A) | TYPE I-A PLAIN RIPRAP 60(B) | TYPE I-A FILTER BLANKET 60(C) |
|------------------------------------|----------------------------|---------------------------|-----------------------------|-------------------------------|
| | | SY | TON | TON |
| STA. 411+00.00 TO STA. 426+00.00 | LT., RT. AND MEDIAN | 16,560 | | |
| STA. 412+52.00 | STR. 151 AROUND OUTLET LT. | | 8.86 | 2.81 |
| STA. 412+74.00 | STR. 152 AROUND OUTLET RT. | | 14.54 | 4.62 |
| STA. 418+00.00 | STR. 154 AROUND OUTLET RT. | | 13.16 | 4.18 |
| STA. 425+00.00 | STR. 209 AROUND OUTLET RT. | | 13.45 | 4.27 |
| STA. 426+00.00 TO STA. 441+00.00 | LT., RT. AND MEDIAN | 43,060 | | |
| STA. 432+00.0 | STR. 210 AROUND OUTLET RT. | | 13.45 | 4.27 |
| STA. 440+00.00 | STR. 211 AROUND OUTLET RT. | | 11.88 | 3.77 |
| STA. 441+00.00 TO STA. 456+00.00 | LT., RT. AND MEDIAN | 27,745 | | |
| STA. 449+00.00 | STR. 215 AROUND OUTLET LT. | | 14.22 | 4.52 |
| STA. 456+00.00 TO STA. 471+00.00 | LT., RT. AND MEDIAN | 17,442 | | |
| STA. 457+00.00 | STR. 160 AROUND OUTLET LT. | | 5.41 | 1.72 |
| STA. 468+50.00 | STR. 212 AROUND OUTLET RT. | | 3.17 | 1.01 |
| STA. 469+43.00 | STR. 161 AROUND OUTLET RT. | | 7.73 | 2.45 |
| STA. 470+50.00 | STR. 213 AROUND OUTLET RT. | | 2.26 | 0.72 |
| STA. 471+00.00 TO STA. 485+00.00 | LT., RT. AND MEDIAN | 9,376 | | |
| STA. 482+50.00 | STR. 216 AROUND OUTLET RT. | | 2.26 | 0.72 |
| STA. 485+00.00 TO STA. E.P. | LT., RT. AND MEDIAN | 4,553 | | |
| RAMP A | | | | |
| STA. 142+15.57 TO STA. 153+00.00 | LEFT SIDE | 9,674 | | |
| STA. 146+95.00 | RIGHT SIDE | | 14.90 | 4.73 |
| RAMP A EXTENSION | | | | |
| STA. 243+00.00 TO STA. 246+26.56 | RIGHT SIDE | 158 | | |
| RAMP B | | | | |
| STA. 243+00.00 TO STA. 253+00.00 | LEFT & RIGHT SIDE | 14,554 | | |
| STA. 253+00.00 TO STA. 263+72.12 | LEFT & RIGHT SIDE | 12,304 | | |
| RAMP D | | | | |
| STA. 244+00.00 TO STA. 258+00.00 | LEFT & RIGHT SIDE | 20,823 | | |
| STA. 258+00.00 TO STA. 264+50.00 | LEFT & RIGHT SIDE | 7,548 | | |
| RAMP G | | | | |
| STA. 1173+39.86 TO STA. 1184+00.00 | LEFT & RIGHT SIDE | 18,146 | | |
| STA. 1182+20.00 | STR. 65 AROUND OUTLET LT. | | 22.48 | 7.14 |
| STA. 1184+00.00 TO STA. 1199+00.00 | | 22,137 | | |
| STA. 1185+50.00 | STR. 66 AROUND OUTLET LT. | | 44.66 | 14.19 |
| STA. 1192+00.00 | STR. 68 AROUND OUTLET LT. | | 23.23 | 7.38 |
| STA. 1199+00.00 TO STA. 1201+50.16 | | 2,069 | | |
| STA. 1199+02.00 | STR. 67 AROUND OUTLET LT. | | 22.43 | 7.12 |
| RAMP H | | | | |
| STA. 193+36.11 TO STA. 196+00.00 | RIGHT SIDE | 2,545 | | |
| STA. 196+00.00 TO STA. 205+53.80 | LEFT & RIGHT SIDE | 10,161 | | |
| STA. 197+16.00 | ACROSS DITCH LEFT | | 4.56 | 1.45 |
| RAMP I | | | | |
| STA. 316+44.09 TO STA. 320+00.00 | LEFT & RIGHT SIDE | 2,365 | | |
| STA. 320+00.00 TO STA. 332+95.46 | LEFT & RIGHT SIDE | 17,441 | | |
| STA. 327+65.00 | ACROSS DITCH LEFT | | 3.64 | 1.16 |
| RAMP J | | | | |
| STA. 2187+05.34 TO STA. 2190+00.00 | LEFT & RIGHT SIDE | 2,530 | | |
| STA. 2190+00.00 TO STA. 2205+00.00 | LEFT & RIGHT SIDE | 17,480 | | |
| STA. 2191+00.00 | STR. 70 AROUND OUTLET LT. | | 8.19 | 2.60 |
| STA. 2205+00.00 TO STA. 2217+31.21 | LEFT & RIGHT SIDE | 12,710 | | |
| STA. 2205+00 | END OF CONCRETE DITCH LT. | | 7.66 | 2.43 |
| RAMP K | | | | |
| STA. 3172+99.80 TO STA. 3179+00.00 | LEFT & RIGHT SIDE | 10,491 | | |
| STA. 3179+00.00 TO STA. 3194+00.00 | LEFT & RIGHT SIDE | 14,415 | | |
| STA. 3194+00.00 TO STA. 3203+65.41 | LEFT & RIGHT SIDE | 10,568 | | |
| STA. 3199+00.00 | STR. 74 AROUND OUTLET RT. | | 22.44 | 7.13 |

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY SHEET

| SUMMARY OF EROSION CONTROL | | | | |
|------------------------------------|----------------------------|---------------------------|-----------------------------|-------------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | SOLID SLAB SODDING 230(A) | TYPE I-A PLAIN RIPRAP 60(B) | TYPE I-A FILTER BLANKET 60(C) |
| | | SY | TON | TON |
| RAMP L | | | | |
| STA. 214+68.36 TO STA. 215+00.00 | RIGHT SIDE | 1,601 | | |
| STA. 215+00.00 TO STA. 226+76.90 | LEFT & RIGHT SIDE | 14,160 | | |
| STA. 222+95.00 | ACROSS DITCH LEFT | | 8.99 | 2.86 |
| RAMP M | | | | |
| STA. 298+40.59 TO STA. 302+00.00 | LEFT & RIGHT SIDE | 5,649 | | |
| STA. 302+00.00 TO STA. 310+50.38 | LEFT & RIGHT SIDE | 19,304 | | |
| STA. 308+68.00 | ACROSS DITCH LEFT | | 28.22 | 8.96 |
| RAMP N | | | | |
| STA. 4185+09.01 TO STA. 4215+37.31 | LEFT & RIGHT SIDE | 1,044 | | |
| STA. 4188+00.00 TO STA. 4203+00.00 | LEFT & RIGHT SIDE | 21,266 | | |
| STA. 4192+00.00 | STR. 77 AROUND OUTLET LT. | | 9.72 | 3.09 |
| STA. 4200+00.00 | STR. 78 AROUND OUTLET RT. | | 23.22 | 7.37 |
| STA. 4203+00.00 TO STA. 4215+37.31 | LEFT & RIGHT SIDE | 17,860 | | |
| STA. 4206+35.22 | STR. 79 AROUND OUTLET RT. | | 5.41 | 1.72 |
| RAMP O | | | | |
| STA. 998+66.78 TO STA. 1010+00.00 | LEFT SIDE | 6,346 | | |
| STA. 1010+00.00 TO STA. 1025+00.00 | LEFT & RIGHT SIDE | 14,661 | | |
| STA. 1022+50.00 | STR. 186 AROUND OUTLET LT. | | 28.27 | 8.98 |
| STA. 1025+00.00 TO STA. 1035+21.04 | LEFT & RIGHT SIDE | 3,486 | | |
| RAMP P | | | | |
| STA. 1050+00.00 TO STA. 1060+00.00 | LEFT & RIGHT SIDE | 7,145 | | |
| STA. 1060+00.00 TO STA. 1070+06.58 | LEFT & RIGHT SIDE | 15,314 | | |
| RAMP Q | | | | |
| STA. 1090+52.93 TO STA. 1100+00.00 | RIGHT SIDE | 15,109 | | |
| STA. 1100+00.00 TO STA. 1110+00.00 | LEFT & RIGHT SIDE | 19,350 | | |
| STA. 1109+37.00 | STR. 194 AROUND OUTLET LT. | | 36.22 | 11.50 |
| STA. 1110+00.00 TO STA. 1120+64.80 | LEFT & RIGHT SIDE | 13,608 | | |
| RAMP R | | | | |
| STA. 1150+00.00 TO STA. 1164+00.00 | LEFT & RIGHT SIDE | 15,037 | | |
| STA. 1164+00.00 TO STA. 1173+38.66 | LEFT & RIGHT SIDE | 11,811 | | |
| RAMP S | | | | |
| STA. 1207+76.56 TO STA. 1215+00.00 | LEFT SIDE | 5,328 | | |
| STA. 1215+00.00 TO STA. 1225+25.00 | LEFT & RIGHT SIDE | 10,882 | | |
| STA. 1219+85.00 | STR. 138 AROUND OUTLET RT. | | 27.41 | 8.71 |
| RAMP T | | | | |
| STA. 1250+12.00 TO STA. 1263+00.00 | LEFT & RIGHT SIDE | 11,573 | | |
| STA. 1251+00.00 | STR. 141 AROUND OUTLET RT. | | 27.86 | 8.85 |
| STA. 1263+00.00 TO STA. 12+73.42 | LEFT SIDE | 8,726 | | |
| RAMP U | | | | |
| STA. 1305+00.00 TO STA. 1313+00.00 | LEFT & RIGHT SIDE | 8,880 | | |
| STA. 1311+00.00 | STR. 140 AROUND OUTLET RT. | | 12.69 | 4.03 |
| STA. 1313+00.00 TO STA. 1316+53.90 | LEFT & RIGHT SIDE | 1,251 | | |
| RAMP V | | | | |
| STA. 1350+12.00 TO STA. 1365+00.00 | LEFT & RIGHT SIDE | 11,124 | | |
| STA. 1353+00.00 | STR. 143 AROUND OUTLET RT. | | 17.66 | 5.61 |
| STA. 1365+00.00 TO STA. 1374+12.67 | RIGHT SIDE | 6,177 | | |
| RAMP AA | | | | |
| STA. 46870+62.00 TO STA. 482+00.00 | LEFT & RIGHT SIDE | 3,731 | | |
| STA. 482+00.00 TO STA. 488+06.02 | LEFT & RIGHT SIDE | 4,454 | | |
| STA. 483+82.00 | STR. 225 AROUND OUTLET RT. | | 27.96 | 8.88 |
| RAMP CC | | | | |
| STA. 482+40.00 TO STA. 475+00.00 | LEFT & RIGHT SIDE | 8,353 | | |
| STA. 475+00.00 TO STA. 488+12.03 | LEFT & RIGHT SIDE | 7,055 | | |

| SUMMARY OF EROSION CONTROL | | | | |
|----------------------------------|----------------------------|---------------------------|-----------------------------|-------------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | SOLID SLAB SODDING 230(A) | TYPE I-A PLAIN RIPRAP 60(B) | TYPE I-A FILTER BLANKET 60(C) |
| | | SY | TON | TON |
| I-44 | | | | |
| STA. 245+65.00 TO STA. 252+00.00 | LEFT & RIGHT SIDE | 2,284 | | |
| STA. 252+00.00 TO STA. 267+00.00 | LEFT & RIGHT SIDE | 15,680 | | |
| STA. 261+25.00 | STR. 122 AROUND OUTLET LT. | | 205.19 | 65.18 |
| STA. 267+00.00 TO STA. 282+00.00 | LEFT & RIGHT SIDE | 11,445 | | |
| STA. 282+00.00 TO STA. 297+00.00 | LEFT & RIGHT SIDE | 5,669 | | |
| STA. 292+13.00 | STR. 100 AROUND OUTLET LT. | | 45.29 | 14.39 |
| STA. 297+00.00 TO STA. 312+00.00 | LEFT & RIGHT SIDE | 18,225 | | |
| STA. 312+00.00 TO STA. 327+00.00 | LEFT & RIGHT SIDE | 8,899 | | |
| STA. 327+00.00 TO STA. 333+30.00 | LEFT & RIGHT SIDE | 2,932 | | |
| STA. 328+66.00 | STR. 123 AROUND OUTLET LT. | | 66.07 | 20.99 |
| COUNTRY CLUB RD. | | | | |
| STA. 100+00.00 TO STA. 115+00.00 | LEFT & RIGHT SIDE | 11,242 | | |
| STA. 102+50.00 | STR. 189 AROUND OUTLET | | 19.89 | 6.32 |
| STA. 115+00.00 TO STA. 123+00.00 | LEFT & RIGHT SIDE | 8,549 | | |
| STA. 121+60.00 | STR. 193 AROUND OUTLET | | 8.60 | 2.73 |
| IOWA AVE. | | | | |
| STA. 53+80.00 TO STA. 62+65.00 | LEFT & RIGHT SIDE | 3,168 | | |
| EW135.5 | | | | |
| STA. 11+96.45 TO STA. 25+45.00 | LEFT & RIGHT SIDE | 3,534 | | |
| STA. 22+17.00 | STR. 165 AROUND OUTLET LT. | | 1.78 | 0.57 |
| NS 281.5 | | | | |
| STA. 101+85.00 TO STA. 104+15.00 | LEFT & RIGHT SIDE | 962 | | |
| COUNTY RD. | | | | |
| STA. 300+00.00 TO STA. 315+00.00 | LEFT & RIGHT SIDE | 7,224 | | |
| STA. 315+00.00 TO STA. 327+42.02 | LEFT & RIGHT SIDE | 6,588 | | |
| US 62 DRIVE | | | | |
| | LEFT & RIGHT SIDE | 248 | | |
| TOTALS | | 1,182,103 | 2,171.84 | 689.94 |

SUMMARIES

SUMMARY OF TEMPORARY SEDIMENT CONTROL

| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 221(C) | TEMPORARY SEDIMENT FILTER 221(D) | TEMPORARY SILT DIKE 221(F) | TEMPORARY ROCK FILTER DAM TYPE 1 221(G) | TEMPORARY FIBER LOG 221(K) |
|----------------------------|------------------------------------|------|-----------------------------|----------------------------------|----------------------------|---|----------------------------|
| | | | LF | EA | LF | C.Y. | LF |
| US 81 | | | | | | | |
| STA. 141+22 TO STA. 141+65 | ALONG TOE LT. | | 176 | | | | |
| STA. 141+22 TO STA. 142+56 | ALONG TOE RT. | | 279 | | | | |
| STA. 142+30 TO STA. 149+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 668 |
| STA. 142+00 TO STA. 149+00 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 702 |
| STA. 142+00 TO STA. 148+68 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 677 |
| STA. 142+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 144+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 145+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 145+69 TO STA. 149+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 327 |
| STA. 147+00 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 148+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 149+00 TO STA. 164+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 1,495 |
| STA. 149+00 TO STA. 155+06 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 598 |
| STA. 149+00 TO STA. 164+00 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 1,504 |
| STA. 149+50 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 149+70 | AROUND INLET LT. | | | 1 | | | |
| STA. 151+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 153+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 154+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 157+75 | ALONG MEDIAN DITCH | | | | | 5 | |
| STA. 158+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 158+75 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 159+03 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 160+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 160+35 | ALONG MEDIAN DITCH | | | | | 5 | |
| STA. 163+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 164+00 TO STA. 179+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 1,498 |
| STA. 164+00 TO STA. 173+44 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 936 |
| STA. 164+00 TO STA. 179+00 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 1,501 |
| STA. 164+38 TO STA. 172+99 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 869 |
| STA. 165+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 166+50 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 168+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 169+00 | ALONG DITCH RT. | | | | | 8 | |
| STA. 169+50 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 170+00 | ALONG DITCH LT. | | | | | 8 | |
| STA. 171+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 172+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 172+50 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |
| STA. 174+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 175+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 177+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 178+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 179+00 TO STA. 194+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 1,500 |
| STA. 179+00 TO STA. 194+00 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 1,501 |
| STA. 180+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 180+99 TO STA. 192+29 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,131 |
| STA. 181+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 182+06 TO STA. 191+89 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 984 |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

SUMMARY OF TEMPORARY SEDIMENT CONTROL

| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 221(C) | TEMPORARY SEDIMENT FILTER 221(D) | TEMPORARY SILT DIKE 221(F) | TEMPORARY ROCK FILTER DAM TYPE 1 221(G) | TEMPORARY FIBER LOG 221(K) |
|----------------------------|------------------------------------|------|-----------------------------|----------------------------------|----------------------------|---|----------------------------|
| | | | LF | EA | LF | C.Y. | LF |
| STA. 183+00 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 184+50 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 185+96 TO STA. 187+97 | ALONG TOE LT. | | 201 | | | | |
| STA. 186+00 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 186+00 | ALONG DITCH RT. | | | | | | 6 |
| STA. 188+00 | AROUND MEDIAN INLET | | | | 1 | | |
| STA. 188+03 TO STA. 190+37 | ALONG TOE LT. | | 234 | | | | |
| STA. 188+03 TO STA. 190+45 | ALONG TOE RT. | | 243 | | | | |
| STA. 189+00 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 190+75 | ACROSS DITCH LT., RT. & MEDIAN | | | | | 63 | |
| STA. 192+00 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 193+50 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 194+00 TO STA. 194+70 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 70 |
| STA. 194+00 TO STA. 194+71 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 71 |
| STA. 198+34 TO STA. 209+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 1,066 |
| STA. 198+41 TO STA. 209+00 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 1,059 |
| STA. 199+50 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 200+82 TO STA. 209+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 818 |
| STA. 201+00 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 201+23 TO STA. 209+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 778 |
| STA. 202+50 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 203+27 TO STA. 204+72 | ALONG TOE LT. | | 146 | | | | |
| STA. 204+00 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 204+75 | AROUND MEDIAN INLET | | | | 1 | | |
| STA. 204+78 TO STA. 206+31 | ALONG TOE LT. | | 155 | | | | |
| STA. 205+50 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 207+00 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 208+50 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 208+75 | AROUND MEDIAN INLET | | | | 1 | | |
| STA. 209+00 TO STA. 211+30 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 230 |
| STA. 209+00 TO STA. 224+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 1,500 |
| STA. 209+00 TO STA. 224+00 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 1,500 |
| STA. 209+00 TO STA. 212+54 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 354 |
| STA. 210+00 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 211+50 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 212+75 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 214+50 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 216+00 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 217+35 | AROUND MEDIAN INLET | | | | 1 | | |
| STA. 219+00 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 220+01 TO STA. 224+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 400 |
| STA. 220+50 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 221+05 TO STA. 224+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 295 |
| STA. 221+07 TO STA. 222+25 | ALONG TOE RT. | | 120 | | | | |
| STA. 221+75 | AROUND MEDIAN INLET | | | | 1 | | |
| STA. 222+00 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 223+15 TO STA. 225+00 | ALONG DITCH LT. | | | | | | 6 |
| STA. 223+50 | ACROSS DITCH RT. & MEDIAN | | | | | 42 | |
| STA. 224+00 TO STA. 239+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,501 |
| STA. 224+00 TO STA. 239+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 1,500 |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY SHEET

SUMMARY OF TEMPORARY SEDIMENT CONTROL

| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 221(C) | TEMPORARY SEDIMENT FILTER 221(D) | TEMPORARY SILT DIKE 221(F) | TEMPORARY ROCK FILTER DAM TYPE 1 221(G) | TEMPORARY FIBER LOG 221(K) |
|----------------------------|------------------------------------|------|-----------------------------|----------------------------------|----------------------------|---|----------------------------|
| | | | LF | EA | LF | C.Y. | LF |
| STA. 224+00 TO STA. 239+00 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 1,500 |
| STA. 224+00 TO STA. 239+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,501 |
| STA. 224+09 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 225+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 225+00 | ALONG DITCHES LT. & RT. | | | | | 11 | |
| STA. 225+60 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 226+25 | ALONG DITCHES LT. & RT. | | | | | 13 | |
| STA. 226+50 | ACROSS DITCH MEDIAN | | | | 21 | | |
| STA. 228+00 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |
| STA. 229+25 | ALONG DITCH RT. | | | | | 5 | |
| STA. 229+50 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 231+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 232+50 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 233+50 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 234+00 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 235+00 TO STA. 239+00 | ALONG TOE RT. | | 403 | | | | |
| STA. 235+50 | ACROSS DITCH MEDIAN | | | | 21 | | |
| STA. 236+00 TO STA. 239+00 | ALONG TOE LT. | | 303 | | | | |
| STA. 237+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 238+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 239+00 TO STA. 242+09 | ALONG TOE LT. | | 316 | | | | |
| STA. 239+00 TO STA. 248+64 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 964 |
| STA. 239+00 TO STA. 248+92 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 992 |
| STA. 239+00 TO STA. 248+92 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 992 |
| STA. 239+00 TO STA. 248+92 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 992 |
| STA. 239+00 TO STA. 239+97 | ALONG TOE RT. | | 99 | | | | |
| STA. 240+03 TO STA. 242+89 | ALONG TOE RT. | | 295 | | | | |
| STA. 240+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 241+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 242+18 TO STA. 250+00 | ALONG TOE LT. | | 893 | | | | |
| STA. 242+97 TO STA. 250+13 | ALONG TOE RT. | | 855 | | | | |
| STA. 243+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 244+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 246+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 247+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 252+05 TO STA. 254+00 | ALONG TOE LT. | | 293 | | | | |
| STA. 252+05 TO STA. 254+00 | ALONG TOE RT. | | 287 | | | | |
| STA. 252+98 TO STA. 253+68 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 70 |
| STA. 252+98 TO STA. 254+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 102 |
| STA. 252+85 TO STA. 254+00 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 115 |
| STA. 253+25 TO STA. 254+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 75 |
| STA. 253+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 254+00 TO STA. 257+02 | ALONG TOE LT. | | 304 | | | | |
| STA. 254+00 TO STA. 269+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 1,500 |
| STA. 254+00 TO STA. 269+00 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 1,500 |
| STA. 254+00 TO STA. 269+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,500 |
| STA. 254+00 TO STA. 257+00 | ALONG TOE RT. | | 304 | | | | |
| STA. 255+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 256+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 258+00 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

SUMMARY OF TEMPORARY SEDIMENT CONTROL

| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 221(C) | TEMPORARY SEDIMENT FILTER 221(D) | TEMPORARY SILT DIKE 221(F) | TEMPORARY ROCK FILTER DAM TYPE 1 221(G) | TEMPORARY FIBER LOG 221(K) |
|-----------------------------|------------------------------------|------|-----------------------------|----------------------------------|----------------------------|---|----------------------------|
| | | | LF | EA | LF | C.Y. | LF |
| STA. 259+50 | ACROSS DITCH RT. & MEDIAN | | | | | 42 | |
| STA. 261+00 | ACROSS DITCH RT. & MEDIAN | | | | | 42 | |
| STA. 262+50 | ACROSS DITCH RT. & MEDIAN | | | | | 42 | |
| STA. 264+00 | ACROSS DITCH RT. & MEDIAN | | | | | 42 | |
| STA. 265+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 265+50 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 267+00 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 268+00 | ALONG DITCH RT. | | | | | 6 | |
| STA. 268+50 | ACROSS DITCH RT. & MEDIAN | | | | | 42 | |
| STA. 269+00 TO STA. 284+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 1,503 |
| STA. 269+00 TO STA. 284+00 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 1,497 |
| STA. 269+80 TO STA. 2281+30 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,158 |
| STA. 269+00 TO STA. 272+12 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 317 |
| STA. 270+00 | ACROSS DITCH RT. & MEDIAN | | | | | 42 | |
| STA. 271+50 | ACROSS DITCH RT. & MEDIAN | | | | | 42 | |
| STA. 272+75 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 273+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 274+50 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 276+00 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 276+00 | ALONG DITCH LT. | | | | | 6 | |
| STA. 277+50 | ACROSS DITCH LT. & MEDIAN | | | | | 42 | |
| STA. 279+00 | ACROSS DITCH LT. & MEDIAN | | | | | 42 | |
| STA. 280+50 | ACROSS DITCH MEDIAN | | | | | 21 | |
| STA. 282+50 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 284+00 TO STA. 299+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 1,504 |
| STA. 284+00 TO STA. 299+00 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 1,495 |
| STA. 285+00 | ACROSS MEDIAN DITCH | | | | | 21 | |
| STA. 286+50 | ACROSS DITCH LT. & MEDIAN | | | | | 42 | |
| STA. 286+65 TO STA. 299+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,254 |
| STA. 287+62 TO STA. 298+53 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,078 |
| STA. 288+00 | ACROSS DITCH LT. & MEDIAN | | | | | 42 | |
| STA. 289+50 | ACROSS DITCH LT., RT. & MEDIAN | | | | | 63 | |
| STA. 290+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 291+00 | ACROSS DITCH RT. & MEDIAN | | | | | 42 | |
| STA. 292+50 | ACROSS DITCH RT. & MEDIAN | | | | | 42 | |
| STA. 293+75 | ALONG DITCH LT. | | | | | 9 | |
| STA. 294+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 295+00 | ALONG DITCH RT. | | | | | 5 | |
| STA. 295+50 | ACROSS DITCH LT. & MEDIAN | | | | | 42 | |
| STA. 297+00 | ACROSS DITCH LT. & MEDIAN | | | | | 42 | |
| STA. 299+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 298+50 | ACROSS DITCH LT. & MEDIAN | | | | | 42 | |
| STA. 299+00 TO STA. 314+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,523 |
| STA. 299+00 TO STA. 314+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 1,504 |
| STA. 299+00 TO STA. 314+00 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 1,495 |
| STA. 300+00 | ACROSS DITCH LT. & MEDIAN | | | | | 42 | |
| STA. 301+50 | ACROSS DITCH LT. & MEDIAN | | | | | 42 | |
| STA. 302+07 TO STA. 314+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,174 |
| STA. 303+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 303+25 | ALONG DITCH RT. | | | | | 6 | |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY SHEET

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|-----------------------------------|------|-------------------------------|------------------------------------|------------------------------|---|------------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 22(I)(C) | TEMPORARY SEDIMENT FILTER 22(I)(D) | TEMPORARY SILT DIKE 22(I)(F) | TEMPORARY ROCK FILTER DAM TYPE 1 22(I)(G) | TEMPORARY FIBER LOG 22(I)(K) |
| | | | LF | EA | LF | C.Y. | LF |
| STA. 303+50 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 304+50 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 304+75 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |
| STA. 305+50 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 305+75 | ALONG DITCH LT. | | | | | 6 | |
| STA. 306+00 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |
| STA. 307+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 309+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 309+61 TO STA. 314+00 | ALONG TOE RT. | | 432 | | | | |
| STA. 310+50 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 312+00 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 313+00 TO STA. 314+00 | ALONG TOE LT. | | 103 | | | | |
| STA. 313+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 314+00 TO STA. 317+00 | ALONG TOE LT. | | 306 | | | | |
| STA. 314+00 TO STA. 319+00 | ALONG TOE RT. | | 491 | | | | |
| STA. 314+00 TO STA. 323+00 | ALONG INSIDE EDGE OF BACKFILL LT | | | | | | 902 |
| STA. 314+00 TO STA. 323+00 | ALONG INSIDE EDGE OF BACKFILL RT | | | | | | 898 |
| STA. 314+00 TO STA. 323+00 | ALONG OUTSIDE EDGE OF BACKFILL LT | | | | | | 791 |
| STA. 314+00 TO STA. 323+00 | ALONG OUTSIDE EDGE OF BACKFILL RT | | | | | | 766 |
| STA. 314+75 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 317+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 318+50 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 320+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 321+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 321+50 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 322+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 323+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 323+00 TO STA. 338+00 | ALONG INSIDE EDGE OF BACKFILL LT | | | | | | 1,479 |
| STA. 323+00 TO STA. 338+00 | ALONG INSIDE EDGE OF BACKFILL RT | | | | | | 1,478 |
| STA. 324+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 326+50 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 327+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 329+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 329+22 TO STA. 337+76 | ALONG OUTSIDE EDGE OF BACKFILL LT | | | | | | 855 |
| STA. 329+69 TO STA. 337+79 | ALONG OUTSIDE EDGE OF BACKFILL RT | | | | | | 811 |
| STA. 330+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 332+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 333+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 334+04 TO STA. 338+00 | ALONG TOE LT. | | 402 | | | | |
| STA. 334+04 TO STA. 338+00 | ALONG TOE RT. | | 399 | | | | |
| STA. 335+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 336+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 338+00 TO STA. 338+53 | ALONG TOE LT. | | 77 | | | | |
| STA. 338+00 TO STA. 338+69 | ALONG TOE RT. | | 92 | | | | |
| STA. 339+62 TO STA. 340+91 | ALONG TOE RT. | | 159 | | | | |
| STA. 340+34 TO STA. 353+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 1,266 |
| STA. 340+34 TO STA. 353+00 | ALONG INSIDE EDGE OF BACKFILL RT | | | | | | 1,266 |
| STA. 340+34 TO STA. 348+15 | ALONG OUTSIDE EDGE OF BACKFILL LT | | | | | | 782 |
| STA. 340+34 TO STA. 348+91 | ALONG OUTSIDE EDGE OF BACKFILL RT | | | | | | 857 |
| STA. 340+75 | ACROSS DITCH LT. | | | | 21 | | |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|-----------------------------------|------|-------------------------------|------------------------------------|------------------------------|---|------------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 22(I)(C) | TEMPORARY SEDIMENT FILTER 22(I)(D) | TEMPORARY SILT DIKE 22(I)(F) | TEMPORARY ROCK FILTER DAM TYPE 1 22(I)(G) | TEMPORARY FIBER LOG 22(I)(K) |
| | | | LF | EA | LF | C.Y. | LF |
| STA. 341+80 | ACROSS DITCH LT. | | | | | 6 | |
| STA. 341+06 TO STA. 345+05 | ALONG TOE RT. | | 399 | | | | |
| STA. 342+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 343+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 345+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 346+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 348+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 349+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 351+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 352+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 353+00 TO STA. 368+00 | ALONG INSIDE EDGE OF BACKFILL LT | | | | | | 1,500 |
| STA. 353+00 TO STA. 368+00 | ALONG INSIDE EDGE OF BACKFILL RT | | | | | | 1,500 |
| STA. 354+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 355+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 357+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 358+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 360+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 361+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 361+50 | ACROSS DITCH LT. | | | | | 10 | |
| STA. 362+20 TO STA. 368+00 | ALONG OUTSIDE EDGE OF BACKFILL LT | | | | | | 580 |
| STA. 362+80 TO STA. 368+00 | ALONG OUTSIDE EDGE OF BACKFILL RT | | | | | | 520 |
| STA. 362+75 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |
| STA. 364+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 364+25 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 365+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 365+50 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 366+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 366+30 | ACROSS MEDIAN DITCH | | | | | 6 | |
| STA. 366+30 | ACROSS DITCH RT. | | | | | 9 | |
| STA. 367+00 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 368+00 TO STA. 383+00 | ALONG OUTSIDE EDGE OF BACKFILL LT | | | | | | 1,500 |
| STA. 368+00 TO STA. 383+00 | ALONG OUTSIDE EDGE OF BACKFILL RT | | | | | | 1,500 |
| STA. 368+00 TO STA. 383+00 | ALONG INSIDE EDGE OF BACKFILL LT | | | | | | 1,500 |
| STA. 368+00 TO STA. 383+00 | ALONG INSIDE EDGE OF BACKFILL RT | | | | | | 1,500 |
| STA. 368+50 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 370+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 371+50 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 373+00 | ACROSS DITCH LT. | | | | | 10 | |
| STA. 373+00 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |
| STA. 376+00 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |
| STA. 376+25 | ACROSS DITCH RT. | | | | | 8 | |
| STA. 376+00 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 377+50 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 379+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 380+50 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 382+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 383+00 TO STA. 398+00 | ALONG OUTSIDE EDGE OF BACKFILL LT | | | | | | 1,528 |
| STA. 383+00 TO STA. 398+00 | ALONG OUTSIDE EDGE OF BACKFILL RT | | | | | | 1,471 |
| STA. 383+00 TO STA. 398+00 | ALONG INSIDE EDGE OF BACKFILL LT | | | | | | 1,505 |
| STA. 383+00 TO STA. 398+00 | ALONG INSIDE EDGE OF BACKFILL RT | | | | | | 1,493 |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY SHEET

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|--|------|-------------------------------|------------------------------------|------------------------------|---|------------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 22(I)(C) | TEMPORARY SEDIMENT FILTER 22(I)(D) | TEMPORARY SILT DIKE 22(I)(F) | TEMPORARY ROCK FILTER DAM TYPE 1 22(I)(G) | TEMPORARY FIBER LOG 22(I)(K) |
| | | | LF | EA | LF | C.Y. | LF |
| STA. 383+50 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 384+00 | ACROSS DITCH LT. | | | | | 7 | |
| STA. 385+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 385+30 | ACROSS DITCH RT. | | | | | 8 | |
| STA. 386+50 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 388+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 389+75 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 391+00 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |
| STA. 394+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 395+50 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 397+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 398+00 TO STA. 411+79 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,311 |
| STA. 398+00 TO STA. 411+79 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,351 |
| STA. 398+00 TO STA. 413+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 1,506 |
| STA. 398+00 TO STA. 413+00 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 1,492 |
| STA. 398+50 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 400+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 401+50 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 403+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 405+00 | ALONG DITCH LT. & RT. | | | | | 22 | |
| STA. 405+50 | ALONG MEDIAN DITCH | | | | | 10 | |
| STA. 405+75 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 406+00 | ACROSS DITCH LT. & MEDIAN INLET | | | 1 | 21 | | |
| STA. 407+50 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 409+00 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 410+50 | ACROSS DITCH RT. & MEDIAN | | | | 21 | 12 | |
| STA. 411+00 | ACROSS DITCH LT. & AROUND MEDIAN INLET | | | 1 | | 12 | |
| STA. 412+22 TO STA. 413+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 80 |
| STA. 412+22 TO STA. 413+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 77 |
| STA. 413+00 TO STA. 428+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,535 |
| STA. 413+00 TO STA. 428+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,468 |
| STA. 413+00 TO STA. 428+00 | ALONG INSIDE EDGE OF BACKFILL LT. | | | | | | 1,506 |
| STA. 413+00 TO STA. 428+00 | ALONG INSIDE EDGE OF BACKFILL RT. | | | | | | 1,492 |
| STA. 413+50 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 415+00 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 416+50 | ACROSS MEDIAN DITCH | | | | 21 | | |
| STA. 417+40 | ACROSS DITCH RT. | | | | | 12 | |
| STA. 417+90 | ACROSS DITCH LT. | | | | | 12 | |
| STA. 418+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 419+50 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 421+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 422+50 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 424+00 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 424+50 | ALONG DITCH RT. | | | | | 6 | |
| STA. 425+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 425+50 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 427+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 428+00 TO STA. 443+00 | ALONG OUTSIDE EDGE OF BACKFILL LT | | | | | | 1,513 |
| STA. 428+00 TO STA. 443+00 | ALONG OUTSIDE EDGE OF BACKFILL RT | | | | | | 1,485 |
| STA. 428+00 TO STA. 443+00 | ALONG INSIDE EDGE OF BACKFILL LT | | | | | | 1,503 |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|-----------------------------------|------|-------------------------------|------------------------------------|------------------------------|---|------------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 22(I)(C) | TEMPORARY SEDIMENT FILTER 22(I)(D) | TEMPORARY SILT DIKE 22(I)(F) | TEMPORARY ROCK FILTER DAM TYPE 1 22(I)(G) | TEMPORARY FIBER LOG 22(I)(K) |
| | | | LF | EA | LF | C.Y. | LF |
| STA. 428+00 TO STA. 443+00 | ALONG INSIDE EDGE OF BACKFILL RT | | | | | | 1,496 |
| STA. 428+50 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |
| STA. 429+00 | ACROSS DITCH LT. | | | | | 8 | |
| STA. 430+00 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 431+50 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 431+35 | ACROSS DITCH RT. | | | | | 8 | |
| STA. 432+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 433+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 434+50 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 436+00 | ACROSS DITCH LT., RT. & MEDIAN | | | | 63 | | |
| STA. 437+50 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 439+00 | ACROSS DITCH LT. & MEDIAN | | | | 42 | | |
| STA. 439+35 | ACROSS DITCH RT. | | | | | 10 | |
| STA. 440+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 440+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 442+00 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |
| STA. 442+00 | ACROSS DITCH LT. | | | | | 8 | |
| STA. 442+00 TO STA. 443+00 | ALONG TOE LT. | | 100 | | | | |
| STA. 443+00 TO STA. 458+00 | ALONG TOE LT. | | 1,465 | | | | |
| STA. 443+00 TO STA. 457+92 | ALONG OUTSIDE EDGE OF BACKFILL LT | | | | | | 1,474 |
| STA. 443+00 TO STA. 457+43 | ALONG OUTSIDE EDGE OF BACKFILL RT | | | | | | 1,461 |
| STA. 443+00 TO STA. 457+92 | ALONG INSIDE EDGE OF BACKFILL LT | | | | | | 1,488 |
| STA. 443+00 TO STA. 457+43 | ALONG INSIDE EDGE OF BACKFILL RT | | | | | | 1,448 |
| STA. 443+50 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |
| STA. 445+00 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |
| STA. 446+50 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |
| STA. 448+00 TO STA. 458+00 | ALONG TOE RT. | | 1,113 | | | | |
| STA. 448+00 | ACROSS DITCH RT. & MEDIAN | | | | 42 | | |
| STA. 448+50 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 449+50 | ACROSS DITCH MEDIAN | | | | 21 | | |
| STA. 451+00 | ACROSS DITCH MEDIAN | | | | 21 | | |
| STA. 452+50 | ACROSS DITCH MEDIAN | | | | 21 | | |
| STA. 454+00 | ACROSS DITCH MEDIAN | | | | 21 | | |
| STA. 455+50 | ACROSS DITCH MEDIAN | | | | 21 | | |
| STA. 457+00 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 458+00 TO STA. 458+85 | ALONG TOE LT. & RT. | | 357 | | | | |
| STA. 459+60 TO STA. 461+68 | ALONG TOE RT. | | 385 | | | | |
| STA. 460+12 TO STA. 468+71 | ALONG TOE LT. | | 916 | | | | |
| STA. 460+46 TO STA. 473+00 | ALONG INSIDE EDGE OF BACKFILL RT | | | | | | 1,261 |
| STA. 460+46 TO STA. 461+68 | ALONG OUTSIDE EDGE OF BACKFILL RT | | | | | | 125 |
| STA. 461+11 TO STA. 473+00 | ALONG INSIDE EDGE OF BACKFILL LT | | | | | | 1,181 |
| STA. 461+11 TO STA. 468+71 | ALONG OUTSIDE EDGE OF BACKFILL LT | | | | | | 740 |
| STA. 462+50 | ACROSS DITCH MEDIAN | | | | 21 | | |
| STA. 464+00 | ACROSS DITCH MEDIAN | | | | 21 | | |
| STA. 465+50 | ACROSS DITCH MEDIAN | | | | 21 | | |
| STA. 467+00 | ACROSS DITCH MEDIAN | | | | 21 | | |
| STA. 468+50 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 469+43 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 470+50 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 471+50 | ACROSS DITCH MEDIAN | | | | 21 | | |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY SHEET

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|------------------------------------|------|-----------------------------|----------------------------------|----------------------------|---|----------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 221(C) | TEMPORARY SEDIMENT FILTER 221(D) | TEMPORARY SILT DIKE 221(F) | TEMPORARY ROCK FILTER DAM TYPE 1 221(G) | TEMPORARY FIBER LOG 221(K) |
| | | | LF | EA | LF | C.Y. | LF |
| STA. 473+00 | ACROSS DITCH MEDIAN | | | | | 21 | |
| STA. 473+00 TO STA. 475+08 | ALONG INSIDE EDGE OF BACKFILL LT | | | | | | 207 |
| STA. 473+00 TO STA. 475+43 | ALONG INSIDE EDGE OF BACKFILL RT | | | | | | 245 |
| STA. 473+78 TO STA. 475+08 | ALONG OUTSIDE EDGE OF BACKFILL LT | | | | | | 128 |
| STA. 474+09 TO STA. 475+43 | ALONG OUTSIDE EDGE OF BACKFILL RT | | | | | | 137 |
| STA. 481+20 TO STA. 487+18 | ALONG TOE RT. | | 787 | | | | |
| STA. 481+20 TO STA. 488+14 | ALONG TOE LT. | | 827 | | | | |
| STA. 481+76 TO STA. 486+38 | ALONG INSIDE EDGE OF BACKFILL LT | | | | | | 464 |
| STA. 481+76 TO STA. 486+38 | ALONG INSIDE EDGE OF BACKFILL RT | | | | | | 423 |
| STA. 482+00 TO STA. 486+22 | ALONG OUTSIDE EDGE OF BACKFILL LT | | | | | | 682 |
| STA. 482+00 TO STA. 486+22 | ALONG OUTSIDE EDGE OF BACKFILL RT | | | | | | 423 |
| STA. 482+50 | AROUND MEDIAN INLET | | | 1 | | | |
| STA. 483+50 | ACROSS DITCH MEDIAN | | | | | 21 | |
| STA. 485+00 | ACROSS DITCH MEDIAN | | | | | 21 | |
| RAMP A | | | | | | | |
| STA. 142+15 TO STA. 148+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 556 |
| STA. 142+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 143+25 | ALONG DITCH LT. | | | | | | 4 |
| STA. 145+44 TO STA. 148+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 280 |
| STA. 145+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 147+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 148+00 TO STA. 154+26 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 570 |
| STA. 148+00 TO STA. 149+90 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 208 |
| STA. 148+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 150+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 151+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 153+00 | ACROSS DITCH LT. | | | | | 21 | |
| RAMP B | | | | | | | |
| STA. 242+77 TO STA. 253+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,020 |
| STA. 243+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 244+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 246+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 246+35 TO STA. 253+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 671 |
| STA. 247+50 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 249+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 249+50 | ALONG DITCH RT. | | | | | | 4 |
| STA. 250+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 250+50 | ALONG DITCH RT. | | | | | | 4 |
| STA. 251+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 252+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 252+50 | ALONG DITCH LT. | | | | | | 8 |
| STA. 253+00 TO STA. 263+72 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,066 |
| STA. 253+00 TO STA. 254+98 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 202 |
| STA. 253+50 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 255+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 256+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 258+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 259+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 260+00 | ALONG DITCH LT. | | | | | | 8 |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|------------------------------------|------|-----------------------------|----------------------------------|----------------------------|---|----------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 221(C) | TEMPORARY SEDIMENT FILTER 221(D) | TEMPORARY SILT DIKE 221(F) | TEMPORARY ROCK FILTER DAM TYPE 1 221(G) | TEMPORARY FIBER LOG 221(K) |
| | | | LF | EA | LF | C.Y. | LF |
| RAMP D | | | | | | | |
| STA. 243+15 | ACROSS DITCH RT. | | | | | | 21 |
| STA. 244+10 TO STA. 248+62 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 458 |
| STA. 244+33 TO STA. 257+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,262 |
| STA. 244+50 | ACROSS DITCH LT. & RT. | | | | | | 42 |
| STA. 246+00 | ACROSS DITCH LT. & RT. | | | | | | 42 |
| STA. 249+00 | ALONG DITCH RT. | | | | | | 7 |
| STA. 250+50 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 251+50 | ALONG DITCH RT. | | | | | | 6 |
| STA. 255+25 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 256+50 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 257+00 TO STA. 264+50 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 753 |
| STA. 258+00 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 259+50 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 260+25 | ALONG DITCH RT. | | | | | | 7 |
| STA. 264+00 | ACROSS DITCH RT. | | | | | 21 | |
| RAMP G | | | | | | | |
| STA. 1173+40 TO STA. 1184+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,055 |
| STA. 1174+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1175+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1176+25 | ALONG DITCH LT. | | | | | | 6 |
| STA. 1179+75 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1181+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1182+20 | AROUND INLET RT. | | | | | 1 | |
| STA. 1182+25 TO STA. 1184+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 177 |
| STA. 1182+50 | ALONG DITCH LT. | | | | | | 8 |
| STA. 1184+00 TO STA. 1199+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,481 |
| STA. 1184+00 TO STA. 1199+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,521 |
| STA. 1184+75 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 1185+50 | AROUND INLET RT. | | | | | 1 | |
| STA. 1186+00 | ACROSS DITCH LT. & RT. | | | | | | 42 |
| STA. 1187+50 | ACROSS DITCH LT. & RT. | | | | | | 42 |
| STA. 1189+00 | ACROSS DITCH LT. & RT. | | | | | | 42 |
| STA. 1190+50 | ACROSS DITCH LT. & RT. | | | | | | 42 |
| STA. 1191+75 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1193+50 | ACROSS DITCH LT. & RT. | | | | | | 42 |
| STA. 1195+00 | ACROSS DITCH LT. & RT. | | | | | | 42 |
| STA. 1196+50 | ACROSS DITCH LT. & RT. | | | | | | 42 |
| STA. 1198+00 | ACROSS DITCH LT. & RT. | | | | | | 42 |
| STA. 1199+00 TO STA. 1210+46 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,140 |
| STA. 1199+00 TO STA. 1201+49 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 252 |
| STA. 1199+50 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 1201+48 | ALONG DITCH LT. | | | | | | 6 |
| STA. 1202+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1204+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1205+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1207+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1208+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1208+78 TO STA. 1210+46 | ALONG TOE LT. | | | | | | 170 |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY SHEET

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|------------------------------------|------|-----------------------------|----------------------------------|----------------------------|---|----------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 221(C) | TEMPORARY SEDIMENT FILTER 221(D) | TEMPORARY SILT DIKE 221(F) | TEMPORARY ROCK FILTER DAM TYPE 1 221(G) | TEMPORARY FIBER LOG 221(K) |
| | | | LF | EA | LF | C.Y. | LF |
| RAMP H | | | | | | | |
| STA. 192+99 TO STA. 194+89 | ALONG TOE RT. | | 198 | | | | |
| STA. 193+36 TO STA. 205+50 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,123 |
| STA. 195+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 196+04 TO STA. 205+52 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,036 |
| STA. 197+00 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 197+21 TO STA. 199+29 | ALONG TOE LT. | | 270 | | | | |
| STA. 198+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 200+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 201+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 203+00 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 204+50 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 206+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 207+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 208+25 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 208+40 | AROUND INLET RT. | | | 1 | | | |
| RAMP I | | | | | | | |
| STA. 317+80 | ALONG DITCH RT. | | | | | 6 | |
| STA. 319+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 319+94 TO STA. 329+52 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,047 |
| STA. 319+95 TO STA. 331+98 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,114 |
| STA. 321+00 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 322+50 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 323+75 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 325+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 325+70 TO STA. 327+63 | ALONG TOE LT. | | 282 | | | | |
| STA. 327+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 328+50 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 331+00 | ALONG DITCH RT. | | | | | 6 | |
| STA. 331+50 | ACROSS DITCH RT. | | | | 21 | | |
| RAMP J | | | | | | | |
| STA. 2177+98 TO STA. 2181+95 | ALONG TOE LT. | | 401 | | | | |
| STA. 2177+98 TO STA. 2190+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,195 |
| STA. 2181+98 | AROUND INLET LT. | | | 1 | | | |
| STA. 2182+50 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 2184+00 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 2185+50 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 2187+00 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 2187+06 TO STA. 2190+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 298 |
| STA. 2188+50 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 2190+00 TO STA. 2205+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,482 |
| STA. 2190+00 TO STA. 2205+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,521 |
| STA. 2190+00 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 2191+00 | AROUND INLET RT. | | | 1 | | | |
| STA. 2191+50 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 2191+50 | ALONG DITCH RT. | | | | | 6 | |
| STA. 2192+00 | ALONG DITCH LT. | | | | | 6 | |
| STA. 2193+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 2194+50 | ACROSS DITCH LT. & RT. | | | | 42 | | |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|------------------------------------|------|-----------------------------|----------------------------------|----------------------------|---|----------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 221(C) | TEMPORARY SEDIMENT FILTER 221(D) | TEMPORARY SILT DIKE 221(F) | TEMPORARY ROCK FILTER DAM TYPE 1 221(G) | TEMPORARY FIBER LOG 221(K) |
| | | | LF | EA | LF | C.Y. | LF |
| STA. 2196+00 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 2197+75 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 2199+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 2199+25 | ALONG DITCH RT. | | | | | | 6 |
| STA. 2200+25 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 2200+50 TO STA. 2203+78 | ALONG TOE RT. | | 340 | | | | |
| STA. 2202+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 2203+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 2204+00 | ALONG DITCH RT. | | | | | | 5 |
| STA. 2204+75 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 2205+00 TO STA. 2217+31 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,224 |
| STA. 2205+00 TO STA. 2208+63 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 368 |
| STA. 2205+50 TO STA. 2210+16 | ALONG TOE LT. | | 439 | | | | |
| STA. 2206+50 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 2210+22 TO STA. 2211+88 | ALONG TOE LT. | | 158 | | | | |
| STA. 2212+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 2212+92 TO STA. 2217+29 | ALONG TOE LT. | | 494 | | | | |
| RAMP K | | | | | | | |
| STA. 3173+00 TO STA. 3179+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 600 |
| STA. 3173+00 | ALONG DITCH RT. | | | | | | 9 |
| STA. 3176+25 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 3178+00 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 3179+00 TO STA. 3194+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,484 |
| STA. 3179+50 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 3180+00 | ALONG DITCH RT. | | | | | | 8 |
| STA. 3181+10 | AROUND INLET LT. | | | 1 | | | |
| STA. 3181+20 TO STA. 3194+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,296 |
| STA. 3183+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 3184+25 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 3185+75 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 3187+00 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 3188+50 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 3190+00 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 3191+50 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 3193+00 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 3194+00 TO STA. 3203+64 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 976 |
| STA. 3194+00 TO STA. 32+09 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,487 |
| STA. 3194+50 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 3196+00 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 3198+75 | ALONG DITCH RT. | | | | | | 5 |
| STA. 3197+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 3198+75 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 3199+00 | AROUND INLET LT. | | | 1 | | | |
| STA. 3199+25 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 3200+50 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 3201+75 | AROUND INLET LT. | | | 1 | | | |
| STA. 3202+00 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 3203+50 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 3205+00 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 3206+50 | ACROSS DITCH RT. | | | | | 21 | |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY SHEET

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|------------------------------------|------|-----------------------------|----------------------------------|----------------------------|---|----------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 221(C) | TEMPORARY SEDIMENT FILTER 221(D) | TEMPORARY SILT DIKE 221(F) | TEMPORARY ROCK FILTER DAM TYPE 1 221(G) | TEMPORARY FIBER LOG 221(K) |
| | | | LF | EA | LF | C.Y. | LF |
| STA. 3208+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 3209+00 TO STA. 3213+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 401 |
| STA. 3209+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 3213+00 | ALONG DITCH RT. | | | | | 8 | |
| RAMP L | | | | | | | |
| STA. 213+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 214+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 214+71 TO STA. 224+40 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,058 |
| STA. 214+72 TO STA. 226+77 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,115 |
| STA. 216+00 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 217+50 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 219+00 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 220+50 TO STA. 223+25 | ALONG TOE LT. | | 226 | | | | |
| STA. 220+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 222+00 | ACROSS DITCH RT. | | | | 21 | | |
| RAMP M | | | | | | | |
| STA. 298+40 TO STA. 310+50 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,116 |
| STA. 299+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 301+16 TO STA. 310+52 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,024 |
| STA. 302+50 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 303+50 | ALONG DITCH RT. | | | | | 6 | |
| STA. 303+75 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 306+25 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 307+50 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 308+75 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 310+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 311+50 | ACROSS DITCH RT. | | | | 21 | | |
| RAMP N | | | | | | | |
| STA. 4175+94 TO STA. 4188+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,199 |
| STA. 4176+00 | ALONG DITCH RT. | | | | | 8 | |
| STA. 4179+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 4181+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 4182+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 4184+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 4185+10 TO STA. 4188+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 294 |
| STA. 4185+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 4187+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 4188+00 TO STA. 4203+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,521 |
| STA. 4188+00 TO STA. 4203+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,481 |
| STA. 4188+50 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 4190+00 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 4191+50 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 4193+00 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 4194+50 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 4196+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 4197+00 TO STA. 4199+96 | ALONG TOE RT. | | 290 | | | | |
| STA. 4198+40 | ALONG DITCH LT. | | | | | 4 | |
| STA. 4199+25 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 4200+04 TO STA. 4203+00 | ALONG TOE RT. | | 280 | | | | |
| STA. 4200+25 | ALONG DITCH LT. | | | | | 6 | |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|------------------------------------|------|-----------------------------|----------------------------------|----------------------------|---|----------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 221(C) | TEMPORARY SEDIMENT FILTER 221(D) | TEMPORARY SILT DIKE 221(F) | TEMPORARY ROCK FILTER DAM TYPE 1 221(G) | TEMPORARY FIBER LOG 221(K) |
| | | | LF | EA | LF | C.Y. | LF |
| STA. 4202+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 4203+00 TO STA. 4206+87 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 393 |
| STA. 4203+00 TO STA. 4215+37 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,229 |
| STA. 4203+00 TO STA. 4206+32 | ALONG TOE RT. | | 306 | | | | |
| STA. 4203+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 4204+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 4205+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 4206+35 | AROUND INLET LT. | | | 1 | | | |
| STA. 4206+38 TO STA. 4209+58 | ALONG TOE RT. | | 305 | | | | |
| STA. 4211+00 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 4213+04 TO STA. 4214+27 | ALONG TOE RT. | | 138 | | | | |
| STA. 4214+32 TO STA. 4215+37 | ALONG TOE RT. | | 107 | | | | |
| RAMP O | | | | | | | |
| STA. 998+67 TO STA. 1010+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,132 |
| STA. 1003+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1005+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1006+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1008+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1009+75 | ALONG DITCH LT. | | | | | | 7 |
| STA. 1010+00 TO STA. 1025+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,492 |
| STA. 1011+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1012+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1014+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1014+77 TO STA. 1025+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,033 |
| STA. 1015+50 | ALONG DITCH LT. | | | | | | 6 |
| STA. 1017+00 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 1018+50 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 1020+00 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 1022+00 | ALONG DITCH LT. | | | | | | 6 |
| STA. 1021+50 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 1023+00 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 1023+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1024+50 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 1025+00 TO STA. 1034+80 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,004 |
| STA. 1025+00 TO STA. 1027+68 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 260 |
| STA. 1025+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1026+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1027+00 TO STA. 1031+50 | ALONG TOE LT. | | 495 | | | | |
| STA. 1032+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1034+00 | ACROSS DITCH LT. | | | | | 21 | |
| RAMP P | | | | | | | |
| STA. 1050+41 TO STA. 1060+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 913 |
| STA. 1050+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1051+25 | ALONG DITCH LT. | | | | | | 6 |
| STA. 1054+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1055+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1057+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1057+00 TO STA. 1060+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 323 |
| STA. 1057+25 | AROUND INLET RT. | | | 1 | | | |
| STA. 1058+50 | ACROSS DITCH LT. | | | | | 21 | |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY SHEET

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|------------------------------------|------|-----------------------------|----------------------------------|----------------------------|---|----------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 221(C) | TEMPORARY SEDIMENT FILTER 221(D) | TEMPORARY SILT DIKE 221(F) | TEMPORARY ROCK FILTER DAM TYPE 1 221(G) | TEMPORARY FIBER LOG 221(K) |
| | | | LF | EA | LF | C.Y. | LF |
| STA. 1060+00 TO STA. 1070+07 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 970 |
| STA. 1060+00 TO STA. 1064+67 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 504 |
| STA. 1060+00 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 1061+50 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 1062+00 | AROUND INLET RT. | | | 1 | | | |
| STA. 1063+00 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 1064+90 | ALONG DITCH LT. | | | | | 6 | |
| STA. 1066+00 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 1069+00 | ACROSS DITCH LT. | | | | 21 | | |
| RAMP Q | | | | | | | |
| STA. 1090+53 TO STA. 1100+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 946 |
| STA. 1093+00 | ALONG DITCH RT. | | | | | 6 | |
| STA. 1093+37 | AROUND INLET RT. | | | 1 | | | |
| STA. 1095+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1097+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1099+75 | ALONG DITCH RT. | | | | | 7 | |
| STA. 1100+00 TO STA. 1110+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 958 |
| STA. 1100+27 | AROUND INLET RT. | | | 1 | | | |
| STA. 1103+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1104+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1105+76 TO STA. 1110+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 455 |
| STA. 1107+50 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 1108+75 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 1108+85 | ALONG DITCH RT. | | | | | 8 | |
| STA. 1109+70 | ALONG DITCH RT. | | | | | 7 | |
| STA. 1110+00 TO STA. 1111+09 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 117 |
| STA. 1110+00 TO STA. 1120+24 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 980 |
| STA. 1113+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1115+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1116+15 | ALONG DITCH RT. | | | | | 6 | |
| STA. 1118+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1119+50 | ACROSS DITCH RT. | | | | 21 | | |
| RAMP R | | | | | | | |
| STA. 1150+41 TO STA. 1164+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,377 |
| STA. 1151+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1152+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1154+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1155+50 | ALONG DITCH RT. | | | | | 7 | |
| STA. 1157+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1158+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1160+00 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1160+08 TO STA. 1164+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 396 |
| STA. 1161+50 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1163+00 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 1164+00 TO STA. 1169+92 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 604 |
| STA. 1164+00 TO STA. 1173+39 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 922 |
| STA. 1164+50 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 1166+30 | ALONG DITCH LT. | | | | | 5 | |
| STA. 1166+50 | ALONG DITCH RT. | | | | | 7 | |
| STA. 1167+50 | ACROSS DITCH LT. & RT. | | | | 42 | | |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|------------------------------------|------|-----------------------------|----------------------------------|----------------------------|---|----------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 221(C) | TEMPORARY SEDIMENT FILTER 221(D) | TEMPORARY SILT DIKE 221(F) | TEMPORARY ROCK FILTER DAM TYPE 1 221(G) | TEMPORARY FIBER LOG 221(K) |
| | | | LF | EA | LF | C.Y. | LF |
| STA. 1169+00 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 1170+50 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 1172+00 | ACROSS DITCH RT. | | | | | 21 | |
| RAMP S | | | | | | | |
| STA. 1207+77 TO STA. 1208+98 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 122 |
| STA. 1208+90 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1208+98 TO STA. 1224+46 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,547 |
| STA. 1210+45 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1210+50 TO STA. 1211+17 | ALONG TOE LT. | | 88 | | | | |
| STA. 1211+66 TO STA. 1212+74 | ALONG TOE LT. | | 110 | | | | |
| STA. 1213+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1214+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1215+18 TO STA. 1224+46 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 924 |
| STA. 1217+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1219+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1219+50 | ALONG DITCH RT. | | | | | | 6 |
| STA. 1220+50 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 1222+00 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 1223+50 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 1224+46 TO STA. 1225+02 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 55 |
| STA. 1224+46 TO STA. 1225+00 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 54 |
| RAMP T | | | | | | | |
| STA. 1250+36 TO STA. 1264+58 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 1,424 |
| STA. 1250+42 TO STA. 1259+76 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 929 |
| STA. 1251+25 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 1251+50 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 1253+00 TO STA. 1256+20 | ALONG TOE RT. | | 317 | | | | |
| STA. 1253+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1254+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1256+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1257+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1259+00 TO STA. 1262+00 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 383 |
| STA. 1263+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1264+58 TO STA. 1273+78 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 920 |
| STA. 1265+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1266+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 1269+50 | ALONG DITCH LT. | | | | | | 9 |
| STA. 1269+70 TO STA. 1270+96 | ALONG TOE LT. | | 203 | | | | |
| STA. 1271+48 TO STA. 1272+50 | ALONG TOE LT. | | 124 | | | | |
| RAMP U | | | | | | | |
| STA. 1298+77 TO STA. 1299+99 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 122 |
| STA. 1299+99 TO STA. 1315+50 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,550 |
| STA. 1300+00 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 1301+50 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 1301+83 TO STA. 1302+83 | ALONG TOE RT. | | 114 | | | | |
| STA. 1303+32 TO STA. 1310+96 | ALONG TOE RT. | | 794 | | | | |
| STA. 1306+65 TO STA. 1315+50 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 879 |
| STA. 1310+50 | ALONG DITCH LT. | | | | | 6 | |
| STA. 1311+04 TO STA. 1316+30 | ALONG TOE RT. | | 546 | | | | |
| STA. 1311+50 TO STA. 1313+00 | ALONG TOE LT. | | 150 | | | | |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY SHEET

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|------------------------------------|------|-------------------------------|------------------------------------|------------------------------|---|------------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 22(I)(C) | TEMPORARY SEDIMENT FILTER 22(I)(D) | TEMPORARY SILT DIKE 22(I)(F) | TEMPORARY ROCK FILTER DAM TYPE 1 22(I)(G) | TEMPORARY FIBER LOG 22(I)(K) |
| | | | LF | EA | LF | C.Y. | LF |
| STA. 1315+50 TO STA. 1316+28 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 78 |
| STA. 1315+50 TO STA. 1316+28 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 78 |
| STA. 1315+50 TO STA. 1316+50 | ALONG TOE RT. | | 104 | | | | |
| STA. 1316+30 | ACROSS DITCH LT. | | | | 21 | | |
| RAMP V | | | | | | | |
| STA. 1350+38 TO STA. 1360+25 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 982 |
| STA. 1350+38 TO STA. 1364+32 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,393 |
| STA. 1350+38 TO STA. 1352+94 | ALONG TOE RT. | | 270 | | | | |
| STA. 1352+85 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 1352+92 TO STA. 1356+41 | ALONG TOE LT. | | 332 | | | | |
| STA. 1353+06 TO STA. 1364+32 | ALONG TOE RT. | | 1,110 | | | | |
| STA. 1364+32 TO STA. 1374+13 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 981 |
| STA. 1364+32 TO STA. 1366+29 | ALONG TOE RT. | | 198 | | | | |
| STA. 1366+35 TO STA. 1367+94 | ALONG TOE RT. | | 160 | | | | |
| STA. 1369+00 | AROUND INLET RT. | | | 1 | | | |
| STA. 1369+33 TO STA. 1369+92 | ALONG TOE RT. | | 59 | | | | |
| STA. 1370+43 TO STA. 1371+11 | ALONG TOE RT. | | 115 | | | | |
| STA. 1371+75 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 1373+00 | ACROSS DITCH RT. | | | | 21 | | |
| RAMP AA | | | | | | | |
| STA. 468+71 TO STA. 472+95 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 418 |
| STA. 468+71 TO STA. 472+95 | ALONG TOE LT. | | 415 | | | | |
| STA. 472+95 TO STA. 475+44 | ALONG TOE LT. | | 361 | | | | |
| STA. 472+95 TO STA. 474+35 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 140 |
| STA. 473+65 TO STA. 474+35 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 71 |
| STA. 480+61 TO STA. 483+95 | ALONG TOE LT. | | 372 | | | | |
| STA. 480+61 TO STA. 483+00 | ALONG TOE RT. | | 286 | | | | |
| STA. 481+05 TO STA. 487+21 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 622 |
| STA. 481+05 TO STA. 487+96 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 682 |
| STA. 483+25 | ACROSS DITCH RT. | | | | 21 | | |
| STA. 484+75 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 485+75 TO STA. 487+74 | ALONG TOE LT. | | 239 | | | | |
| STA. 486+25 | ACROSS DITCH RT. | | | | 21 | | |
| RAMP CC | | | | | | | |
| STA. 461+68 TO STA. 473+23 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 1,163 |
| STA. 473+23 TO STA. 475+95 | ALONG TOE RT. | | 546 | | | | |
| STA. 473+23 TO STA. 476+12 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 287 |
| STA. 474+33 TO STA. 476+12 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 178 |
| STA. 482+22 TO STA. 482+63 | ALONG TOE LT. | | 94 | | | | |
| STA. 482+22 TO STA. 486+69 | ALONG TOE RT. | | 490 | | | | |
| STA. 482+73 TO STA. 486+68 | ALONG OUTSIDE EDGE OF BACKFILL RT. | | | | | | 398 |
| STA. 482+73 TO STA. 488+06 | ALONG OUTSIDE EDGE OF BACKFILL LT. | | | | | | 526 |
| STA. 482+75 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 484+25 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 485+75 | ACROSS DITCH LT. & RT. | | | | 42 | | |
| STA. 487+25 | ACROSS DITCH RT. | | | | 21 | | |
| I-44 | | | | | | | |
| STA. 246+00 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 247+50 | ACROSS DITCH LT. | | | | 21 | | |
| STA. 249+00 | ACROSS DITCH LT. | | | | 21 | | |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|--------------------------|------|-------------------------------|------------------------------------|------------------------------|---|------------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 22(I)(C) | TEMPORARY SEDIMENT FILTER 22(I)(D) | TEMPORARY SILT DIKE 22(I)(F) | TEMPORARY ROCK FILTER DAM TYPE 1 22(I)(G) | TEMPORARY FIBER LOG 22(I)(K) |
| | | | LF | EA | LF | C.Y. | LF |
| STA. 250+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 252+00 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 251+85 TO STA. 252+00 | ALONG TOE RT. | | 15 | | | | |
| STA. 252+00 TO STA. 259+00 | ALONG TOE RT. | | 741 | | | | |
| STA. 253+00 TO STA. 259+50 | ALONG TOE RT. | | 668 | | | | |
| STA. 258+20 | AROUND INLET LT. | | | 1 | | | |
| STA. 258+43 | AROUND INLET RT. | | | 1 | | | |
| STA. 259+05 TO STA. 266+54 | ALONG TOE RT. | | 861 | | | | |
| STA. 261+00 | ACROSS DITCHES RT. | | | | | 8 | |
| STA. 263+26 TO STA. 264+23 | ALONG TOE LT. | | 103 | | | | |
| STA. 278+33 TO STA. 280+50 | ALONG TOE RT. | | 218 | | | | |
| STA. 277+90 | ALONG DITCH LT. | | | | | 6 | |
| STA. 280+50 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 282+00 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 283+00 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 295+25 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 296+50 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 297+15 | ALONG DITCH LT. | | | | | 6 | |
| STA. 298+00 | ACROSS DITCH RT. | | | | | 21 | |
| STA. 299+50 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 300+36 | AROUND INLET RT. | | | 1 | | | |
| STA. 301+00 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 302+00 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 214+71 TO STA. 322+55 | ALONG TOES LT. & RT. | | 1,575 | | | | |
| STA. 325+00 | ACROSS DITCHES LT. & RT. | | | | | 16 | |
| STA. 325+10 TO STA. 326+00 | ALONG TOE LT. | | 90 | | | | |
| STA. 325+10 TO STA. 327+00 | ALONG TOE RT. | | 190 | | | | |
| STA. 327+00 TO STA. 327+50 | ALONG TOE RT. | | 50 | | | | |
| STA. 328+00 TO STA. 329+40 | ALONG TOE LT. | | 140 | | | | |
| STA. 329+50 TO STA. 333+30 | ALONG TOE RT. | | 381 | | | | |
| COUNTRY CLUB RD. | | | | | | | |
| STA. 101+50 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 103+00 | ACROSS DITCH LT. & RT. | | | | | 42 | |
| STA. 104+50 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 105+75 | ACROSS DITCH LT. | | | | | 21 | |
| STA. 106+00 | ACROSS DITCH RT. | | | | | 14 | |
| STA. 112+50 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 113+50 | ACROSS DITCH RT. | | | | | 14 | |
| STA. 115+00 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 116+50 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 118+00 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 119+50 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 121+00 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 121+60 | AROUND INLET RT. | | | 1 | | | |
| STA. 122+50 | ACROSS DITCH LT. | | | | | 14 | |
| STA. 123+00 | ACROSS DITCH RT. | | | | | 7 | |
| STA. 124+00 | ACROSS DITCH LT. | | | | | 7 | |
| COUNTY RD. | | | | | | | |
| STA. 300+42 TO STA. 301+75 | ALONG TOE RT. | | 134 | | | | |
| STA. 301+50 | ACROSS DITCH LT. | | | | | 14 | |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY SHEET

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|--------------------------|------|-----------------------------|----------------------------------|----------------------------|---|----------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 221(C) | TEMPORARY SEDIMENT FILTER 221(D) | TEMPORARY SILT DIKE 221(F) | TEMPORARY ROCK FILTER DAM TYPE 1 221(G) | TEMPORARY FIBER LOG 221(K) |
| | | | LF | EA | LF | C.Y. | LF |
| STA. 302+75 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 303+25 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 304+50 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 306+00 | ACROSS DITCH LT. | | | | | 14 | |
| STA. 307+50 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 309+00 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 310+50 TO STA. 311+50 | ALONG TOE RT. | | 100 | | | | |
| STA. 311+00 | ACROSS DITCH LT. | | | | | 14 | |
| STA. 312+50 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 315+50 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 316+00 TO STA. 317+00 | ALONG TOE RT. | | 101 | | | | |
| STA. 317+00 | ACROSS DITCH LT. | | | | | 14 | |
| STA. 318+50 | ACROSS DITCH LT. | | | | | 14 | |
| STA. 318+50 TO STA. 319+78 | ALONG TOE RT. | | 129 | | | | |
| STA. 320+00 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 321+50 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 324+25 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 325+75 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 327+10 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| IOWA AVE | | | | | | | |
| STA. 55+00 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 56+50 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 58+00 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 59+50 | ACROSS DITCH LT. & RT. | | | | | 28 | |
| STA. 61+09 TO STA. 62+65 | ALONG TOE LT. | | 160 | | | | |
| STA. 61+10 TO STA. 62+65 | ALONG TOE RT. | | 160 | | | | |
| CR 281.5 | | | | | | | |
| STA. 100+75 | ACROSS DITCH LT. | | | | | 14 | |
| STA. 102+25 | ACROSS DITCH LT. | | | | | 14 | |
| STA. 102+00 TO STA. 102+66 | ALONG TOE RT. | | 67 | | | | |
| STA. 103+35 TO STA. 104+25 | ALONG TOE RT. | | 93 | | | | |
| EW 135.5 | | | | | | | |
| STA. 11+96 TO STA. 13+00 | ALONG TOE LT. | | 105 | | | | |
| STA. 13+50 | ACROSS DITCH RT. | | | | | 7 | |
| STA. 15+00 | ACROSS DITCH LT. & RT. | | | | | 14 | |
| STA. 16+50 | ACROSS DITCH LT. & RT. | | | | | 14 | |
| STA. 17+75 | ACROSS DITCH LT. & RT. | | | | | 14 | |
| STA. 18+00 TO STA. 19+43 | ALONG TOE LT. | | 143 | | | | |
| STA. 18+25 TO STA. 19+47 | ALONG TOE RT. | | 121 | | | | |
| STA. 19+43 TO STA. 24+00 | ALONG TOE LT. | | 444 | | | | |
| STA. 19+47 TO STA. 25+41 | ALONG TOE RT. | | 628 | | | | |
| US 62 | | | | | | | |
| STA. 394+34 TO STA. 394+66 | ALONG TOE LT. | | 96 | | | | |
| STA. 394+88 TO STA. 395+18 | ALONG TOE RT. | | 96 | | | | |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

| SUMMARY OF TEMPORARY SEDIMENT CONTROL | | | | | | | |
|---------------------------------------|--------------------------|------|-----------------------------|----------------------------------|----------------------------|---|----------------------------|
| STATION EXTENTS | LOCATION AND DESCRIPTION | TYPE | TEMPORARY SILT FENCE 221(C) | TEMPORARY SEDIMENT FILTER 221(D) | TEMPORARY SILT DIKE 221(F) | TEMPORARY ROCK FILTER DAM TYPE 1 221(G) | TEMPORARY FIBER LOG 221(K) |
| | | | LF | EA | LF | C.Y. | LF |
| US 81 INTERIM CONN. 2 | | | | | | | |
| STA. 489+57 | ACROSS DITCH LT. | | | | | 8 | |
| STA. 489+63 | ACROSS DITCH RT. | | | | | 8 | |
| STA. 489+90 | ACROSS DITCH LT. | | | | 16 | | |
| STA. 490+30 TO STA. 494+78 | ALONG TOE LT. | | 457 | | | | |
| STA. 490+50 TO STA. 496+00 | ALONG TOE LT. | | 102 | | | | |
| STA. 492+17 | ACROSS DITCH RT. | | | | | 8 | |
| STA. 492+80 | ACROSS DITCH RT. | | | | | 16 | |
| STA. 494+98 | ACROSS DITCH RT. | | | | | 16 | |
| STA. 496+37 | ACROSS DITCH LT. | | | | | 16 | |
| STA. 496+92 | ACROSS DITCH LT. | | | | | 16 | |
| STA. 497+47 | ACROSS DITCH LT. | | | | | 16 | |
| STA. 498+00 | ACROSS DITCH LT. | | | | | 16 | |
| STA. 498+68 | ACROSS DITCH LT. | | | | | 16 | |
| STA. 500+29 | ACROSS DITCH LT. | | | | | 16 | |
| STA. 504+22 | ACROSS DITCH LT. | | | | | 16 | |
| STA. 506+40 | ACROSS DITCH LT. | | | | | 16 | |
| STA. 506+90 | ACROSS DITCH LT. | | | | | 8 | |
| TOTALS | | | 35,380 | 65 | 13,854 | 604 | 183,349 |

● TEMPORARY ROCK FILTER DAM TYPE 1 TO INCLUDE COST OF SEDIMENT TRAP AS SHOWN ON EROSION CONTROL SHEETS.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY SHEET

GENERAL NOTES - TRAFFIC SIGNING

REMOVED MATERIAL TO BECOME PROPERTY OF CONTRACTOR AND IT SHALL BE DISPOSED OF IN A MANNER APPROVED BY THE ENGINEER.

ANY DAMAGE CAUSED BY THE CONTRACTOR TO ANY STRUCTURES, ROADWAY SURFACES, STRIPING, RAISED PAVEMENT MARKERS, GUARDRAIL, SLOPES, AND SIGNS SHALL BE REPAIRED AT CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER.

ALL REGULATORY SIGNS SHALL HAVE HIGH INTENSITY SHEETING. THE HIGH INTENSITY SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION) FOR TYPE III SHEETING.

ALL WARNING SIGNS SHALL HAVE FLUORESCENT YELLOW SHEETING. THE FLUORESCENT YELLOW SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION) REQUIREMENTS FOR TYPE VIII SHEETING.

ALL GREEN AND BLUE SIGNS ON CONVENTIONAL HIGHWAYS SHALL HAVE HIGH INTENSITY SHEETING. THE HIGH INTENSITY SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION) FOR TYPE III SHEETING.

ALL PANEL AND OVERHEAD SIGNS SHALL HAVE TYPE III HIGH INTENSITY BACKGROUND WITH TYPE VIII LEGENDS AND BORDERS. THE TYPE III BACKGROUND AND THE TYPE VIII LEGENDS AND BORDERS SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION).

THE MANUFACTURER SHALL FURNISH A TYPE 'A' CERTIFICATION IN ACCORDANCE WITH ODOT STANDARD SPECIFICATIONS, LATEST EDITION, AND SUBSECTION 106.04. THE CERTIFICATION SHALL INCLUDE TEST RESULTS ON THE MATERIAL SUBMITTED FOR APPROVAL.

ALL BROKEN CONCRETE INCLUDING OLD SIGN FOOTINGS WITH STUBS, WASTE MATERIAL AND DEBRIS SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED FROM THE LIMITS OF THE PROJECT AND DISPOSED OF IN AN AREA APPROVED BY THE ENGINEER. NO PAYMENT SHALL BE MADE FOR THE DISPOSAL OF THIS MATERIAL. ANY PIPE POST OR WIDE FLANGE POST ABOVE THE OLD SIGN FOOTINGS SHALL BE CUT AND HANDLED AS PROPERTY OF THE STATE AND SHALL BE NEATLY STACKED ON THE JOB SITE, AS DESIGNATED BY THE ENGINEER UNTIL SUCH TIME AS DIVISION PERSONNEL CAN REMOVE THE MATERIAL FROM THE JOB SITE.

NO SPLICES SHALL BE PERMITTED IN ANY PIPE OR WIDE FLANGE SIGN POSTS.

ALL ANCHOR BOLTS SHALL BE GRADE A-36 STEEL.

THE STATIONS AND LOCATIONS OF THE SIGN PLACEMENT, AS SHOWN ON THE PLAN SHEETS, ARE APPROXIMATE. EXACT STATIONS AND LOCATIONS SHALL BE DETERMINED BY THE CONTRACTOR SO THAT THE SIGN IS INSTALLED IN ACCORDANCE WITH OTA STANDARDS AND THE MUTCD IN ORDER TO PROVIDE OPTIMUM VISIBILITY TO THE ONCOMING/ APPROACHING MOTORIST. IF A PROPOSED LOCATION CONFLICTS WITH OTHER SIGNS, UTILITIES OR OTHER ROADWAY FEATURES, THE ENGINEER SHALL BE NOTIFIED.

POST LENGTHS SHOWN ON SIGN SUMMARY ARE APPROXIMATE. EXACT LENGTH SHALL BE DETERMINED BY FIELD SURVEY BY THE CONTRACTOR.

AFTER REMOVAL OF ANY SIGN FOOTINGS, THE HOLES SHALL BE FILLED WITH SOIL AND TAMPED AND SHAPED IN A MANNER APPROVED BY THE ENGINEER.

FOR NEW OR EXISTING GROUND MOUNTED SIGNS, MAXIMUM STUB POST PROJECTION ABOVE FOOTING/GROUND LINE SHALL BE 1-3/4" +/- 1/4". MAXIMUM FOOTING PROJECTION ABOVE GROUND LINE SHALL BE NO MORE THAN 2". SHOULD ADDITIONAL SOIL BE REQUIRED, THE ENGINEER WILL DESIGNATE AN AREA TO OBTAIN ADDITIONAL SOIL. ALL ASSOCIATED COSTS SHALL BE INCLUDED IN OTHER ITEMS OF WORK.

PAY QUANTITY NOTES - TRAFFIC SIGNING

(TC-14) SEE STANDARD DRAWING PM1-1, PM2-1, PM3-1, PM4-1, PM5-1, PM6-1, PM7-1, PM8-1 (LATEST REVISION). A PART, OR ALL, OF THE QUANTITY SHOWN IS TO BE USED AS FINAL PAVEMENT MARKING.

(TS-19) QUANTITY SHOWN INCLUDES 1,450 L.F. TRAFFIC STRIPE (PLASTIC)(WHITE) AND 2,030 L.F. TRAFFIC STRIPE (PLASTIC) (YELLOW) AND WILL BE MEASURED BY THE LINEAR FOOT OF FOUR INCH (4") WIDE TRAFFIC STRIPE.

(TS-23) QUANTITY SHOWN INCLUDES 80 L.F. TRAFFIC STRIPE (PLASTIC)(WHITE) AND WILL BE MEASURED BY THE LINEAR FOOT OF TWENTY-FOUR INCH (24") WIDE TRAFFIC STRIPE.

(TS-33) INCLUDED IN THIS PAY ITEM IS ALL HARDWARE ASSOCIATED WITH PROPERLY ANCHORING AND MOUNTING THE HIGHWAY SIGN IN ACCORDANCE WITH O.D.O.T. PLANS AND STANDARD DRAWINGS SSA1-1 AND SSP1-1-(LATEST REVISION).

(TS-34) INCLUDED IN THIS PAY ITEM IS THE REMOVAL OF ANY EXISTING SIGNS TO BE REPLACED BY NEW ASSEMBLIES AND THE REMOVAL OF ANY EXISTING SIGNS THAT WILL BE IN CONFLICT WITH THE NEW ROADWAY OR NEW SIGNAGE.

(TS-41) *REMOVAL OF EXISTING SIGNS* SHALL INCLUDE THE REMOVAL OF A COMPLETE SIGN ASSEMBLY WHICH MAY INCLUDE MULTIPLE SIGNS, POSTS, FOOTINGS, AND ANY FOOTINGS ADJACENT TO THE SIGN ASSEMBLY. WHEN APPROVED BY THE ENGINEER, FOOTINGS MAY BE OBLITERATED TO A POINT BELOW GROUND LEVEL IN LIEU OF BEING COMPLETELY REMOVED. SEE GENERAL CONSTRUCTION NOTES FOR DISPOSAL OF OLD CONCRETE FOOTING MATERIAL.

(121) INCLUDED IN THIS ITEM IS THE REMOVAL OR COVERING OF ANY EXISTING SIGNS THAT CONFLICT WITH THE TRAFFIC CONTROL AS SHOWN IN THESE PLANS. ALSO INCLUDED IS THE COST OF UNCOVERING THESE SIGNS UPON COMPLETION OF THE PROJECT OR AS DIRECTED BY THE ENGINEER. NO ADHESIVE SHALL BE PLACED ON THE FACE OF ANY SIGN.

(122) POST LENGTHS SHOWN ON SUMMARY ARE APPROXIMATE; EXACT LENGTHS SHALL BE DETERMINED BY FIELD SURVEY BY THE CONTRACTOR.

(123) QUANTITY SHOWN INCLUDES 7,020 L.F. TRAFFIC STRIPE (MULTI-POLYMER)(WHITE), 1,420 L.F. TRAFFIC STRIPE (MULTI-POLYMER)(BLACK), AND 5,600 L.F. TRAFFIC STRIPE (MULTI-POLYMER)(YELLOW) AND WILL BE MEASURED BY THE LINEAR FOOT OF SIX INCH (6") WIDE TRAFFIC STRIPE.

(124) PAY ITEM IS FOR THE INSTALLATION OF PERMANENT BARRICADE UNITS IN ACCORDANCE WITH ODOT STANDARD DRAWING TCS4-1-01. EACH UNIT SHOWN IN THE PAY ITEMS SHALL INCLUDE THREE (3) BARRICADES AND ONE (1) "ROAD CLOSED" SIGN (R11-2) TO BE INSTALLED AS APPROVED BY THE ENGINEER. THE THREE LOCATIONS ARE SHOWN IN THE PLANS AND AS DESCRIBED BELOW:

1. FRISCO AVENUE JUST WEST OF THE PRESENT R/W FOR US-81 REALIGNMENT.
2. FRISCO AVENUE JUST EAST OF THE PRESENT R/W FOR US-81 REALIGNMENT.
3. CS 2815 JUST SOUTH OF THE PRESENT R/W FOR US-81 REALIGNMENT.

| PAY QUANTITIES | | | | | |
|--|----------|--|-------------------|------|-----------|
| 0310 TRAFFIC SIGNING AND STRIPING | | | | | |
| ITEM NO. | CODE NO. | ITEM DESCRIPTION | NOTES | UNIT | QUANTITY |
| 805(A) | 8722 | (PL) REMOVAL OF EXSTING SIGNS | (TS-41) | LSUM | 1.00 |
| 850(A) | 8110 | SHEET ALUMINUM SIGNS | (TS-34) | SF | 155.27 |
| 851(C) | 8327 | 2 1/4" SQUARE TUBE POST | (TS-33)(121)(122) | LF | 214.50 |
| 851(C) | 8330 | 2 1/2" SQUARE TUBE POST | (TS-33)(121)(122) | LF | 24.00 |
| 855(A) | 8812 | TRAFFIC STRIPE (PLASTIC) (4" WIDE) | (TS-19) | LF | 3,480.00 |
| 855(A) | 8825 | TRAFFIC STRIPE (PLASTIC) (24" WIDE) | (TS-23) | LF | 80.00 |
| 856(A) | 8535 | TRAFFIC STRIPE (MULTI-POLY.) (6" WIDE) | (123) | LF | 14,040.00 |
| 880(C) | 8845 | BARRICADES (TYPE III) | | EA | 3.00 |
| 880(C) | 8850 | PERMANENT BARRICADE UNIT | (124) | EA | 3.00 |

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PAY QUANTITIES & NOTES - TRAFFIC SIGNING & STRIPING

SUMMARY OF TRAFFIC CONTROL

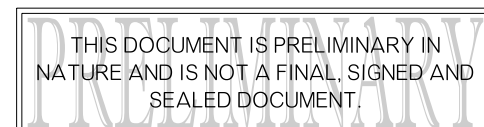
| PHASE OF CONSTRUCTION | REMARKS | ESTIMATED CALENDAR DAYS PER PHASE | (PL) | (PL) | (SP) | DELIVER | RELOCATION OF | CONSTRUCTION | CONSTRUCTION | CONSTRUCTION | CONSTRUCTION | WING | WARNING | DRUMS | PORTABLE | |
|-----------------------|------------------------------|-----------------------------------|--|---|-------------------------------------|-------------------------------|-------------------------------|--------------------|---------------------------|---------------------------|-----------------------|-----------------|-----------------|------------------|-------------------------|---------------|
| | | | CONSTRUCTION ZONE PAVEMENT MARKERS (FLEX TAB) TYPE 2-2 (WHITE) | CONSTRUCTION ZONE PAVEMENT MARKERS (FLEX TAB) TYPE 2-2 (YELLOW) | CONSTRUCTION ZONE IMPACT ATTENUATOR | PORTABLE LONGITUDINAL BARRIER | PORTABLE LONGITUDINAL BARRIER | SIGNS 0 TO 6.25 SF | SIGNS 6.26 SF TO 15.99 SF | SIGNS 16.0 SF TO 32.99 SF | BARRICADES (TYPE III) | BARRICADES | LIGHTS (TYPE A) | | CHANGEABLE MESSAGE SIGN | |
| | | | 857(E) | 857(E) | 871(B) | 877(A) | 877(B) | 880(B) | 880(B) | 880(B) | 880(C) | 880(C) | 880(E) | 880(F) | 882(B) | |
| | | | EA. | EA. | S.D. | L.F. | L.F. | S.D. | S.D. | S.D. | S.D. | S.D. | S.D. | S.D. | S.D. | |
| 1A | NS 2185 INC. 218.5 CLOSURE. | 45 | - | - | - | - | - | 180 | 360 | - | 270 | 180 | 1,080 | 720 | - | |
| 1B | 2185 & EW 135.5 INC. | 45 | - | - | - | - | - | 270 | 540 | - | 405 | 270 | 1,620 | 1,080 | - | |
| 1C | IDAHO INC. | 14 | - | - | - | - | - | 56 | 112 | - | 84 | 56 | 336 | 224 | - | |
| 1D | IDAHO & 29TH INC. | 14 | - | - | - | - | - | 112 | 224 | - | 168 | 112 | 672 | 448 | - | |
| 1E | US-62 & US-81 INC. | 45 | - | - | - | - | - | 540 | 540 | 540 | 405 | 270 | 2,160 | 1,080 | - | |
| IDAHO DTR. (1G - 4C) | IDAHO DETOUR | 120 | - | - | - | - | - | 4,320 | 3,480 | 960 | 1,680 | - | 4,080 | - | - | |
| 2A | COUNTRY CLUB & GRAND INC | 14 | - | - | - | - | - | 112 | 224 | - | 168 | 112 | 672 | 448 | - | |
| 2C, 2D | I-44, NORGE, QUAIL INC. | 28 | - | - | - | - | - | 336 | 672 | 672 | 504 | 336 | 2,016 | 1,344 | 56 | |
| 2E | NORGE DETOUR | 14 | - | - | - | - | - | 210 | 308 | 84 | 168 | - | 420 | - | - | |
| 2F | I-44 INC. | 32 | - | - | - | - | - | 128 | 256 | 256 | 320 | 128 | 1,536 | 1,024 | 64 | |
| 2G | US-81, 16TH AVE., QUAIL INC. | 45 | - | - | - | - | - | 540 | 1,080 | 1,080 | 810 | 540 | 3,240 | 2,160 | - | |
| 3A | I-44 INC. | 14 | - | - | - | - | - | 56 | 112 | 112 | 140 | 56 | 672 | 448 | 28 | |
| 3B, 3C, 3D | I-44 INSIDE SHIFT | 90 | 97 | 401 | 180 | 4,150 | - | 2,160 | 1,440 | 2,880 | 540 | 360 | 2,160 | 1,980 | 360 | |
| 3E, 3F, 3G | I-44 OUTSIDE SHIFT | 75 | 123 | 611 | 150 | - | 1,025 | 1,800 | 1,200 | 2,400 | 450 | 300 | 1,800 | 9,000 | 300 | |
| 4A, 4B, 4C | COUNTRY CLUB DETOUR | 120 | - | - | - | - | - | 1,800 | 2,040 | 720 | 1,080 | 480 | 2,160 | - | - | |
| TOTALS | | | - | 220.00 | 1,012.00 | 330.00 | 4,150.00 | 1,025.00 | 12,620.00 | 12,588.00 | 9,704.00 | 7,192.00 | 3,200.00 | 24,624.00 | 19,956.00 | 808.00 |

SUMMARY OF SIGNS

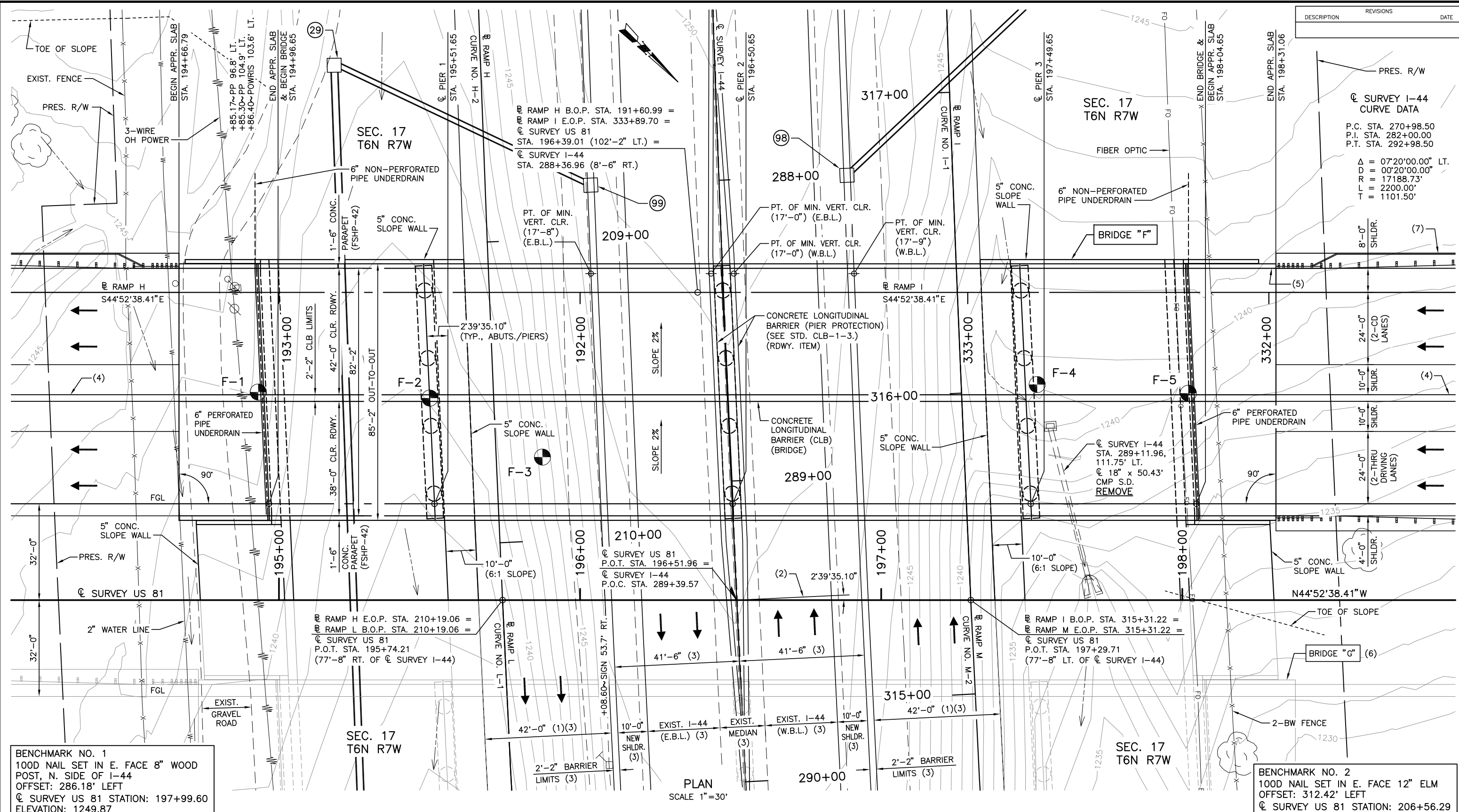
| SIGN NO. | STATION | OFFSET | ROADWAY ALIGNMENT | SIGN PANEL TYPE(S) | SIGN PANELS | | SIGN POSTS | | | | | REMARKS | | | | |
|---------------|---------|--------|-------------------|---|----------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|--------------|--------|--|--------|--------|--------|--------|
| | | | | | SHEET ALUMINUM SIGNS | 2 1/4" SQUARE TUBE POST "A" | 2 1/4" SQUARE TUBE POST "B" | 2 1/2" SQUARE TUBE POST "A" | 2 1/2" SQUARE TUBE POST "B" | POST SPACING | | | | | | |
| | | | | | | | | | | | 850(A) | | 851(C) | 851(C) | 851(C) | 851(C) |
| | | | | | | | | | | | SF | | LF | LF | LF | LF |
| 01 | - | RT. | FRISCO AVE. | W14-1 | 6.25 | 11.5 | | | | | | INSTALL JUST EAST OF 29TH | | | | |
| 02 | - | LT. | FRISCO AVE. | W14-1 | 6.25 | 11.5 | | | | | | INSTALL JUST WEST OF DAN ALLEN. | | | | |
| 03 | 325+45 | RT. | COUNTY RD. | W1-6L | 8.00 | 10.0 | 10.5 | | | 2.3 | | FACE WEST TOWARD TRAFFIC. | | | | |
| 04 | 325+85 | RT. | COUNTY RD. | W1-6R | 8.00 | 10.0 | 10.5 | | | 2.3 | | FACE NORTH TOWARD TRAFFIC. | | | | |
| 05 | 327+15 | LT. | COUNTY RD. | W14-1 | 6.25 | 11.5 | | | | | | | | | | |
| 06 | 327+15 | RT. | COUNTY RD. | R1-1 | 5.18 | 11.0 | | | | | | | | | | |
| 07 | 13+27 | RT. | EW 135.5 RD. | W1-6R | 8.00 | 10.0 | 10.5 | | | 2.3 | | FACE EAST TOWARD TRAFFIC. | | | | |
| 08 | 13+66 | RT. | EW 135.5 RD. | W1-6L | 8.00 | 10.0 | 10.5 | | | 2.3 | | FACE WEST TOWARD TRAFFIC. | | | | |
| 09 | 23+56 | RT. | EW 135.5 RD. | W1-6L | 8.00 | 10.0 | 10.5 | | | 2.3 | | FACE WEST TOWARD TRAFFIC. | | | | |
| 10 | 23+90 | RT. | EW 135.5 RD. | W1-6R | 8.00 | 10.0 | 10.5 | | | 2.3 | | FACE EAST TOWARD TRAFFIC. | | | | |
| 11 | 53+80 | CENTER | W IOWA AVE. | (3) TYPE III BARRICADES; (2) R11-2 | 20.00 | | | | | | | TO BE REMOVED BY OTHERS AFTER IOWA IS CONSTRUCTED TO THE WEST. | | | | |
| 12 | 60+50 | LT. | W IOWA AVE. | W14-1 | 6.25 | 11.5 | | | | | | | | | | |
| 13 | 60+62 | LT. | W IOWA AVE. | R1-1E; R1-3P | 8.21 | 11.5 | | | | | | FACE NORTH TOWARD CS 2815 TRAFFIC. | | | | |
| 14 | 60+65 | RT. | W IOWA AVE. | R1-1E; R1-3P; (2) SNS (CS 2815); (2) SNS (IOWA) | 17.21 | | 12.0 | | | | | | | | | |
| 15 | 61+07 | LT. | W IOWA AVE. | W14-1 | 6.25 | 11.5 | | | | | | FACE SOUTH TOWARD CS 2815 TRAFFIC. | | | | |
| 16 | 61+10 | LT. | W IOWA AVE. | R1-1E; R1-3P; (2) SNS (CS 2815); (2) SNS (IOWA) | 17.21 | | 12.0 | | | | | | | | | |
| 17 | 61+30 | RT. | W IOWA AVE. | R1-1E; R1-3P | 8.21 | 11.5 | | | | | | FACE SOUTH TOWARD CS 2815 TRAFFIC. | | | | |
| TOTALS | | | | | 155.27 | 151.50 | 63.00 | 24.00 | 0.00 | | | | | | | |
| | | | | | | 214.50 | 24.00 | | | | | | | | | |

SUMMARY OF STRIPING

| SHEET NO. | STATION TO STATION | HIGHWAY / STREET NAME | TRAFFIC STRIPE (PLASTIC) (4" WIDE) | | TRAFFIC STRIPE (PLASTIC) (24" WIDE) | TRAFFIC STRIPE (MULTI-POLYMER) (6" WIDE) | | | |
|---------------|--------------------|-----------------------|------------------------------------|-----------------|-------------------------------------|--|-----------------|------------------|--|
| | | | 856(A) | | 856(A) | 856(A) | | | |
| | | | LF | | LF | LF | | | |
| | | | WHITE | YELLOW | WHITE | WHITE | BLACK | YELLOW | |
| SIGNING (4) | 53+85 TO 62+65 | IOWA AVE. | 1,450 | 2,030 | 80 | | | | |
| SIGNING (5) | 277+00 TO 305+00 | I-44 | | | | 7,020 | 1,420 | 5,600 | |
| TOTALS | | | 1,450.00 | 2,030.00 | 80.00 | 7,020.00 | 1,420.00 | 5,600.00 | |
| | | | | | | 3,480.00 | 80.00 | 14,040.00 | |



SUMMARY OF SHEETS - TRAFFIC (1)



BENCHMARK NO. 1
 100D NAIL SET IN E. FACE 8" WOOD
 POST, N. SIDE OF I-44
 OFFSET: 286.18' LEFT
 CL SURVEY US 81 STATION: 197+99.60
 ELEVATION: 1249.87

BENCHMARK NO. 2
 100D NAIL SET IN E. FACE 12" ELM
 OFFSET: 312.42' LEFT
 CL SURVEY US 81 STATION: 206+56.29
 ELEVATION: 1230.50

- (1) NEW COLLECTOR/DISTRIBUTOR ROAD AND RAMP
- (2) RADIAL LINE AT CL SURVEY I-44 STA. 289+39.57
- (3) RADIAL DIMENSION
- (4) CONCRETE LONGITUDINAL BARRIER (RDWY. ITEM BEYOND APPR. SLABS) (SEE ROADWAY PLANS)
- (5) PROVIDE PARAPET TRANSITION AND GUARDRAIL CONNECTION HOLES PER STD. FSHP-42. SEE ROADWAY PLANS FOR GUARDRAIL REQUIREMENTS. (TYP. EA. CORNER)
- (6) BRIDGE "G" WILL BE BUILT UNDER JOB PIECE NO. 24428(17).
- (7) SEE SHEET NO. B038 FOR SLOPE DRAIN, SPLASH BASIN AND CURBING DETAILS. (TYP.)

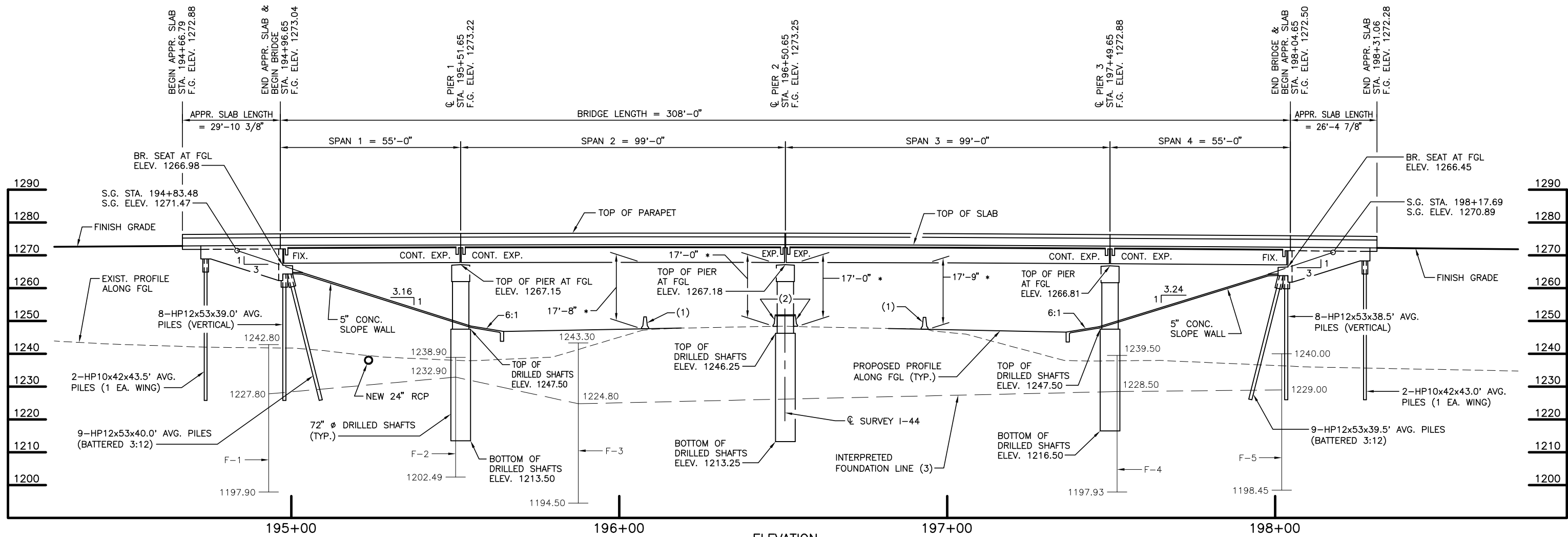
NOTES: ALL STATIONING FOLLOWS CL SURVEY US 81, UNLESS NOTED OTHERWISE.
 FOR GENERAL BRIDGE ELEVATION, SEE SHEET NO. B002.
 SEE SHEET NO. B003 FOR DESIGN DATA, FINISH GRADE DATA, FOUNDATION DATA, SUMMARY OF QUANTITIES, VERTICAL CLEARANCE SIGN DETAILS AND INDEX OF SHEETS.
 FOR FOUNDATION REPORTS, SEE SHEET NOS. B004-B006.
 SEE ROADWAY PLANS FOR PRES. AND NEW R/W, AND GEOMETRIC DATA NOT SHOWN.

| DESIGN | DATE | DESCRIPTION |
|----------|-----------|-------------|
| Design | . | |
| Drawn | PKW 01/19 | |
| Checked | . | |
| Approved | . | |
| Squad | . | |

BRIDGE "F"
 SB US-81 OVER I-44
GENERAL PLAN AND ELEVATION (1 OF 3)
 CONST. 55'-99'-99'-55' TYPE IV P.C. BEAM W/ 38'-0" AND 42'-0" CLR. RDWYS. AND CONC. PARAPETS (FSHP-42) SKEWED 2'39"35.10" RF CL STA. 196+50.65
 Job Piece No. 24428(04) GRADY COUNTY
 Sheet No. B001

M:\W2M Projects\Project Files\M179\Final Design F & G\B002-2442804-GENERAL PLAN AND ELEVATION 2.dwg 3/2/2020 11:28:09 AM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



BENCHMARK NO. 1
 100D NAIL SET IN E. FACE 8" WOOD
 POST, N. SIDE OF I-44
 OFFSET: 286.18' LEFT
 ☉ SURVEY US 81 STATION: 197+99.60
 ELEVATION: 1249.87

BENCHMARK NO. 2
 100D NAIL SET IN E. FACE 12" ELM
 OFFSET: 312.42' LEFT
 ☉ SURVEY US 81 STATION: 206+56.29
 ELEVATION: 1230.50

* MINIMUM THEORETICAL VERTICAL CLEARANCE.
 (SEE SHEET NO. B001 FOR LOCATIONS AND
 SHEET NO. B003 FOR SIGNAGE DETAILS
 AND NOTES.)

- (1) CONCRETE LONGITUDINAL BARRIER
 (SEE ROADWAY PLANS) (RDWY. ITEM)
- (2) CONCRETE LONGITUDINAL BARRIER
 (PIER PROTECTION) (SEE STD.
 CLB-1-3.) (RDWY. ITEM)
- (3) FOR ESTIMATION PURPOSES ONLY.

NOTES: ALL STATIONING FOLLOWS ☉ SURVEY US 81,
 UNLESS NOTED OTHERWISE.

FOR PLAN VIEW, SEE SHEET NO. B001.

SEE SHEET NO. B003 FOR DESIGN DATA, FINISH
 GRADE DATA, FOUNDATION DATA, SUMMARY OF
 QUANTITIES, VERTICAL CLEARANCE SIGN DETAILS
 AND INDEX OF SHEETS.

FOR FOUNDATION REPORTS, SEE SHEET NOS.
 B004-B006.

| | | | | |
|----------|-----|-------|--|----------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 01/19 | SB US-81 OVER I-44 | |
| Checked | . | | GENERAL PLAN AND ELEVATION (2 OF 3) | |
| Approved | . | | CONST. 55'-99'-99'-55' TYPE IV P.C. BEAM W/ 38'-0" AND 42'-0" | |
| Squad | | | CLR. RDWYS. AND CONC. PARAPETS (FSHP-42) SKEWED 2'39"35.10" RF | |
| | | | ☉ STA. 196+50.65 | |
| | | | Job Piece No. 24428(04) | Sheet No. B002 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B003-2442804-GENERAL PLAN AND ELEVATION 3.dwg 3/2/2020 11:32:45 AM

DESIGN DATA
(LOAD AND RESISTANCE FACTOR DESIGN)

CLASS "AA" CONCRETE $f'_c = 4,000$ PSI
 CLASS "A" CONCRETE $f'_c = 3,000$ PSI
 REINFORCING STEEL (GRADE 60) $f_y = 60,000$ PSI
 STRUCTURAL STEEL M270 (Gr. 50W) $f_y = 50,000$ PSI
 STAINLESS STEEL A240 (TYPE 316) $f_y = 30,000$ PSI

LOADING: HL-93 OR OKLAHOMA OVERLOAD TRUCK
 20 PSF FUTURE WEARING SURFACE
 5 PSF STAY-IN-PLACE FORMS

DESIGN: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 8TH EDITION.
 ANSI/AASHTO/AWS D1.5 BRIDGE WELDING CODE.
 STAINLESS STEEL WELDING CODE.

LRFR INVENTORY RATING FACTOR: 1.62
 LFD OPERATING RATING: HS 58

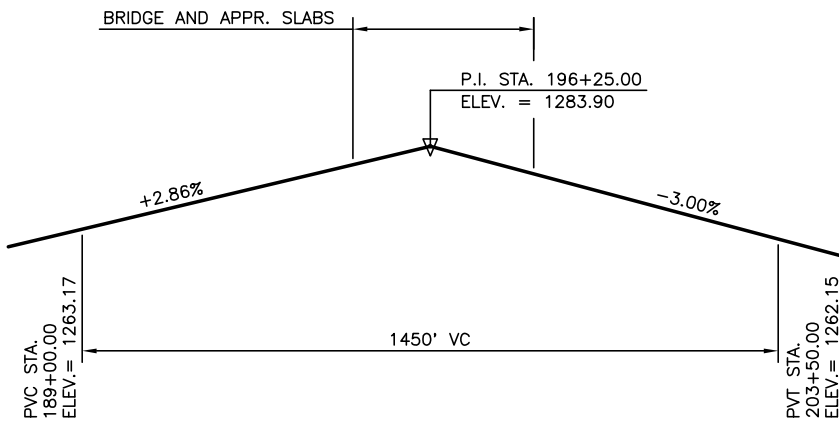
FOUNDATION DATA

| ABUTMENTS (HP12x53 PILING): | | |
|---|---------|---------|
| | ABUT. 1 | ABUT. 2 |
| (1) FACTORED PILE REACTION (TONS/PILE) = | 76.9 | 76.9 |
| PILE LENGTH (AVG.) (BR. SEAT, VERTICAL) (FT.) = | 39.0 | 38.5 |
| PILE LENGTH (AVG.) (BR. SEAT, BATTERED) (FT.) = | 40.0 | 39.5 |
| (2) PILE LENGTH (AVG.) (WINGS) (FT.) = | 43.5 | 43.0 |

| PIERS (72" DIAMETER DRILLED SHAFTS) | | | |
|--|--------|--------|--------|
| | PIER 1 | PIER 2 | PIER 3 |
| MINIMUM DEPTH INTO ROCK (FT.) = | 12.0 | 12.0 | 12.0 |
| DEPTH OF ROCK NEGLECTED FOR FRICTION (FT.) = | 6.0 | 6.0 | 6.0 |
| UNIT BEARING RESISTANCE (TSF) = | 24.8 | 24.8 | 24.8 |
| BEARING RESISTANCE FACTOR = | 0.7 | 0.7 | 0.7 |
| FACTORED BEARING RESISTANCE (TONS/SHAFT) = | 490.8 | 490.8 | 490.8 |
| UNIT FRICTION RESISTANCE (TSF) = | 7.6 | 7.6 | 7.6 |
| FRICTION RESISTANCE FACTOR = | 0.45 | 0.45 | 0.45 |
| FACTORED FRICTION RESISTANCE (TONS/SHAFT) = | 386.8 | 386.8 | 386.8 |
| TOTAL FACTORED RESISTANCE (TONS/SHAFT) = | 877.6 | 877.6 | 877.6 |
| TOTAL FACTORED REACTION (TONS/SHAFT) = | 624.2 | 717.6 | 615.7 |

(1) ABUTMENT PILING SHALL BE DRIVEN THROUGH THE COMPACTED FILL. PILING SHALL BE DRIVEN TO POINT BEARING ON SOLID FOUNDATION MATERIAL AT THE APPROXIMATE ELEVATION SHOWN ON THE PLANS. IF THE FACTORED PILE REACTION IS NOT OBTAINED AT THIS ELEVATION, DRIVING SHALL CONTINUE UNTIL THE FACTORED PILE REACTION IS OBTAINED. THE LENGTH OF STEEL PILING SHOWN ON THE PLANS IS FOR ESTIMATING PURPOSES ONLY.

(2) HP10x42 PILES, WINGS



FINISH GRADE DATA
US 81

NOTE: STATIONING FOLLOWS \odot SURVEY US 81. FINISH GRADE IS ALONG FINISH GRADE LINE (FGL).

SUMMARY OF QUANTITIES (BRIDGE "F")

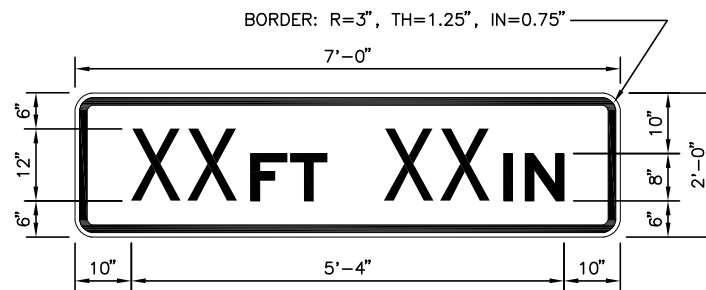
| DESCRIPTION | UNITS | ABUTS. | PIERS | SUPSTR. | APPR. SLABS | SLOPE WALLS | TOTAL |
|--|-------|----------|----------|-----------|-------------|-------------|-----------|
| SUBSTRUCTURE EXCAVATION COMMON | CY | 310.00 | | | | | 310.00 |
| CLSM BACKFILL | CY | 406.70 | | | | | 406.70 |
| PRESTRESSED CONCRETE BEAMS (TYPE IV) | LF | | | 2742.00 | | | 2742.00 |
| APPROACH SLAB | SY | | | | 532.50 | | 532.50 |
| SAW-CUT GROOVING | SY | | | 2737.80 | 500.20 | | 3238.00 |
| SEALED EXPANSION JOINT | LF | | | 83.26 | | | 83.26 |
| 42" F-SHAPED PARAPET | LF | | | 615.60 | 112.60 | | 728.20 |
| STRUCTURAL STEEL | LB | | | 4060.00 | | | 4060.00 |
| STAINLESS STEEL FIXED BEARING ASSEMBLY | EA | | | 18.00 | | | 18.00 |
| STAINLESS STEEL EXPANSION BEARING ASSEMBLY | EA | | | 54.00 | | | 54.00 |
| CLASS AA CONCRETE | CY | | | 792.50 | 9.00 | | 801.50 |
| CLASS A CONCRETE | CY | 166.20 | 393.00 | | | | 559.20 |
| CLASS C CONCRETE | CY | | | | | | 13.20 |
| SLOPE WALL (5") | SY | | | | | 1913.00 | 1913.00 |
| REINFORCING STEEL | LB | | 4870.00 | | | | 4870.00 |
| EPOXY COATED REINFORCING STEEL | LB | 20390.00 | 54420.00 | 212630.00 | 1010.00 | | 288450.00 |
| PILES, FURNISHED (HP10x42) | LF | 173.00 | | | | | 173.00 |
| PILES, FURNISHED (HP12x53) | LF | 1336.00 | | | | | 1336.00 |
| PILES, DRIVEN (HP10x42) | LF | 173.00 | | | | | 173.00 |
| PILES, DRIVEN (HP12x53) | LF | 1336.00 | | | | | 1336.00 |
| PILE SPLICE, H-PILE (NON-BIDDABLE) | EA | 1.00 | | | | | 1.00 |
| WATER REPELLENT (VISUALLY INSPECTED) | SY | 208.00 | 724.00 | 1544.00 | 103.00 | | 2579.00 |
| DRILLED SHAFTS 72" DIAMETER | LF | | 392.00 | | | | 392.00 |
| CROSSHOLE SONIC LOGGING | EA | | 3.00 | | | | 3.00 |
| SEALER CRACK PREPARATION | LF | | | 161.00 | | | 161.00 |
| SEALER RESIN | GAL | | | 1.30 | | | 1.30 |
| 6" PERFORATED PIPE UNDERDRAIN ROUND | LF | 172.00 | | | | 227.00 | 399.00 |
| 6" NON-PERF. PIPE UNDERDRAIN RND. | LF | 70.00 | | | | 390.00 | 460.00 |

INDEX OF SHEETS (BRIDGE "F")

| SHEET NO. | TITLE |
|-----------|--|
| AB01 | PAY QUANTITIES AND GENERAL NOTES |
| B001-B003 | GENERAL PLAN AND ELEVATION |
| B004-B006 | FOUNDATION REPORT |
| B007 | SUBSTRUCTURE STAKING DIAGRAM |
| B008-B009 | ABUTMENT 1 DETAILS |
| B010 | ABUTMENT 1 WINGWALL DETAILS |
| B011-B012 | ABUTMENT 2 DETAILS |
| B013 | ABUTMENT 2 WINGWALL DETAILS |
| B014 | SUBSTRUCTURE EXCAVATION AND PIPE UNDERDRAIN ASSEMBLY DETAILS |
| B015 | PIER 1 DETAILS |
| B016 | PIER 2 DETAILS |
| B017 | PIER 3 DETAILS |
| B018-B019 | MISCELLANEOUS PIER DETAILS |
| B020 | TYPICAL SECTION |
| B021-B022 | LONGITUDINAL SECTION |
| B023 | BEARING DETAILS |
| B024 | P.C. BEAM AND DIAPHRAGM LAYOUT PLAN |
| B025 | DIAPHRAGM DETAILS |
| B026 | BOTTOM SLAB REINFORCING PLAN |
| B027 | TOP SLAB REINFORCING PLAN AND PARAPET LAYOUT |
| B028 | CONCRETE LONGITUDINAL BARRIER DETAILS |
| B029 | MISCELLANEOUS SUPERSTRUCTURE DETAILS |
| B030 | EXPANSION JOINT DETAILS |
| B031 | 55' TYPE IV P.C. BEAM DETAILS |
| B032 | 99' TYPE IV P.C. BEAM DETAILS |
| B033-B035 | APPROACH SLAB DETAILS |
| B036-B037 | SLOPE WALL DETAILS |
| B038 | DRAIN AT END BRIDGE DETAILS |

THE FOLLOWING STANDARDS SHALL BE REQUIRED:

FSHP-42-2-00E LECS-4-2
 EJ-SK-04E PUD-3-3
 EJ-DTL-02E
 HP1-2-01E



| | |
|-----------------|-----------------------------------|
| SIGN NUMBER | W12-2p |
| WIDTH x HEIGHT | 7'-0" x 2'-0" |
| BORDER WIDTH | 1.25" |
| CORNER RADIUS | 3" |
| MOUNTING | BRIDGE PARAPET |
| BACKGROUND | TYPE: REFLECTIVE COLOR: YELLOW |
| LEGEND & BORDER | TYPE: REFLECTIVE COLOR: BLACK |

VERTICAL CLEARANCE SIGN DETAILS

NOTES: INSTALL TWO (2) PERMANENT SIGNS ON THE WEST PARAPET IN SPAN 2 (ONE OVER THE MIDPOINT OF I-44 (EBL) AND ONE OVER THE ADJACENT CD ROAD) AND TWO (2) PERMANENT SIGNS ON THE EAST PARAPET IN SPAN 3 (ONE OVER THE MIDPOINT OF I-44 (WBL) AND ONE OVER THE ADJACENT CD ROAD).

SIGNS SHOULD INDICATE A VERTICAL CLEARANCE OF 3" MIN. LESS THAN THE FINAL MEASURED CLEARANCE. THE CONTRACTOR SHALL CONTACT ODOT DIV. 7 FOR FINAL MEASUREMENT OF VERTICAL CLEARANCES.

HARDWARE AND CONNECTION DETAILS TO THE PARAPETS SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO INSTALLATION.

ALL COSTS FOR FABRICATION, LABOR, MATERIALS, HARDWARE AND INSTALLATION OF THE VERTICAL CLEARANCE SIGNS, COMPLETE-IN-PLACE, SHALL BE INCLUDED IN THE PRICE BID FOR "SHEET ALUMINUM SIGNS", PER SQ. FT. (TRAFFIC ITEM).

THE INSTALLATION OF THE PERMANENT VERTICAL CLEARANCE SIGNS DOES NOT RELIEVE THE CONTRACTOR OF MAINTAINING APPROPRIATE VERTICAL CLEARANCE SIGNS DURING CONSTRUCTION.

| | | | | |
|----------|-----|-------|--|----------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 01/19 | SB US-81 OVER I-44 | |
| Checked | . | | GENERAL PLAN AND ELEVATION (3 OF 3) | |
| Approved | . | | CONST. 55'-99'-99'-55' TYPE IV P.C. BEAM W/ 38'-0" AND 42'-0" CLR. RDWYS. AND CONC. PARAPETS (FSHP-42) SKEWED 2'39"35.10" RF | |
| Squad | | | \odot STA. 196+50.65 | |
| | | | Job Piece No. 24428(04) | Sheet No. B003 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B004-2442804-FOUNDATION REPORT 1.dwg 3/2/2020 11:49:33 AM

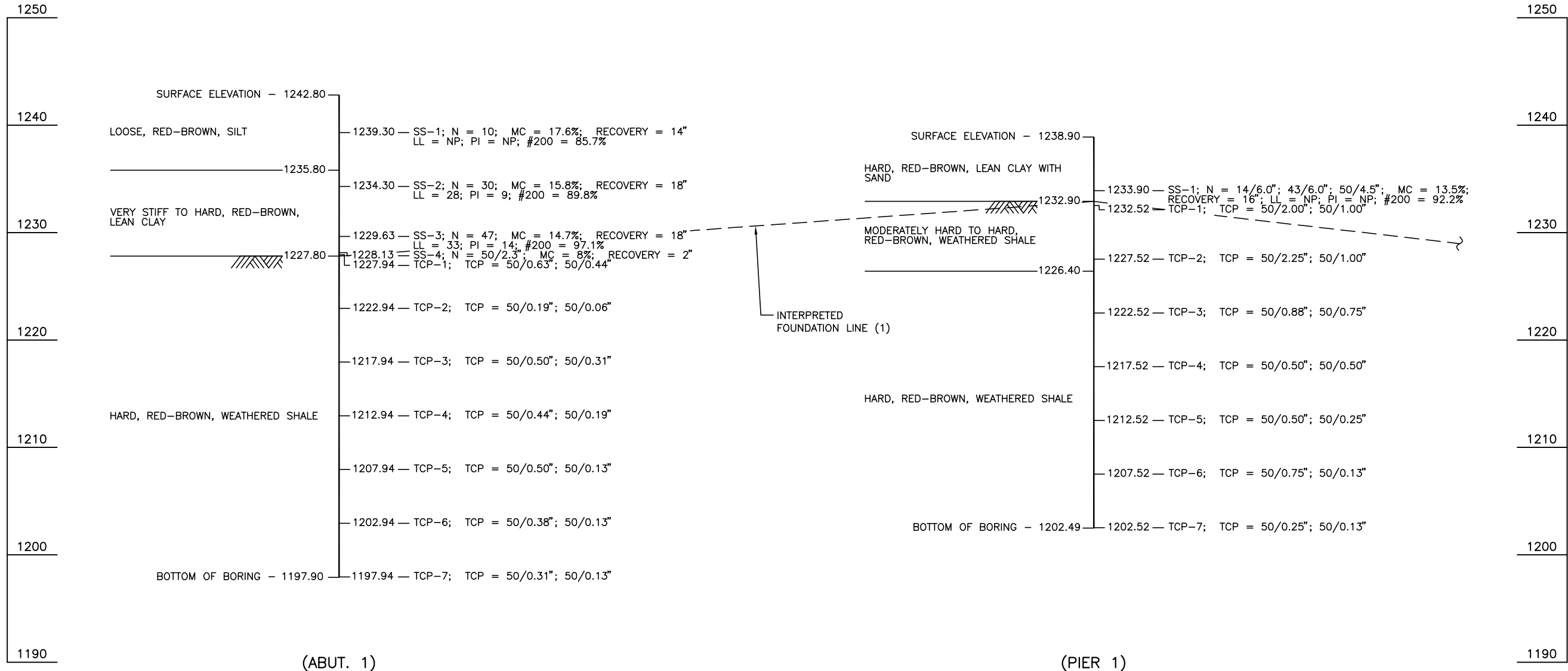
| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

BORING NO. F-1
STA. 194+93.10
69.1' LT.

(MAY 10, 2019)

BORING NO. F-2
STA. 195+50.10
67.1' LT.

(JULY 31, 2019)



NOTES: ALL GEOTECHNICAL INFORMATION CONTAINED ON THIS SHEET IS COVERED BY THE ENGINEERING SEAL AFFIXED TO AN ORIGINAL GEOTECHNICAL ENGINEERING REPORT THAT HAS BEEN STAMPED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN OKLAHOMA. TO OBTAIN A COPY OF THE COMPLETE REPORT, CONTACT THE ODOT OFFICE ENGINEER AT (405)521-2625. THE CONTRACTOR SHOULD BE FULLY AWARE OF THE SITE CONDITIONS PRIOR TO BEGINNING WORK. ANY ADDITIONAL GEOTECHNICAL INFORMATION WHICH MAY BE DESIRED IS THE RESPONSIBILITY OF THE CONTRACTOR.

BORING LOCATIONS (STATIONS) ARE NOT TO SCALE HORIZONTALLY.
ALL STATIONS AND OFFSETS ARE REFERENCED TO THE \odot SURVEY US 81.

GEOLOGY:

THE SUBJECT BRIDGE IS SITUATED IN A GEOLOGIC AREA BEST DESCRIBED AS BEING PART OF THE DOG CREEK-BLAINE SUBUNIT (PDB) UNDIFFERENTIATED. ACCORDING TO PUBLISHED MATERIALS ("ENGINEERING CLASSIFICATION OF GEOLOGIC MATERIALS: DIVISION SEVEN", 1969, OKLAHOMA HIGHWAY DEPARTMENT), THE DOG CREEK-BLAINE SUBUNIT CONSISTS OF DARK RED SHALES INTERBEDDED WITH MINOR AMOUNTS OF FINE-GRAINED GYPSIFEROUS SANDSTONES THAT LOCALLY GRADE INTO PURE GYPSUM. MUDSTONE CONGLOMERATES A FEW FEET IN THICKNESS OCCUR SPARINGLY WITHIN THE STRATA.

NOTE: GROUNDWATER WAS NOT ENCOUNTERED IN ANY OF THE BORINGS DURING OR AFTER DRILLING OPERATIONS. GROUNDWATER LEVELS MAY FLUCTUATE THROUGHOUT THE YEAR.

(1) FOR ESTIMATION PURPOSES ONLY.

TOP OF ROCK

| | | | | |
|----------|-----|-------|---|----------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 09/19 | SB US-81 OVER I-44 | |
| Checked | . | | FOUNDATION REPORT (1 OF 3) Ⓞ STA. 196+50.65 | |
| Approved | . | | | |
| Squad | | | | |
| | | | Job Piece No. 24428(04) | Sheet No. B004 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B005-2442804-FOUNDATION REPORT 2.dwg 3/2/2020 11:50:02 AM

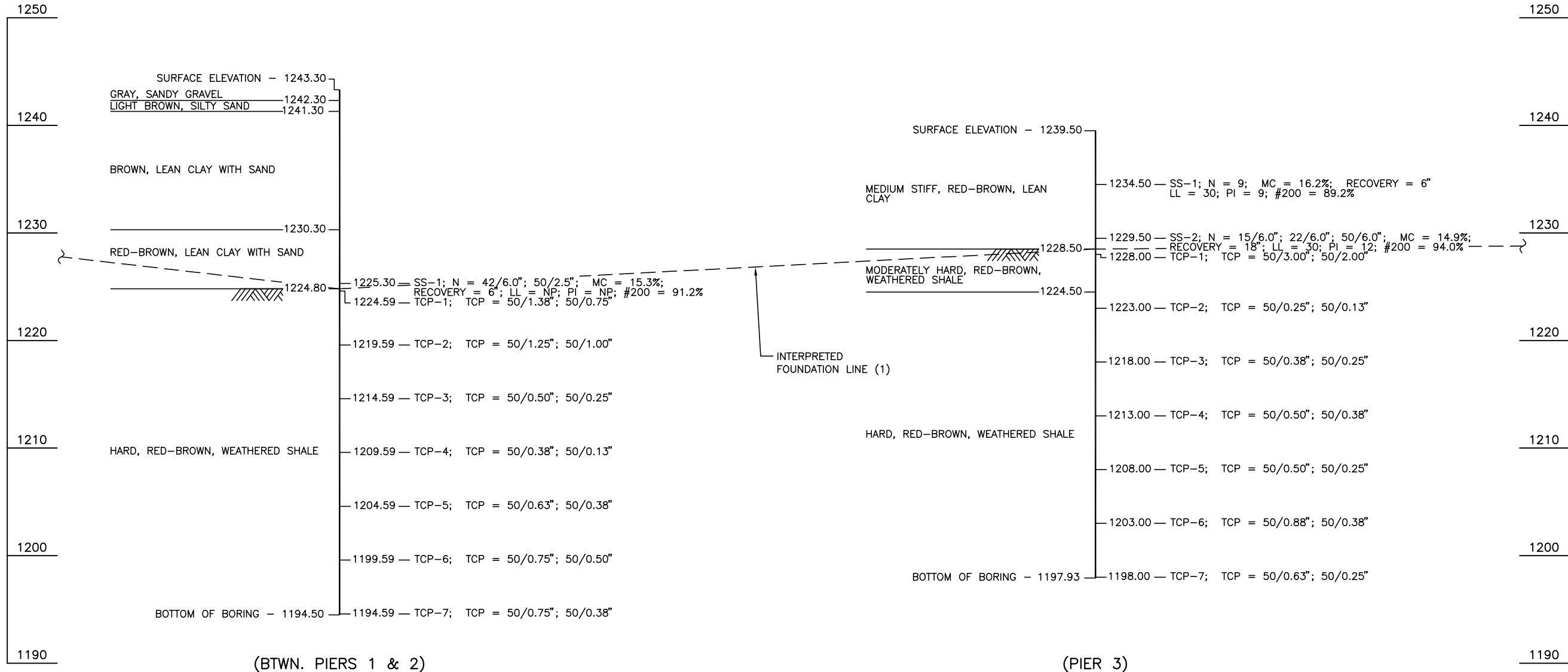
| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

BORING NO. F-3
STA. 195+87.50
47.5' LT.

(AUGUST 1, 2019)

BORING NO. F-4
STA. 197+51.80
71.6' LT.

(MAY 8, 2019)



(BTWN. PIERS 1 & 2)

(PIER 3)

NOTES: ALL GEOTECHNICAL INFORMATION CONTAINED ON THIS SHEET IS COVERED BY THE ENGINEERING SEAL AFFIXED TO AN ORIGINAL GEOTECHNICAL ENGINEERING REPORT THAT HAS BEEN STAMPED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN OKLAHOMA. TO OBTAIN A COPY OF THE COMPLETE REPORT, CONTACT THE ODOT OFFICE ENGINEER AT (405)521-2625. THE CONTRACTOR SHOULD BE FULLY AWARE OF THE SITE CONDITIONS PRIOR TO BEGINNING WORK. ANY ADDITIONAL GEOTECHNICAL INFORMATION WHICH MAY BE DESIRED IS THE RESPONSIBILITY OF THE CONTRACTOR.

BORING LOCATIONS (STATIONS) ARE NOT TO SCALE HORIZONTALLY.

ALL STATIONS AND OFFSETS ARE REFERENCED TO THE \odot SURVEY US 81.

GEOLOGY:

THE SUBJECT BRIDGE IS SITUATED IN A GEOLOGIC AREA BEST DESCRIBED AS BEING PART OF THE DOG CREEK-BLAINE SUBUNIT (PDB) UNDIFFERENTIATED. ACCORDING TO PUBLISHED MATERIALS ("ENGINEERING CLASSIFICATION OF GEOLOGIC MATERIALS: DIVISION SEVEN", 1969, OKLAHOMA HIGHWAY DEPARTMENT), THE DOG CREEK-BLAINE SUBUNIT CONSISTS OF DARK RED SHALES INTERBEDDED WITH MINOR AMOUNTS OF FINE-GRAINED GYPSIFEROUS SANDSTONES THAT LOCALLY GRADE INTO PURE GYPSUM. MUDSTONE CONGLOMERATES A FEW FEET IN THICKNESS OCCUR SPARINGLY WITHIN THE STRATA.

NOTE: GROUNDWATER WAS NOT ENCOUNTERED IN ANY OF THE BORINGS DURING OR AFTER DRILLING OPERATIONS. GROUNDWATER LEVELS MAY FLUCTUATE THROUGHOUT THE YEAR.

(1) FOR ESTIMATION PURPOSES ONLY.

TOP OF ROCK

| | | | | |
|----------|-----|-------|---|-------------------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 09/19 | SB US-81 OVER I-44 | |
| Checked | . | | FOUNDATION REPORT (2 OF 3) Ⓞ STA. 196+50.65 | |
| Approved | . | | | |
| Squad | | | | Job Piece No. 24428(04) |

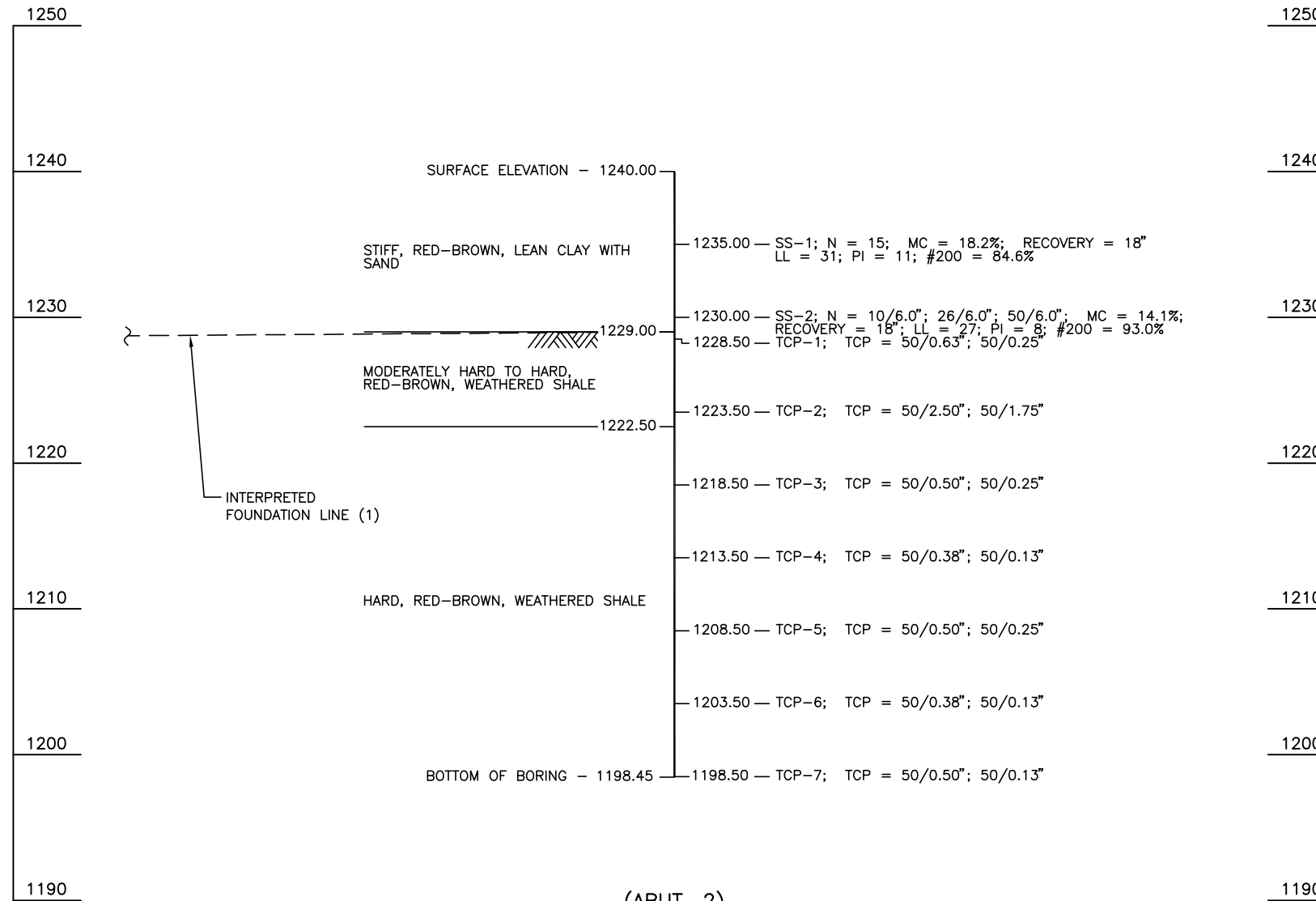
"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B006-2442804-FOUNDATION REPORT 3.dwg 3/2/2020 11:50:26 AM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

BORING NO. F-5
 STA. 198+02.00
 68.7' LT.

(MAY 8, 2019)



(ABUT. 2)

GEOLOGY:

THE SUBJECT BRIDGE IS SITUATED IN A GEOLOGIC AREA BEST DESCRIBED AS BEING PART OF THE DOG CREEK-BLAINE SUBUNIT (PDB) UNDIFFERENTIATED. ACCORDING TO PUBLISHED MATERIALS ("ENGINEERING CLASSIFICATION OF GEOLOGIC MATERIALS: DIVISION SEVEN", 1969, OKLAHOMA HIGHWAY DEPARTMENT), THE DOG CREEK-BLAINE SUBUNIT CONSISTS OF DARK RED SHALES INTERBEDDED WITH MINOR AMOUNTS OF FINE-GRAINED GYPSIFEROUS SANDSTONES THAT LOCALLY GRADE INTO PURE GYPSUM. MUDSTONE CONGLOMERATES A FEW FEET IN THICKNESS OCCUR SPARINGLY WITHIN THE STRATA.

NOTE: GROUNDWATER WAS NOT ENCOUNTERED IN ANY OF THE BORINGS DURING OR AFTER DRILLING OPERATIONS. GROUNDWATER LEVELS MAY FLUCTUATE THROUGHOUT THE YEAR.

(1) FOR ESTIMATION PURPOSES ONLY.

NOTES: ALL GEOTECHNICAL INFORMATION CONTAINED ON THIS SHEET IS COVERED BY THE ENGINEERING SEAL AFFIXED TO AN ORIGINAL GEOTECHNICAL ENGINEERING REPORT THAT HAS BEEN STAMPED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN OKLAHOMA. TO OBTAIN A COPY OF THE COMPLETE REPORT, CONTACT THE ODOT OFFICE ENGINEER AT (405)521-2625. THE CONTRACTOR SHOULD BE FULLY AWARE OF THE SITE CONDITIONS PRIOR TO BEGINNING WORK. ANY ADDITIONAL GEOTECHNICAL INFORMATION WHICH MAY BE DESIRED IS THE RESPONSIBILITY OF THE CONTRACTOR.

BORING LOCATIONS (STATIONS) ARE NOT TO SCALE HORIZONTALLY.

ALL STATIONS AND OFFSETS ARE REFERENCED TO THE \odot SURVEY US 81.

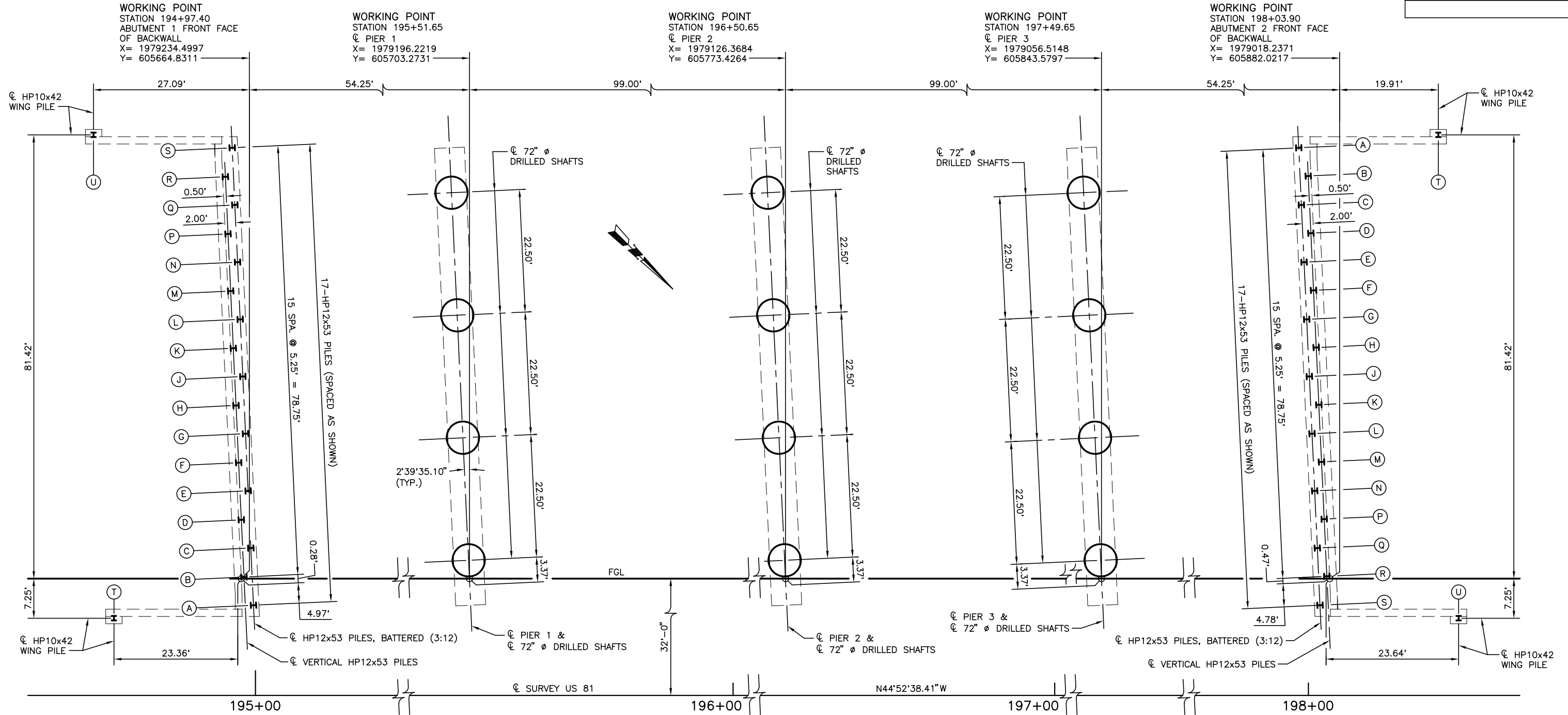
TOP OF ROCK

| | | | | |
|----------|-----|-------|---|-------------------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 09/19 | SB US-81 OVER I-44 | |
| Checked | . | | FOUNDATION REPORT (3 OF 3) \odot STA. 196+50.65 | |
| Approved | . | | | |
| Squad | | | | Job Piece No. 24428(04) |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

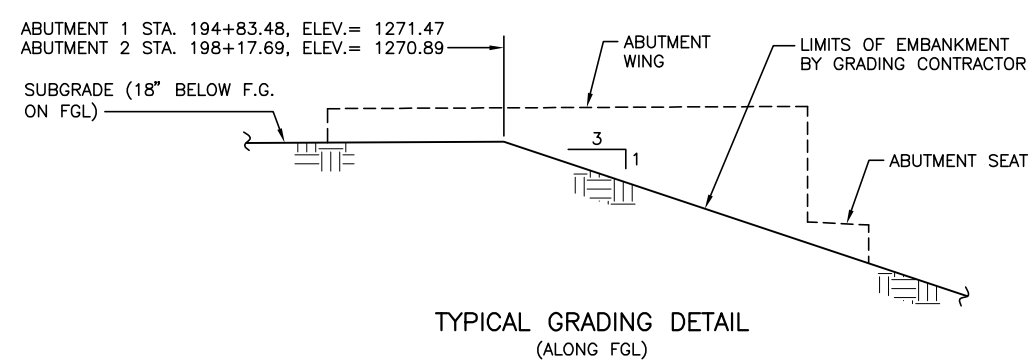
M:\W2M Projects\Project Files\M179\Final Design F & G\B007-2442804-SUBSTRUCTURE STAKING DIAGRAM.dwg 3/2/2020 11:51:12 AM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



| PILE | ABUTMENT 1 | ABUTMENT 2 |
|------|------------|------------|
| A | 1265.43 | 1263.25 |
| B | 1265.33 | 1263.35 |
| C | 1265.22 | 1263.46 |
| D | 1265.11 | 1263.56 |
| E | 1265.01 | 1263.66 |
| F | 1264.90 | 1263.77 |
| G | 1264.80 | 1263.87 |
| H | 1264.69 | 1263.97 |
| J | 1264.58 | 1264.08 |
| K | 1264.48 | 1264.18 |
| L | 1264.37 | 1264.28 |
| M | 1264.27 | 1264.39 |
| N | 1264.16 | 1264.49 |
| P | 1264.05 | 1264.59 |
| Q | 1263.95 | 1264.69 |
| R | 1263.84 | 1264.80 |
| S | 1263.74 | 1264.90 |
| T | 1270.03 | 1267.73 |
| U | 1268.30 | 1269.40 |

SUBSTRUCTURE STAKING DIAGRAM
(NOT TO SCALE)
(PILE ENCASEMENTS NOT SHOWN FOR CLARITY.)



NOTES: BRIDGE SEAT PILES SHALL BE ORIENTED SUCH THAT THE FACE OF THE PILE WEB IS PERPENDICULAR TO THE FACE OF THE BRIDGE SEAT.

ALL STATIONING FOLLOWS CL SURVEY US 81.

ALL DIMENSIONS ARE HORIZONTAL DIMENSIONS.

FOR SUMMARY OF QUANTITIES, SEE SHEET NO. B003.

SEE STD. HP1-2 FOR STEEL PILE ENCASEMENT DETAILS.

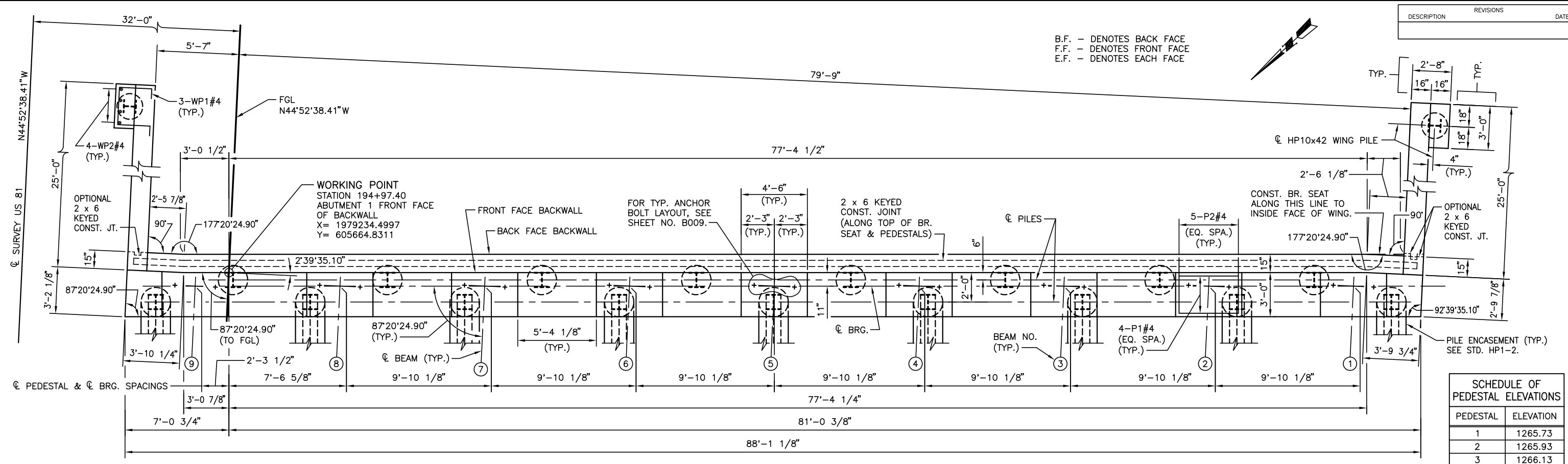
| | | | |
|----------|-----------|-------------------------------------|----------------|
| Design | . | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW 02/19 | SB US-81 OVER I-44 | |
| Checked | . | SUBSTRUCTURE STAKING DIAGRAM | |
| Approved | . | CL STA. 196+50.65 | |
| Squad | | Job Piece No. 24428(04) | Sheet No. B007 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

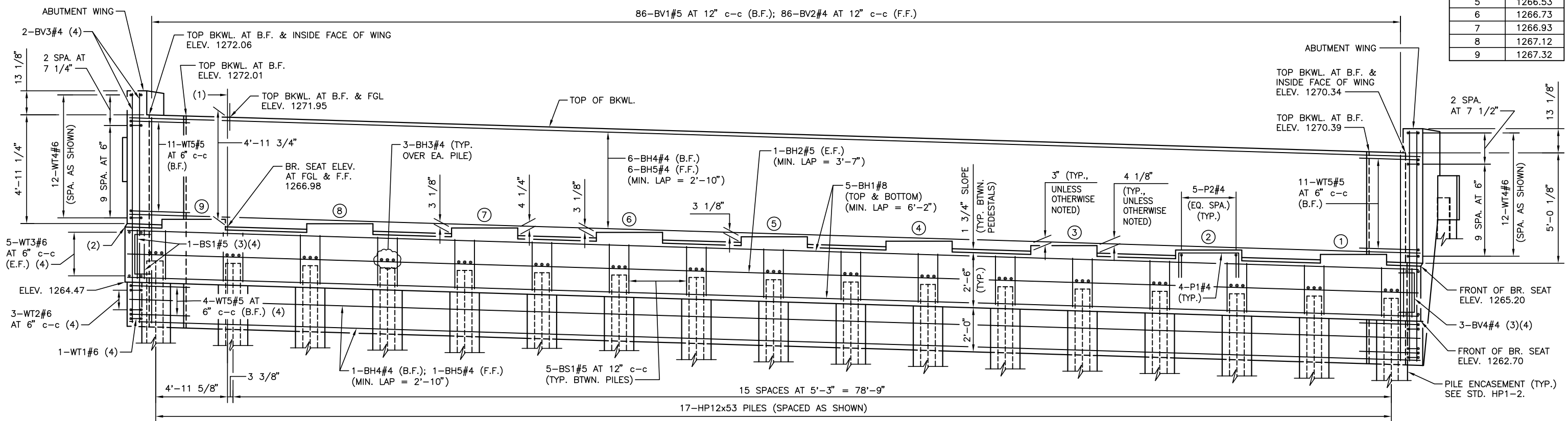
M:\W2M Projects\Project Files\M179\Final Design F & C\B008-2442804-ABUTMENT 1 DETAILS 1.dwg 3/2/2020 1:43:15 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

B.F. - DENOTES BACK FACE
 F.F. - DENOTES FRONT FACE
 E.F. - DENOTES EACH FACE



| PEDESTAL | ELEVATION |
|----------|-----------|
| 1 | 1265.73 |
| 2 | 1265.93 |
| 3 | 1266.13 |
| 4 | 1266.33 |
| 5 | 1266.53 |
| 6 | 1266.73 |
| 7 | 1266.93 |
| 8 | 1267.12 |
| 9 | 1267.32 |



NOTES: ALL REINFORCING STEEL CONNECTING THE BRIDGE SEAT AND/OR BACKWALL TO THE WINGS SHALL BE TIED IN PLACE PRIOR TO POURING BRIDGE SEAT AND BACKWALL (WT BARS).
 BH1 BAR LAPS SHALL BE STAGGERED.

FOR ADDITIONAL ABUTMENT 1 DETAILS, BAR BENDS, BAR LIST AND QUANTITIES, SEE SHEET NO. B009.
 FOR WING DETAILS, SEE SHEET NO. B010.
 FOR SUBSTRUCTURE STAKING DIAGRAM AND TOP OF PILE ELEVATIONS, SEE SHEET NO. B007.

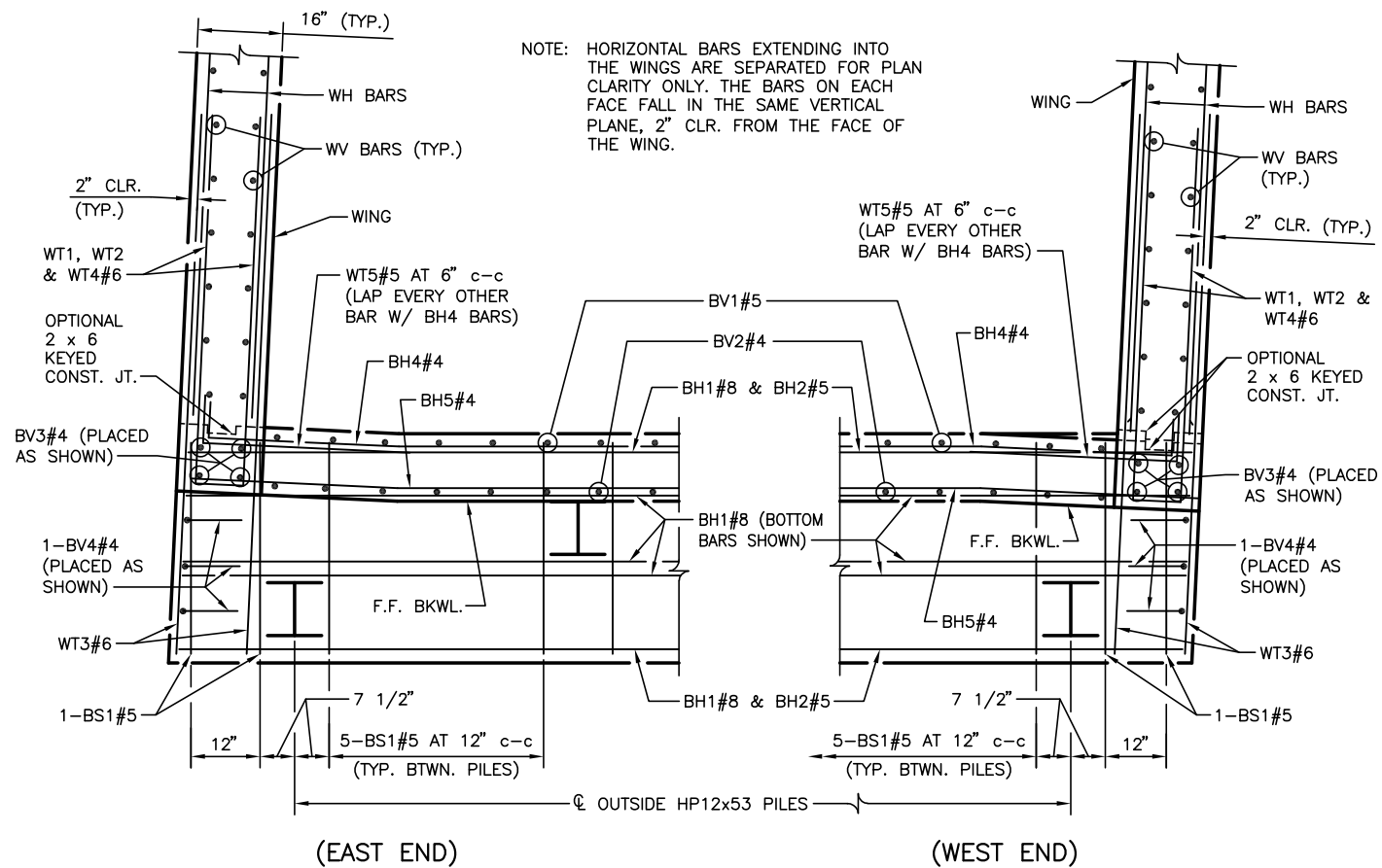
- (1) FGL AT F.F. BKWL.
- (2) FRONT OF BR. SEAT ELEV. 1266.97
- (3) PLACED AS SHOWN ON SHEET NO. B009.
- (4) TYP. EA. END

| | | |
|----------|-----|-------|
| Design | . | |
| Drawn | PKW | 02/19 |
| Checked | . | |
| Approved | . | |
| Squad | | |

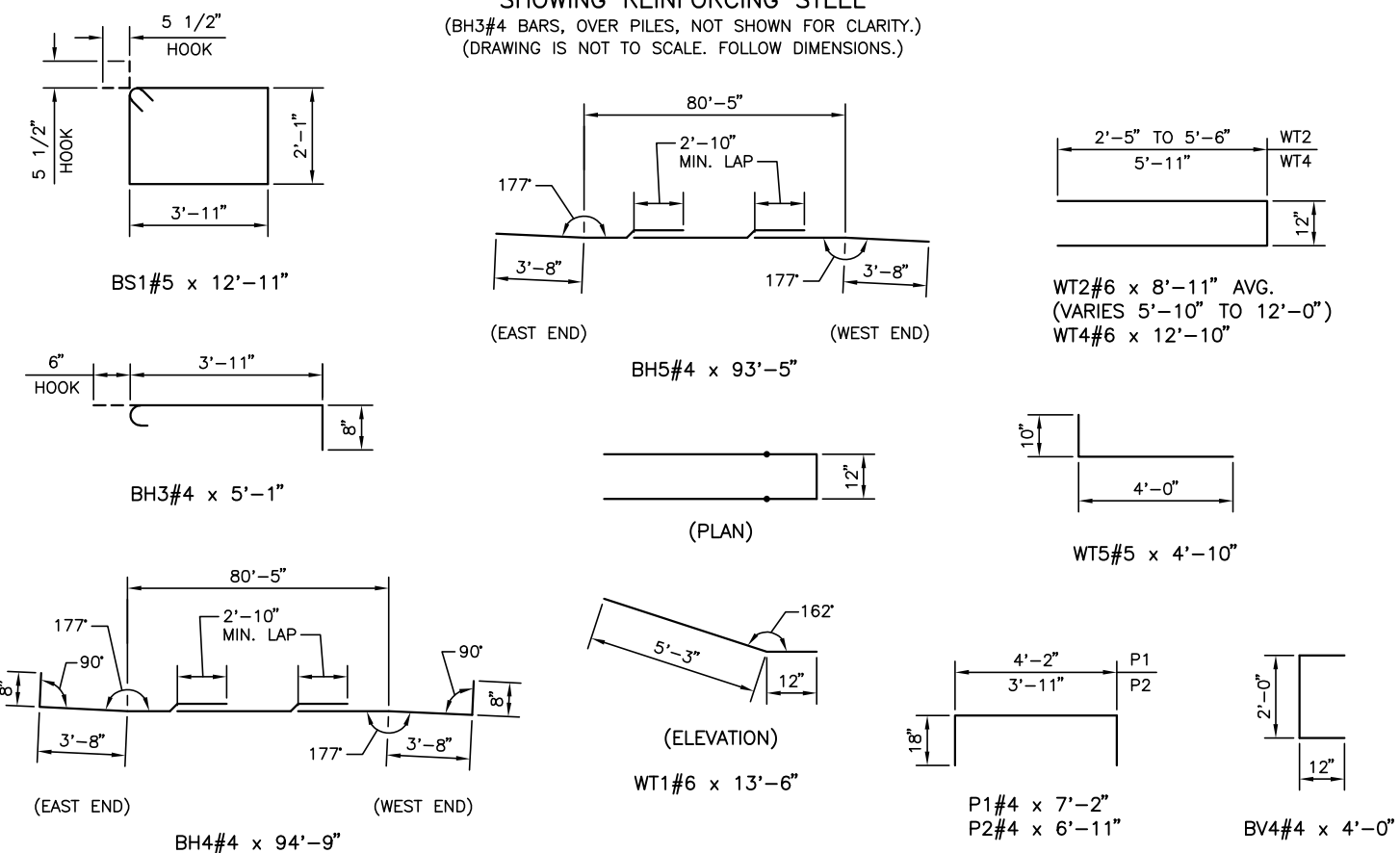
BRIDGE "F"
 SB US-81 OVER I-44
 GRADY COUNTY
ABUTMENT 1 DETAILS (1 OF 2)
 @ STA. 196+50.65
 Job Piece No. 24428(04)
 Sheet No. B008

(NOT TO SCALE)

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

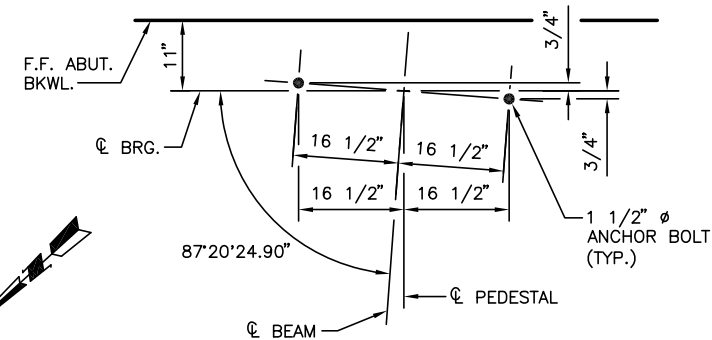


PART PLAN OF BRIDGE SEAT AND BACKWALL SHOWING REINFORCING STEEL (BH3#4 BARS, OVER PILES, NOT SHOWN FOR CLARITY.) (DRAWING IS NOT TO SCALE. FOLLOW DIMENSIONS.)



BAR BEND DETAILS

NOTE: FOR BAR BEND DETAILS NOT SHOWN, SEE SHEET NO. B010.



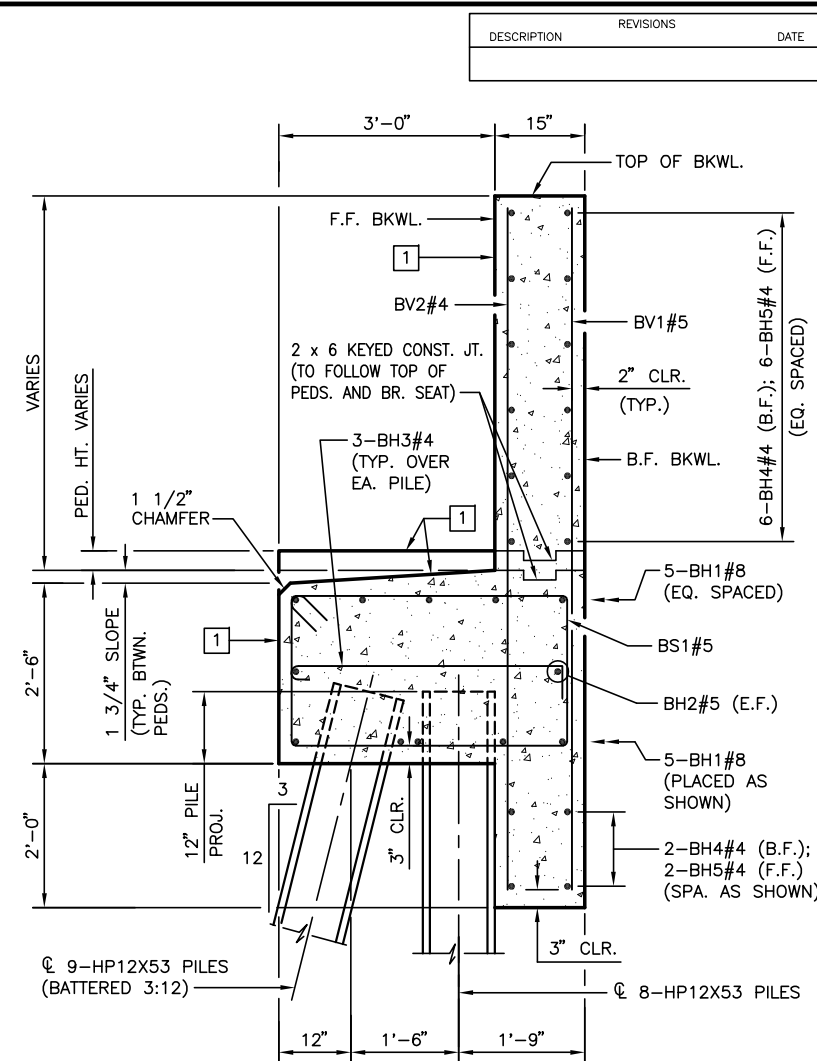
| ABUTMENT 1 BAR LIST | | | | |
|---------------------|-----|------|-------|-----------------|
| MARK | NO. | SIZE | SHAPE | LENGTH |
| (EPOXY COATED) | | | | |
| BH1 | 10 | 8 | STR. | 93'-11" |
| BH2 | 2 | 5 | STR. | 94'-11" |
| BH3 | 51 | 4 | BNT. | 5'-1" |
| BH4 | 8 | 4 | BNT. | 94'-9" |
| BH5 | 8 | 4 | BNT. | 93'-5" |
| BS1 | 84 | 5 | BNT. | 12'-11" |
| BV1 | 86 | 5 | STR. | 9'-2" |
| BV2 | 86 | 4 | STR. | 9'-2" |
| BV3 | 8 | 4 | STR. | 10'-3" |
| BV4 | 6 | 4 | BNT. | 4'-0" |
| P1 | 36 | 4 | BNT. | 7'-2" |
| P2 | 45 | 4 | BNT. | 6'-11" |
| WH1 | 4 | 6 | BNT. | 24'-1" |
| WH2 | 20 | 6 | STR. | 13'-0" AVG. |
| WH3 | 28 | 6 | STR. | 23'-5" |
| WH4 | 10 | 6 | STR. | 8'-9 1/2" AVG. |
| WH5 | 10 | 6 | STR. | 16'-10" AVG. |
| WP1 | 6 | 4 | BNT. | 8'-8" |
| WP2 | 8 | 4 | STR. | 1'-7" |
| WT1 | 2 | 6 | BNT. | 13'-6" |
| WT2 | 6 | 6 | BNT. | 8'-11" AVG. |
| WT3 | 20 | 6 | STR. | 9'-2" |
| WT4 | 24 | 6 | BNT. | 12'-10" |
| WT5 | 30 | 5 | BNT. | 4'-10" |
| WV1 | 16 | 4 | STR. | 3'-7" |
| WV2 | 80 | 4 | STR. | 6'-11 1/2" AVG. |

- (1) LENGTH INCLUDES ONE (1) 6'-2" LAP SPLICE.
- (2) LENGTH INCLUDES TWO (2) 3'-7" LAP SPLICES.
- (3) LENGTH INCLUDES TWO (2) 2'-10" LAP SPLICES.
- (4) 10 BARS VARYING FROM 5'-11" TO 20'-1"; TWO OF EACH LENGTH.
- (5) 5 BARS VARYING FROM 5'-8" TO 11'-11"; TWO OF EACH LENGTH.
- (6) 5 BARS VARYING FROM 13'-8" TO 20'-0"; TWO OF EACH LENGTH.
- (7) 3 BARS VARYING FROM 5'-10" TO 12'-0"; TWO OF EACH LENGTH.
- (8) 20 BARS VARYING FROM 3'-11" TO 10'-0"; FOUR OF EACH LENGTH.

NOTES: FOR WING DETAILS, SEE SHEET NO. B010.

STEEL PILE CONCRETE ENCASEMENTS NOT SHOWN FOR CLARITY. SEE STD. HP1-2 FOR DETAILS.

ALL EDGES OF THE ABUTMENT AND WINGS SHALL HAVE A 3/4" CHAMFER, UNLESS OTHERWISE SHOWN.



- 1 TREAT WITH A PENETRATING WATER REPELLENT SURFACE TREATMENT, INCLUDING SIDES OF PEDESTALS. THE FRONT FACE OF THE BRIDGE SEAT NEED ONLY BE TREATED ABOVE THE TOP OF SLOPE WALL.

B.F. - DENOTES BACK FACE
F.F. - DENOTES FRONT FACE
E.F. - DENOTES EACH FACE

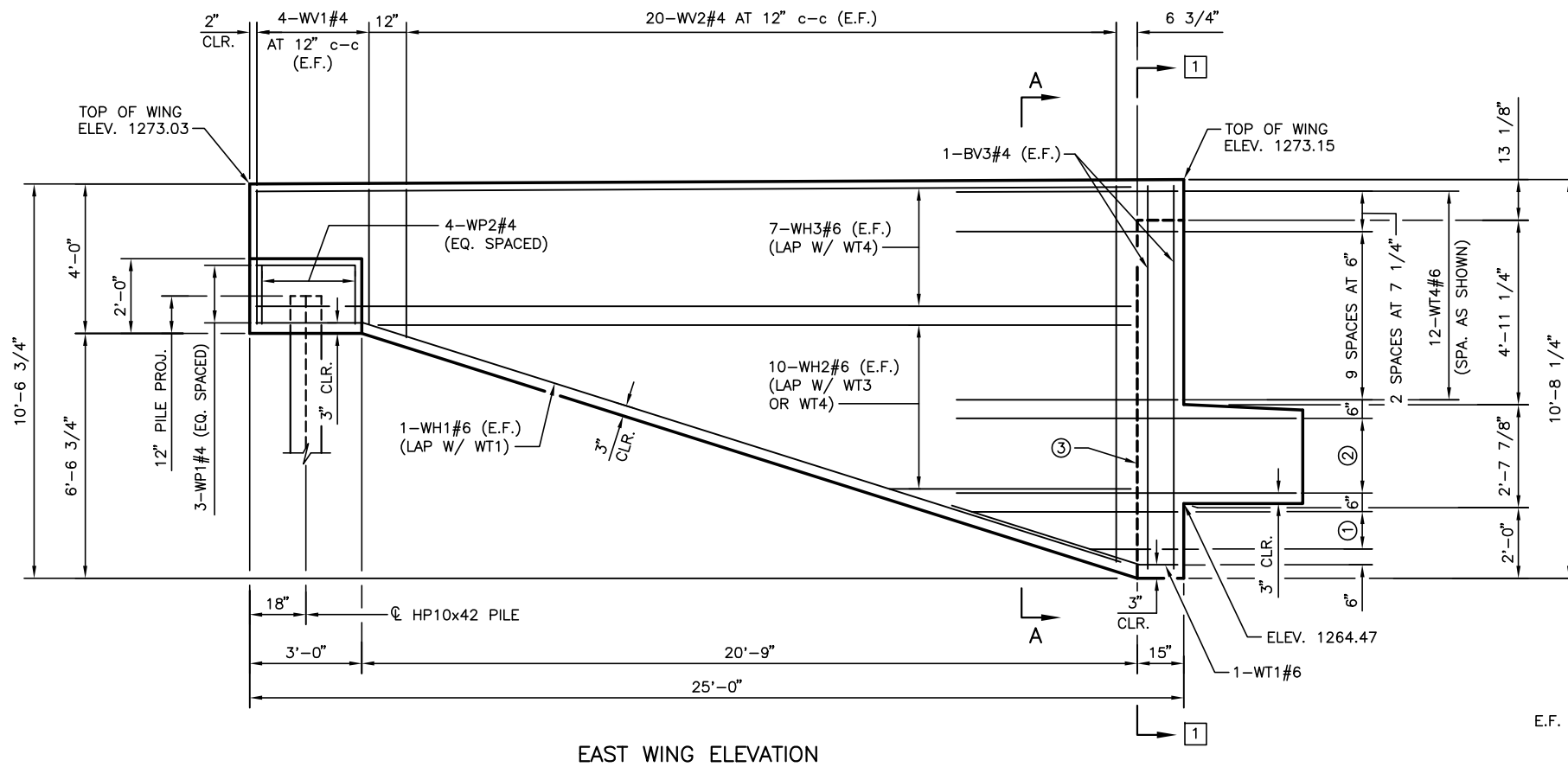
| ABUTMENT 1 QUANTITIES | | |
|--------------------------------------|-------|----------|
| ITEM | UNITS | TOTAL |
| SUBSTRUCTURE EXCAVATION COMMON | CY | 155.00 |
| CLSM BACKFILL | CY | 203.50 |
| CLASS A CONCRETE | CY | 83.10 |
| EPOXY COATED REINFORCING STEEL | LB | 10190.00 |
| PILES, FURNISHED (HP10x42) | LF | 87.00 |
| PILES, FURNISHED (HP12x53) | LF | 672.00 |
| PILES, DRIVEN (HP10x42) | LF | 87.00 |
| PILES, DRIVEN (HP12x53) | LF | 672.00 |
| PILE SPLICE, H-PILE (NON-BIDDABLE) | EA | 1.00 |
| WATER REPELLENT (VISUALLY INSPECTED) | SY | 104.00 |
| 6" PERFORATED PIPE UNDERDRAIN ROUND | LF | 86.00 |
| 6" NON-PERF. PIPE UNDERDRAIN RND. | LF | 35.00 |

| | | | |
|----------|-----------|------------------------------------|----------------|
| Design | . | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW 03/19 | SB US-81 OVER I-44 | |
| Checked | . | ABUTMENT 1 DETAILS (2 OF 2) | |
| Approved | . | | |
| Squad | . | Job Piece No. 24428(04) | Sheet No. B009 |

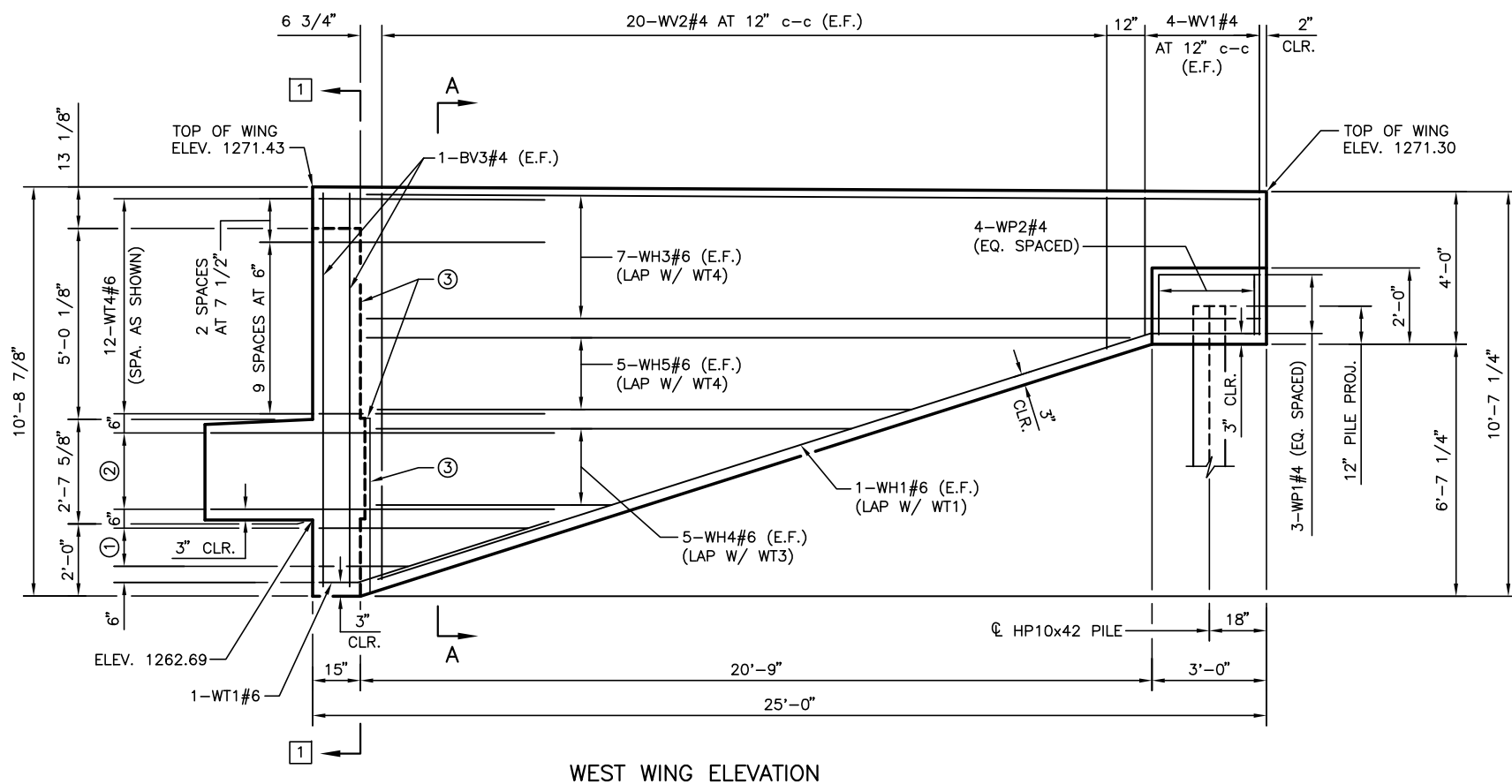
"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B010-2442804-ABUTMENT 1 WINGWALL DETAILS.dwg 3/2/2020 2:16:28 PM

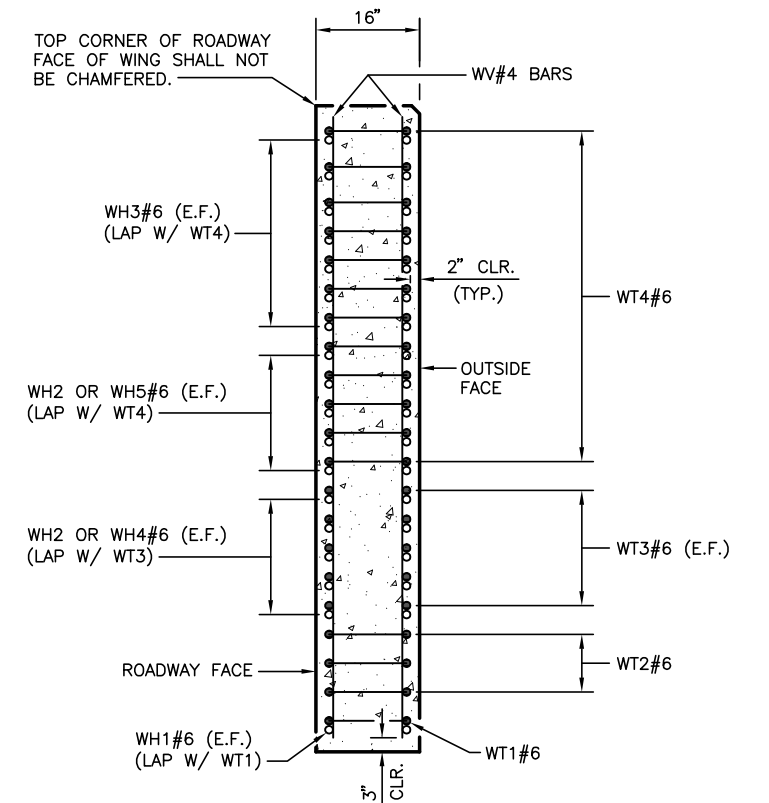
| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



EAST WING ELEVATION



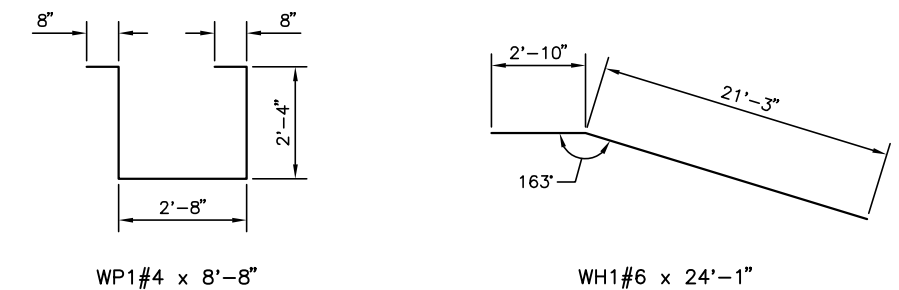
WEST WING ELEVATION



SECTION A-A

E.F. - DENOTES EACH FACE

- ① 3-WT2#6 AT 6" c-c
- ② 5-WT3#6 AT 6" c-c (E.F.)
- ③ OPTIONAL KEYED CONST. JOINT



BAR BEND DETAILS

NOTE: FOR ABUTMENT BAR LIST AND WT BAR BEND DETAILS, SEE SHEET NO. B009.

① TREAT ENDS OF BRIDGE SEAT AND ALL EXPOSED SURFACES OF WINGS WITH A WATER REPELLENT SURFACE TREATMENT, ABOVE THE FINISHED GROUND SURFACE AND IN FRONT (AHEAD) OF THE BACK FACE OF BACKWALL LINE.

| | | | | |
|----------|-----|-------|------------------------------------|----------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 03/19 | SB US-81 OVER I-44 | |
| Checked | . | | ABUTMENT 1 WINGWALL DETAILS | |
| Approved | . | | Ⓞ STA. 196+50.65 | |
| Squad | | | Job Piece No. 24428(04) | Sheet No. B010 |

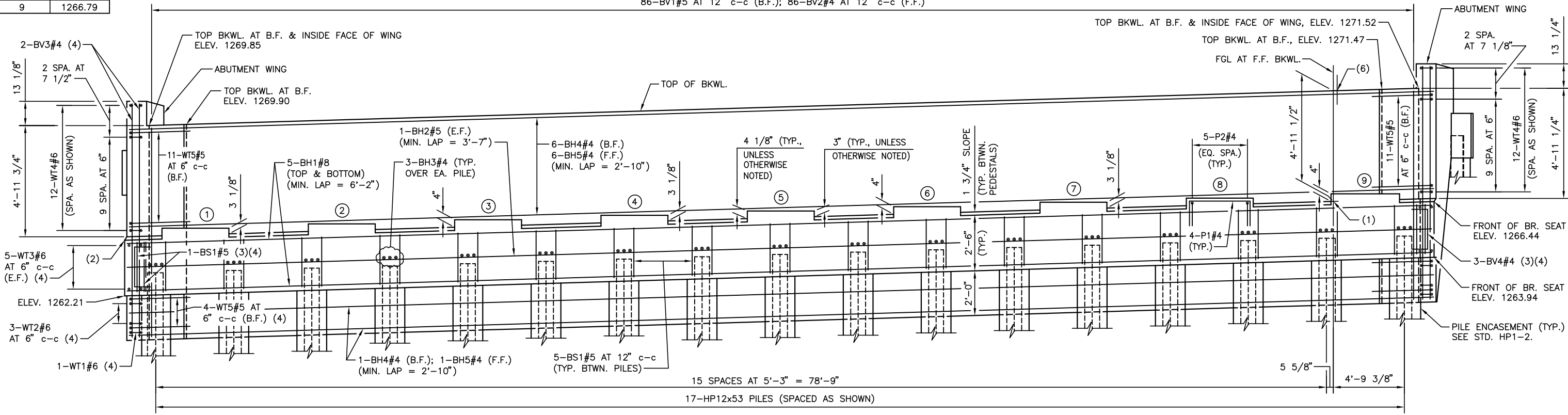
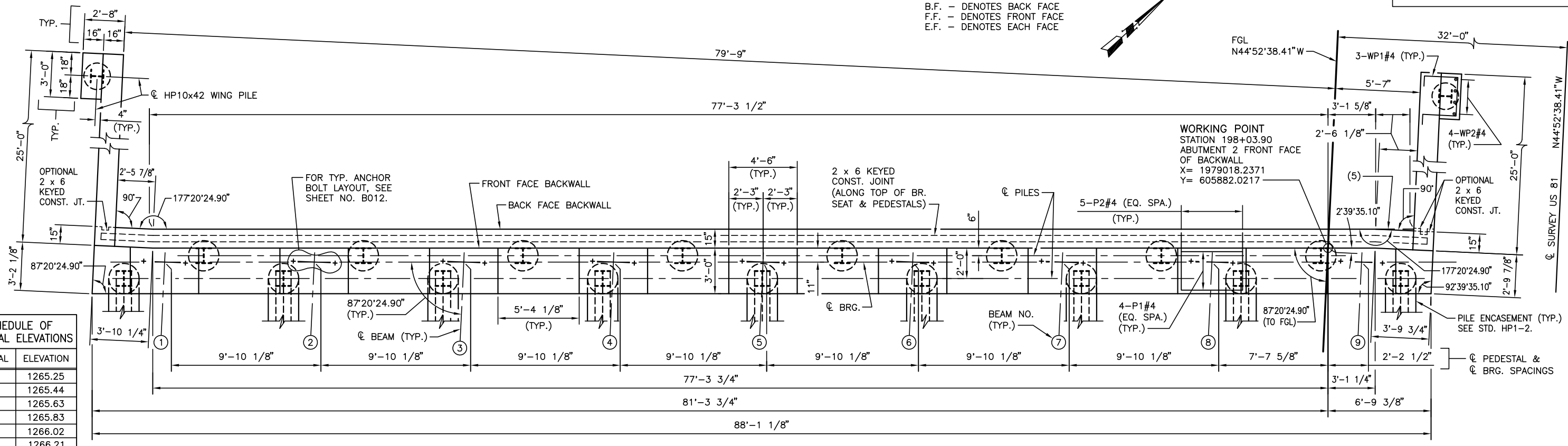
"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & C\B011-2442804-ABUTMENT 2 DETAILS 1.dwg 3/2/2020 2:18:31 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

B.F. - DENOTES BACK FACE
 F.F. - DENOTES FRONT FACE
 E.F. - DENOTES EACH FACE

| PEDESTAL | ELEVATION |
|----------|-----------|
| 1 | 1265.25 |
| 2 | 1265.44 |
| 3 | 1265.63 |
| 4 | 1265.83 |
| 5 | 1266.02 |
| 6 | 1266.21 |
| 7 | 1266.41 |
| 8 | 1266.60 |
| 9 | 1266.79 |



NOTES: ALL REINFORCING STEEL CONNECTING THE BRIDGE SEAT AND/OR BACKWALL TO THE WINGS SHALL BE TIED IN PLACE PRIOR TO POURING BRIDGE SEAT AND BACKWALL (WT BARS).
 BH1 BAR LAPS SHALL BE STAGGERED.

FOR ADDITIONAL ABUTMENT 2 DETAILS, BAR BENDS, BAR LIST AND QUANTITIES, SEE SHEET NO. B012.
 FOR WING DETAILS, SEE SHEET NO. B013.
 FOR SUBSTRUCTURE STAKING DIAGRAM AND TOP OF PILE ELEVATIONS, SEE SHEET NO. B007.

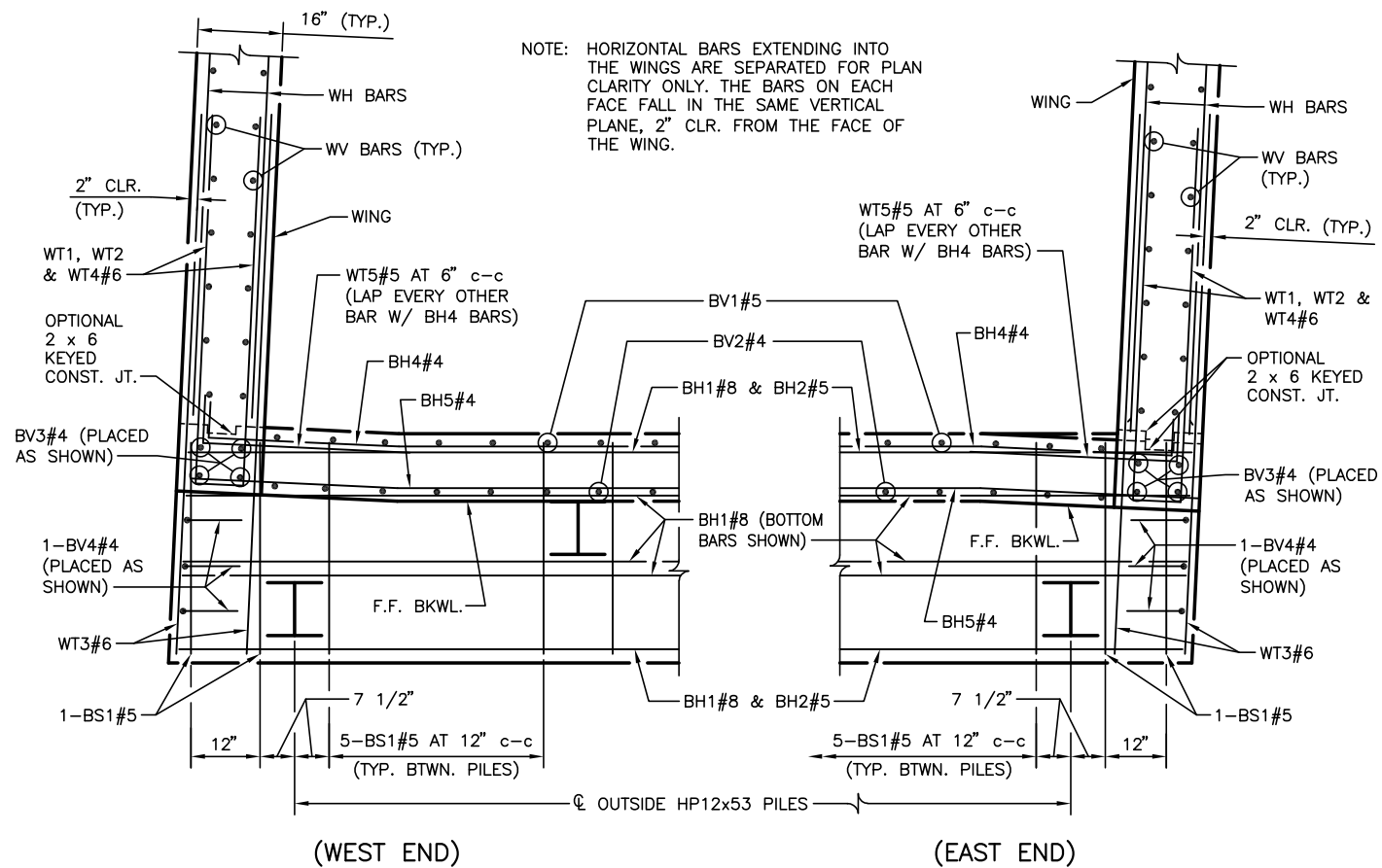
- (1) BR. SEAT ELEV. AT FGL & F.F. 1266.45
- (2) FRONT OF BR. SEAT ELEV. 1264.71
- (3) PLACED AS SHOWN ON SHEET NO. B012.
- (4) TYP. EA. END
- (5) CONST. BR. SEAT ALONG THIS LINE TO INSIDE FACE OF WING.
- (6) TOP BKWL. AT B.F. & FGL ELEV. 1271.41

(NOT TO SCALE)

| | | | |
|----------|-----------|-------------------------------|----------------|
| Design | . | BRIDGE "F" SB US-81 OVER I-44 | GRADY COUNTY |
| Drawn | PKW 03/19 | | |
| Checked | . | | |
| Approved | . | | |
| Squad | . | Job Piece No. 24428(04) | Sheet No. B011 |

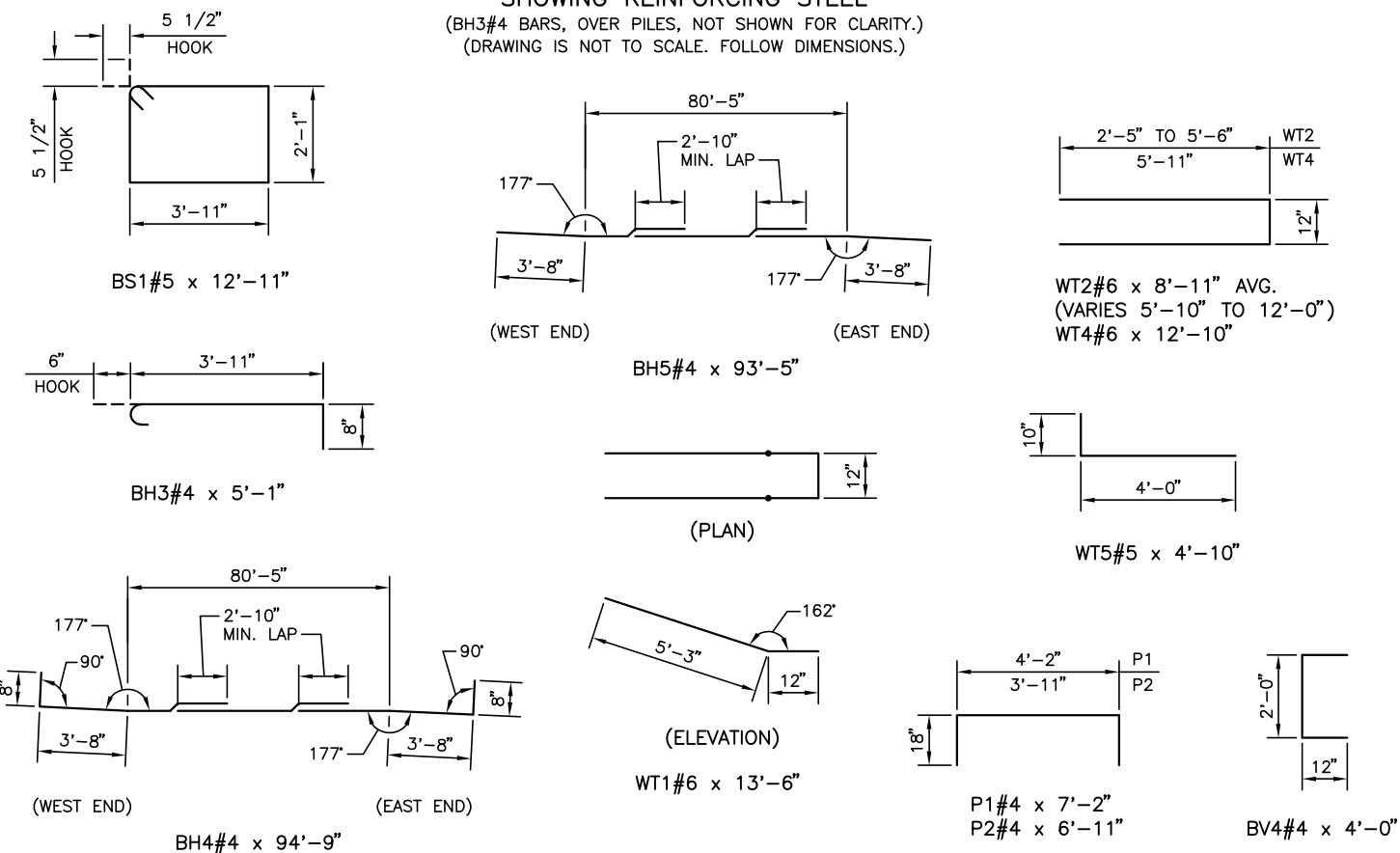
ABUTMENT 2 DETAILS (1 OF 2)
 @ STA. 196+50.65

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"



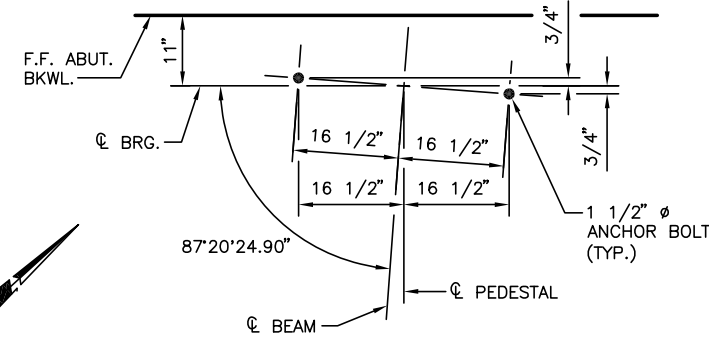
PART PLAN OF BRIDGE SEAT AND BACKWALL SHOWING REINFORCING STEEL

(BH3#4 BARS, OVER PILES, NOT SHOWN FOR CLARITY.)
(DRAWING IS NOT TO SCALE. FOLLOW DIMENSIONS.)



BAR BEND DETAILS

NOTE: FOR BAR BEND DETAILS NOT SHOWN, SEE SHEET NO. B013.



TYPICAL ANCHOR BOLT LAYOUT

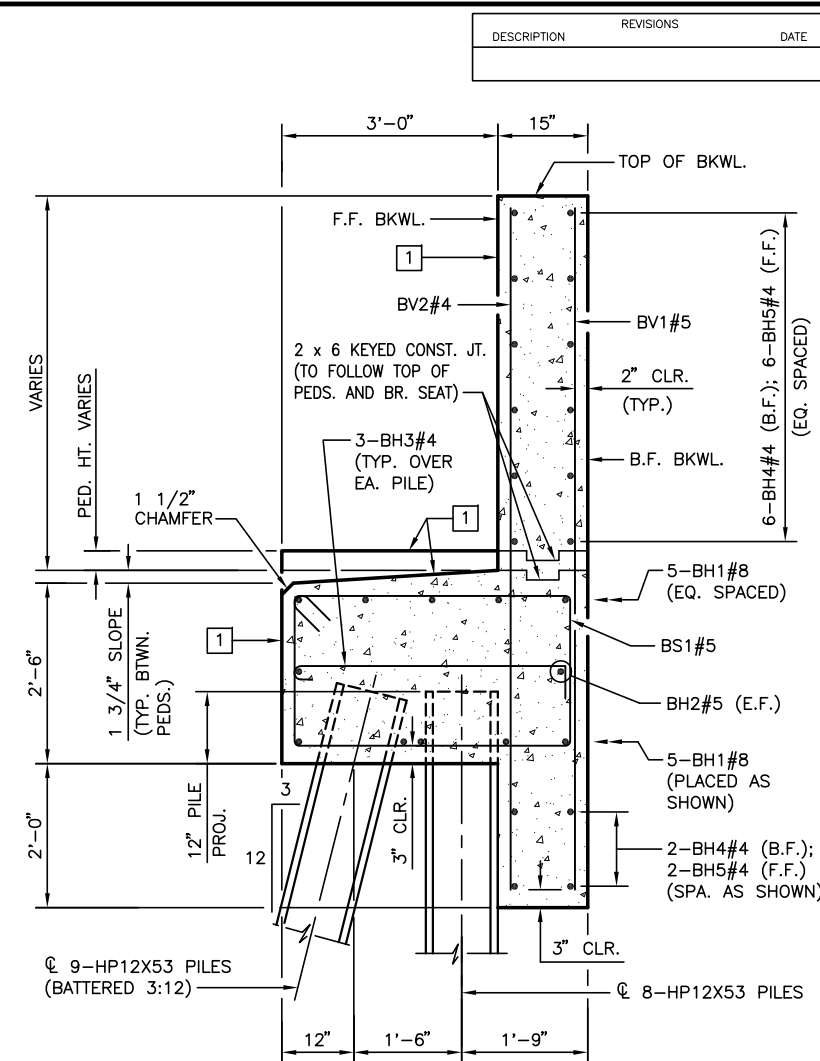
| ABUTMENT 2 BAR LIST | | | | |
|---------------------|-----|------|-------|-----------------|
| MARK | NO. | SIZE | SHAPE | LENGTH |
| (EPOXY COATED) | | | | |
| BH1 | 10 | 8 | STR. | 93'-11" |
| BH2 | 2 | 5 | STR. | 94'-11" |
| BH3 | 51 | 4 | BNT. | 5'-1" |
| BH4 | 8 | 4 | BNT. | 94'-9" |
| BH5 | 8 | 4 | BNT. | 93'-5" |
| BS1 | 84 | 5 | BNT. | 12'-11" |
| BV1 | 86 | 5 | STR. | 9'-2" |
| BV2 | 86 | 4 | STR. | 9'-2" |
| BV3 | 8 | 4 | STR. | 10'-3" |
| BV4 | 6 | 4 | BNT. | 4'-0" |
| P1 | 36 | 4 | BNT. | 7'-2" |
| P2 | 45 | 4 | BNT. | 6'-11" |
| WH1 | 4 | 6 | BNT. | 24'-1" |
| WH2 | 20 | 6 | STR. | 13'-0" AVG. |
| WH3 | 28 | 6 | STR. | 23'-5" |
| WH6 | 10 | 6 | STR. | 9'-0 1/2" AVG. |
| WH7 | 10 | 6 | STR. | 17'-2 1/2" AVG. |
| WP1 | 6 | 4 | BNT. | 8'-8" |
| WP2 | 8 | 4 | STR. | 1'-7" |
| WT1 | 2 | 6 | BNT. | 13'-6" |
| WT2 | 6 | 6 | BNT. | 8'-11" AVG. |
| WT3 | 20 | 6 | STR. | 9'-2" |
| WT4 | 24 | 6 | BNT. | 12'-10" |
| WT5 | 30 | 5 | BNT. | 4'-10" |
| WV1 | 16 | 4 | STR. | 3'-7" |
| WV2 | 80 | 4 | STR. | 6'-11 1/2" AVG. |

- (1) LENGTH INCLUDES ONE (1) 6'-2" LAP SPLICE.
- (2) LENGTH INCLUDES TWO (2) 3'-7" LAP SPLICES.
- (3) LENGTH INCLUDES TWO (2) 2'-10" LAP SPLICES.
- (4) 10 BARS VARYING FROM 5'-11" TO 20'-1"; TWO OF EACH LENGTH.
- (5) 5 BARS VARYING FROM 5'-10" TO 12'-3"; TWO OF EACH LENGTH.
- (6) 5 BARS VARYING FROM 14'-0" TO 20'-5"; TWO OF EACH LENGTH.
- (7) 3 BARS VARYING FROM 5'-10" TO 12'-0"; TWO OF EACH LENGTH.
- (8) 20 BARS VARYING FROM 3'-11" TO 10'-0"; FOUR OF EACH LENGTH.

NOTES: FOR WING DETAILS, SEE SHEET NO. B013.

STEEL PILE CONCRETE ENCASEMENTS NOT SHOWN FOR CLARITY. SEE STD. HP1-2 FOR DETAILS.

ALL EDGES OF THE ABUTMENT AND WINGS SHALL HAVE A 3/4" CHAMFER, UNLESS OTHERWISE SHOWN.



SECTION THRU BRIDGE SEAT

(SHOWN BETWEEN BENDS IN BACKWALL.)
(PILE ENCASEMENTS ARE NOT SHOWN FOR CLARITY.)

- 1 TREAT WITH A PENETRATING WATER REPELLENT SURFACE TREATMENT, INCLUDING SIDES OF PEDESTALS. THE FRONT FACE OF THE BRIDGE SEAT NEED ONLY BE TREATED ABOVE THE TOP OF SLOPE WALL.

B.F. - DENOTES BACK FACE
F.F. - DENOTES FRONT FACE
E.F. - DENOTES EACH FACE

| ABUTMENT 2 QUANTITIES | | |
|--------------------------------------|-------|----------|
| ITEM | UNITS | TOTAL |
| SUBSTRUCTURE EXCAVATION COMMON | CY | 155.00 |
| CLSM BACKFILL | CY | 203.20 |
| CLASS A CONCRETE | CY | 83.10 |
| EPOXY COATED REINFORCING STEEL | LB | 10200.00 |
| PILES, FURNISHED (HP10x42) | LF | 86.00 |
| PILES, FURNISHED (HP12x53) | LF | 664.00 |
| PILES, DRIVEN (HP10x42) | LF | 86.00 |
| PILES, DRIVEN (HP12x53) | LF | 664.00 |
| PILE SPLICE, H-PILE (NON-BIDDABLE) | EA | 0.00 |
| WATER REPELLENT (VISUALLY INSPECTED) | SY | 104.00 |
| 6" PERFORATED PIPE UNDERDRAIN ROUND | LF | 86.00 |
| 6" NON-PERF. PIPE UNDERDRAIN RND. | LF | 35.00 |

| | | | | |
|----------|-----|-------|--------------------|--------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 03/19 | SB US-81 OVER I-44 | |
| Checked | . | | | |
| Approved | . | | | |
| Squad | | | | |

ABUTMENT 2 DETAILS (2 OF 2)

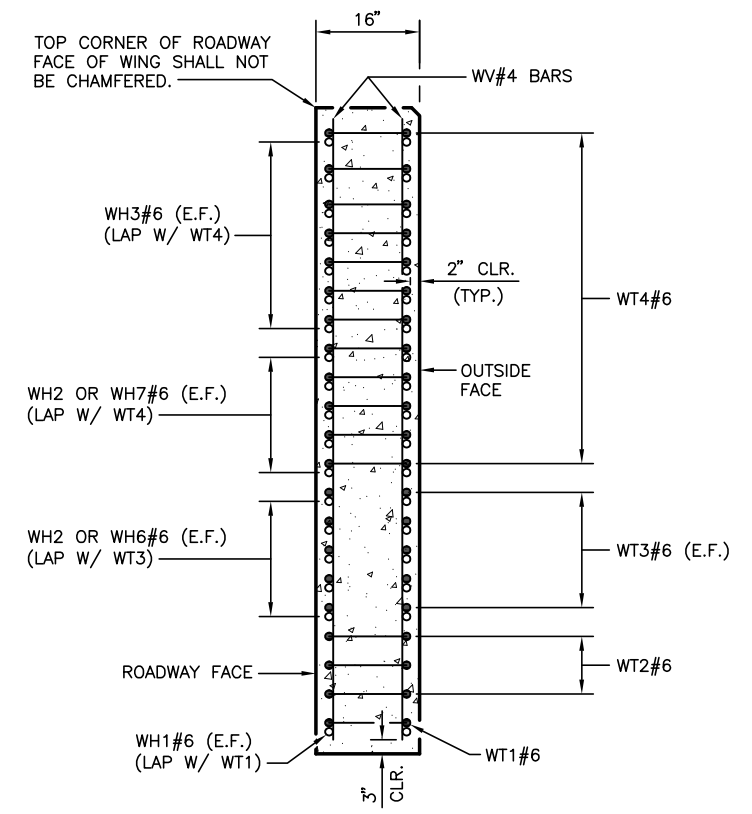
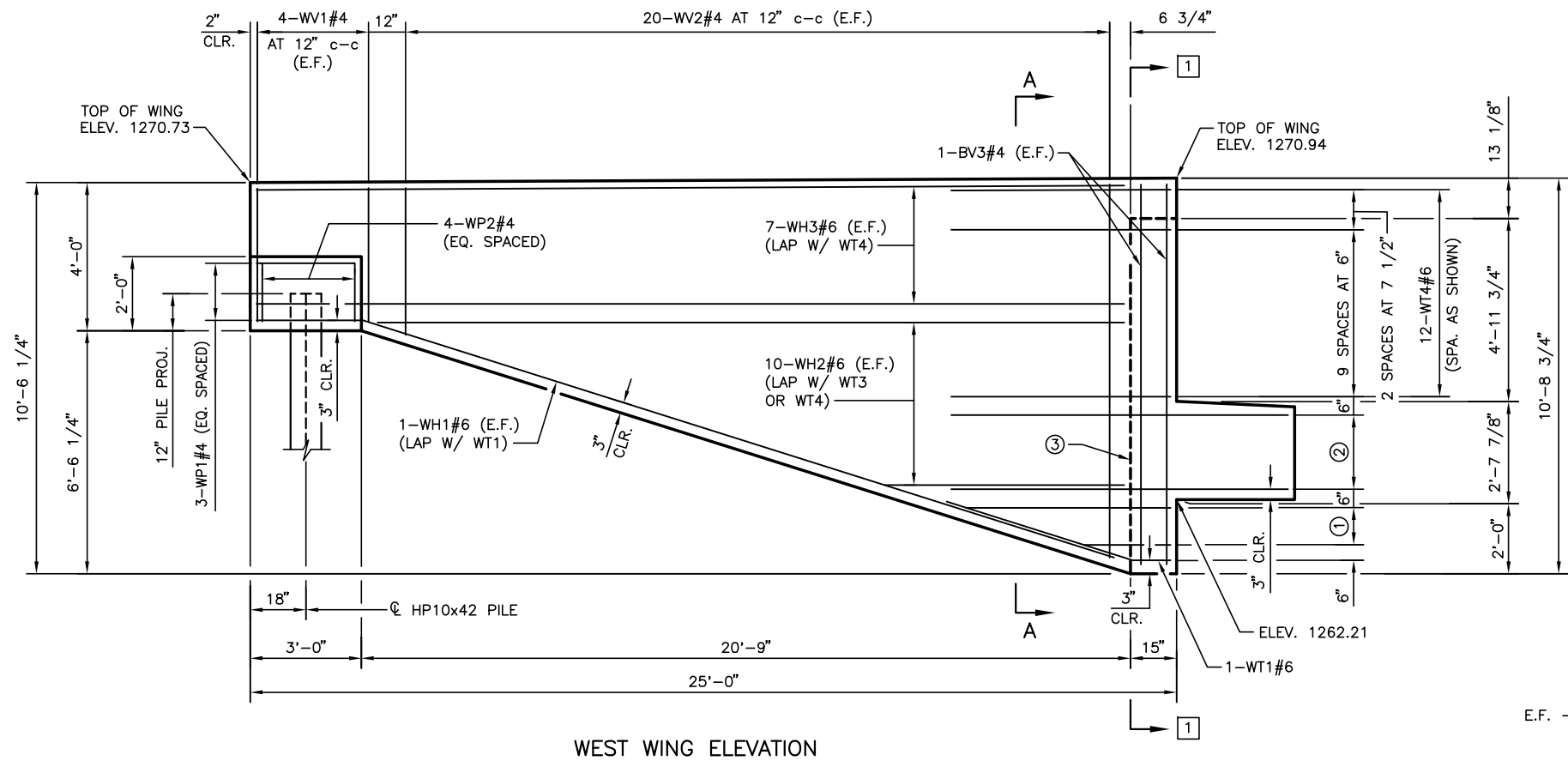
CL STA. 196+50.65

Job Piece No. 24428(04)

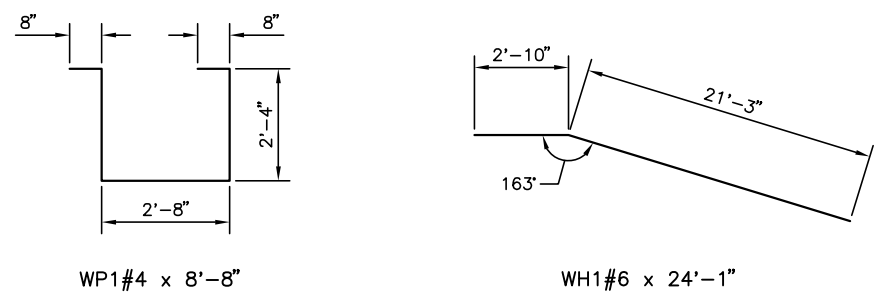
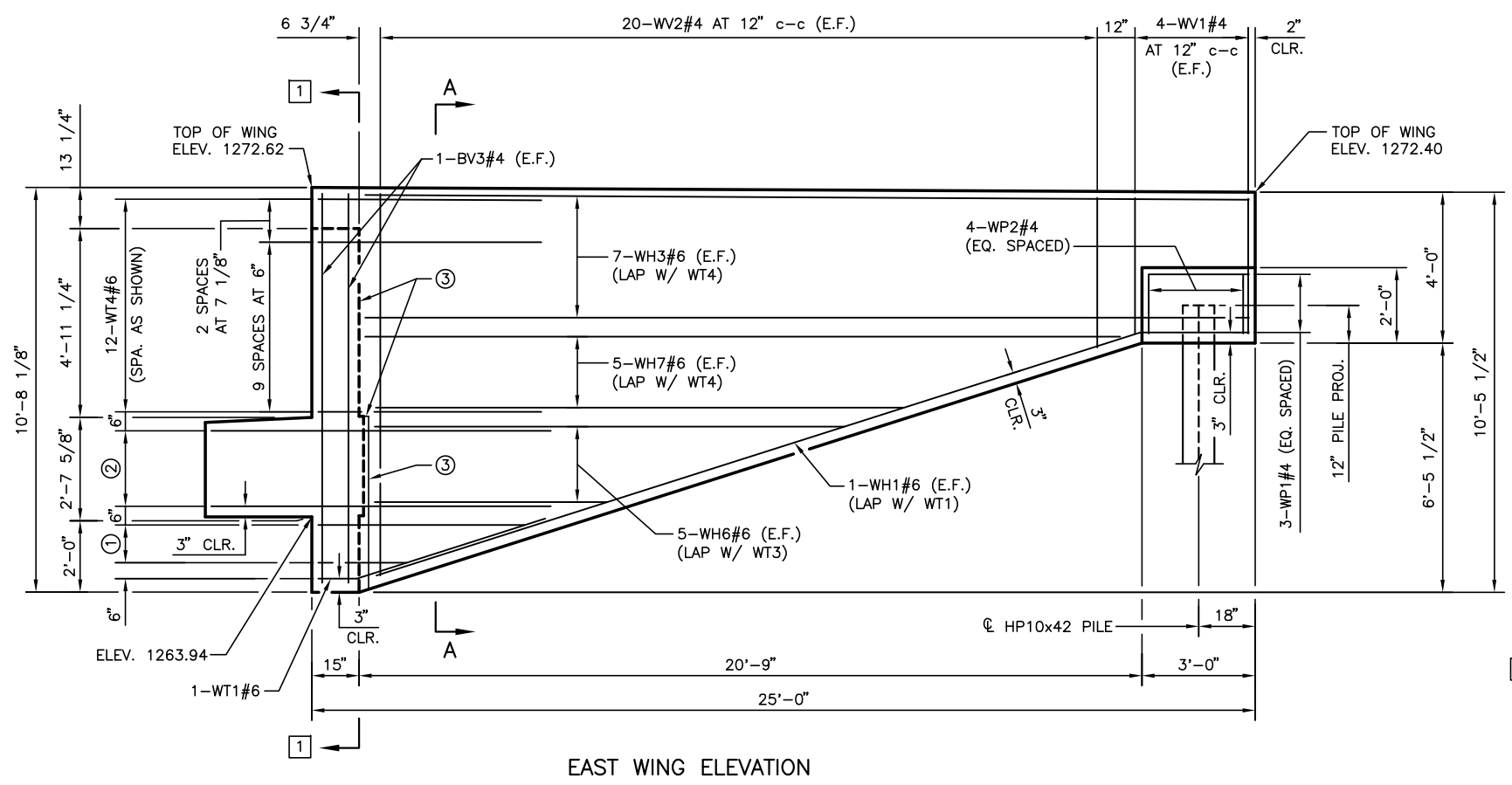
Sheet No. B012

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



- E.F. - DENOTES EACH FACE
- ① 3-WT2#6 AT 6" c-c
 - ② 5-WT3#6 AT 6" c-c (E.F.)
 - ③ OPTIONAL KEYED CONST. JOINT



NOTE: FOR ABUTMENT BAR LIST AND WT BAR BEND DETAILS, SEE SHEET NO. B012.

① TREAT ENDS OF BRIDGE SEAT AND ALL EXPOSED SURFACES OF WINGS WITH A WATER REPELLENT SURFACE TREATMENT, ABOVE THE FINISHED GROUND SURFACE AND IN FRONT (AHEAD) OF THE BACK FACE OF BACKWALL LINE.

| | | | |
|----------|-----------|-------------------------------|----------------|
| Design | . | BRIDGE "F" SB US-81 OVER I-44 | GRADY COUNTY |
| Drawn | PKW 03/19 | | |
| Checked | . | | |
| Approved | . | | |
| Squad | | Job Piece No. 24428(04) | Sheet No. B013 |

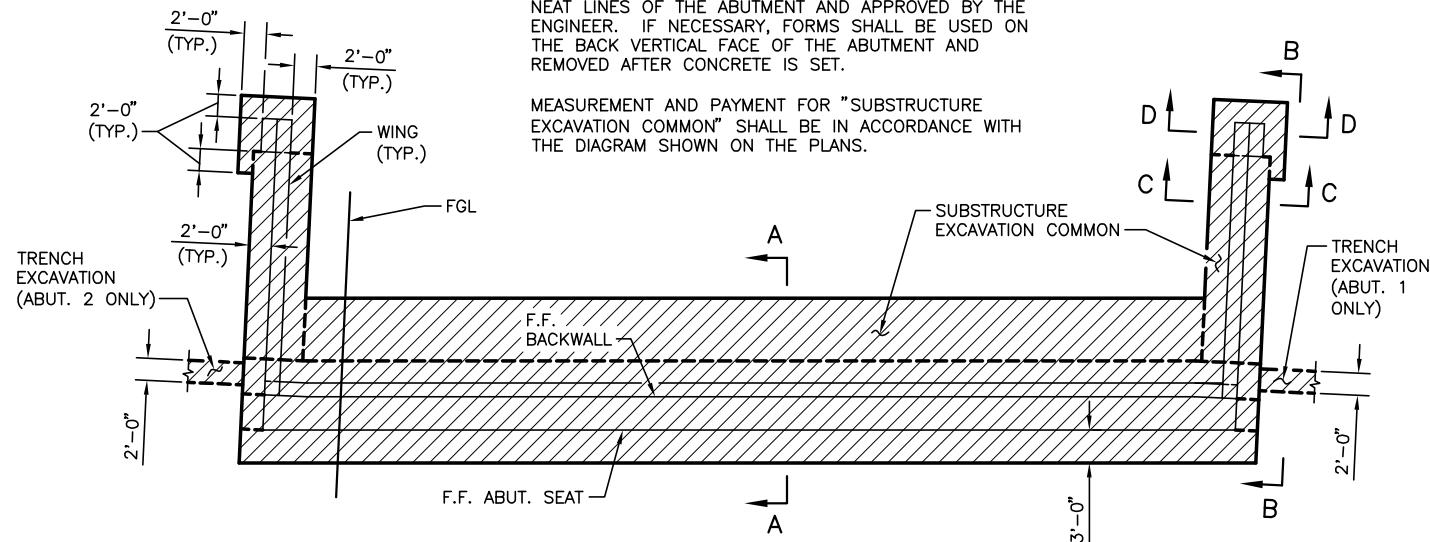
"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B014-2442804-SUBSTRUCTURE EXCAVATION AND PIPE UNDERDRAIN DETAILS.dwg 3/2/2020 2:24:46 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

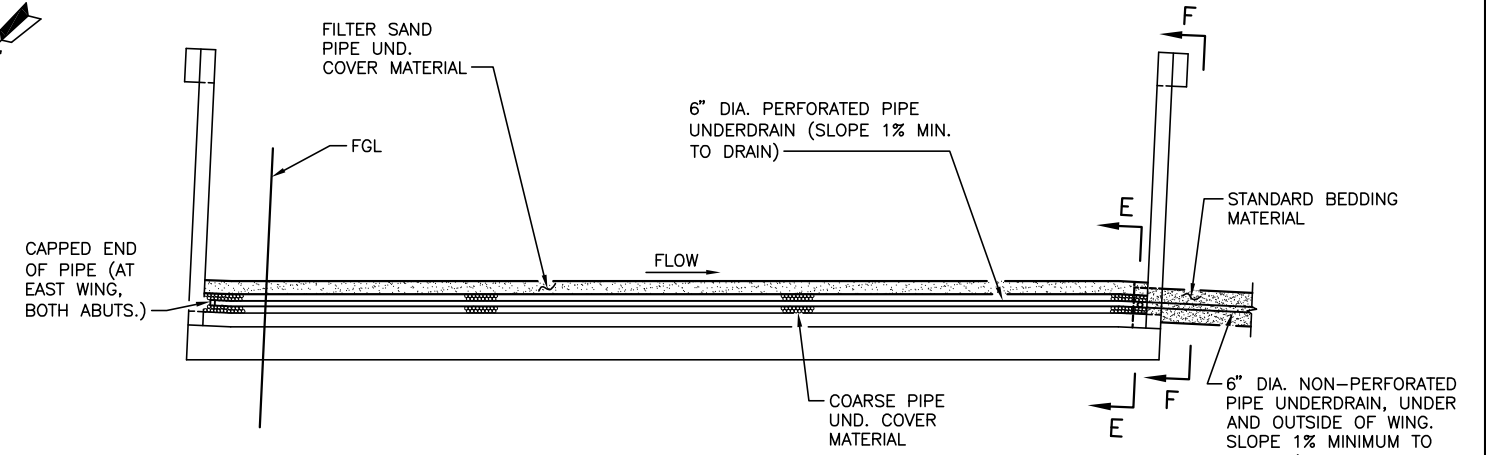
NOTES: CONCRETE MAY BE PLACED AGAINST THE LIMITS OF EXCAVATION IF THE MATERIAL IS EXCAVATED TO THE NEAT LINES OF THE ABUTMENT AND APPROVED BY THE ENGINEER. IF NECESSARY, FORMS SHALL BE USED ON THE BACK VERTICAL FACE OF THE ABUTMENT AND REMOVED AFTER CONCRETE IS SET.

MEASUREMENT AND PAYMENT FOR "SUBSTRUCTURE EXCAVATION COMMON" SHALL BE IN ACCORDANCE WITH THE DIAGRAM SHOWN ON THE PLANS.



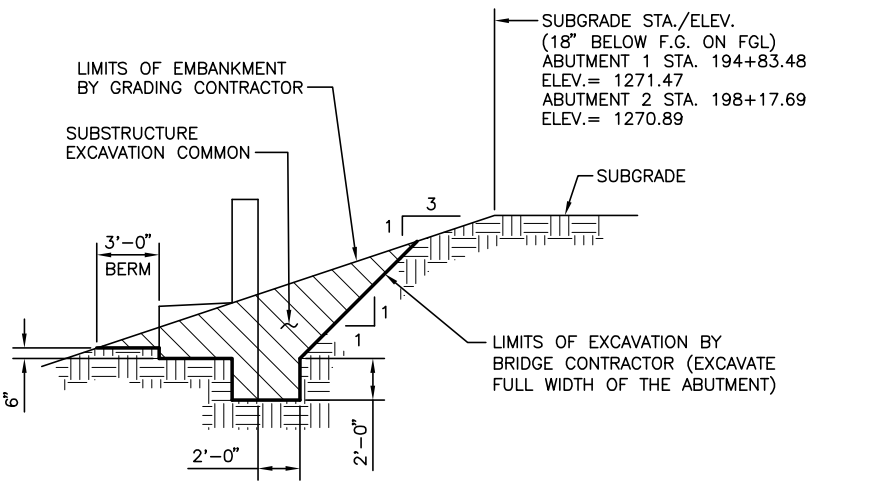
F.F. - DENOTES FRONT FACE

ABUTMENT EXCAVATION PLAN
(ABUTMENT 1 SHOWN. ABUTMENT 2 SIMILAR.)



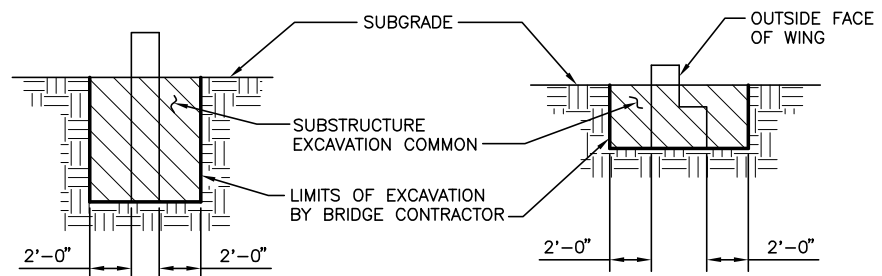
PLAN OF PIPE UNDERDRAIN
(ABUTMENT 1 SHOWN.)

NOTE: THE ENGINEER MAY ADJUST THE EXTENT, LOCATION AND DEPTH OF THE 6" NON-PERFORATED PIPE UNDERDRAIN DURING CONSTRUCTION. INCLUDE THE COST OF PIPE UNDERDRAIN COVER MATERIAL (BOTH FINE AND COARSE), FILTER FABRIC, TRENCH EXCAVATION, STANDARD BEDDING MATERIAL, AND EQUIPMENT AND LABOR FOR THEIR INSTALLATION IN THE CONTRACT UNIT PRICE OF 6" PERFORATED PIPE UNDERDRAIN ROUND AND 6" NON-PERF. PIPE UNDERDRAIN RND. INSTALL AS SHOWN ON THE PLANS AND STD. PUD-3.



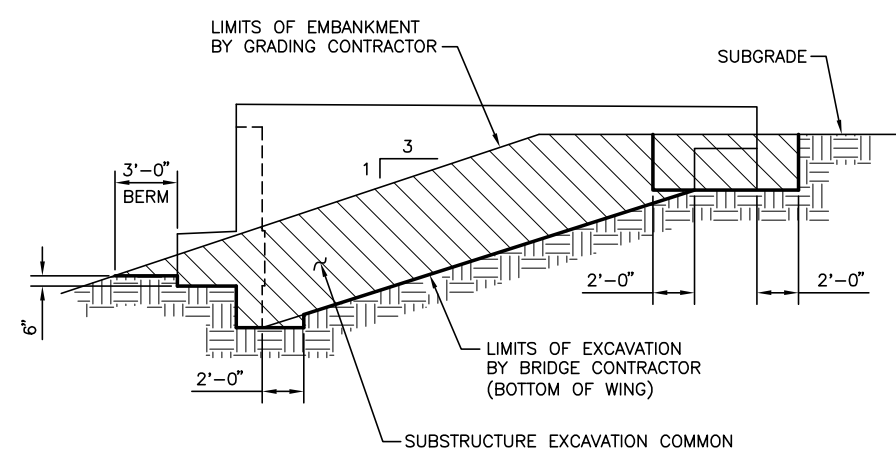
SECTION A-A

NOTE: ALL STATIONING FOLLOWS ϕ SURVEY US 81.

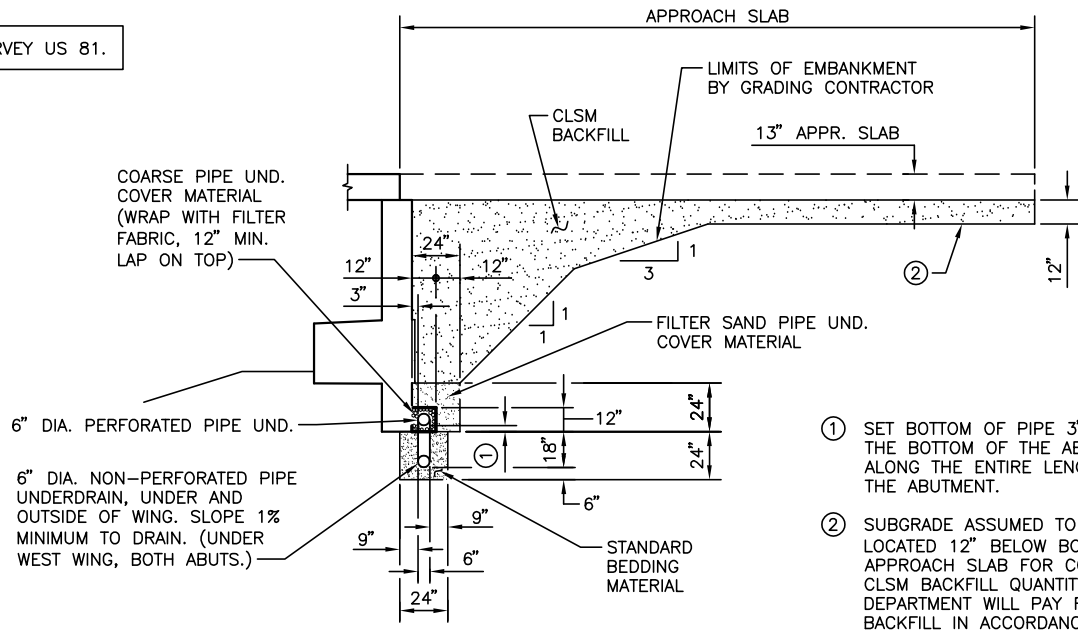


SECTION C-C

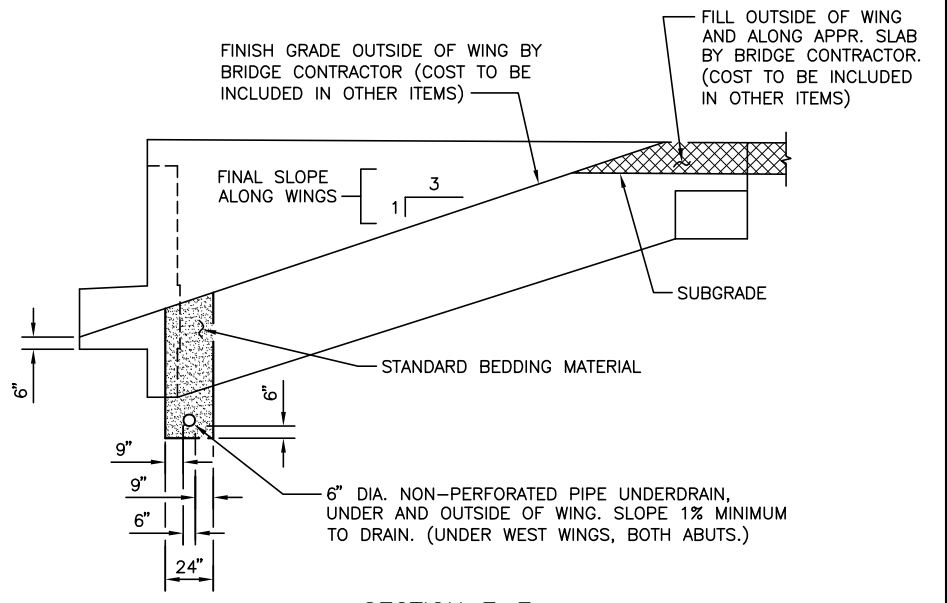
SECTION D-D



SECTION B-B



SECTION E-E



SECTION F-F

NOTE: DO NOT PLACE CLSM BACKFILL UNTIL SUPERSTRUCTURE IS IN PLACE AND THE ABUTMENT WING CONCRETE HAS ATTAINED A STRENGTH OF 3000 PSI.

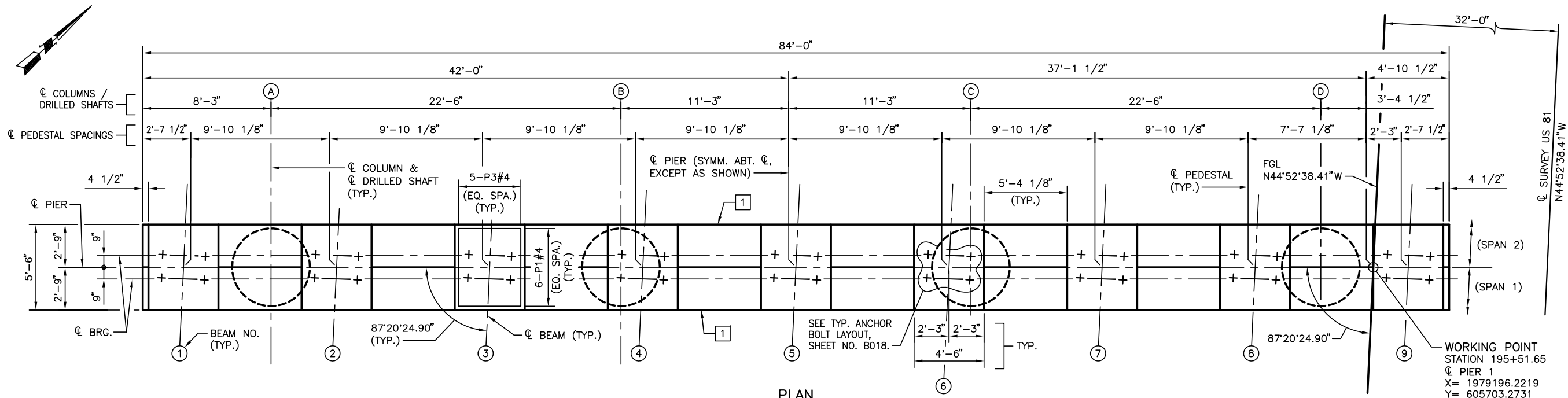
NOTE: FOR DETAILS OF PIPE UNDERDRAIN NOT SHOWN, SEE STD. PUD-3.

- SET BOTTOM OF PIPE 3" ABOVE THE BOTTOM OF THE ABUTMENT, ALONG THE ENTIRE LENGTH OF THE ABUTMENT.
- SUBGRADE ASSUMED TO BE LOCATED 12" BELOW BOTTOM OF APPROACH SLAB FOR COMPUTING CLSM BACKFILL QUANTITY. THE DEPARTMENT WILL PAY FOR CLSM BACKFILL IN ACCORDANCE WITH THE PLAN AND NO ADJUSTMENT WILL BE MADE FOR ACTUAL LOCATION OF SUBGRADE.

| | | | | |
|----------|-----|-------|---|----------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 03/19 | SB US-81 OVER I-44 | |
| Checked | . | | SUBSTRUCTURE EXCAVATION AND PIPE UNDERDRAIN ASSEMBLY DETAILS | |
| Approved | . | | ϕ STA. 196+50.65 | |
| Squad | | | Job Piece No. 24428(04) | Sheet No. B014 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

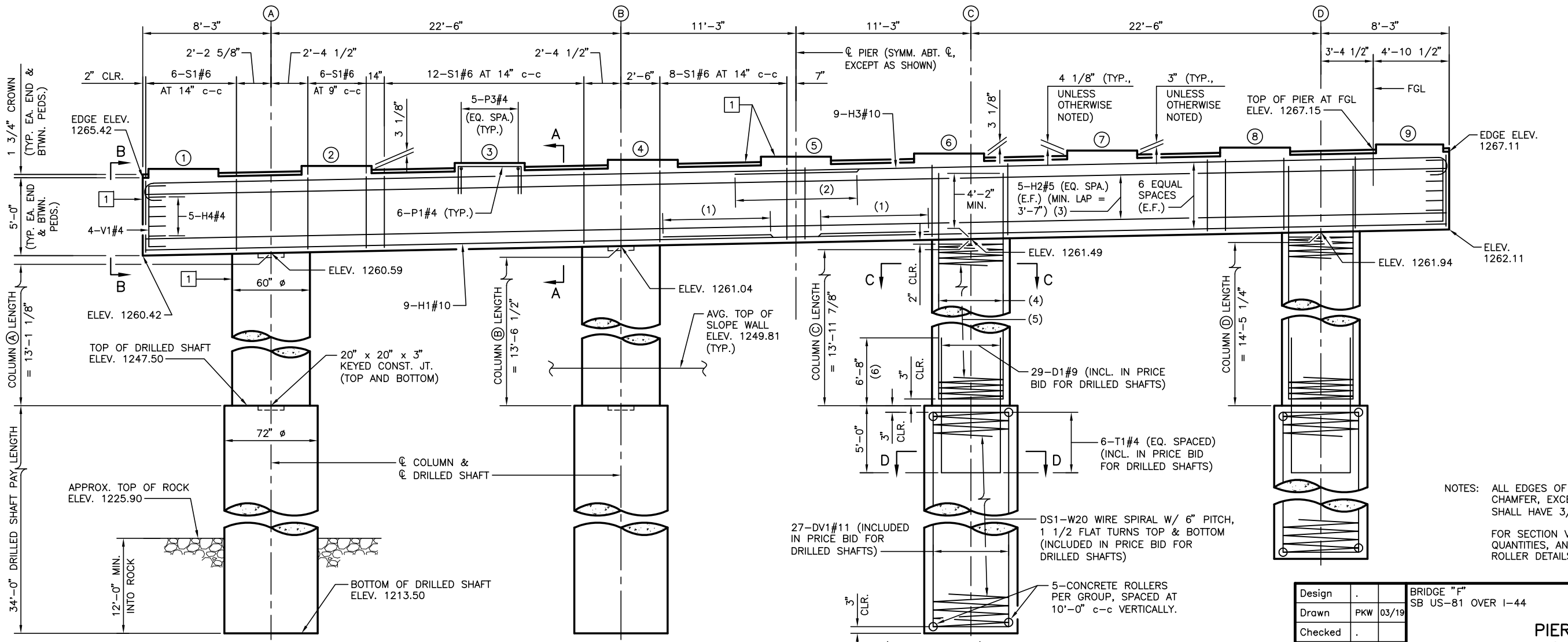


PLAN

| PEDESTAL | ELEVATION |
|----------|-----------|
| 1 | 1265.92 |
| 2 | 1266.12 |
| 3 | 1266.31 |
| 4 | 1266.51 |
| 5 | 1266.71 |
| 6 | 1266.91 |
| 7 | 1267.10 |
| 8 | 1267.30 |
| 9 | 1267.50 |

E.F. - DENOTES EACH FACE

1 TREAT TOP OF PIER CAP, INCLUDING ALL SURFACES OF PEDESTALS, ALL VERTICAL FACES OF THE PIER CAP AND ALL SURFACES OF THE COLUMNS ABOVE THE TOP OF SLOPE WALL, WITH A PENETRATING WATER REPELLENT SURFACE TREATMENT.



ELEVATION

- (1) 6'-11" MIN. LAP (H1 BARS). PROVIDE ONE (1) LAP SPLICE PER BAR AND STAGGER LAPS ADJACENT TO THE INTERIOR COLUMNS, AS SHOWN.
- (2) 7'-10" MIN. LAP (H3 BARS). PROVIDE ONE (1) LAP SPLICE PER BAR AT THE ϕ PIER, AS SHOWN.
- (3) PROVIDE TWO (2) 3'-7" LAP SPLICES PER BAR (NOT SHOWN).
- (4) 29-CV1#9 (COL. A); 29-CV2#9 (COL. B); 29-CV3#9 (COL. C); 29-CV4#9 (COL. D)
- (5) CS1-W20 (COL. A); CS2-W20 (COL. B); CS3-W20 (COL. C); CS4-W20 (COL. D) WIRE SPIRAL W/ 4" PITCH, 1 1/2 FLAT TURNS TOP & BOTTOM
- (6) NOT TO BE CONSIDERED AS ADD'L DRILLED SHAFT PAY LENGTH.

NOTES: ALL EDGES OF THE PIER CAP SHALL HAVE A 1 1/2" CHAMFER, EXCEPT FOR EDGES OF PEDESTALS, WHICH SHALL HAVE 3/4" CHAMFERS.

FOR SECTION VIEWS, BAR BEND DETAILS, BAR LISTS, QUANTITIES, AND SPIRAL REINFORCING AND CONCRETE ROLLER DETAILS, SEE SHEET NOS. B018 AND B019.

| | | | |
|----------|-----------|-------------------------------|----------------|
| Design | . | BRIDGE "F" SB US-81 OVER I-44 | GRADY COUNTY |
| Drawn | PKW 03/19 | | |
| Checked | . | | |
| Approved | . | | |
| Squad | | Job Piece No. 24428(04) | Sheet No. B015 |

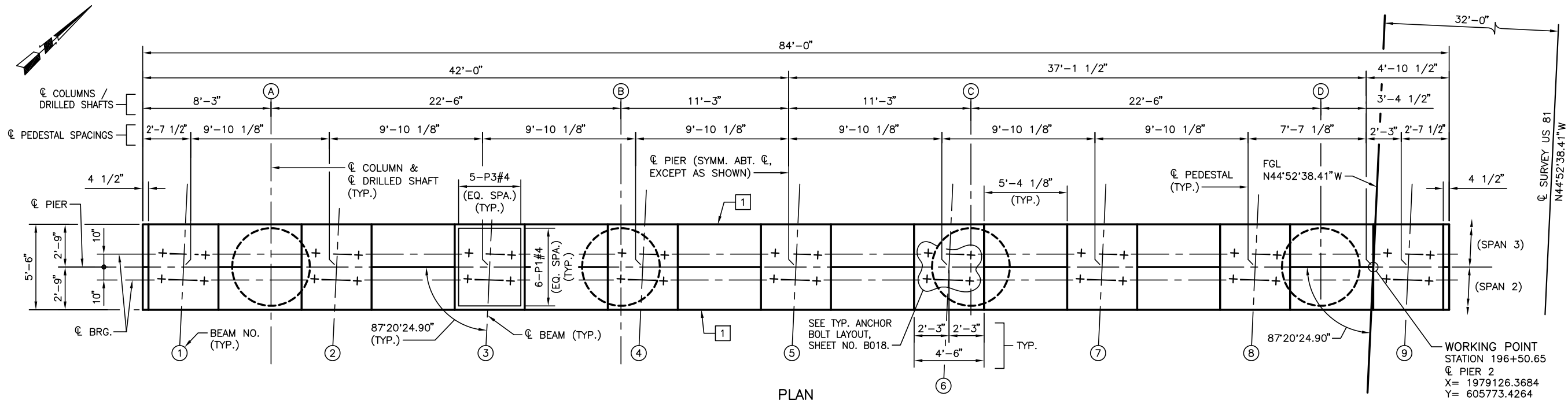
PIER 1 DETAILS
 ϕ STA. 196+50.65

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

(TYPICAL COLUMN/DRILLED SHAFT SIZES, DIMENSIONS AND DETAILS, UNLESS OTHERWISE SHOWN)

(TYPICAL COLUMN/DRILLED SHAFT REINFORCING DETAILS, UNLESS OTHERWISE SHOWN)

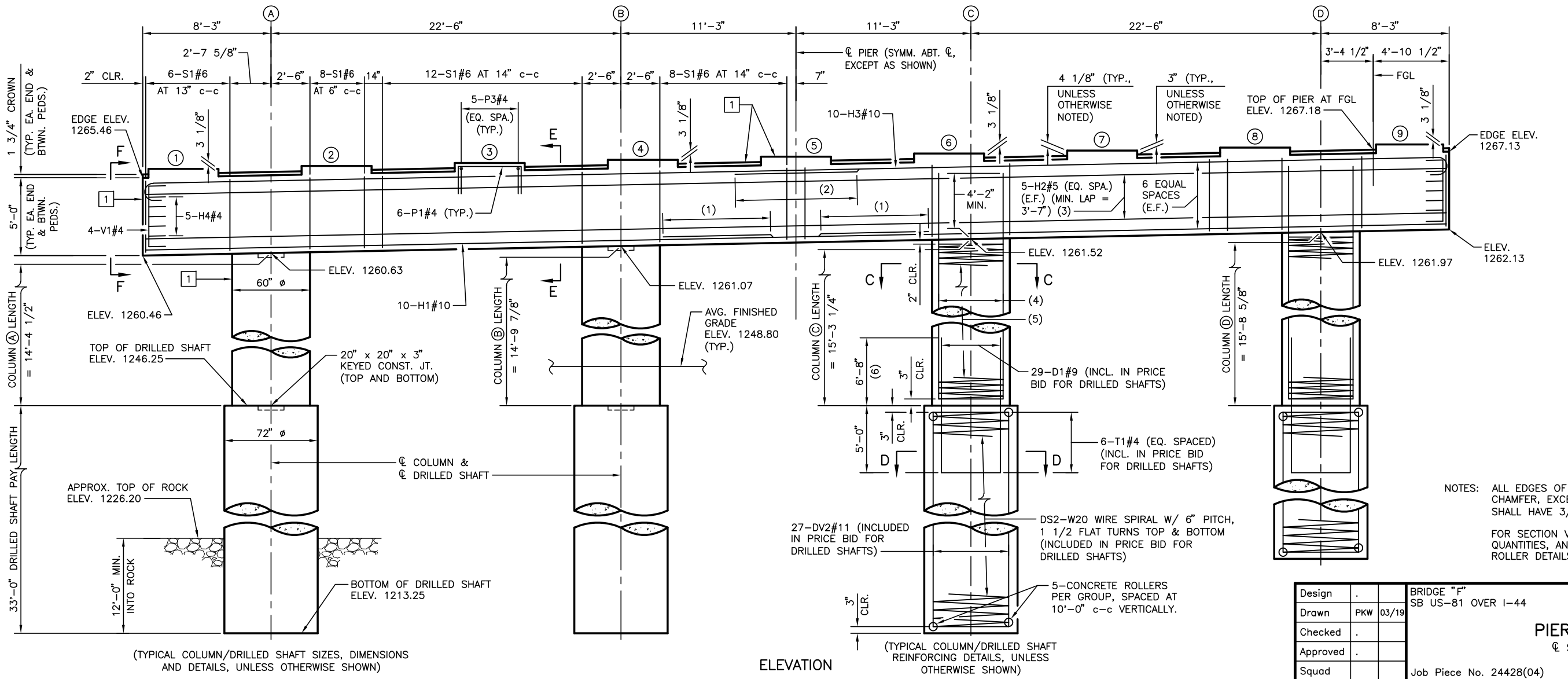
| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



| PEDESTAL | ELEVATION |
|----------|-----------|
| 1 | 1265.96 |
| 2 | 1266.15 |
| 3 | 1266.35 |
| 4 | 1266.55 |
| 5 | 1266.74 |
| 6 | 1266.94 |
| 7 | 1267.13 |
| 8 | 1267.33 |
| 9 | 1267.53 |

E.F. - DENOTES EACH FACE

1 TREAT TOP OF PIER CAP, INCLUDING ALL SURFACES OF PEDESTALS, ALL VERTICAL FACES OF THE PIER CAP AND ALL SURFACES OF THE COLUMNS ABOVE THE FINISHED GROUND SURFACE, WITH A PENETRATING WATER REPELLENT SURFACE TREATMENT.



- (1) 6'-11" MIN. LAP (H1 BARS). PROVIDE ONE (1) LAP SPLICE PER BAR AND STAGGER LAPS ADJACENT TO THE INTERIOR COLUMNS, AS SHOWN.
- (2) 7'-10" MIN. LAP (H3 BARS). PROVIDE ONE (1) LAP SPLICE PER BAR AT THE ϕ PIER, AS SHOWN.
- (3) PROVIDE TWO (2) 3'-7" LAP SPLICES PER BAR (NOT SHOWN).
- (4) 29-CV5#9 (COL. A); 29-CV6#9 (COL. B); 29-CV7#9 (COL. C); 29-CV8#9 (COL. D)
- (5) CS5-W20 (COL. A); CS6-W20 (COL. B); CS7-W20 (COL. C); CS8-W20 (COL. D) WIRE SPIRAL W/ 4" PITCH, 1 1/2 FLAT TURNS TOP & BOTTOM
- (6) NOT TO BE CONSIDERED AS ADD'L DRILLED SHAFT PAY LENGTH.

NOTES: ALL EDGES OF THE PIER CAP SHALL HAVE A 1 1/2" CHAMFER, EXCEPT FOR EDGES OF PEDESTALS, WHICH SHALL HAVE 3/4" CHAMFERS.

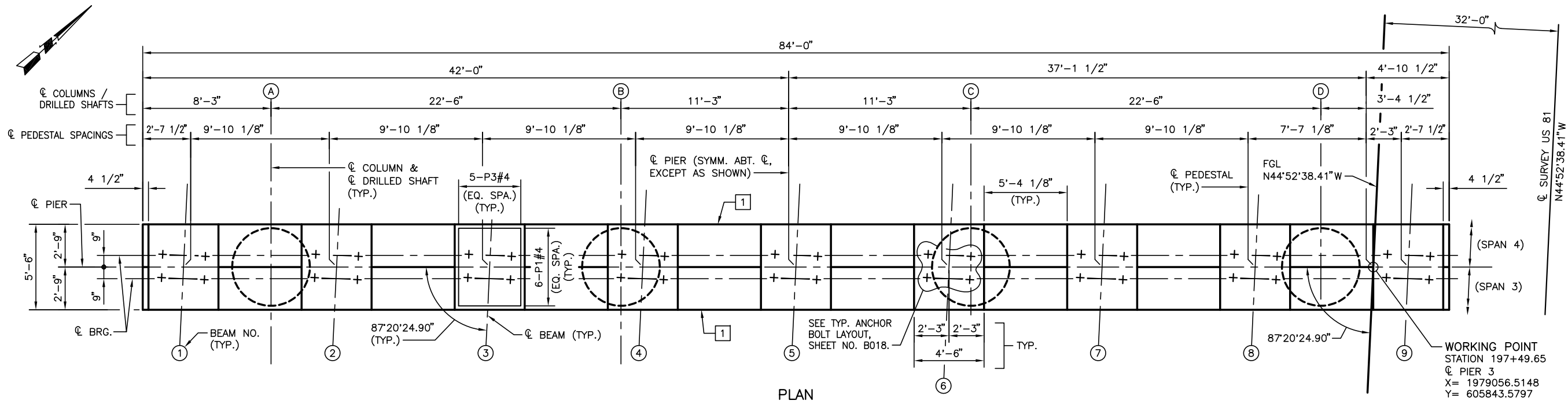
FOR SECTION VIEWS, BAR BEND DETAILS, BAR LISTS, QUANTITIES, AND SPIRAL REINFORCING AND CONCRETE ROLLER DETAILS, SEE SHEET NOS. B018 AND B019.

| | | | |
|----------|-----------|-------------------------------|----------------|
| Design | . | BRIDGE "F" SB US-81 OVER I-44 | GRADY COUNTY |
| Drawn | PKW 03/19 | | |
| Checked | . | | |
| Approved | . | | |
| Squad | | Job Piece No. 24428(04) | Sheet No. B016 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B017-2442804-PIER 3 DETAILS.dwg 3/2/2020 2:27:35 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

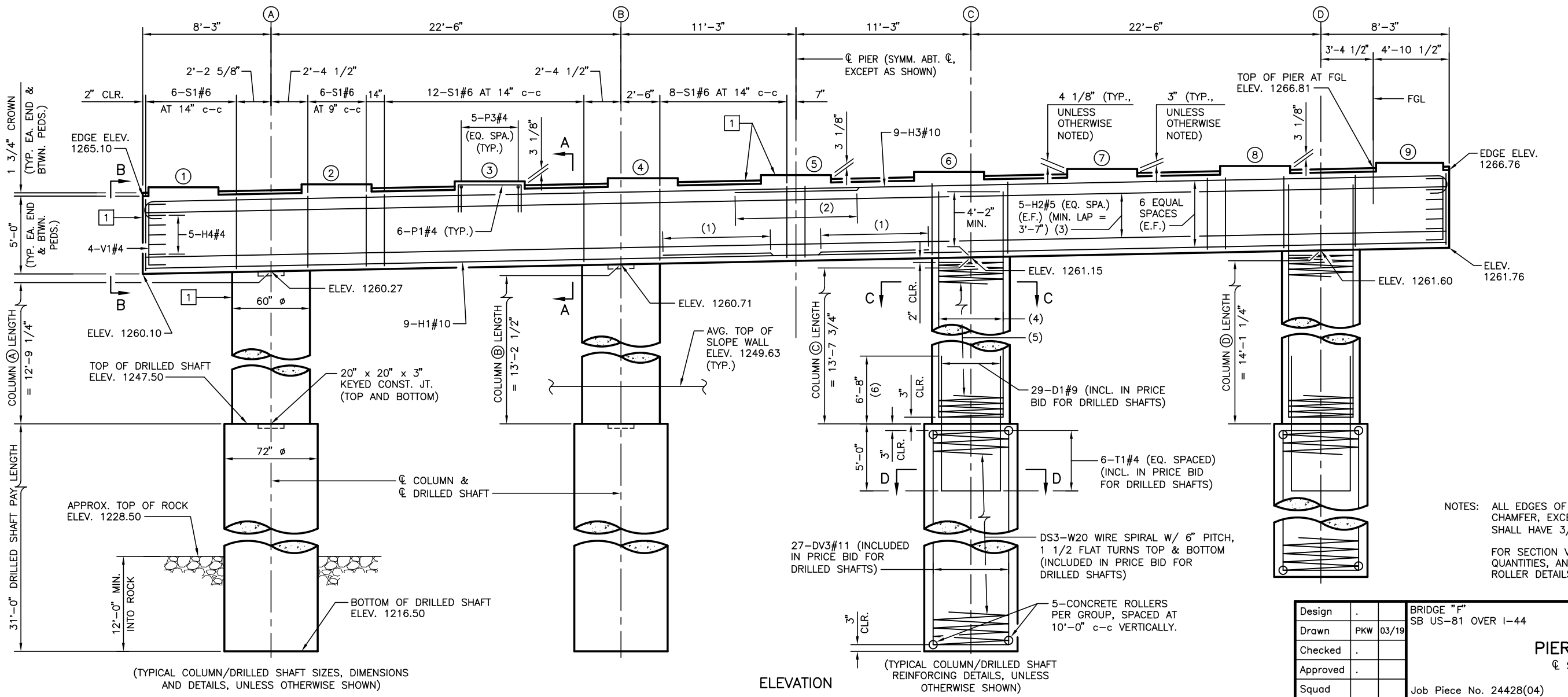


SCHEDULE OF PEDESTAL ELEVATIONS

| PEDESTAL | ELEVATION |
|----------|-----------|
| 1 | 1265.60 |
| 2 | 1265.79 |
| 3 | 1265.99 |
| 4 | 1266.18 |
| 5 | 1266.38 |
| 6 | 1266.57 |
| 7 | 1266.76 |
| 8 | 1266.96 |
| 9 | 1267.15 |

E.F. - DENOTES EACH FACE

1 TREAT TOP OF PIER CAP, INCLUDING ALL SURFACES OF PEDESTALS, ALL VERTICAL FACES OF THE PIER CAP AND ALL SURFACES OF THE COLUMNS ABOVE THE TOP OF SLOPE WALL, WITH A PENETRATING WATER REPELLENT SURFACE TREATMENT.



- (1) 6'-11" MIN. LAP (H1 BARS). PROVIDE ONE (1) LAP SPLICE PER BAR AND STAGGER LAPS ADJACENT TO THE INTERIOR COLUMNS, AS SHOWN.
- (2) 7'-10" MIN. LAP (H3 BARS). PROVIDE ONE (1) LAP SPLICE PER BAR AT THE C PIER, AS SHOWN.
- (3) PROVIDE TWO (2) 3'-7" LAP SPLICES PER BAR (NOT SHOWN).
- (4) 29-CV9#9 (COL. A); 29-CV10#9 (COL. B); 29-CV11#9 (COL. C); 29-CV12#9 (COL. D)
- (5) CS9-W20 (COL. A); CS10-W20 (COL. B); CS11-W20 (COL. C); CS12-W20 (COL. D); WIRE SPIRAL W/ 4" PITCH, 1 1/2 FLAT TURNS TOP & BOTTOM
- (6) NOT TO BE CONSIDERED AS ADD'L DRILLED SHAFT PAY LENGTH.

NOTES: ALL EDGES OF THE PIER CAP SHALL HAVE A 1 1/2" CHAMFER, EXCEPT FOR EDGES OF PEDESTALS, WHICH SHALL HAVE 3/4" CHAMFERS.

FOR SECTION VIEWS, BAR BEND DETAILS, BAR LISTS, QUANTITIES, AND SPIRAL REINFORCING AND CONCRETE ROLLER DETAILS, SEE SHEET NOS. B018 AND B019.

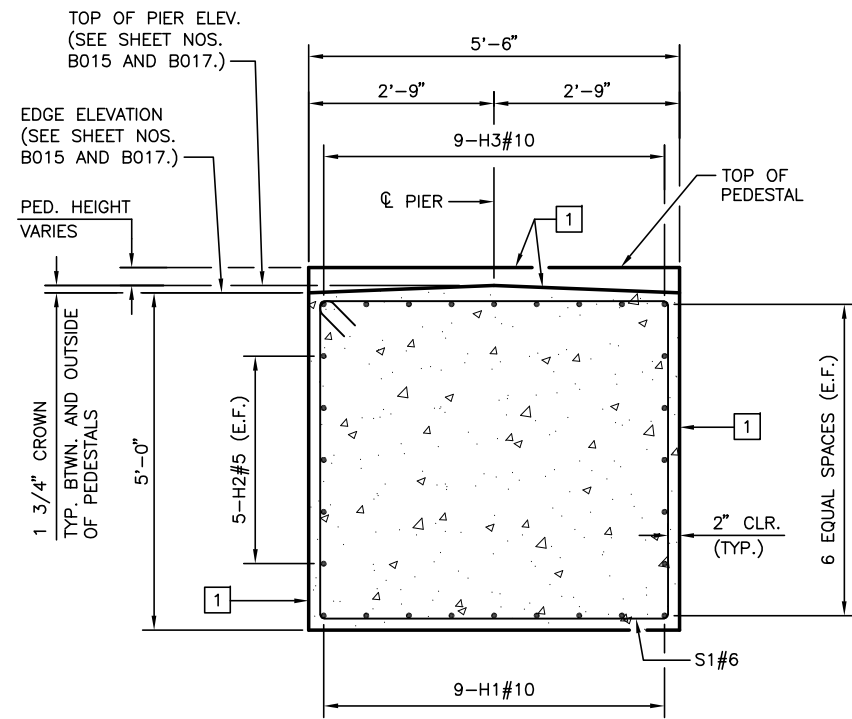
| | | | |
|----------|-----------|-------------------------------|----------------|
| Design | . | BRIDGE "F" SB US-81 OVER I-44 | GRADY COUNTY |
| Drawn | PKW 03/19 | | |
| Checked | . | | |
| Approved | . | | |
| Squad | | Job Piece No. 24428(04) | Sheet No. B017 |

PIER 3 DETAILS
C STA. 196+50.65

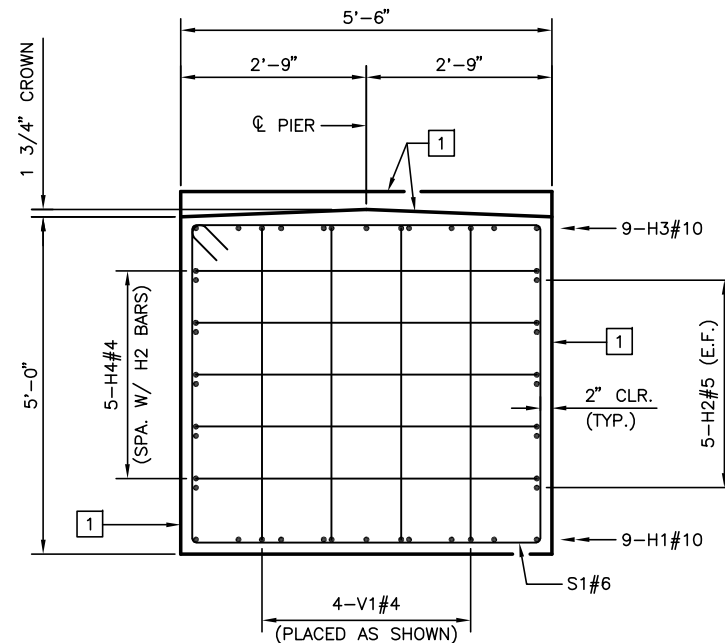
"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B018-2442804-MISCELLANEOUS PIER DETAILS 1.dwg 3/2/2020 2:28:33 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

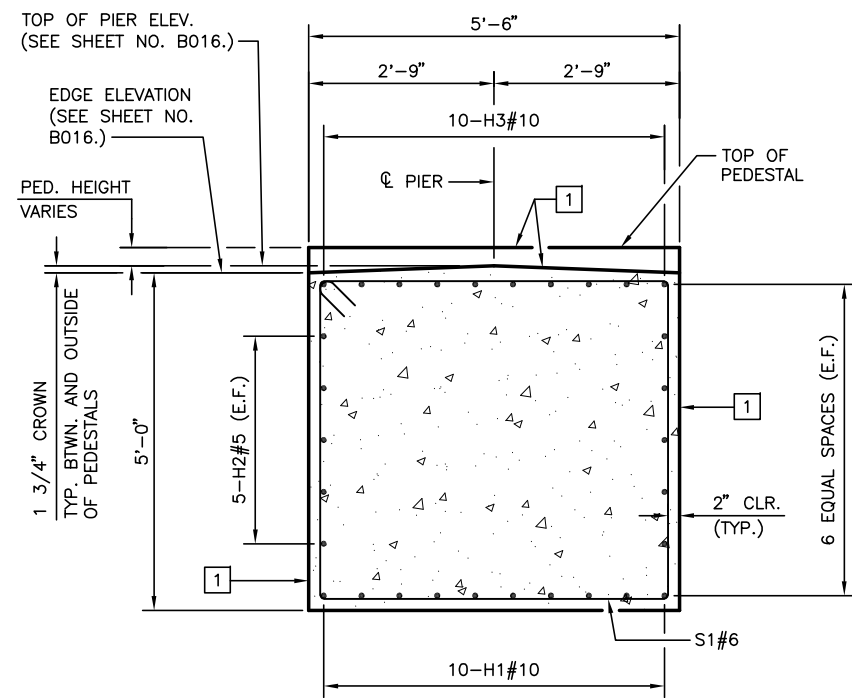


SECTION A-A

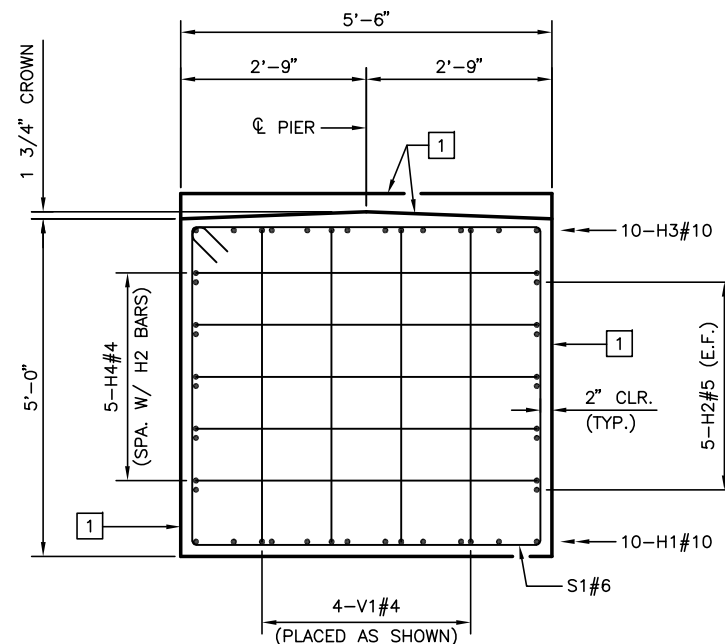


VIEW B-B
(TYP. EACH END OF CAP.)
(FOR DETAILS NOT SHOWN, SEE SECTION A-A.)

PIERS 1 AND 3 CAP SECTIONS

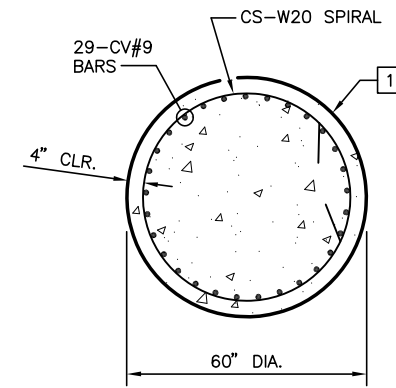


SECTION E-E

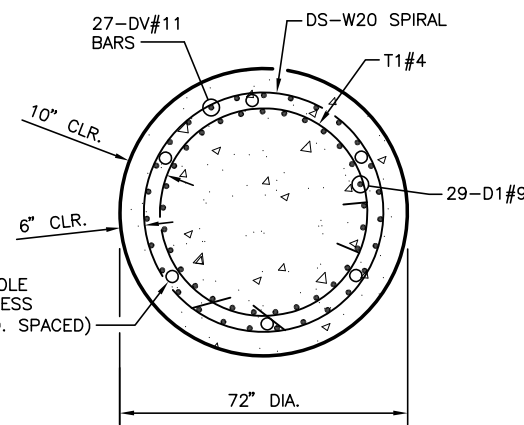


VIEW F-F
(TYP. EACH END OF CAP.)
(FOR DETAILS NOT SHOWN, SEE SECTION E-E.)

PIER 2 CAP SECTIONS



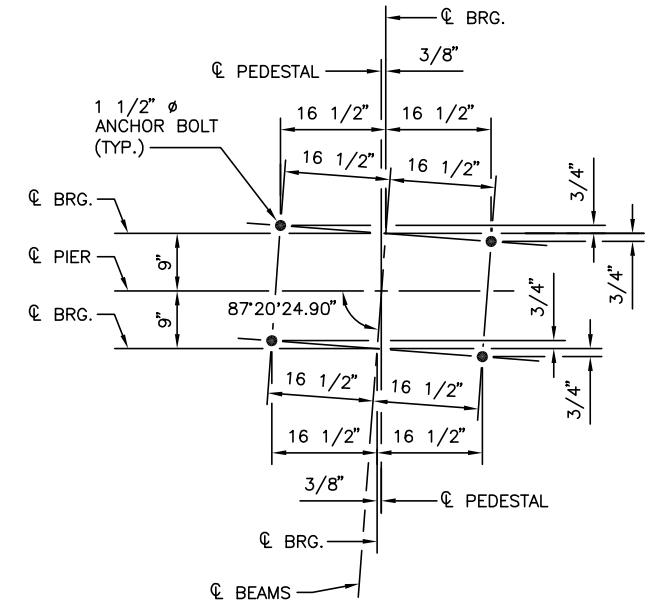
SECTION C-C



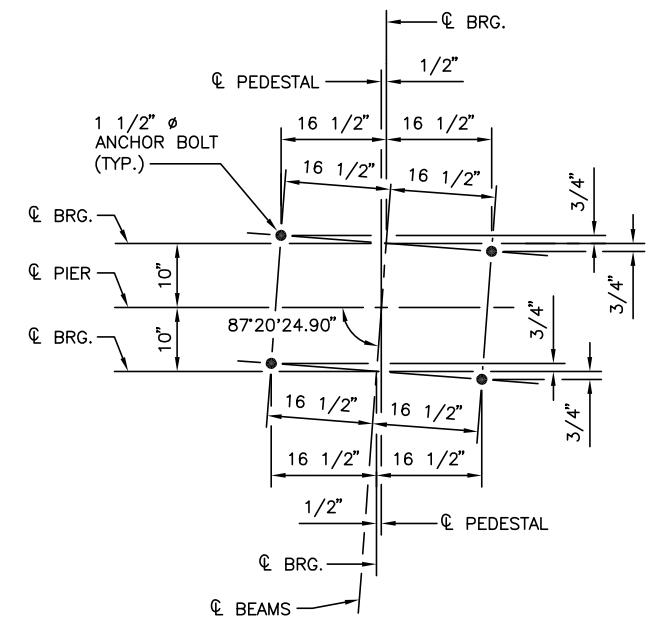
SECTION D-D

1 TREAT TOP OF PIER CAP, INCLUDING ALL SURFACES OF THE PEDESTALS, ALL VERTICAL FACES OF THE PIER CAP AND ALL SURFACES OF THE COLUMNS ABOVE THE FINISHED GROUND SURFACE OR TOP OF SLOPE WALL, WITH A PENETRATING WATER REPELLENT SURFACE TREATMENT.

E.F. - DENOTES EACH FACE



TYPICAL ANCHOR BOLT LAYOUT - PIERS 1 AND 3



TYPICAL ANCHOR BOLT LAYOUT - PIER 2

NOTES: PEDESTAL REINFORCING NOT SHOWN FOR CLARITY.

FOR BAR BEND DETAILS, BAR LISTS AND QUANTITIES AND SPIRAL REINFORCING AND CONCRETE ROLLER DETAILS, SEE SHEET NO. B019.

| | | | | |
|----------|-----|-------|--|----------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 03/19 | SB US-81 OVER I-44 | |
| Checked | . | | MISCELLANEOUS PIER DETAILS (1 OF 2) | |
| Approved | . | | ☉ STA. 196+50.65 | |
| Squad | | | Job Piece No. 24428(04) | Sheet No. B018 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

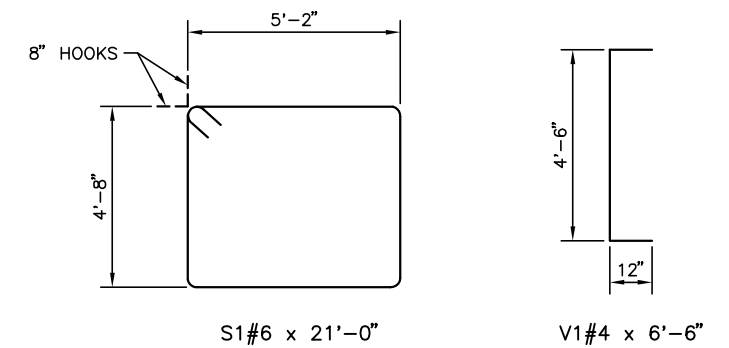
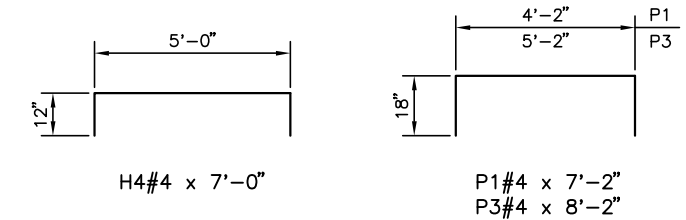
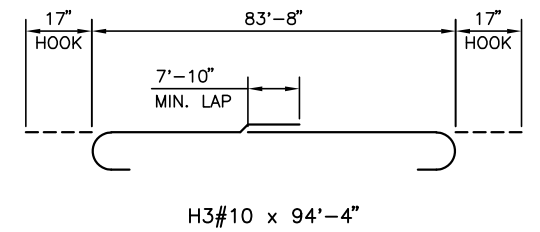
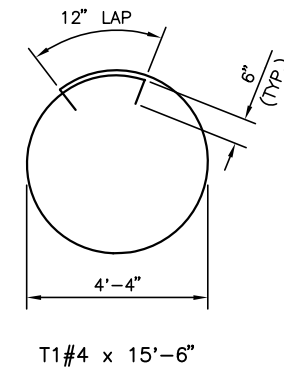
M:\W2M Projects\Project Files\M179\Final Design F & G\B019-2442804-MISCELLANEOUS PIER DETAILS 2.dwg 3/2/2020 2:29:16 PM

| PIER 1 BAR LIST | | | | |
|-----------------------|-----|------|--------|----------|
| MARK | NO. | SIZE | SHAPE | LENGTH |
| (EPOXY COATED) | | | | |
| CV1 | 29 | 9 | STR. | 17'-1" |
| CV2 | 29 | 9 | STR. | 17'-6" |
| CV3 | 29 | 9 | STR. | 17'-11" |
| CV4 | 29 | 9 | STR. | 18'-5" |
| H1 | 9 | 10 | STR. | 90'-7" |
| H2 | 10 | 5 | STR. | 90'-10" |
| H3 | 9 | 10 | BNT. | 94'-4" |
| H4 | 10 | 4 | BNT. | 7'-0" |
| P1 | 54 | 4 | BNT. | 7'-2" |
| P3 | 45 | 4 | BNT. | 8'-2" |
| S1 | 64 | 6 | BNT. | 21'-0" |
| V1 | 8 | 4 | BNT. | 6'-6" |
| (NON-EPOXY COATED) | | | | |
| CS1 | 1 | W20 | SPIRAL | 555'-1" |
| CS2 | 1 | W20 | SPIRAL | 573'-4" |
| CS3 | 1 | W20 | SPIRAL | 591'-6" |
| CS4 | 1 | W20 | SPIRAL | 609'-9" |
| PIER 1 DRILLED SHAFTS | | | | |
| (NON-EPOXY COATED) | | | | |
| D1 | 116 | 9 | STR. | 11'-8" |
| DS1 | 4 | W20 | SPIRAL | 1093'-4" |
| DV1 | 108 | 11 | STR. | 33'-6" |
| T1 | 24 | 4 | BNT. | 15'-6" |

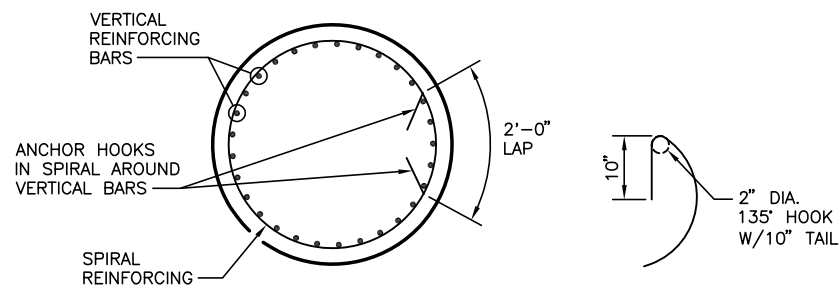
| PIER 2 BAR LIST | | | | |
|-----------------------|-----|------|--------|----------|
| MARK | NO. | SIZE | SHAPE | LENGTH |
| (EPOXY COATED) | | | | |
| CV5 | 29 | 9 | STR. | 18'-4" |
| CV6 | 29 | 9 | STR. | 18'-9" |
| CV7 | 29 | 9 | STR. | 19'-3" |
| CV8 | 29 | 9 | STR. | 19'-8" |
| H1 | 10 | 10 | STR. | 90'-7" |
| H2 | 10 | 5 | STR. | 90'-10" |
| H3 | 10 | 10 | BNT. | 94'-4" |
| H4 | 10 | 4 | BNT. | 7'-0" |
| P1 | 54 | 4 | BNT. | 7'-2" |
| P3 | 45 | 4 | BNT. | 8'-2" |
| S1 | 68 | 6 | BNT. | 21'-0" |
| V1 | 8 | 4 | BNT. | 6'-6" |
| (NON-EPOXY COATED) | | | | |
| CS5 | 1 | W20 | SPIRAL | 607'-4" |
| CS6 | 1 | W20 | SPIRAL | 625'-2" |
| CS7 | 1 | W20 | SPIRAL | 643'-4" |
| CS8 | 1 | W20 | SPIRAL | 661'-7" |
| PIER 2 DRILLED SHAFTS | | | | |
| (NON-EPOXY COATED) | | | | |
| D1 | 116 | 9 | STR. | 11'-8" |
| DS2 | 4 | W20 | SPIRAL | 1062'-2" |
| DV2 | 108 | 11 | STR. | 32'-6" |
| T1 | 24 | 4 | BNT. | 15'-6" |

| PIER 3 BAR LIST | | | | |
|-----------------------|-----|------|--------|----------|
| MARK | NO. | SIZE | SHAPE | LENGTH |
| (EPOXY COATED) | | | | |
| CV9 | 29 | 9 | STR. | 16'-9" |
| CV10 | 29 | 9 | STR. | 17'-2" |
| CV11 | 29 | 9 | STR. | 17'-7" |
| CV12 | 29 | 9 | STR. | 18'-1" |
| H1 | 9 | 10 | STR. | 90'-7" |
| H2 | 10 | 5 | STR. | 90'-10" |
| H3 | 9 | 10 | BNT. | 94'-4" |
| H4 | 10 | 4 | BNT. | 7'-0" |
| P1 | 54 | 4 | BNT. | 7'-2" |
| P3 | 45 | 4 | BNT. | 8'-2" |
| S1 | 64 | 6 | BNT. | 21'-0" |
| V1 | 8 | 4 | BNT. | 6'-6" |
| (NON-EPOXY COATED) | | | | |
| CS9 | 1 | W20 | SPIRAL | 542'-2" |
| CS10 | 1 | W20 | SPIRAL | 560'-0" |
| CS11 | 1 | W20 | SPIRAL | 577'-9" |
| CS12 | 1 | W20 | SPIRAL | 596'-0" |
| PIER 3 DRILLED SHAFTS | | | | |
| (NON-EPOXY COATED) | | | | |
| D1 | 116 | 9 | STR. | 11'-8" |
| DS3 | 4 | W20 | SPIRAL | 999'-10" |
| DV3 | 108 | 11 | STR. | 30'-6" |
| T1 | 24 | 4 | BNT. | 15'-6" |

- (1) LENGTH INCLUDES ONE (1) 6'-11" LAP SPLICE.
- (2) LENGTH INCLUDES TWO (2) 3'-7" LAP SPLICES.
- (3) LENGTH INCLUDES ONE (1) 7'-10" LAP SPLICE.
- (4) INCLUDED IN THE PRICE BID PER LIN. FT. OF DRILLED SHAFT.

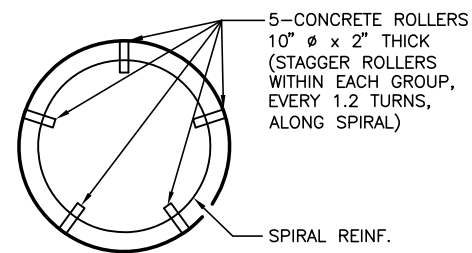


BAR BEND DETAILS



SPIRAL REINFORCING SPLICE DETAIL

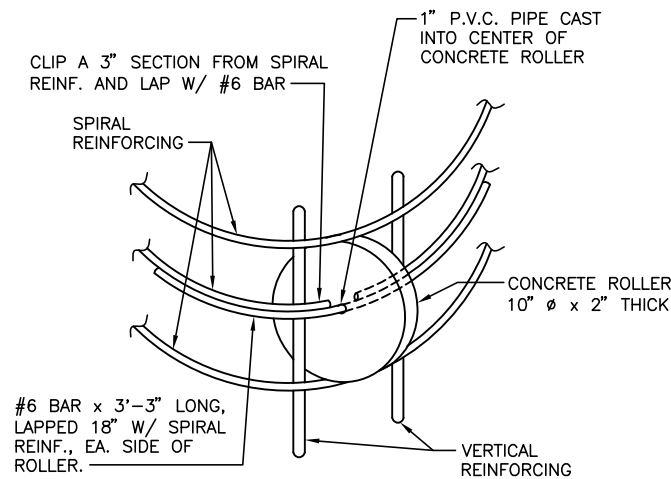
NOTE: SPIRAL BARS SHALL CONFORM TO AASHTO M32. SPIRAL BAR LENGTHS DO NOT INCLUDE LAPS. IF LAPS ARE REQUIRED, THE LENGTH OF THE LAP SHALL BE 2'-0", WITH 10" HOOKS.



ROLLER PLACEMENT DETAIL

NOTES: CONCRETE USED IN THE CONCRETE ROLLERS SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 4000 PSI.

PLASTIC ROLLERS MAY BE SUBSTITUTED FOR THE CONCRETE ROLLERS IN ACCORDANCE WITH THE SPECIAL PROVISIONS FOR DRILLED SHAFT FOUNDATIONS.



DETAIL OF ROLLER INSTALLATION

ALL MATERIALS, LABOR AND INCIDENTALS REQUIRED FOR THE INSTALLATION OF THE ROLLERS TO BE INCLUDED IN THE PRICE BID PER LF OF DRILLED SHAFTS.

| PIER QUANTITIES | | | | |
|--------------------------------------|-------|----------|----------|----------|
| ITEM | UNITS | PIER 1 | PIER 2 | PIER 3 |
| CLASS A CONCRETE | CY | 130.10 | 133.80 | 129.10 |
| REINFORCING STEEL | LB | 1590.00 | 1730.00 | 1550.00 |
| EPOXY COATED REINFORCING STEEL | LB | 17710.00 | 19130.00 | 17580.00 |
| WATER REPELLENT (VISUALLY INSPECTED) | SY | 239.00 | 247.00 | 238.00 |
| DRILLED SHAFTS 72" DIAMETER | LF | 136.00 | 132.00 | 124.00 |
| CROSSHOLE SONIC LOGGING | EA | 1.00 | 1.00 | 1.00 |

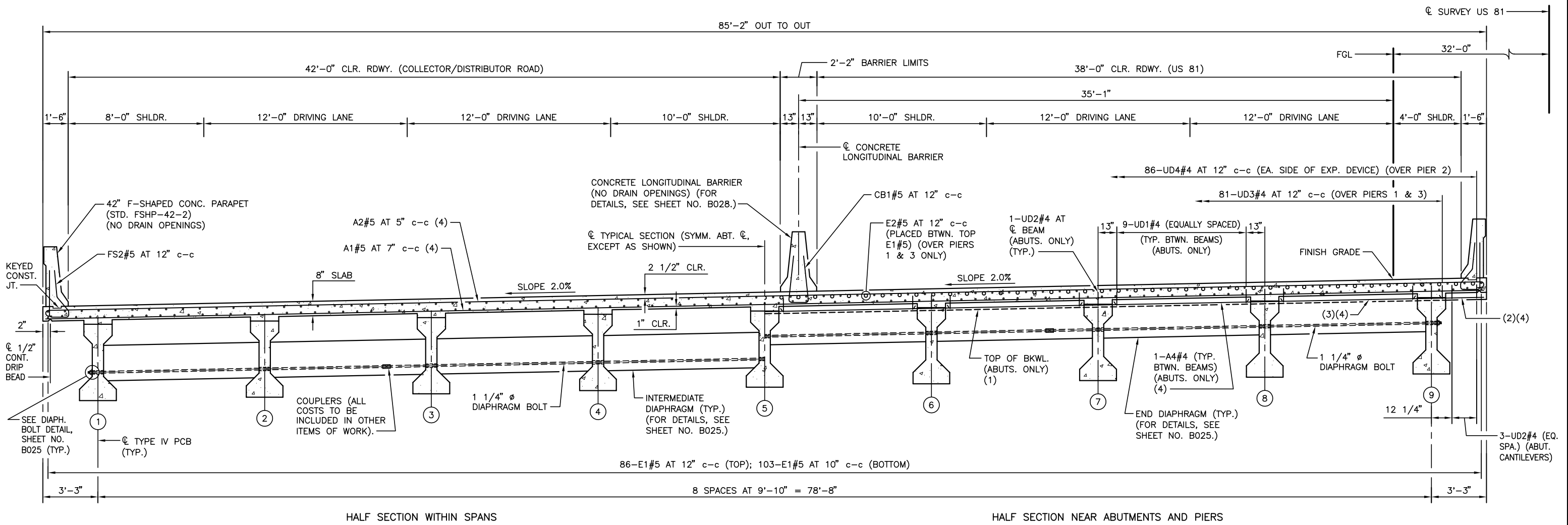
① ESTIMATED QUANTITY ONLY, TO BE USED AS DIRECTED BY THE ENGINEER.

| | | | |
|----------|-----------|--|----------------|
| Design | . | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW 03/19 | SB US-81 OVER I-44 | |
| Checked | . | MISCELLANEOUS PIER DETAILS (2 OF 2) | |
| Approved | . | Ⓢ STA. 196+50.65 | |
| Squad | | Job Piece No. 24428(04) | Sheet No. B019 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B020-2442804-TYPICAL_SECTION.dwg 3/2/2020 2:30:50 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

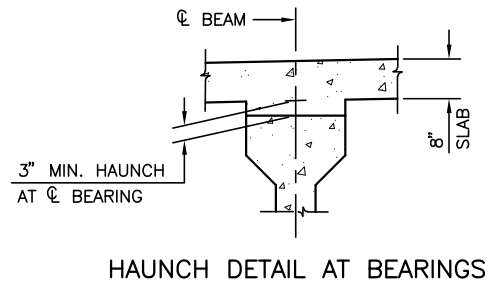
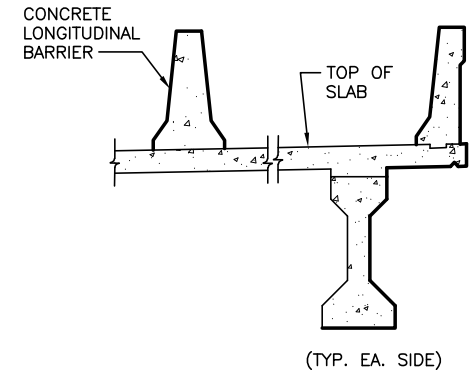


HALF SECTION WITHIN SPANS
 TYPICAL CROSS SECTION
 HALF SECTION NEAR ABUTMENTS AND PIERS

NOTE: DO NOT PLACE CONCRETE FOR THE BRIDGE SLAB OR APPLY OTHER MASSIVE LOADS TO THE BEAMS OR DIAPHRAGMS UNTIL THE CONCRETE IN THE DIAPHRAGMS HAS BEEN IN PLACE A MINIMUM OF 10 DAYS, OR AT THE DISCRETION OF THE ENGINEER. THE ENGINEER MAY APPROVE SHORTENED TIME IF THE BEAM AND DIAPHRAGM CONCRETE HAS ATTAINED 80% OF THE SPECIFIED COMPRESSIVE STRENGTH.

NOTE: ROTATE A2 BARS TO ENSURE MINIMUM CLEARANCE IS MET TOP & BOTTOM OF HOOK(S).

- (1) FOR SLAB FORMING REQUIREMENTS BEHIND END DIAPHRAGMS AND IN CANTILEVERS, SEE SHEET NOS. B021 AND B022.
- (2) 2-A3#4 (CONT. - ABUTS.);
 1-A3#4 (CONT. - EA. SIDE OF EXP. DEVICE, OVER PIER 2);
 1-A6#4 (CANTILEVERS ONLY, EA. SIDE OF EXP. DEVICE, OVER PIER 2)
- (3) 2-A5#4 (CONT. - OVER PIERS 1 AND 3)
- (4) ALL TRANSVERSE BARS SHALL BE PLACED PARALLEL TO THE SKEW ANGLE.



NOTE: PLAN QUANTITIES FOR CLASS "AA" CONCRETE INCLUDE 38.4 CY FOR BEAM HAUNCHES. HAUNCH HEIGHT SHOWN IS THE THEORETICAL HAUNCH HEIGHT AT THE CENTERLINE BEARING ONLY, MEASURED FROM THE BOTTOM OF THE SLAB TO THE TOP OF THE BEAM, AND VARIES ACROSS EACH SPAN. DETERMINE THE ACTUAL HAUNCH HEIGHT (ACCOUNTING FOR BEAM CAMBER, DEAD LOAD DEFLECTION AND ROADWAY GRADE) AFTER ERECTION OF THE BEAMS AND SUBMIT TO THE ENGINEER FOR APPROVAL. THE ENGINEER WILL NOT MEASURE DIFFERENCES BETWEEN THE THEORETICAL AND THE ACTUAL HAUNCH HEIGHTS FOR PAYMENT.

| SUPERSTRUCTURE QUANTITIES | | |
|--|-------|-----------|
| ITEM | UNITS | TOTAL |
| PRESTRESSED CONCRETE BEAMS (TYPE IV) | LF | 2742.00 |
| SAW-CUT GROOVING | SY | 2737.80 |
| SEALED EXPANSION JOINT | LF | 83.26 |
| 42" F-SHAPED PARAPET | LF | 615.60 |
| STRUCTURAL STEEL | LB | 4060.00 |
| STAINLESS STEEL FIXED BEARING ASSEMBLY | EA | 18.00 |
| STAINLESS STEEL EXPANSION BEARING ASSEMBLY | EA | 54.00 |
| CLASS AA CONCRETE | CY | 792.50 |
| EPOXY COATED REINFORCING STEEL | LB | 212630.00 |
| WATER REPELLENT (VISUALLY INSPECTED) | SY | 1544.00 |
| SEALER CRACK PREPARATION | LF | 161.00 |
| SEALER RESIN | GAL | 1.30 |

① QUANTITY SHOWN INCLUDES 48.9 C.Y. OF CLASS AA CONCRETE, 8310 LBS. OF EPOXY COATED REINFORCING STEEL OR 275 S.Y. OF WATER REPELLENT, AS APPLICABLE, FOR THE CONCRETE LONGITUDINAL BARRIER.

NOTES: FS2 BARS IN PARAPETS AND CB1 BARS IN THE CONCRETE LONGITUDINAL BARRIER SHALL BE TIED IN PLACE PRIOR TO SLAB POUR.
 FOR LONGITUDINAL SECTION, SEE SHEET NOS. B021 AND B022.
 FOR P.C. BEAM AND DIAPHRAGM LAYOUT PLAN, SEE SHEET NO. B024.
 FOR INTERMEDIATE AND END DIAPHRAGM DETAILS, SEE SHEET NO. B025.
 FOR SLAB REINFORCING PLANS, SEE SHEET NOS. B026 AND B027.
 FOR CONCRETE LONGITUDINAL BARRIER DETAILS, SEE SHEET NO. B028.
 FOR BRIDGE SLAB AND CONSTRUCTION JOINT NOTES, BAR BEND DETAILS AND SUPERSTRUCTURE BAR LIST, SEE SHEET NO. B029.
 FOR P.C. BEAM DETAILS AND DEAD LOAD DEFLECTION DIAGRAMS, SEE SHEET NOS. B031 AND B032.

WATER REPELLENT SURFACE TREATMENT

NOTE: SURFACES INDICATED WITH HEAVY LINES SHALL BE TREATED WITH A PENETRATING WATER REPELLENT SURFACE TREATMENT.

| | | | |
|----------|-----------|-------------------------|----------------|
| Design | . | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW 01/19 | SB US-81 OVER I-44 | |
| Checked | . | | |
| Approved | . | | |
| Squad | | Job Piece No. 24428(04) | Sheet No. B020 |

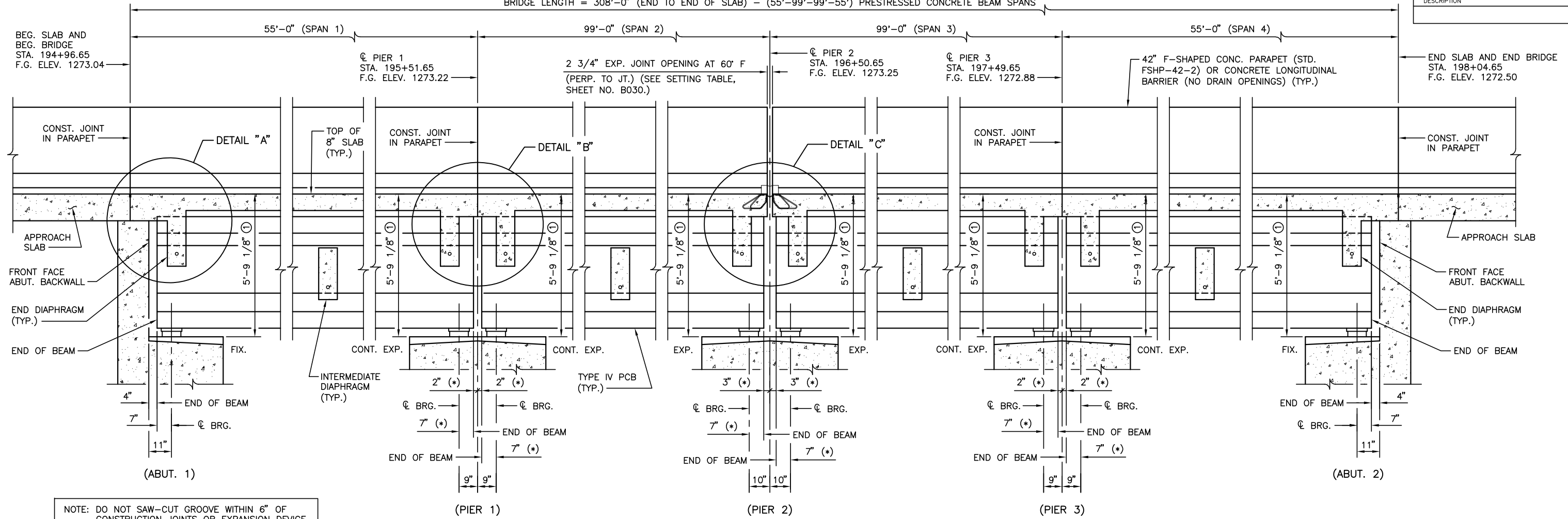
TYPICAL SECTION
 @ STA. 196+50.65

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B021-2442804-LONGITUDINAL SECTION 1.dwg 3/2/2020 2:32:33 PM

BRIDGE LENGTH = 308'-0" (END TO END OF SLAB) - (55'-99'-99'-55') PRESTRESSED CONCRETE BEAM SPANS

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



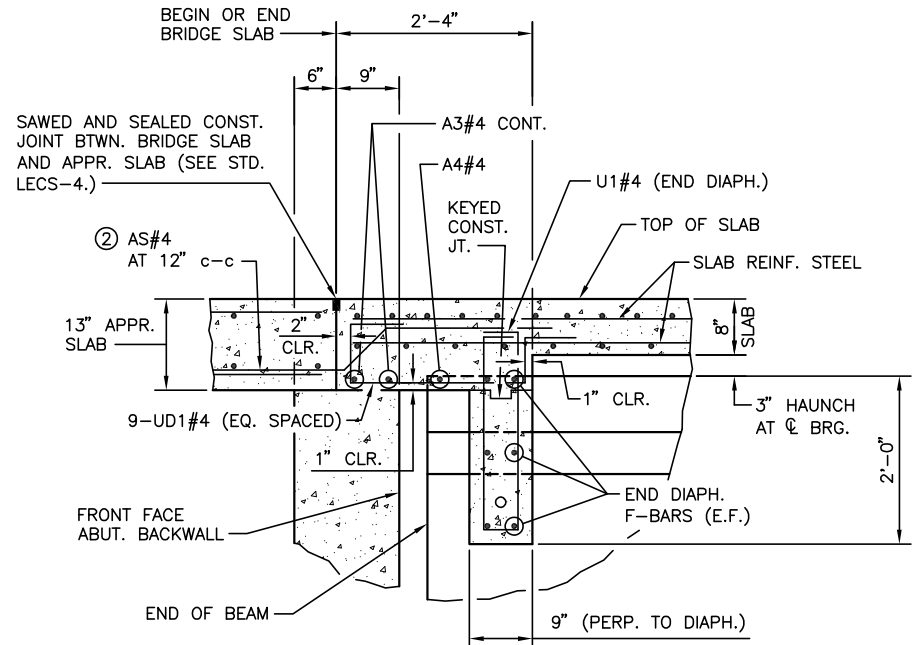
LONGITUDINAL SECTION

(* AT 60' F ONLY.)

- ① DIMENSION IS FROM TOP OF SLAB TO BOTTOM OF BEARING ASSEMBLY AT ϕ BEARING.
- ② AS#4 BARS SHALL BE TIED TO THE TOP MAT OF REINFORCING IN THE BRIDGE SLAB AND THE BOTTOM MAT OF REINFORCING IN THE APPROACH SLAB. THE BOTTOM LEG OF THE AS#4 BAR SHALL CROSS JOINT.

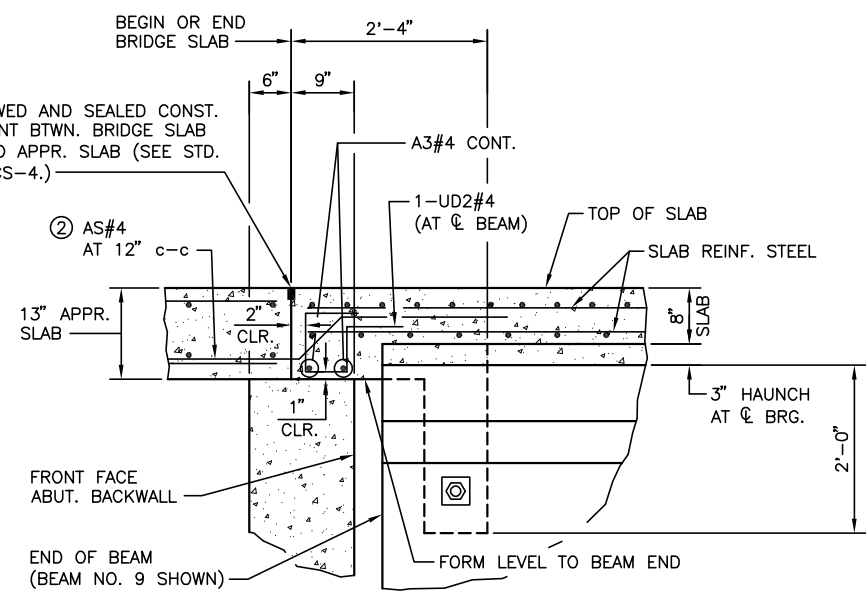
NOTE: DO NOT SAW-CUT GROOVE WITHIN 6" OF CONSTRUCTION JOINTS OR EXPANSION DEVICE.

- NOTES:
- THE WORKING LINE FOR BRIDGE "F" IS THE FGL (32' LT. OF ϕ SURVEY US 81). STATIONING FOLLOWS THE ϕ SURVEY US 81.
 - ALL LONGITUDINAL DIMENSIONS ARE HORIZONTAL DIMENSIONS.
 - ALL UD BARS SHALL BE PLACED PARALLEL TO THE P.C. BEAMS.
 - DETAILS "A" ARE SHOWN AND DIMENSIONED PARALLEL TO THE FGL, UNLESS OTHERWISE NOTED.
 - FOR DETAILS "B" AND "C", SEE SHEET NO. B022.
 - FOR CONTROL CRACK JOINT LOCATIONS IN PARAPETS AND THE CONCRETE LONGITUDINAL BARRIER (NOT SHOWN), SEE STD. FSHP-42-2 AND SHEET NO. B028.
 - FOR P.C. BEAM AND DIAPHRAGM LAYOUT PLAN, SEE SHEET NO. B024.
 - FOR INTERMEDIATE AND END DIAPHRAGM DETAILS, SEE SHEET NO. B025.
 - FOR SLAB REINFORCING PLANS, SEE SHEET NOS. B026 AND B027.
 - FOR BRIDGE SLAB AND CONSTRUCTION JOINT NOTES, BAR BEND DETAILS AND SUPERSTRUCTURE BAR LIST, SEE SHEET NO. B029.
 - FOR EXPANSION JOINT DETAILS, SEE SHEET NO. B030.



DETAIL "A" (BTWN. BEAMS)
(TYP. AT ABUTS.)

E.F. - DENOTES EACH FACE



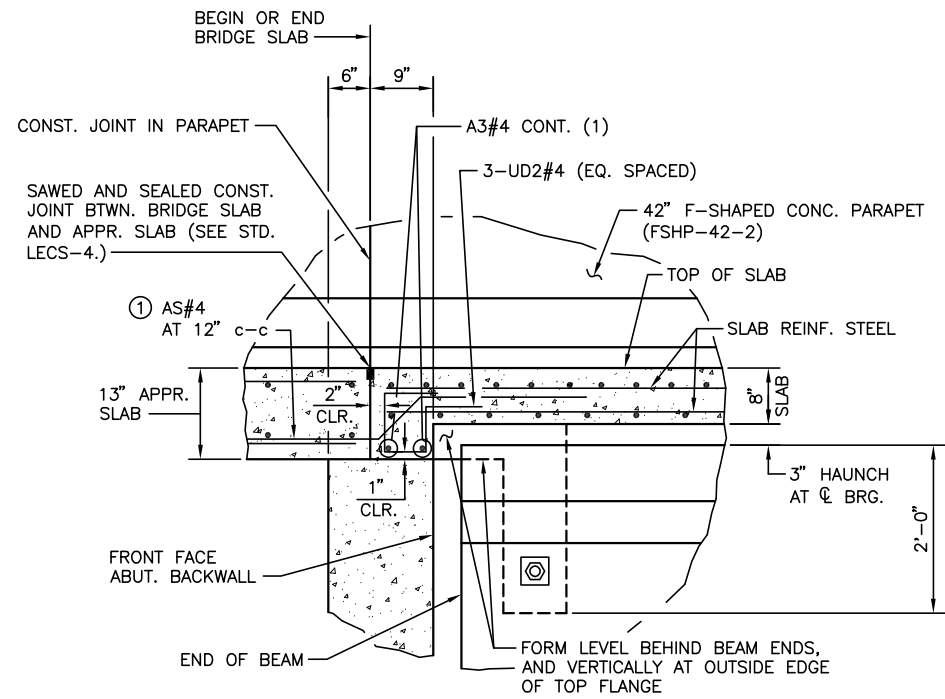
DETAIL "A" (AT BEAMS)
(TYP. AT ABUTS.)

| | | | |
|----------|-----------|-------------------------|----------------|
| Design | . | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW 01/19 | SB US-81 OVER I-44 | |
| Checked | . | | |
| Approved | . | | |
| Squad | | Job Piece No. 24428(04) | Sheet No. B021 |

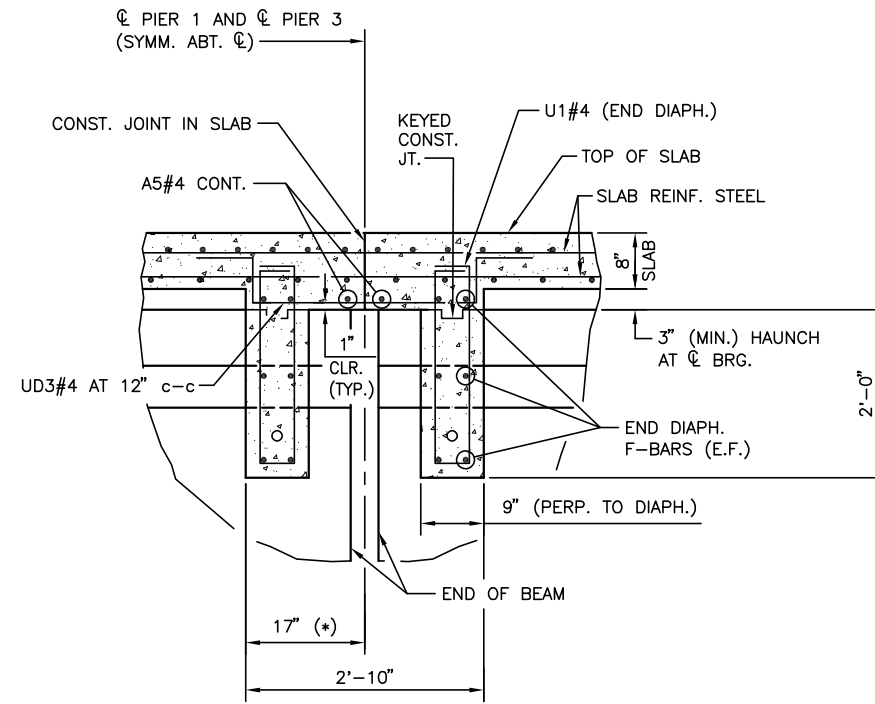
"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B022-2442804-LONGITUDINAL SECTION 2.dwg 3/4/2020 3:20:37 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



DETAIL "A" (AT CANTILEVERS)
(TYP. AT ABUTS.)



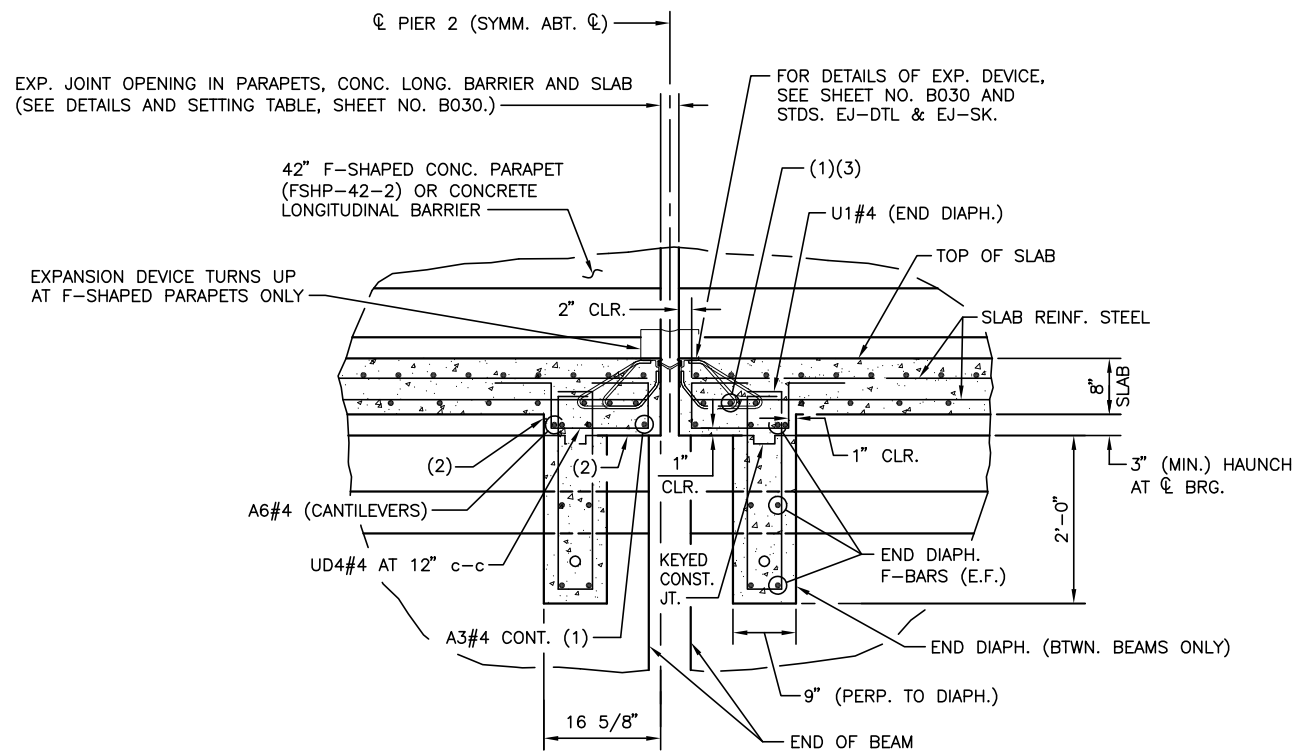
DETAIL "B"
(DETAIL "B" EXTENDS OUT-OUT OF EXTERIOR BEAM TOP FLANGES.)
(SLAB THICKENING NOT REQUIRED IN CANTILEVERS.)
(TYP. AT PIERS 1 & 3)

① AS#4 BARS SHALL BE TIED TO THE TOP MAT OF REINFORCING IN THE BRIDGE SLAB AND THE BOTTOM MAT OF REINFORCING IN THE APPROACH SLAB. THE BOTTOM LEG OF THE AS#4 BAR SHALL CROSS JOINT.

(* AT 60' F ONLY.)

- (1) FIELD BEND A3 AND EJ1 BARS TO FIT SLAB GEOMETRY IN CANTILEVERS.
- (2) FORM SLAB IN CANTILEVERS TO THESE LINES TO PROVIDE ADEQUATE CLEARANCE TO W1 & W2 EXPANSION DEVICE ANCHOR BARS.
- (3) EJ1#5 W-BAR ANCHOR. USE BOTTOM TRANSVERSE A1 BARS TO ANCHOR W-BARS EITHER SIDE.

E.F. - DENOTES EACH FACE



DETAIL "C"
(FULL WIDTH OF BRIDGE SLAB, UNLESS NOTED.)
(AT PIER 2)

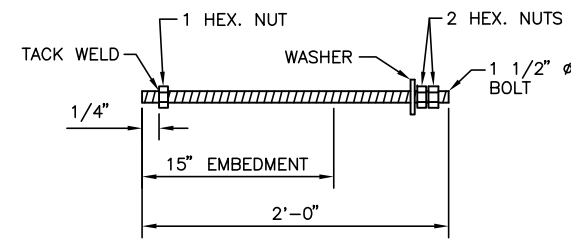
NOTES: ALL LONGITUDINAL DIMENSIONS ARE HORIZONTAL DIMENSIONS.
ALL UD BARS SHALL BE PLACED PARALLEL TO THE P.C. BEAMS.
DETAILS "A", "B" AND "C" ARE SHOWN AND DIMENSIONED PARALLEL TO THE FGL, UNLESS OTHERWISE NOTED.
FOR SLAB REINFORCING PLANS, SEE SHEET NOS. B026 AND B027.
FOR P.C. BEAM AND DIAPHRAGM LAYOUT PLAN, SEE SHEET NO. B024.
FOR DIAPHRAGM DETAILS, SEE SHEET NO. B025.
FOR BRIDGE SLAB AND CONSTRUCTION JOINT NOTES, BAR BEND DETAILS AND SUPERSTRUCTURE BAR LIST, SEE SHEET NO. B029.
FOR EXPANSION JOINT DETAILS, SEE SHEET NO. B030.

| | | | | |
|----------|-----|-------|--------------------------------------|----------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 01/19 | SB US-81 OVER I-44 | |
| Checked | . | | LONGITUDINAL SECTION (2 OF 2) | |
| Approved | . | | CL STA. 196+50.65 | |
| Squad | | | Job Piece No. 24428(04) | Sheet No. B022 |

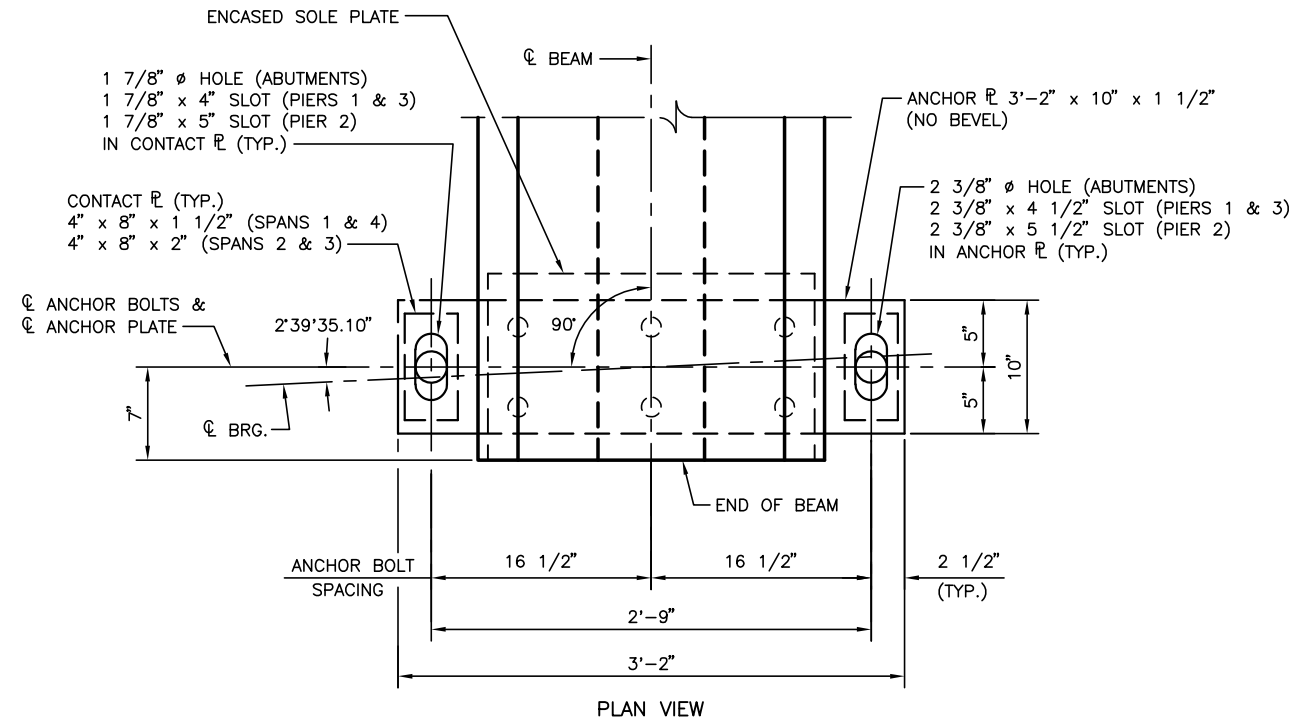
"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B023-2442804-BEARING DETAILS.dwg 3/2/2020 2:35:05 PM

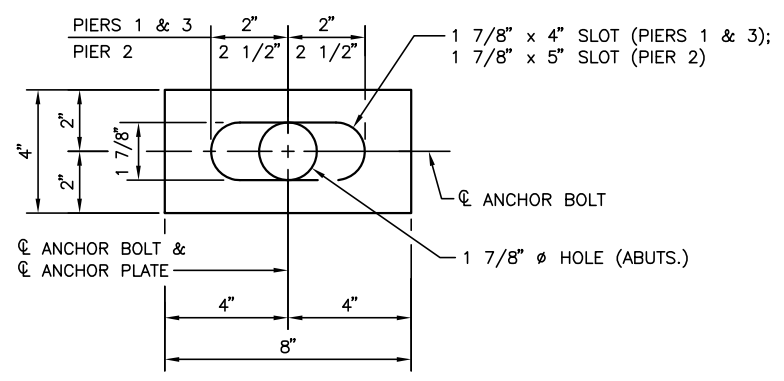
| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



ANCHOR BOLT DETAIL



PLAN VIEW



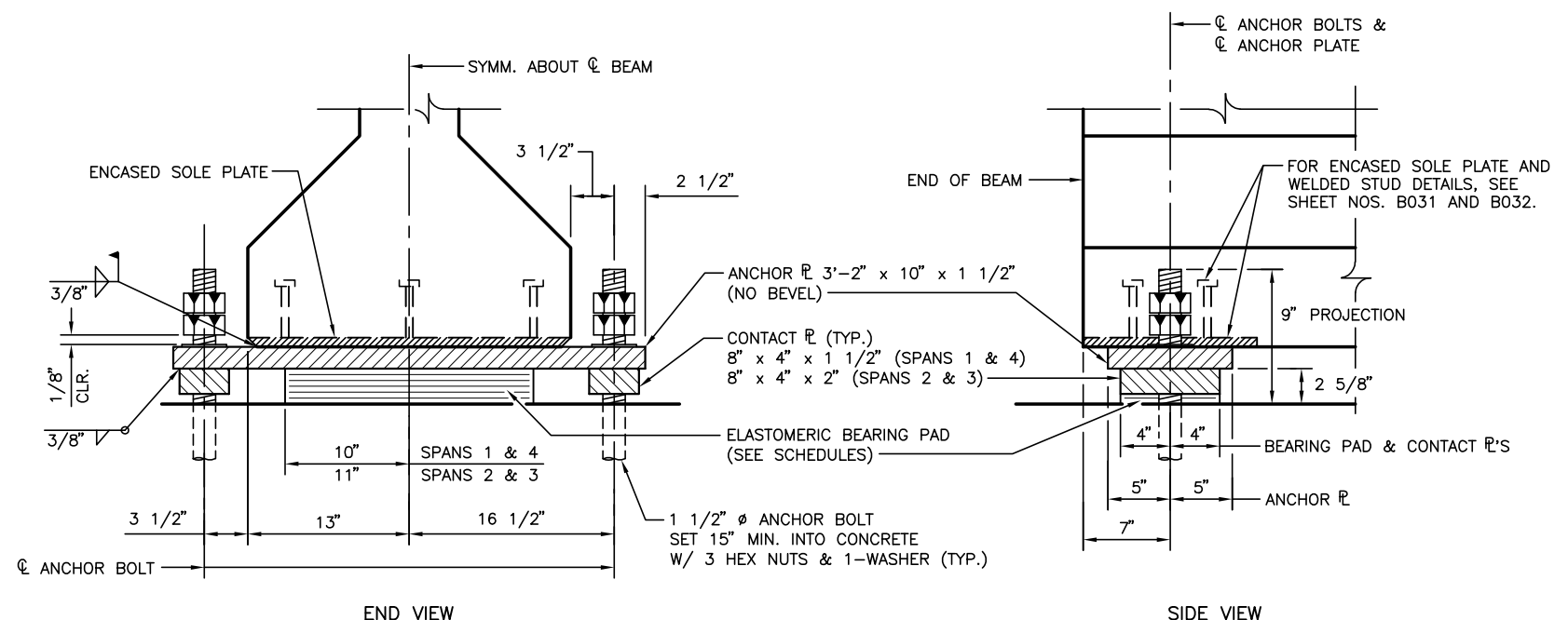
DETAIL OF CONTACT PLATES
 1 1/2" THICK - SPANS 1 & 4
 2" THICK - SPANS 2 & 3

BEARING PAD SCHEDULE FOR SPANS 1 & 4 (DURO 60) (36 REQ'D)

SIZE - 8" x 20" x 2 5/8" WITH
 5 - 1/8" STEEL LAMINATES
 2 - 1/4" EXTERIOR ELASTOMER LAYERS
 4 - 3/8" INTERIOR ELASTOMER LAYERS

BEARING PAD SCHEDULE FOR SPANS 2 & 3 (DURO 60) (36 REQ'D)

SIZE - 8" x 22" x 2 5/8" WITH
 5 - 1/8" STEEL LAMINATES
 2 - 1/4" EXTERIOR ELASTOMER LAYERS
 4 - 3/8" INTERIOR ELASTOMER LAYERS



BEARING ASSEMBLY DETAILS

NOTES: BEARING ASSEMBLIES SHALL BE IN ACCORDANCE WITH SECTION 507 OF THE STANDARD SPECIFICATIONS, AND PER THE ANSI/AWS D1.6 STRUCTURAL WELDING CODE FOR STAINLESS STEEL.

PROVIDE STRUCTURAL STEEL FOR ANCHOR PLATES AND CONTACT PLATES IN ACCORDANCE WITH ASTM A240 (AUSTENITIC STAINLESS STEEL, TYPE 316, CHARPY V-NOTCH TESTING NOT REQUIRED). FOR ANCHOR BOLTS, PROVIDE CONTINUOUSLY THREADED BARS IN ACCORDANCE WITH ASTM A320, CLASS 2, GRADE 88M (AUSTENITIC STAINLESS STEEL, TYPE 316, CHARPY V-NOTCH TESTING NOT REQUIRED). USE AUSTENITIC STAINLESS STEEL NUTS AND WASHERS CONFORMING TO ASTM A194, GRADE 8M AND ASTM A320, RESPECTIVELY. PERFORM ALL WELDING CONSISTENT WITH PROCEDURES FOR STAINLESS STEEL.

ANCHOR PLATES FOR BEARING ASSEMBLIES SHALL BE MATCH-MARKED, SHIPPED LOOSE, AND FIELD WELDED TO THE ENCASED SOLE PLATES IN ORDER THAT MINOR HORIZONTAL ADJUSTMENT OF THE BEARING LOCATION MAY BE MADE IF NECESSARY.

METAL USED IN FIELD WELDS WILL NOT BE MEASURED FOR PAYMENT.

ALL BEARING PADS SHALL BE CENTERED BETWEEN ANCHOR BOLTS.

BONDING BETWEEN THE ANCHOR PLATES AND BEARING PADS WILL NOT BE REQUIRED.

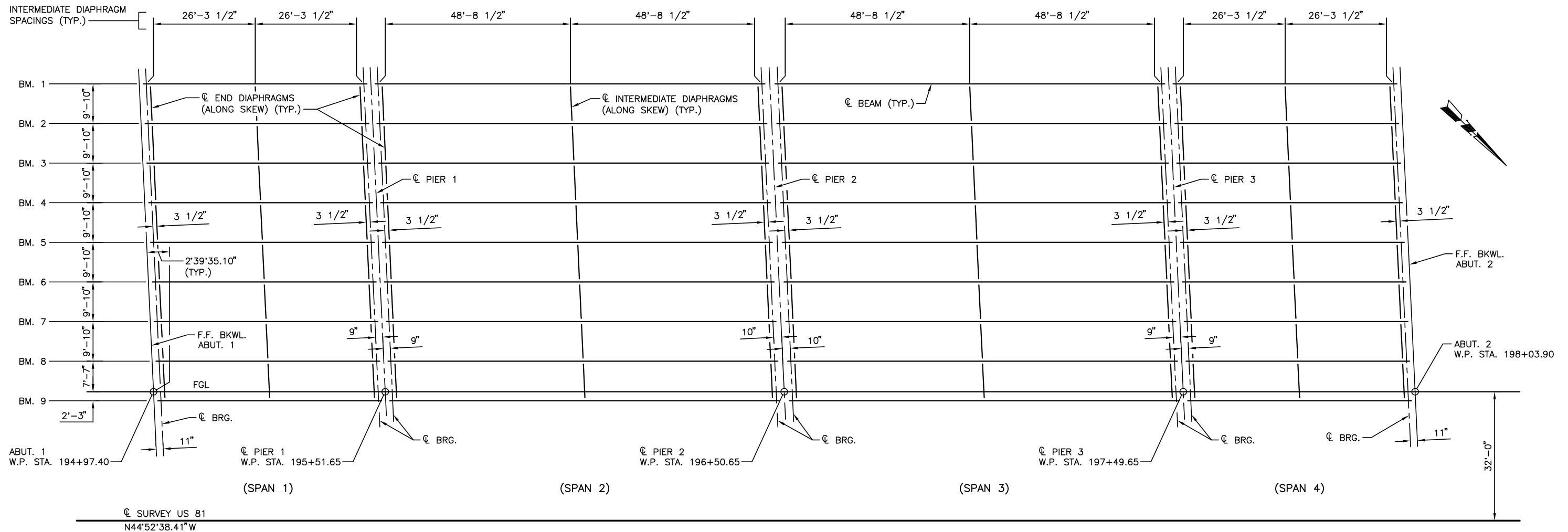
AT THE TIME OF SETTING SLOTTED BEARING ASSEMBLIES (PIERS), THE SLOTS IN THE ANCHOR PLATES SHALL BE CENTERED ON THE ANCHOR BOLTS. THE DIMENSION FROM THE END OF THE BEAM TO THE CENTERLINE OF THE ANCHOR BOLTS MAY VARY FROM THAT SHOWN, DEPENDING ON THE SETTING TEMPERATURE.

| | | | |
|----------|-----------|-------------------------|----------------|
| Design | . | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW 02/19 | SB US-81 OVER I-44 | |
| Checked | . | | |
| Approved | . | | |
| Squad | | Job Piece No. 24428(04) | Sheet No. B023 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & C\B024-2442804-PC BEAM AND DIAPHRAGM LAYOUT PLAN.dwg 3/2/2020 2:35:38 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



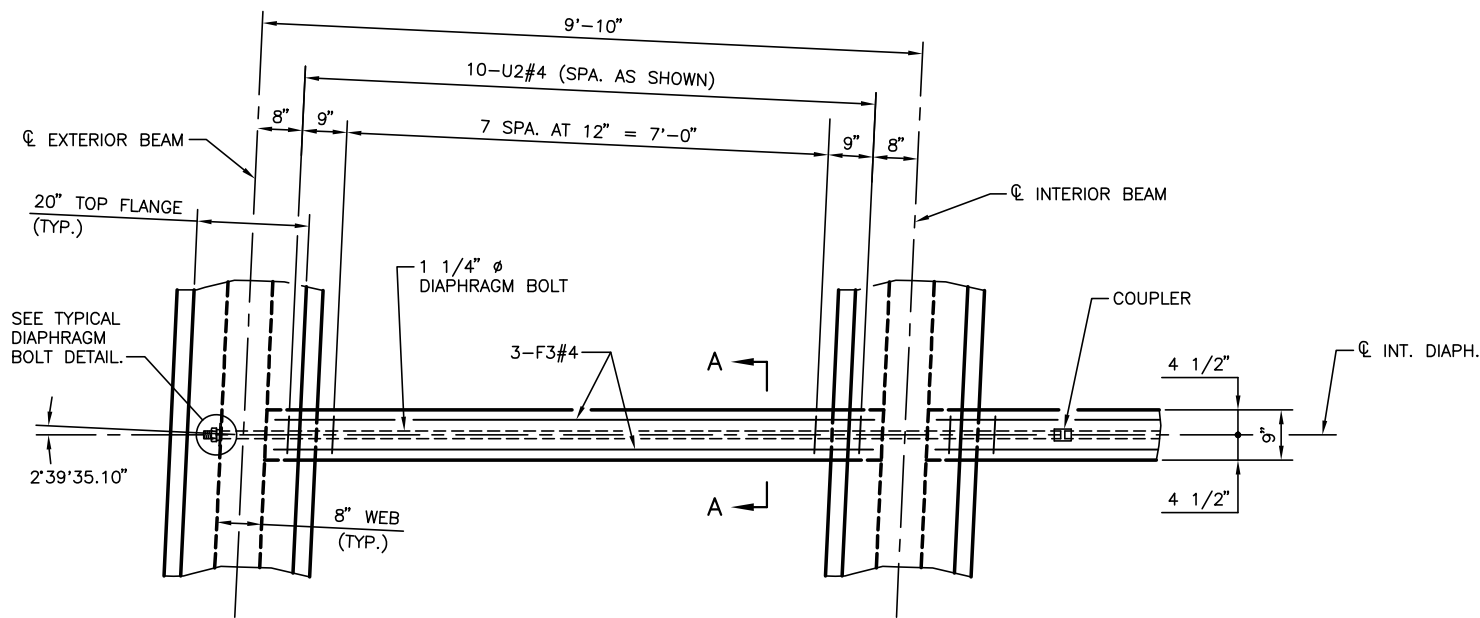
P.C. BEAM AND DIAPHRAGM LAYOUT PLAN
(ALL DIMENSIONS ARE HORIZONTAL DIMENSIONS.)

NOTE: FOR INTERMEDIATE AND END DIAPHRAGM DETAILS, SEE SHEET NO. B025.

| | | | |
|----------|-----------|--|----------------|
| Design | . | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW 01/19 | SB US-81 OVER I-44 | |
| Checked | . | P.C. BEAM AND DIAPHRAGM LAYOUT PLAN | |
| Approved | . | ☉ STA. 196+50.65 | |
| Squad | | Job Piece No. 24428(04) | Sheet No. B024 |

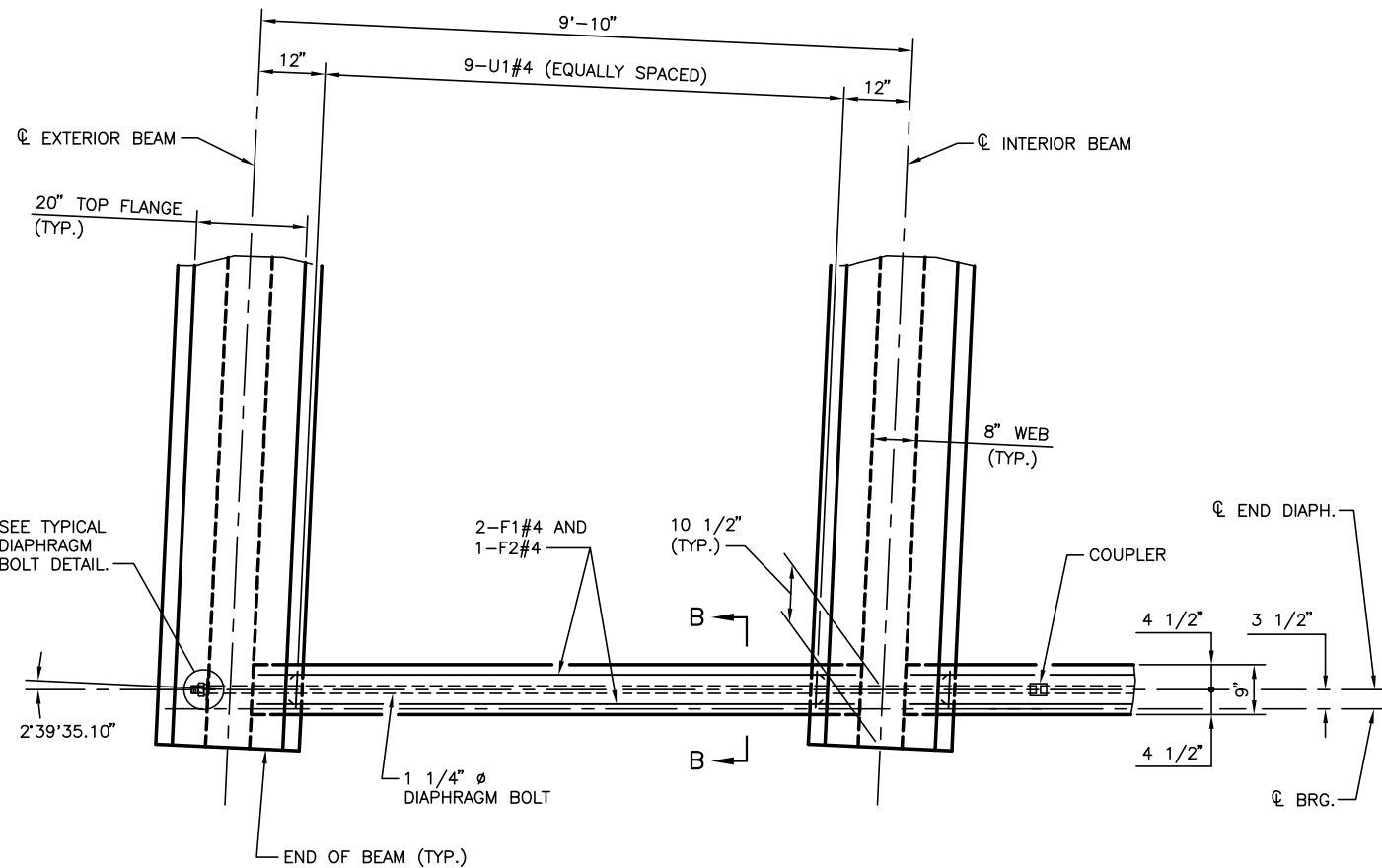
"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B025-2442804-DIAPHRAGM DETAILS.dwg 3/2/2020 2:36:28 PM

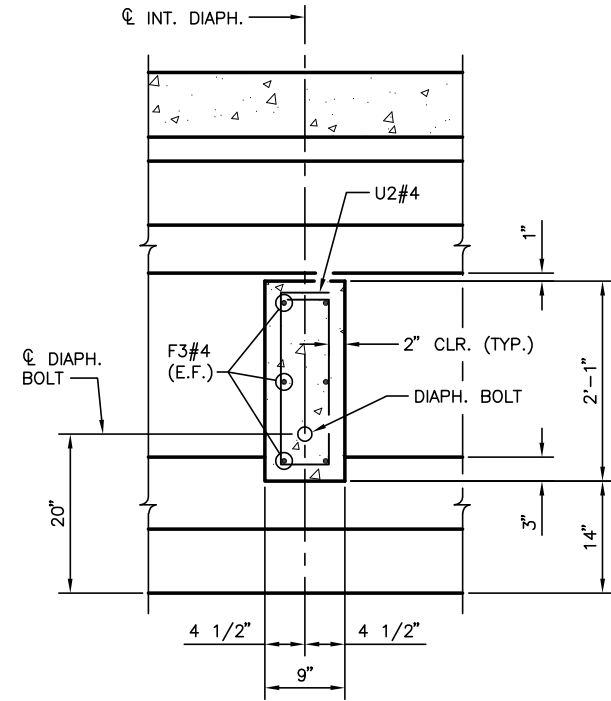


PART PLAN OF INTERMEDIATE DIAPHRAGM
(TYPICAL BTWN. BEAMS.)

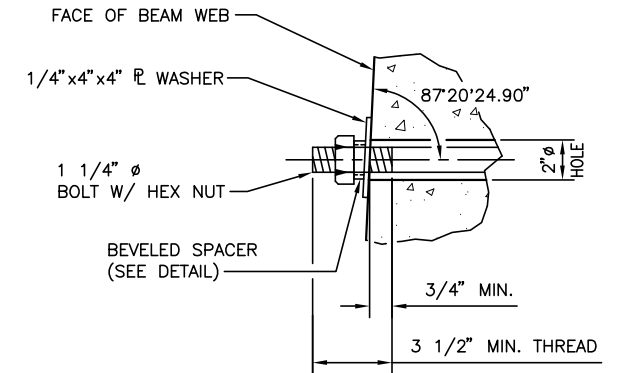
E.F. - DENOTES EACH FACE



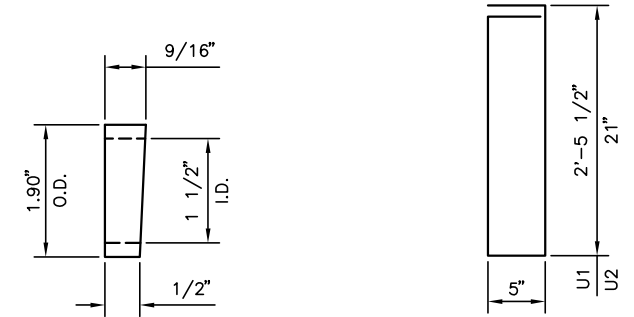
PART PLAN OF END DIAPHRAGM
(TYPICAL BTWN. BEAMS, AT ABUTMENTS AND PIERS.)



SECTION A-A

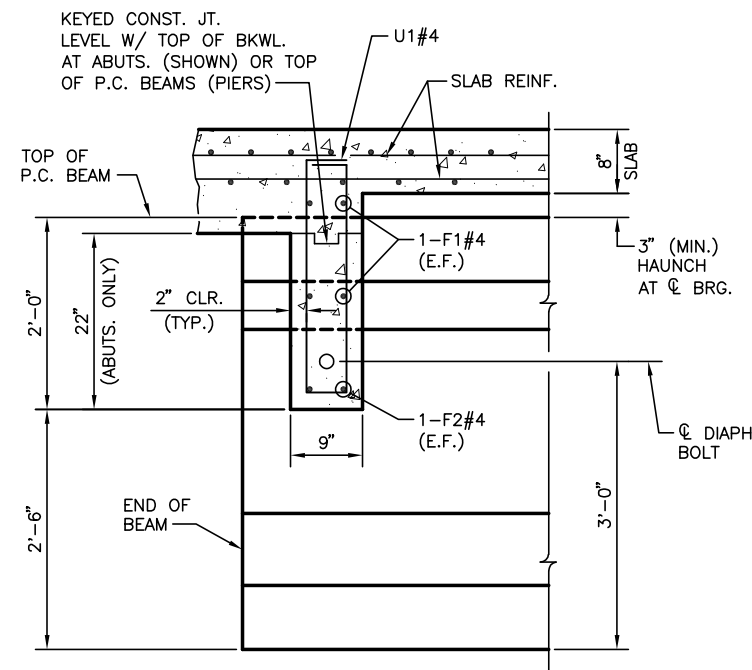


TYPICAL DIAPHRAGM BOLT DETAIL
(EXTERIOR BEAMS ONLY)



BEVELED SPACER
1 1/2" Ø EXTRA STRONG PIPE SLEEVE

U1#4 x 6'-2"
U2#4 x 4'-9"
(USE STIRRUP BEND DIAMETERS)



SECTION B-B

NOTES: STRUCTURAL STEEL FOR DIAPHRAGM BOLTS, SPACERS AND PLATE WASHERS SHALL CONFORM TO AASHTO M270 (ASTM A709), GRADE 50W (WEATHERING STEEL, CHARPY V-NOTCH TESTING NOT REQUIRED). A #10 REINFORCING BAR CONFORMING TO AASHTO M31, GRADE 60, AND THREADED AT THE ENDS AS SHOWN MAY BE SUBSTITUTED FOR THE DIAPHRAGM BOLT AT NO ADDITIONAL COST TO THE DEPARTMENT. HEX NUTS SHALL CONFORM TO AASHTO M291 (ASTM A563).

PAINT EXPOSED DIAPHRAGM BOLTS, HEX NUTS, SPACERS AND PLATE WASHERS WITH TWO (2) COATS OF ZINC RICH PAINT (6 MIL. MIN. THICKNESS) AFTER ASSEMBLY. ALL COSTS FOR DIAPHRAGM BOLTS, PLATE WASHERS, SPACERS AND HEX NUTS TO BE INCLUDED IN PRICE BID FOR STRUCTURAL STEEL.

ALL COSTS FOR DIAPHRAGM BOLT COUPLERS TO BE INCLUDED IN OTHER ITEMS.

FOR P.C. BEAM AND DIAPHRAGM LAYOUT PLAN, SEE SHEET NO. B024.

U1 AND U2 BARS SHALL BE PLACED PARALLEL TO THE P.C. BEAMS.

FOR ADDITIONAL REINFORCING DETAILS OVER END DIAPHRAGMS, SEE SHEET NOS. B020 THRU B022.

FOR SUPERSTRUCTURE BAR LIST, SEE SHEET NO. B029.

CONCRETE FOR ALL INTERMEDIATE AND END DIAPHRAGMS SHALL BE CLASS AA CONCRETE.

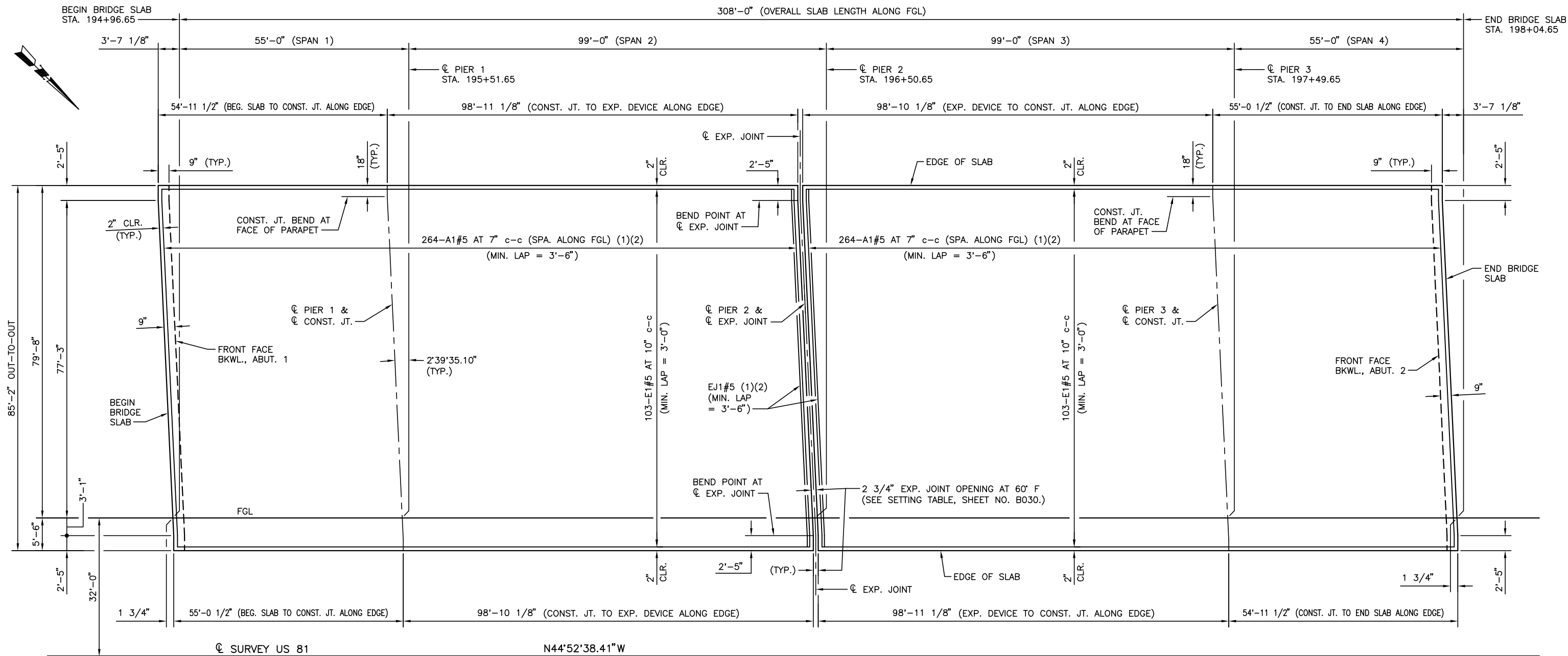
| Design | PKW | 02/19 | BRIDGE "F" SB US-81 OVER I-44 | GRADY COUNTY |
|----------|-----|-------|----------------------------------|----------------|
| Drawn | PKW | 02/19 | | |
| Checked | | | | |
| Approved | | | | |
| Squad | | | Job Piece No. 24428(04) | Sheet No. B025 |

DIAPHRAGM DETAILS
CL STA. 196+50.65

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B026-2442804-BOTTOM SLAB REINFORCING PLAN.dwg 3/2/2020 2:39:54 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



BOTTOM SLAB REINFORCING PLAN
(NOT TO SCALE)

NOTES: ALL STATIONING FOLLOWS ϕ SURVEY US 81.
ALL PLAN DIMENSIONS ARE HORIZONTAL DIMENSIONS.
ALL LAPS SHALL BE STAGGERED.

E1 BARS SHALL BE CONTINUOUS ACROSS THE CONSTRUCTION JOINTS OVER PIERS 1 AND 3. E1 BAR LAPS SHALL NOT BE MADE WITHIN 10' OF THE ϕ PIERS 1 AND 3.

UD1 THRU UD4 AND A3 THRU A6 BARS IN ABUTMENT AND PIER CONCRETE END DIAPHRAGMS ARE NOT SHOWN FOR CLARITY. SEE DETAILS "A", "B" AND "C" ON SHEET NOS. B021 AND B022 FOR PLACEMENT DETAILS. THIS STEEL SHALL BE TIED IN PLACE WITH AND BELOW THE BOTTOM MAT OF SLAB REINFORCING STEEL.

FS2 BARS IN PARAPETS AND CB1 BARS IN THE CONCRETE LONGITUDINAL BARRIER SHALL BE TIED IN PLACE PRIOR TO SLAB POUR. FOR DETAILS AND BAR SPACINGS, SEE STD. FSHP-42-2, AND SHEET NOS. B027 AND B028.

FOR EXPANSION JOINT DETAILS, SEE SHEET NO. B030, AND STDS. EJ-SK AND EJ-DTL. W1 AND W2 EXPANSION JOINT ANCHOR BARS SHALL BE ANCHORED WITH THE A1 AND EJ1 BARS AS SHOWN ON SHEET NO. B022.

FOR BRIDGE SLAB AND CONSTRUCTION JOINT NOTES, BAR BEND DETAILS AND SUPERSTRUCTURE BAR LIST, SEE SHEET NO. B029.

FOR TYPICAL SECTION AND LONGITUDINAL SECTION, SEE SHEET NOS. B020-B022.

FOR TOP SLAB REINFORCING PLAN AND PARAPET LAYOUT, SEE SHEET NO. B027.

FOR CONCRETE LONGITUDINAL BARRIER DETAILS, SEE SHEET NO. B028.

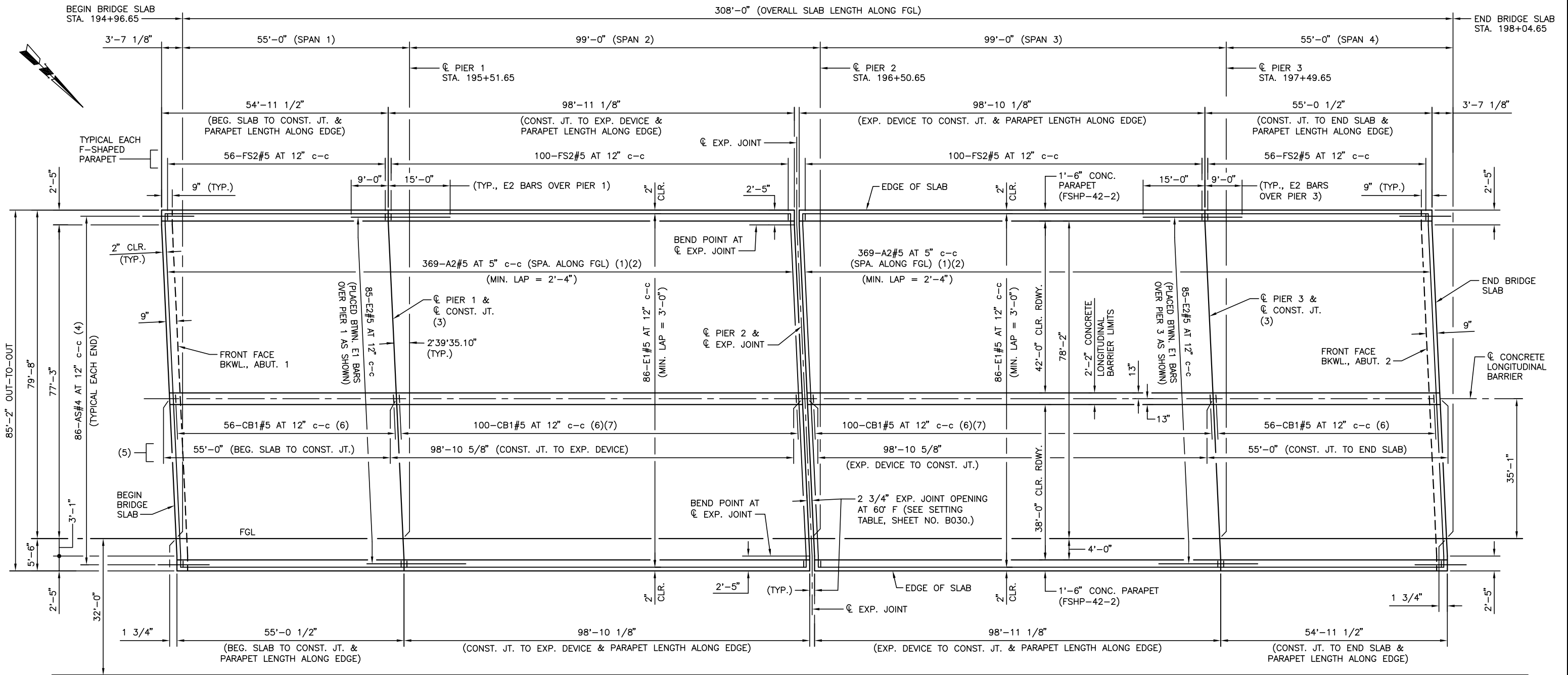
- (1) A1 AND EJ1 BARS SHALL BE PLACED PARALLEL TO THE SKEW.
- (2) FIELD BEND FIRST AND LAST A1 BARS, AND EJ1 BARS, TO FIT SLAB GEOMETRY IN CANTILEVERS.

| | | | | |
|----------|-----|-------|-------------------------------------|----------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 01/19 | SB US-81 OVER I-44 | |
| Checked | . | | BOTTOM SLAB REINFORCING PLAN | |
| Approved | . | | ϕ STA. 196+50.65 | |
| Squad | | | Job Piece No. 24428(04) | Sheet No. B026 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B027-2442804-TOP SLAB REINFORCING PLAN AND PARAPET LAYOUT.dwg 3/2/2020 2:41:38 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



☉ SURVEY US 81
N44°52'38.41"W

TOP SLAB REINFORCING PLAN AND PARAPET LAYOUT
(NOT TO SCALE)

- (1) A2 BARS SHALL BE PLACED PARALLEL TO THE SKEW.
- (2) FIELD BEND FIRST AND LAST A2 BARS TO FIT SLAB GEOMETRY IN CANTILEVERS.
- (3) SEAL CONSTRUCTION JOINT WITH HIGH MOLECULAR WEIGHT METHACRYLATE OR EPOXY RESIN.
- (4) SEE DETAILS "A", SHEET NOS. B021 AND B022.
- (5) CONCRETE LONGITUDINAL BARRIER LAYOUT
- (6) PLACE FIRST AND LAST CB1 BAR ALONG SKEW AS SHOWN.
- (7) POSITION CB1 BAR(S) NEAR PIER 2 TO CLEAR THE EXPANSION DEVICE STEEL RECEPTORS.

NOTES: ALL STATIONING FOLLOWS ☉ SURVEY US 81.
ALL PLAN DIMENSIONS ARE HORIZONTAL DIMENSIONS.
ALL LAPS SHALL BE STAGGERED.
E1 BARS SHALL BE CONTINUOUS ACROSS THE CONSTRUCTION JOINTS OVER PIERS 1 AND 3. E1 BAR LAPS SHALL NOT BE MADE WITHIN 10' OF THE ☉ PIERS 1 AND 3.
FS2 BARS IN PARAPETS AND CB1 BARS IN THE CONCRETE LONGITUDINAL BARRIER SHALL BE TIED IN PLACE PRIOR TO SLAB POUR. FOR ADDITIONAL DETAILS, SEE STD. FSHP-42-2 AND SHEET NO. B028.

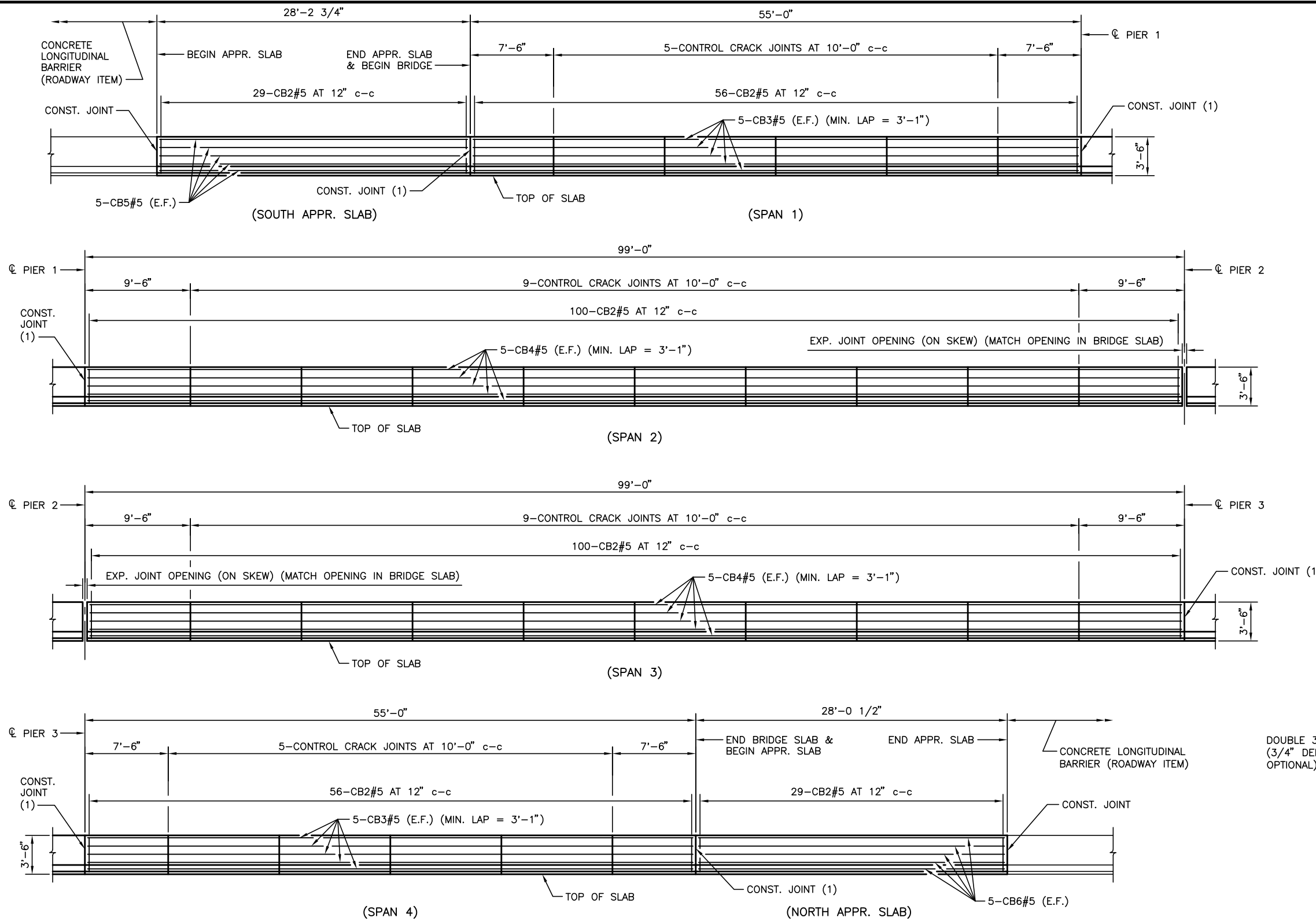
FOR CONTROL CRACK JOINT LOCATIONS IN PARAPETS AND THE CONCRETE LONGITUDINAL BARRIER, SEE STD. FSHP-42-2 AND SHEET NO. B028.
FOR EXPANSION JOINT DETAILS, SEE SHEET NO. B030, AND STDS. EJ-SK AND EJ-DTL.
FOR BRIDGE SLAB AND CONSTRUCTION JOINT NOTES, BAR BEND DETAILS AND SUPERSTRUCTURE BAR LIST, SEE SHEET NO. B029.
FOR TYPICAL SECTION AND LONGITUDINAL SECTION, SEE SHEET NOS. B020-B022.
FOR CONCRETE LONGITUDINAL BARRIER DETAILS, SEE SHEET NO. B028.

| | | | | |
|----------|-----|-------|---|----------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 01/19 | SB US-81 OVER I-44 | |
| Checked | . | | TOP SLAB REINFORCING PLAN AND PARAPET LAYOUT | |
| Approved | . | | ☉ STA. 196+50.65 | |
| Squad | | | Job Piece No. 24428(04) | Sheet No. B027 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & C\B028-2442804-CONCRETE LONGITUDINAL BARRIER DETAILS.dwg 3/2/2020 2:42:19 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



ELEVATION OF CONCRETE LONGITUDINAL BARRIER

NOTE: DIMENSIONS ARE ALONG CL CONCRETE LONGITUDINAL BARRIER.

(1) CONSTRUCTION JOINT SHALL BE MADE PARALLEL TO SKEW.

NOTES: ALL LONGITUDINAL DIMENSIONS ARE HORIZONTAL DIMENSIONS.

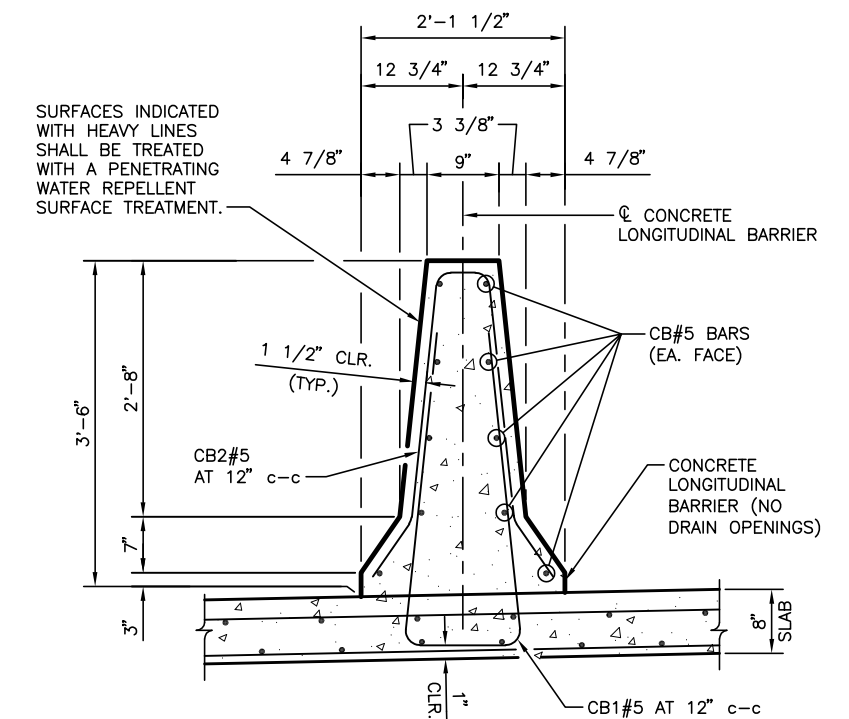
CB1 BARS IN BRIDGE AND APPROACH SLABS SHALL BE TIED IN PLACE PRIOR TO SLAB POURS. FOR LAYOUT OF CB1 BARS, SEE SHEET NOS. B027 AND B034.

FOR PLAN LAYOUT OF CONCRETE LONGITUDINAL BARRIER, SEE SHEET NOS. B027 AND B033.

FOR CONCRETE LONGITUDINAL BARRIER QUANTITIES (ON SUPERSTRUCTURE), SEE SHEET NO. B020.

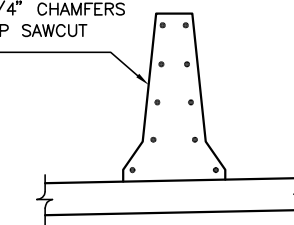
FOR BAR BEND DETAILS AND SUPERSTRUCTURE BAR LIST, SEE SHEET NO. B029.

FOR CONCRETE LONGITUDINAL BARRIER QUANTITIES AND BAR LIST (ON APPROACH SLABS), SEE SHEET NO. B035.



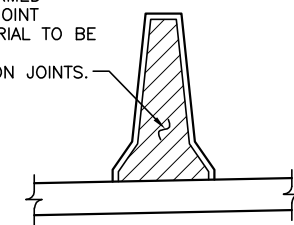
TYPICAL SECTION THRU CONCRETE LONGITUDINAL BARRIER

DOUBLE 3/4" CHAMFERS (3/4" DEEP SAWCUT OPTIONAL)



CONTROL CRACK JOINT DETAIL

1/4" PREFORMED EXPANSION JOINT FILLER MATERIAL TO BE PLACED AT CONSTRUCTION JOINTS.



CONSTRUCTION JOINT DETAIL

NOTES: CB3 AND CB4 BARS SHALL BE CONTINUOUS THRU THE CONTROL CRACK JOINTS.

HORIZONTAL REINFORCING STEEL SHALL END 1" CLEAR OF ALL CONSTRUCTION JOINTS AND EXPANSION JOINTS.

| | | | | |
|----------|-----|-------|--|----------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 02/19 | SB US-81 OVER I-44 | |
| Checked | . | | CONCRETE LONGITUDINAL BARRIER DETAILS | |
| Approved | . | | CL STA. 196+50.65 | |
| Squad | | | Job Piece No. 24428(04) | Sheet No. B028 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B029-2442804-MISCELLANEOUS SUPERSTRUCTURE DETAILS.dwg 3/2/2020 2:43:24 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

BRIDGE SLAB AND CONSTRUCTION JOINT NOTES:

EPOXY-COAT OR GALVANIZE STEEL ITEMS USED TO FACILITATE CONSTRUCTION, SUCH AS FORM HANGER ASSEMBLIES, TY-BAR CLIPS, INSERT WELD ANCHORS, OR OTHER APPURTENANCES THAT WILL REMAIN IN PLACE IN THE BRIDGE SLAB. EPOXY-COAT IN ACCORDANCE WITH AASHTO M284 OR GALVANIZE IN ACCORDANCE WITH AASHTO M111.

THE BRIDGE SLAB SHALL BE POURED ONE SPAN AT A TIME, IN THE UPHILL DIRECTION. NO ADJACENT SPAN AT PIERS 1 AND 3 SHALL BE POURED UNTIL AT LEAST 48 HOURS AFTER THE OPPOSITE SPAN POUR HAS BEEN COMPLETED.

THE FINISHING MACHINE SHALL BE POSITIONED TO OPERATE PARALLEL TO THE SKEW OF THE BRIDGE.

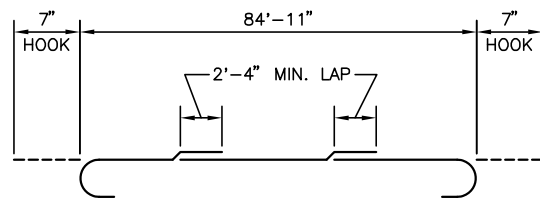
CONSTRUCTION JOINTS OVER PIERS 1 AND 3 SHALL NOT BE KEYED. IN THE EVENT OF AN EMERGENCY, HALT THE PLACEMENT OF CONCRETE BY FORMING A CONSTRUCTION JOINT PARALLEL TO THE SKEW OR AS DIRECTED BY THE ENGINEER. LONGITUDINAL REINFORCING SHALL BE CONTINUOUS THRU ALL TRANSVERSE CONSTRUCTION JOINTS. DO NOT PLACE ANY HEAVY EQUIPMENT ON THE FINISHED BRIDGE SLAB WITHIN 5'-0" OF ANY CONSTRUCTION JOINT UNTIL CONCRETE IS IN PLACE ON BOTH SIDES OF THE RESPECTIVE JOINT AND AT LEAST 48 HOURS HAS ELAPSED SINCE CONCRETE PLACEMENT.

SEAL ALL BRIDGE SLAB CONSTRUCTION JOINTS WITH HIGH MOLECULAR WEIGHT METHACRYLATE OR EPOXY RESIN IN ACCORDANCE WITH SECTION 523 OF THE SPECIFICATIONS. INCLUDE ALL COSTS OF EQUIPMENT AND LABOR FOR THE INSTALLATION OF THE HIGH MOLECULAR WEIGHT METHACRYLATE SEALER OR EPOXY RESIN IN THE CONTRACT UNIT PRICE OF "SEALER CRACK PREPARATION". INCLUDE ALL COSTS OF THE HIGH MOLECULAR WEIGHT METHACRYLATE SEALER OR EPOXY RESIN IN THE CONTRACT UNIT PRICE OF "SEALER RESIN". THE DEPARTMENT WILL NOT MEASURE THE PREPARATION AND SEALER RESIN ON EMERGENCY CONSTRUCTION JOINTS FOR PAYMENT.

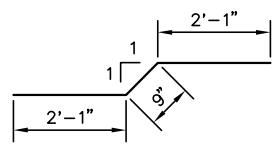
DO NOT SAW CUT GROOVE WITHIN 6" OF ANY CONSTRUCTION JOINT.

| SUPERSTRUCTURE BAR LIST | | | | |
|-------------------------|-----|------|-------|-----------|
| MARK | NO. | SIZE | SHAPE | LENGTH |
| (EPOXY COATED) | | | | |
| A1 | 528 | 5 | STR. | 92'-0" |
| A2 | 738 | 5 | BNT. | 90'-9" |
| A3 | 6 | 4 | STR. | 89'-0" |
| A4 | 16 | 4 | STR. | 7'-10" |
| A5 | 4 | 4 | STR. | 84'-1" |
| A6 | 4 | 4 | STR. | 2'-2" |
| AS | 172 | 4 | BNT. | 4'-11" |
| CB1 | 312 | 5 | BNT. | 8'-0" |
| CB2 | 312 | 5 | BNT. | 7'-1" |
| CB3 | 20 | 5 | STR. | 57'-11" |
| CB4 | 20 | 5 | STR. | 104'-11" |
| E1 | 378 | 5 | STR. | 165'-7" |
| E2 | 170 | 5 | STR. | 24'-0" |
| EJ1 | 2 | 5 | STR. | 92'-0" |
| F1 | 256 | 4 | STR. | 7'-10" |
| F2 | 128 | 4 | STR. | 8'-10" |
| F3 | 192 | 4 | STR. | 8'-8" |
| FS2 | 624 | 5 | BNT. | 7'-4" |
| U1 | 576 | 4 | BNT. | 6'-2" |
| U2 | 320 | 4 | BNT. | 4'-9" |
| UD1 | 144 | 4 | BNT. | 4'-8" |
| UD2 | 30 | 4 | BNT. | 3'-1" |
| UD3 | 162 | 4 | BNT. | 5'-1" |
| UD4 | 172 | 4 | BNT. | 3'-6 1/2" |

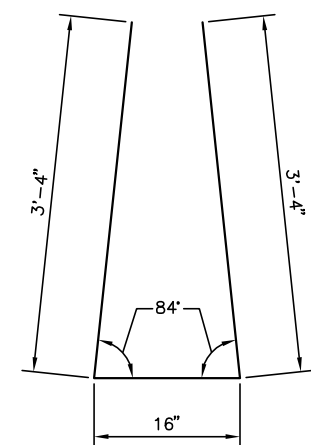
- (1) LENGTH INCLUDES TWO (2) 3'-6" LAP SPLICES.
- (2) LENGTH INCLUDES TWO (2) 2'-4" LAP SPLICES.
- (3) LENGTH INCLUDES TWO (2) 2'-0" LAP SPLICES.
- (4) LENGTH INCLUDES ONE (1) 3'-1" LAP SPLICE.
- (5) LENGTH INCLUDES TWO (2) 3'-1" LAP SPLICES.
- (6) LENGTH INCLUDES FOUR (4) 3'-0" LAP SPLICES.
- (7) FOR BAR BEND DETAILS, SEE STD. FSHP-42-2.



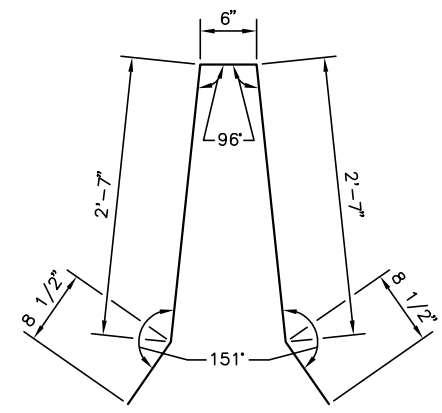
A2#5 x 90'-9"



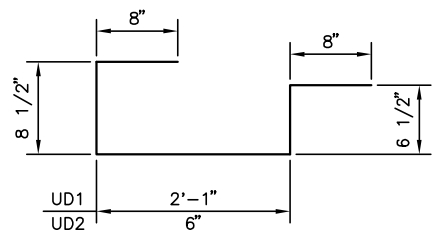
AS#4 x 4'-11"



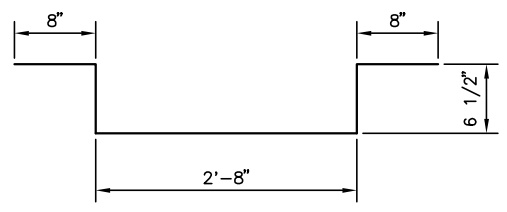
CB1#5 x 8'-0"



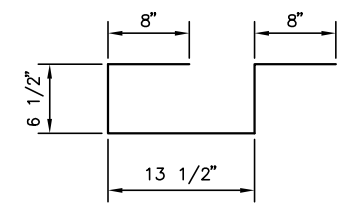
CB2#5 x 7'-1"



UD1#4 x 4'-8"
UD2#4 x 3'-1"



UD3#4 x 5'-1"



UD4#4 x 3'-6 1/2"

BAR BEND DETAILS

(USE STIRRUP BEND DIAMETERS FOR CB1, CB2 AND ALL UD BARS.)

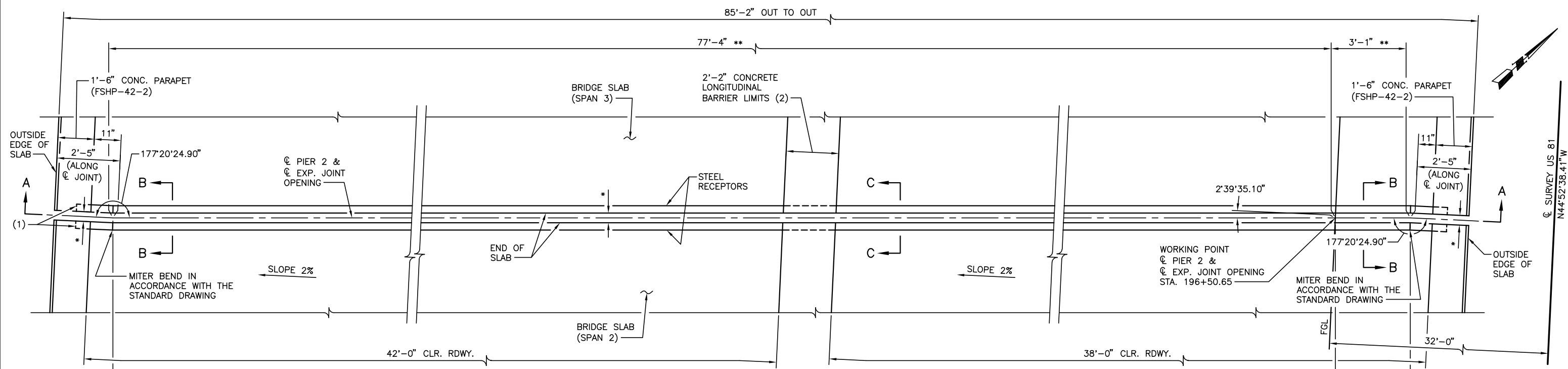
NOTE: FOR U1 AND U2 BAR BEND DETAILS, SEE SHEET NO. B025.

| | | | |
|----------|-----------|---|----------------|
| Design | . | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW 02/19 | SB US-81 OVER I-44 | |
| Checked | . | MISCELLANEOUS SUPERSTRUCTURE DETAILS | |
| Approved | . | Ⓢ STA. 196+50.65 | |
| Squad | | Job Piece No. 24428(04) | Sheet No. B029 |

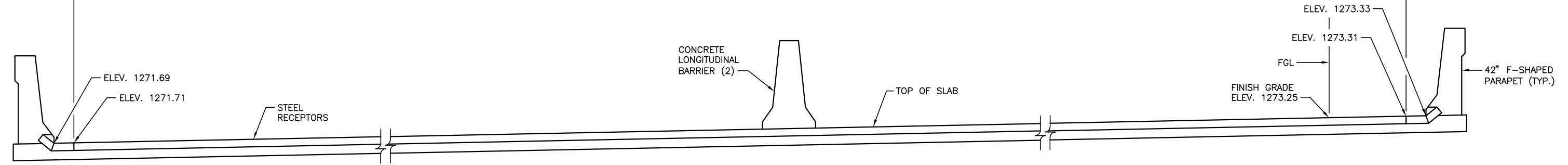
"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\179\Final Design F & G\B030-2442804-EXPANSION JOINT DETAILS.dwg 3/2/2020 2:43:59 PM

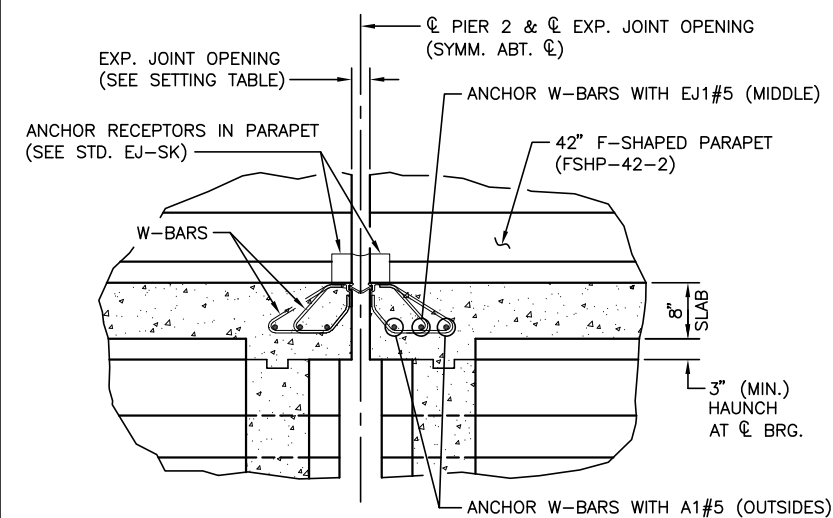
| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



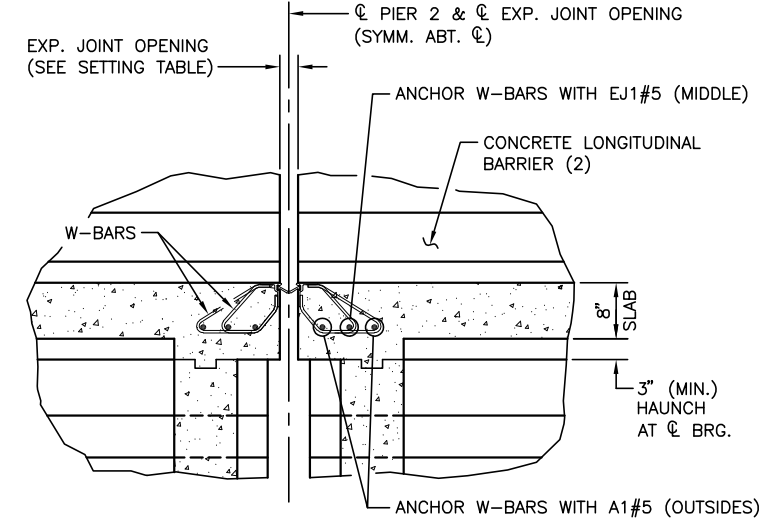
PART PLAN AT PIER 2
(NOT TO SCALE)



SECTION A-A



SECTION B-B



SECTION C-C

| TEMPERATURE | OPENING |
|-------------|---------|
| 0°F | 4 1/16" |
| 15°F | 3 3/4" |
| 30°F | 3 7/16" |
| 45°F | 3 1/16" |
| 60°F | 2 3/4" |
| 75°F | 2 7/16" |
| 90°F | 2 1/16" |
| 105°F | 1 3/4" |
| 120°F | 1 7/16" |

- (1) ANCHOR RECEPTORS IN PARAPET (SEE STD. EJ-SK) (TYP.)
- (2) CONCRETE LONGITUDINAL BARRIER SHALL BE CONSTRUCTED ON TOP OF STEEL RECEPTORS AND THE EXPANSION JOINT OPENING IN THE CONCRETE LONGITUDINAL BARRIER SHALL MATCH BRIDGE SLAB OPENING.

* EXPANSION JOINT OPENING - SEE SETTING TABLE
** ALONG C/J

NOTE: FOR ADDITIONAL DETAILS AND EXPANSION JOINT NOTES, SEE STDS. EJ-SK AND EJ-DTL.

| | | | |
|----------|-----------|--------------------|--------------|
| Design | . | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW 02/19 | SB US-81 OVER I-44 | |
| Checked | . | | |
| Approved | . | | |
| Squad | | | |

EXPANSION JOINT DETAILS
C/J STA. 196+50.65

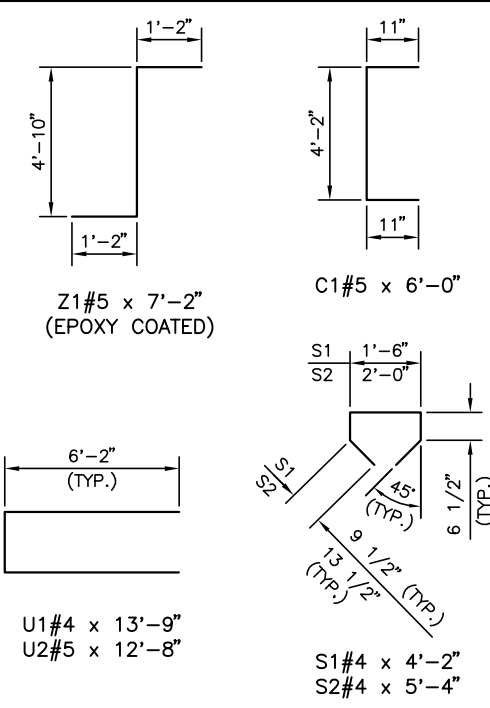
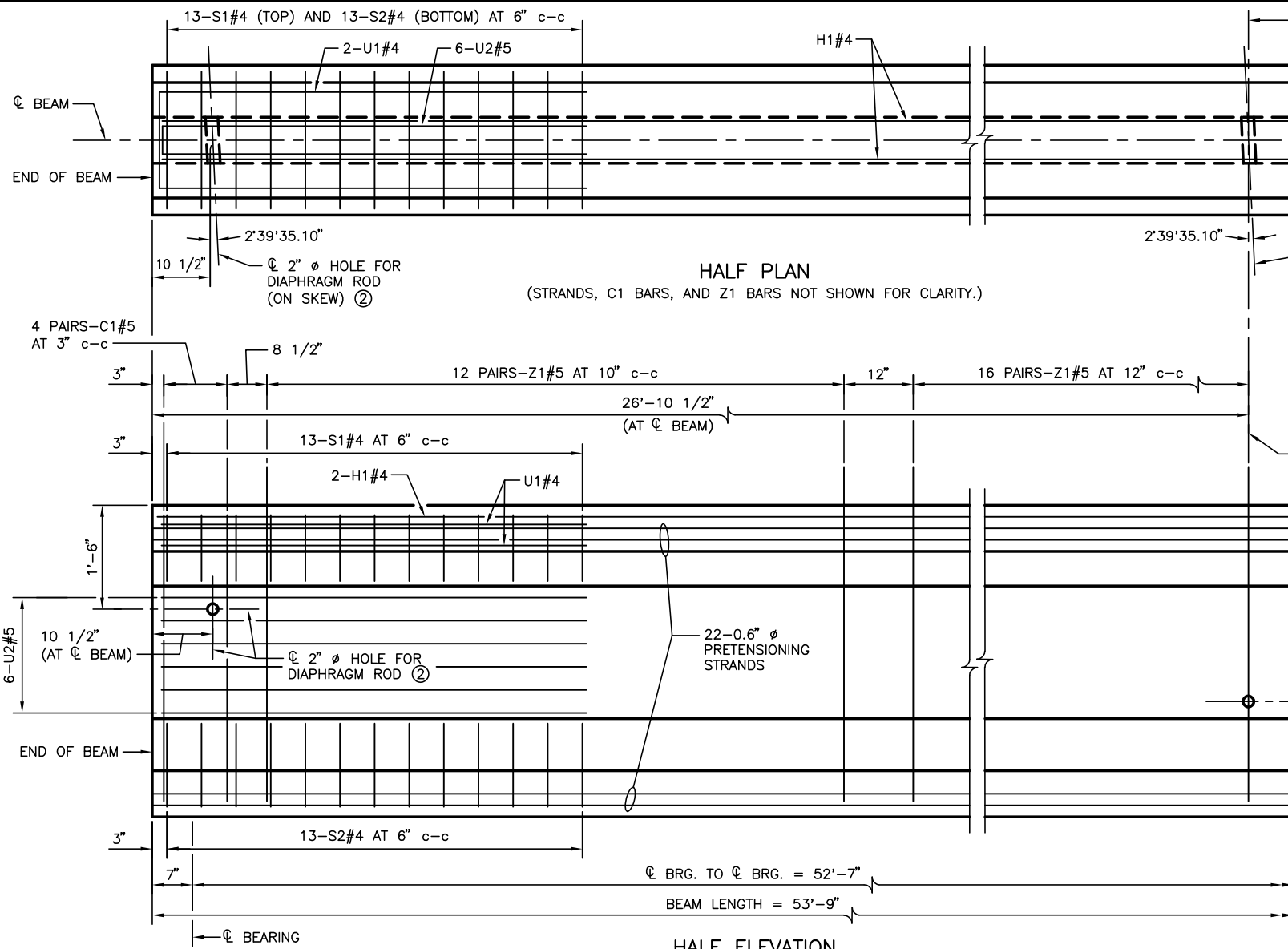
Job Piece No. 24428(04)

Sheet No. B030

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B031-2442804-55\TYPE IV PC BEAM DETAILS.dwg 3/2/2020 2:44:44 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



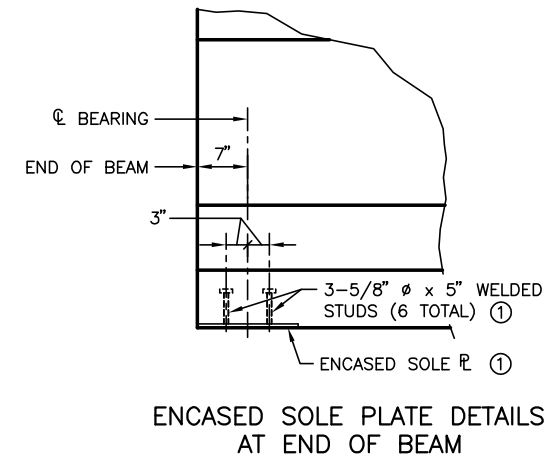
PC BEAM NOTES:

COMPRESSIVE STRENGTH: PROVIDE CONCRETE WITH A COMPRESSIVE STRENGTH OF 4,500 PSI AT TRANSFER OF PRESTRESS AND 6,000 PSI AT 28 DAYS.

STRAND TYPE: PROVIDE LOW-RELAXATION STRANDS HAVING A NOMINAL DIAMETER OF 0.6" WITH AN ULTIMATE TENSILE STRENGTH OF 270 KSI.

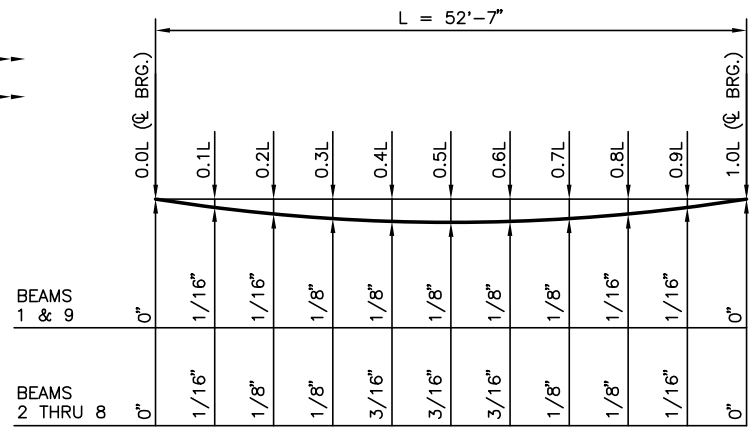
LRFR INVENTORY RATING FACTOR = 2.00
LFD OPERATING RATING = HS 72

FOR ADDITIONAL DESIGN DATA, SEE SHEET NO. B003.



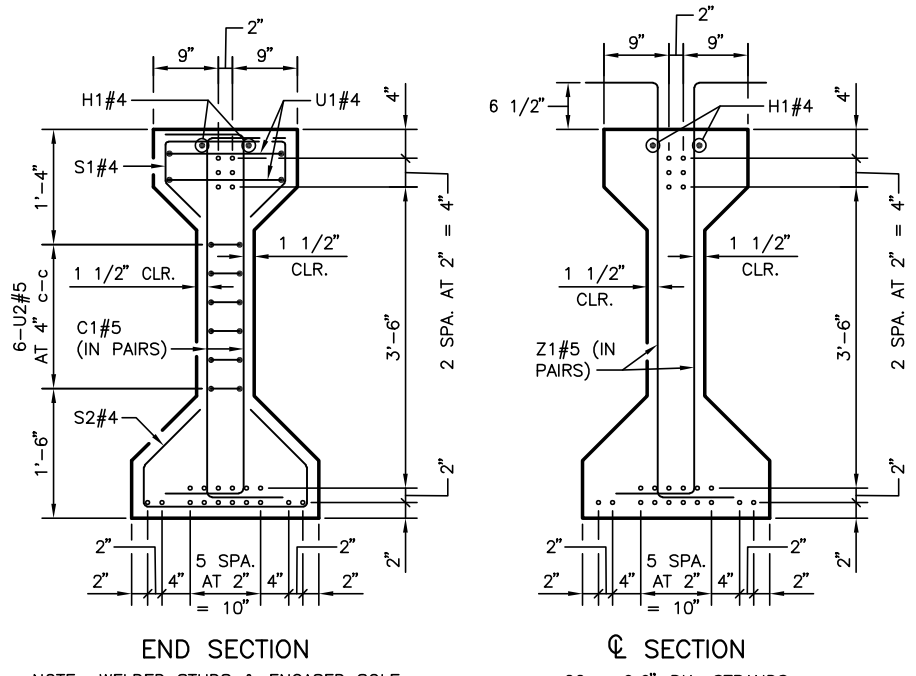
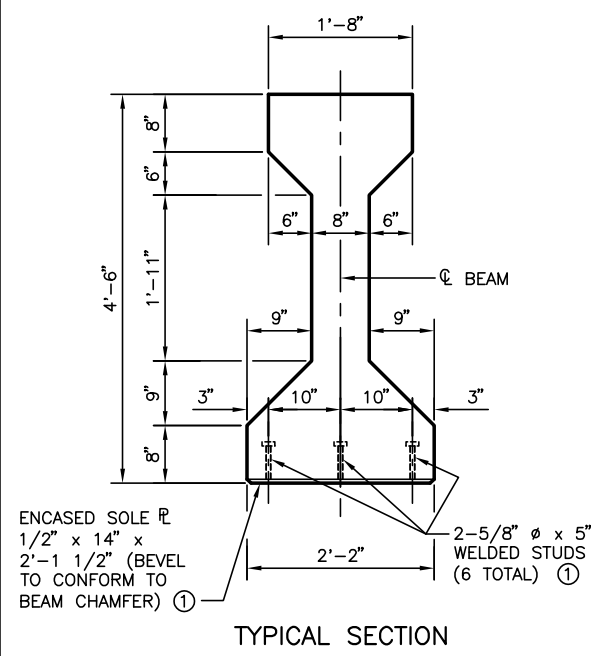
- ① INSTALL AT BOTH ENDS OF EACH BEAM.
- ② SHIFT PLAIN REINFORCING STEEL AS NECESSARY TO PROVIDE 1" MIN. CLR. TO DIAPHRAGM ROD HOLE.

NOTE: ALL LONGITUDINAL DIMENSIONS ARE HORIZONTAL DIMENSIONS.



INTENTIONALLY ROUGHENED SURFACE EXAMPLES

INTENTIONALLY ROUGHEN THE ENTIRE TOP SURFACE OF THE P.C. BEAMS TO A MINIMUM HEIGHT OF 1/4" OVER A MAXIMUM PITCH OF 2" MEASURED LONGITUDINALLY ALONG THE LENGTH OF THE BEAM. PROVIDE A CREST AND TROUGH ASSOCIATED WITH THE HEIGHT OF NOT LESS THAN 1/2", THE FULL WIDTH OF THE TOP FLANGE. PRODUCE THE ROUGHENED SURFACE BY USING A SPECIAL TROWEL TO FORM ONE OF THE SURFACES SHOWN IN THE DETAILS, BY CLEANING THE CONCRETE SURFACE WITH A STIFF WIRE BRUSH (OR BLASTING) TO EXPOSE THE AGGREGATE TO A HEIGHT OF 1/4", OR BY USING ANOTHER APPROVED METHOD. SUBMIT THE METHOD TO BE USED FOR APPROVAL BY THE ENGINEER. REPAIR ANY DAMAGE TO REINFORCEMENT'S EPOXY COATING BEFORE PLACEMENT OF SLAB CONCRETE.

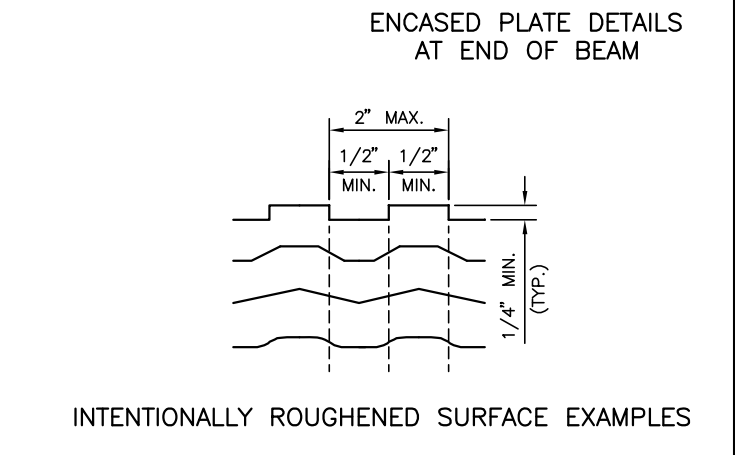
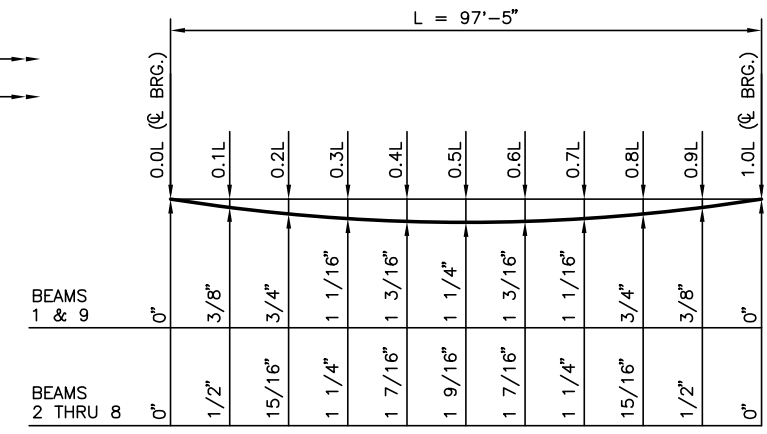
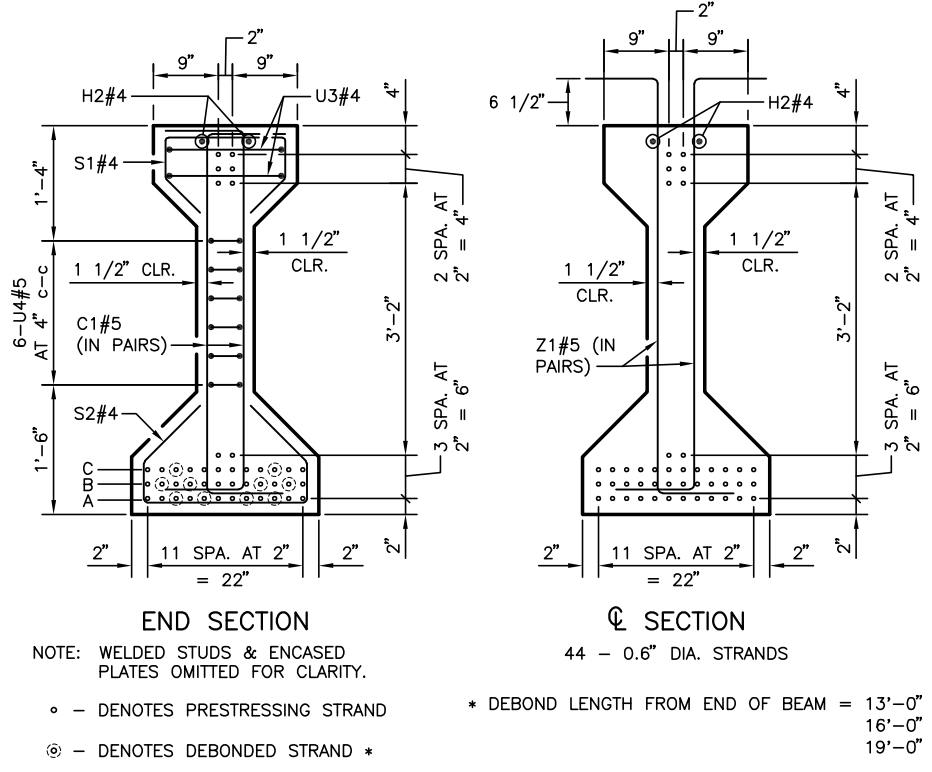
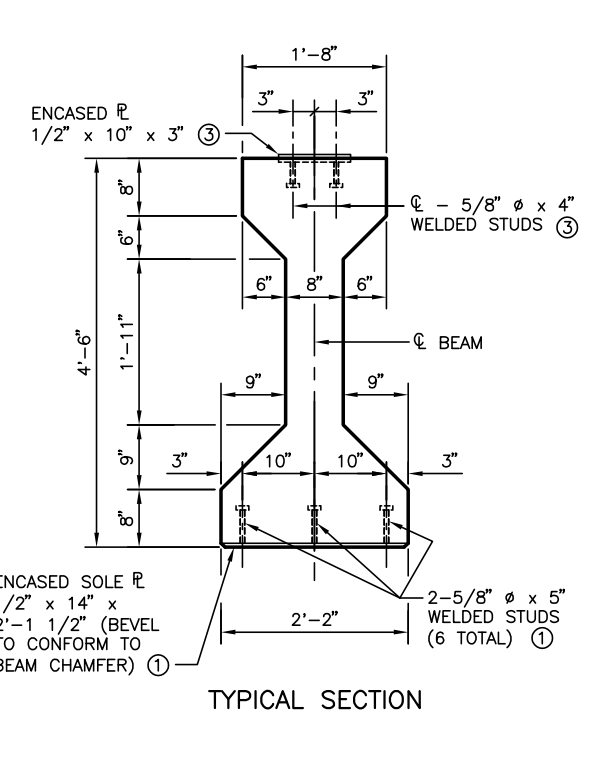
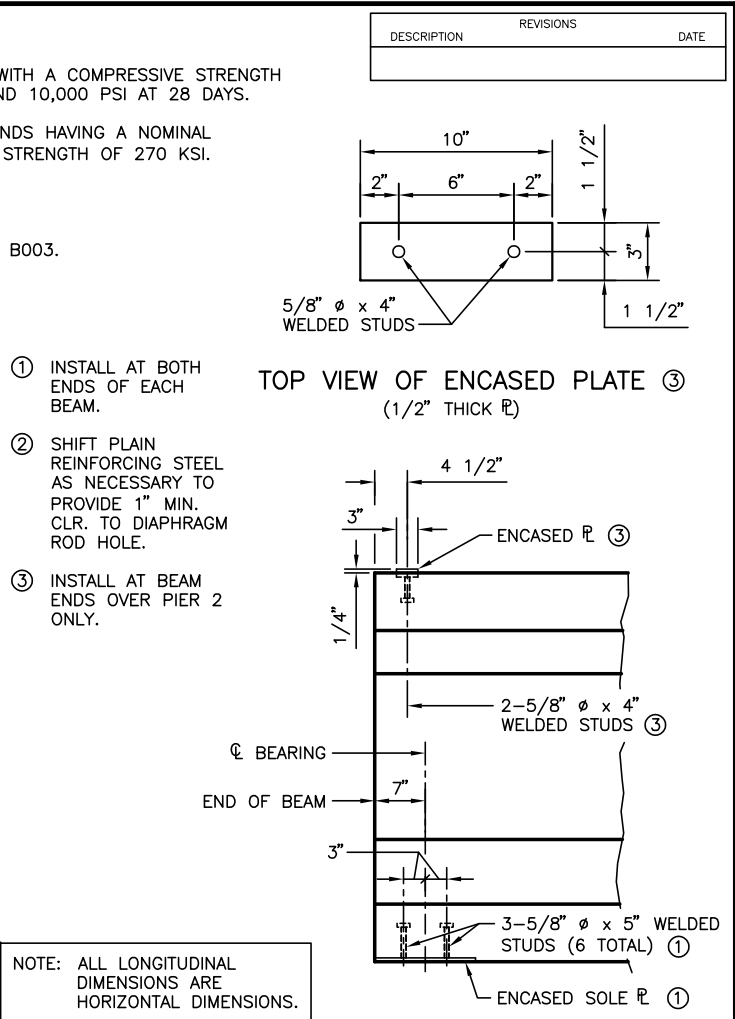
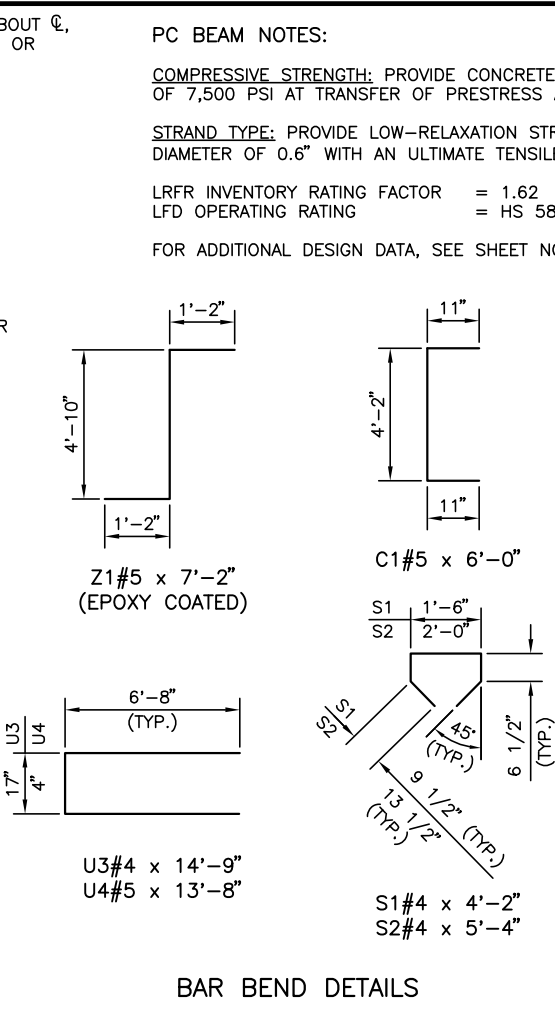
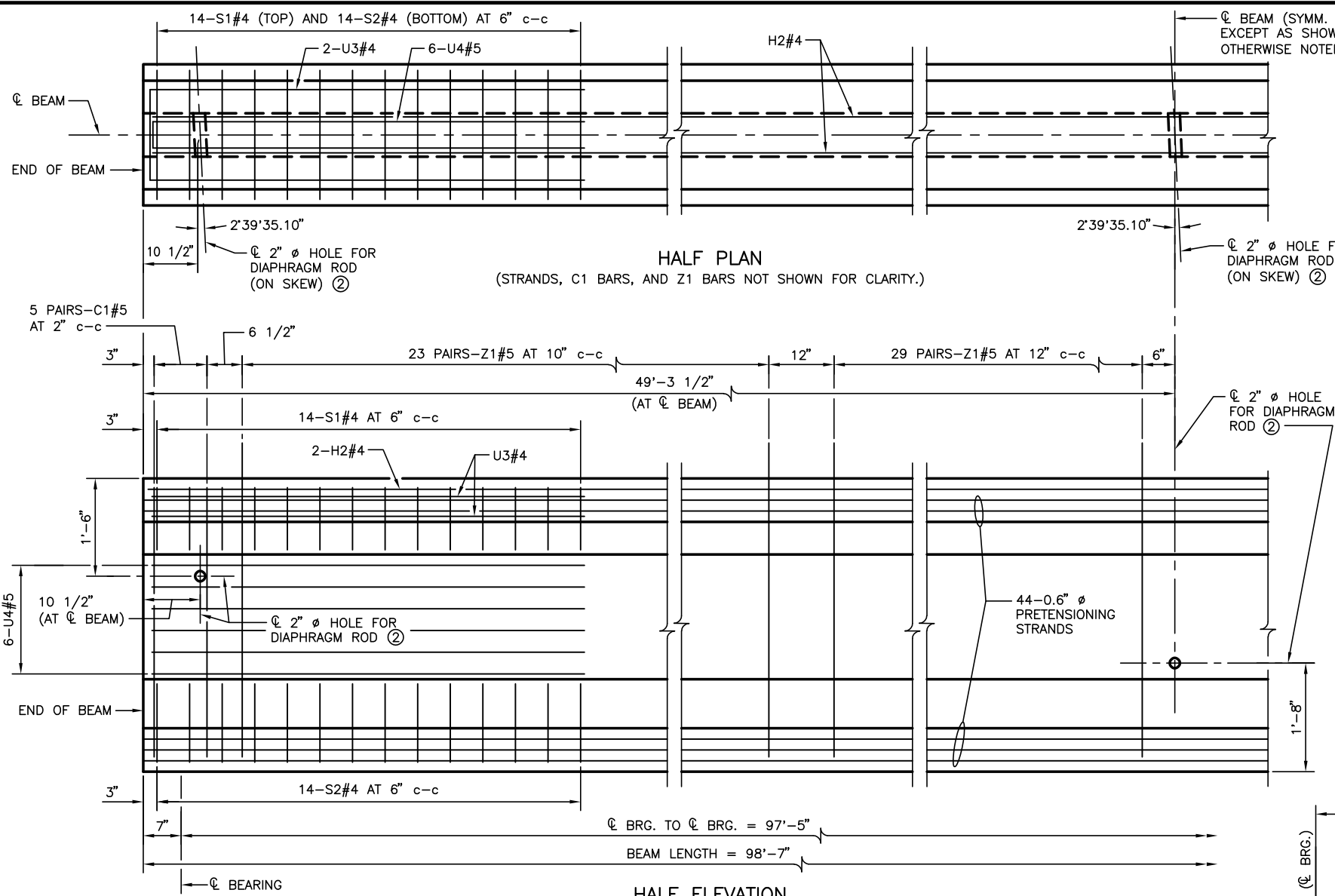


- NOTES:
- FOR 99' TYPE IV P.C. BEAM DETAILS, SEE SHEET NO. B032.
 - FOR TYPICAL SECTION, SEE SHEET NO. B020.
 - FOR LONGITUDINAL SECTION, SEE SHEET NO. B021.
 - FOR BEARING DETAILS, SEE SHEET NO. B023.
 - FOR P.C. BEAM LAYOUT AND DIAPHRAGM DETAILS, SEE SHEET NOS. B024 AND B025.

| | | | | |
|----------|-----|-------|-------------------------|----------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 02/19 | SB US-81 OVER I-44 | |
| Checked | . | | | |
| Approved | . | | | |
| Squad | | | Job Piece No. 24428(04) | Sheet No. B031 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B032-2442804-99\TYPE IV PC BEAM DETAILS.dwg 3/2/2020 2:45:39 PM



NOTES: FOR TYPICAL SECTION, SEE SHEET NO. B020.
 FOR LONGITUDINAL SECTION, SEE SHEET NO. B021.
 FOR BEARING DETAILS, SEE SHEET NO. B023.
 FOR P.C. BEAM LAYOUT AND DIAPHRAGM DETAILS, SEE SHEET NOS. B024 AND B025.

| | | | | |
|----------|-----|-------|----------------------------------|--------------|
| Design | . | | BRIDGE "F" SB US-81 OVER I-44 | GRADY COUNTY |
| Drawn | PKW | 02/19 | | |
| Checked | . | | | |
| Approved | . | | | |
| Squad | | | | |

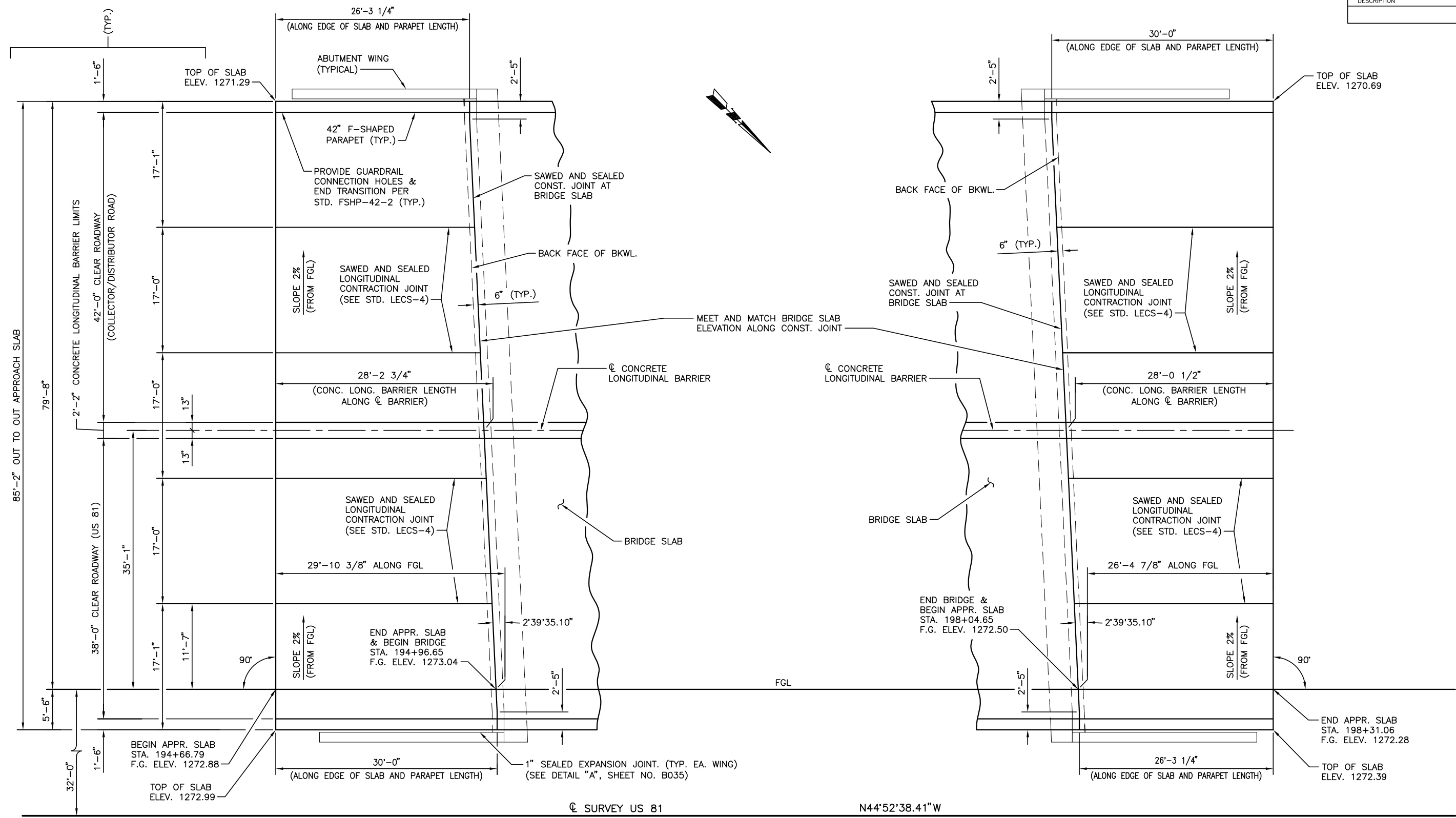
99' TYPE IV P.C. BEAM DETAILS
 STA. 196+50.65

Job Piece No. 24428(04) Sheet No. B032

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & C\B033-2442804-APPROACH SLAB DETAILS 1.dwg 3/2/2020 2:49:07 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



ABUTMENT 1

APPROACH SLAB LAYOUT PLAN
(NOT TO SCALE)

ABUTMENT 2

NOTE: DO NOT SAW-CUT GROOVE WITHIN 6" OF THE SAWED AND SEALED JOINTS.

NOTES: ALL DIMENSIONS ARE HORIZONTAL DIMENSIONS.
DIMENSIONS AND DETAILS ARE TYPICAL FOR BOTH APPROACH SLABS, UNLESS OTHERWISE SHOWN.
FOR APPROACH SLAB REINFORCING PLAN, SEE SHEET NO. B034.

FOR ADDITIONAL APPROACH SLAB DETAILS, BAR LISTS AND QUANTITIES, SEE SHEET NO. B035.

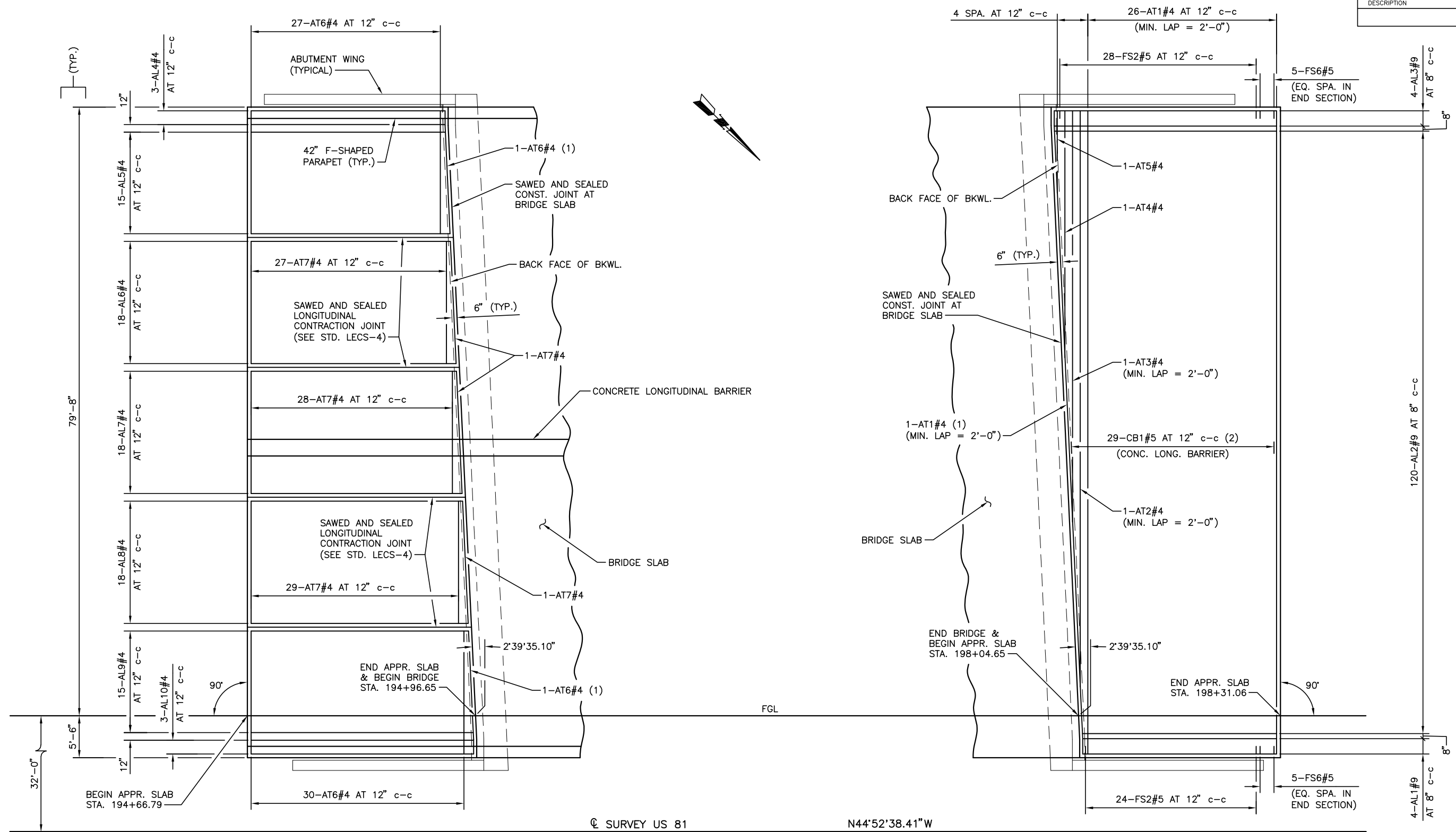
| | | |
|----------|-----|-------|
| Design | . | |
| Drawn | PKW | 02/19 |
| Checked | . | |
| Approved | . | |
| Squad | | |

BRIDGE "F"
SB US-81 OVER I-44
GRADY COUNTY
APPROACH SLAB DETAILS (1 OF 3)
CL STA. 196+50.65
Job Piece No. 24428(04)
Sheet No. B033

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & C\B034-2442804-APPROACH SLAB DETAILS 2.dwg 3/2/2020 2:56:37 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



ABUTMENT 1
TYPICAL TOP REINFORCING

ABUTMENT 2
TYPICAL BOTTOM REINFORCING AND
PARAPET/BARRIER REINFORCING

PLAN SHOWING APPROACH SLAB REINFORCING STEEL
(NOT TO SCALE)

- (1) FIELD BEND AT1 AND AT6 BARS, ALONG BRIDGE SLAB CONST. JOINT, TO FIT SLAB GEOMETRY NEAR EDGES.
- (2) PLACE CB1 BAR NEAREST THE BEGIN/END BRIDGE SLAB ALONG SKEW AS SHOWN.

NOTES: ALL DIMENSIONS ARE HORIZONTAL DIMENSIONS. DIMENSIONS AND DETAILS ARE TYPICAL FOR BOTH APPROACH SLABS, UNLESS OTHERWISE SHOWN. FOR APPROACH SLAB LAYOUT PLAN, SEE SHEET NO. B033.

FOR ADDITIONAL APPROACH SLAB DETAILS, BAR LISTS AND QUANTITIES, SEE SHEET NO. B035. FS2 AND FS6 BARS IN PARAPETS, AND CB1 BARS IN CONCRETE LONGITUDINAL BARRIERS SHALL BE TIED IN PLACE PRIOR TO POURING THE APPROACH SLABS. FOR ADDITIONAL DETAILS, SEE STD. FSHP-42-2 AND SHEET NO. B028.

| | | |
|----------|-----|-------|
| Design | . | |
| Drawn | PKW | 02/19 |
| Checked | . | |
| Approved | . | |
| Squad | | |

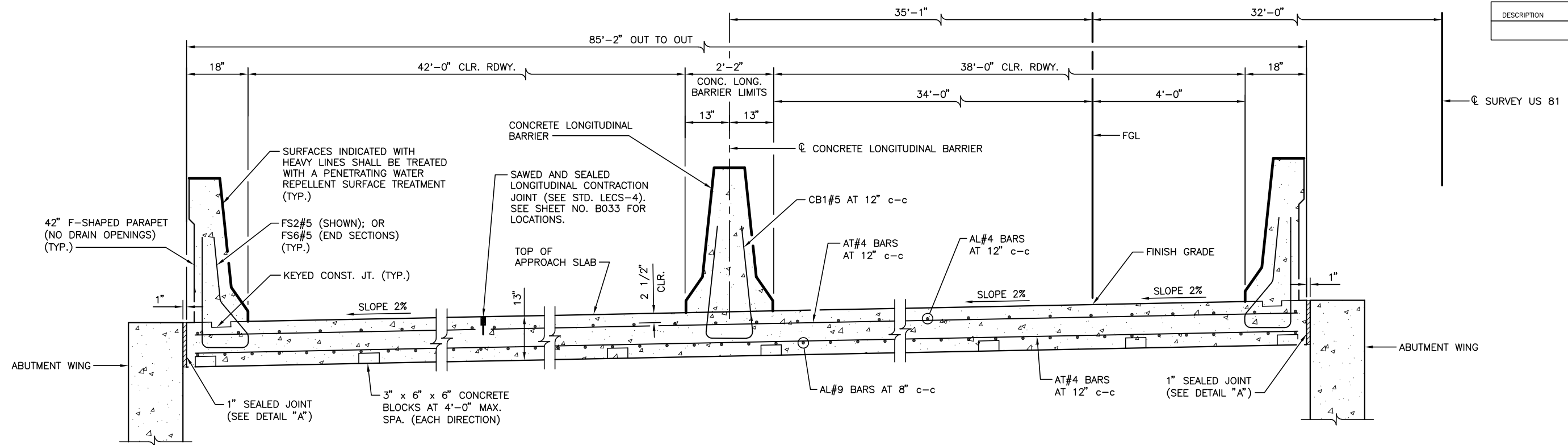
BRIDGE "F"
SB US-81 OVER I-44
GRADY COUNTY

APPROACH SLAB DETAILS (2 OF 3)
@ STA. 196+50.65

Job Piece No. 24428(04) Sheet No. B034

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & C\B035-2442804-APPROACH SLAB DETAILS 3.dwg 3/2/2020 2:57:41 PM



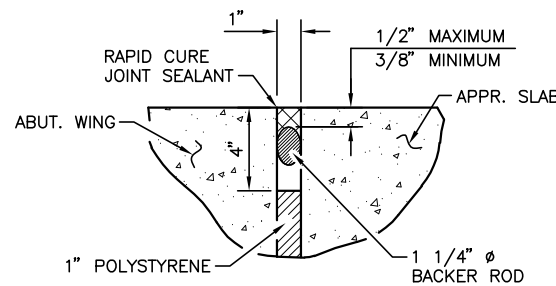
TYPICAL SECTION THRU APPROACH SLAB

- NOTES: THE REINFORCING STEEL IN THE TOP OF THE APPROACH SLABS SHALL END 2" EITHER SIDE OF THE LONGITUDINAL CONTRACTION JOINTS.
- FOR PARAPET DETAILS NOT SHOWN, SEE STD. FSHP-42-2.
- FOR CONCRETE LONGITUDINAL BARRIER DETAILS NOT SHOWN, SEE SHEET NO. B028.

| BAR LIST - ONE APPROACH SLAB (TWO REQUIRED) | | | | |
|---|-----|------|-------|---------------------|
| MARK | NO. | SIZE | SHAPE | LENGTH |
| (EPOXY COATED) | | | | |
| AL1 | 4 | 9 | STR. | 25'-11" |
| AL2 | 120 | 9 | STR. | 27'-9" AVG. (1) |
| AL3 | 4 | 9 | STR. | 29'-8" |
| AL4 | 3 | 4 | STR. | 25'-11" |
| AL5 | 15 | 4 | STR. | 26'-3" AVG. (2) |
| AL6 | 18 | 4 | STR. | 26'-11" AVG. (3) |
| AL7 | 18 | 4 | STR. | 27'-9 1/2" AVG. (4) |
| AL8 | 18 | 4 | STR. | 28'-6 1/2" AVG. (5) |
| AL9 | 15 | 4 | STR. | 29'-3 1/2" AVG. (6) |
| AL10 | 3 | 4 | STR. | 29'-8" |
| AT1 | 27 | 4 | STR. | 88'-11" |
| AT2 | 1 | 4 | STR. | 83'-5" (7) |
| AT3 | 1 | 4 | STR. | 59'-11" (8) |
| AT4 | 1 | 4 | STR. | 36'-3" |
| AT5 | 1 | 4 | STR. | 14'-9" |
| AT6 | 59 | 4 | STR. | 16'-9" |
| AT7 | 87 | 4 | STR. | 16'-8" |
| CB1 | 29 | 5 | BNT. | 8'-0" (9) |
| FS2 | 52 | 5 | BNT. | 7'-4" (10) |
| FS6 | 10 | 5 | BNT. | 7'-6 1/2" (10) |

- (1) 120 BARS VARYING FROM 25'-11" TO 29'-7".
- (2) 15 BARS VARYING FROM 25'-11" TO 26'-7".
- (3) 18 BARS VARYING FROM 26'-6" TO 27'-4".
- (4) 18 BARS VARYING FROM 27'-5" TO 28'-2".
- (5) 18 BARS VARYING FROM 28'-2" TO 28'-11".
- (6) 15 BARS VARYING FROM 29'-0" TO 29'-7".
- (7) LENGTH INCLUDES TWO (2) 2'-0" LAP SPLICES.
- (8) LENGTH INCLUDES ONE (1) 2'-0" LAP SPLICE.
- (9) FOR BAR BEND DETAILS, SEE SHEET NO. B029.
- (10) FOR BAR BEND DETAILS, SEE STD. FSHP-42-2.

| CONCRETE LONGITUDINAL BARRIER BAR LIST (TOTAL) | | | | |
|--|-----|------|-------|-----------|
| MARK | NO. | SIZE | SHAPE | LENGTH |
| (EPOXY COATED) | | | | |
| CB2 | 58 | 5 | BNT. | 7'-1" (9) |
| CB5 | 10 | 5 | STR. | 28'-0" |
| CB6 | 10 | 5 | STR. | 27'-9" |



DETAIL "A"

| APPROACH SLAB QUANTITIES | | |
|--------------------------------------|-------|---------|
| ITEM | UNITS | TOTAL |
| APPROACH SLAB | SY | 532.50 |
| SAW-CUT GROOVING | SY | 500.20 |
| 42" F-SHAPED PARAPET | LF | 112.60 |
| CLASS AA CONCRETE | CY | 9.00 |
| EPOXY COATED REINFORCING STEEL | LB | 1010.00 |
| WATER REPELLENT (VISUALLY INSPECTED) | SY | 103.00 |

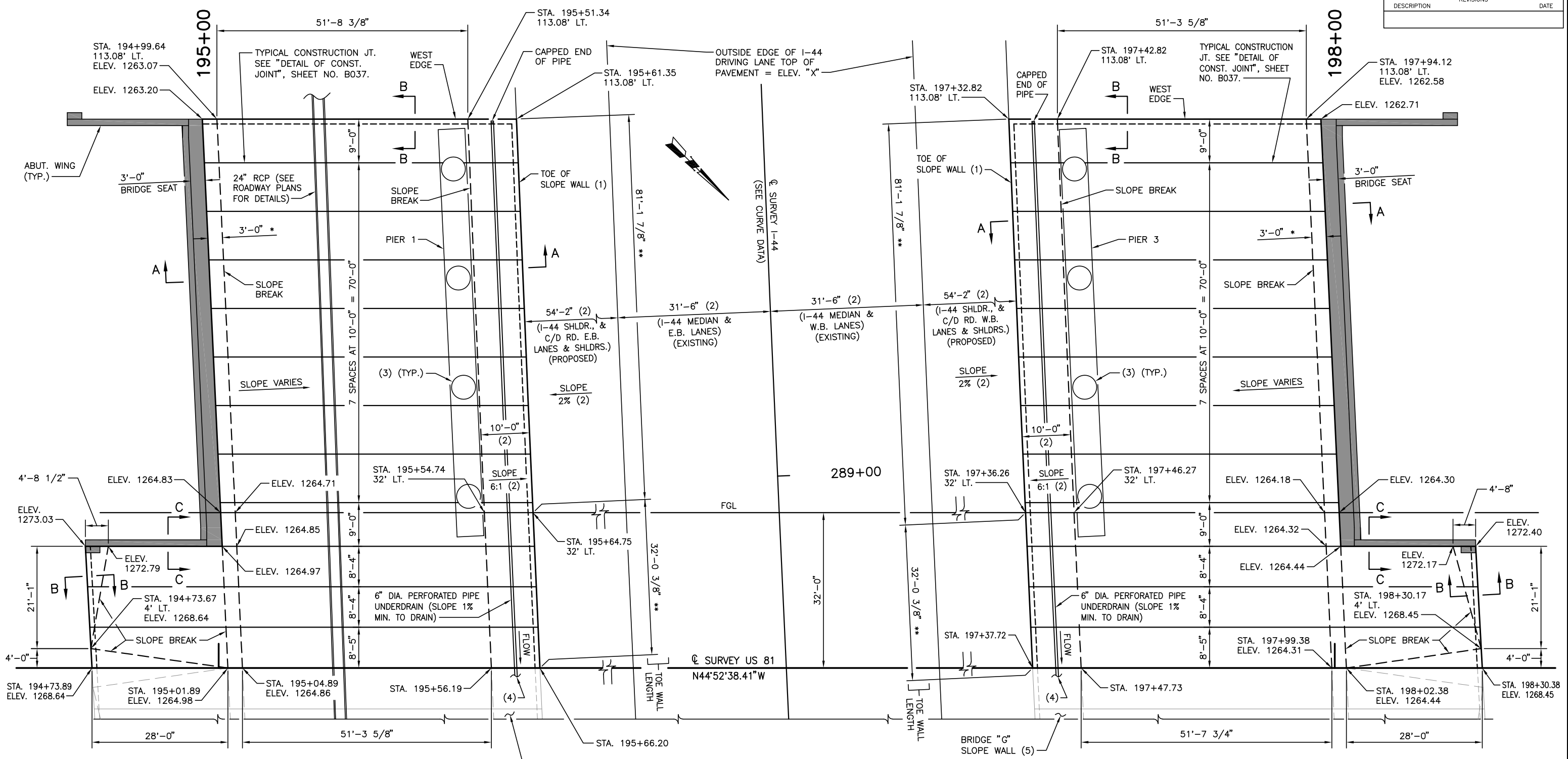
- ① THE UNIT PRICE BID FOR APPROACH SLAB SHALL INCLUDE ALL CONCRETE AND REINFORCING STEEL IN THE APPROACH SLABS, INCLUDING CB1, FS2 AND FS6 BARS, RAPID CURE JOINT SEALANT, BACKER RODS, POLYSTYRENE, LABOR, EQUIPMENT AND OTHER INCIDENTALS NECESSARY TO COMPLETE THE WORK AS SHOWN. THERE IS AN ESTIMATED 192.3 C.Y. OF CLASS AA CONCRETE, AND AN ESTIMATED 35660 LBS. OF EPOXY COATED REINFORCING STEEL IN THE APPROACH SLABS.
- ② QUANTITY SHOWN IS FOR THE CONCRETE LONGITUDINAL BARRIERS.
- ③ QUANTITY SHOWN INCLUDES 51.0 S.Y. OF WATER REPELLENT FOR THE CONCRETE LONGITUDINAL BARRIERS.

| | | | |
|----------|-----------|---------------------------------------|----------------|
| Design | . | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW 02/19 | SB US-81 OVER I-44 | |
| Checked | . | APPROACH SLAB DETAILS (3 OF 3) | |
| Approved | . | CL STA. 196+50.65 | |
| Squad | | Job Piece No. 24428(04) | Sheet No. B035 |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B036-2442804-SLOPE WALL DETAILS 1.dwg 3/2/2020 2:58:25 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



PLAN OF SLOPE WALLS
SCALE 1"=20'

- * SLOPE 1 1/2" IN 3'-0"
- ** HORIZONTAL ARC DIMENSION ALONG TOE.

NOTES: ELEVATIONS SHOWN ARE AT TOP OF SLOPE WALL CONCRETE.
 ALL STATIONS AND OFFSETS FOLLOW ϕ SURVEY US 81, UNLESS NOTED OR SHOWN OTHERWISE.
 ALL DIMENSIONS ARE HORIZONTAL DIMENSIONS.
 THE BRIDGE SUPERSTRUCTURE AND PIER 2 ARE NOT SHOWN FOR CLARITY.
 FOR SLOPE WALL SECTIONS, NOTES AND QUANTITIES, SEE SHEET NO. B037.

- (1) TOE OF SLOPE WALL ELEVATION AT ANY PARTICULAR POINT IS GIVEN BY THE FOLLOWING EQUATION:
TOE ELEV. = ELEV. "X" - 1.08'
- (2) RADIAL DIMENSION.
- (3) PROVIDE 1/2" PREFORMED EXPANSION JOINT FILLER MATERIAL AROUND COLUMNS (SEC. 701.08.B). SEAL WITH LOW MODULUS SILICONE JOINT SEALANT (SEC. 701.08.E).
- (4) PROVIDE 195 LF (EA. SLOPE WALL) OF 6" DIA. NON-PERFORATED PIPE UNDERDRAIN TO DRAIN UNDERNEATH THE BRIDGE "G" HEADER. SLOPE 1% MIN. TO DRAIN, AS DIRECTED BY THE ENGINEER.
- (5) BRIDGE "G" WILL BE BUILT UNDER JOB PIECE NO. 24428(17).

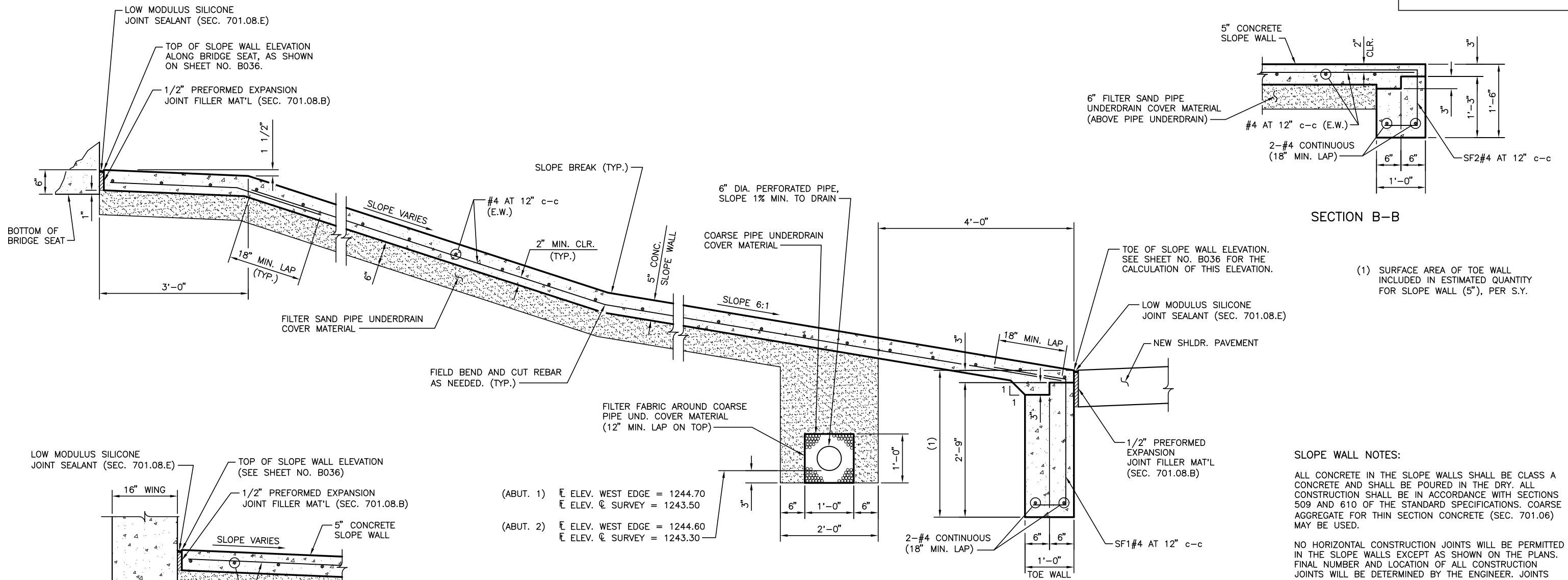
ϕ SURVEY I-44
CURVE DATA
 P.C. STA. 270+98.50
 P.I. STA. 282+00.00
 P.T. STA. 292+98.50
 Δ = 07°20'00.00" LT.
 D = 00°20'00.00"
 R = 17188.73'
 L = 2200.00'
 T = 1101.50'

| | | | | |
|----------|-----|-------|--|--------------------------------|
| Design | . | | BRIDGE "F" SB US-81 OVER I-44 SLOPE WALL DETAILS (1 OF 2) ϕ STA. 196+50.65 Job Piece No. 24428(04) | GRADY COUNTY Sheet No. B036 |
| Drawn | PKW | 03/19 | | |
| Checked | . | | | |
| Approved | . | | | |
| Squad | | | | |

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

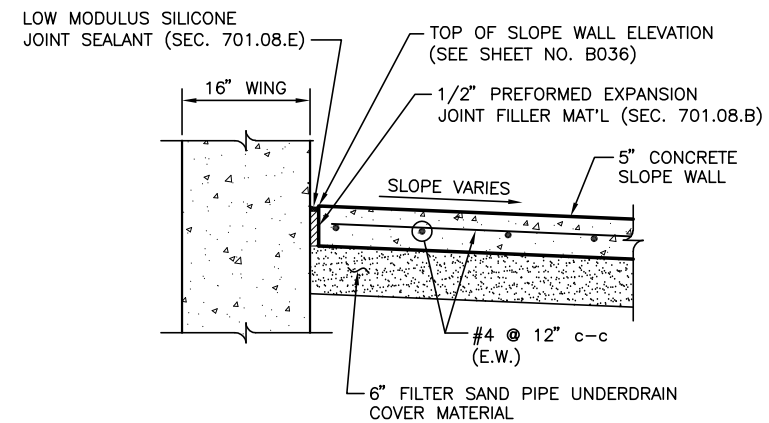
M:\W2M Projects\Project Files\M179\Final Design F & G\B037-2442804-SLOPE WALL DETAILS 2.dwg 3/2/2020 2:59:42 PM

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |



SECTION B-B

(1) SURFACE AREA OF TOE WALL INCLUDED IN ESTIMATED QUANTITY FOR SLOPE WALL (5"), PER S.Y.



SECTION C-C

- (ABUT. 1) ELEV. WEST EDGE = 1244.70
ELEV. C SURVEY = 1243.50
- (ABUT. 2) ELEV. WEST EDGE = 1244.60
ELEV. C SURVEY = 1243.30

SECTION A-A

NOTE: EXTENT, LOCATION AND DEPTH OF 6" PERFORATED PIPE UNDERDRAINS MAY BE ADJUSTED BY THE ENGINEER DURING CONSTRUCTION.

SLOPE WALL NOTES:

ALL CONCRETE IN THE SLOPE WALLS SHALL BE CLASS A CONCRETE AND SHALL BE POURED IN THE DRY. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH SECTIONS 509 AND 610 OF THE STANDARD SPECIFICATIONS. COARSE AGGREGATE FOR THIN SECTION CONCRETE (SEC. 701.06) MAY BE USED.

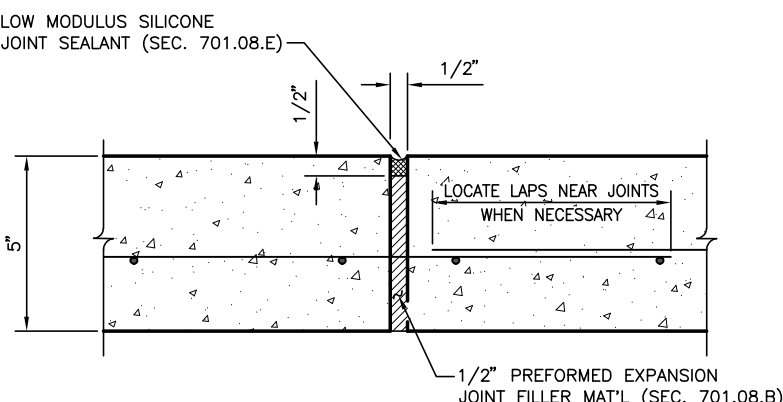
NO HORIZONTAL CONSTRUCTION JOINTS WILL BE PERMITTED IN THE SLOPE WALLS EXCEPT AS SHOWN ON THE PLANS. FINAL NUMBER AND LOCATION OF ALL CONSTRUCTION JOINTS WILL BE DETERMINED BY THE ENGINEER. JOINTS SHALL HAVE A MAXIMUM SPACING OF 10'-0".

5" CONCRETE SLOPE WALL WILL BE MEASURED FROM EDGE TO EDGE AND FROM TOP TO BOTTOM ALONG THE TOP SURFACE OF THE SLOPE WALL AND ALONG THE FRONT FACE OF THE TOE WALL. PAYMENT WILL BE MADE AT THE CONTRACT PRICE BID FOR SLOPE WALL (5"), PER S.Y. PRICE BID SHALL INCLUDE ALL COSTS OF JOINT SEALERS, FILLER MATERIALS, REINFORCING STEEL, CONCRETE, EXCAVATIONS, LABOR, FORMS, AND INCIDENTALS NECESSARY TO COMPLETE THE WORK AS SHOWN.

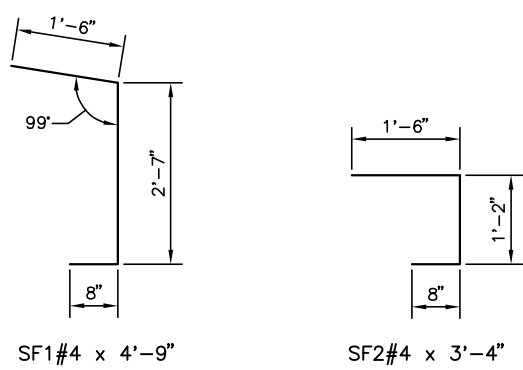
ALL COSTS OF PIPE UNDERDRAIN COVER MATERIALS (BOTH FILTER SAND AND COARSE MATERIAL), FILTER FABRICS, EXCAVATIONS, EQUIPMENT, LABOR AND INCIDENTALS FOR THEIR INSTALLATION SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE BID FOR 6" PERFORATED PIPE UNDERDRAIN ROUND. INSTALLATION SHALL BE AS SHOWN ON THE PLANS AND STD. PUD-3.

ALL COSTS OF TRENCH EXCAVATION AND STANDARD BEDDING MATERIAL FOR INSTALLATION OF THE NON-PERF. PIPE UNDERDRAIN BEYOND THE LIMITS OF THE SLOPE WALLS, INCLUDING MATERIALS, LABOR, EQUIPMENT AND INCIDENTALS FOR THEIR INSTALLATION SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF 6" NON-PERF. PIPE UNDERDRAIN RND. INSTALLATION SHALL BE AS SHOWN ON THE PLANS AND STD. PUD-3.

| SLOPE WALL QUANTITIES | | |
|-------------------------------------|-------|---------|
| ITEM | UNITS | TOTAL |
| SLOPE WALL (5") | SY | 1913.00 |
| 6" PERFORATED PIPE UNDERDRAIN ROUND | LF | 227.00 |
| 6" NON-PERF. PIPE UNDERDRAIN RND. | LF | 390.00 |



DETAIL OF CONSTRUCTION JOINT



BAR BEND DETAILS

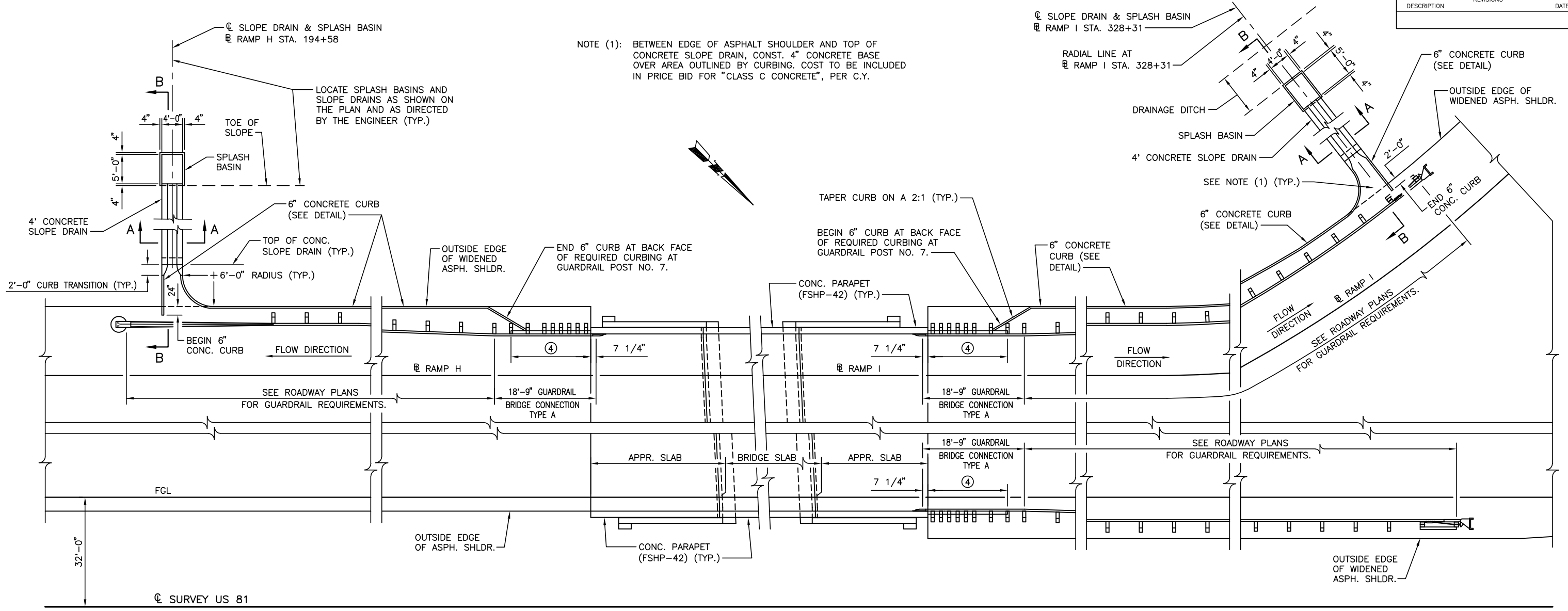
| | | | |
|----------|-----------|-------------------------|----------------|
| Design | . | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW 03/19 | SB US-81 OVER I-44 | |
| Checked | . | | |
| Approved | . | | |
| Squad | | Job Piece No. 24428(04) | Sheet No. B037 |

SLOPE WALL DETAILS (2 OF 2)

C STA. 196+50.65

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"

M:\W2M Projects\Project Files\M179\Final Design F & G\B038-2442804-DRAIN AT END BRIDGE DETAILS.dwg 3/2/2020 3:01:15 PM



NOTE (1): BETWEEN EDGE OF ASPHALT SHOULDER AND TOP OF CONCRETE SLOPE DRAIN, CONST. 4" CONCRETE BASE OVER AREA OUTLINED BY CURBING. COST TO BE INCLUDED IN PRICE BID FOR "CLASS C CONCRETE", PER C.Y.

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

PLAN OF GUARD RAIL WIDENING, CURBING, SLOPE DRAINS AND SPLASH BASINS

NOTE: CURBING, SLOPE DRAINS AND SPLASH BASINS ARE NOT REQUIRED ON THE EAST SIDE OF BRIDGE "F".

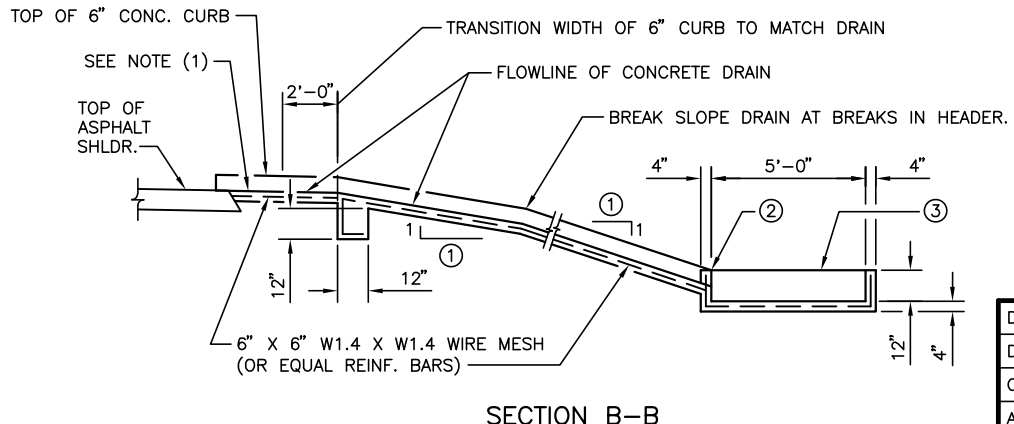
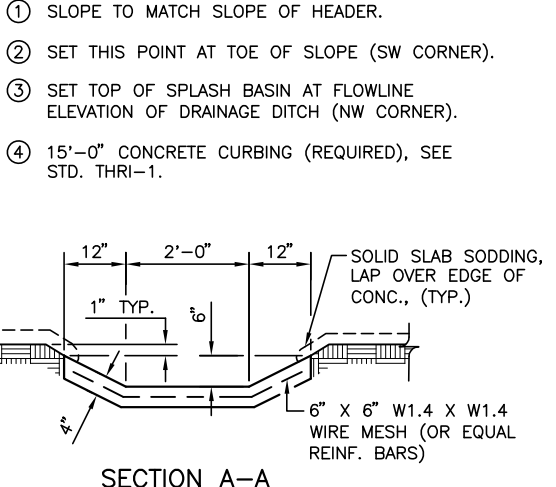
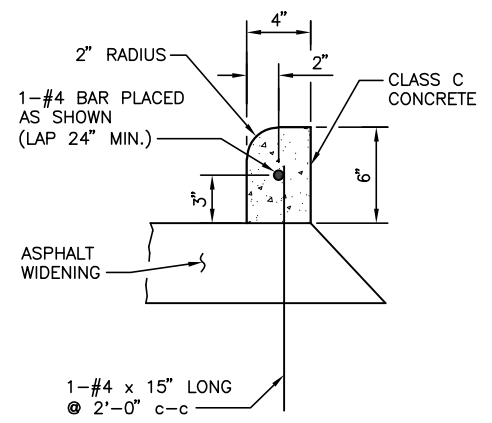
NOTE: SLOPE DRAINS, SPLASH BASINS AND CURBING TO BE LOCATED AS SHOWN ON THE PLAN AND AS DIRECTED BY THE ENGINEER.

NOTES: ASPHALT SHOULDER WIDENING SHALL BE IN ACCORDANCE WITH THE ROADWAY PLANS AND DETAILS SHOWN ON THIS SHEET. ALL COSTS OF ASPHALT SHOULDER WIDENING SHALL BE INCLUDED IN THE ROADWAY PAY ITEMS.

THE SLOPE DRAINS AND SPLASH BASINS SHALL BE CONSTRUCTED USING CLASS "C" CONCRETE AS SHOWN ON THIS SHEET. ALL COSTS OF THE SLOPE DRAINS AND SPLASH BASINS INCLUDING WIRE MESH, EXCAVATIONS AND BACKFILLING SHALL BE INCLUDED IN THE BRIDGE PAY ITEM FOR "CLASS C CONCRETE".

THE 6" CONCRETE CURBS SHALL BE CONSTRUCTED AS SHOWN ON THIS SHEET. ALL COSTS TO CONSTRUCT THE 6" CONCRETE CURBS INCLUDING CONCRETE AND REINFORCING STEEL SHALL BE INCLUDED IN THE BRIDGE PAY ITEM FOR "CLASS C CONCRETE".

PAYMENT FOR THE 15'-0" CONCRETE CURBING IS INCLUDED IN THE ROADWAY PAY ITEMS FOR GUARDRAIL.

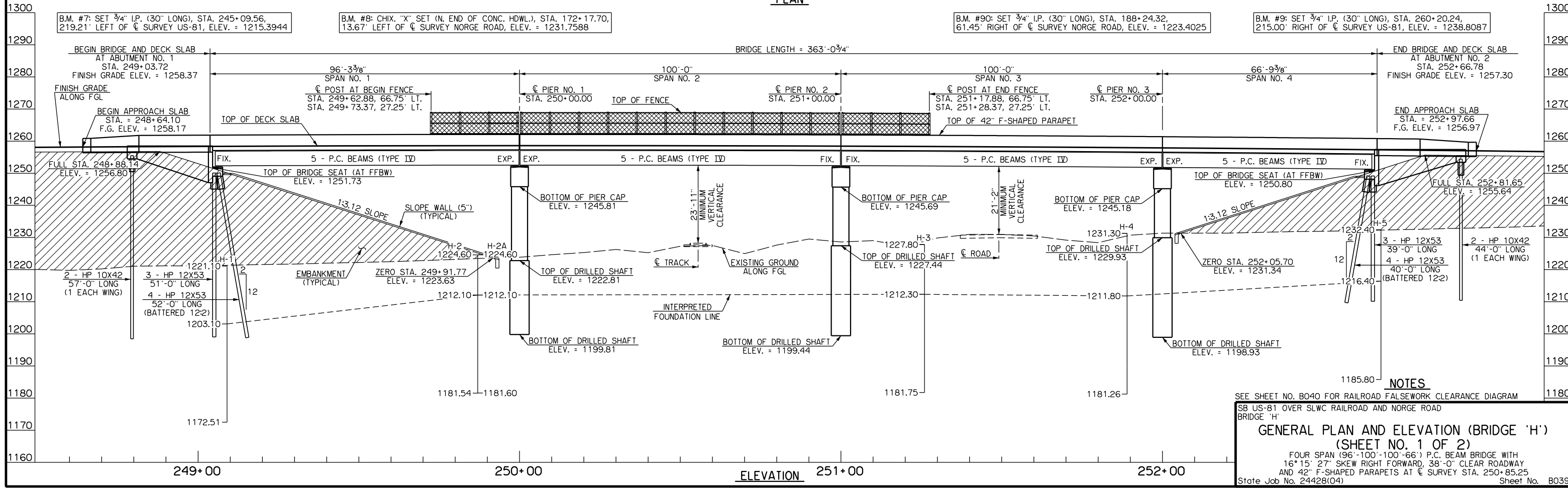
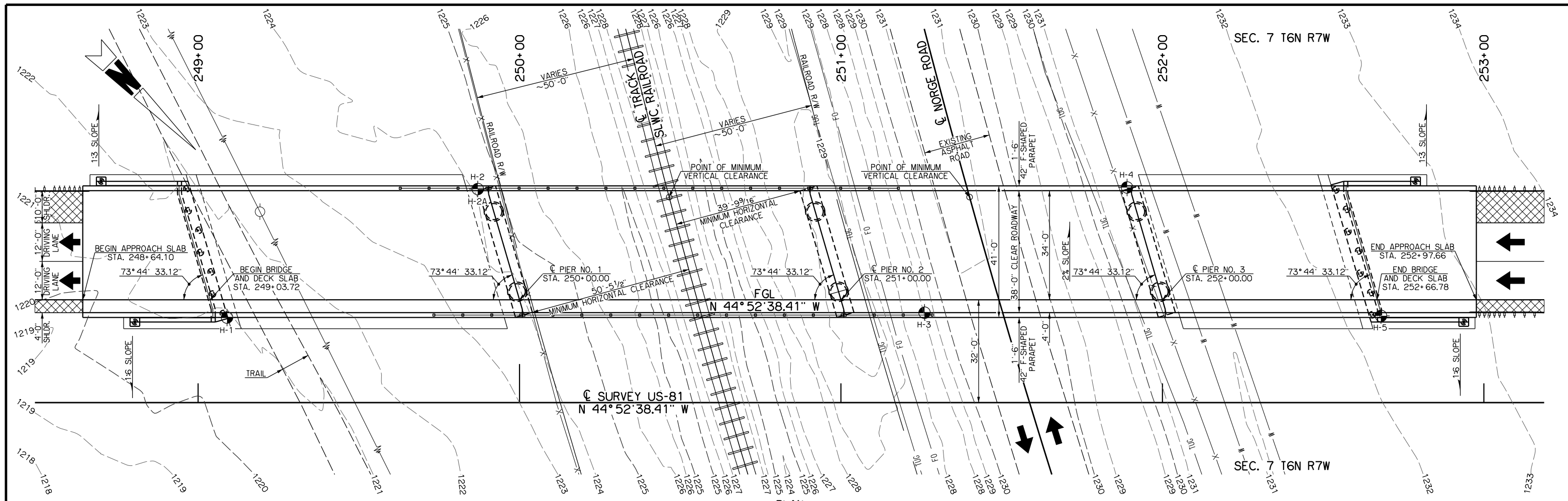


| | | | | |
|----------|-----|-------|--------------------|--------------|
| Design | . | | BRIDGE "F" | GRADY COUNTY |
| Drawn | PKW | 09/19 | SB US-81 OVER I-44 | |
| Checked | . | | | |
| Approved | . | | | |
| Squad | | | | |

DRAIN AT END BRIDGE DETAILS
 @ STA. 196+50.65

Job Piece No. 24428(04) Sheet No. B038

"PRELIMINARY PLANS - NOT FOR CONSTRUCTION"



NOTES

SEE SHEET NO. B040 FOR RAILROAD FALSEWORK CLEARANCE DIAGRAM

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

GENERAL PLAN AND ELEVATION (BRIDGE 'H')
(SHEET NO. 1 OF 2)

FOUR SPAN (96'-100'-100'-66') P.C. BEAM BRIDGE WITH
16° 15' 27" SKEW RIGHT FORWARD, 38'-0" CLEAR ROADWAY
AND 42" F-SHAPED PARAPETS AT C SURVEY STA. 250+85.25

State Job No. 24428(04) Sheet No. B039

| SUMMARY OF QUANTITIES - BRIDGE 'H' | | | | | | | |
|--|------|-----------|-----------|----------------|----------------|------------|------------|
| ITEM | UNIT | ABUTMENTS | PIERS | SUPERSTRUCTURE | APPROACH SLABS | SLOPE WALL | TOTAL |
| SUBSTRUCTURE EXCAVATION COMMON | CY | 190.00 | - | - | - | - | 190.00 |
| CLSM BACKFILL | CY | 287.00 | - | - | - | - | 287.00 |
| PRESTRESSED CONCRETE BEAMS (TYPE II) | LF | - | - | 1,794.17 | - | - | 1,794.17 |
| APPROACH SLAB | SY | - | - | - | 321.20 | - | 321.20 |
| SAW-CUT GROOVING | SY | - | - | 1,514.00 | 274.00 | - | 1,788.00 |
| SEALED EXPANSION JOINT | LF | - | - | 85.00 | - | - | 85.00 |
| 42" F-SHAPED PARAPET | LF | - | - | 756.20 | 141.00 | - | 897.20 |
| STRUCTURAL STEEL | LB | - | - | 1,870.00 | - | - | 1,870.00 |
| STAINLESS STEEL FIXED BEARING ASSEMBLY | EA | - | - | 20.00 | - | - | 20.00 |
| STAINLESS STEEL EXPANSION BEARING ASSEMBLY | EA | - | - | 20.00 | - | - | 20.00 |
| CLASS AA CONCRETE | CY | - | - | 410.00 | - | - | 410.00 |
| CLASS A CONCRETE | CY | 113.40 | 265.10 | - | - | - | 378.50 |
| SLOPE WALL (5') | SY | - | - | - | - | 871.00 | 871.00 |
| REINFORCING STEEL | LB | - | 2,230.00 | - | - | - | 2,230.00 |
| EPOXY COATED REINFORCING STEEL | LB | 11,730.00 | 13,500.00 | 89,500.00 | - | - | 114,730.00 |
| PILES, FURNISHED (HP 10X42) | LF | 202.00 | - | - | - | - | 202.00 |
| PILES, FURNISHED (HP 12X53) | LF | 638.00 | - | - | - | - | 638.00 |
| PILES, DRIVEN (HP 10X42) | LF | 202.00 | - | - | - | - | 202.00 |
| PILES, DRIVEN (HP 12X53) | LF | 638.00 | - | - | - | - | 638.00 |
| WATER REPELLENT (VISUALLY INSPECTED) | SY | 122.00 | 282.00 | 1,778.00 | 70.00 | - | 2,252.00 |
| DRILLED SHAFT 72" DIAMETER | LF | - | 164.00 | - | - | - | 164.00 |
| SEALER CRACK PREPARATION | LF | - | - | 128.00 | - | - | 128.00 |
| SEALER RESIN | GAL | - | - | 1.30 | - | - | 1.30 |
| 6" PERFORATED PIPE UNDERDRAIN ROUND | LF | 96.00 | - | - | - | 98.00 | 194.00 |
| 6" NON-PERF. PIPE UNDERDRAIN RND. | LF | 40.00 | - | - | - | 20.00 | 60.00 |
| OUTLET LATERAL HEADWALL | EA | - | - | - | - | 2.00 | 2.00 |
| FENCE-STYLE CLF (8' HIGH, CLASS A) | LF | - | - | 312.00 | - | - | 312.00 |

STRUCTURAL AND FOUNDATION DESIGN DATA

MATERIAL:
 CLASS A CONCRETE, f'c = 3 KSI
 CLASS AA CONCRETE, f'c = 4 KSI
 REINFORCING STEEL, fy = 60 KSI
 STRUCTURAL STEEL M270 (GRADE 50W), Fy = 50 KSI
 STAINLESS STEEL A240 (TYPE 316), Fy = 30 KSI

LOADING:
 HL-93 OR OKLAHOMA OVERLOAD TRUCK
 20 PSF FUTURE WEARING SURFACE
 5 PSF STAY-IN-PLACE FORMS

DESIGN:
 AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS,
 7TH EDITION WITH 2015 INTERIMS
 ANSI/AASHTO/AWS D1.5 BRIDGE WELDING CODE
 ANSI/AWS D1.6 STRUCTURAL WELDING CODE - STAINLESS STEEL

HL-93 INVENTORY RATING FACTOR: 1.00
 HL-93 OPERATING RATING FACTOR: 1.30

ABUTMENTS (HP 12X53 PILING)

ABUTMENTS NO. 1 2
 MAXIMUM FACTORED PILE LOAD (TONS/PILE) = 108 108

ALL ABUTMENT PILING SHALL BE DRIVEN THROUGH THE COMPACTED FILL. PILING SHALL BE DRIVEN TO POINT BEARING ON SOLID FOUNDATION MATERIAL AT THE APPROXIMATE ELEVATION SHOWN IN THE PLANS. IF THE MAXIMUM FACTORED PILE LOAD IS NOT OBTAINED AT THIS ELEVATION, DRIVING SHALL CONTINUE UNTIL THE MAXIMUM FACTORED PILE LOAD IS OBTAINED. THE LENGTH OF STEEL PILING SHOWN IN THE PLANS IS FOR ESTIMATING PURPOSES ONLY.

PIERS

| PIER NO. | 1 | 2 | 3 |
|---|---------|-------|-------|
| DRILLED SHAFTS DIAMETER (INCHES) | = 72 | 72 | 72 |
| FACTORED REACTION (TONS/SHAFT) | = 702 | 720 | 655 |
| NOMINAL UNIT BEARING RESISTANCE (TSF) | = 60.0 | 60.0 | 60.0 |
| BEARING RESISTANCE FACTOR | = 0.70 | 0.70 | 0.70 |
| FACTORED BEARING RESISTANCE (TONS/SHAFT) | = 1,187 | 1,187 | 1,187 |
| NOMINAL UNIT FRICTION RESISTANCE (TSF) | = 2.2 | 2.2 | 2.2 |
| FRICTION RESISTANCE FACTOR | = 0.45 | 0.45 | 0.45 |
| FACTORED FRICTION RESISTANCE (TONS/SHAFT) | = 534 | 534 | 534 |
| DEPTH OF ROCK NEGLECTED FOR FRICTION (FEET) | = 5 | 5 | 5 |
| TOTAL FACTORED RESISTANCE (TONS/SHAFT) | = 1,721 | 1,721 | 1,721 |

REQUIRED STANDARD DRAWINGS

| ROADWAY | BRIDGE |
|----------|---------------|
| LECS-4-1 | FSHP-42-2-00E |
| LTU-4-0 | EJ-SK-04E |
| PED-3-2 | EJ-DTL-02E |
| PUD-3-2 | HP1-2-01E |

UTILITIES

TELEPHONE:
 CHICKASAW TELEPHONE CO. - (580) 618-5455
 SOUTHWESTERN BELL - (800) 522-6543
 AT&T - (800) 778-9140
 DOBSON TECHNOLOGIES - (800) 778-9140
 INTELLEQ COMMUNICATIONS - (800) 335-4343
 MEDICINE PARK TELEPHONE CO. - (580) 529-2700

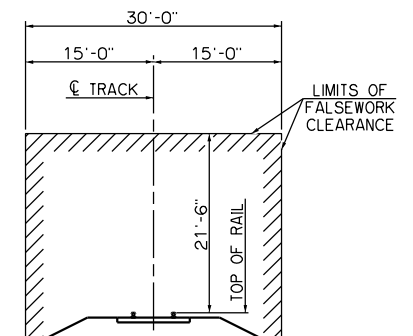
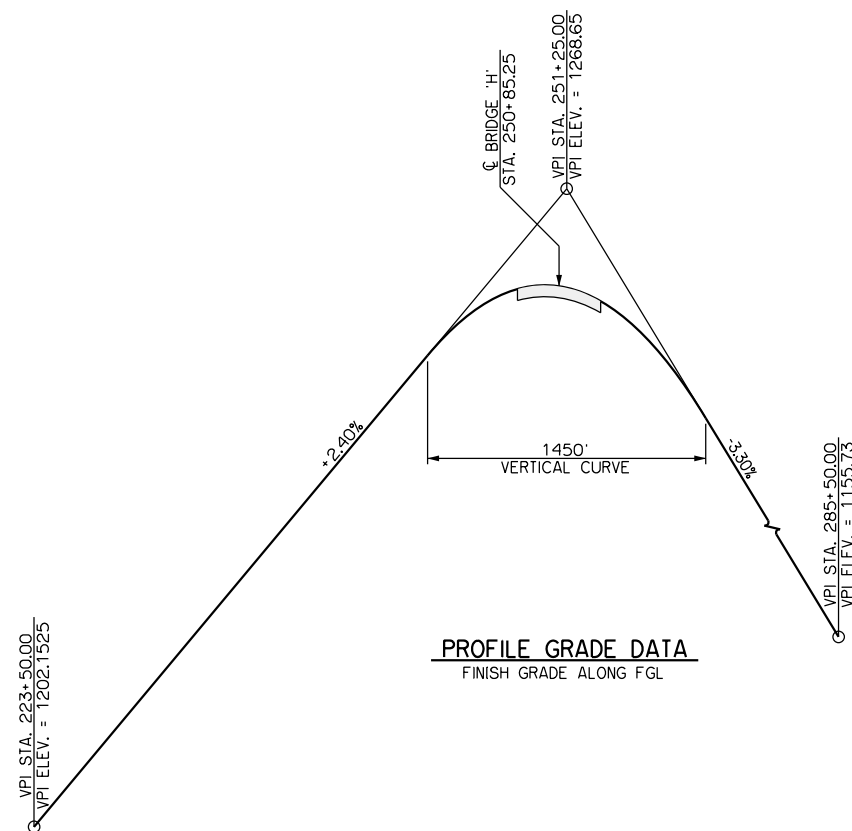
ELECTRIC:
 AEP PUBLIC SERVICE CO. OF OKLAHOMA - (888) 216-3523
 OKLAHOMA ELECTRIC COOPERATIVE - (405) 321-2024

WATER AND SANITARY SEWER:
 CITY OF CHICKASHA PUBLIC WORKS - (405) 222-6080
 RURAL WATER DISTRICT #6 - (405) 459-6626
 RURAL WATER DISTRICT #7 - (405) 779-6224

GAS AND PETROLEUM PIPELINES:
 ENABLE MIDSTREAM - (800) 522-8048
 CONTINUUM ENERGY - (877) 587-0026
 DCP MIDSTREAM - (800) 435-1679
 UNIT PETROLEUM - (918) 493-7700
 SUNOCO LOGISTICS - (800) 753-5531
 KEPCO OPERATING INC. - (855) 421-2088

INDEX OF SHEETS

| SHEET NO. | SHEET DESCRIPTION |
|------------|---|
| AB02, AB03 | PAY QUANTITIES AND GENERAL NOTES |
| B039, B040 | GENERAL PLAN AND ELEVATION (BRIDGE 'H') |
| B041- B043 | FOUNDATION BORING LOGS |
| B044 | SUBSTRUCTURE STAKING DIAGRAM |
| B045 | DETAILS OF GRADING, SUBSTRUCTURE EXCAVATION AND PIPE UNDERDRAIN ASSEMBLY AT ABUTMENTS |
| B046, B047 | DETAILS OF ABUTMENT NO. 1 |
| B048, B049 | DETAILS OF ABUTMENT NO. 2 |
| B050 | DETAILS OF WINGS AT ABUTMENT NO. 1 |
| B051 | DETAILS OF WINGS AT ABUTMENT NO. 2 |
| B052- B054 | DETAILS OF PIERS |
| B055- B059 | DETAILS OF SUPERSTRUCTURE |
| B060 | DETAILS OF BEARING ASSEMBLIES |
| B061 | DETAILS OF PRESTRESSED CONCRETE BEAMS (TYPE II) IN SPAN NO. 1 |
| B062 | DETAILS OF PRESTRESSED CONCRETE BEAMS (TYPE II) IN SPAN NOS. 2 AND 3 |
| B063 | DETAILS OF PRESTRESSED CONCRETE BEAMS (TYPE II) IN SPAN NO. 4 |
| B064 | DETAILS OF APPROACH SLABS |
| B065, B066 | LAYOUT OF 42" F-SHAPED PARAPET |
| B067 | DETAILS OF SLOPE WALLS |
| B068 | DETAILS OF FENCE ON CONCRETE PARAPETS |



RAILROAD FALSEWORK CLEARANCE DIAGRAM

CLEARANCE OF FALSEWORK IS REQUIRED BY RAILROAD FOR OPERATION DURING CONSTRUCTION. NO CONSTRUCTION ACTIVITIES OR OTHER OBSTRUCTIONS SHALL BE PLACED WITHIN THE LIMITS SHOWN. HORIZONTAL DIMENSIONS SHOWN ARE MEASURED AT RIGHT ANGLES TO CENTERLINE TRACK, AND VERTICAL DIMENSION SHOWN IS MEASURED PERPENDICULAR TO PLANE OF THE TOP OF THE RAILS.

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
 BRIDGE 'H'

**GENERAL PLAN AND ELEVATION (BRIDGE 'H')
 (SHEET NO. 2 OF 2)**

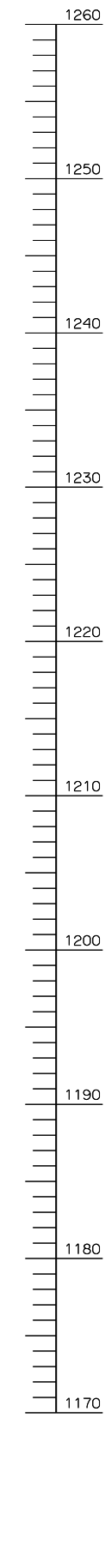
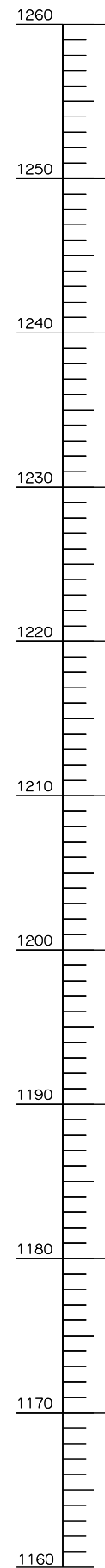
FOUR SPAN (96'-100'-100'-66') P.C. BEAM BRIDGE WITH
 16' 15' 27" SKEW RIGHT FORWARD, 38'-0" CLEAR ROADWAY
 AND 42" F-SHAPED PARAPETS AT \bar{C} SURVEY STA. 250+85.25
 State Job No. 24428(04) Sheet No. B040

BORING NO. H-1

STATION 249+09.0, 26.5' LEFT OF CL SURVEY US-81
(AUGUST 21, 2019)

BORING NO. H-2

STATION 249+87.0, 66.5' LEFT OF CL SURVEY US-81
(AUGUST 20, 2019)



LEGEND

- DCD = DIAMOND CORE DRILLING, ASTM D2113-83
- SS = SPLIT SPOON SAMPLER
- N = NUMBER OF BLOWS PER 12 INCHES
- MC = MOISTURE CONTENT
- LL = LIQUID LIMIT (NV=NO VALUE)
- PI = PLASTICITY INDEX (NP=NO PLASTICITY)
- #200= PERCENT PASSING #200 SIEVE
- UCS = UNCONFINED COMPRESSIVE STRENGTH
- TCP = TEXAS CONE PENETROMETER
- RQD = ROCK QUALITY DESIGNATION
- NP = NON-PLASTIC
- PCF = POUNDS PER CUBIC FOOT
- PSI = POUNDS PER SQUARE INCH
- ▽ = WATER LEVEL WHILE DRILLING OR SAMPLING
- ▼ = WATER LEVEL AFTER DRILLING
- ▽ = WATER LEVEL 24 HOURS AFTER DRILLING
- //// = TOP OF ROCK

NOTE: TOP OF ROCK LINE SHOWN FOR ESTIMATING PURPOSES ONLY.

NOTE: WATER LEVEL ELEVATION SHOWN WERE OBTAINED AT THE TIME THE BORINGS WERE DRILLED AND MAY FLUCTUATE THROUGHOUT THE YEAR.

NOTE: ROCK CLASSIFICATION IS BASED ON DRILLING CHARACTERISTICS AND VISUAL OBSERVATION OF ROCK CORE SAMPLES. PETROGRAPHIC ANALYSIS OF THIN SECTIONS OF THE ROCK CORE SAMPLES MAY REVEAL OTHER TYPES.

SITE GEOLOGY

THE SUBJECT BRIDGE IS SITUATED IN A GEOLOGIC AREA BEST DESCRIBED AS BEING PART OF THE DOG CREEK-BLAINE SUBUNIT (PDB) UNDIFFERENTIATED. ACCORDING TO PUBLISHED MATERIALS (ENGINEERING CLASSIFICATION OF GEOLOGIC MATERIALS, DIVISION SEVEN, 1969, OKLAHOMA HIGHWAY DEPARTMENT), THE DOG CREEK-BLAINE SUBUNIT CONSISTS OF DARK RED SHALES INTERBEDDED WITH MINOR AMOUNTS OF FINE-GRAINED GYPSIFEROUS SANDSTONES THAT LOCALLY GRADE INTO PURE GYPSUM. MUDSTONE CONGLOMERATES A FEW FEET IN THICKNESS OCCUR SPARINGLY WITHIN THE STRATA.

GEOTECHNICAL REPORT

ALL GEOTECHNICAL INFORMATION CONTAINED ON THIS SHEET IS COVERED BY THE ENGINEERING SEAL AFFIXED TO AN ORIGINAL GEOTECHNICAL ENGINEERING REPORT THAT HAS BEEN STAMPED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN OKLAHOMA. TO OBTAIN A COPY OF THE COMPLETE REPORT, CONTACT THE ODOT OFFICE ENGINEER. THE CONTRACTOR SHOULD BE FULLY AWARE OF THE SITE CONDITIONS PRIOR TO BEGINNING WORK. ANY ADDITIONAL GEOTECHNICAL INFORMATION WHICH MAY BE DESIRED IS THE RESPONSIBILITY OF THE CONTRACTOR.

SURFACE ELEVATION - 1221.10

MEDIUM STIFF, RED-BROWN, LEAN CLAY

STIFF, RED-BROWN, LEAN CLAY WITH SAND

HARD, RED-BROWN, SILTY, WEATHERED SHALE

BOTTOM OF BORING - 1172.51

| | |
|---------|---|
| 1216.10 | SS-1; N = 9; MC = 14%; RECOVERY = 18" LL = 30; PI = 13; #200 = 87.2% |
| 1211.10 | SS-2; N = 6; MC = 18.9%; RECOVERY = 18" LL = 26; PI = 8; #200 = 86.3% |
| 1206.10 | SS-3; N = 12; MC = 22.4%; RECOVERY = 18" LL = 26; PI = 10; #200 = 77.6% |
| 1203.60 | SS-4; N = 25/6.0"; 50/6.0"; MC = 12.2%; RECOVERY = 12" LL = 25; PI = 9; #200 = 88.9% |
| 1202.60 | TCP-1; TCP = 50/1.25"; 50/0.50" |
| 1197.60 | TCP-2; TCP = 50/1.00"; 50/0.38" |
| 1192.60 | TCP-3; TCP = 50/1.13"; 50/0.44" |
| 1187.60 | TCP-4; TCP = 50/1.50"; 50/0.63" |
| 1182.60 | TCP-5; TCP = 50/1.25"; 50/0.63" |
| 1177.60 | TCP-6; TCP = 50/1.13"; 50/0.38" |
| 1172.60 | TCP-7; TCP = 50/0.88"; 50/0.25" |

SURFACE ELEVATION - 1224.60

RED-BROWN, LEAN CLAY

MODERATELY HARD, RED-BROWN, SILTY, WEATHERED SHALE

HARD, RED-BROWN, SILTY, WEATHERED SHALE

BOTTOM OF BORING - 1181.54

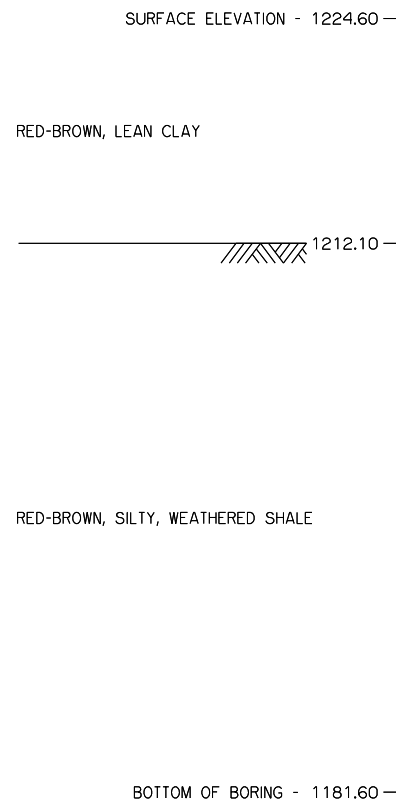
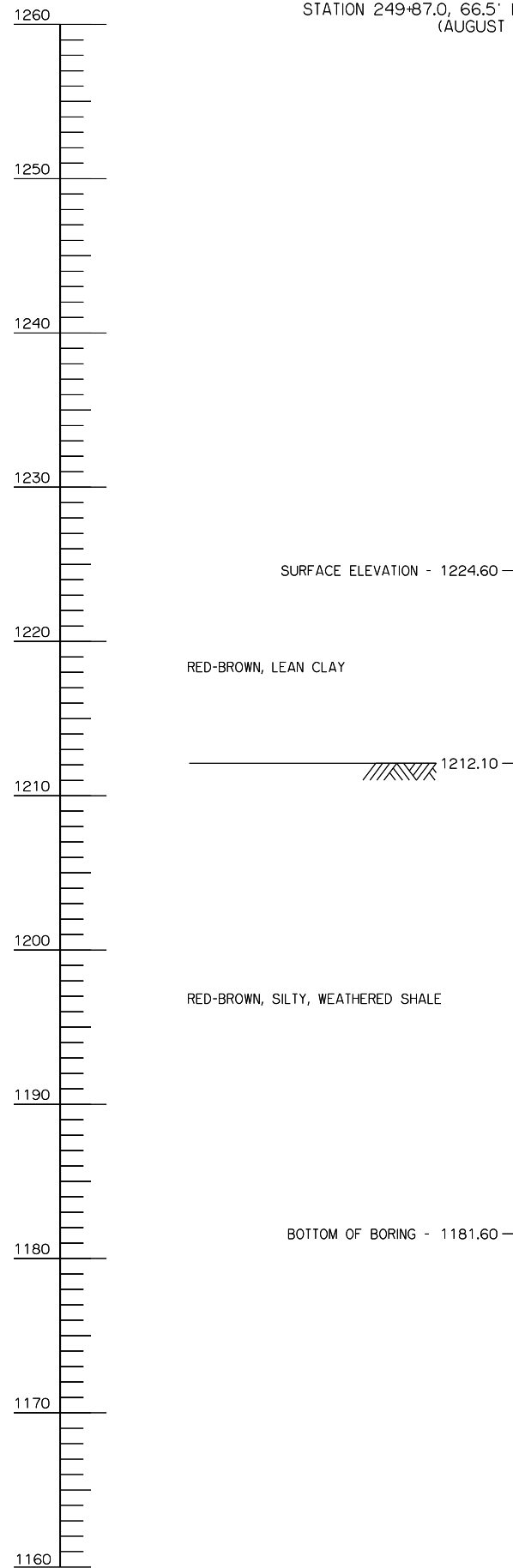
| | |
|---------|---|
| 1213.10 | SS-1; N = 8/6.0"; 16/6.0"; 50/6.0"; MC = 11.5%; RECOVERY = 18"; LL = 32; PI = 16; #200 = 85.7% |
| 1211.60 | TCP-1; TCP = 50/2.13"; 50/1.25" |
| 1206.60 | TCP-2; TCP = 50/1.50"; 50/0.63" |
| 1201.60 | TCP-3; TCP = 50/0.50"; 50/0.25" |
| 1196.60 | TCP-4; TCP = 50/0.88"; 50/0.38" |
| 1191.60 | TCP-5; TCP = 50/0.63"; 50/0.25" |
| 1186.60 | TCP-6; TCP = 50/1.25"; 50/0.50" |
| 1181.60 | TCP-7; TCP = 50/0.50"; 50/0.25" |

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

**FOUNDATION BORING LOGS
(SHEET NO. 1 OF 3)**

BORING NO. H-2A

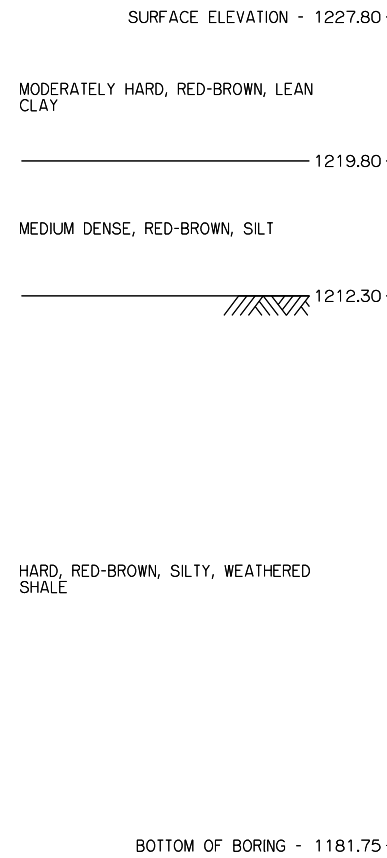
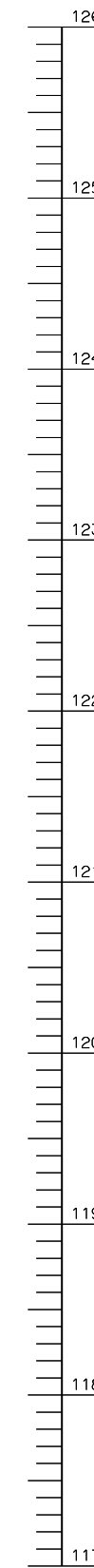
STATION 249+87.0, 66.5' LEFT OF CL SURVEY US-81
(AUGUST 21, 2019)



| | |
|---------|--|
| 1211.60 | DCD-1; RECOVERY = 2"; RQD = 0% |
| 1206.60 | DCD-2; RECOVERY = 55"; RQD = 40%; MC = 7.4%; DRY DENSITY = 121 PCF; UCS = 432 PSI |
| 1201.60 | DCD-3; RECOVERY = 50"; RQD = 24%; MC = 13.0%; DRY DENSITY = 126 PCF; UCS = 126 PSI |
| 1196.60 | DCD-4; RECOVERY = 42"; RQD = 33%; MC = 9.0%; DRY DENSITY = 131 PCF; UCS = 127 PSI |
| 1191.60 | DCD-5; RECOVERY = 2"; RQD = 0% |
| 1186.60 | DCD-6; RECOVERY = 2"; RQD = 0% |

BORING NO. H-3

STATION 251+26.0, 28.0' LEFT OF CL SURVEY US-81
(AUGUST 19, 2019)



| | |
|---------|--|
| 1223.30 | SS-1; N = 8; MC = 17%; RECOVERY = 18"; LL = 32; PI = 14; #200 = 92.5% |
| 1218.30 | SS-2; N = 14; MC = 14.5%; RECOVERY = 18"; LL = NP; PI = NP; #200 = 85.3% |
| 1213.30 | SS-3; N = 17/6.0"; 23/6.0"; 50/5.0"; RECOVERY = 0" |
| 1211.88 | TCP-1; TCP = 50/1.88"; 50/1.00" |
| 1206.88 | TCP-2; TCP = 50/0.50"; 50/0.25" |
| 1201.88 | TCP-3; TCP = 50/1.13"; 50/0.63" |
| 1196.88 | TCP-4; TCP = 50/0.75"; 50/0.25" |
| 1191.88 | TCP-5; TCP = 50/0.50"; 50/0.25" |
| 1186.88 | TCP-6; TCP = 50/0.50"; 50/0.13" |
| 1181.88 | TCP-7; TCP = 50/1.00"; 50/0.50" |

LEGEND

- DCD = DIAMOND CORE DRILLING, ASTM D2113-83
- SS = SPLIT SPOON SAMPLER
- N = NUMBER OF BLOWS PER 12 INCHES
- MC = MOISTURE CONTENT
- LL = LIQUID LIMIT (NV=NO VALUE)
- PI = PLASTICITY INDEX (NP=NO PLASTICITY)
- #200= PERCENT PASSING #200 SIEVE
- UCS = UNCONFINED COMPRESSIVE STRENGTH
- TCP = TEXAS CONE PENETROMETER
- RQD = ROCK QUALITY DESIGNATION
- NP = NON-PLASTIC
- PCF = POUNDS PER CUBIC FOOT
- PSI = POUNDS PER SQUARE INCH
- ▽ = WATER LEVEL WHILE DRILLING OR SAMPLING
- ▼ = WATER LEVEL AFTER DRILLING
- ▽ = WATER LEVEL 24 HOURS AFTER DRILLING
- ▨ = TOP OF ROCK

NOTE: TOP OF ROCK LINE SHOWN FOR ESTIMATING PURPOSES ONLY.

NOTE: WATER LEVEL ELEVATION SHOWN WERE OBTAINED AT THE TIME THE BORINGS WERE DRILLED AND MAY FLUCTUATE THROUGHOUT THE YEAR.

NOTE: ROCK CLASSIFICATION IS BASED ON DRILLING CHARACTERISTICS AND VISUAL OBSERVATION OF ROCK CORE SAMPLES. PETROGRAPHIC ANALYSIS OF THIN SECTIONS OF THE ROCK CORE SAMPLES MAY REVEAL OTHER TYPES.

SITE GEOLOGY

THE SUBJECT BRIDGE IS SITUATED IN A GEOLOGIC AREA BEST DESCRIBED AS BEING PART OF THE DOG CREEK-BLAINE SUBUNIT (PDB) UNDIFFERENTIATED. ACCORDING TO PUBLISHED MATERIALS (ENGINEERING CLASSIFICATION OF GEOLOGIC MATERIALS, DIVISION SEVEN, 1969, OKLAHOMA HIGHWAY DEPARTMENT), THE DOG CREEK-BLAINE SUBUNIT CONSISTS OF DARK RED SHALES INTERBEDDED WITH MINOR AMOUNTS OF FINE-GRAINED GYPSIFEROUS SANDSTONES THAT LOCALLY GRADE INTO PURE GYPSUM. MUDSTONE CONGLOMERATES A FEW FEET IN THICKNESS OCCUR SPARINGLY WITHIN THE STRATA.

GEOTECHNICAL REPORT

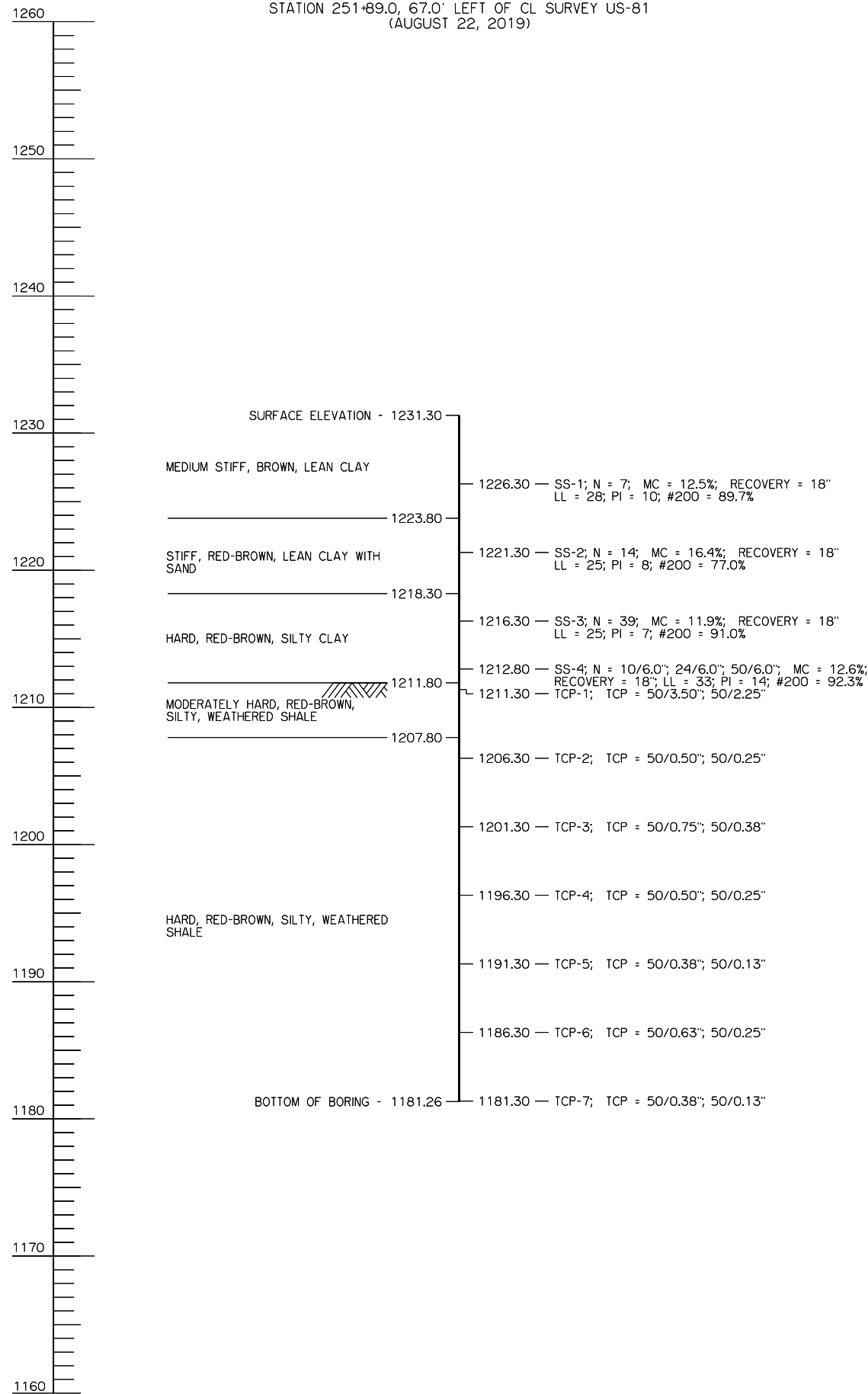
ALL GEOTECHNICAL INFORMATION CONTAINED ON THIS SHEET IS COVERED BY THE ENGINEERING SEAL AFFIXED TO AN ORIGINAL GEOTECHNICAL ENGINEERING REPORT THAT HAS BEEN STAMPED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN OKLAHOMA. TO OBTAIN A COPY OF THE COMPLETE REPORT, CONTACT THE ODOT OFFICE ENGINEER. THE CONTRACTOR SHOULD BE FULLY AWARE OF THE SITE CONDITIONS PRIOR TO BEGINNING WORK. ANY ADDITIONAL GEOTECHNICAL INFORMATION WHICH MAY BE DESIRED IS THE RESPONSIBILITY OF THE CONTRACTOR.

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

FOUNDATION BORING LOGS
(SHEET NO. 2 OF 3)

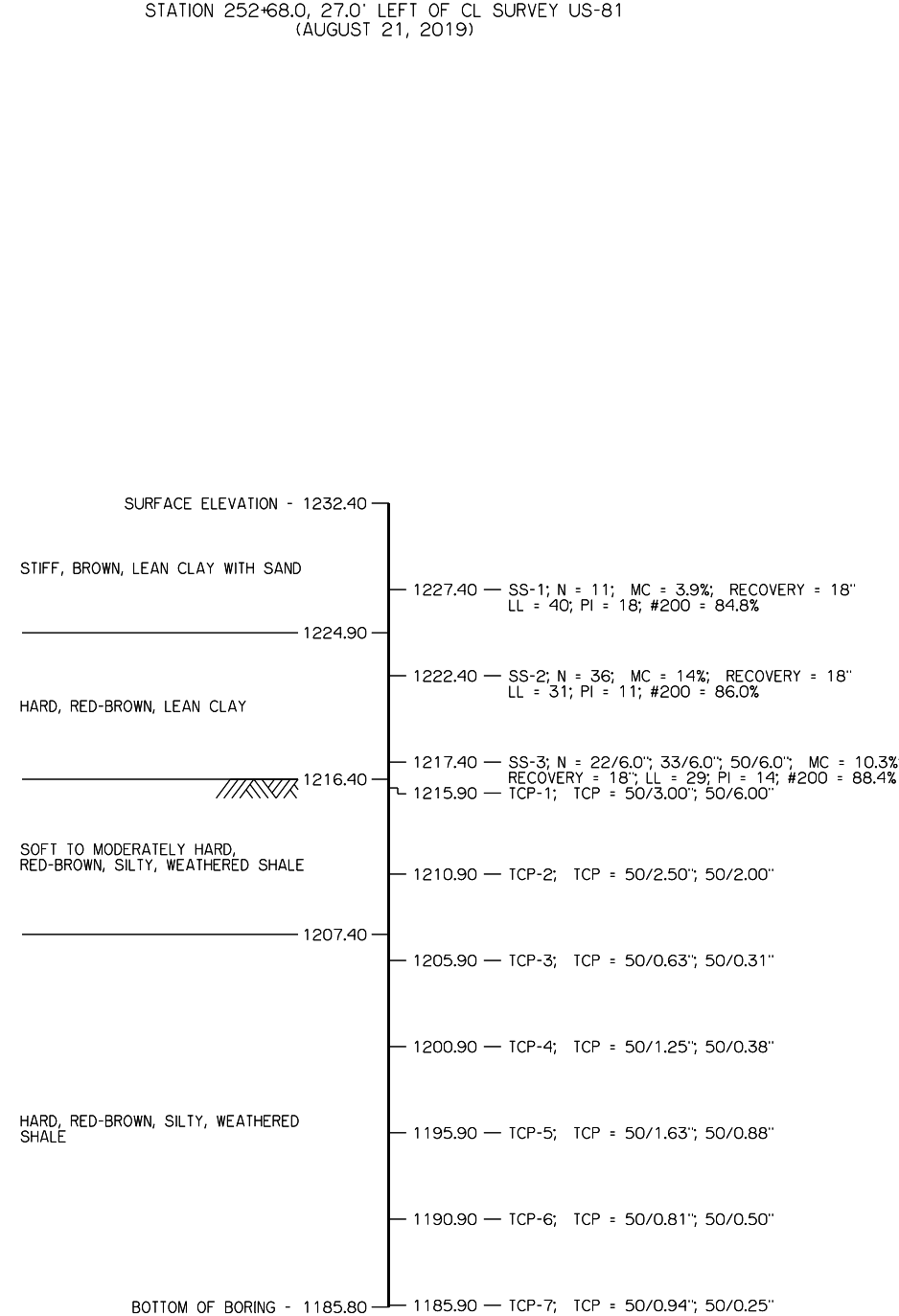
BORING NO. H-4

STATION 251+89.0, 67.0' LEFT OF CL SURVEY US-81
(AUGUST 22, 2019)



BORING NO. H-5

STATION 252+68.0, 27.0' LEFT OF CL SURVEY US-81
(AUGUST 21, 2019)



LEGEND

- DCD = DIAMOND CORE DRILLING, ASTM D2113-83
- SS = SPLIT SPOON SAMPLER
- N = NUMBER OF BLOWS PER 12 INCHES
- MC = MOISTURE CONTENT
- LL = LIQUID LIMIT (NV=NO VALUE)
- PI = PLASTICITY INDEX (NP=NO PLASTICITY)
- #200 = PERCENT PASSING #200 SIEVE
- UCS = UNCONFINED COMPRESSIVE STRENGTH
- TCP = TEXAS CONE PENETROMETER
- RQD = ROCK QUALITY DESIGNATION
- NP = NON-PLASTIC
- PCF = POUNDS PER CUBIC FOOT
- PSI = POUNDS PER SQUARE INCH
- ▽ = WATER LEVEL WHILE DRILLING OR SAMPLING
- ▼ = WATER LEVEL AFTER DRILLING
- ▽ = WATER LEVEL 24 HOURS AFTER DRILLING

▨ = TOP OF ROCK

NOTE: TOP OF ROCK LINE SHOWN FOR ESTIMATING PURPOSES ONLY.

NOTE: WATER LEVEL ELEVATION SHOWN WERE OBTAINED AT THE TIME THE BORINGS WERE DRILLED AND MAY FLUCTUATE THROUGHOUT THE YEAR.

NOTE: ROCK CLASSIFICATION IS BASED ON DRILLING CHARACTERISTICS AND VISUAL OBSERVATION OF ROCK CORE SAMPLES. PETROGRAPHIC ANALYSIS OF THIN SECTIONS OF THE ROCK CORE SAMPLES MAY REVEAL OTHER TYPES.

SITE GEOLOGY

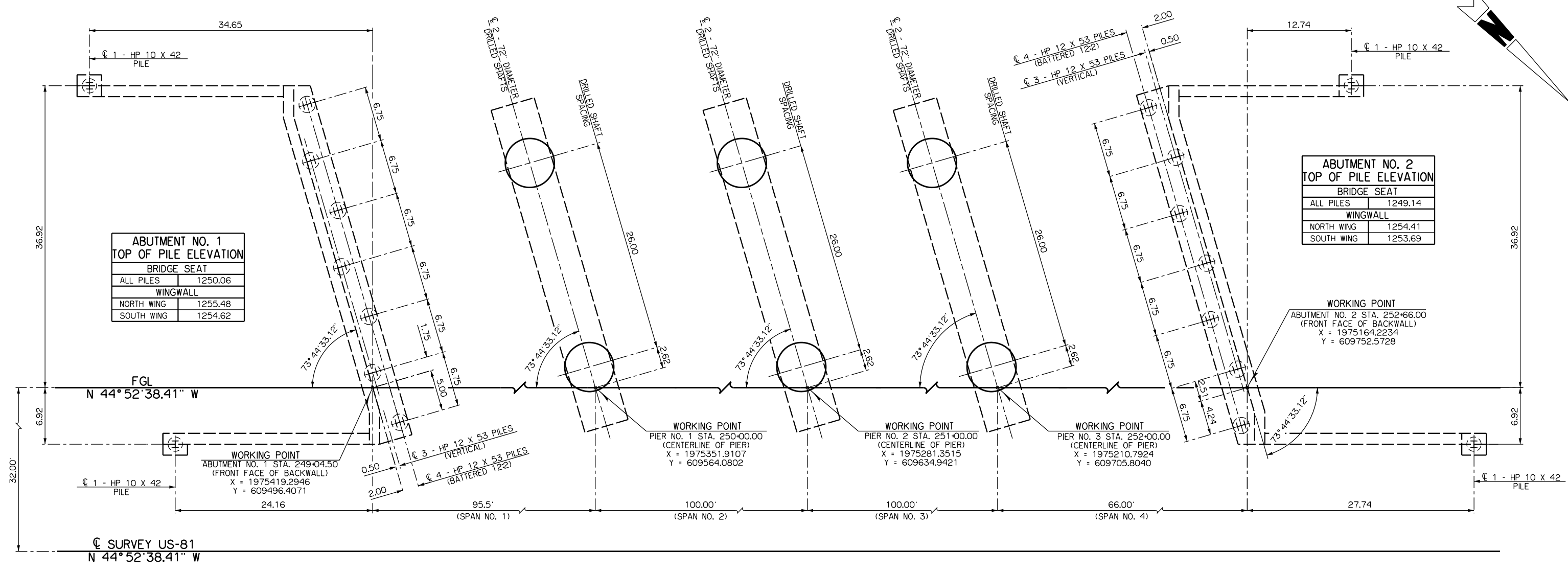
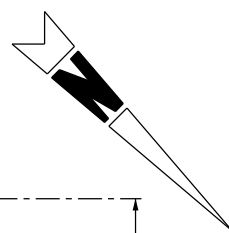
THE SUBJECT BRIDGE IS SITUATED IN A GEOLOGIC AREA BEST DESCRIBED AS BEING PART OF THE DOG CREEK-BLAINE SUBUNIT (PDB) UNDIFFERENTIATED. ACCORDING TO PUBLISHED MATERIALS (ENGINEERING CLASSIFICATION OF GEOLOGIC MATERIALS, DIVISION SEVEN, 1969, OKLAHOMA HIGHWAY DEPARTMENT), THE DOG CREEK-BLAINE SUBUNIT CONSISTS OF DARK RED SHALES INTERBEDDED WITH MINOR AMOUNTS OF FINE-GRAINED GYPSIFEROUS SANDSTONES THAT LOCALLY GRADE INTO PURE GYPSUM. MUDSTONE CONGLOMERATES A FEW FEET IN THICKNESS OCCUR SPARINGLY WITHIN THE STRATA.

GEOTECHNICAL REPORT

ALL GEOTECHNICAL INFORMATION CONTAINED ON THIS SHEET IS COVERED BY THE ENGINEERING SEAL AFFIXED TO AN ORIGINAL GEOTECHNICAL ENGINEERING REPORT THAT HAS BEEN STAMPED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN OKLAHOMA. TO OBTAIN A COPY OF THE COMPLETE REPORT, CONTACT THE ODOT OFFICE ENGINEER. THE CONTRACTOR SHOULD BE FULLY AWARE OF THE SITE CONDITIONS PRIOR TO BEGINNING WORK. ANY ADDITIONAL GEOTECHNICAL INFORMATION WHICH MAY BE DESIRED IS THE RESPONSIBILITY OF THE CONTRACTOR.

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

**FOUNDATION BORING LOGS
(SHEET NO. 3 OF 3)**



**ABUTMENT NO. 1
TOP OF PILE ELEVATION**

| BRIDGE SEAT | |
|-------------|---------|
| ALL PILES | 1250.06 |
| WINGWALL | |
| NORTH WING | 1255.48 |
| SOUTH WING | 1254.62 |

**ABUTMENT NO. 2
TOP OF PILE ELEVATION**

| BRIDGE SEAT | |
|-------------|---------|
| ALL PILES | 1249.14 |
| WINGWALL | |
| NORTH WING | 1254.41 |
| SOUTH WING | 1253.69 |

WORKING POINT
 ABUTMENT NO. 2 STA. 252+66.00
 (FRONT FACE OF BACKWALL)
 X = 1975164.2234
 Y = 609752.5728

WORKING POINT
 PIER NO. 1 STA. 250+00.00
 (CENTERLINE OF PIER)
 X = 1975351.9107
 Y = 609564.0802

WORKING POINT
 PIER NO. 2 STA. 251+00.00
 (CENTERLINE OF PIER)
 X = 1975281.3515
 Y = 609634.9421

WORKING POINT
 PIER NO. 3 STA. 252+00.00
 (CENTERLINE OF PIER)
 X = 1975210.7924
 Y = 609705.8040

WORKING POINT
 ABUTMENT NO. 1 STA. 249+04.50
 (FRONT FACE OF BACKWALL)
 X = 1975419.2946
 Y = 609496.4071

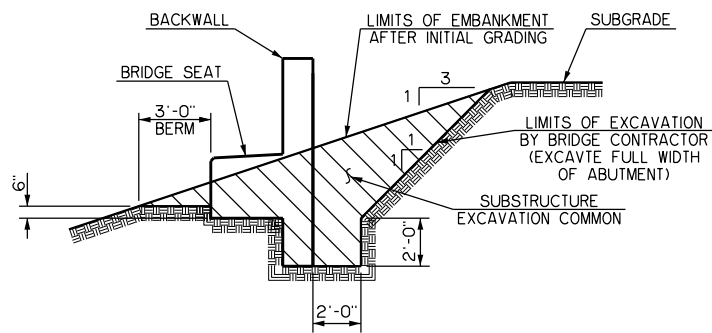
SUBSTRUCTURE STAKING DIAGRAM

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
 BRIDGE 'H'

SUBSTRUCTURE STAKING DIAGRAM

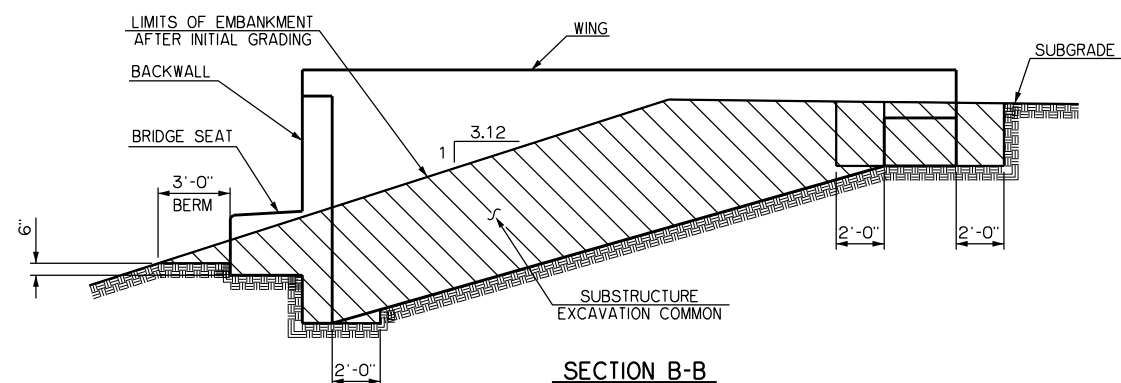
State Job No. 24428(04) Sheet No. B044

US 81 REALIGNMENT GRADY COUNTY



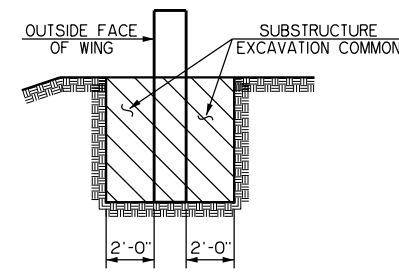
SECTION A-A

NOTE: DIMENSIONS SHOWN ARE MEASURED PERPENDICULAR TO BACK WALL.

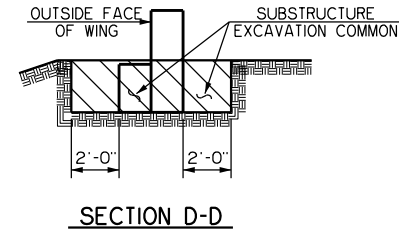


SECTION B-B

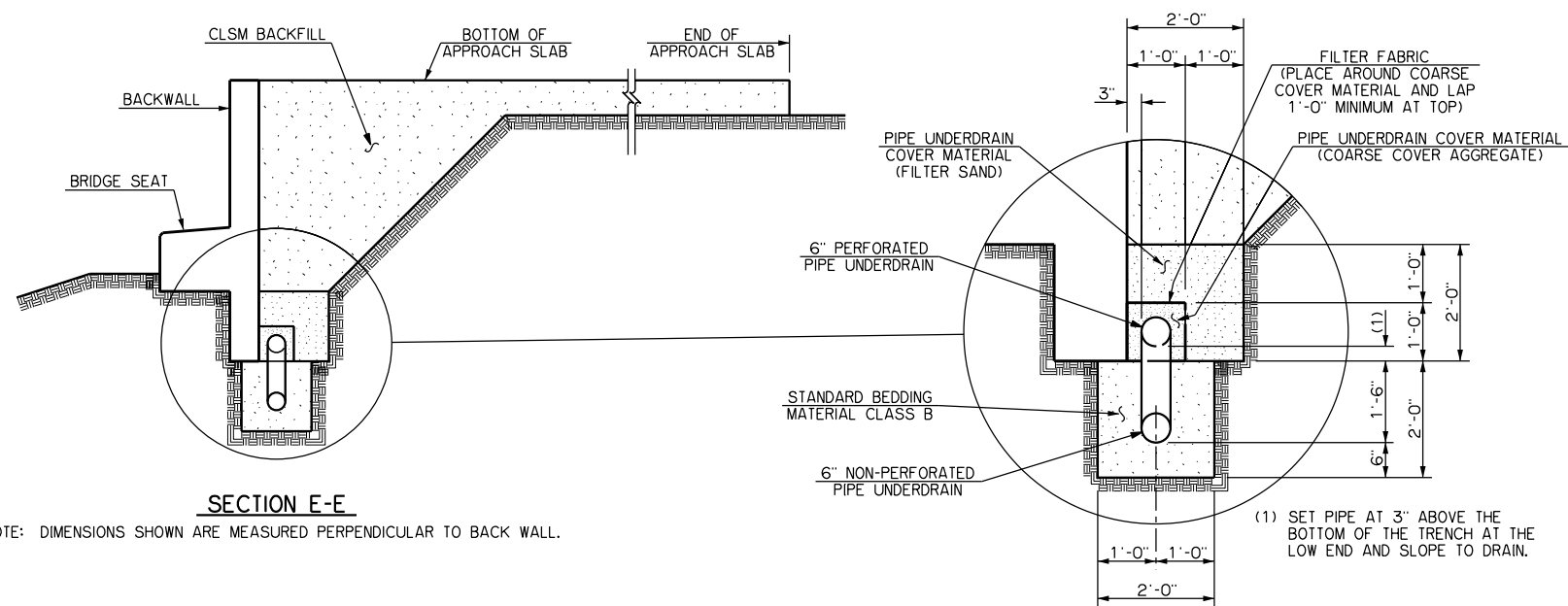
NOTE: DIMENSIONS SHOWN ARE MEASURED ALONG THE C/B BRIDGE.



SECTION C-C



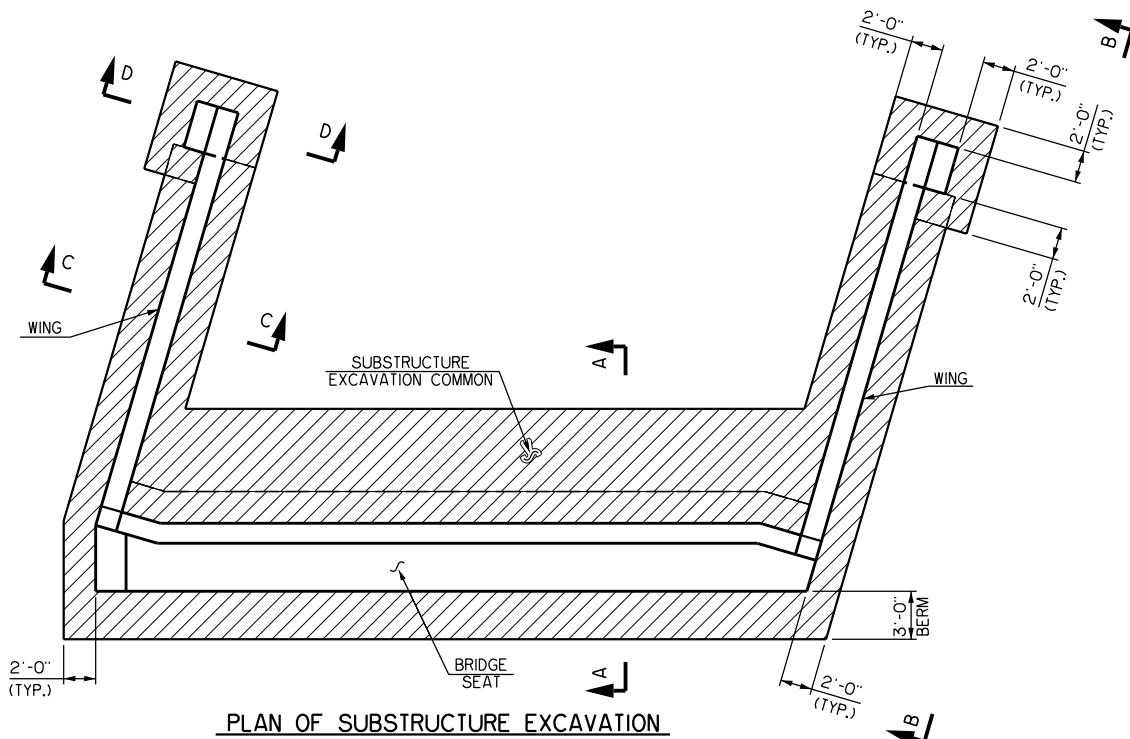
SECTION D-D



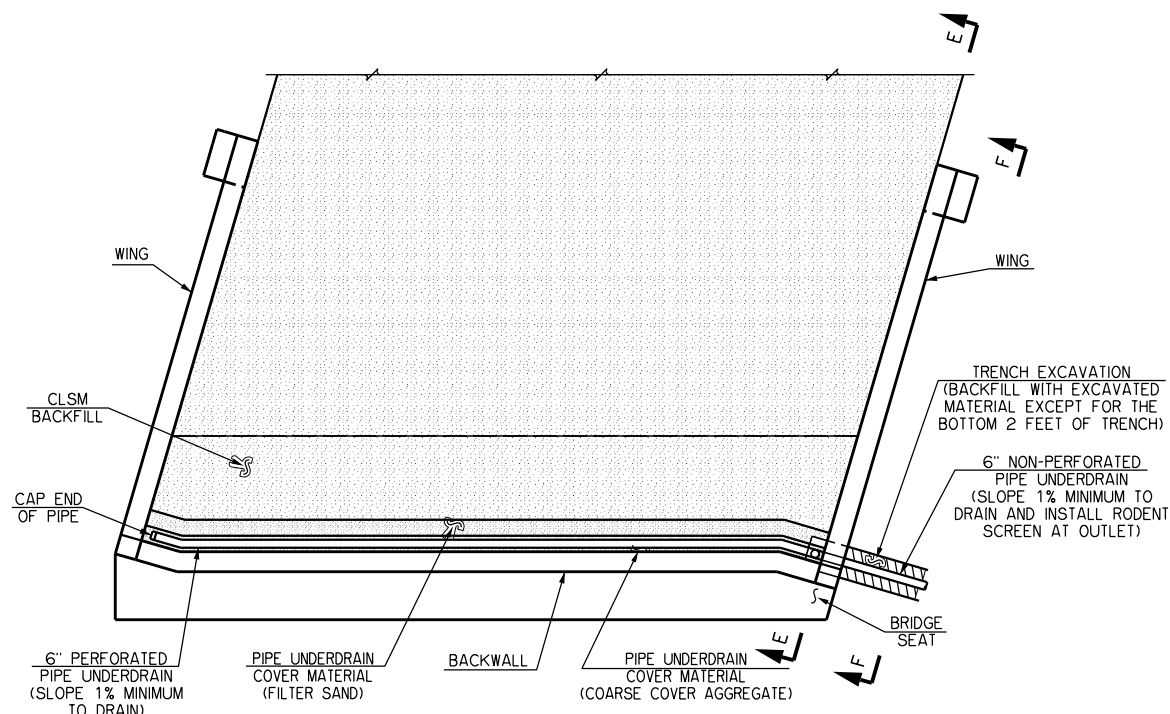
SECTION E-E

NOTE: DIMENSIONS SHOWN ARE MEASURED PERPENDICULAR TO BACK WALL.

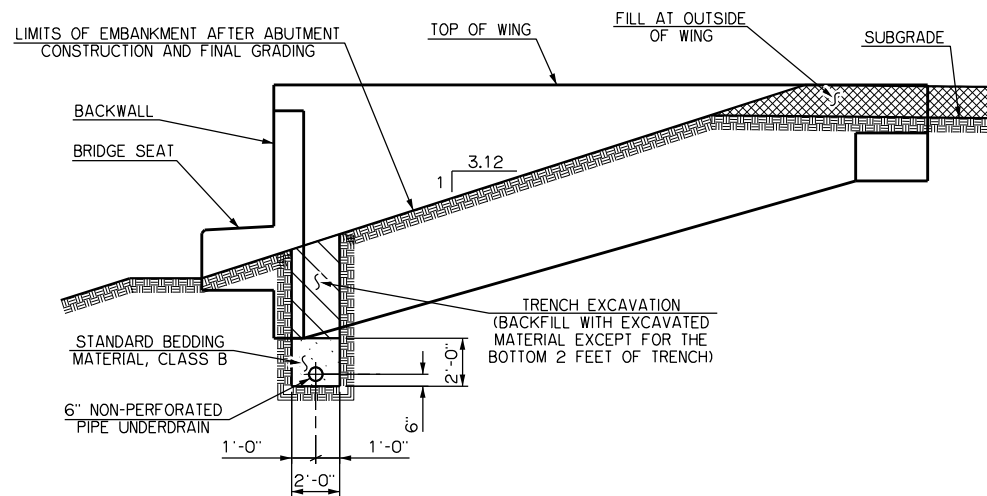
(1) SET PIPE AT 3" ABOVE THE BOTTOM OF THE TRENCH AT THE LOW END AND SLOPE TO DRAIN.



PLAN OF SUBSTRUCTURE EXCAVATION



PLAN OF PIPE UNDERDRAIN



SECTION F-F

NOTE: DIMENSIONS SHOWN ARE MEASURED ALONG THE C/B BRIDGE.

NOTE

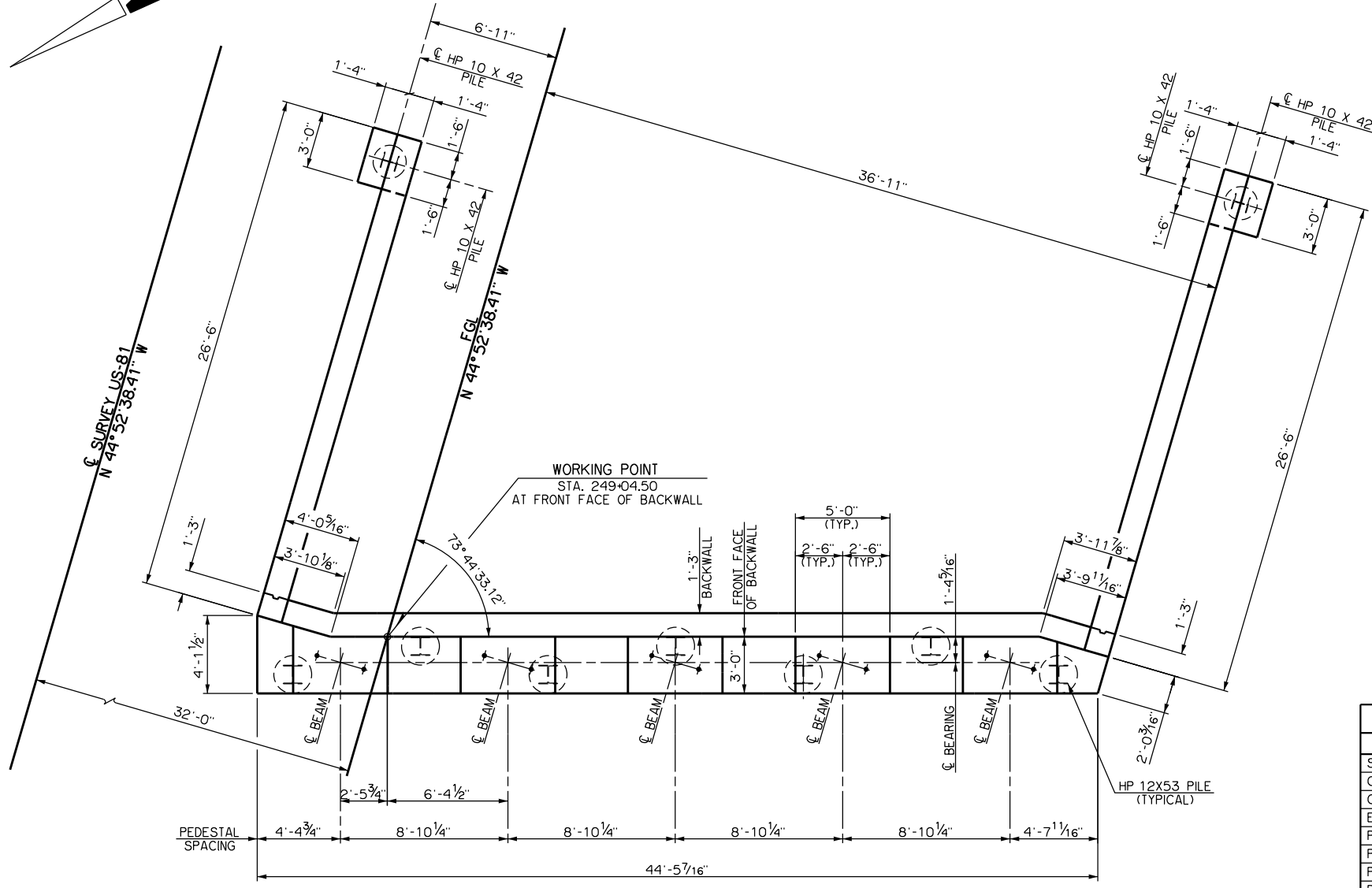
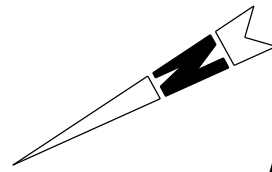
CONCRETE MAY BE PLACED AGAINST THE LIMITS OF EXCAVATION IF THE MATERIAL IS EXCAVATED TO THE NEAT LINES OF THE ABUTMENT AND APPROVED BY THE ENGINEER. IF NECESSARY, FORMS SHALL BE USED ON THE BACK VERTICAL FACE OF THE ABUTMENT AND REMOVED AFTER THE CONCRETE HAS SET. THE MEASUREMENT AND PAYMENT FOR "SUBSTRUCTURE EXCAVATION COMMON" AT THE ABUTMENTS SHALL BE IN ACCORDANCE WITH THE DETAILS SHOWN IN THE PLANS.

CLSM BACKFILL SHALL NOT BE PLACED UNTIL THE CONCRETE IN THE ABUTMENT WINGS HAS ATTAINED A STRENGTH OF 3,000 PSI.

INSTALLATION ON THE PIPE UNDERDRAIN SHALL BE AS SHOWN IN THE PLANS AND ON ROADWAY STANDARD DRAWING PUD-3. THE EXTENT, LOCATION AND DEPTH OF THE 6" NON-PERFORATED PIPE UNDERDRAIN MAY BE ADJUSTED BY THE ENGINEER DURING CONSTRUCTION. ALL COSTS OF THE PERFORATED AND NON-PERFORATED PIPE, PIPE UNDERDRAIN COVER MATERIAL, FILTER FABRIC, TRENCH EXCAVATION, STANDARD BEDDING MATERIAL, PIPE CAPS, RODENT SCREEN, BACKFILLING OF TRENCH EXCAVATION, MATERIAL, LABOR, EQUIPMENT AND INCIDENTALS SHALL BE INCLUDED IN THE UNIT PRICE BID PER LINEAR FOOT OF "6" PERFORATED PIPE UNDERDRAIN" AND "6" NON-PERFORATED PIPE UNDERDRAIN.

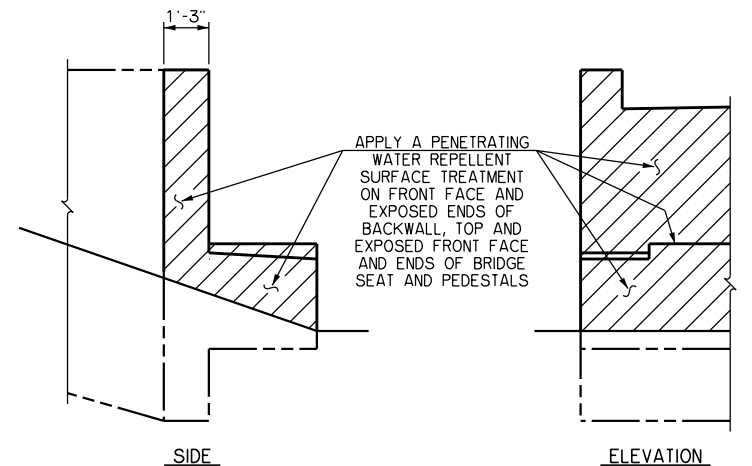
SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

DETAILS OF GRADING, SUBSTRUCTURE EXCAVATION AND PIPE UNDERDRAIN ASSEMBLY AT ABUTMENTS



PLAN

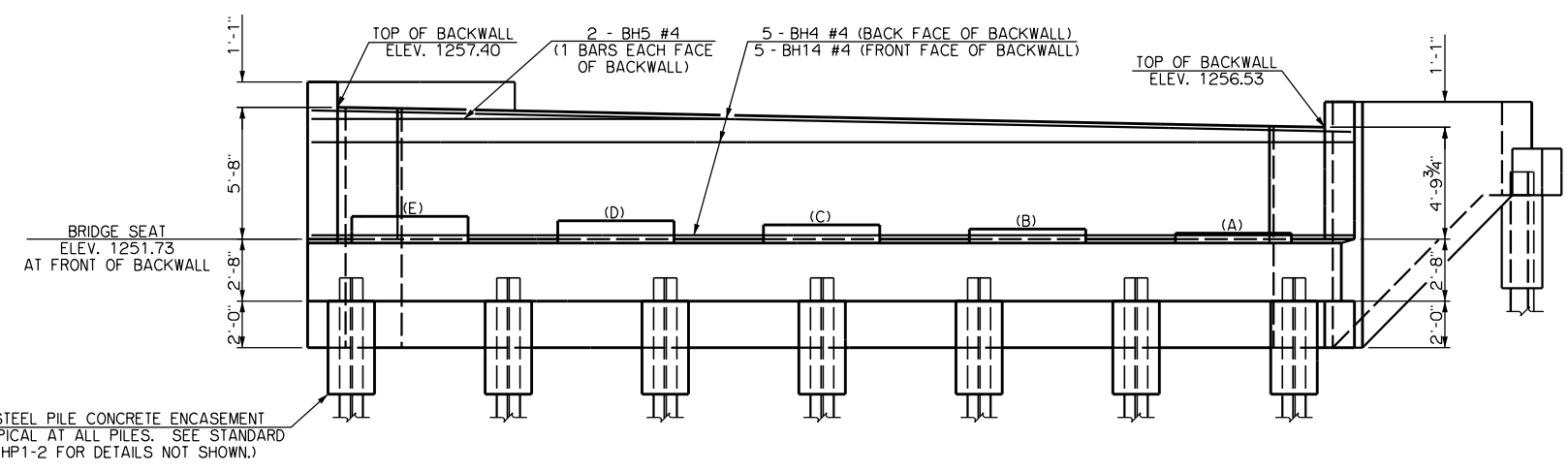
| PEDESTAL ELEVATIONS | |
|---------------------|-----------|
| PEDESTAL | ELEVATION |
| (A) | 1251.98 |
| (B) | 1252.16 |
| (C) | 1252.34 |
| (D) | 1252.52 |
| (E) | 1252.70 |



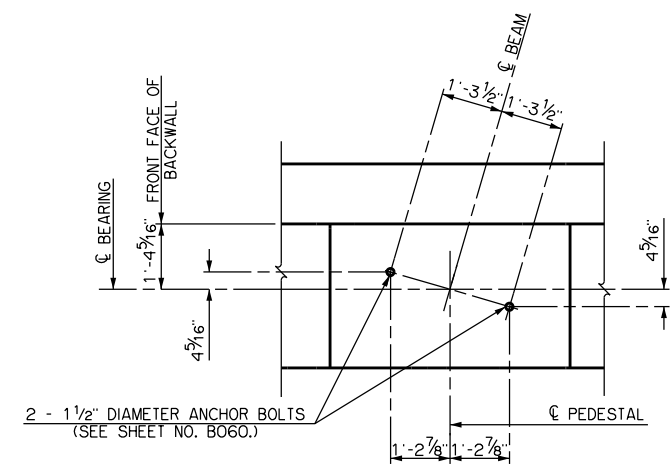
DETAIL OF PENETRATING WATER REPELLENT TREATMENT

| BAR LIST - ABUTMENT NO. 1 | | | | | |
|---------------------------|------|-----|------|---------------|------------------|
| MARK | SIZE | NO. | FORM | LENGTH | LENGTH VARIATION |
| EPOXY COATED | | | | | |
| BH1 | #9 | 2 | STR. | 44'-2" | |
| BH2 | #9 | 2 | BNT. | 45'-0" | |
| BH3 | #9 | 2 | BNT. | 44'-11" | |
| BH4 | #4 | 8 | BNT. | 46'-3" | |
| BH5 | #4 | 2 | BNT. | 17'-8" | |
| BH6 | #4 | 1 | STR. | 44'-2" | |
| BH7 | #4 | 3 | BNT. | 5'-8" (AVG.) | 5'-7" TO 5'-9" |
| BH8 | #4 | 15 | BNT. | 5'-1" | |
| BH9 | #4 | 3 | BNT. | 4'-10" (AVG.) | 4'-9" TO 4'-11" |
| BH10 | #9 | 4 | STR. | 10'-0" | |
| BH11 | #4 | 3 | BNT. | 6'-3" | |
| BH12 | #4 | 3 | BNT. | 4'-4" | |
| BH13 | #9 | 1 | STR. | 44'-5" | |
| BH14 | #4 | 8 | BNT. | 45'-0" | |
| BH15 | #9 | 1 | STR. | 44'-9" | |
| BH16 | #9 | 2 | STR. | 44'-8" | |
| BH17 | #4 | 1 | BNT. | 44'-11" | |
| BV1 | #5 | 38 | STR. | 9'-6" (AVG.) | 9'-1" TO 9'-10" |
| BV2 | #5 | 3 | STR. | 10'-4" | |
| BV3 | #4 | 3 | BNT. | 3'-6" | |
| BV4 | #4 | 38 | STR. | 9'-6" (AVG.) | 9'-1" TO 9'-10" |
| BV5 | #4 | 3 | STR. | 10'-4" | |
| BV6 | #4 | 3 | STR. | 9'-6" | |
| BV7 | #4 | 1 | STR. | 2'-3" | |
| BV8 | #5 | 3 | STR. | 9'-6" | |
| BV9 | #4 | 4 | STR. | 11'-0" | |
| BV10 | #4 | 4 | STR. | 10'-2" | |
| P1 | #4 | 30 | BNT. | 6'-7" | |
| P2 | #4 | 20 | BNT. | 8'-7" | |
| S1 | #5 | 79 | BNT. | 9'-4" | |
| S2 | #5 | 75 | BNT. | 11'-2" | |
| S3 | #5 | 4 | BNT. | 9'-11" (AVG.) | 9'-6" TO 10'-4" |
| SU1 | #5 | 5 | BNT. | 9'-3" | |
| T1 | #4 | 3 | BNT. | 15'-5" | |
| WT1 | #6 | 2 | BNT. | 11'-6" | |
| WT2 | #6 | 3 | BNT. | 9'-0" (AVG.) | 6'-0" TO 12'-0" |
| WT3 | #6 | 5 | BNT. | 17'-0" | |
| WT4 | #6 | 27 | BNT. | 13'-0" | |
| WT5 | #6 | 3 | BNT. | 9'-11" (AVG.) | 6'-6" TO 13'-4" |

| SUMMARY OF QUANTITIES - ABUTMENT NO. 1 | | |
|--|------|----------|
| ITEM | UNIT | TOTAL |
| SUBSTRUCTURE EXCAVATION COMMON | CY | 95.00 |
| CLSM BACKFILL | CY | 140.30 |
| CLASS A CONCRETE | CY | 56.80 |
| EPOXY COATED REINFORCING STEEL | LB | 5,890.00 |
| PILES, FURNISHED (HP10X42) | LF | 114.00 |
| PILES, FURNISHED (HP12X53) | LF | 361.00 |
| PILES, DRIVEN (HP10X42) | LF | 114.00 |
| PILES, DRIVEN (HP12X53) | LF | 361.00 |
| WATER REPELLENT (VISUALLY INSPECTED) | SY | 61.00 |
| 6" PERFORATED PIPE UNDERDRAIN ROUND | LF | 48.00 |
| 6" NON-PERF. PIPE UNDERDRAIN RND. | LF | 20.00 |



ELEVATION



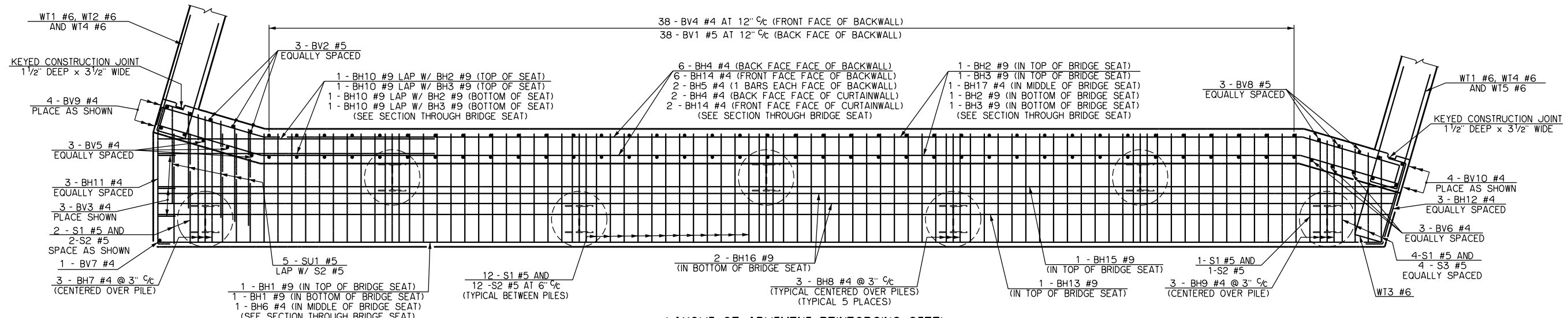
ANCHOR BOLT LAYOUT

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

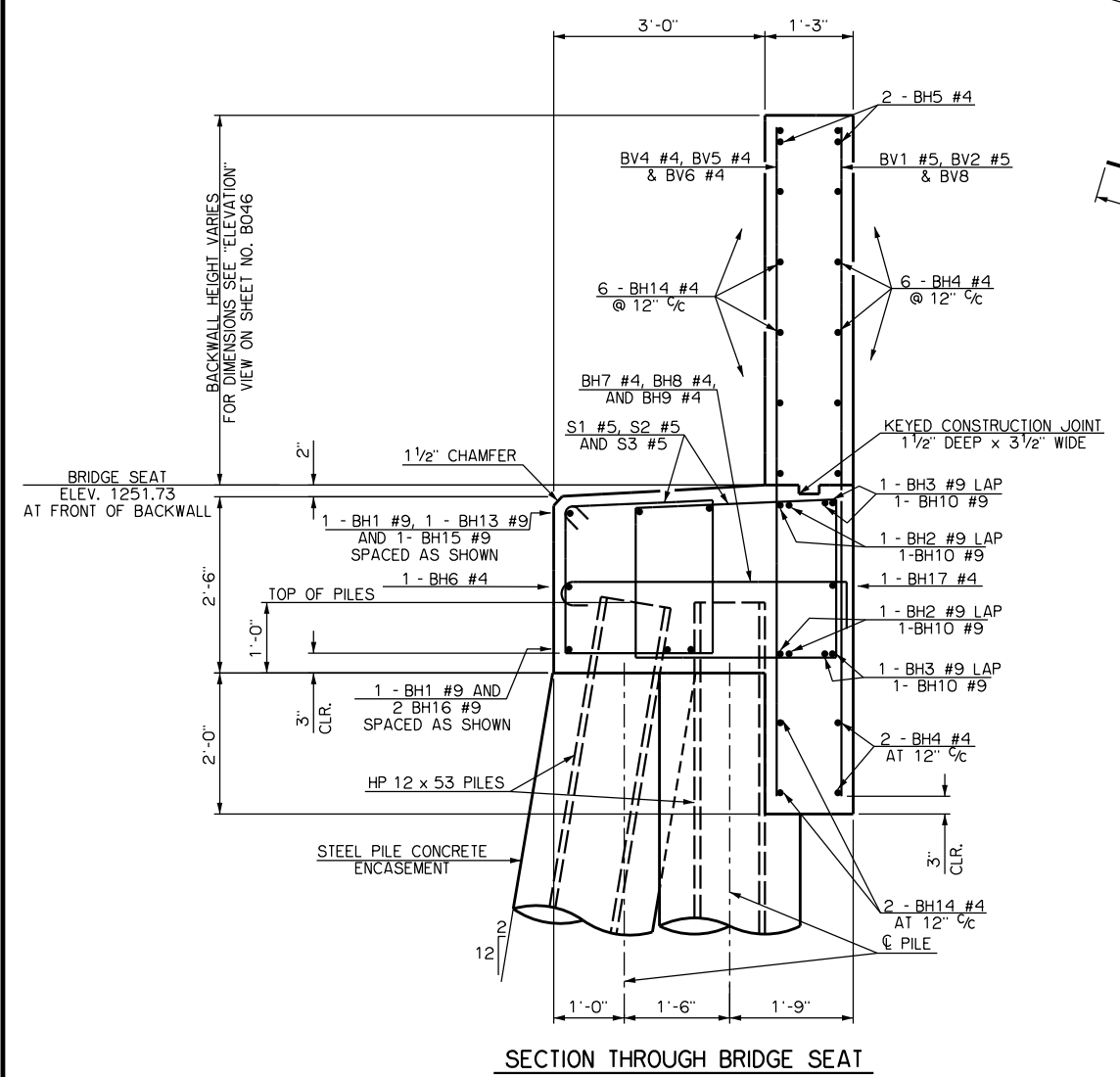
DETAILS OF ABUTMENT NO. 1
(SHEET NO. 1 OF 2)

State Job No. 24428(04) Sheet No. B046

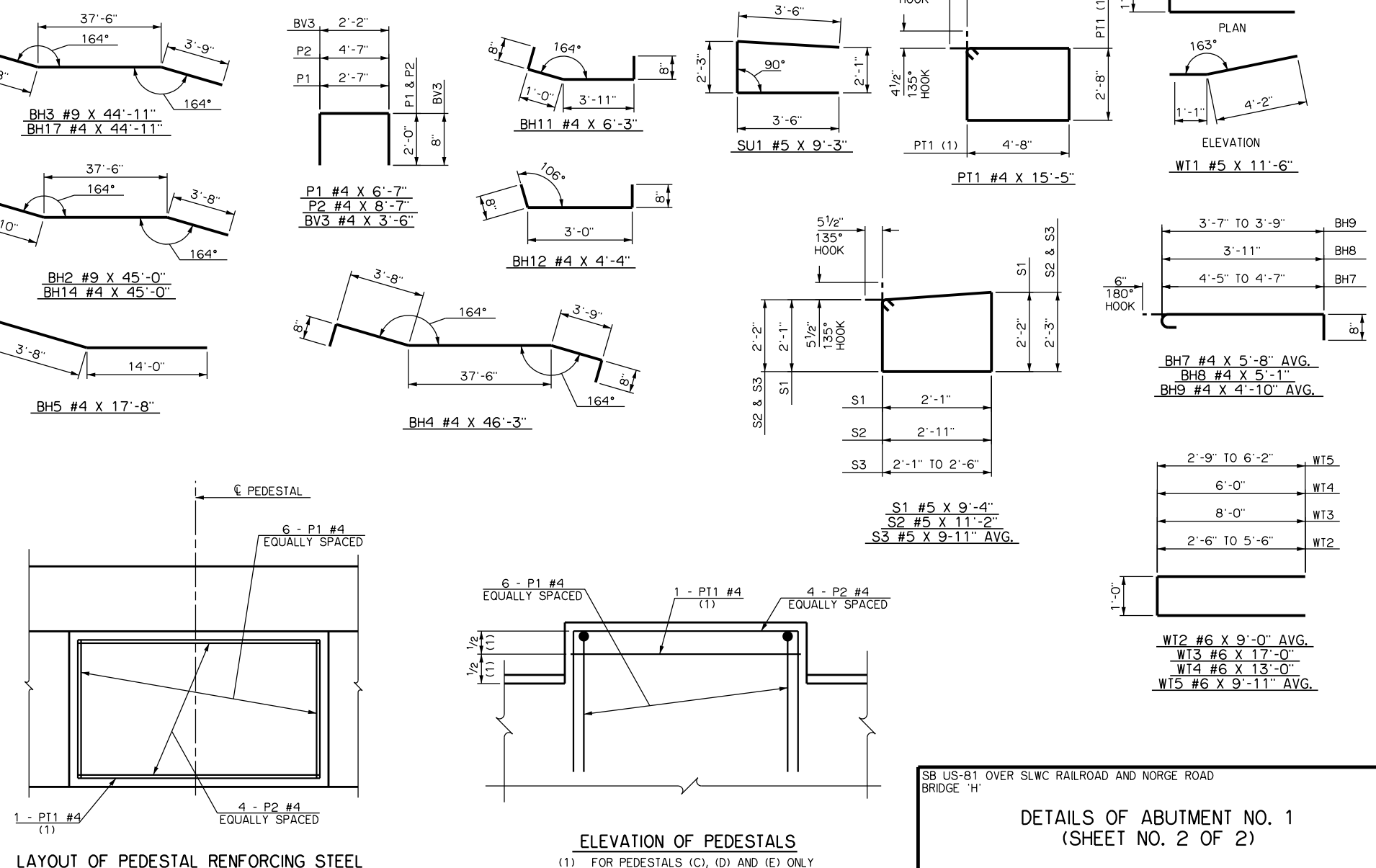
US 81 REALIGNMENT
GRADY COUNTY



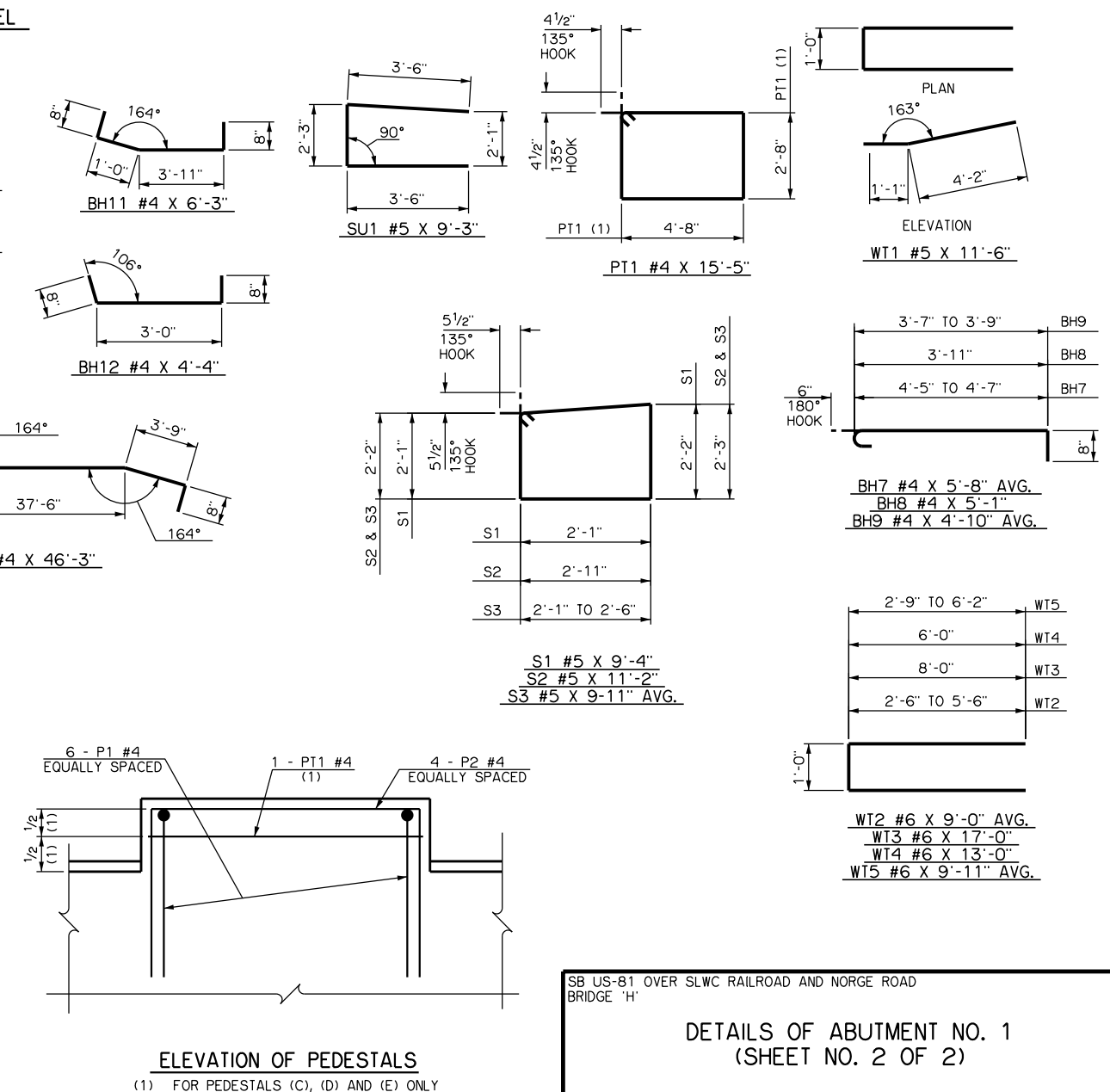
LAYOUT OF ABUTMENT REINFORCING STEEL



SECTION THROUGH BRIDGE SEAT



LAYOUT OF PEDESTAL REINFORCING STEEL



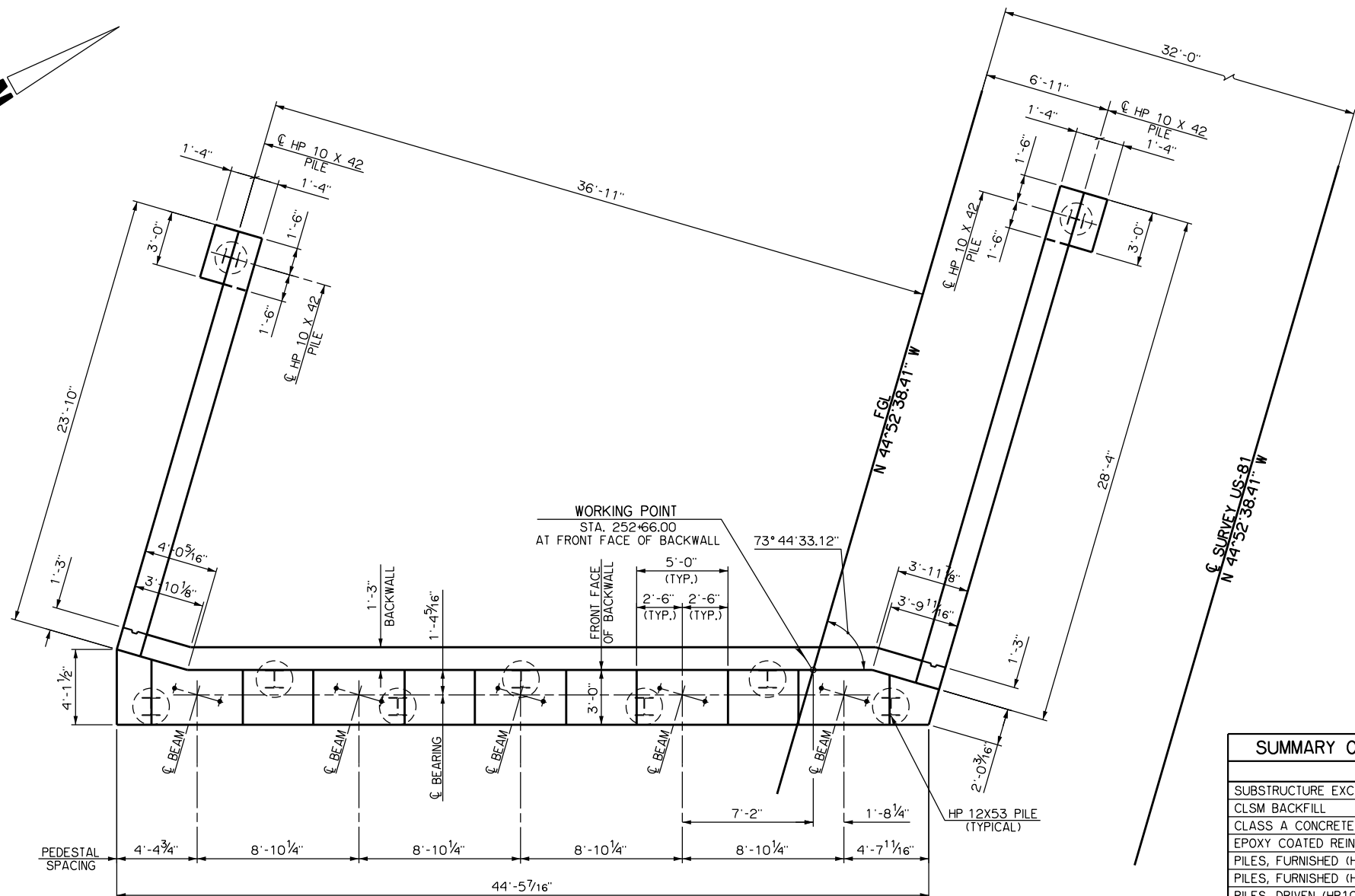
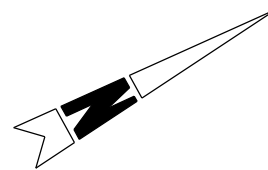
ELEVATION OF PEDESTALS
(1) FOR PEDESTALS (C), (D) AND (E) ONLY

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

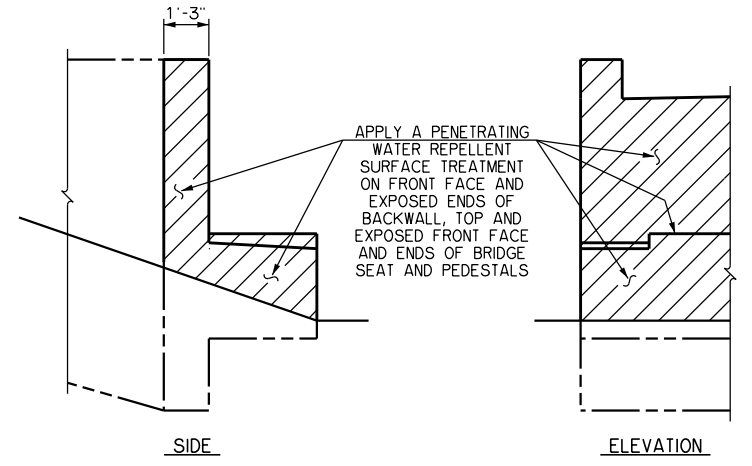
DETAILS OF ABUTMENT NO. 1
(SHEET NO. 2 OF 2)

State Job No. 24428(04) Sheet No. B047

US 81 REALIGNMENT
GRADY COUNTY



PLAN



DETAIL OF PENETRATING WATER REPELLENT TREATMENT

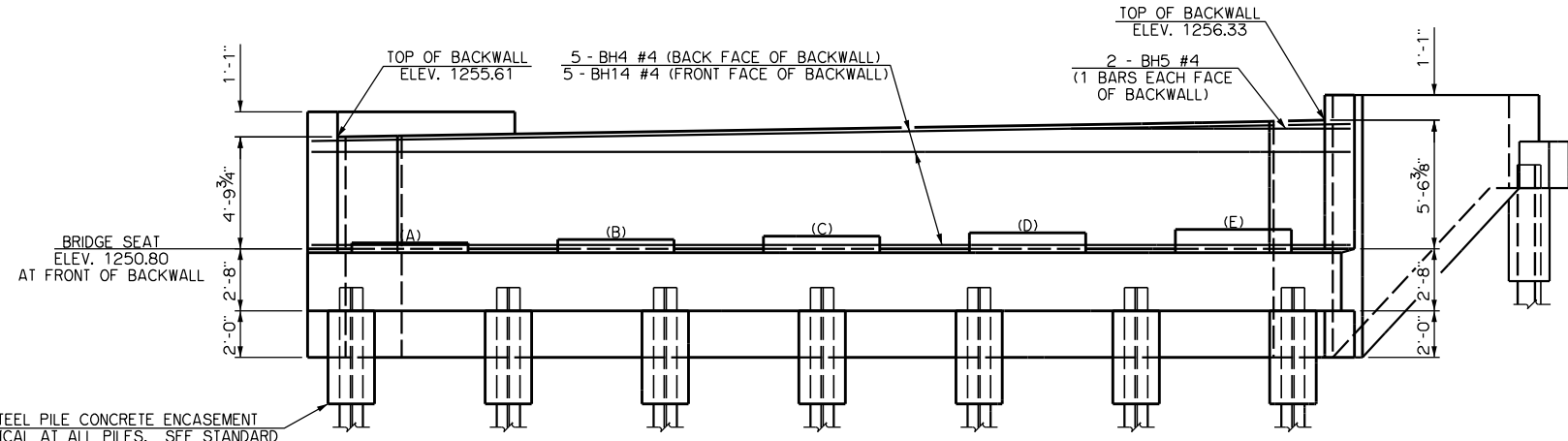
| PEDESTAL ELEVATIONS | |
|---------------------|-----------|
| PEDESTAL | ELEVATION |
| (A) | 1251.05 |
| (B) | 1251.20 |
| (C) | 1251.34 |
| (D) | 1251.49 |
| (E) | 1251.64 |

BAR LIST - ABUTMENT NO. 2

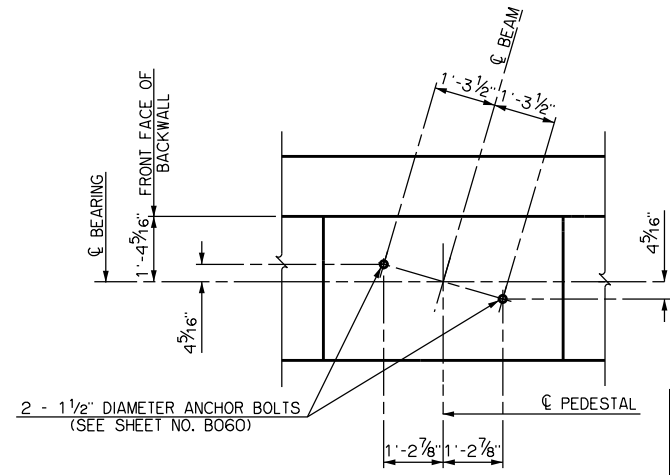
| MARK | SIZE | NO. | FORM | LENGTH | LENGTH VARIATION |
|--------------|------|-----|------|---------------|------------------|
| EPOXY COATED | | | | | |
| BH1 | #9 | 2 | STR. | 44'-2" | |
| BH2 | #9 | 2 | BNT. | 45'-0" | |
| BH3 | #9 | 2 | BNT. | 44'-11" | |
| BH4 | #4 | 8 | BNT. | 46'-3" | |
| BH5 | #4 | 2 | BNT. | 12'-9" | |
| BH6 | #4 | 1 | STR. | 44'-2" | |
| BH7 | #4 | 3 | BNT. | 5'-8" (AVG.) | 5'-7" TO 5'-9" |
| BH8 | #4 | 15 | BNT. | 5'-1" | |
| BH9 | #4 | 3 | BNT. | 4'-10" (AVG.) | 4'-9" TO 4'-11" |
| BH10 | #9 | 4 | STR. | 10'-0" | |
| BH11 | #4 | 3 | BNT. | 6'-3" | |
| BH12 | #4 | 3 | BNT. | 4'-4" | |
| BH13 | #9 | 1 | STR. | 44'-5" | |
| BH14 | #4 | 8 | BNT. | 45'-0" | |
| BH15 | #9 | 1 | STR. | 44'-9" | |
| BH16 | #9 | 2 | STR. | 44'-8" | |
| BV1 | #5 | 38 | STR. | 9'-5" (AVG.) | 9'-1" TO 9'-9" |
| BV2 | #5 | 3 | STR. | 9'-4" | |
| BV3 | #4 | 3 | BNT. | 3'-6" | |
| BV4 | #4 | 38 | STR. | 9'-3" (AVG.) | 8'-11" TO 9'-7" |
| BV5 | #4 | 3 | STR. | 9'-4" | |
| BV6 | #4 | 3 | STR. | 10'-0" | |
| BV7 | #4 | 1 | STR. | 2'-3" | |
| BV8 | #5 | 3 | STR. | 10'-0" | |
| BV9 | #4 | 4 | STR. | 10'-0" | |
| BV10 | #4 | 4 | STR. | 10'-8" | |
| P1 | #4 | 30 | BNT. | 6'-7" | |
| P2 | #4 | 20 | BNT. | 8'-7" | |
| SI | #5 | 79 | BNT. | 9'-4" | |
| S2 | #5 | 75 | BNT. | 11'-2" | |
| S3 | #5 | 4 | BNT. | 9'-11" (AVG.) | 9'-6" TO 10'-4" |
| SU1 | #5 | 5 | BNT. | 9'-3" | |
| T1 | #4 | 3 | BNT. | 15'-7" | |
| WT1 | #6 | 2 | BNT. | 11'-6" | |
| WT6 | #6 | 3 | BNT. | 9'-0" (AVG.) | 6'-0" TO 12'-0" |
| WT3 | #6 | 5 | BNT. | 17'-0" | |
| WT4 | #6 | 27 | BNT. | 13'-0" | |
| WT7 | #6 | 3 | BNT. | 9'-8" (AVG.) | 6'-4" TO 13'-0" |

SUMMARY OF QUANTITIES - ABUTMENT NO. 2

| ITEM | UNIT | TOTAL |
|--------------------------------------|------|----------|
| SUBSTRUCTURE EXCAVATION COMMON | CY | 95.00 |
| CLSM BACKFILL | CY | 146.70 |
| CLASS A CONCRETE | CY | 56.60 |
| EPOXY COATED REINFORCING STEEL | LB | 5,840.00 |
| PILES, FURNISHED (HP10X42) | LF | 88.00 |
| PILES, FURNISHED (HP12X53) | LF | 277.00 |
| PILES, DRIVEN (HP10X42) | LF | 88.00 |
| PILES, DRIVEN (HP12X53) | LF | 277.00 |
| WATER REPELLENT (VISUALLY INSPECTED) | SY | 61.00 |
| 6" PERFORATED PIPE UNDERDRAIN ROUND | LF | 48.00 |
| 6" NON-PERF. PIPE UNDERDRAIN RND. | LF | 20.00 |



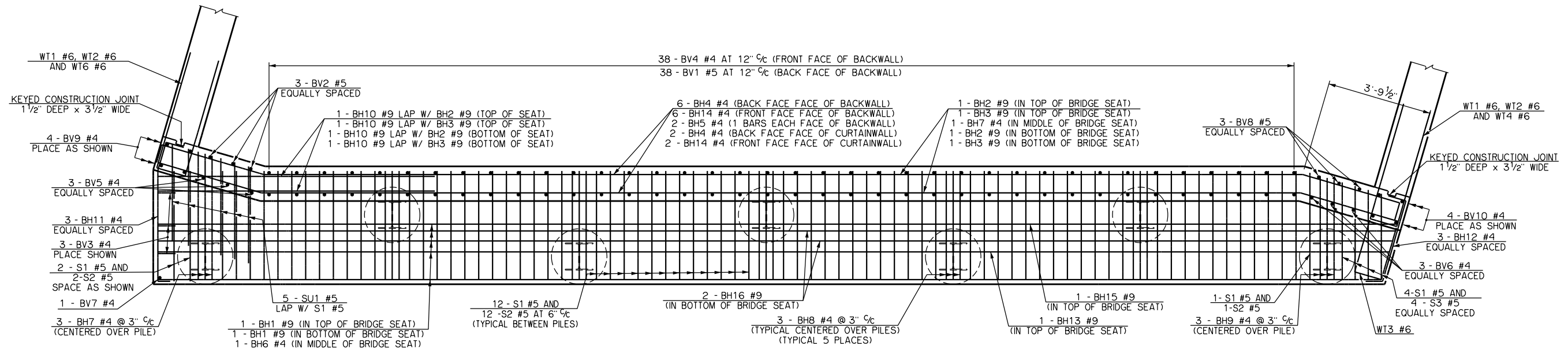
ELEVATION



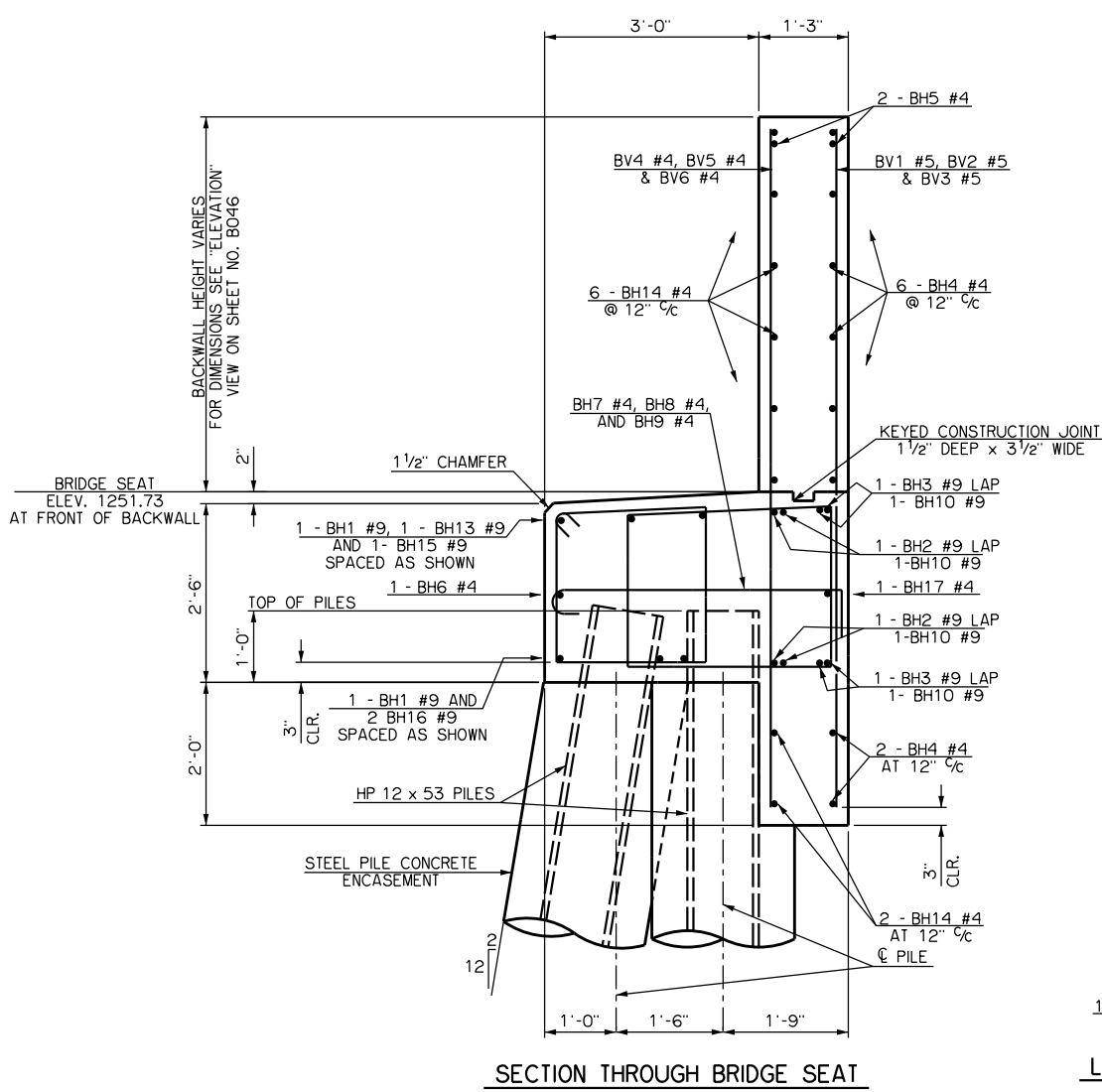
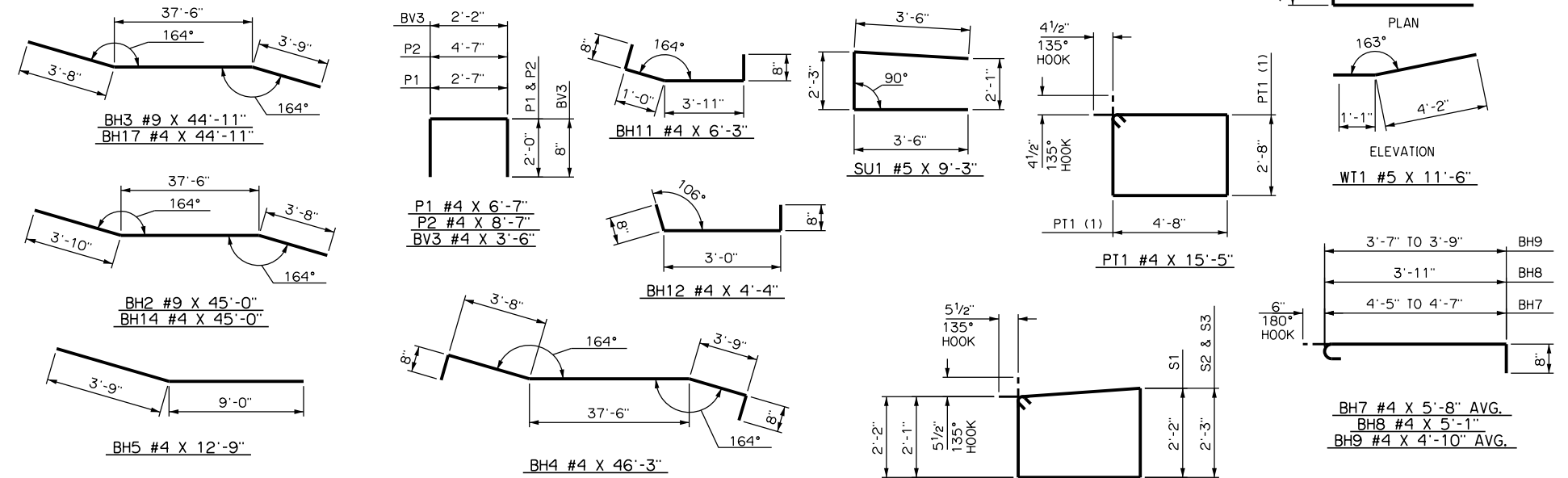
ANCHOR BOLT LAYOUT

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD BRIDGE 'H'

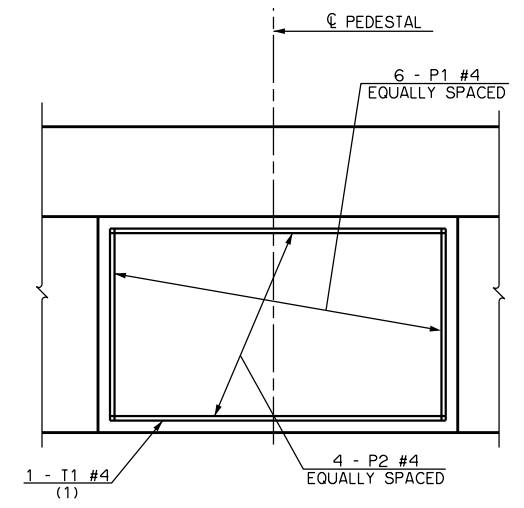
DETAILS OF ABUTMENT NO. 2 (SHEET NO. 1 OF 2)



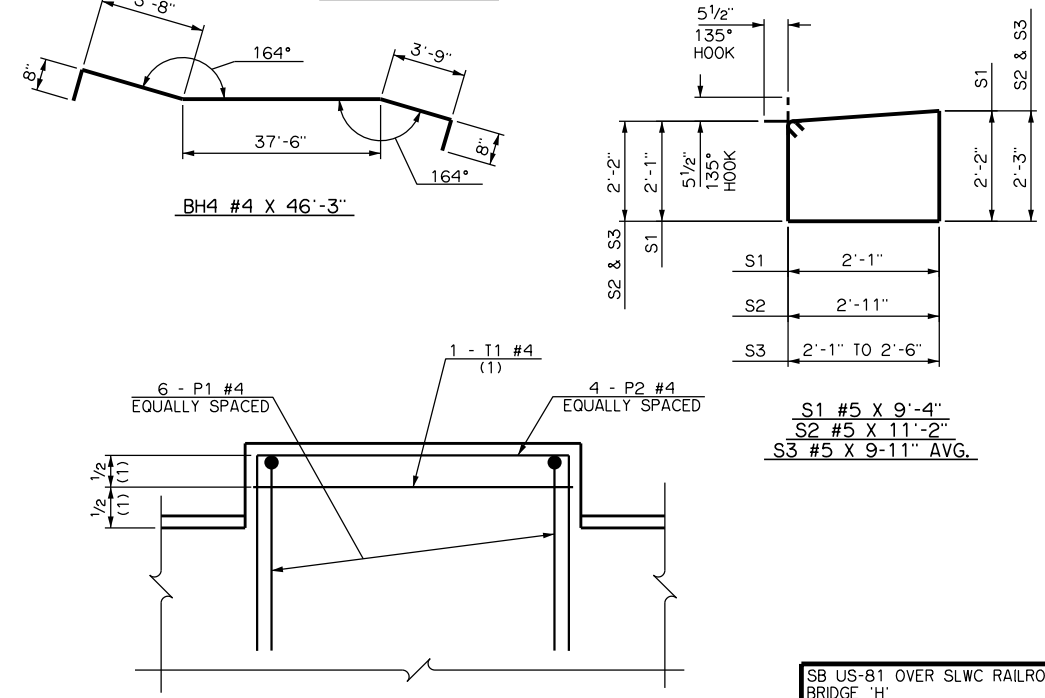
LAYOUT OF ABUTMENT REINFORCING STEEL



SECTION THROUGH BRIDGE SEAT



LAYOUT OF PEDESTAL REINFORCING STEEL



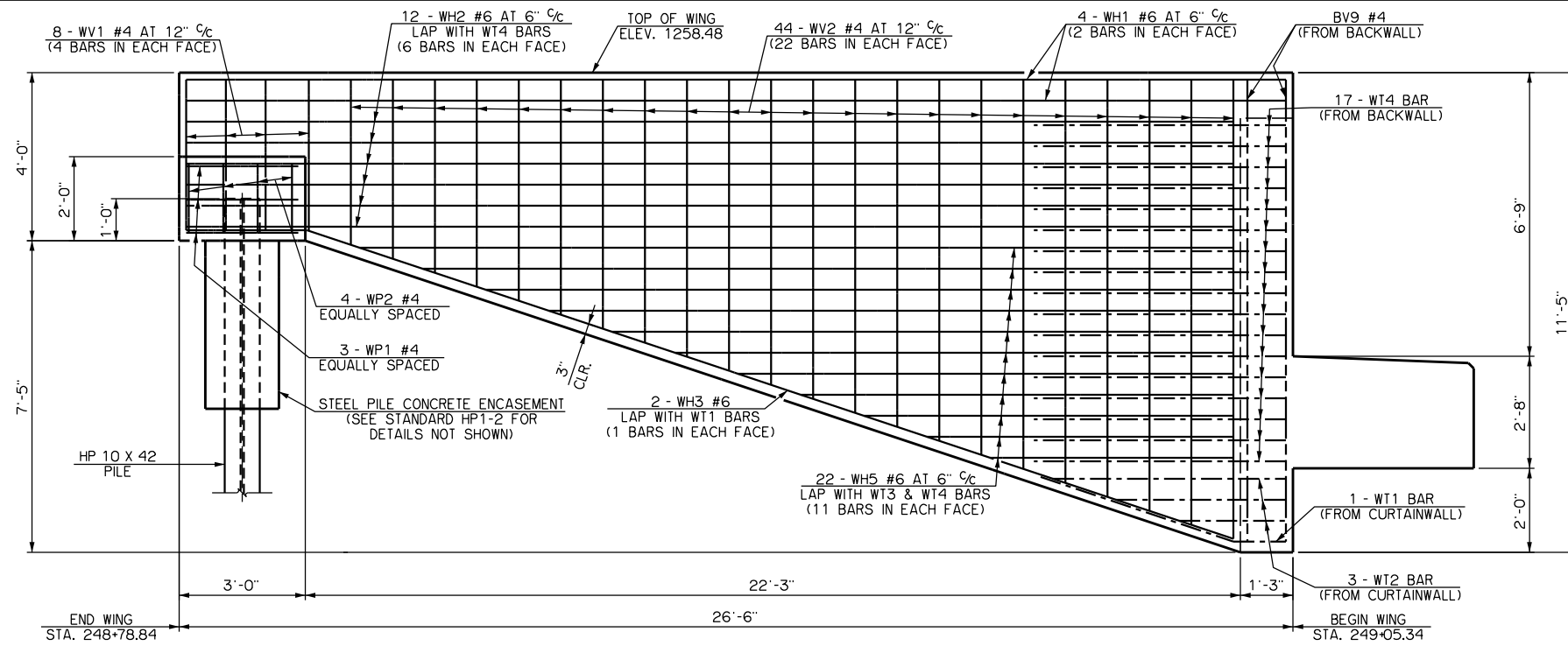
ELEVATION OF PEDESTALS

(1) FOR PEDESTALS (C), (D) AND (E) ONLY

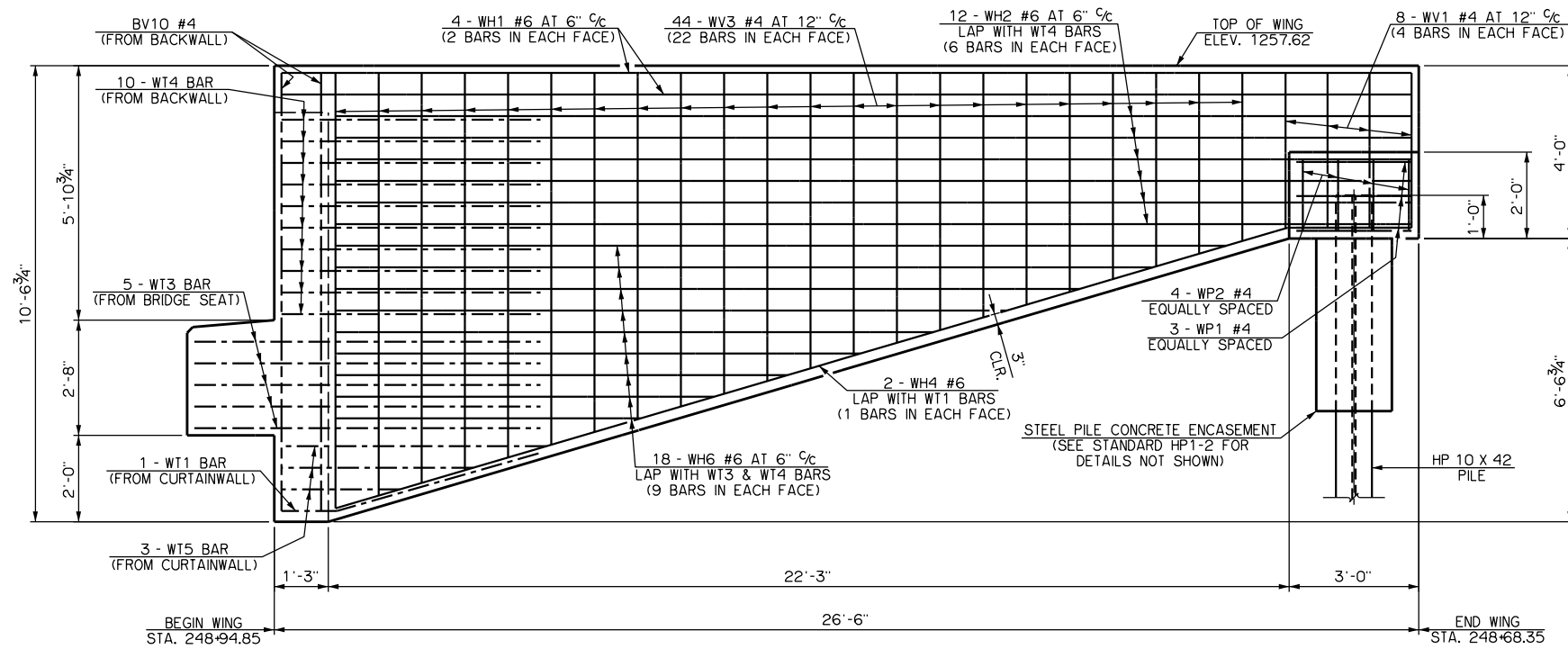
SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
 BRIDGE 'H'

DETAILS OF ABUTMENT NO. 2
 (SHEET NO. 2 OF 2)

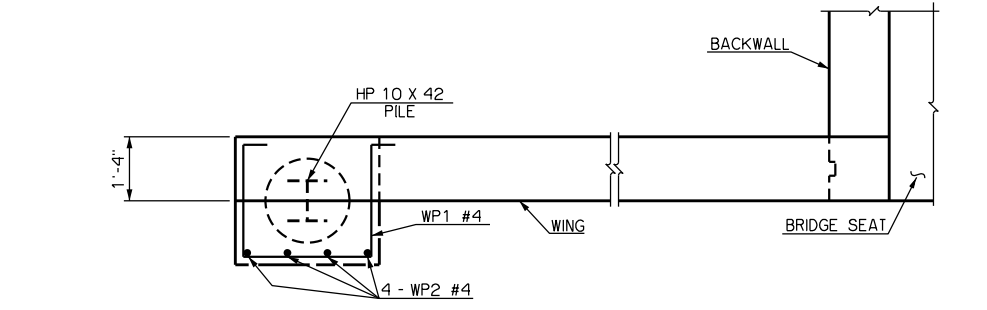
State Job No. 24428(04) Sheet No. B049



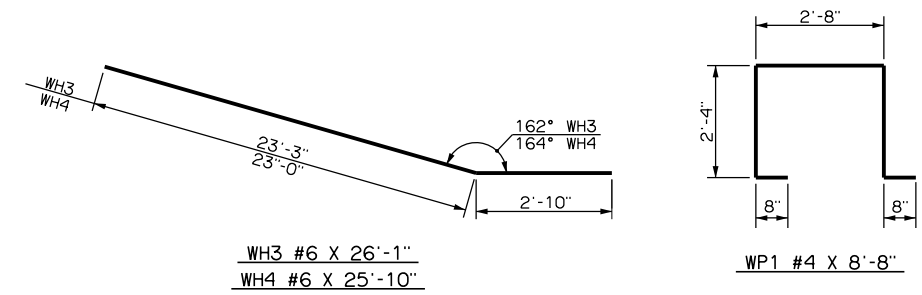
ELEVATION OF NORTH WING (LOOKING SOUTH)



ELEVATION OF SOUTH WING (LOOKING NORTH)

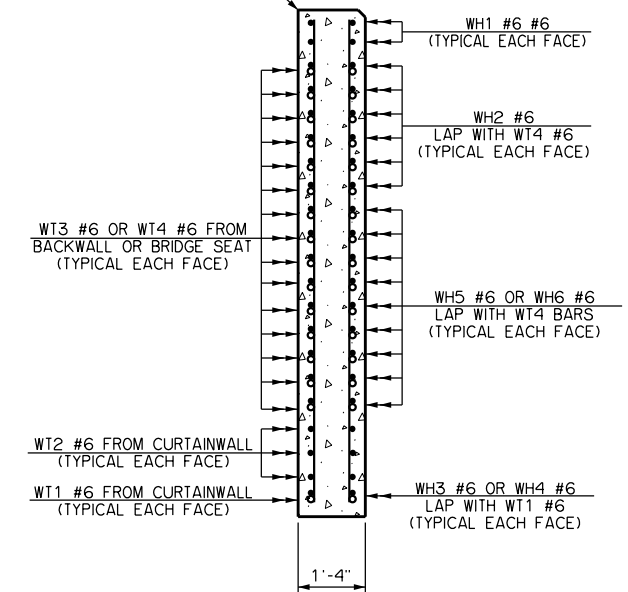


PLAN OF REINFORCING STEEL AT WING PILE CONCRETE BLOCK OUT



DETAILS OF BENT REINFORCING STEEL

NOTE: THE TOP CORNER OF THE ROADWAY FACE OF WINGS SHALL NOT BE CHAMFERED



SECTION THRU WING (AT BACK FACE OF BRIDGE SEAT)

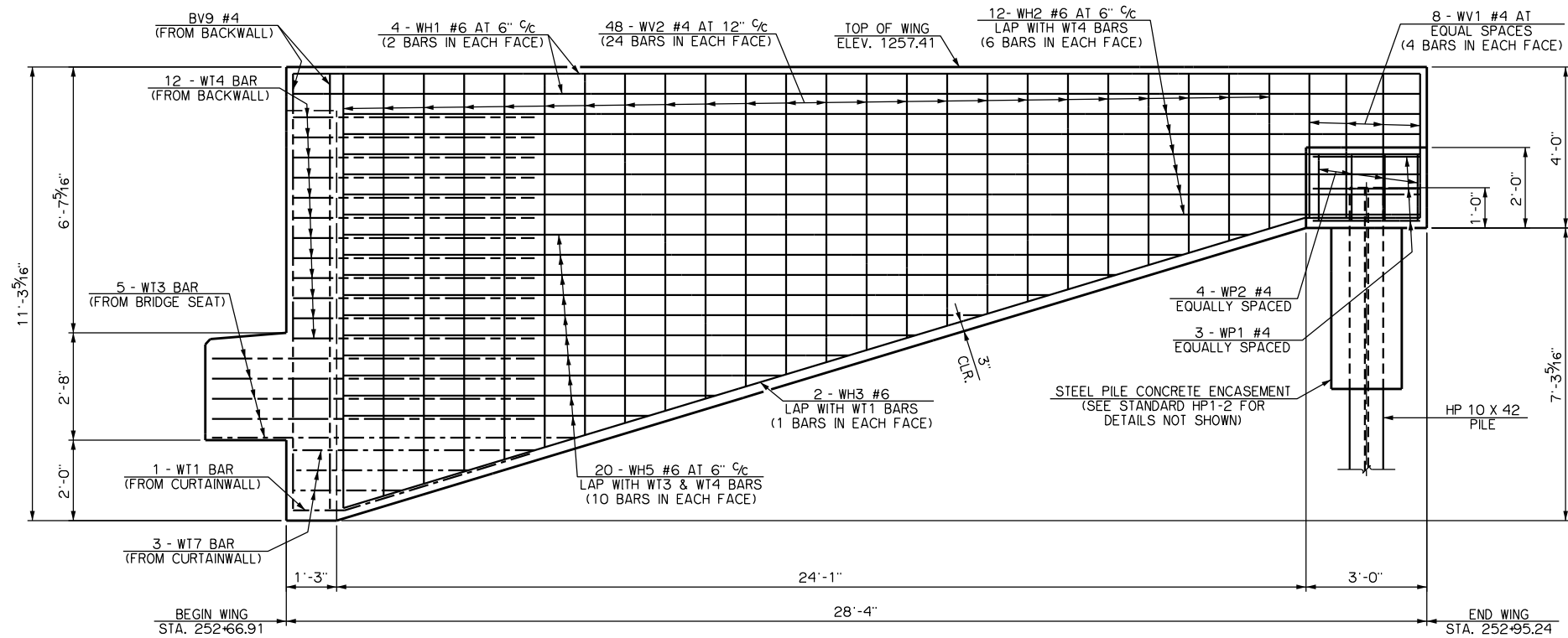
- (A) INCLUDES TWO SETS OF 11 BARS
- (B) INCLUDES TWO SETS OF 22 BARS
- (C) INCLUDES TWO SETS OF 9 BARS
- (D) INCLUDES TWO SETS OF 22 BARS

| BAR LIST - NORTH WING HIGH SIDE | | | | | |
|---------------------------------|------|-----|------|---------------|-------------------|
| MARK | SIZE | NO. | FORM | LENGTH | LENGTH VARIATION |
| EPOXY COATED | | | | | |
| WH1 | #6 | 4 | STR. | 26'-2" | |
| WH2 | #6 | 12 | STR. | 24'-11" | |
| WH3 | #6 | 2 | BNT. | 26'-1" | |
| (A) WH5 | #6 | 22 | STR. | 13'-3" (AVG.) | 5'-9" TO 20'-9" |
| WV1 | #4 | 8 | STR. | 3'-7" | |
| (B) WV2 | #4 | 44 | STR. | 7'-5" (AVG.) | 3'-11" TO 10'-11" |
| WP1 | #4 | 3 | BNT. | 8'-8" | |
| WP2 | #4 | 4 | STR. | 1'-7" | |

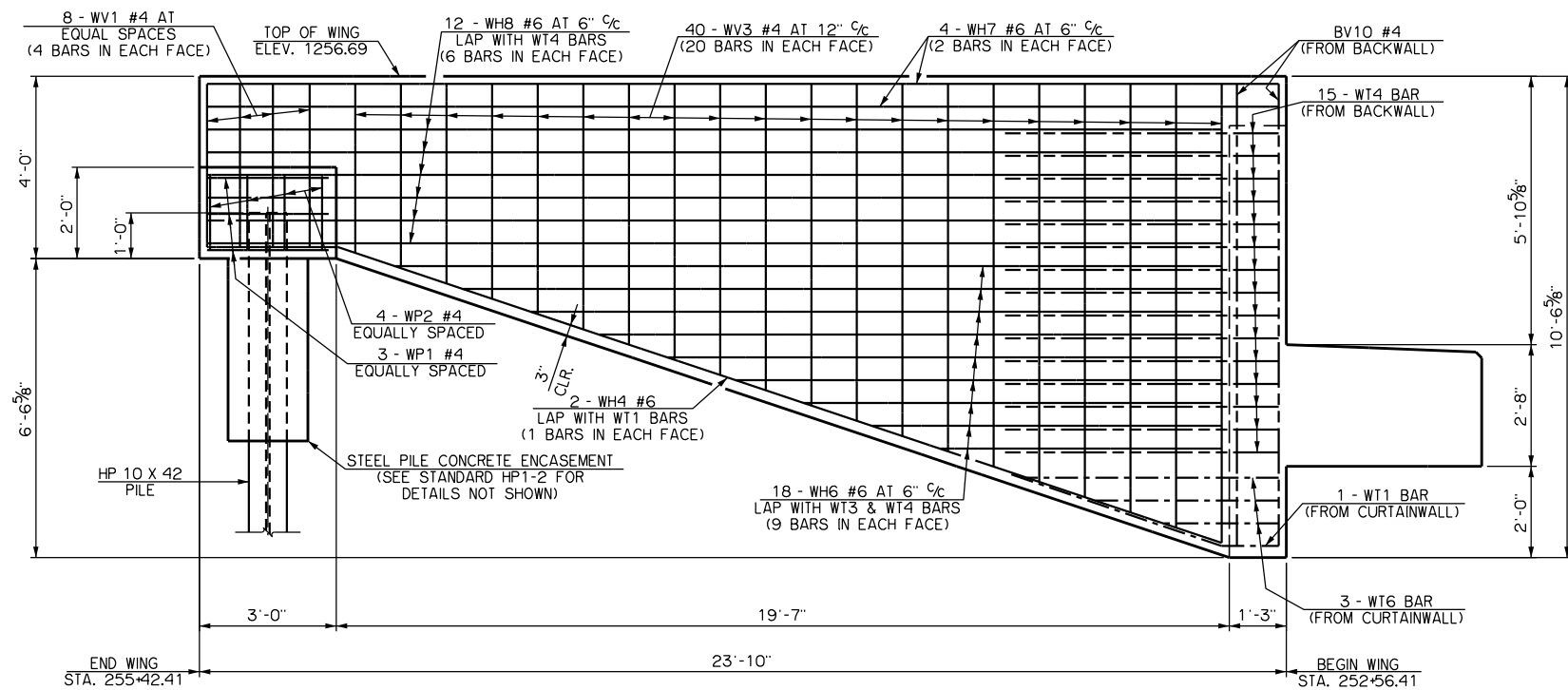
| BAR LIST - SOUTH WING LOW SIDE | | | | | |
|--------------------------------|------|-----|------|----------------|------------------|
| MARK | SIZE | NO. | FORM | LENGTH | LENGTH VARIATION |
| EPOXY COATED | | | | | |
| WH1 | #6 | 4 | STR. | 26'-2" | |
| WH2 | #6 | 12 | STR. | 24'-11" | |
| WH4 | #6 | 2 | BNT. | 25'-10" | |
| (C) WH6 | #6 | 18 | STR. | 13'-11" (AVG.) | 7'-1" TO 20'-8" |
| WV1 | #4 | 8 | STR. | 3'-7" | |
| (D) WV3 | #4 | 44 | STR. | 7'-0" (AVG.) | 3'-11" TO 10'-1" |
| WP1 | #4 | 3 | BNT. | 8'-8" | |
| WP2 | #4 | 4 | STR. | 1'-7" | |

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD BRIDGE 'H'

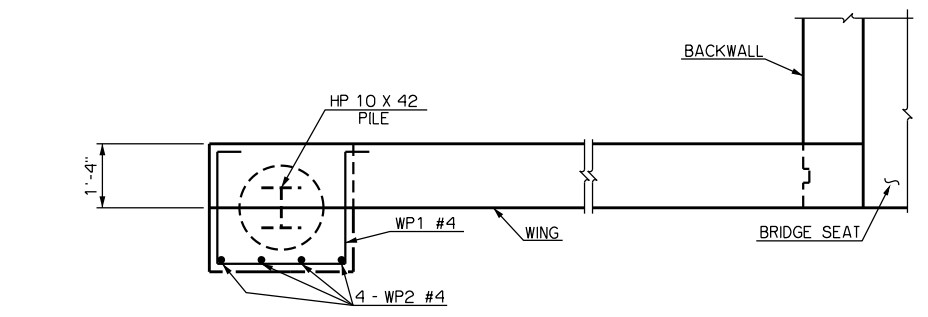
DETAILS OF WINGS AT ABUTMENT NO. 1



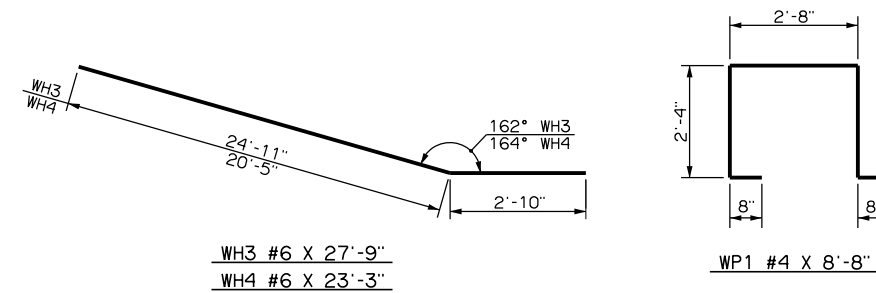
ELEVATION OF NORTH WING (LOOKING SOUTH)



ELEVATION OF SOUTH WING (LOOKING NORTH)

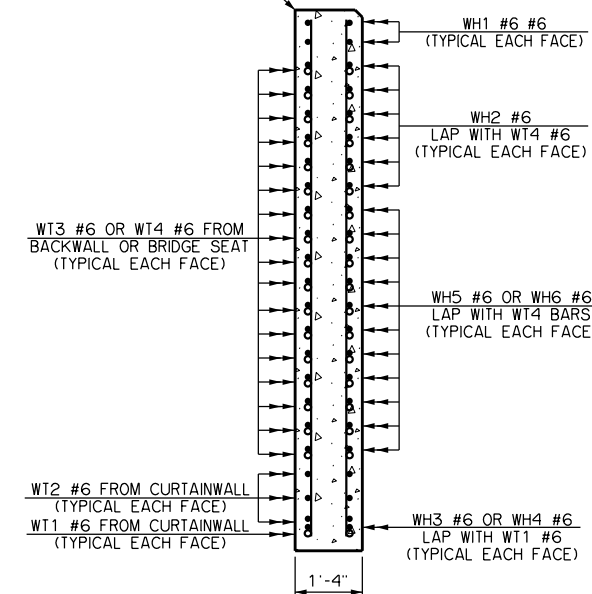


PLAN OF REINFORCING STEEL AT WING PILE CONCRETE BLOCK OUT



DETAILS OF BENT REINFORCING STEEL

NOTE: THE TOP CORNER OF THE ROADWAY FACE OF WINGS SHALL NOT BE CHAMFERED



SECTION THRU WING (AT BACK FACE OF BRIDGE SEAT)

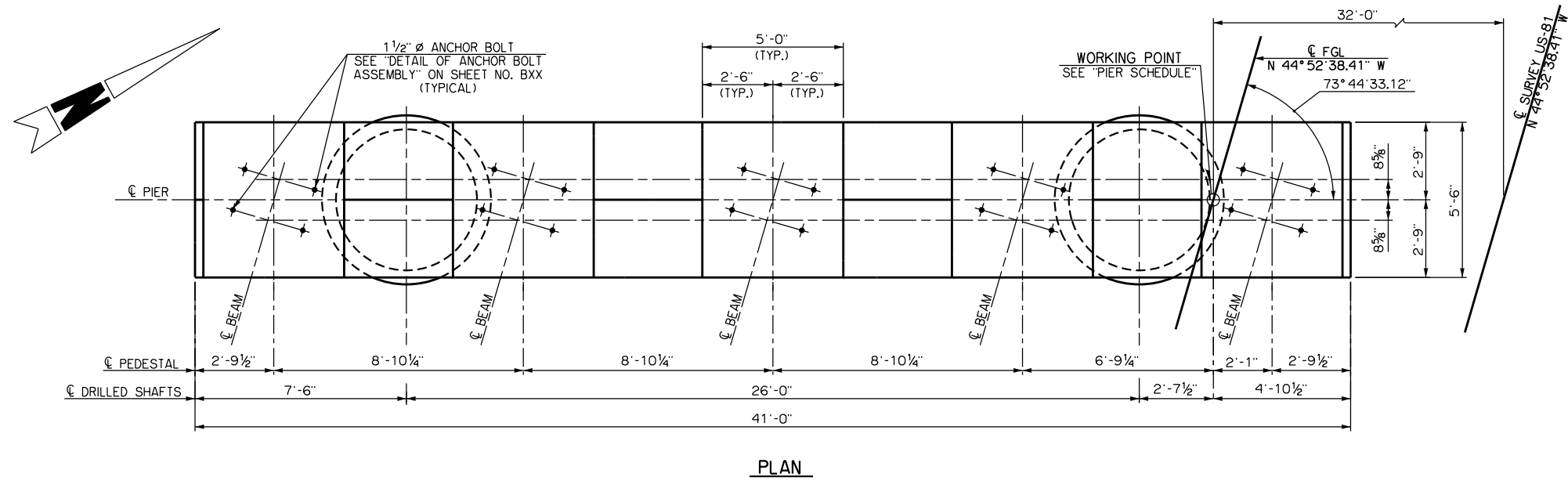
| BAR LIST - NORTH WING HIGH SIDE | | | | | | |
|---------------------------------|------|-----|------|--------|------------------|------------------|
| MARK | SIZE | NO. | FORM | LENGTH | LENGTH VARIATION | |
| EPOXY COATED | | | | | | |
| WH1 | #6 | 4 | STR. | 28'-0" | | |
| WH2 | #6 | 12 | STR. | 26'-9" | | |
| WH3 | #6 | 2 | BNT. | 27'-9" | | |
| (A) | WH5 | #6 | 20 | STR. | 15'-1" (AVG.) | 7'-7" TO 22'-6" |
| (B) | WV2 | #4 | 48 | STR. | 7'-4" (AVG.) | 3'-10" TO 10'-9" |
| | WP1 | #4 | 3 | BNT. | 8'-8" | |
| | WP2 | #4 | 4 | STR. | 1'-7" | |

- (A) INCLUDES TWO SETS OF 10 BARS
- (B) INCLUDES TWO SETS OF 24 BARS
- (C) INCLUDES TWO SETS OF 9 BARS
- (D) INCLUDES TWO SETS OF 20 BARS

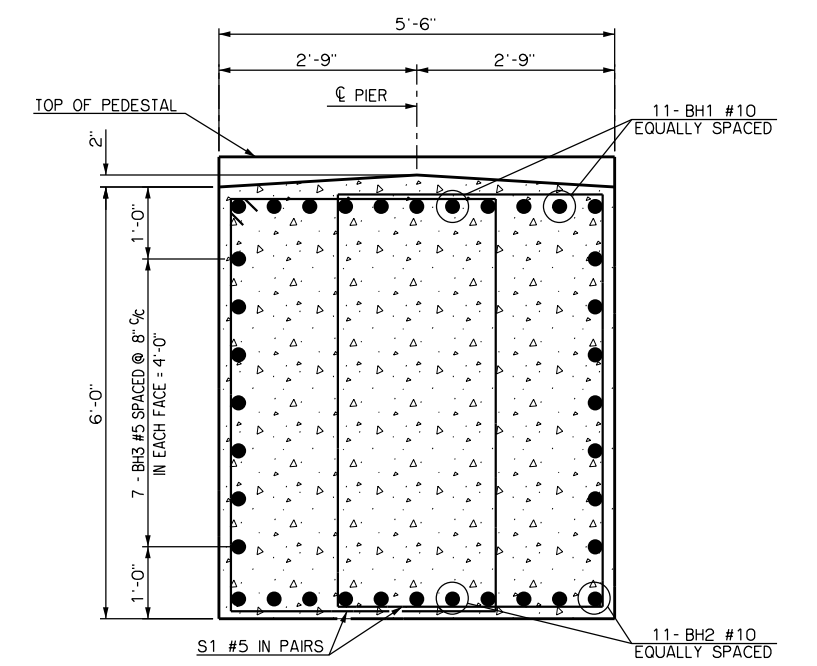
| BAR LIST - SOUTH WING LOW SIDE | | | | | | |
|--------------------------------|------|-----|------|--------|------------------|-----------------|
| MARK | SIZE | NO. | FORM | LENGTH | LENGTH VARIATION | |
| EPOXY COATED | | | | | | |
| WH7 | #6 | 4 | STR. | 23'-6" | | |
| WH8 | #6 | 12 | STR. | 22'-3" | | |
| WH4 | #6 | 2 | BNT. | 23'-3" | | |
| (C) | WH6 | #6 | 18 | STR. | 12'-11" (AVG.) | 7'-8" TO 18'-1" |
| | WV1 | #4 | 8 | STR. | 3'-7" | |
| (D) | WV3 | #4 | 48 | STR. | 6'-11" (AVG.) | 3'-8" TO 10'-1" |
| | WP1 | #4 | 3 | BNT. | 8'-8" | |
| | WP2 | #4 | 4 | STR. | 1'-7" | |

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD BRIDGE 'H'

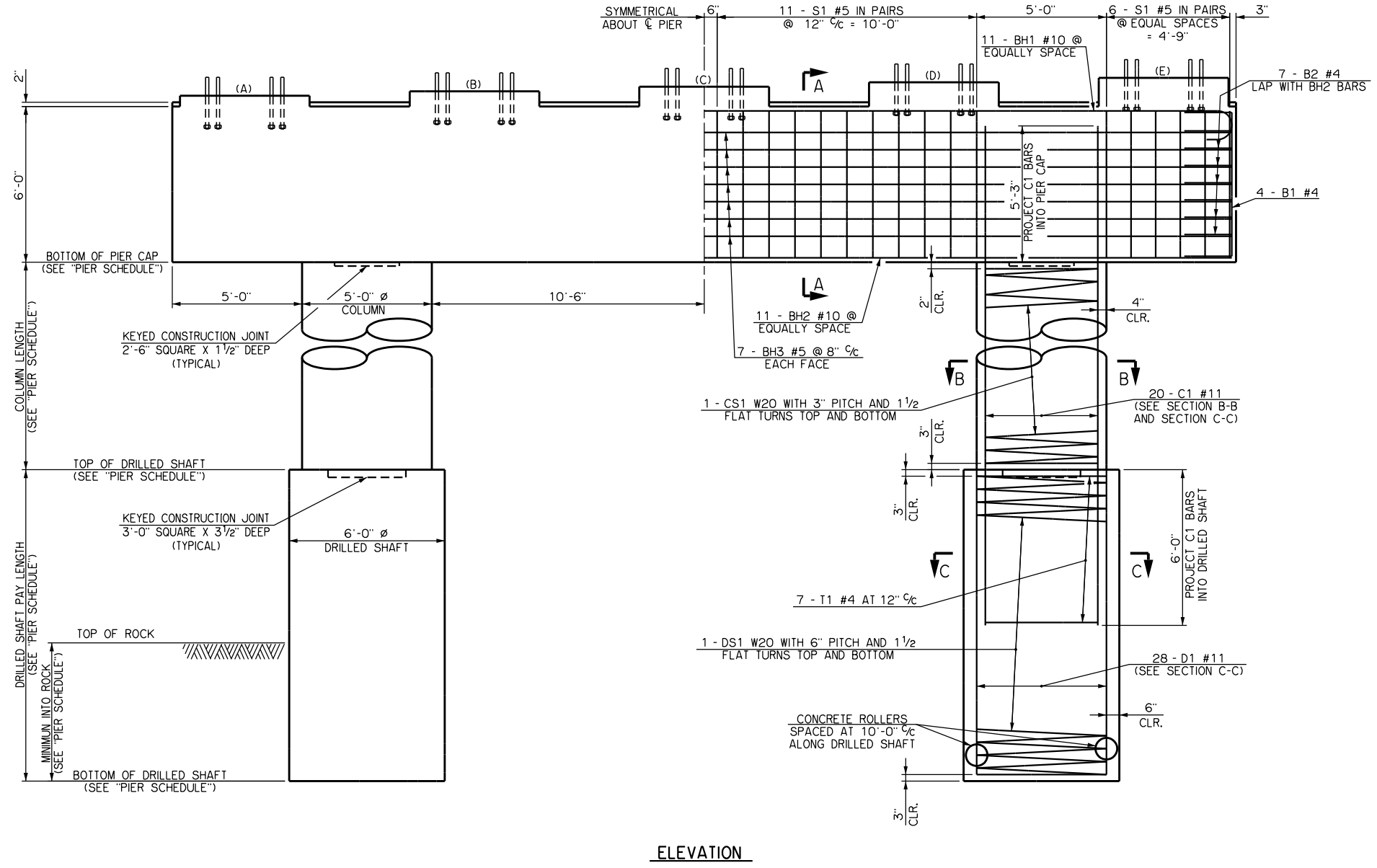
DETAILS OF WINGS AT ABUTMENT NO. 2



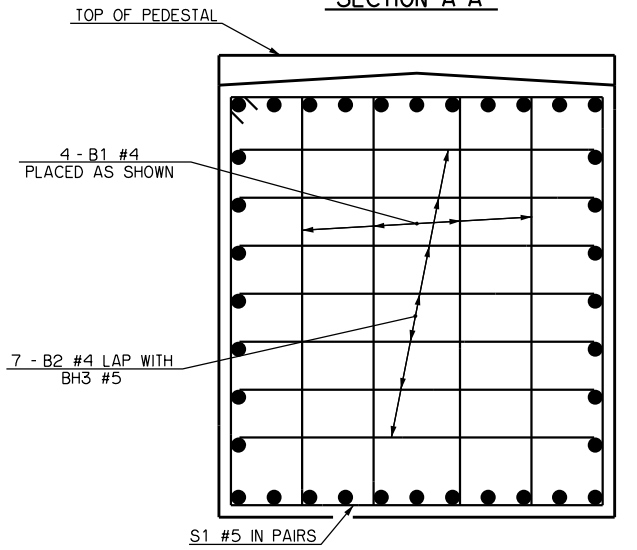
PLAN



SECTION A-A



ELEVATION



END VIEW OF PIER CAP

| PIER SCHEDULE | | | | | | | |
|---------------|-----------------------|------------------------------|--------------------------------|-----------------------------------|---------------|--------------------------|-------------------------|
| PIER NO. | WORKING POINT STATION | BOTTOM OF PIER CAP ELEVATION | TOP OF DRILLED SHAFT ELEVATION | BOTTOM OF DRILLED SHAFT ELEVATION | COLUMN LENGTH | DRILLED SHAFT PAY LENGTH | MINIMUM DEPTH INTO ROCK |
| 1 | 250+00 | 1245.81 | 1222.81 | 1199.81 | 23.00 | 23.00 | 12.00 |
| 2 | 251+00 | 1245.69 | 1227.44 | 1199.44 | 18.25 | 28.00 | 12.00 |
| 3 | 252+00 | 1245.18 | 1229.93 | 1198.93 | 15.25 | 31.00 | 12.00 |

| PEDESTAL ELEVATIONS | | | |
|---------------------|------------|------------|------------|
| PEDESTAL | ELEVATION | | |
| | PIER NO. 1 | PIER NO. 2 | PIER NO. 3 |
| (A) | 1252.22 | 1252.10 | 1251.59 |
| (B) | 1252.39 | 1252.27 | 1251.74 |
| (C) | 1252.57 | 1252.43 | 1251.90 |
| (D) | 1252.74 | 1252.59 | 1252.05 |
| (E) | 1252.91 | 1252.75 | 1252.20 |

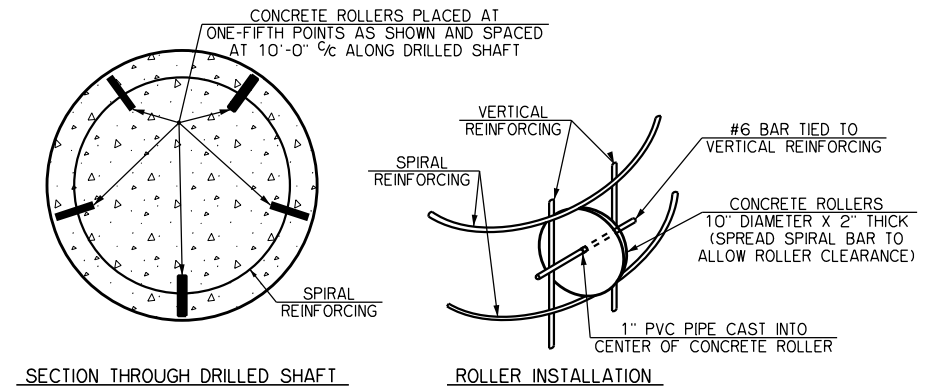
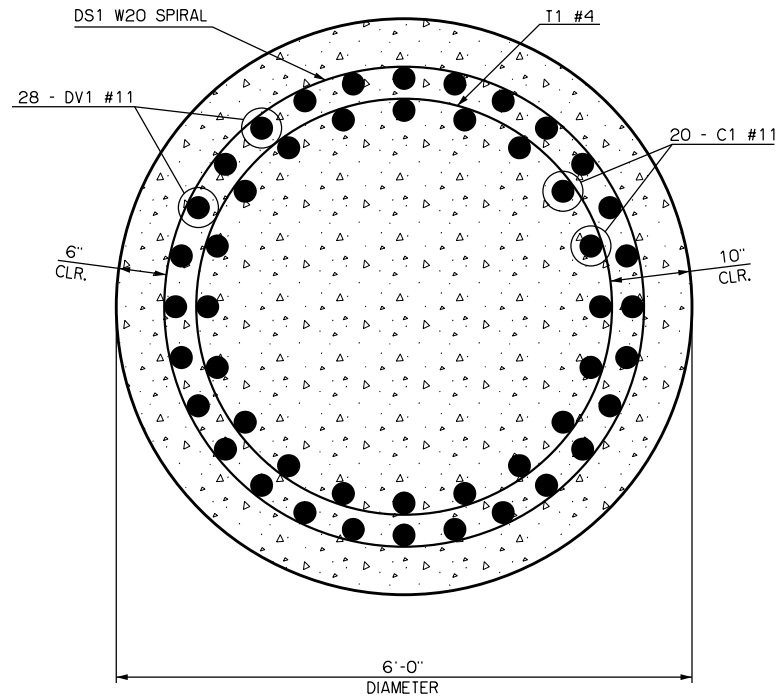
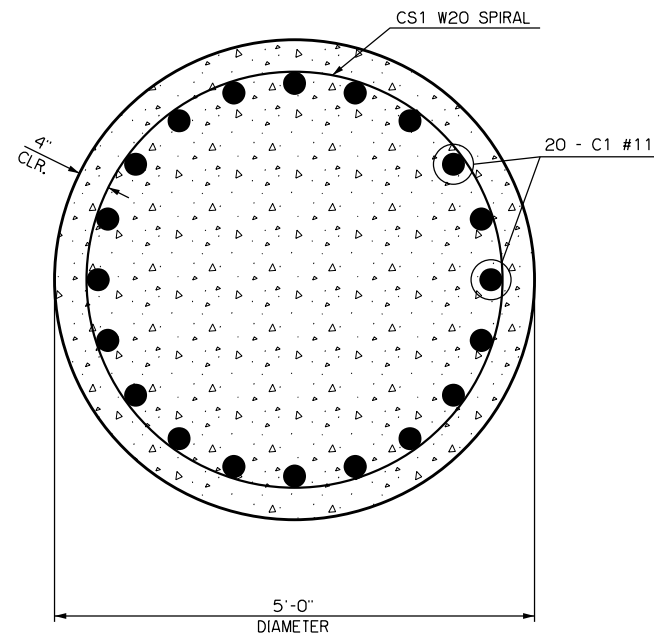
NOTES:
 SEE SHEETS NO. B053 & B054 FOR SECTION B-B, SECTION C-C AND OTHER DETAILS.
 ALL EDGES OF PIER CAP SHALL HAVE A 1/2" CHAMFER EXCEPT FOR PEDESTAL EDGES WHICH SHALL HAVE A 3/4" CHAMFER.
 PENETRATING WATER REPELLENT TREATMENT SHALL BE APPLIED TO THE TOP OF THE PIER CAP, INCLUDING ALL SURFACES OF THE PEDESTALS, AND ALL VERTICAL FACES OF THE PIER CAP.

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
 BRIDGE 'H'

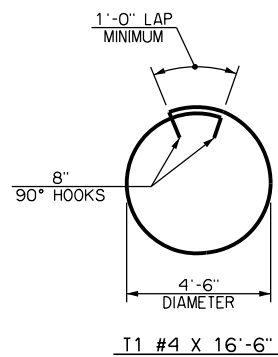
DETAILS OF PIERS
 (SHEET NO. 1 OF 3)

State Job No. 24428(04) Sheet No. B052

GRADY COUNTY US 81 REALIGNMENT

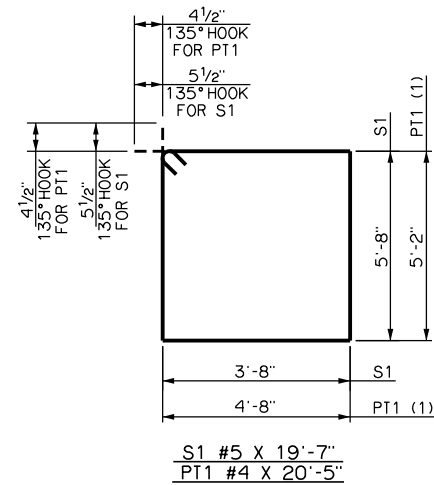
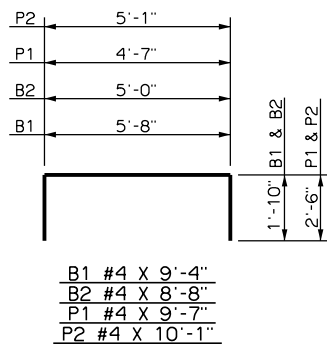


DETAILS OF CONCRETE ROLLERS
 CONCRETE USED IN THE CONCRETE ROLLERS SHALL HAVE A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 4,000 P.S.I. SLAB BOLSTERS, HIGH CHAIRS AND PLASTIC ROLLERS SHALL NOT BE SUBSTITUTED FOR THE CONCRETE ROLLERS. COST OF CONCRETE ROLLERS SHALL BE INCLUDED IN THE UNIT PRICE BID PER LINEAR FEET OF DRILLED SHAFT.

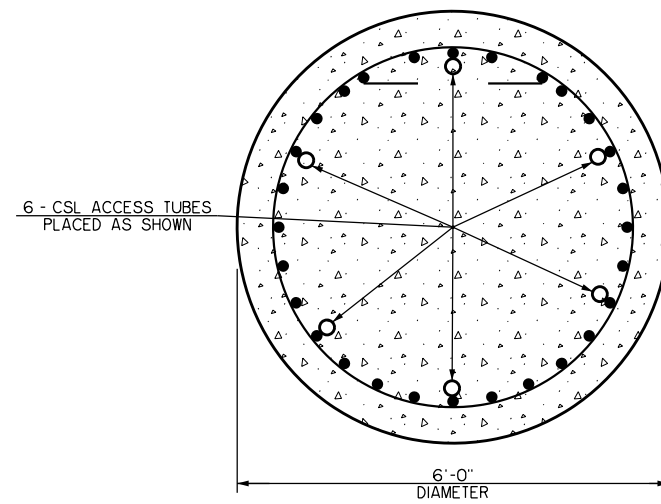


SECTION B-B

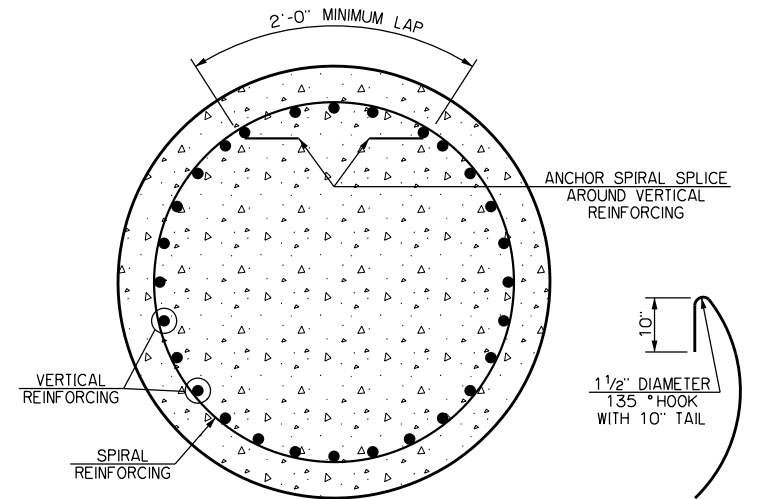
SECTION C-C



(1) FOR PEDESTALS (C), (D) AND (E) ONLY

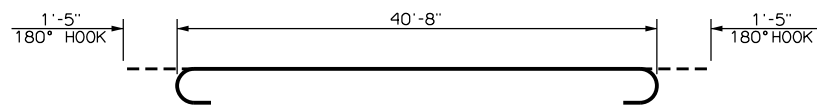


SECTION THROUGH DRILLED SHAFT SHOWING PLACEMENT OF CROSSHOLE SONIC LOGGING ACCESS TUBES



SECTION THROUGH COLUMN OR DRILLED SHAFT **DETAIL OF 135° HOOK**

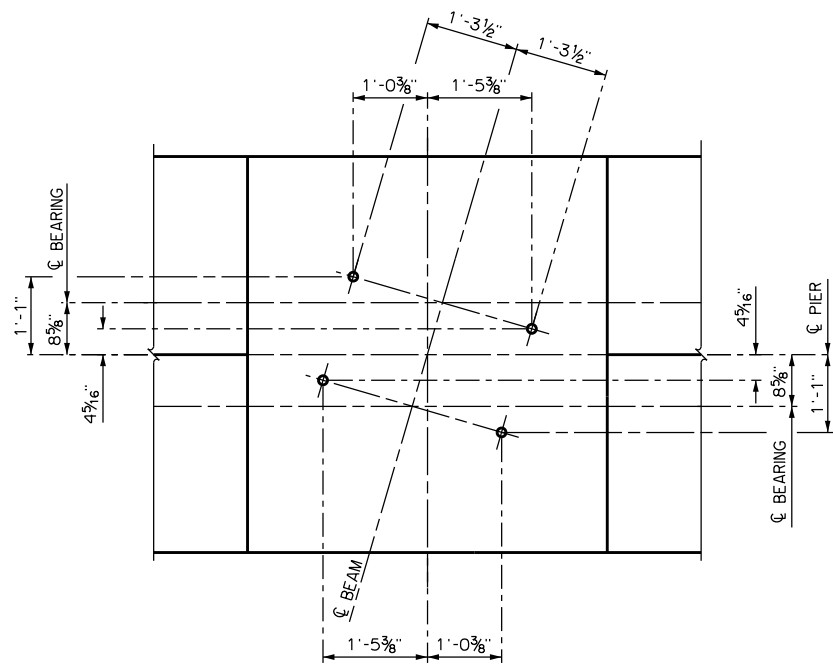
DETAILS OF SPIRAL REINFORCING STEEL SPLICE
 SPIRAL BARS SHALL CONFORM TO AASHTO M32. SPIRAL BAR LENGTH DOES NOT INCLUDE LAP. IF LAP IS REQUIRED, THE LENGTH OF THE LAP SHALL BE AS SHOWN.



DETAILS OF BENT REINFORCING STEEL

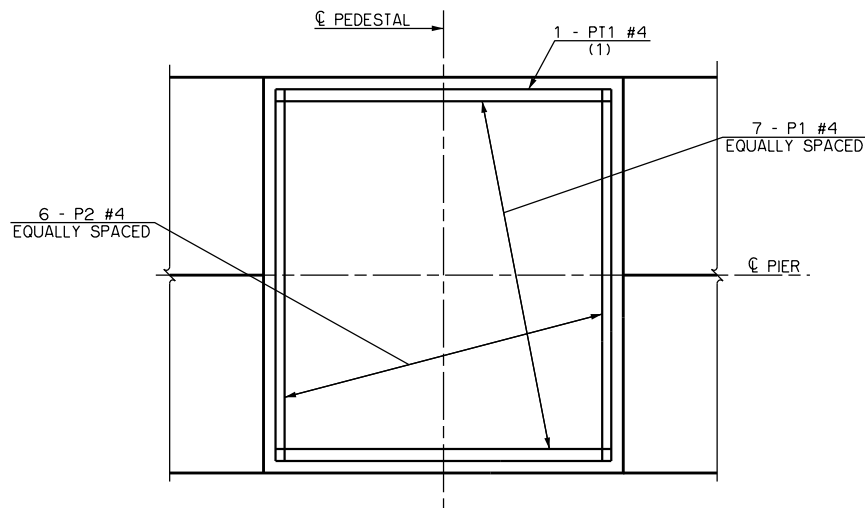
SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
 BRIDGE 'H'

DETAILS OF PIERS
 (SHEET NO. 2 OF 3)

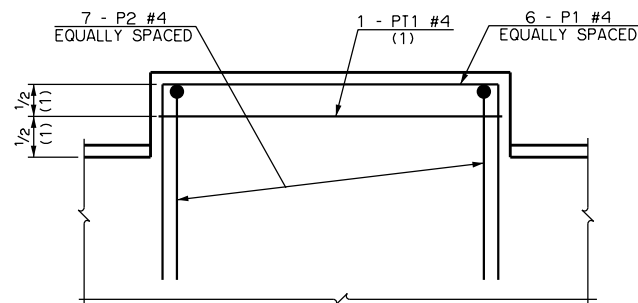


PLAN OF PEDESTAL WITH ANCHOR BOLT LAYOUT

ANCHOR BOLTS SHALL BE 1 1/2" DIAMETER. SEE "DETAIL OF ANCHOR BOLT ASSEMBLY" ON SHEET B060 FOR DETAILS (TYPICAL AT EACH PEDESTAL)



LAYOUT OF PEDESTAL REINFORCING STEEL



ELEVATION OF PEDESTALS

(1) FOR PEDESTALS (C), (D) AND (E) ONLY

| BAR LIST - PIER NO. 1 | | | | |
|------------------------|------|-----|------|---------|
| MARK | SIZE | NO. | FORM | LENGTH |
| EPOXY COATED | | | | |
| BH1 | #10 | 6 | BNT. | 43'-6" |
| BH2 | #10 | 9 | STR. | 40'-8" |
| BH3 | #5 | 12 | STR. | 40'-8" |
| S1 | #5 | 34 | BNT. | 19'-7" |
| B1 | #4 | 8 | BNT. | 9'-4" |
| B2 | #4 | 14 | BNT. | 8'-8" |
| P1 | #4 | 35 | BNT. | 9'-7" |
| P2 | #4 | 30 | BNT. | 10'-1" |
| PT1 | #4 | 3 | BNT. | 20'-5" |
| UNCOATED | | | | |
| CS1 | W20 | 2 | BNT. | 656'-2" |
| TWO DRILLED SHAFTS (2) | | | | |
| EPOXY COATED | | | | |
| C1 | #11 | 40 | STR. | 34'-3" |
| UNCOATED | | | | |
| D1 | #11 | 56 | STR. | 22'-6" |
| DS1 | W20 | 2 | BNT. | 754'-5" |
| T1 | #4 | 14 | BNT. | 16'-6" |

(1)

(2) INCLUDED IN PRICE BID PER LINEAR FOOT OF DRILLED SHAFT

| BAR LIST - PIER NO. 2 | | | | |
|------------------------|------|-----|------|---------|
| MARK | SIZE | NO. | FORM | LENGTH |
| EPOXY COATED | | | | |
| BH1 | #10 | 6 | BNT. | 43'-6" |
| BH2 | #10 | 9 | STR. | 40'-8" |
| BH3 | #5 | 12 | STR. | 40'-8" |
| S1 | #5 | 34 | BNT. | 19'-7" |
| B1 | #4 | 8 | BNT. | 9'-4" |
| B2 | #4 | 14 | BNT. | 8'-8" |
| P1 | #4 | 35 | BNT. | 9'-7" |
| P2 | #4 | 30 | BNT. | 10'-1" |
| PT1 | #4 | 3 | BNT. | 20'-5" |
| UNCOATED | | | | |
| CS1 | W20 | 2 | BNT. | 526'-9" |
| TWO DRILLED SHAFTS (2) | | | | |
| EPOXY COATED | | | | |
| C1 | #11 | 40 | STR. | 29'-6" |
| UNCOATED | | | | |
| D1 | #11 | 56 | STR. | 27'-6" |
| DS1 | W20 | 2 | BNT. | 911'-6" |
| T1 | #4 | 14 | BNT. | 16'-6" |

(1)

(2) INCLUDED IN PRICE BID PER LINEAR FOOT OF DRILLED SHAFT

| BAR LIST - PIER NO. 3 | | | | |
|------------------------|------|-----|------|------------|
| MARK | SIZE | NO. | FORM | LENGTH |
| EPOXY COATED | | | | |
| BH1 | #10 | 6 | BNT. | 43'-6" |
| BH2 | #10 | 9 | STR. | 40'-8" |
| BH3 | #5 | 12 | STR. | 40'-8" |
| S1 | #5 | 34 | BNT. | 19'-7" |
| B1 | #4 | 8 | BNT. | 9'-4" |
| B2 | #4 | 14 | BNT. | 8'-8" |
| P1 | #4 | 35 | BNT. | 9'-7" |
| P2 | #4 | 30 | BNT. | 10'-1" |
| PT1 | #4 | 3 | BNT. | 20'-5" |
| UNCOATED | | | | |
| CS1 | W20 | 2 | BNT. | 445'-0" |
| TWO DRILLED SHAFTS (2) | | | | |
| EPOXY COATED | | | | |
| C1 | #11 | 40 | STR. | 26'-6" |
| UNCOATED | | | | |
| D1 | #11 | 56 | STR. | 30'-6" |
| DS1 | W20 | 2 | BNT. | 1,005'-10" |
| T1 | #4 | 14 | BNT. | 16'-6" |

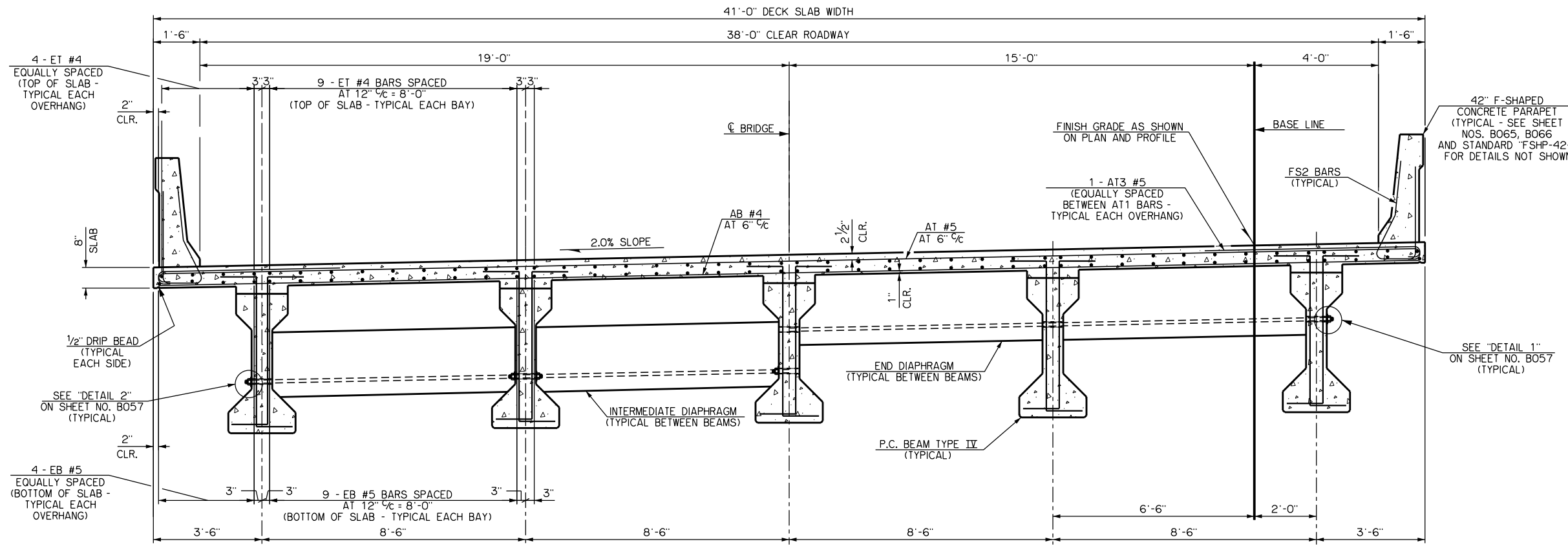
(1)

(2) INCLUDED IN PRICE BID PER LINEAR FOOT OF DRILLED SHAFT

| SUMMARY OF QUANTITIES FOR PIERS | | | | | |
|--------------------------------------|------|------------|------------|------------|-----------|
| ITEM | UNIT | PIER NO. 1 | PIER NO. 2 | PIER NO. 3 | TOTAL |
| CLASS A CONCRETE | CY | 94.50 | 87.50 | 83.10 | 265.10 |
| REINFORCING STEEL | LB | 900.00 | 720.00 | 610.00 | 2,230.00 |
| EPOXY COATED REINFORCING STEEL | LB | 4,500.00 | 4,500.00 | 4,500.00 | 13,500.00 |
| WATER REPELLENT (VISUALLY INSPECTED) | SY | 94.00 | 94.00 | 94.00 | 282.00 |
| DRILLED SHAFTS 72" DIAMETER | LF | 46.00 | 56.00 | 62.00 | 164.00 |

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

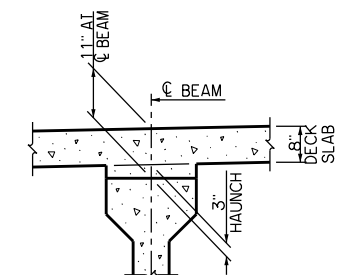
**DETAILS OF PIERS
(SHEET NO. 3 OF 3)**



TYPICAL SECTION

SCHEDULE OF EXPANSION JOINT OPENING SIZE WITH TEMPERATURE

| TEMPERATURE (4) | OPENING AT PIER NOS. 1 | OPENING AT PIER NO. 3 |
|-----------------|------------------------|-----------------------|
| 0°F | 2 5/8" | 2 1/2" |
| 10°F | 2 1/2" | 2 3/8" |
| 20°F | 2 3/8" | 2 1/4" |
| 30°F | 2 1/4" | 2 1/8" |
| 40°F | 2" | 2" |
| 50°F | 1 7/8" | 1 7/8" |
| 60°F | 1 3/4" | 1 3/4" |
| 70°F | 1 5/8" | 1 5/8" |
| 80°F | 1 1/2" | 1 5/8" |
| 90°F | 1 3/8" | 1 1/2" |
| 100°F | 1 1/4" | 1 3/8" |
| 110°F | 1 1/8" | 1 1/4" |



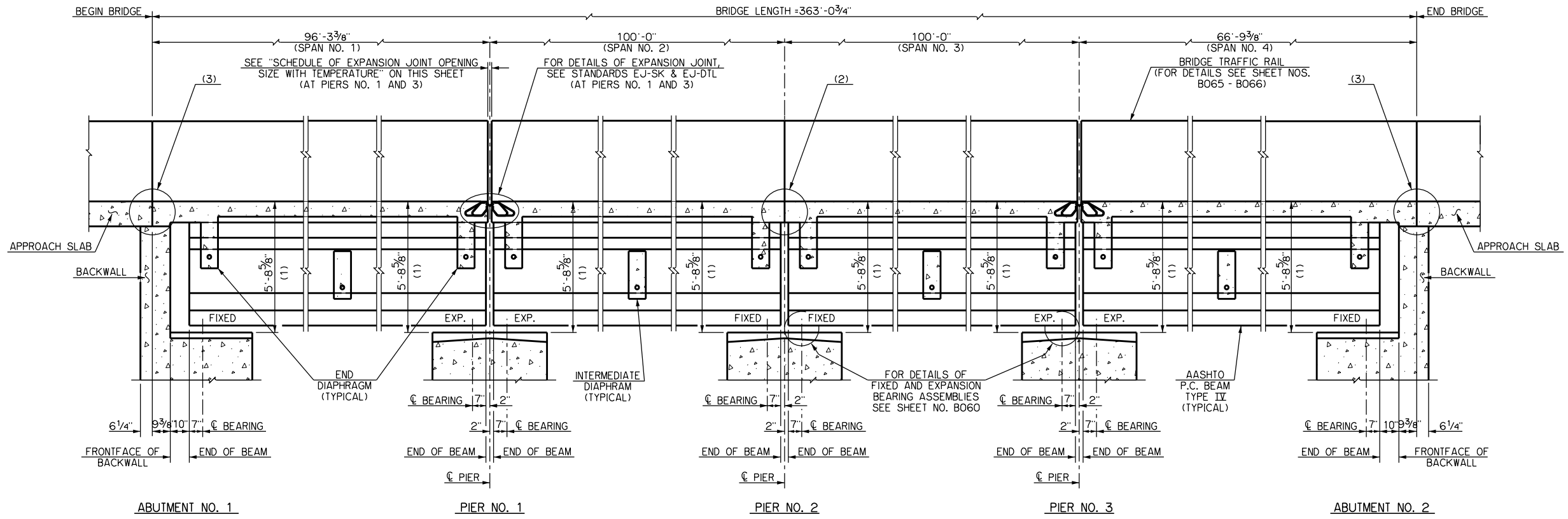
AT SPAN NO. 1, 2, 3, AND 4

DETAILS OF HAUNCH

NOTE: PLAN QUANTITIES FOR CLASS 'AA' CONCRETE INCLUDES 13.9 CUBIC YARDS FOR HAUNCHES OVER BEAMS. HAUNCH HEIGHT SHOWN IS PLANNED HEIGHT AT CENTERLINE OF BEARING ONLY, AND VARIES ACROSS THE SPAN. HAUNCH HEIGHT TO BE DETERMINED AFTER ERECTION OF BEAMS TO PROVIDE FOR DEAD LOAD DEFLECTION AND GRADE ADJUSTMENT, BUT THE PAY QUANTITY WILL BE AS SHOWN. FOR DEAD LOAD DEFLECTIONS SEE P.C. BEAM DETAIL SHEET NOS. B061, B062 AND B063.

NOTES

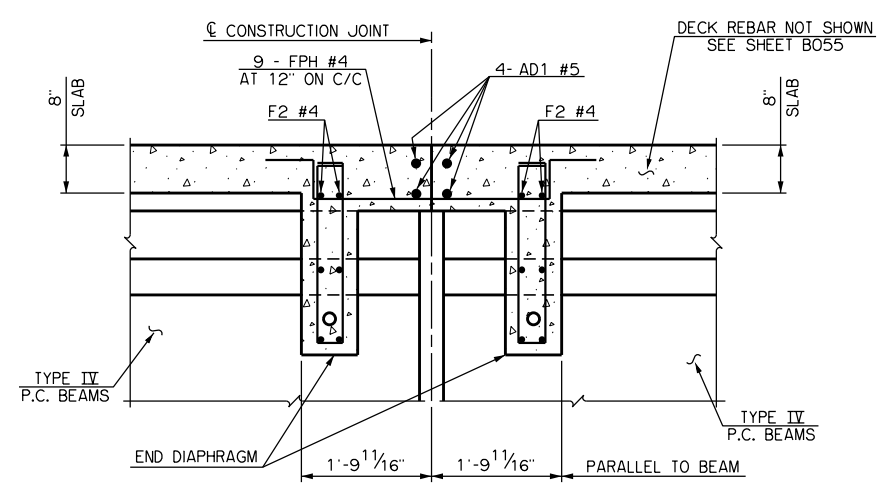
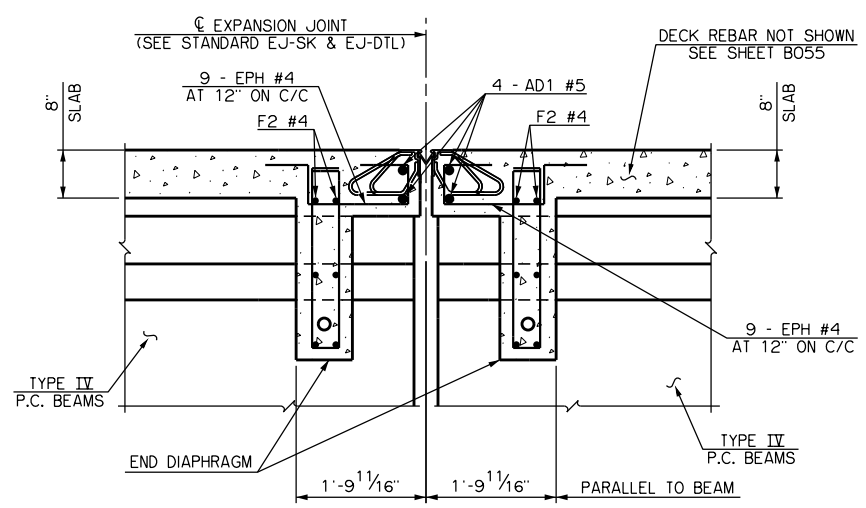
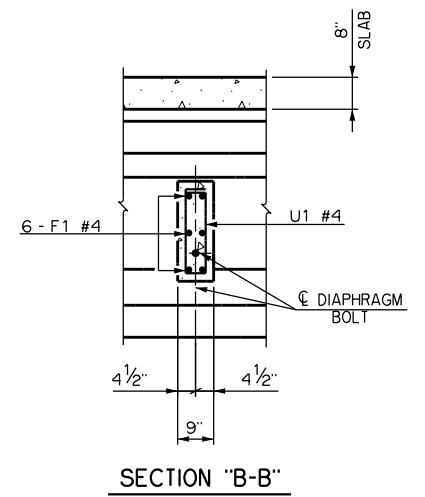
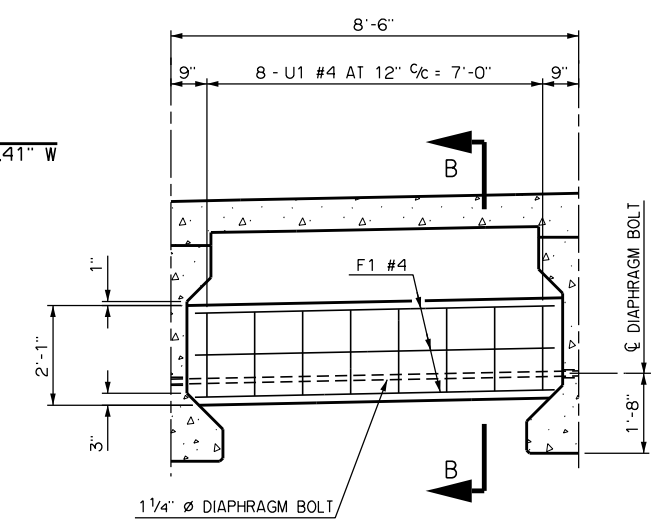
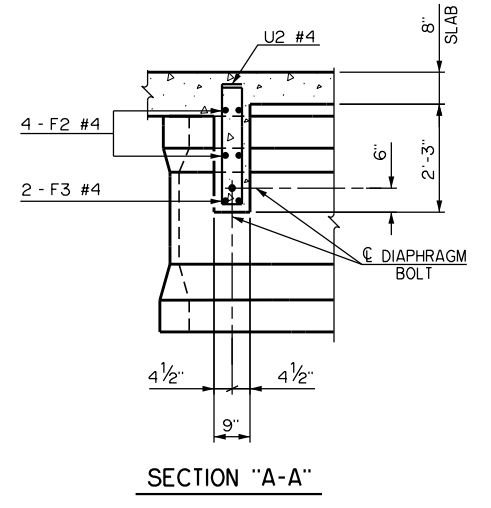
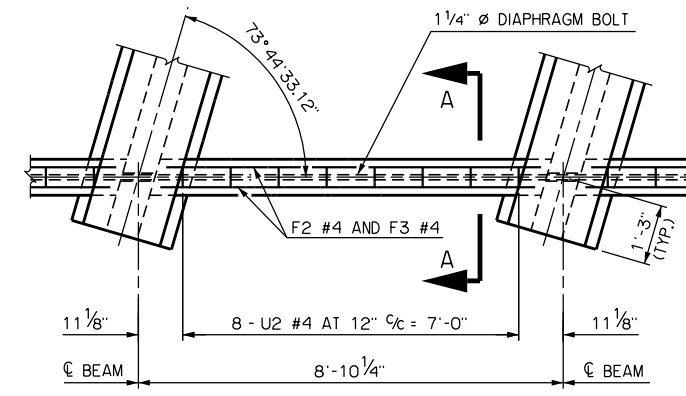
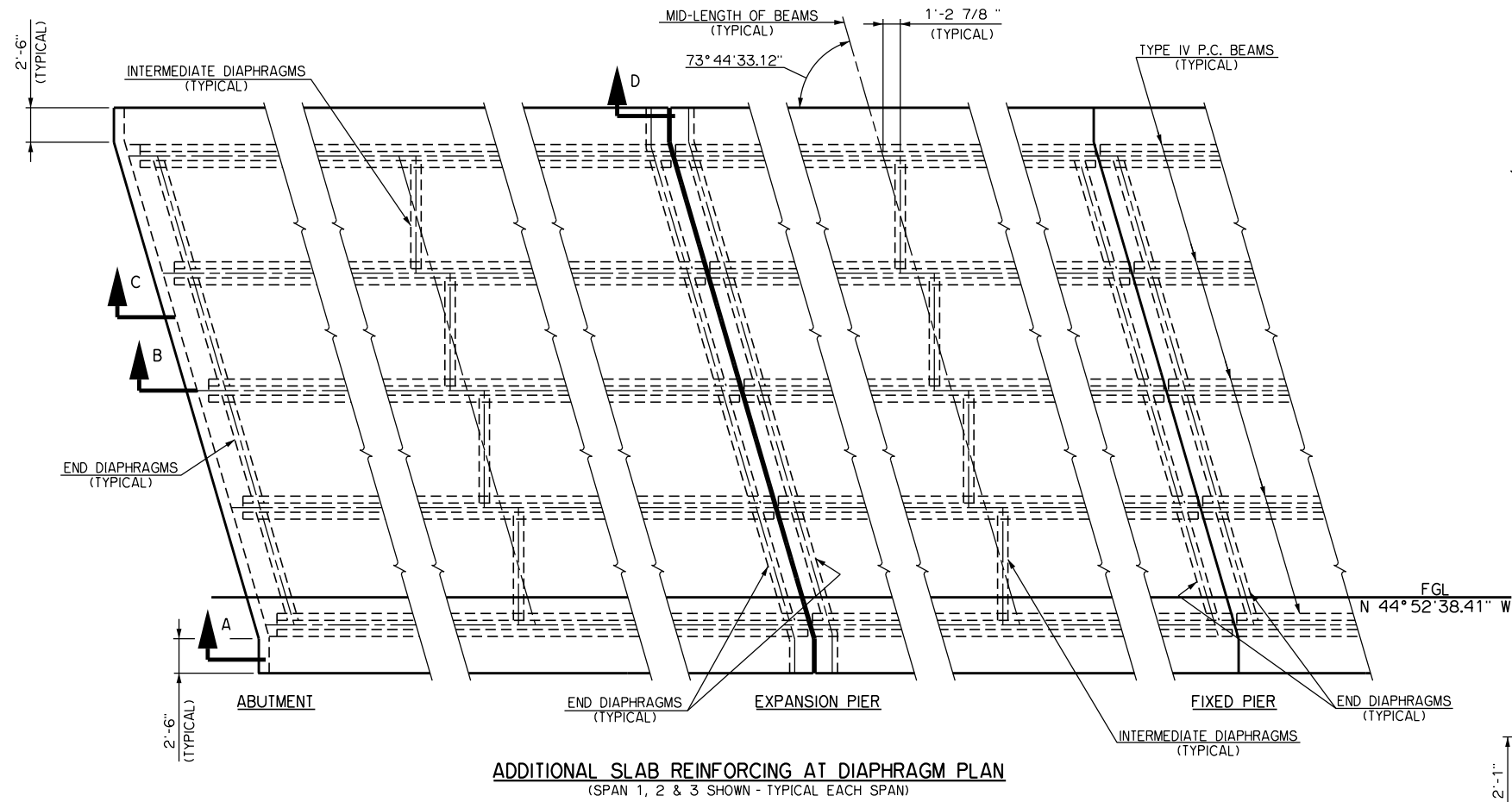
- (1) DIMENSION IS FROM TOP OF SLAB TO BOTTOM OF BEARING ASSEMBLY AT \bar{C} BEARING.
 - (2) UNDER NO CIRCUMSTANCES SHALL THE SLAB BE A CONTINUOUS POUR OVER THE FIXED PIER NOS. 2. THIS LOCATION SHALL HAVE A CONSTRUCTION JOINT IN THE SLAB AND SHALL BE SEALED WITH SEALER RESIN (SEE GENERAL NOTE ON SHEET NO. A02). THE LONGITUDINAL REINFORCING STEEL SHALL BE CONTINUOUS THRU THE CONSTRUCTION JOINT.
 - (3) UNDER NO CIRCUMSTANCES SHALL THE DECK SLAB AND THE APPROACH SLAB OVER THE BACKWALLS OF ABUTMENT NOS. 1 AND 2 BE A CONTINUOUS POUR. THESE LOCATIONS SHALL HAVE A SAWS AND SEALED CONSTRUCTION JOINT IN THE SLAB. SEE "DETAILS OF APPROACH SLABS" ON SHEET NO. B064 FOR DETAILS OF THIS JOINT.
 - (4) AMBIENT AIR TEMPERATURE AT THE TIME THE DECK SLAB CONCRETE IS POURED.
- ROTATE HOOKS ON AT BARS TO MAINTAIN MINIMUM CLEARANCE.
- ALL DIMENSIONS SHOWN IN THE LONGITUDINAL SECTION ARE ALONG THE \bar{C} OF THE P.C. BEAM.
- DO NOT SAW CUT GROOVE WITHIN 6" OF ALL CONSTRUCTION JOINTS.



LONGITUDINAL SECTION

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD BRIDGE 'H'

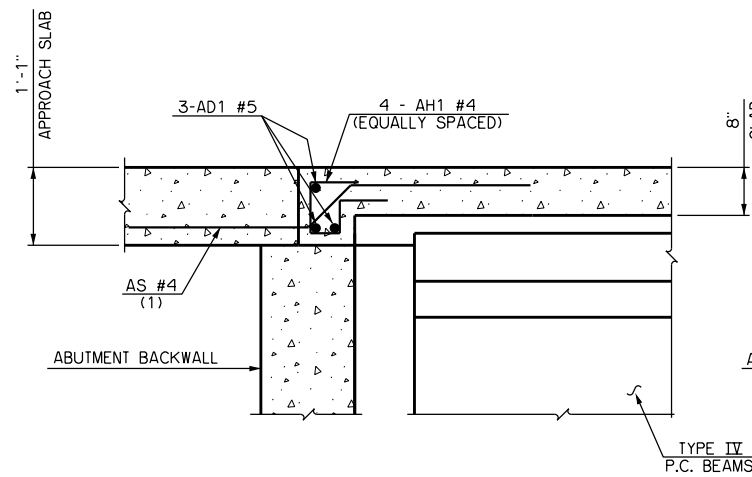
DETAILS OF SUPERSTRUCTURE (SHEET NO. 1 OF 5)



NOTE:
FOR SECTION A, B, C AND D SEE SHEET NO. B057

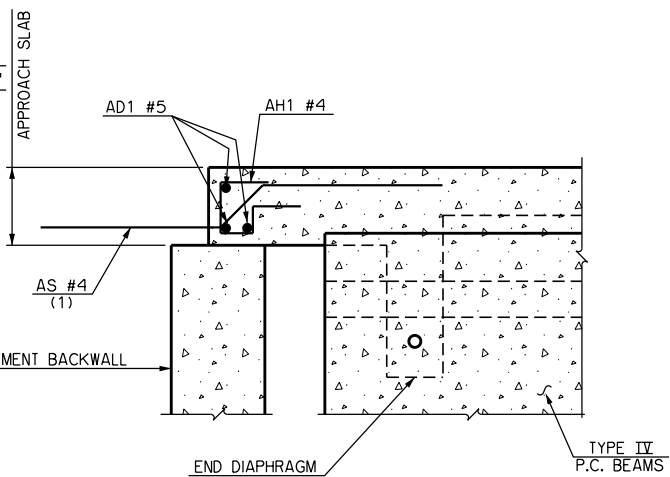
SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

DETAILS OF SUPERSTRUCTURE
(SHEET NO. 2 OF 5)

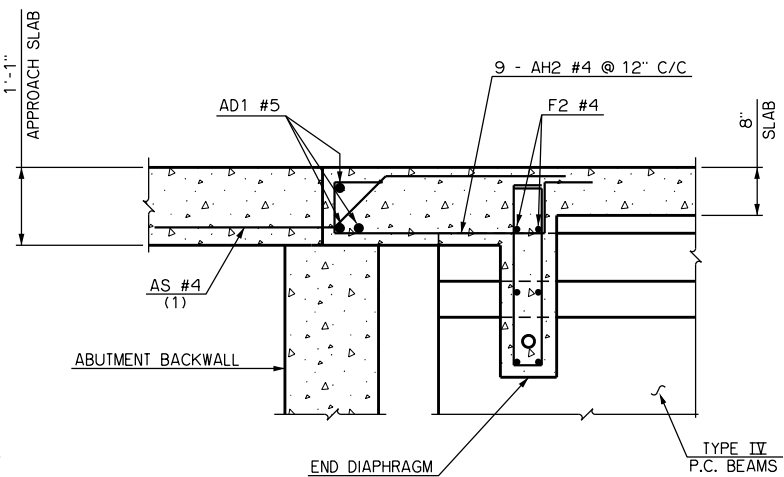


SECTION "A"
AT OVERHANG

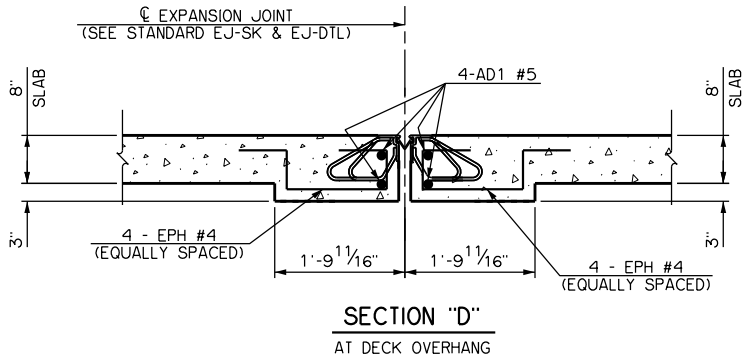
(1) AS BARS SHALL BE TIED TO THE TOP MAT OF REINFORCING IN THE BRIDGE SLAB AND TO THE BOTTOM MAT OF REINFORCING IN THE APPROACH SLAB. AS BARS MUST BE IN PLACE PRIOR TO POURING THE DECK SLAB CONCRETE.



SECTION "B"
AT C BEAMS



SECTION "C"
AT BETWEEN BEAMS

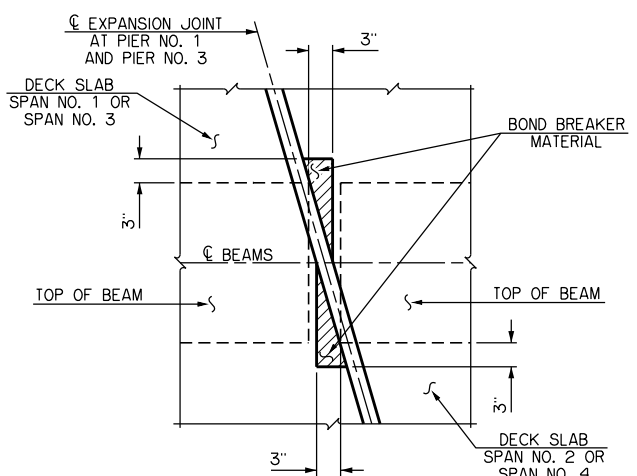


SECTION "D"
AT DECK OVERHANG

BAR LIST - SUPERSTRUCTURE

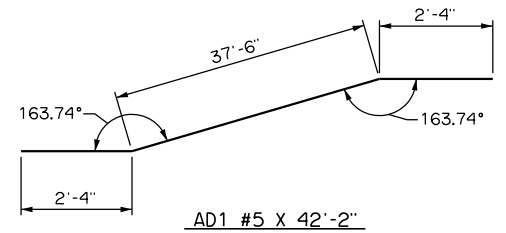
| MARK | SIZE | NO. | FORM | LENGTH | LENGTH VARIATION |
|--------------|------|-----|------|---------------|------------------|
| EPOXY COATED | | | | | |
| AB1 | #4 | 637 | STR. | 40'-8" | |
| AB2 | #4 | 168 | STR. | 21'-2" (AVG.) | 4'-0" TO 38'-4" |
| AD1 | #5 | 18 | BNT. | 42'-2" | |
| AS | #4 | 84 | BNT. | 5'-11" | |
| AT1 | #5 | 637 | STR. | 40'-8" | |
| AT2 | #5 | 168 | STR. | 21'-2" (AVG.) | 4'-0" TO 38'-4" |
| AT3 | #5 | 657 | BNT. | 8'-10" | |
| EB1 | #5 | 44 | STR. | 99'-0" | |
| EB2 | #5 | 44 | STR. | 208'-7" | |
| EB3 | #5 | 44 | STR. | 69'-6" | |
| ET1 | #4 | 44 | STR. | 98'-0" | |
| ET2 | #4 | 44 | STR. | 205'-7" | |
| ET3 | #4 | 44 | STR. | 68'-6" | |
| F1 | #4 | 96 | STR. | 7'-6" | |
| F2 | #4 | 128 | STR. | 7'-0" | |
| F3 | #4 | 64 | STR. | 7'-10" | |
| U1 | #4 | 128 | BNT. | 4'-9" | |
| U2 | #4 | 256 | BNT. | 6'-3" | |
| EPH | #4 | 144 | BNT. | 3'-5" | |
| FPH | #4 | 36 | BNT. | 5'-6" | |
| AH1 | #4 | 26 | BNT. | 3'-1" | |
| AH2 | #4 | 72 | BNT. | 5'-6" | |

- ① INCLUDES EIGHT SETS OF 21 BARS
- ② INCLUDES EIGHT SETS OF 21 BARS
- ③ LENGTH INCLUDES ONE 2'-11" LAP, LAPS SHALL BE STAGGERED
- ④ LENGTH INCLUDES THREE 2'-11" LAPS, LAPS SHALL BE STAGGERED
- ⑤ LENGTH INCLUDES ONE 2'-11" LAP, LAPS SHALL BE STAGGERED
- ⑥ LENGTH INCLUDES ONE 1'-11" LAP, LAPS SHALL BE STAGGERED
- ⑦ LENGTH INCLUDES THREE 1'-11" LAPS, LAPS SHALL BE STAGGERED
- ⑧ LENGTH INCLUDES ONE 1'-11" LAP, LAPS SHALL BE STAGGERED

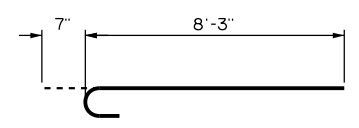


DETAIL OF BOND BREAK AT BEAM CORNER

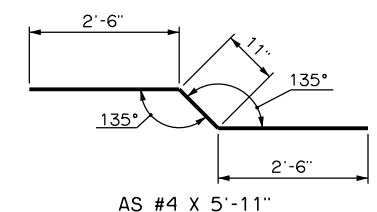
WHERE THE TOP CORNER OF BEAM PROJECTS UNDER THE SLAB OF THE ADJACENT SPAN, A MINIMUM OF 1" CLEARANCE BETWEEN TOP OF BEAM AND BOTTOM OF SLAB SHALL BE PROVIDED IN THE HATCHED AREAS SHOWN ABOVE. ONE-INCH-THICK EXPANSION MATERIAL SHALL BE USED AS A BOND BREAKER. ALL COST OF EXPANSION MATERIAL SHALL BE INCLUDED IN OTHER ITEMS OF WORK.



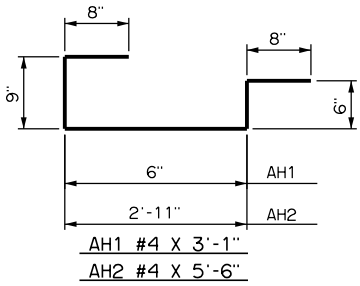
AD1 #5 X 42'-2"



AT3 #5 X 8'-10"

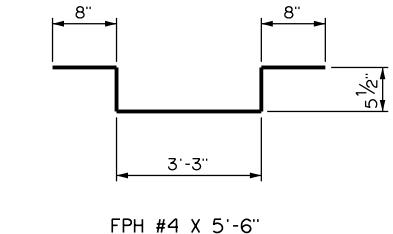


AS #4 X 5'-11"

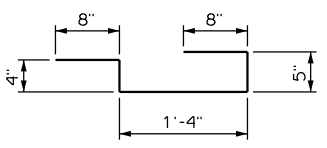


AH1 #4 X 3'-1"

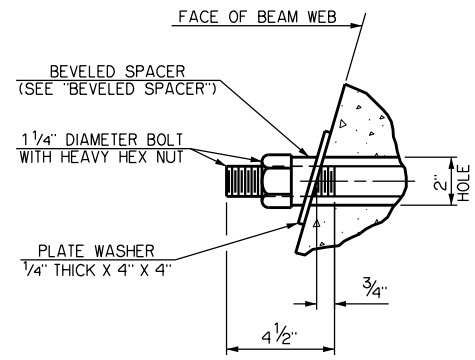
AH2 #4 X 5'-6"



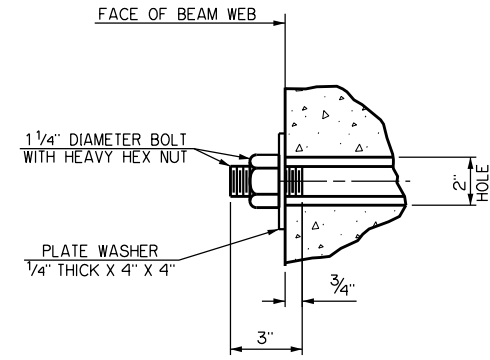
FPH #4 X 5'-6"



EPH #4 X 3'-5"

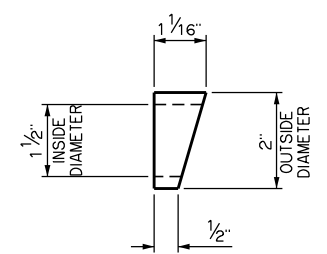


DETAIL 1

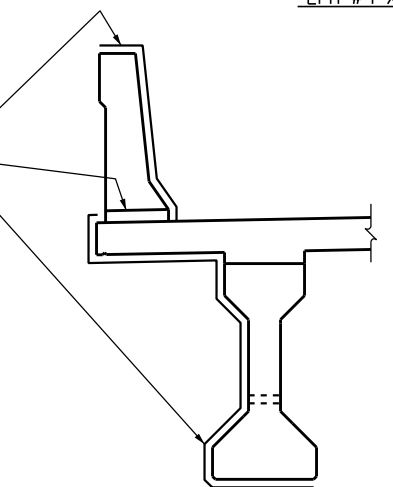


DETAIL 2

SURFACES INDICATED WITH HEAVY LINES SHALL BE TREATED WITH A PENETRATING WATER REPELLENT SURFACE TREATMENT AND THE DRAIN OPENINGS.



BEVELED SPACER
EXTRA STRONG PIPE SLEEVE



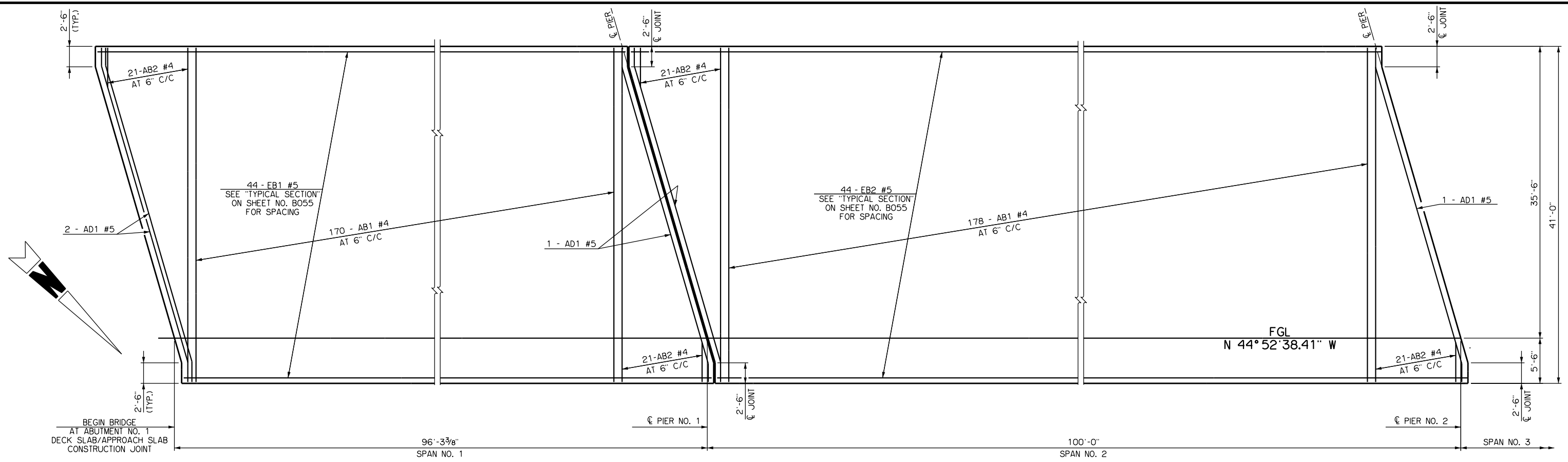
PENETRATING WATER REPELLENT TREATMENT

SUMMARY OF QUANTITIES - SUPERSTRUCTURE

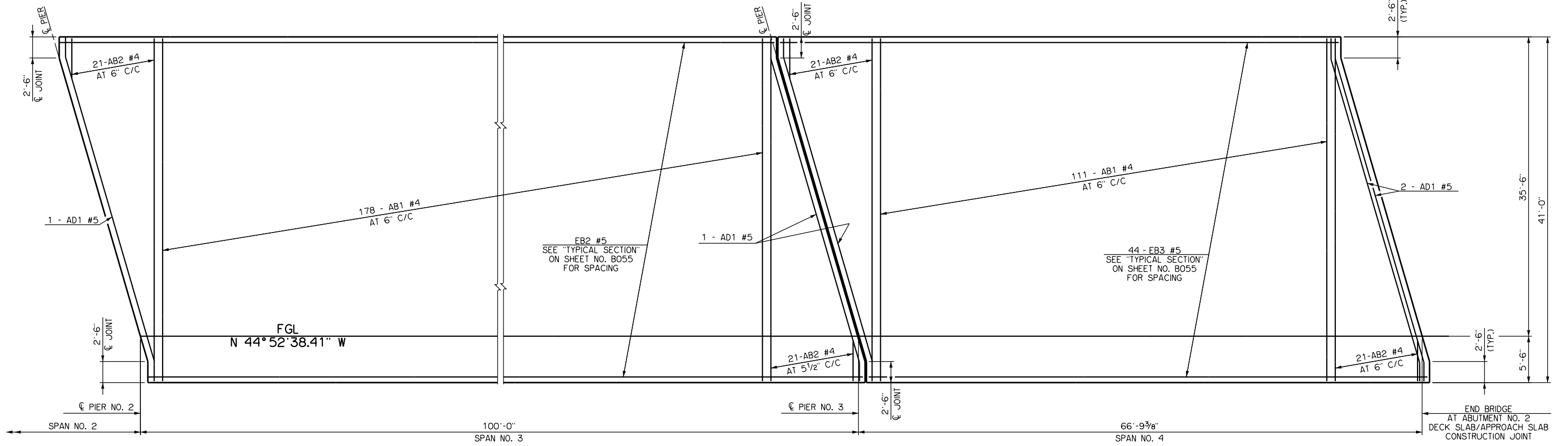
| ITEM | UNIT | TOTAL |
|--|------|-----------|
| PRESTRESSED CONCRETE BEAMS (TYPE IV) | LF | 1,794.17 |
| SAW-CUT GROOVING | SY | 1,514.00 |
| SEALED EXPANSION JOINT | LF | 85.00 |
| 42" F-SHAPED PARAPET | LF | 756.20 |
| STRUCTURAL STEEL | LB | 1,870.00 |
| STAINLESS STEEL FIXED BEARING ASSEMBLY | EA | 20.00 |
| STAINLESS STEEL EXPANSION BEARING ASSEMBLY | EA | 20.00 |
| CLASS AA CONCRETE | CY | 410.00 |
| EPOXY COATED REINFORCING STEEL | LB | 89,500.00 |
| WATER REPELLENT (VISUALLY INSPECTED) | SY | 1,778.00 |
| SEALER CRACK PREPARATION | LF | 128.00 |
| SEALER RESIN | GAL | 1.30 |
| FENCE-STYLE CLF (8' HIGH, CLASS A) | LF | 312.00 |

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

DETAILS OF SUPERSTRUCTURE
(SHEET NO. 3 OF 5)



BOTTOM OF DECK SLAB REINFORCING STEEL LAYOUT



BOTTOM OF DECK SLAB REINFORCING STEEL LAYOUT

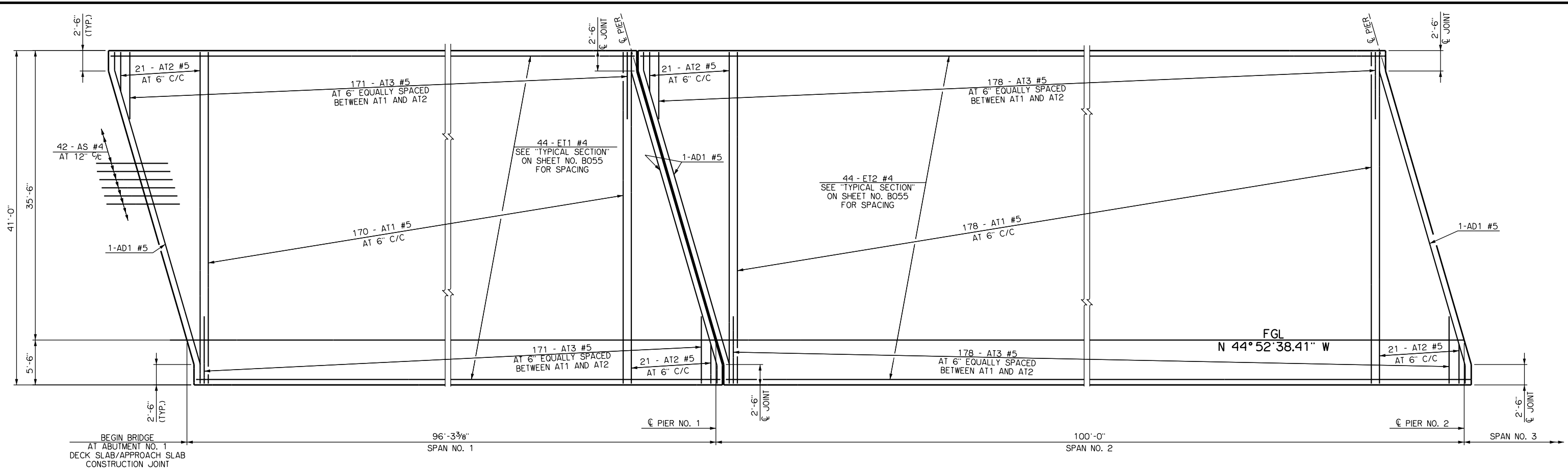
NOTE
 FS1 #5, FS2 #5, AND L #4 BARS PROJECTING FROM DECK SLAB INTO CONCRETE PARAPET HAVE BEEN OMITTED FROM THE BOTTOM OF DECK SLAB REINFORCING LAYOUT FOR CLARITY. SEE STANDARD 42" F-SHAPED CONCRETE PARAPET AND LAYOUT OF 42" F-SHAPED PARAPET ON SHEET NOS. B065 AND B066 FOR PLACEMENT OF BARS.

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
 BRIDGE 'H'

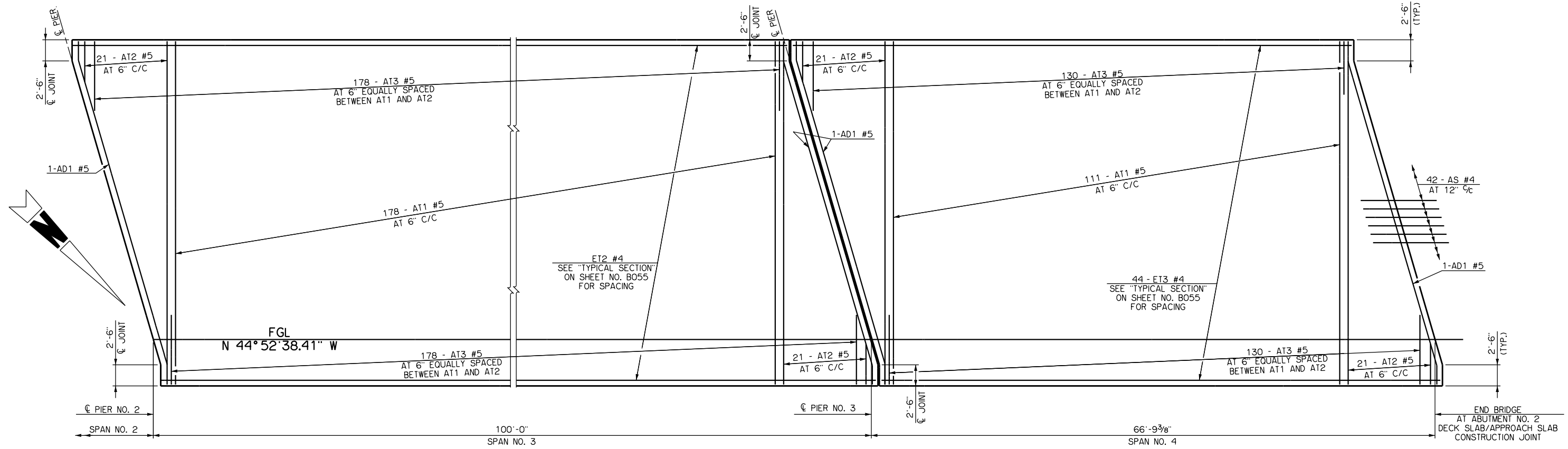
**DETAILS OF SUPERSTRUCTURE
 (SHEET NO. 4 OF 5)**

State Job No. 24428(04) Sheet No. B058

US 81 REALIGNMENT
 GRADY COUNTY



TOP OF DECK SLAB REINFORCING STEEL LAYOUT



TOP OF DECK SLAB REINFORCING STEEL LAYOUT

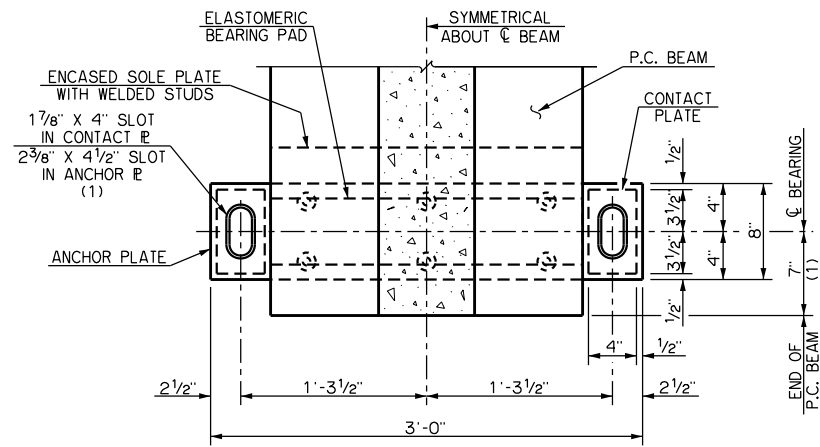
NOTE
 FS1 #5, FS2 #5, AND L #4 BARS PROJECTING FROM DECK SLAB INTO CONCRETE PARAPET HAVE BEEN OMITTED FROM THE TOP OF DECK SLAB REINFORCING LAYOUT FOR CLARITY. SEE STANDARD 42" F-SHAPED CONCRETE PARAPET AND "LAYOUT OF 42" F-SHAPED PARAPET" ON SHEET NO. B065 AND B066 FOR PLACEMENT OF BARS.

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
 BRIDGE 'H'

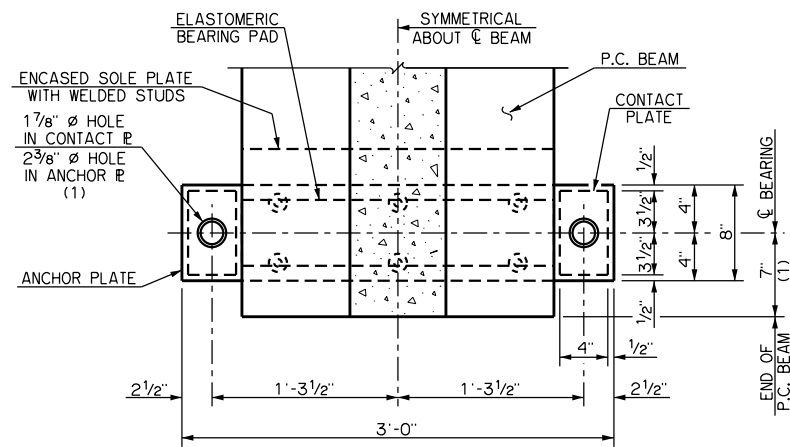
**DETAILS OF SUPERSTRUCTURE
 (SHEET NO. 5 OF 5)**

State Job No. 24428(04) Sheet No. B059

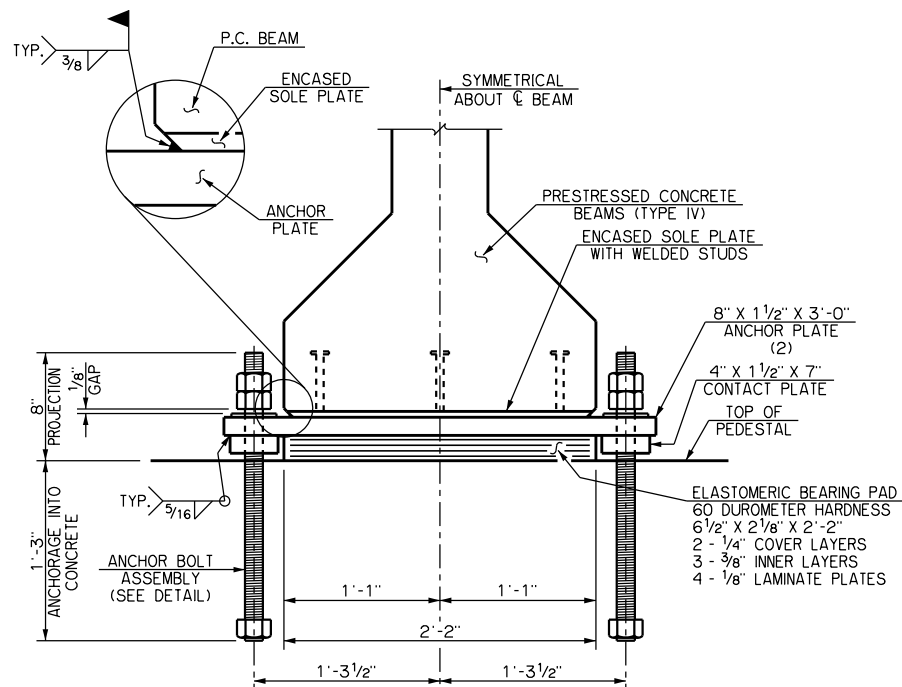
US 81 REALIGNMENT
 GRADY COUNTY



PLAN VIEW AT EXPANSION LOCATIONS
ANCHOR BOLT ASSEMBLIES NOT SHOWN

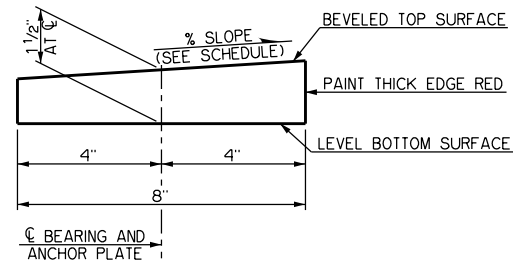


PLAN VIEW AT FIXED LOCATIONS
ANCHOR BOLT ASSEMBLIES NOT SHOWN



END VIEW

DETAILS OF BEARING ASSEMBLY WITHIN SPAN NOS. 1, 2 AND 3

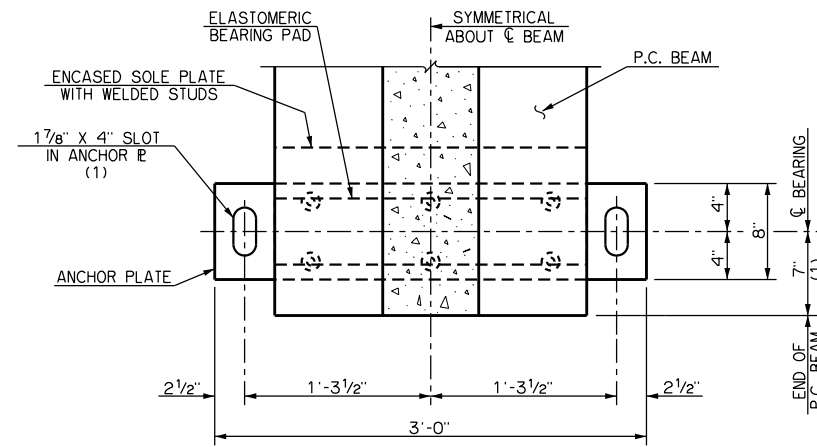


SIDE VIEW

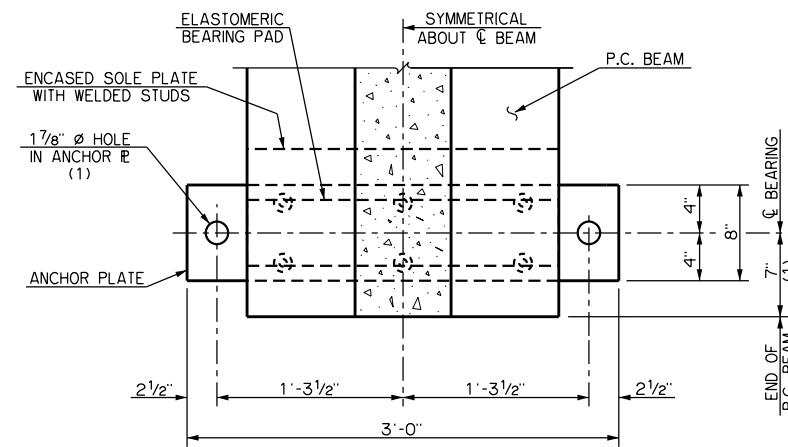
DETAIL OF BEVELED ANCHOR PLATES

| LOCATION | SLOPE | ORIENTATION |
|------------------------------|-------|-------------|
| SPAN NO. 1 AT ABUTMENT NO. 1 | +1.2% | (US) |
| SPAN NO. 1 AT PIER NO. 1 | 0% | - |
| SPAN NO. 2 AT PIER NO. 1 | 0% | - |
| SPAN NO. 2 AT PIER NO. 2 | -1.2% | (DS) |
| SPAN NO. 3 AT PIER NO. 2 | 0% | - |
| SPAN NO. 3 AT PIER NO. 3 | -1.6% | (DS) |
| SPAN NO. 4 AT PIER NO. 3 | 0% | - |
| SPAN NO. 4 AT ABUTMENT NO. 2 | -1.2% | (DS) |

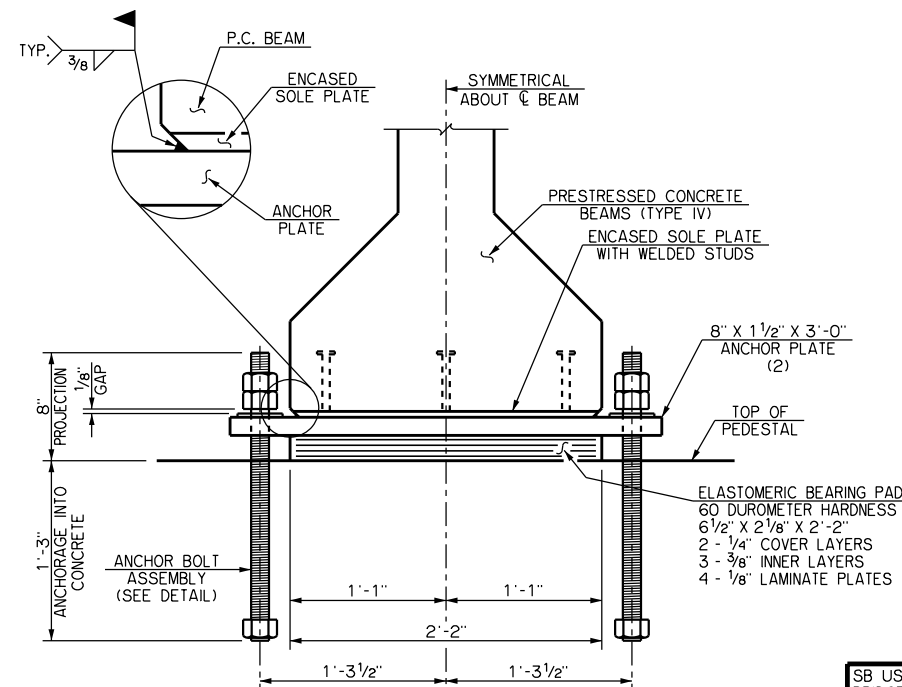
(US) THICK RED EDGE FACES UP STATION TOWARDS END BRIDGE
(DS) THICK RED EDGE FACES DOWN STATION TOWARDS BEGIN BRIDGE



PLAN VIEW AT EXPANSION LOCATIONS
ANCHOR BOLT ASSEMBLIES NOT SHOWN

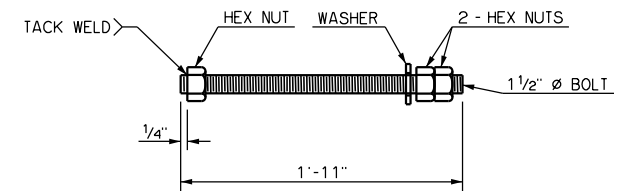


PLAN VIEW AT FIXED LOCATIONS
ANCHOR BOLT ASSEMBLIES NOT SHOWN

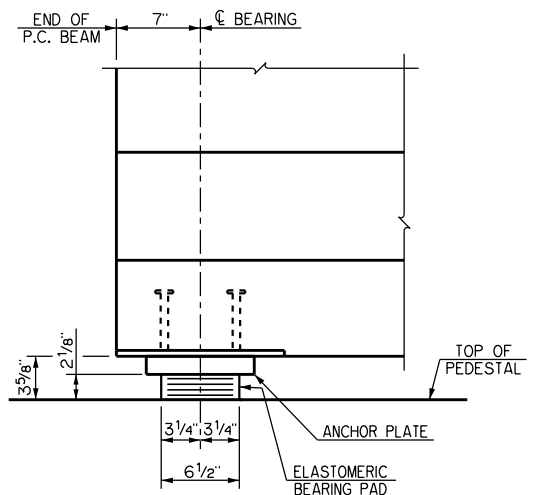


END VIEW

DETAILS OF BEARING ASSEMBLY WITHIN SPAN NOS. 4



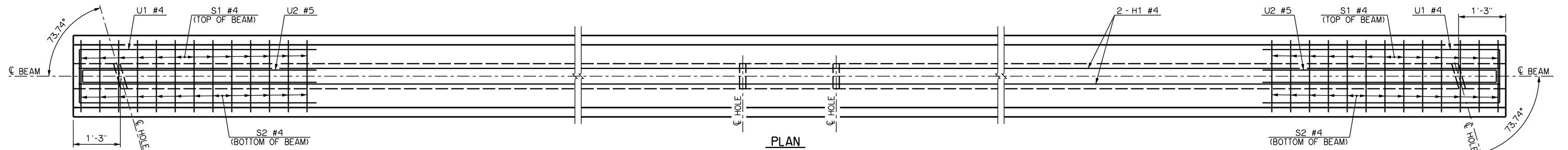
DETAIL OF ANCHOR BOLT ASSEMBLY



SIDE VIEW
ANCHOR BOLT ASSEMBLY OMITTED FOR CLARITY

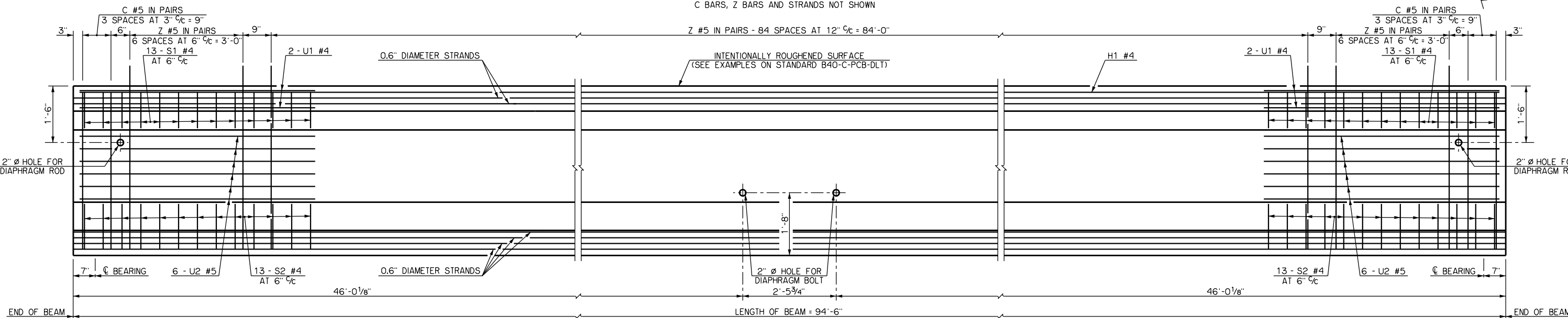
SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

DETAILS OF BEARING ASSEMBLIES



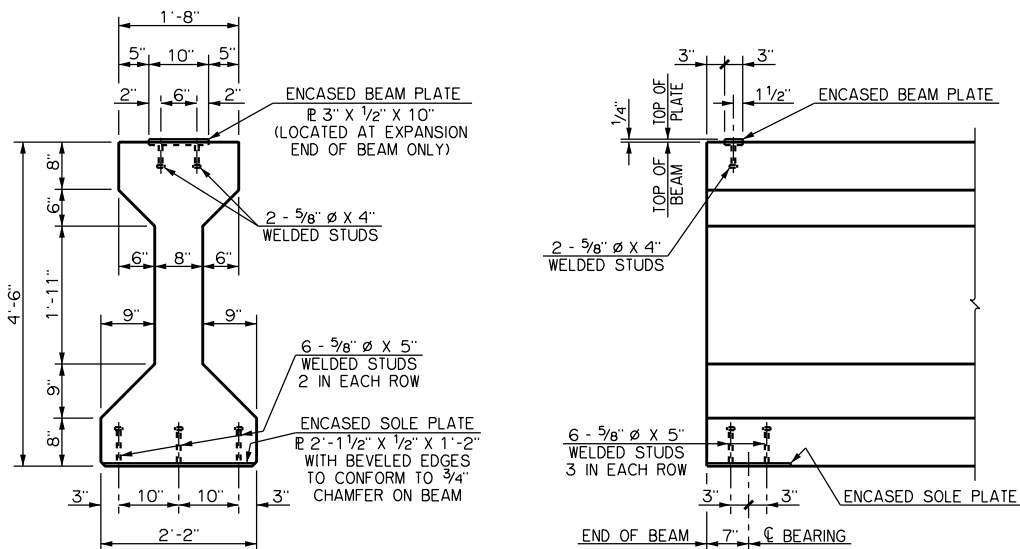
PLAN

C BARS, Z BARS AND STRANDS NOT SHOWN



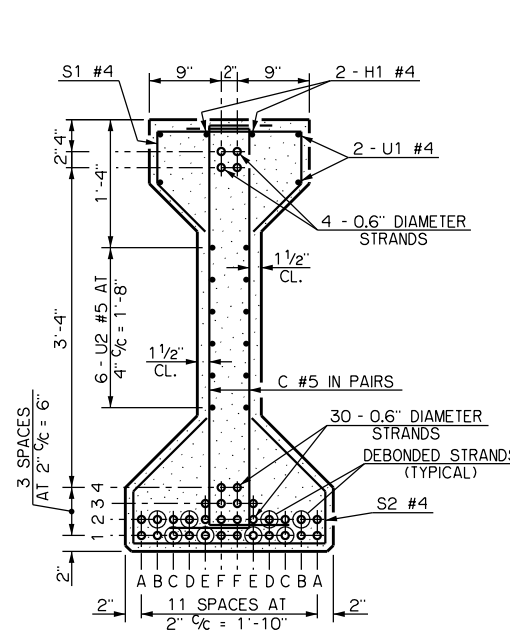
ELEVATION

ENCASED PLATES NOT SHOWN

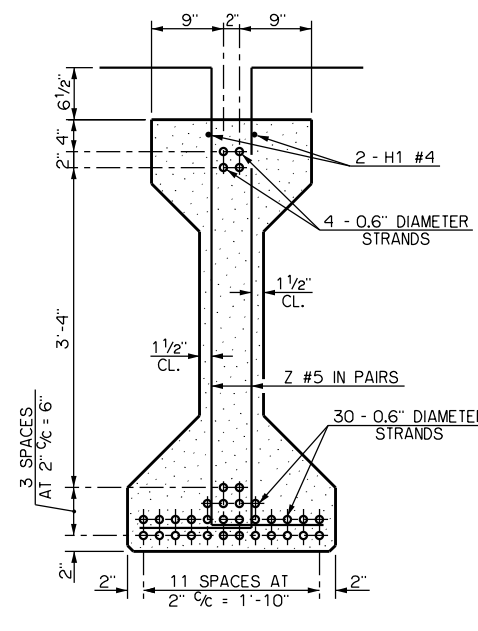


END VIEW

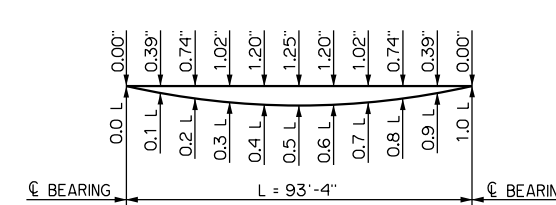
END ELEVATION



END SECTION



SECTION



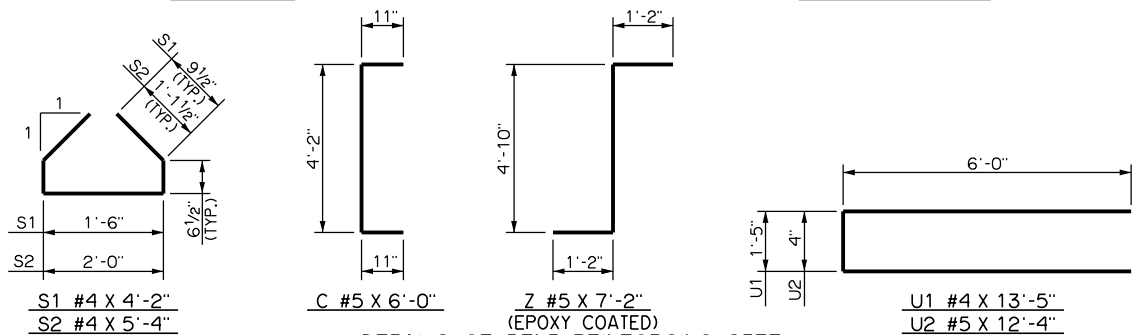
DEAD LOAD DEFLECTIONS

THE DEAD LOAD DEFLECTIONS SHOWN ABOVE AT THE TENTH POINTS ARE THE INITIAL THEORETICAL BEAM DEFLECTIONS DUE TO THE DECK SLAB, HAUNCH, CONCRETE RAIL AND A 5 PSF STEEL STAY-IN-PLACE FORM ALLOWANCE. THE DEAD LOAD DEFLECTIONS SHALL BE ACCOUNTED FOR IN THE HAUNCH DEPTH CALCULATIONS.

MATERIAL PROPERTIES

THE COMPRESSIVE STRENGTH OF THE CONCRETE IN THE P.C. BEAM SHALL BE NO LESS THAN 6,000 PSI AT THE TIME OF TRANSFER OF THE PRESTRESSING FORCE AND NO LESS THAN 8,000 PSI AT 28 DAYS AFTER THE POURING OF THE CONCRETE.

THE TYPE OF PRESTRESSING STRANDS REQUIRED IN THE P.C. BEAM SHALL BE LOW RELAXATION 7-WIRE STRAND WITH A NOMINAL DIAMETER OF 0.6 INCHES AND AN ULTIMATE TENSILE STRENGTH OF 270 KSI.



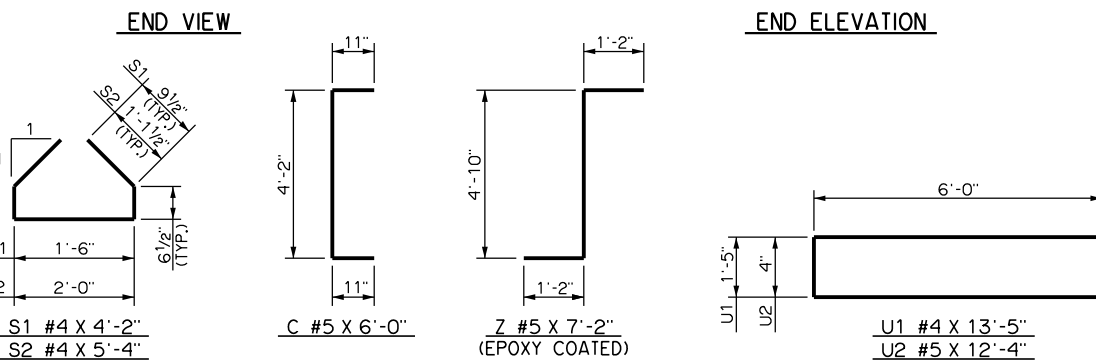
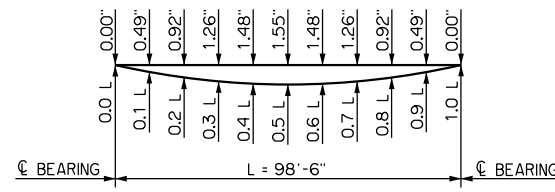
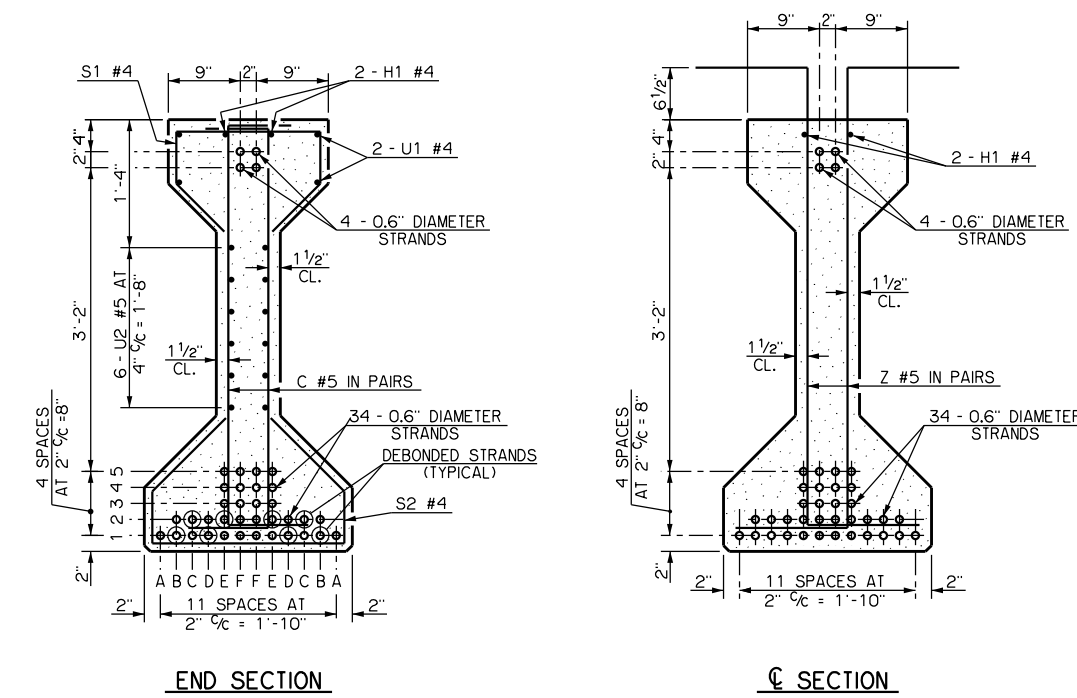
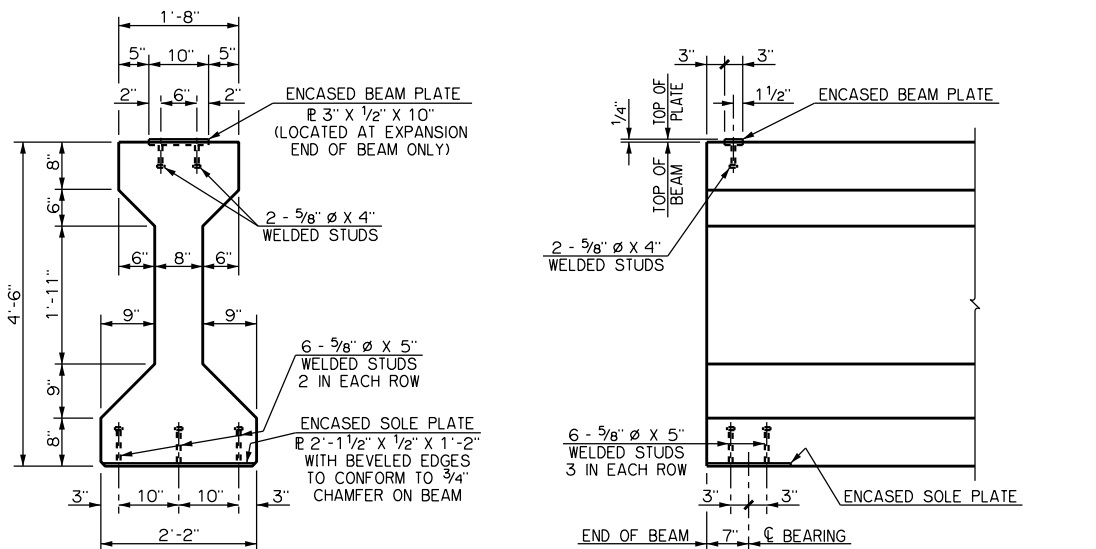
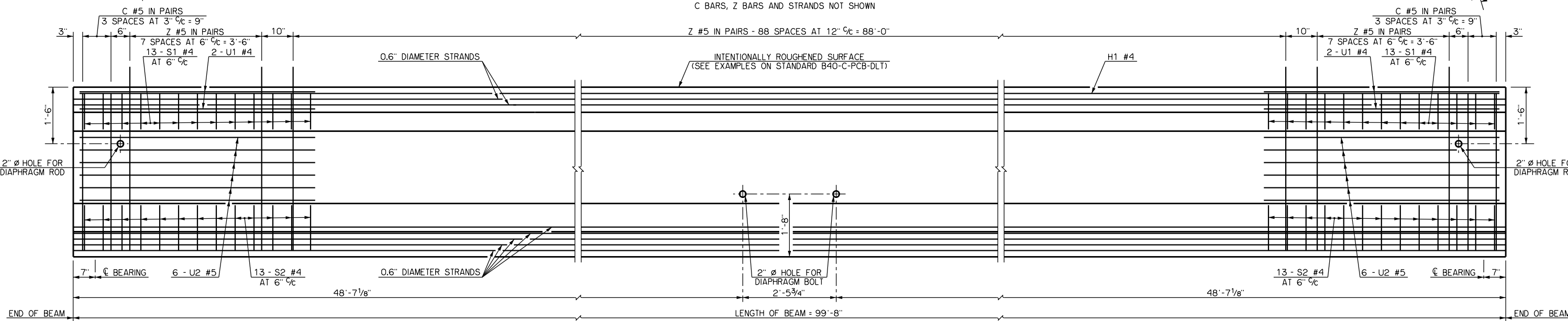
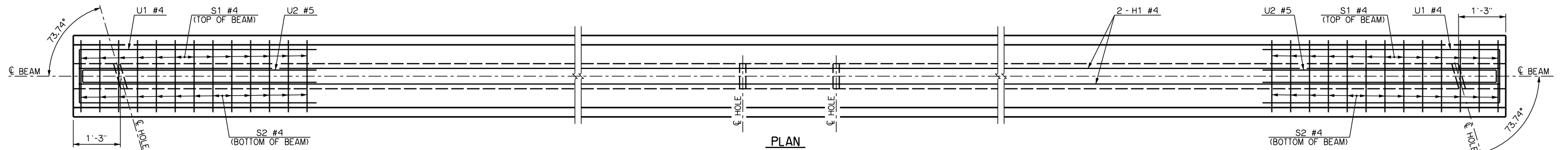
DETAILS OF BENT REINFORCING STEEL

BEAM SECTIONS
(34 - 0.6" Ø STRANDS)

| DEBOND SCHEDULE | |
|-----------------|--------------------------------|
| DEBOND PAIR | DEBOND LENGTH FROM END OF BEAM |
| B2 | 8'-0" |
| C1 | 8'-0" |
| D2 | 4'-0" |
| E1 | 4'-0" |

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD BRIDGE 'H'

DETAILS OF PRESTRESSED CONCRETE BEAMS (TYPE IV) IN SPAN NO. 1



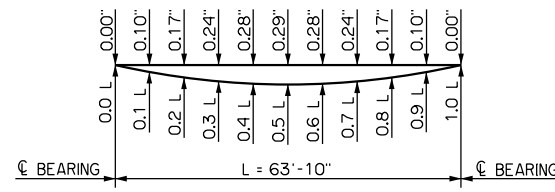
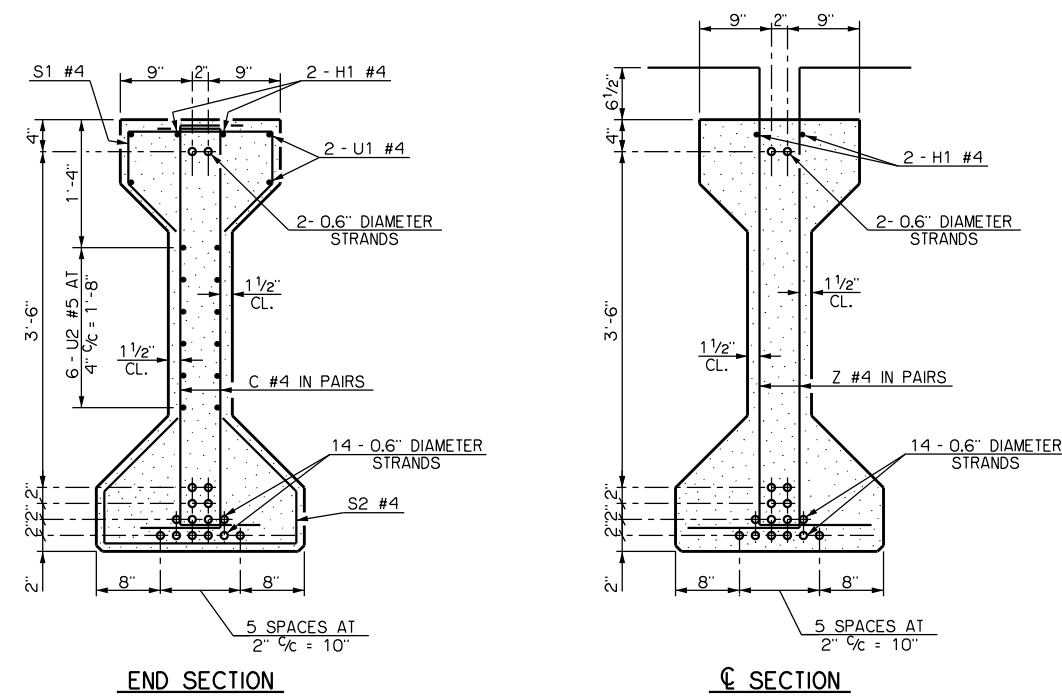
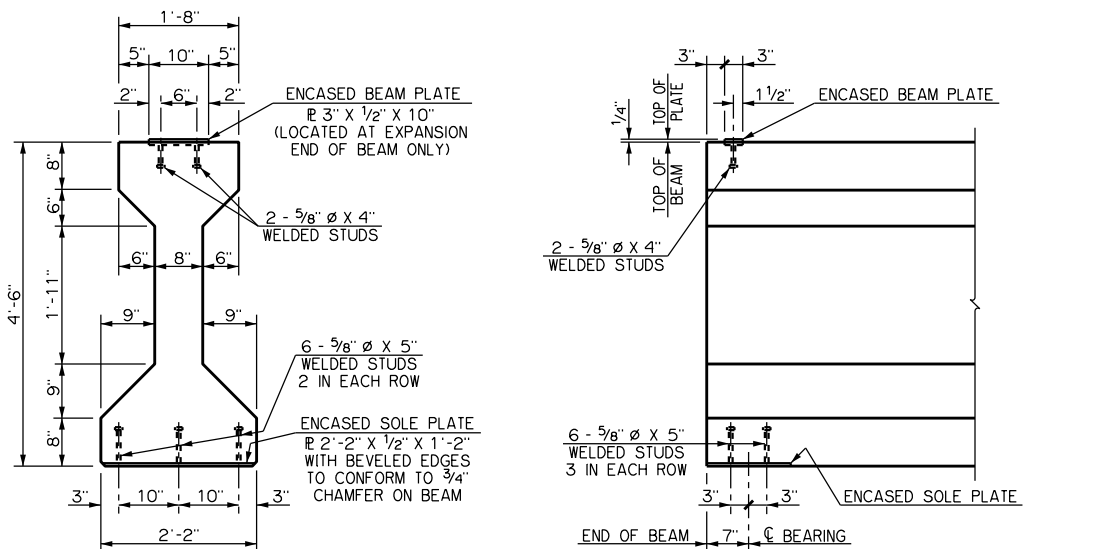
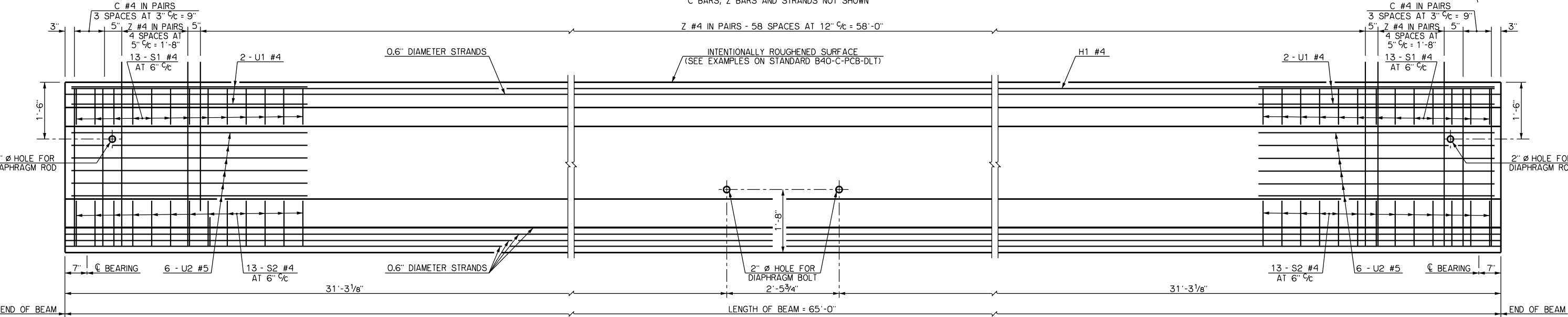
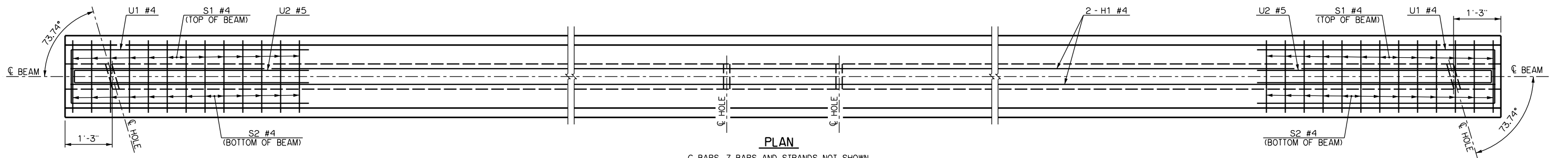
BEAM SECTIONS
(38 - 0.6" Ø STRANDS)

DEBOND SCHEDULE

| DEBOND PAIR | DEBOND LENGTH FROM END OF BEAM |
|-------------|--------------------------------|
| B1 | 12'-0" |
| C2 | 12'-0" |
| D1 | 4'-0" |
| E2 | 4'-0" |

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

DETAILS OF PRESTRESSED CONCRETE BEAMS (TYPE IV) IN SPAN NOS. 2 AND 3

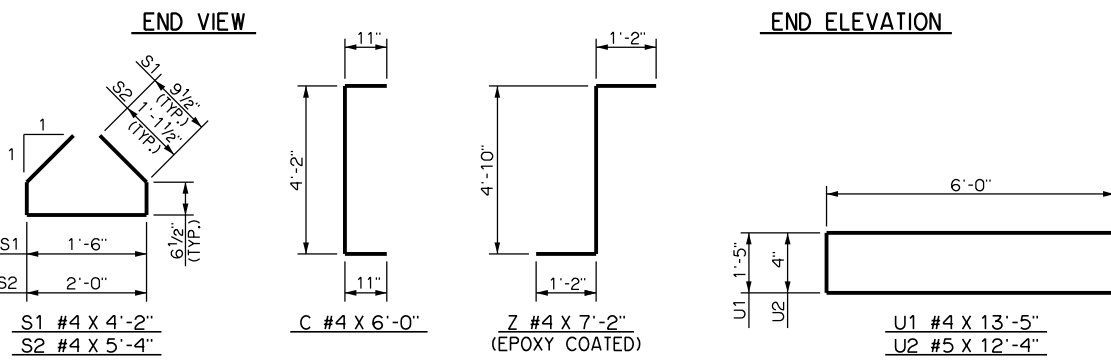


THE DEAD LOAD DEFLECTIONS SHOWN ABOVE AT THE TENTH POINTS ARE THE INITIAL THEORETICAL BEAM DEFLECTIONS DUE TO THE DECK SLAB, HAUNCH, CONCRETE RAIL AND A 5 PSF STEEL STAY-IN-PLACE FORM ALLOWANCE. THE DEAD LOAD DEFLECTIONS SHALL BE ACCOUNTED FOR IN THE HAUNCH DEPTH CALCULATIONS.

MATERIAL PROPERTIES

THE COMPRESSIVE STRENGTH OF THE CONCRETE IN THE P.C. BEAM SHALL BE NO LESS THAN 5,250 PSI AT THE TIME OF TRANSFER OF THE PRESTRESSING FORCE AND NO LESS THAN 7,000 PSI AT 28 DAYS AFTER THE POURING OF THE CONCRETE. THE COARSE AGGREGATE FOR CLASS P CONCRETE SHALL BE FURNISHED IN THE NO. 67 SIZE.

THE TYPE OF PRESTRESSING STRANDS REQUIRED IN THE P.C. BEAM SHALL BE LOW RELAXATION 7-WIRE STRAND WITH A NOMINAL DIAMETER OF 0.6 INCHES AND AN ULTIMATE TENSILE STRENGTH OF 270 KSI.



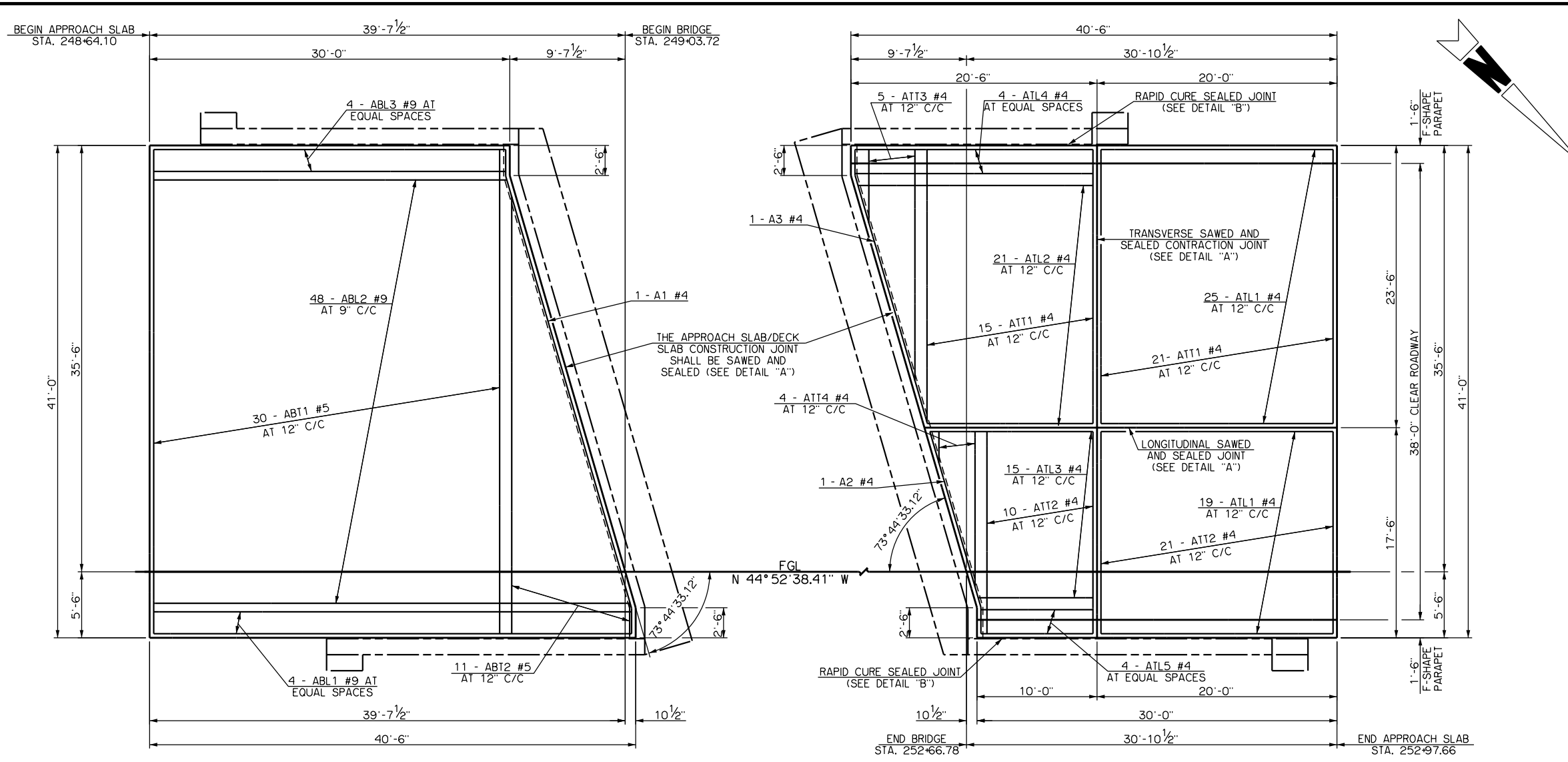
BEAM SECTIONS
(16 - 0.6" Ø STRANDS)

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

DETAILS OF PRESTRESSED CONCRETE BEAMS (TYPE IV) IN SPAN NO. 4

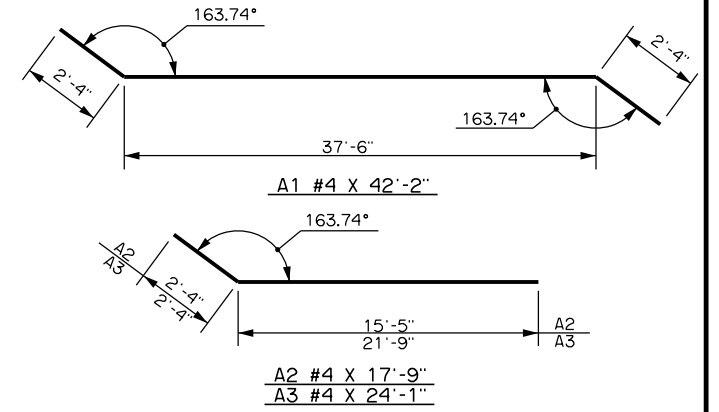
State Job No. 24428(04) Sheet No. B063

US 81 REALIGNMENT
GRADY COUNTY



PLAN OF APPROACH SLAB AT ABUTMENT NO. 1
 BOTTOM LAYER OF REINFORCING STEEL IS SHOWN AND IS TYPICAL FOR EACH APPROACH SLAB.

PLAN OF APPROACH SLAB AT ABUTMENT NO. 2
 TOP LAYER OF REINFORCING STEEL IS SHOWN AND IS TYPICAL FOR EACH APPROACH SLAB.

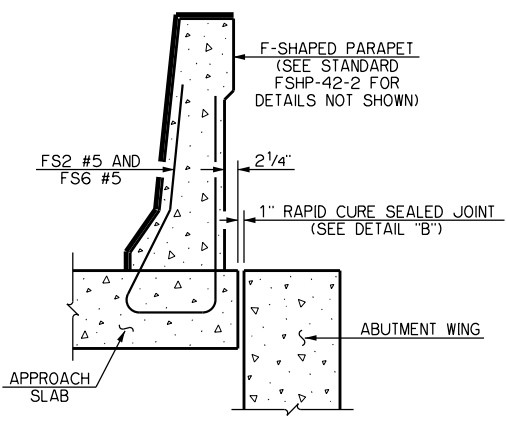


DETAILS OF BENT REINFORCEMENT STEEL

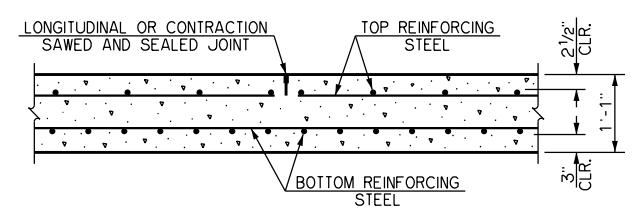
| BAR LIST - APPROACH SLAB (TWO REQUIRED) | | | | | |
|---|------|-----|------|----------------|-------------------|
| MARK | SIZE | NO. | FORM | LENGTH | LENGTH VARIATION |
| EPOXY COATED | | | | | |
| ABL1 | #9 | 4 | STR. | 40'-2" | |
| ABL2 | #9 | 54 | STR. | 34'-11" (AVG.) | 29'-9" TO 40'-1" |
| ABL3 | #9 | 4 | STR. | 29'-8" | |
| ABT1 | #5 | 30 | STR. | 40'-8" | |
| ABT2 | #5 | 11 | STR. | 20'-0" (AVG.) | 2'-10" TO 37'-2" |
| ATL1 | #4 | 43 | STR. | 19'-8" | |
| ATL2 | #4 | 31 | STR. | 17'-1" (AVG.) | 14'-2" TO 19'-11" |
| ATL3 | #4 | 15 | STR. | 12'-0" (AVG.) | 9'-11" TO 14'-0" |
| ATL4 | #4 | 4 | STR. | 20'-2" | |
| ATL5 | #4 | 4 | STR. | 9'-8" | |
| ATT1 | #4 | 36 | STR. | 23'-2" | |
| ATT2 | #4 | 31 | STR. | 17'-2" | |
| ATT3 | #4 | 5 | STR. | 13'-2" (AVG.) | 6'-4" TO 20'-0" |
| ATT4 | #4 | 4 | STR. | 8'-6" (AVG.) | 3'-4" TO 13'-8" |
| A1 | #4 | 1 | BNT. | 42'-2" | |
| A2 | #4 | 1 | BNT. | 17'-9" | |
| A3 | #4 | 1 | BNT. | 24'-1" | |

| SUMMARY OF QUANTITIES - ONE APPROACH SLAB (TWO REQUIRED) | | | |
|--|------|--------|--|
| ITEM | UNIT | TOTAL | |
| (1) APPROACH SLAB | SY | 160.60 | |
| SAW-CUT GROOVING | SY | 137.00 | |
| 42" F-SHAPE PARAPET | LF | 70.50 | |
| WATER REPELLENT (VISUALLY INSPECTED) | SY | 35.00 | |

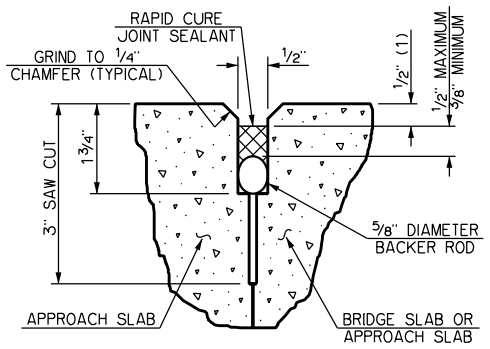
(1) QUANTITY INCLUDES ALL COSTS OF CONCRETE, EPOXY COATED REINFORCING STEEL INCLUDING FS2 #5 AND FS6 #5 BARS, POLYSTYRENE, BACKER ROD, RAPID CURE JOINT SEALANT, SAWING, GRINDING, FILL AND EXCAVATION.



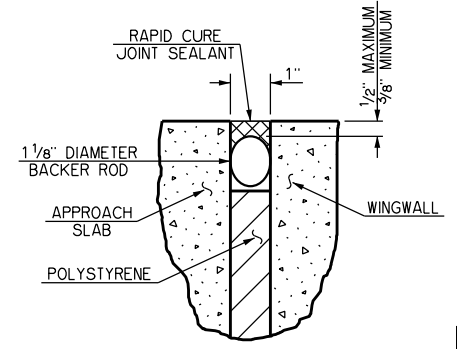
DETAIL OF APPROACH SLAB AT ABUTMENT WING
 SURFACES INDICATED WITH HEAVY LINES SHALL BE TREATED WITH A PENETRATING WATER REPELLANT SURFACE TREATMENT.



SECTION THROUGH APPROACH SLAB
 NOTE: THE REINFORCING STEEL IN THE TOP OF THE APPROACH SLAB SHALL END 2" EITHER SIDE OF THE LONGITUDINAL OR TRANSVERSE JOINT.



DETAIL "A"
 (1) THIS DIMENSION SHALL TAPER FROM 1/2" AT EDGE OF DRIVING LANE / SHOULDER TO 1/8" AT CONCRETE TRAFFIC RAIL FOR TRANSVERSE JOINTS ONLY.

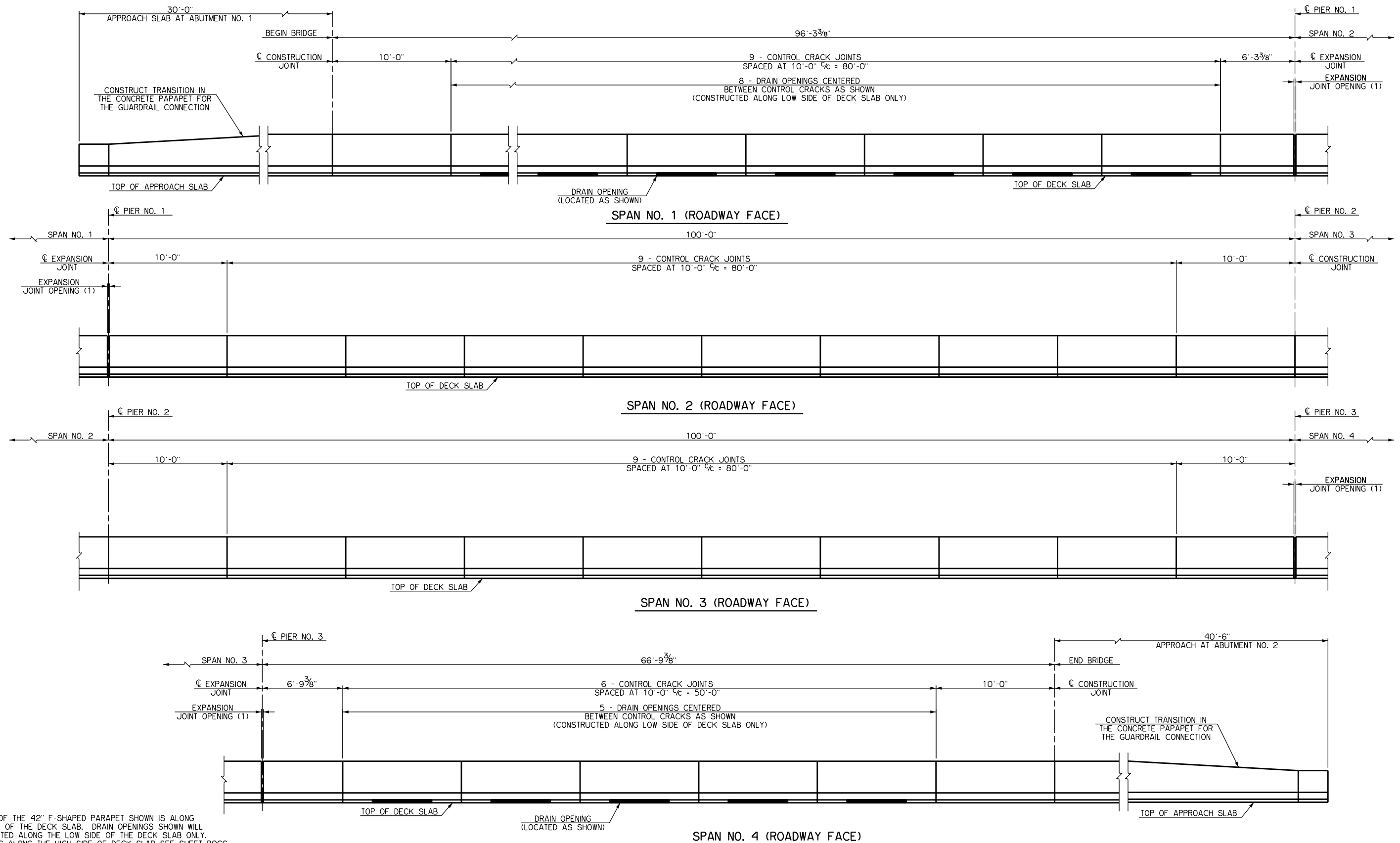


DETAIL "B"

NOTES
 THE FS2 #5 AND FS6 #5 BARS PROJECTING FROM THE APPROACH SLAB INTO THE CONCRETE TRAFFIC RAILS HAVE BEEN OMITTED IN THE APPROACH SLAB PLAN VIEWS FOR CLARITY. SEE STANDARD FSHP-42-2 AND "LAYOUT OF 42" F-SHAPE PARAPET" ON SHEET NOS. B065 AND B066 FOR PLACEMENT OF FS2 #5 AND FS6 #5 BARS.
 DO NOT SAW CUT GROOVE WITHIN 6" OF ALL CONSTRUCTION JOINTS AND CONTRACTION JOINTS.

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
 BRIDGE 'H'

DETAILS OF APPROACH SLABS



NOTES

THE LAYOUT OF THE 42" F-SHAPED PARAPET SHOWN IS ALONG THE LOW SIDE OF THE DECK SLAB. DRAIN OPENINGS SHOWN WILL BE CONSTRUCTED ALONG THE LOW SIDE OF THE DECK SLAB ONLY. THE PARAPETS ALONG THE HIGH SIDE OF DECK SLAB SEE SHEET B066

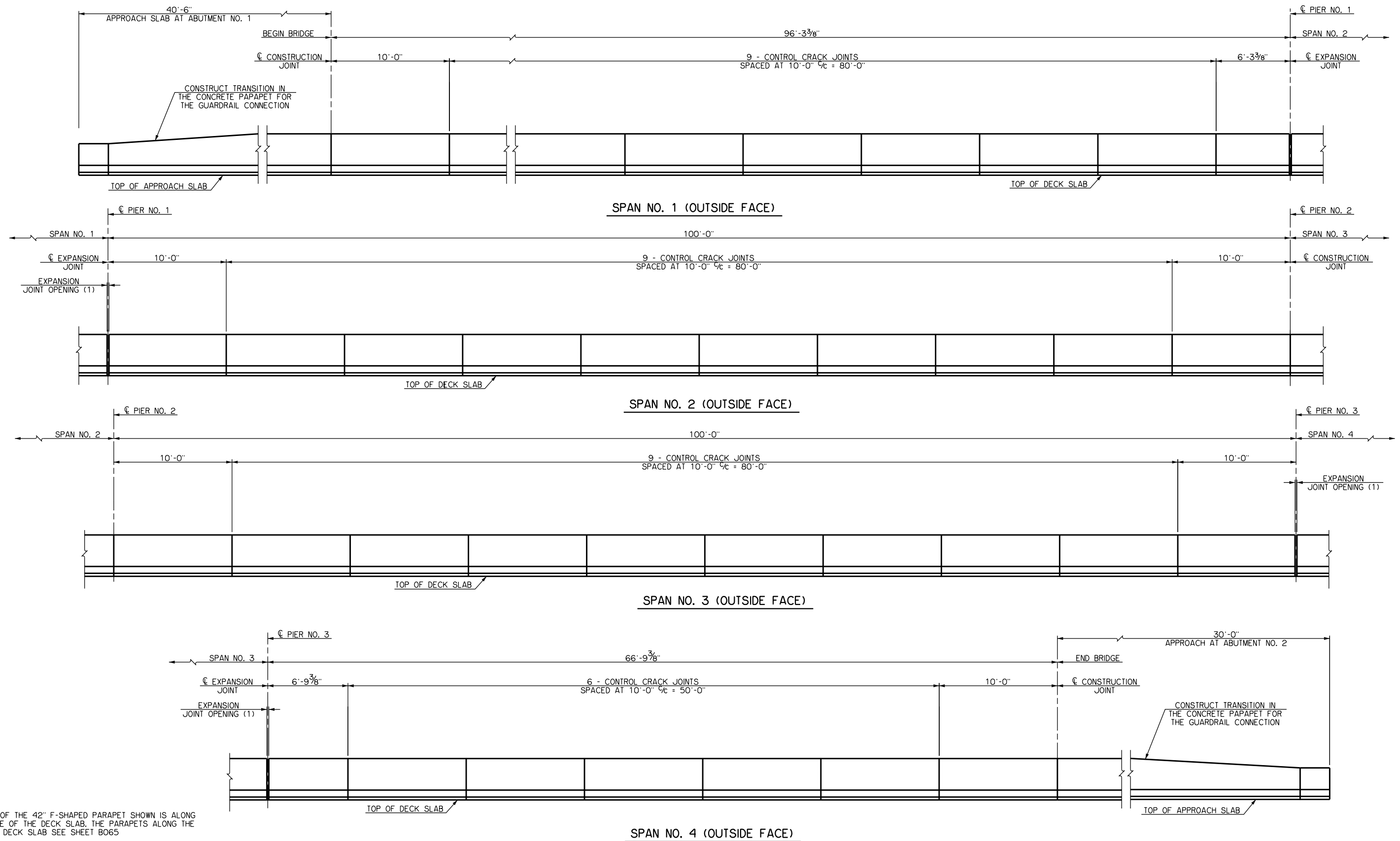
FH BARS IN 42" F-SHAPED PARAPET SHALL NOT EXTEND THROUGH CONSTRUCTION JOINTS.
SEE STANDARD FSHP-42-2 FOR ADDITIONAL DIMENSIONS, DETAILS, AND INFORMATION.

(1) EXPANSION JOINT OPENING IN THE 42" F-SHAPED PARAPET SHALL MATCH THE EXPANSION JOINT OPENING IN THE DECK SLAB.

LAYOUT OF 42" F-SHAPED PARAPET LOW SIDE OF BRIDGE

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

LAYOUT OF 42" F-SHAPED PARAPET
(SHEET NO. 1 OF 2)



NOTES

THE LAYOUT OF THE 42" F-SHAPED PARAPET SHOWN IS ALONG THE HIGH SIDE OF THE DECK SLAB. THE PARAPETS ALONG THE LOW SIDE OF DECK SLAB SEE SHEET B065

FH BARS IN 42" F-SHAPED PARAPET SHALL NOT EXTEND THROUGH CONSTRUCTION JOINTS.

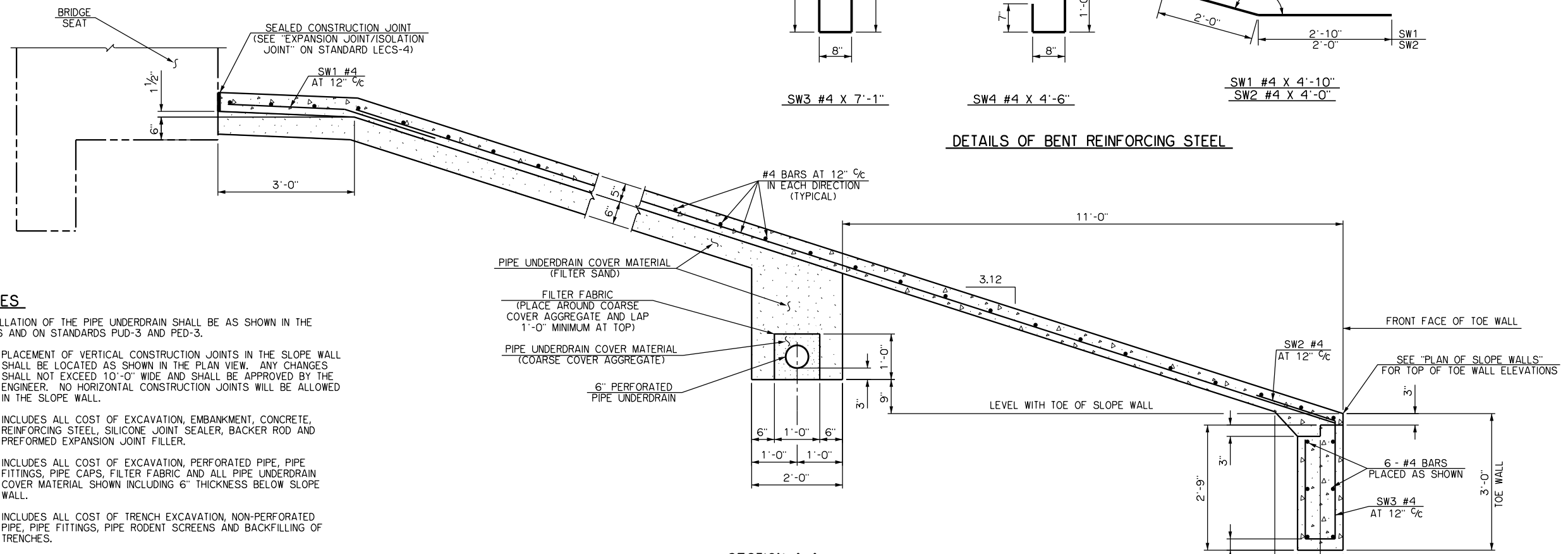
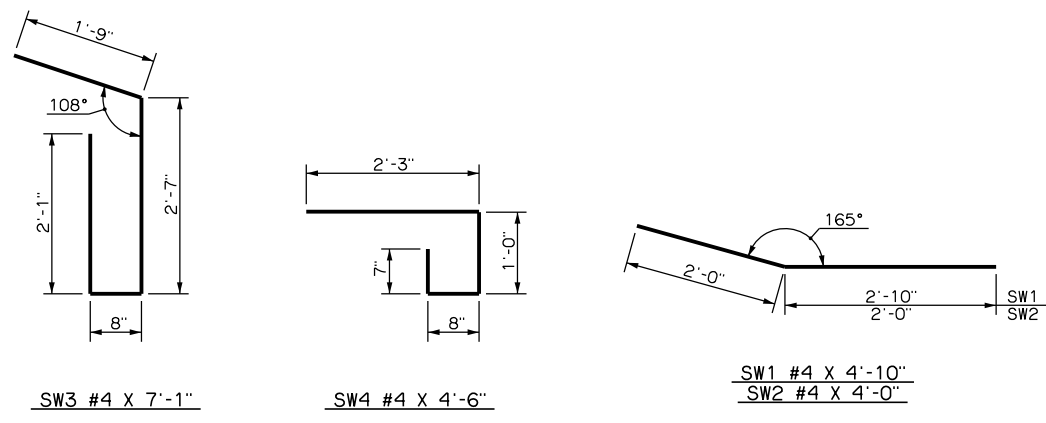
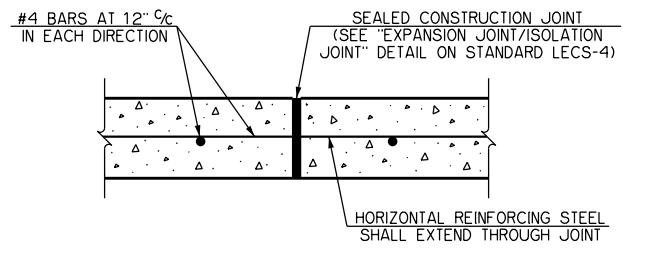
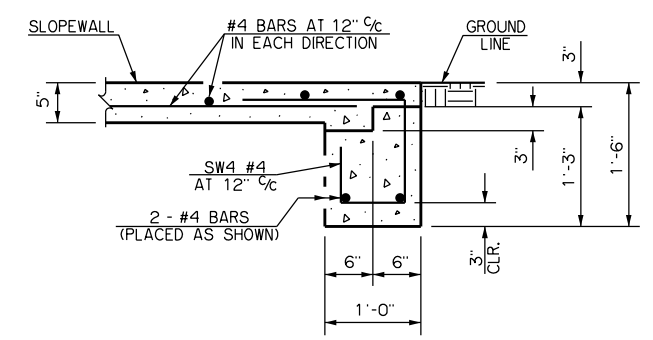
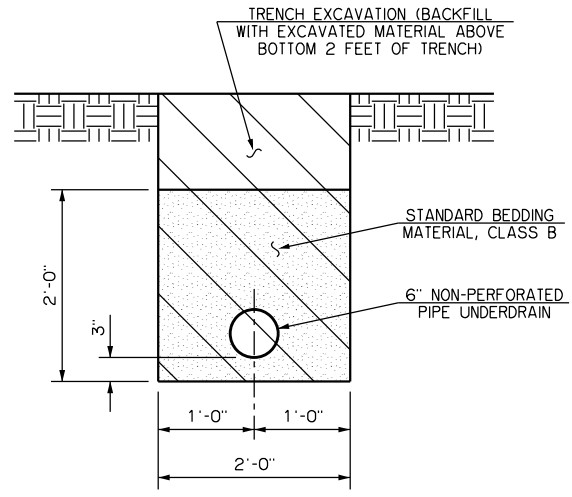
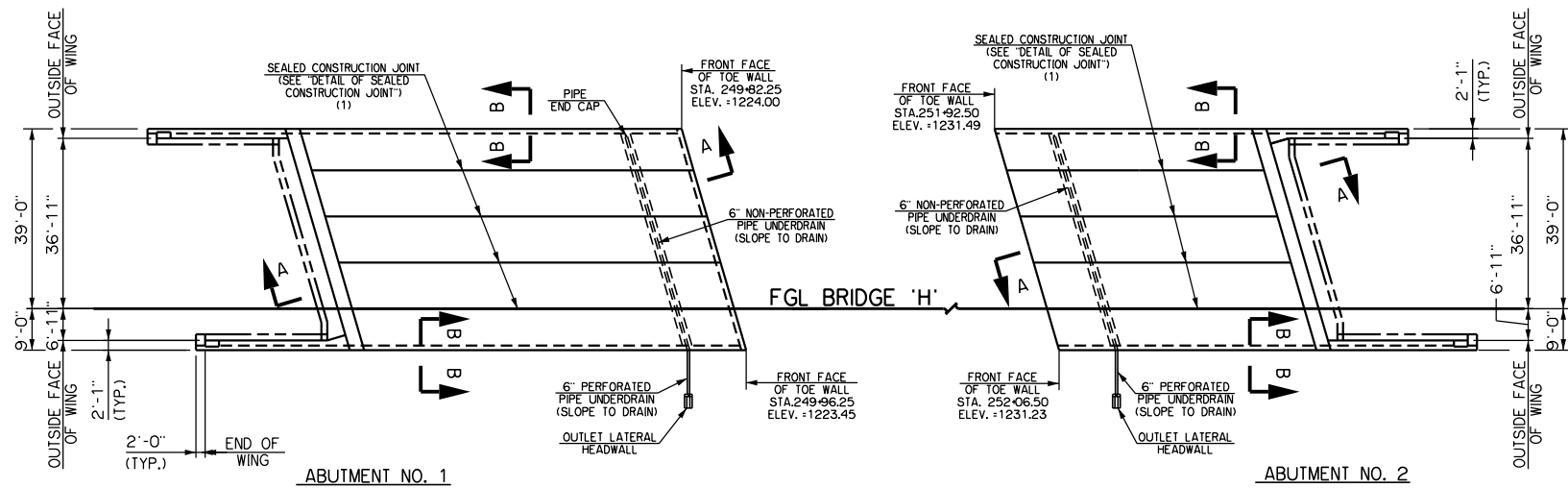
SEE STANDARD FSHP-42-2 FOR ADDITIONAL DIMENSIONS, DETAILS, AND INFORMATION.

(1) EXPANSION JOINT OPENING IN THE 42" F-SHAPED PARAPET SHALL MATCH THE EXPANSION JOINT OPENING IN THE DECK SLAB.

LAYOUT OF 42" F-SHAPED PARAPET HIGH SIDE OF BRIDGE

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

LAYOUT OF 42" F-SHAPED PARAPET
(SHEET NO. 2 OF 2)



- NOTES**
- INSTALLATION OF THE PIPE UNDERDRAIN SHALL BE AS SHOWN IN THE PLANS AND ON STANDARDS PUD-3 AND PED-3.
- (1) PLACEMENT OF VERTICAL CONSTRUCTION JOINTS IN THE SLOPE WALL SHALL BE LOCATED AS SHOWN IN THE PLAN VIEW. ANY CHANGES SHALL NOT EXCEED 10'-0" WIDE AND SHALL BE APPROVED BY THE ENGINEER. NO HORIZONTAL CONSTRUCTION JOINTS WILL BE ALLOWED IN THE SLOPE WALL.
 - (2) INCLUDES ALL COST OF EXCAVATION, EMBANKMENT, CONCRETE, REINFORCING STEEL, SILICONE JOINT SEALER, BACKER ROD AND PREFORMED EXPANSION JOINT FILLER.
 - (3) INCLUDES ALL COST OF EXCAVATION, PERFORATED PIPE, PIPE FITTINGS, PIPE CAPS, FILTER FABRIC AND ALL PIPE UNDERDRAIN COVER MATERIAL SHOWN INCLUDING 6" THICKNESS BELOW SLOPE WALL.
 - (4) INCLUDES ALL COST OF TRENCH EXCAVATION, NON-PERFORATED PIPE, PIPE FITTINGS, PIPE RODENT SCREENS AND BACKFILLING OF TRENCHES.

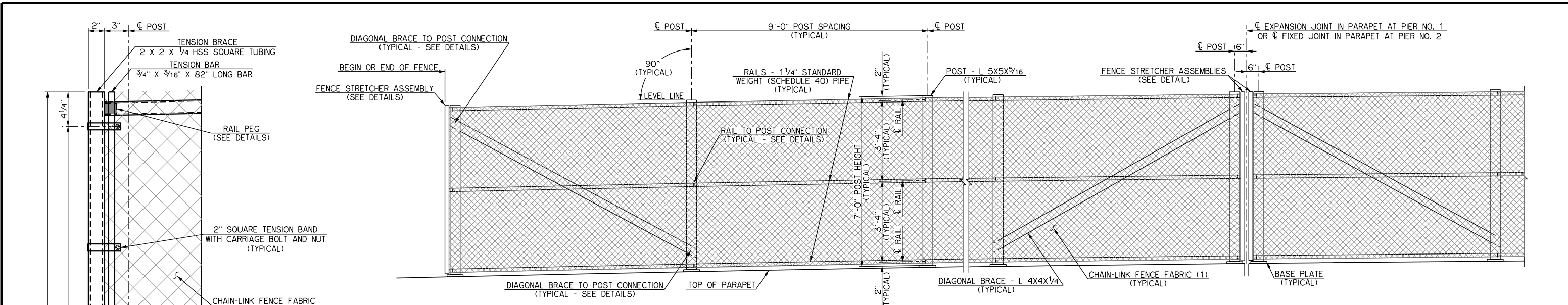
| BRIDGE 'H' SUMMARY OF QUANTITIES - SLOPEWALL | | | | |
|--|------|-------------|-------------|--------|
| ITEM | UNIT | ABUT. NO. 1 | ABUT. NO. 2 | TOTAL |
| (2) SLOPE WALL (5") | SY | 511.00 | 360.00 | 871.00 |
| (3) 6" PERFORATED PIPE UNDERDRAIN ROUND | LF | 49.00 | 49.00 | 98.00 |
| (4) 6" NON-PERF. PIPE UNDERDRAIN RND. | LF | 10.00 | 10.00 | 20.00 |
| OUTLET LATERAL HEADWALL | EA | 1.00 | 1.00 | 2.00 |

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD
BRIDGE 'H'

DETAILS OF SLOPE WALLS

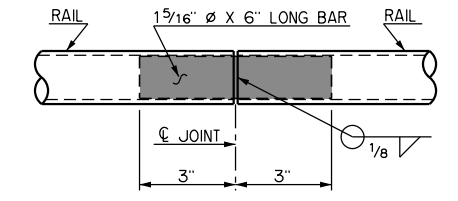
State Job No. 24428(04) Sheet No. B067

US 81 REALIGNMENT
GRADY COUNTY

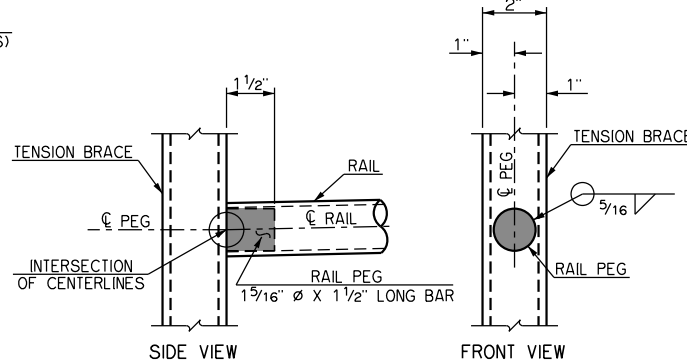


PARTIAL FENCE ELEVATION

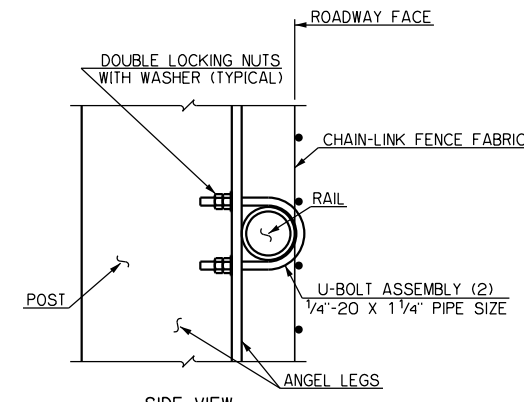
(1) FABRIC SHALL BE ATTACHED TO RAILS WITH TIE WIRES SPACED AT A MAXIMUM OF 1'-0" ALONG THE FULL LENGTH OF ALL THREE RAILS



DETAIL OF RAIL SPLICE

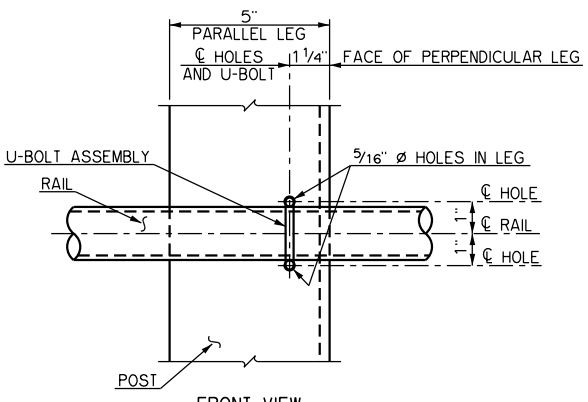


DETAILS OF RAIL PEG
TYPICAL AT ENDS OF ALL RAILS

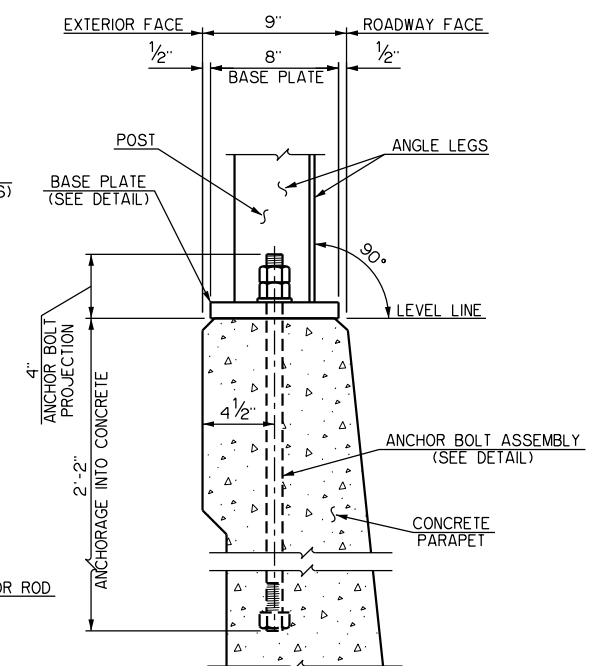


DETAILS OF RAIL TO POST CONNECTION

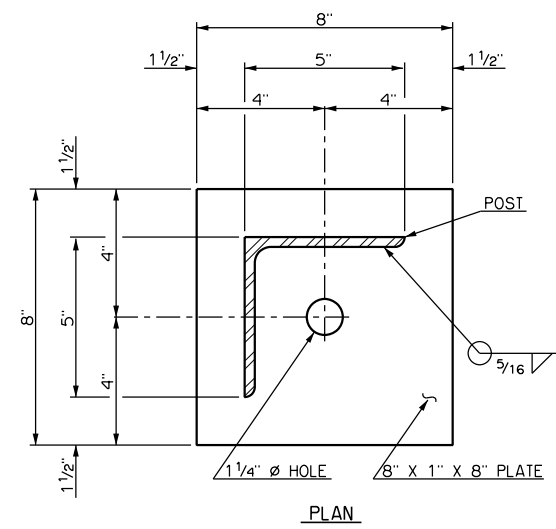
(2) U-BOLT SHALL BE 1 3/4" INSIDE WIDTH X 3" INSIDE LENGTH X 1 3/8" THREAD LENGTH



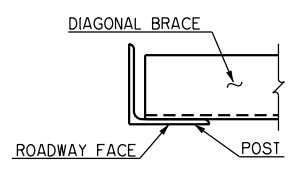
FRONT VIEW



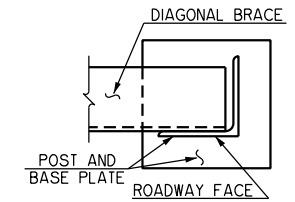
SECTION THROUGH PARAPET AT POST AND BASE PLATE



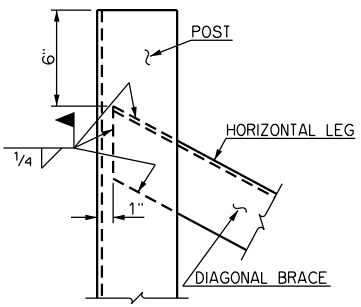
DETAIL OF BASE PLATE



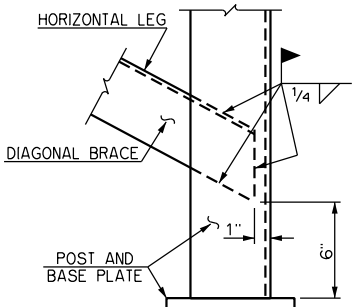
TOP VIEW AT TOP OF POST



TOP VIEW AT BOTTOM OF POST

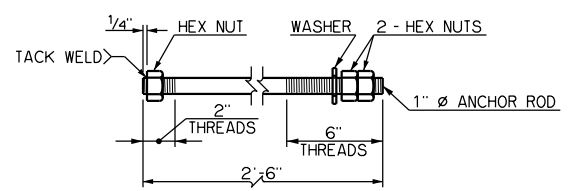


FRONT VIEW AT TOP OF POST



FRONT VIEW AT BOTTOM OF POST

DETAIL OF FENCE STRETCHER ASSEMBLY
POST AND BASE PLATE OMITTED FOR CLARITY



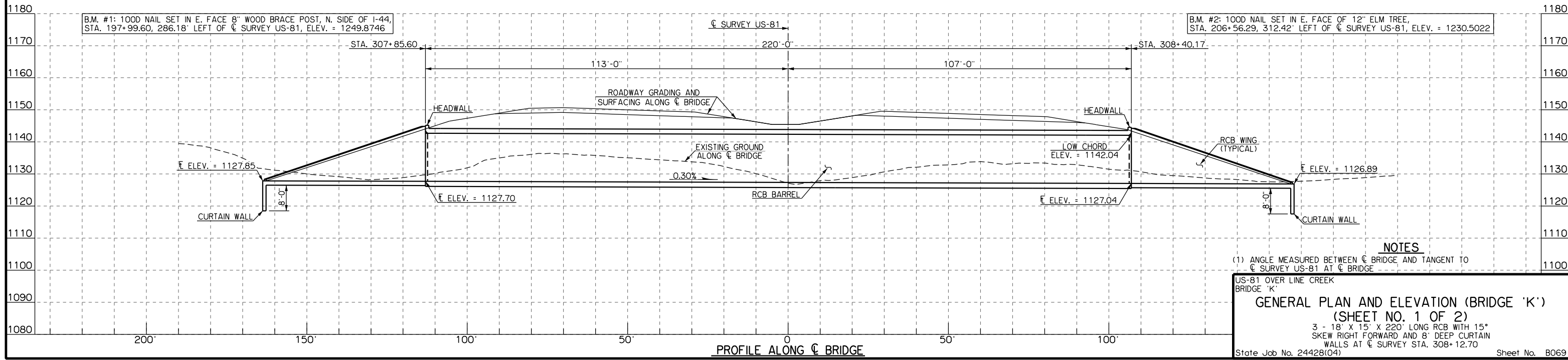
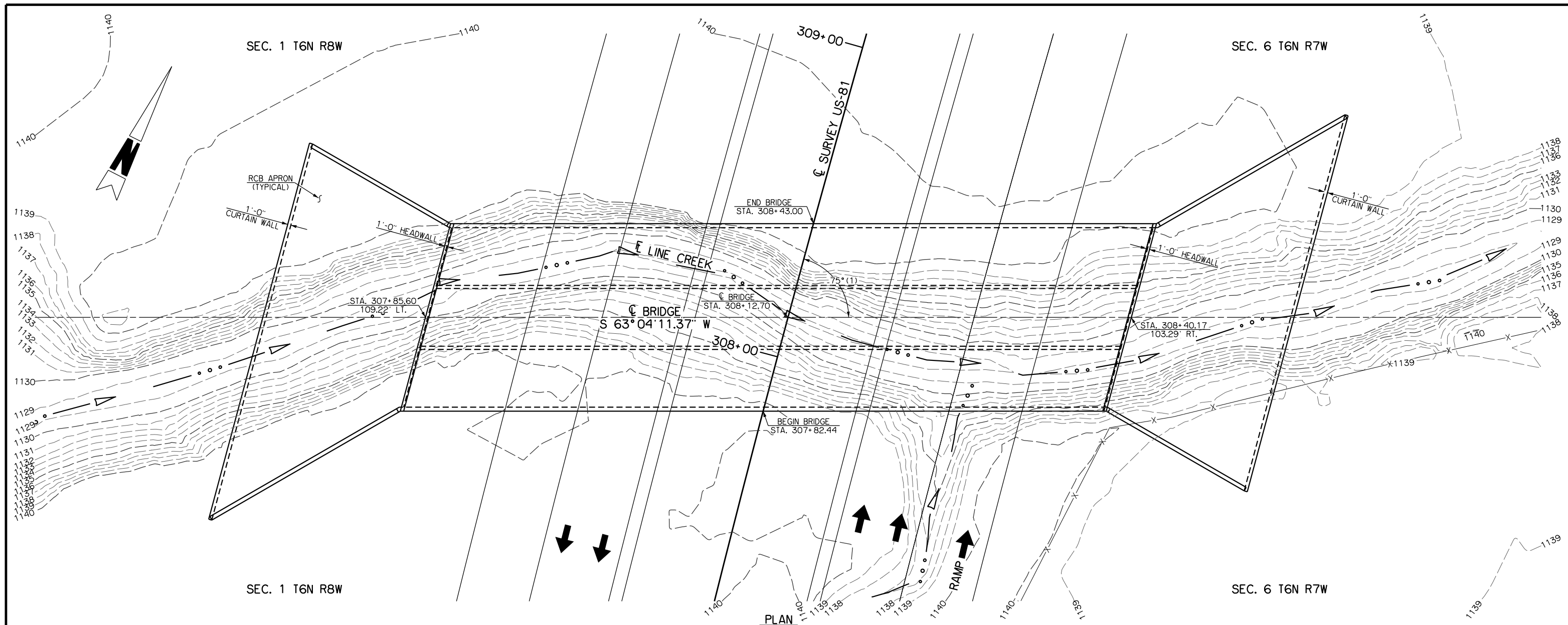
DETAIL OF ANCHOR BOLT ASSEMBLY

NOTES

- BASE PLATES SHALL BE SEATED IN FULL CONTACT WITH TOP OF PARAPET. POSTS SHALL BE MADE PLUMB IN BOTH VERTICAL PLANES.
- FOR ANCHOR BOLT ASSEMBLIES, ANCHOR RODS SHALL CONFORM TO ASTM A 449, NUTS SHALL CONFORM TO ASTM A 563 AND WASHERS SHALL CONFORM TO ASTM F 436.
- ALL PLATES, SQUARE BARS, ROUND BARS AND ANGLES SHALL BE FABRICATED FROM STRUCTURAL STEEL CONFORMING TO ASTM A 572 GRADE 50.
- ALL PIPE AND HSS SQUARE TUBING SHALL BE FABRICATED FROM STRUCTURAL STEEL CONFORMING TO ASTM A 500 GRADE B.
- FOR U-BOLT ASSEMBLIES, U-BOLTS SHALL CONFORM TO ASTM A 307A, NUTS SHALL CONFORM TO ASTM A 563 AND WASHERS SHALL CONFORM TO ASTM F 436.
- CHAIN-LINK FENCE FABRIC SHALL BE 9 GAUGE WIRE WOVEN INTO A 2 INCH DIAMOND MESH CONFORMING TO SECTION 732.07.C.(1).(c) OF THE STANDARD SPECIFICATIONS.
- TENSION BANDS, CARRIAGE BOLTS AND NUTS AND TIE WIRES SHALL CONFORM TO ASTM F 626.
- ALL STEEL PARTS OF FENCE INCLUDING PLATES, BARS, ANGLES, PIPE, HSS TUBING, ANCHOR RODS, U-BOLTS, TENSION BANDS, CARRIAGE BOLTS, NUTS, WASHERS, FENCE FABRIC AND TIE WIRES SHALL BE HOT-DIPPED GALVANIZED IN ACCORDANCE WITH SECTION 724.06 OF THE STANDARD SPECIFICATIONS.
- FENCE SHALL BE INSTALLED ON BOTH LEFT AND RIGHT PARAPETS. SEE GENERAL PLAN AND ELEVATION (BRIDGE 'H') FOR LOCATIONS.
- ALL COSTS TO FABRICATE AND INSTALL FENCE INCLUDING THE COST OF ALL PLATES, BARS, ANGLES, PIPE, HSS TUBING, ANCHOR RODS, U-BOLTS, TENSION BANDS, CARRIAGE BOLTS, NUTS, WASHERS, FENCE FABRIC, TIE WIRES, GALVANIZING, MATERIAL, LABOR, EQUIPMENT AND INCIDENTALS SHALL BE INCLUDED IN THE UNIT PRICE BID PER LINEAR FOOT OF "FENCE-STYLE CLF (8' HIGH, CLASS A)."

SB US-81 OVER SLWC RAILROAD AND NORGE ROAD BRIDGE 'H'

DETAILS OF FENCE ON CONCRETE PARAPETS



HYDRAULIC DATA

DRAINAGE AREA = 7.08 SQ. MI.
 CONTROLLED AREA = 0.00 SQ. MI.
 EFFECTIVE AREA = 7.08 SQ. MI.

Q2 = 590 CFS
 V2 = 1.69 FPS
 CHW2 = 1,134.31 FT.

Q5 = 1,280 CFS
 V5 = 2.52 FPS
 CHW5 = 1,137.25 FT.

Q10 = 1,970 CFS
 V10 = 3.35 FPS
 CHW10 = 1,138.90 FT.

Q25 = 3,130 CFS
 V25 = 4.96 FPS
 CHW25 = 1,140.44 FT.

Q50 = 4,020 CFS
 V50 = 6.18 FPS
 CHW50 = 1,140.57 FT.

Q100 = 5,070 CFS
 V100 = 7.60 FPS
 CHW100 = 1,141.16 FT.

Q500 = 8,070 CFS
 V500 = 11.45 FPS
 CHW500 = 1,143.76 FT.

Q0T-Q500 > 8,070 CFS
 V0T = 11.45 FPS
 CHW0T = 1,143.90 FT.

DESIGN DATA

MATERIALS:
 CLASS AA CONCRETE, f'c = 4 KSI
 REINFORCING STEEL, fy = 60 KSI

LOADING:
 HL-93 AND OKLAHOMA OVERLOAD TRUCK
 DESIGN FILL HEIGHT = 7.0 FT.

DESIGN:
 AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS,
 7TH EDITION WITH 2015 INTERIMS

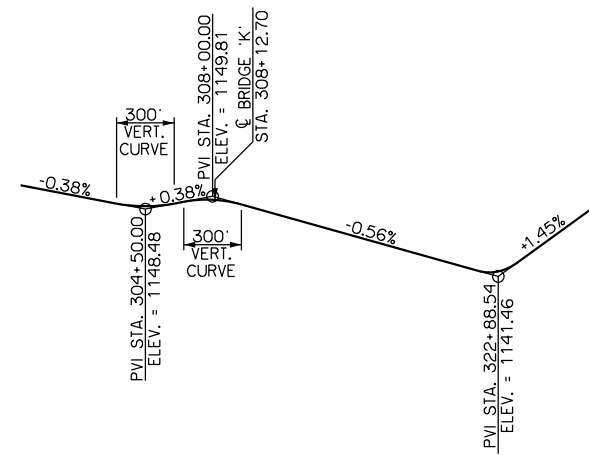
HL-93 INVENTORY RATING FACTOR: 1.00
 HL-93 OPERATING RATING FACTOR: 1.30

INDEX OF SHEETS

| SHEET NO. | SHEET DESCRIPTION |
|------------|---|
| AB04 | PAY QUANTITIES AND GENERAL NOTES |
| B069, B070 | GENERAL PLAN AND ELEVATION (BRIDGE 'K') |
| B071- B076 | DETAILS OF REINFORCED CONCRETE BOX |

REQUIRED STANDARD DRAWINGS

ROADWAY
 SBI-4-2

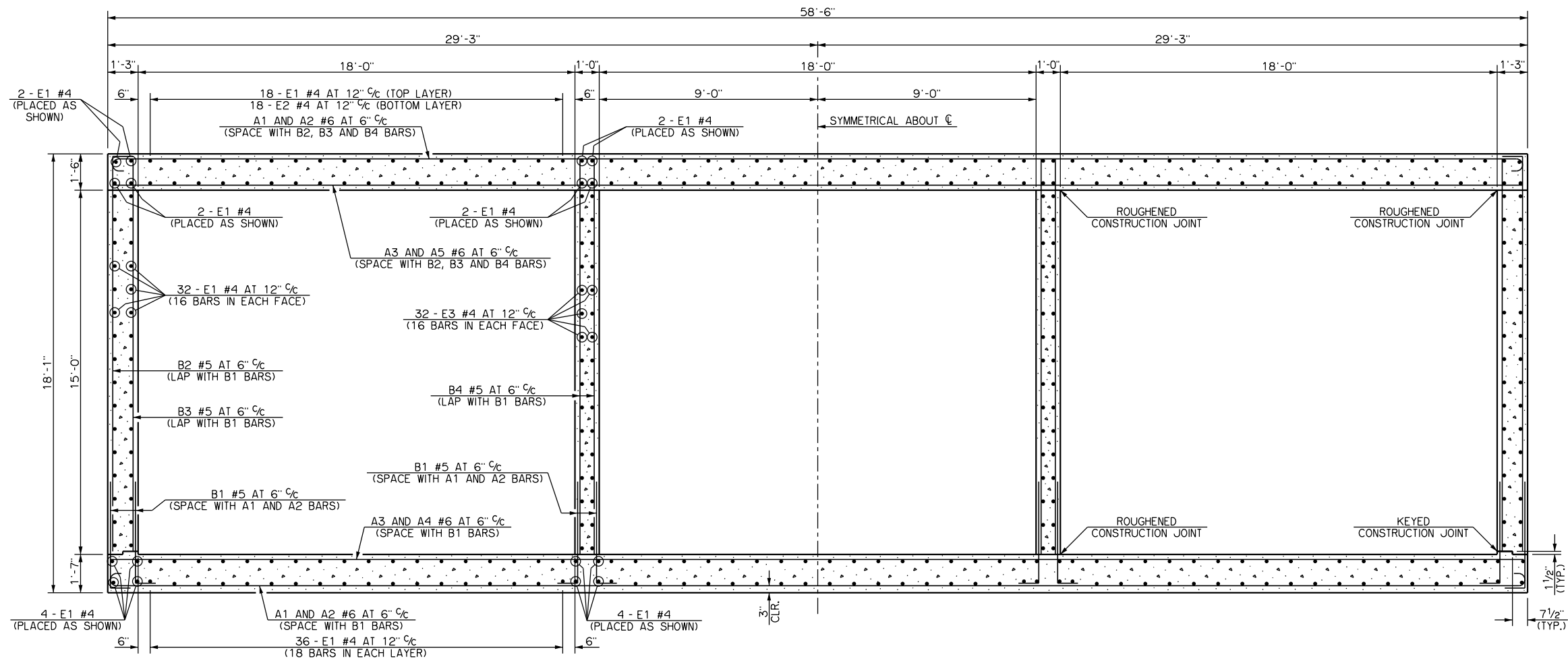


PROFILE GRADE DATA
 ALONG PROFILE GRADE LINE

| SUMMARY OF QUANTITIES - BRIDGE 'K' | | |
|------------------------------------|------|------------|
| ITEM | UNIT | TOTAL |
| UNCLASSIFIED EXCAVATION | CY | 11,460.00 |
| STRUCTURAL EXCAVATION UNCLASSIFIED | CY | 1,050.00 |
| CLASS AA CONCRETE | CY | 2,670.70 |
| REINFORCING STEEL | LB | 392,820.00 |

US-81 OVER LINE CREEK
 BRIDGE 'K'
GENERAL PLAN AND ELEVATION (BRIDGE 'K')
 (SHEET NO. 2 OF 2)
 3 - 18' X 15' X 220' LONG RCB WITH 15°
 SKEW RIGHT FORWARD AND 8' DEEP CURTAIN
 WALLS AT ϕ SURVEY STA. 308+12.70
 State Job No. 24428(04) Sheet No. B070

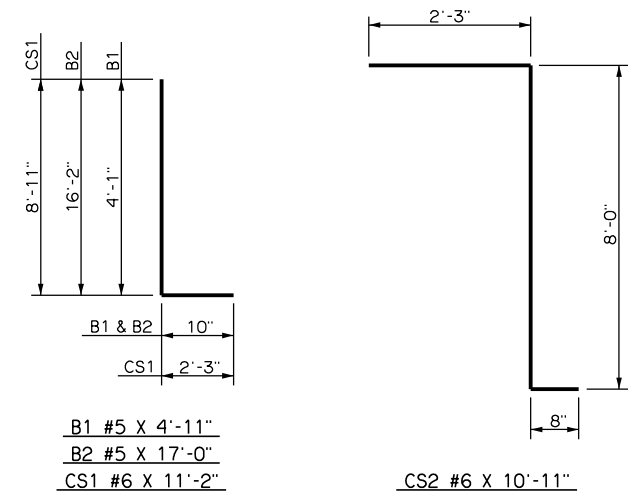
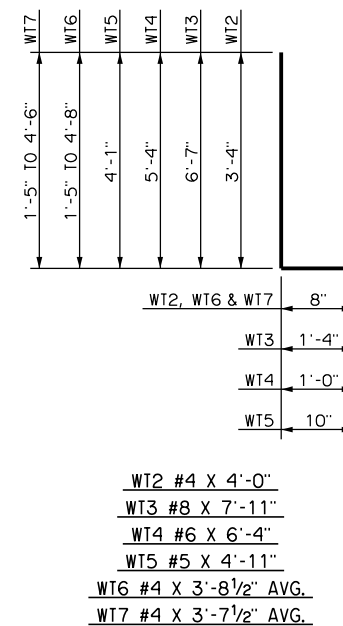
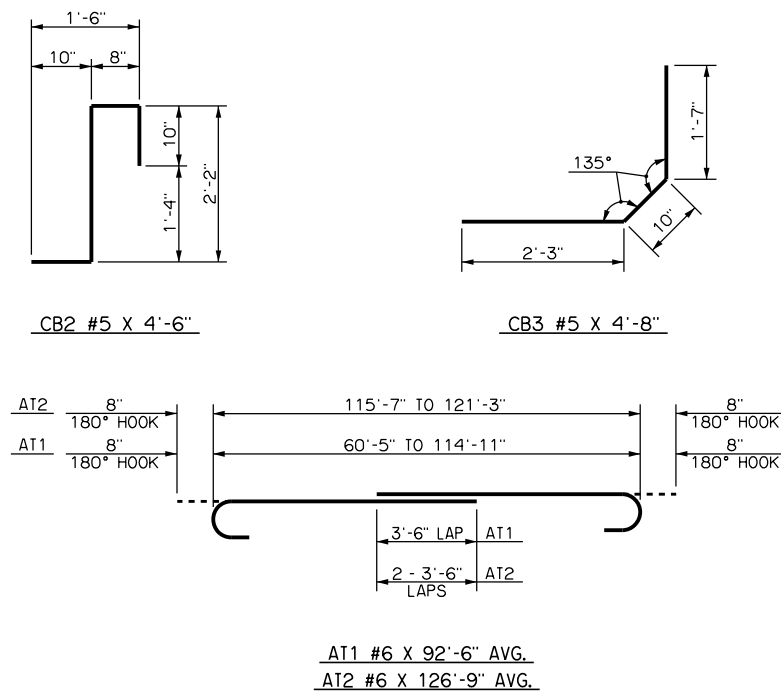
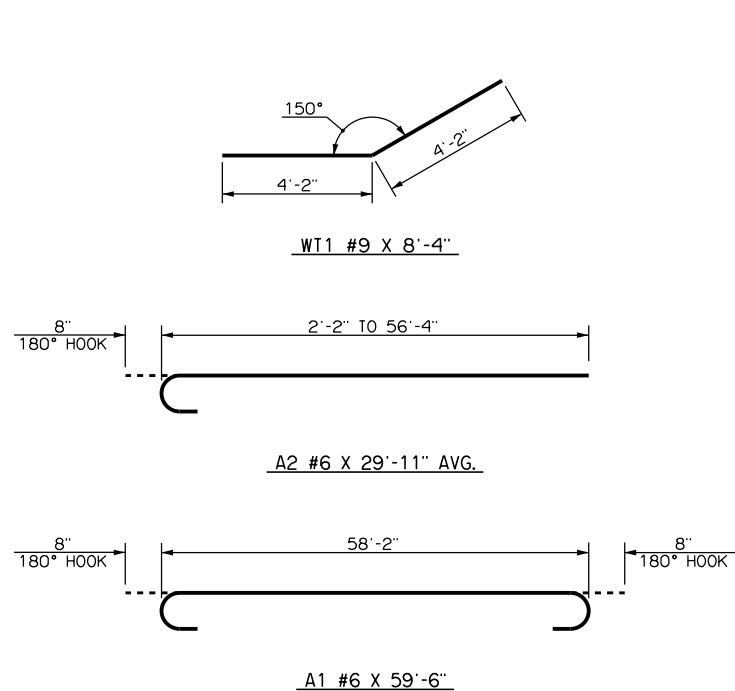
US 81 REALIGNMENT
 GRADY COUNTY



TYPICAL SECTION THROUGH REINFORCED CONCRETE BOX

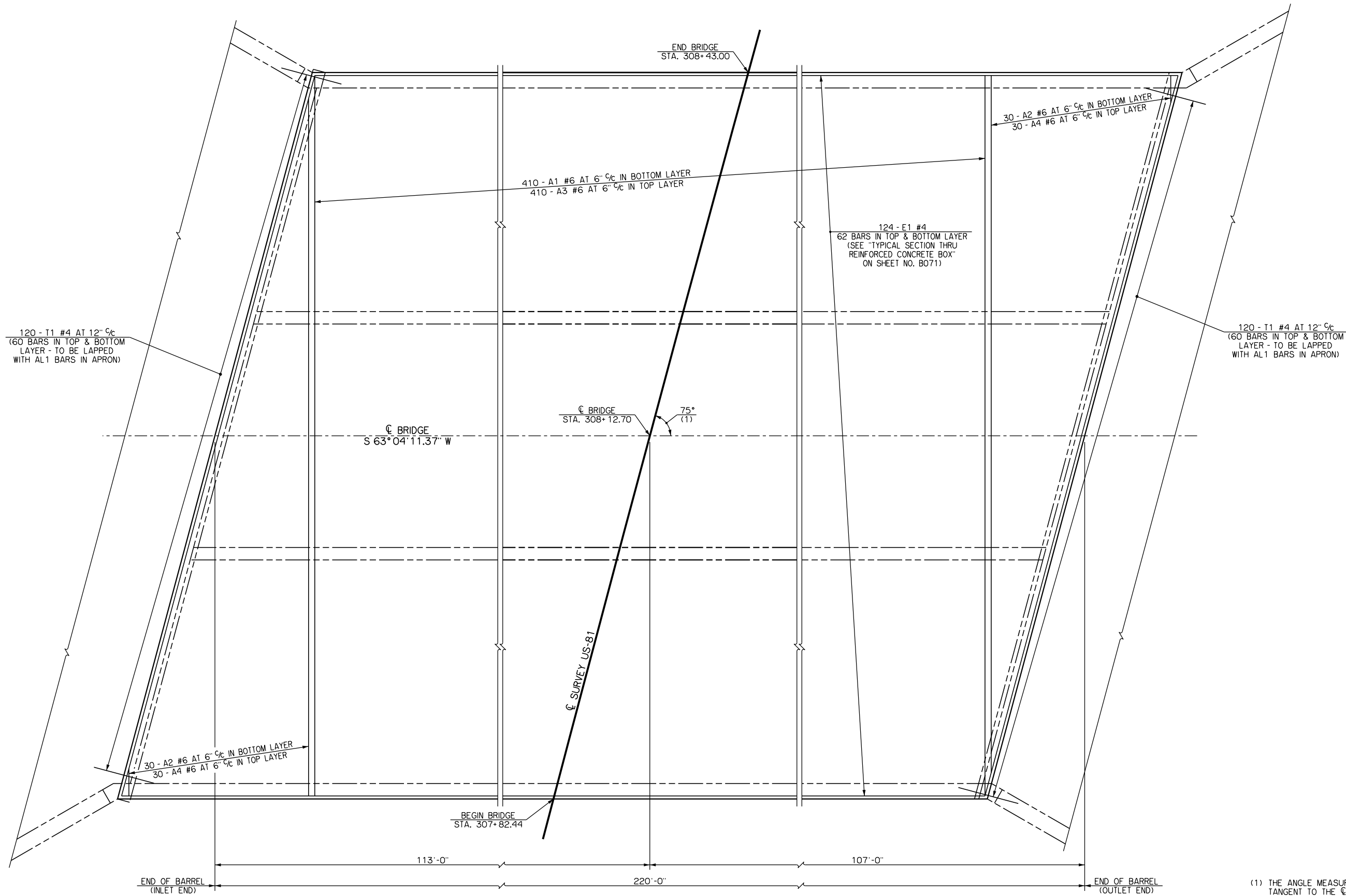
| BAR LIST - REINFORCED CONCRETE BOX | | | | | |
|------------------------------------|------|-------|------|--------------|------------------|
| MARK | SIZE | NO. | FORM | LENGTH | LENGTH VARIATION |
| EPOXY COATED | | | | | |
| A1 | #6 | 820 | BNT. | 59'-6" | - |
| (1) A2 | #6 | 120 | BNT. | 29'-11" AVG. | 2'-10" TO 57'-0" |
| A3 | #6 | 818 | STR. | 58'-2" | - |
| (2) A4 | #6 | 60 | STR. | 29'-3" AVG. | 2'-2" TO 56'-4" |
| (2) A5 | #6 | 60 | STR. | 29'-2" AVG. | 2'-1" TO 56'-3" |
| B1 | #5 | 3,512 | BNT. | 4'-11" | - |
| B2 | #5 | 880 | BNT. | 17'-0" | - |
| B3 | #5 | 880 | STR. | 16'-2" | - |
| B4 | #5 | 1,752 | STR. | 16'-4" | - |
| (3) CB1 | #8 | 16 | STR. | 66'-0" | - |
| CB2 | #5 | 124 | BNT. | 4'-6" | - |
| CB3 | #5 | 124 | BNT. | 4'-8" | - |
| (4) E1 | #4 | 258 | STR. | 224'-2" | - |
| (4) E2 | #4 | 54 | STR. | 223'-2" | - |
| (4) E3 | #4 | 64 | STR. | 222'-10" | - |
| T1 | #4 | 240 | STR. | 5'-0" | - |
| WT1 | #9 | 68 | BNT. | 8'-4" | - |

- (1) INCLUDES FOUR SETS OF 30 BARS
- (2) INCLUDES TWO SETS OF 30 BARS
- (3) LENGTH INCLUDES ONE 5'-6" LAP, LAPS SHALL BE STAGGERED
- (4) LENGTH INCLUDES TWO 2'-3" LAP, LAPS SHALL BE STAGGERED



DETAILS OF BENT REINFORCING STEEL

US-81 OVER LINE CREEK
BRIDGE 'K'
DETAILS OF REINFORCED CONCRETE BOX
(SHEET NO. 1 OF 6)



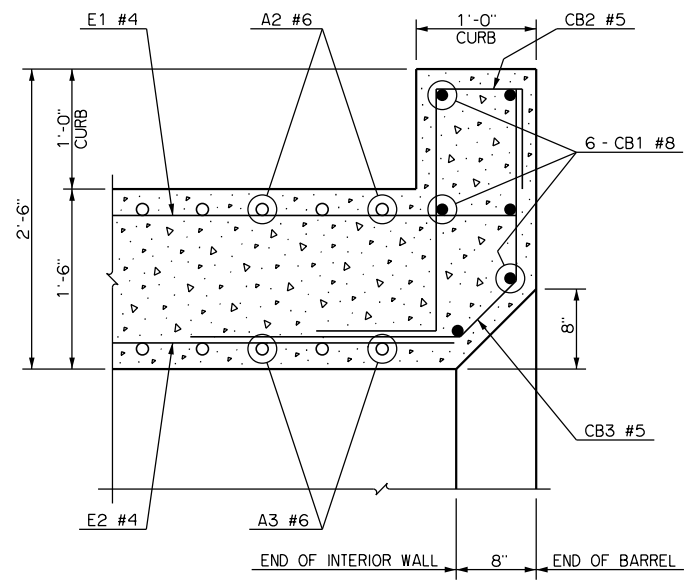
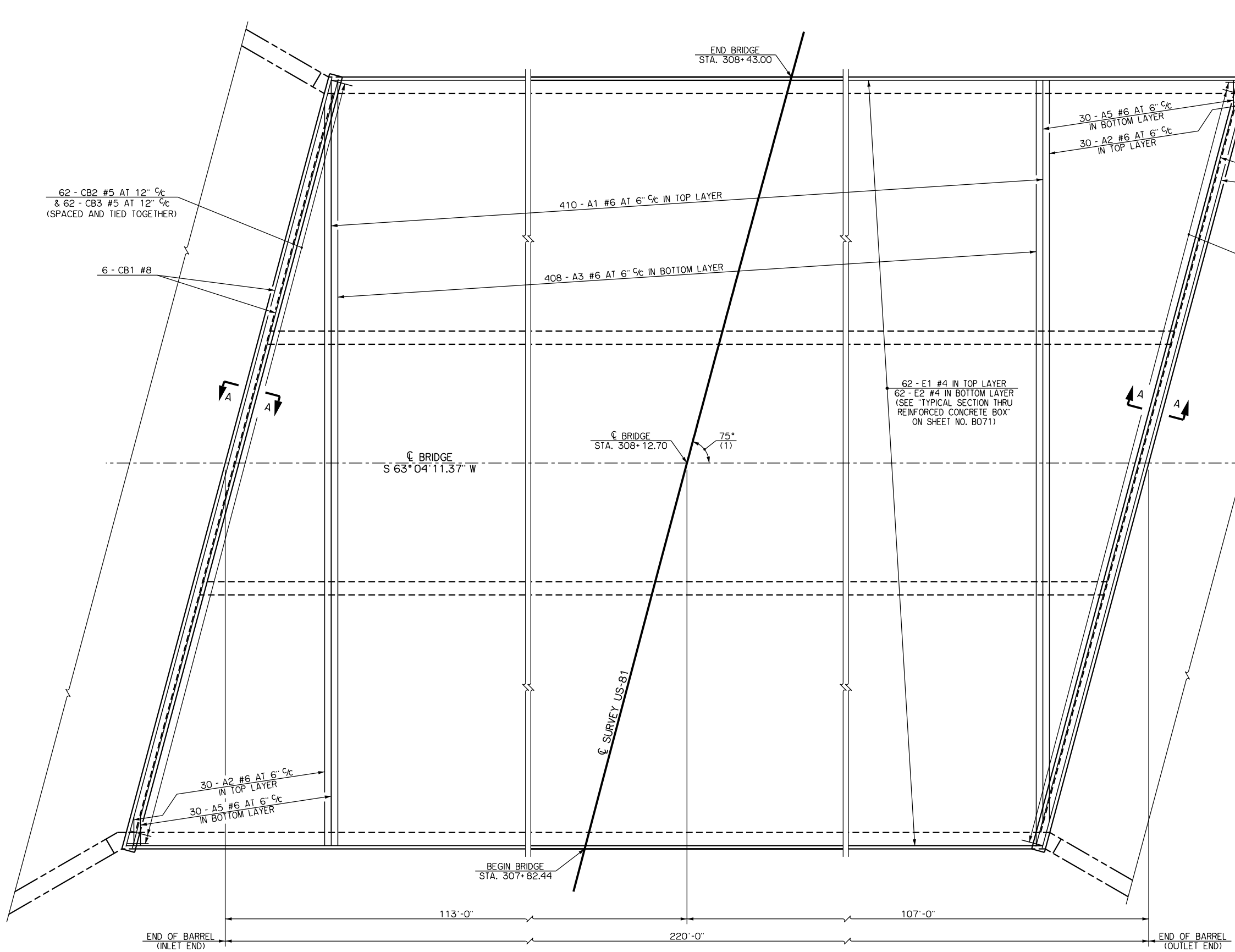
BOTTOM SLAB REINFORCEMENT LAYOUT

NOTE: FOR REINFORCEMENT LAYOUT OF APRON, SEE SHEET NO. B074

NOTES

(1) THE ANGLE MEASURED IS BETWEEN THE C BRIDGE AND THE TANGENT TO THE C SURVEY US-81 AT THE C BRIDGE.

US-81 OVER LINE CREEK
BRIDGE 'K'
DETAILS OF REINFORCED CONCRETE BOX
(SHEET NO. 2 OF 6)



SECTION "A-A"

NOTES

(1) THE ANGLE MEASURED IS BETWEEN THE CL BRIDGE AND THE TANGENT TO THE CL SURVEY US-81 AT THE CL BRIDGE.

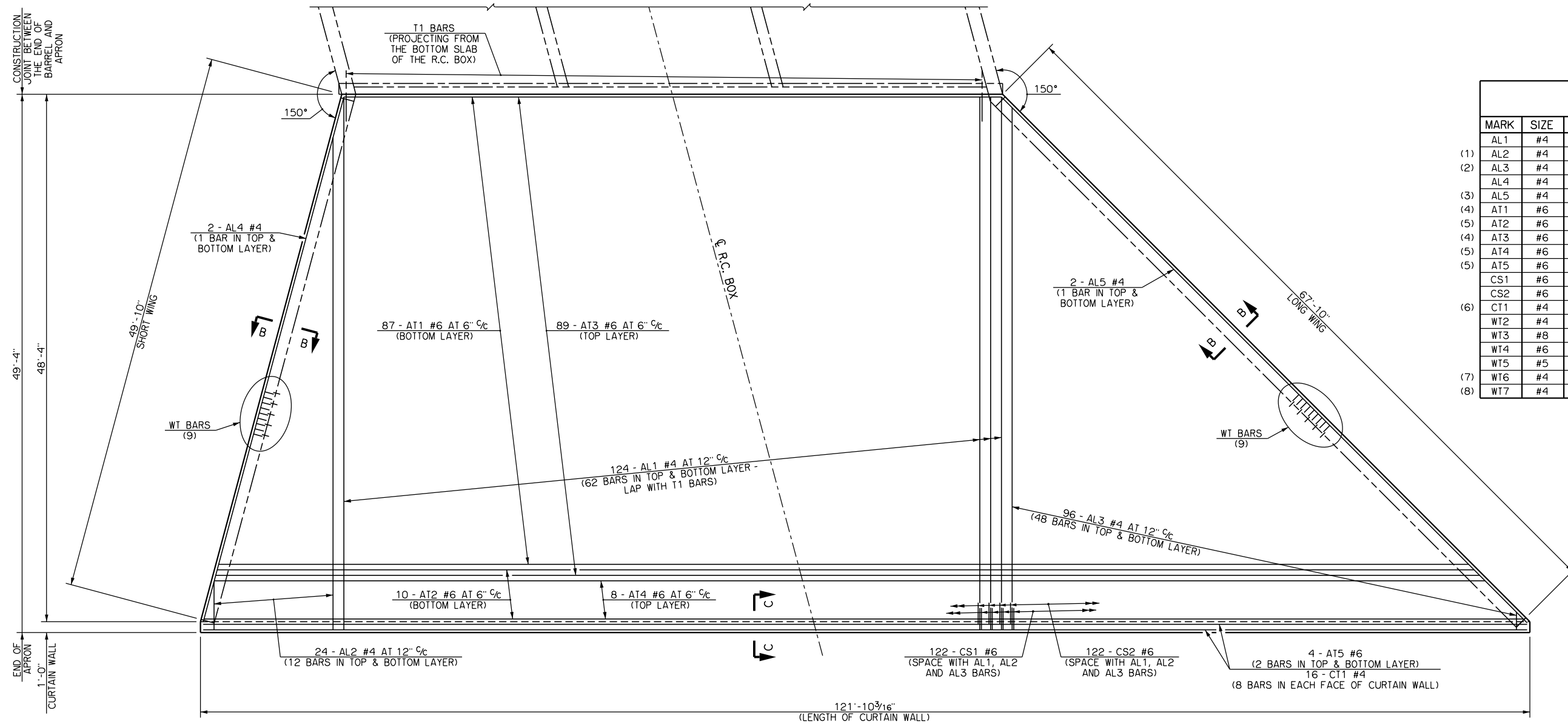
US-81 OVER LINE CREEK
BRIDGE 'K'

DETAILS OF REINFORCED CONCRETE BOX
(SHEET NO. 3 OF 6)

State Job No. 24428(04) Sheet No. B073

TOP SLAB REINFORCEMENT LAYOUT

US 81 REALIGNMENT
GRADY COUNTY

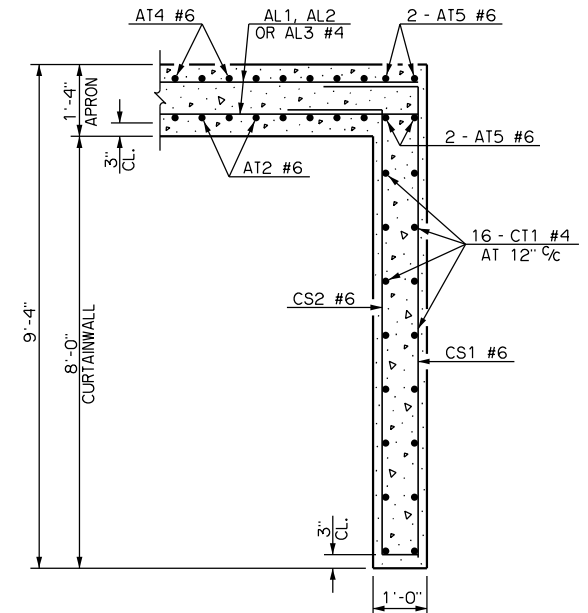


| BAR LIST - ONE APRON (TWO REQUIRED) | | | | | | |
|--|------|-----|------|--------|------------------|---------------------|
| MARK | SIZE | NO. | FORM | LENGTH | LENGTH VARIATION | |
| | AL1 | #4 | 124 | STR. | 49'-0" | - |
| (1) | AL2 | #4 | 24 | STR. | 24'-10 1/2" AVG. | 4'-6" TO 45'-3" |
| (2) | AL3 | #4 | 96 | STR. | 24'-10 1/2" AVG. | 1'-9" TO 48'-0" |
| | AL4 | #4 | 2 | STR. | 49'-11" | - |
| (3) | AL5 | #4 | 2 | STR. | 70'-6" | - |
| (4) | AT1 | #6 | 87 | BNT. | 92'-6" AVG. | 65'-3" TO 119'-9" |
| (5) | AT2 | #6 | 10 | BNT. | 126'-9" AVG. | 123'-11" TO 129'-7" |
| (4) | AT3 | #6 | 89 | STR. | 91'-9 1/2" AVG. | 63'-11" TO 119'-8" |
| (5) | AT4 | #6 | 8 | STR. | 126'-0 1/2" AVG. | 123'-10" TO 128'-3" |
| (5) | AT5 | #6 | 4 | STR. | 128'-7" | - |
| | CS1 | #6 | 122 | BNT. | 11'-2" | - |
| | CS2 | #6 | 122 | BNT. | 10'-11" | - |
| (6) | CT1 | #4 | 16 | STR. | 126'-1" | - |
| | WT2 | #4 | 98 | BNT. | 4'-0" | - |
| | WT3 | #8 | 48 | BNT. | 7'-11" | - |
| | WT4 | #6 | 54 | BNT. | 6'-4" | - |
| | WT5 | #5 | 69 | BNT. | 4'-11" | - |
| (7) | WT6 | #4 | 28 | BNT. | 3'-8 1/2" AVG. | 2'-1" TO 5'-4" |
| (8) | WT7 | #4 | 20 | BNT. | 3'-7 1/2" AVG. | 2'-1" TO 5'-2" |

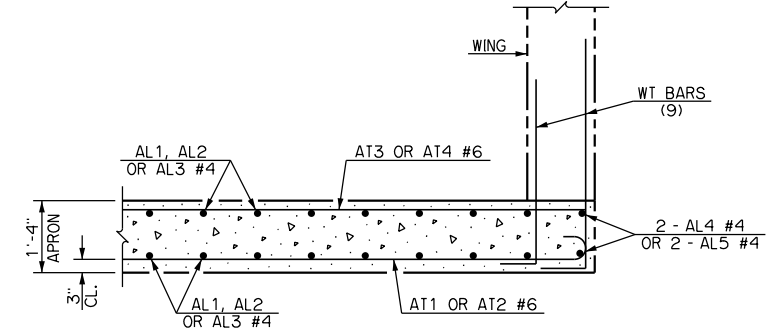
- (1) INCLUDES TWO SETS OF 12 BARS
- (2) INCLUDES TWO SETS OF 48 BARS
- (3) LENGTH INCLUDES ONE 2'-3" LAP, LAPS SHALL BE STAGGERED
- (4) LENGTH INCLUDES ONE 3'-6" LAP, LAPS SHALL BE STAGGERED
- (5) LENGTH INCLUDES TWO 3'-6" LAPS, LAPS SHALL BE STAGGERED
- (6) LENGTH INCLUDES TWO 2'-3" LAPS, LAPS SHALL BE STAGGERED
- (7) INCLUDES TWO SETS OF 14 BARS
- (8) INCLUDES TWO SETS OF 10 BARS

PLAN OF APRON

TYPICAL AT BOTH UPSTREAM AND DOWNSTREAM ENDS OF R.C. BOX



SECTION 'C-C'



SECTION 'B-B'

NOTES

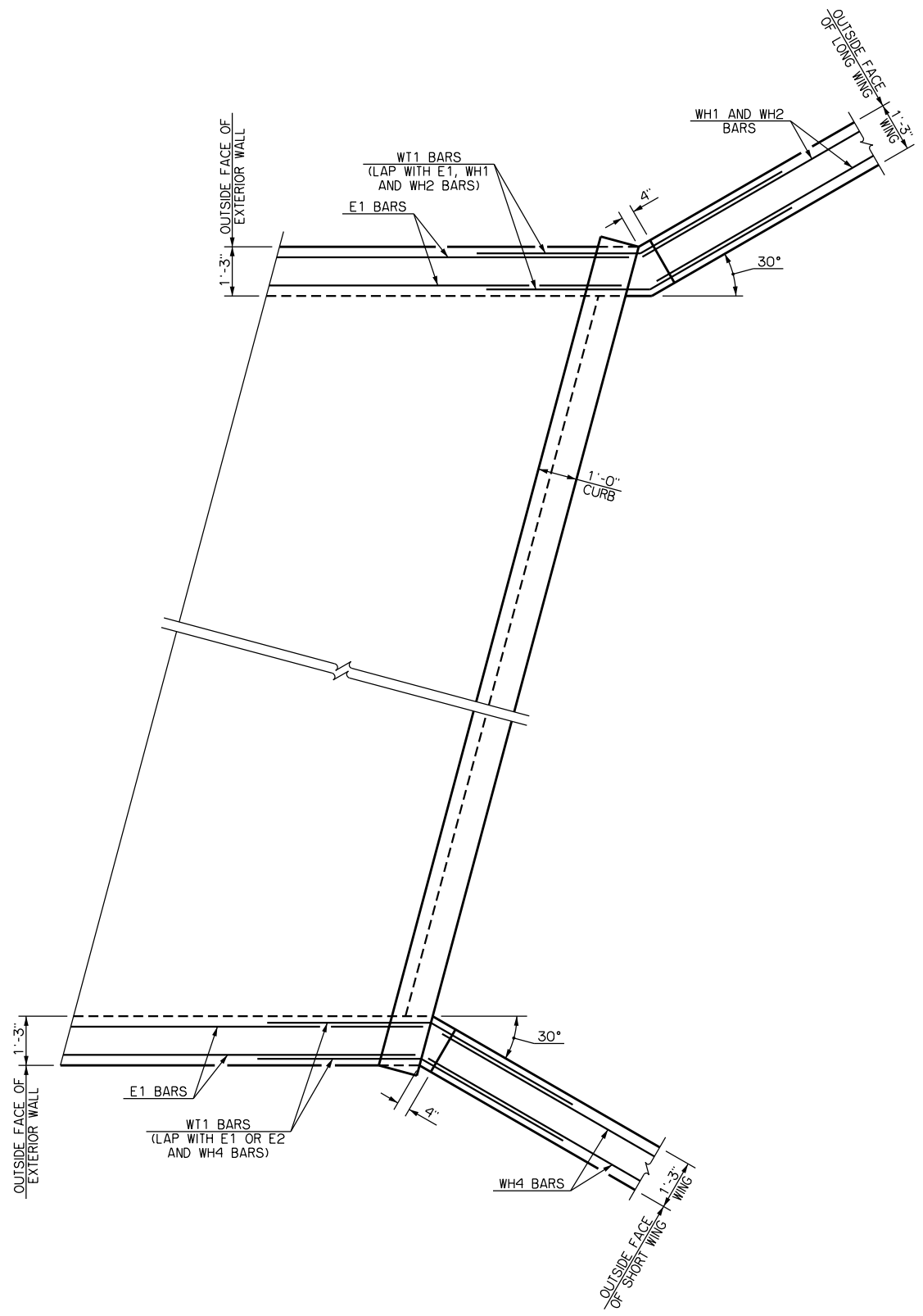
(9) THE WT BARS PROJECT FROM THE APRON INTO THE WINGS AND LAP WITH WY WING BARS. FOR DETAILS OF WT BARS NOT SHOWN INCLUDING BAR MARK, SIZE AND SPACING, SEE SHEET NO. B076.

US-81 OVER LINE CREEK BRIDGE 'K'

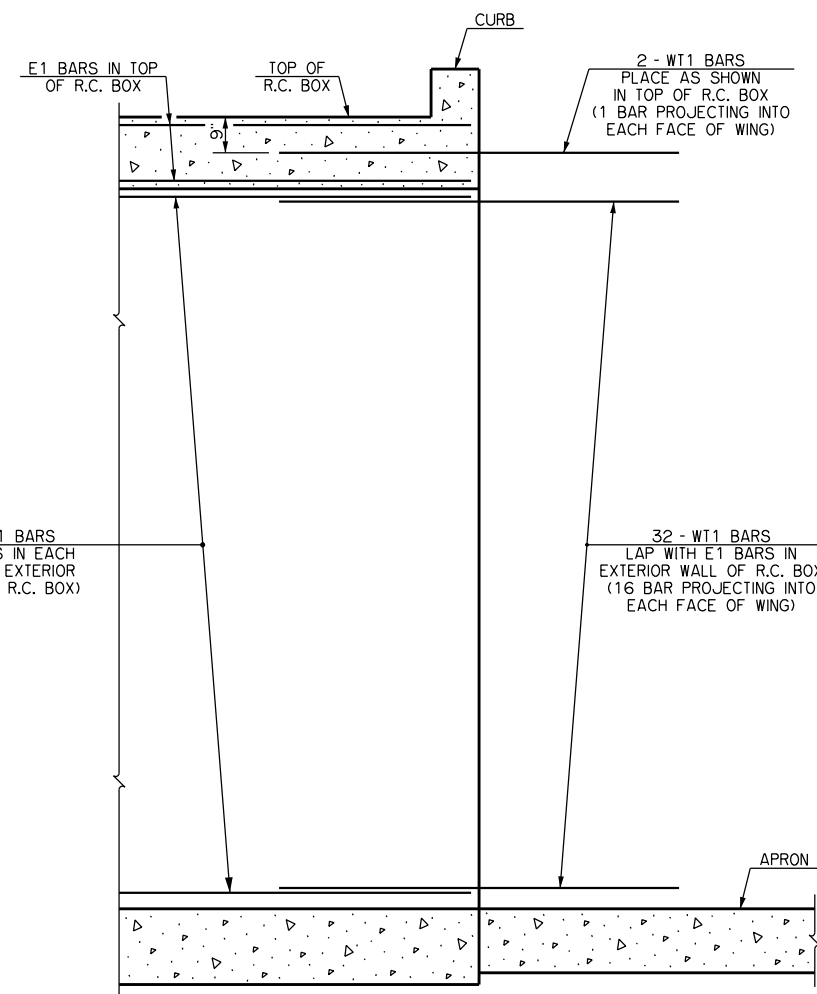
DETAILS OF REINFORCED CONCRETE BOX
(SHEET NO. 4 OF 6)

State Job No. 24428(04) Sheet No. B074

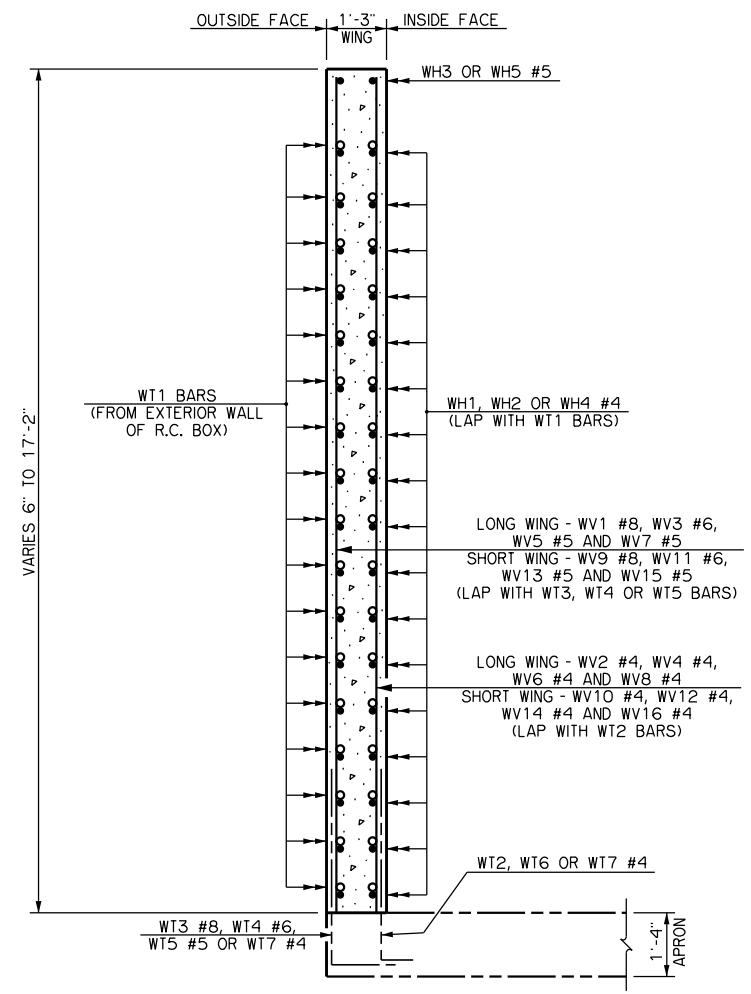
US 81 REALIGNMENT GRADY COUNTY



PLAN SHOWING HORIZONTAL REINFORCING CONNECTING WINGS TO R.C. BOX



ELEVATION SHOWING TYPICAL HORIZONTAL REINFORCING CONNECTING WINGS TO R.C. BOX



TYPICAL SECTION THRU WING AT END OF BARREL

**BAR LIST - ONE LONG WING
(TWO REQUIRED)**

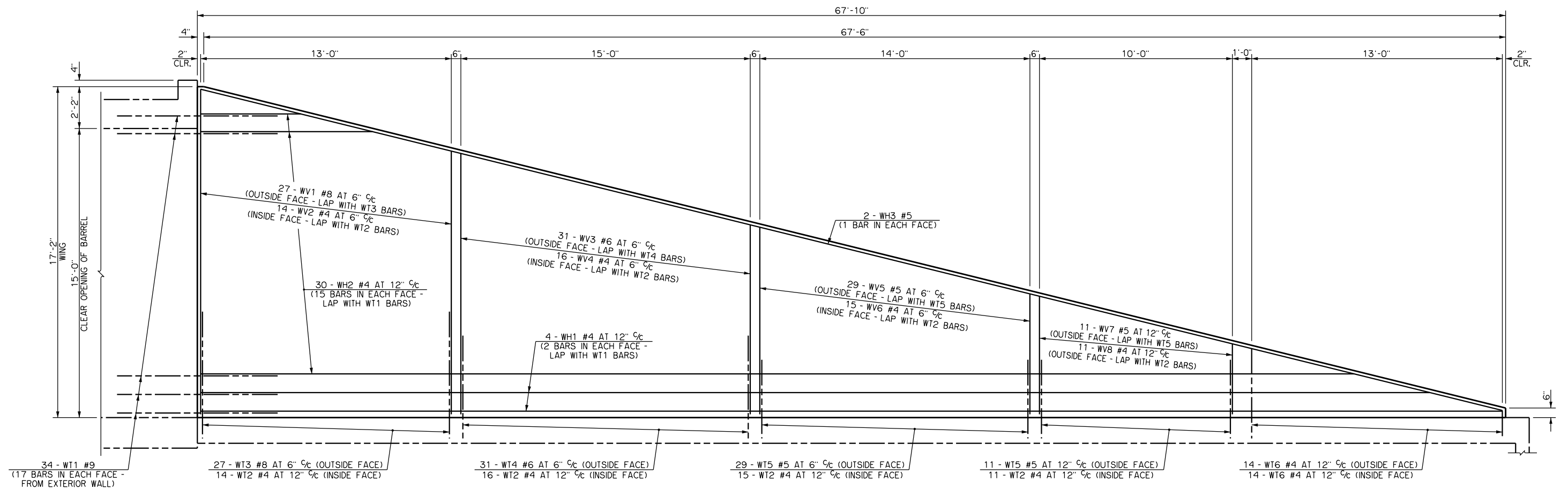
| MARK | SIZE | NO. | FORM | LENGTH | LENGTH VARIATION |
|---------|------|-----|------|------------------|-------------------|
| (1) WH1 | #4 | 4 | STR. | 67'-10 1/2" AVG. | 66'-0" TO 69'-9" |
| (2) WH2 | #4 | 30 | STR. | 32'-6 1/2" AVG. | 5'-3" TO 59'-10" |
| (3) WH3 | #5 | 2 | STR. | 72'-7" | - |
| WV1 | #8 | 27 | STR. | 15'-5" AVG. | 13'-10" TO 17'-0" |
| WV2 | #4 | 14 | STR. | 15'-5" AVG. | 13'-10" TO 17'-0" |
| WV3 | #6 | 31 | STR. | 11'-10" AVG. | 10'-0" TO 13'-8" |
| WV4 | #4 | 16 | STR. | 11'-10" AVG. | 10'-0" TO 13'-8" |
| WV5 | #5 | 29 | STR. | 8'-1 1/2" AVG. | 6'-5" TO 9'-10" |
| WV6 | #4 | 15 | STR. | 8'-1 1/2" AVG. | 6'-5" TO 9'-10" |
| WV7 | #5 | 11 | STR. | 5'-0 1/2" AVG. | 3'-10" TO 6'-3" |
| WV8 | #4 | 11 | STR. | 5'-0 1/2" AVG. | 3'-10" TO 6'-3" |

- (1) INCLUDES TWO SETS OF 2 BARS; LENGTH INCLUDES ONE 2'-3" LAP, LAPS SHALL BE STAGGERED
- (2) INCLUDES TWO SETS OF 15 BARS
- (3) INCLUDES ONE 3'-0" LAP, LAPS SHALL BE STAGGERED
- (4) INCLUDES TWO SETS OF 16 BARS

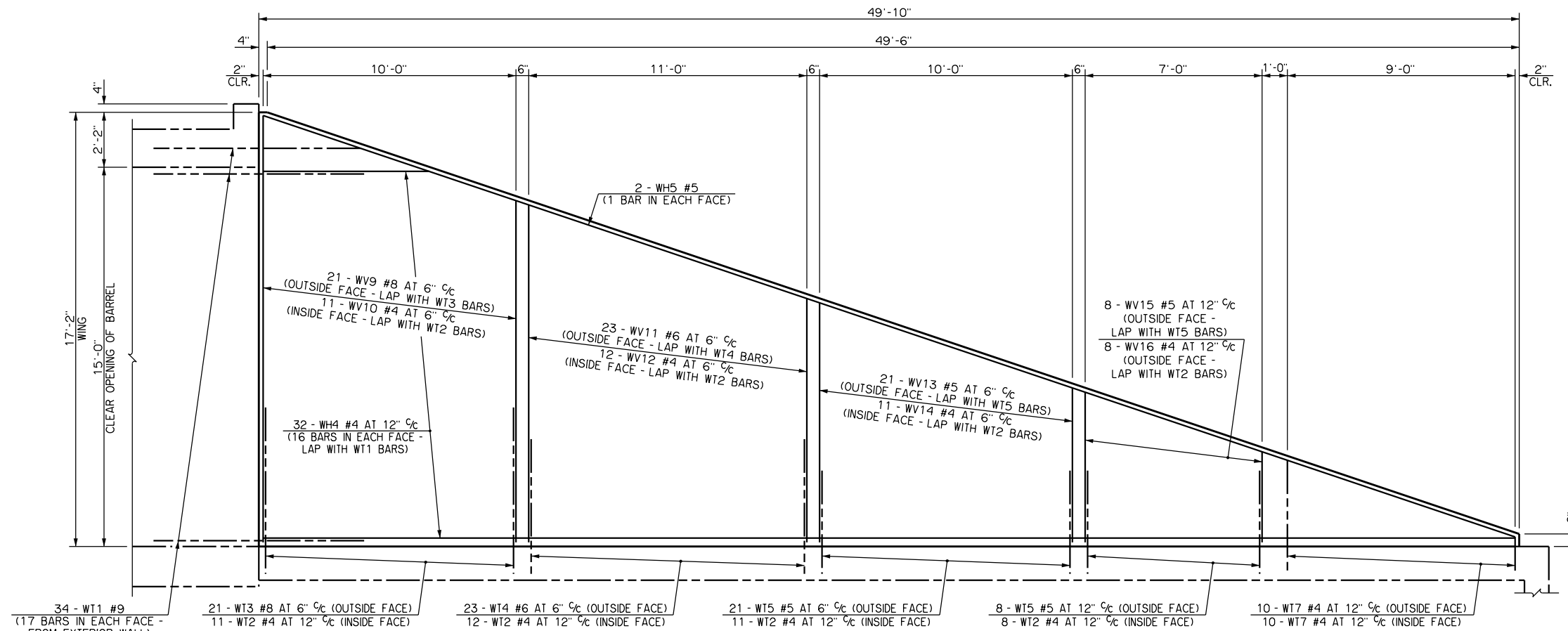
**BAR LIST - ONE SHORT WING
(TWO REQUIRED)**

| MARK | SIZE | NO. | FORM | LENGTH | LENGTH VARIATION |
|---------|------|-----|------|-----------------|------------------|
| (4) WH4 | #4 | 32 | STR. | 28'-0 1/2" AVG. | 6'-7" TO 49'-6" |
| WH5 | #5 | 2 | STR. | 52'-3" | - |
| WV9 | #8 | 21 | STR. | 15'-4" AVG. | 13'-8" TO 17'-0" |
| WV10 | #4 | 11 | STR. | 15'-4" AVG. | 13'-8" TO 17'-0" |
| WV11 | #6 | 23 | STR. | 11'-8" AVG. | 9'-10" TO 13'-6" |
| WV12 | #4 | 12 | STR. | 11'-8" AVG. | 9'-10" TO 13'-6" |
| WV13 | #5 | 21 | STR. | 7'-11 1/2" AVG. | 6'-3" TO 9'-8" |
| WV14 | #4 | 11 | STR. | 7'-11 1/2" AVG. | 6'-3" TO 9'-8" |
| WV15 | #5 | 8 | STR. | 4'-11" AVG. | 3'-9" TO 6'-1" |
| WV16 | #4 | 8 | STR. | 4'-11" AVG. | 3'-9" TO 6'-1" |

US-81 OVER LINE CREEK
BRIDGE 'K'
**DETAILS OF REINFORCED CONCRETE BOX
(SHEET NO. 5 OF 6)**



ELEVATION OF REINFORCING STEEL IN LONG WING (SOUTHWEST AND NORTHEAST WINGS)



ELEVATION OF REINFORCING STEEL IN SHORT WING (NORTHWEST AND SOUTHEAST WINGS)

NOTES

ALL HORIZONTAL DIMENSIONS SHOWN ARE MEASURED ALONG THE OUTSIDE FACE OF WINGS.

US-81 OVER LINE CREEK
BRIDGE 'K'

DETAILS OF REINFORCED CONCRETE BOX
(SHEET NO. 6 OF 6)

| REVISIONS | | |
|-----------|-------------|------|
| REV. NO. | DESCRIPTION | DATE |
| | | |

U.S. ARMY CORPS OF ENGINEERS SECTION 404 PERMIT CONDITIONS

| 404 PERMIT INFORMATION | PERMIT GENERAL CONDITIONS | PERMIT GENERAL CONDITIONS | | | | | | | | | | | | | | |
|---|--|---|---|--|--------|----------|--------|------------------------|----------|--|------------------------|--|-------------------|---|----------------|--|
| <p>NATIONWIDE PERMIT NO. _____</p> <p><input type="checkbox"/> TO BE PROVIDED AT A LATER DATE</p> <p>SECTION 404 OF THE CLEAN WATER ACT REQUIRES PRIOR AUTHORIZATION FROM SECRETARY OF THE ARMY (CORPS) FOR THE DISCHARGE OF DREDGED OR FILL MATERIAL INTO WATERS OF THE UNITED STATES.</p> <p><input type="checkbox"/> NO PRE-CONSTRUCTION NOTIFICATION REQUIRED: PROJECT DOES NOT REQUIRE NOTIFICATION TO THE US ARMY CORPS OF ENGINEERS (USACE) IN ORDER TO COMMENCE.</p> <p><input type="checkbox"/> PRE-CONSTRUCTION NOTIFICATION REQUIRED: RESIDENT ENGINEER MUST NOTIFY THE USACE WITHIN 30 DAYS OF THE START OF CONSTRUCTION AND 30 DAYS PRIOR TO COMPLETION OF CONSTRUCTION, FORMS LOCATED IN THE CONTRACT.</p> <p><input type="checkbox"/> INDIVIDUAL PERMIT: WILL BE MONITORED CLOSELY BY THE USACE.</p> <p><input type="checkbox"/> GENERAL PERMIT: PROJECT WITHIN A DESIGNATED CRITICAL RESOURCE WATER AND WILL REQUIRE PRE-CONSTRUCTION NOTIFICATION SEE ABOVE FOR EXPLANATION OF PRE-CONSTRUCTION NOTIFICATION.</p> <p><input type="checkbox"/> NO PERMIT REQUIRED</p> <p>SWT TRACKING NO. _____</p> | <p>THE CONTRACTOR SHALL BE RESPONSIBLE BUT NOT LIMITED TO THE FOLLOWING HIGHLIGHTS OF THE 404 PERMIT (SEE CONTRACT FOR COMPLETE LIST):</p> <p>TEMPORARY FILLS: APPROPRIATE MEASURES MUST BE TAKEN TO MAINTAIN NORMAL DOWNSTREAM FLOWS AND MINIMIZE FLOODING TO THE MAXIMUM EXTENT PRACTICABLE. WHEN TEMPORARY STRUCTURES (WORK ROADS, WORK PADS, ETC.) WORK, AND DISCHARGES, INCLUDING COFFERDAMS, ARE NECESSARY FOR CONSTRUCTION ACTIVITIES, ACCESS FILLS, OR DE WATERING OF CONSTRUCTION SITES. TEMPORARY FILLS MUST CONSIST OF MATERIALS, AND BE PLACED IN A MANNER, THAT WILL NOT BE ERODED BY EXPECTED HIGH FLOWS. TEMPORARY FILLS MUST BE REMOVED IN THEIR ENTIRETY AND THE AFFECTED AREAS RETURNED TO PRE-CONSTRUCTION ELEVATIONS. THE AREAS AFFECTED BY TEMPORARY FILLS MUST BE RE VEGETATED, AS APPROPRIATE.</p> <p>NAVIGATION: NO ACTIVITY MAY CAUSE MORE THAN A MINIMAL ADVERSE EFFECT ON NAVIGATION WITHIN A NAVIGABLE WATER OF THE U.S. IF THIS PROJECT IS LOCATED WITHIN A NAVIGABLE WATER OF THE U.S., IT WILL BE IDENTIFIED IN THE SPECIAL CONDITIONS.</p> <p>AQUATIC LIFE MOVEMENTS & ADVERSE EFFECTS FROM IMPOUNDMENTS: NO ACTIVITY MAY LARGELY DISRUPT THE NECESSARY LIFE CYCLE MOVEMENTS OF THOSE SPECIES INDIGENOUS TO THE BODY OF WATER, INCLUDING THOSE SPECIES THAT NORMALLY MIGRATE THROUGH THE AREA. CULVERTS WILL BE DESIGNED TO PROVIDE SUFFICIENT PASSAGE FOR AQUATIC LIFE AND INSTALLED TO MAINTAIN LOW FLOW. RATE OF FLOW CANNOT BE MADE HIGHER THAN WHAT WAS PRIOR TO THE START OF CONSTRUCTION. EROSION CONTROL MEASURES SHOULD BE UTILIZED AROUND THE PERIMETER OF NEW STRUCTURES TO AVOID SILT BUILD UP. CAUTION SHOULD BE TAKEN TO MINIMIZE HARM IF CONSTRUCTION ACTIVITIES TAKE PLACE WITHIN A STREAM OR RIVER CHANNEL AND CREATE A CONFINED BODY OF WATER, CAUSE ADVERSE EFFECTS TO THE AQUATIC SYSTEM IN ANY WAY AND/OR RESTRICTING ITS FLOW.</p> <p>MANAGEMENT OF WATER FLOWS: CONSTRUCTION ACTIVITIES MAY NOT IMPEDE THE PASSAGE OF NORMAL OR HIGH FLOWS. TO THE GREATEST EXTENT POSSIBLE, THE PRE- CONSTRUCTION COULSE, CONDITIONS, CAPACITY AND LOCATION OF OPEN WATERS MUST BE MAINTAINED. THIS INCLUDES STREAM CHANNELIZATION AND STORAGE WATER MANAGEMENT.</p> <p>SUITABLE MATERIAL: NO ACTIVITY MAY USE UNSUITABLE MATERIAL (E.G., TRASH, DEBRIS, CAR BODIES, ASPHALT, ETC.). MATERIALS USED FOR CONSTRUCTION OR DISCHARGED MUST BE FREE FROM TOXIC POLLUTANTS IN TOXIC AMOUNTS (SEE SECTION 307 OF CLEAN WATER ACT).</p> <p>PROPER MAINTENANCE: ANY AUTHORIZED STRUCTURE OR FILL SHALL BE PROPERLY MAINTAINED, INCLUDING MAINTENANCE TO ENSURE PUBLIC SAFETY AND COMPLIANCE WITH APPLICABLE NATION WIDE PERMIT GENERAL CONDITIONS, AS WELL AS ANY ACTIVITY- SPECIFIC CONDITIONS ADDED BY THE DISTRICT ENGINEER TO AN NATIONWIDE PERMIT AUTHORIZATION</p> <p>HAZARDOUS MATERIALS: HAZARDOUS MATERIALS, CHEMICALS, FUELS, LUBRICATING OILS AND OTHER SUCH SUBSTANCES SHOULD BE STORED AWAY FROM ANY STREAM OR RIVERCHANNEL (SEE SECTION 307 OF CLEAN WATER ACT)</p> <p>EQUIPMENT: HEAVY EQUIPMENT WORKING IN WETLANDS OR MUDFLATS MUST BE PLACED ON MATS, OR OTHER MEASURES MUST BE TAKEN TO MINIMIZE SOIL DISTURBANCE; FOR EXAMPLE IF WETLANDS ARE PRESENT WITHIN THE CONSTRUCTION, THE FOOTPRINT WILL BE SHOWN ON THE PLANS. MEASURES SHOULD BE TAKEN TO PREVENT DISCHARGE INTO ANY WATERS OF THE STATE (e.g. CONCRETE WASHOUT).</p> <p>SOIL EROSION AND SEDIMENT CONTROLS: APPROPRIATE SOIL EROSION AND SEDIMENT CONTROLS MUST BE USED AND MAINTAINED IN EFFECTIVE OPERATING CONDITION DURING CONSTRUCTION, AND ALL EXPOSED SOILS AND OTHER FILLS, AS WELL AS ANY WORK WITHIN STREAM OR RIVER CHANNELS OR BANKS, MUST BE PERMANENTLY STABILIZED AS SOON AS POSSIBLE.</p> <p>404 COMPLIANCE: IN ORDER TO REMAIN COMPLIANT WITH THE 404 PERMIT, THE PROJECT MUST COMPLY WITH ALL FEDERAL ENVIRONMENTAL PROTECTION LAWS ASSOCIATED AND, THE ENVIRONMENTAL COMMITMENTS AS SHOWN ON THE PLANS. THIS INCLUDES BUT IS NOT LIMITED TO COMPLIANCE WITH ALL ENVIRONMENTAL NOTES IN THE PLANS, INCLUDING CULTURAL RESOURCES, HAZARDOUS WASTE, BIOLOGICAL FOR PROTECTED SPECIES, AND DEQ STORM WATER REGULATIONS AS THEY PERTAIN TO THE SWMP SHEET WITHIN THE PLANS. ALL OF THE 404 PERMIT GENERAL AND SPECIFIC CONDITIONS MUST BE ADHERED TO. A COPY OF THESE CONDITIONS CAN BE FOUND IN THE CONTRACT WITH THE 404 PERMIT.</p> | <p>FUELING: ALL FUELING AND SERVICING OF VEHICLES AND EQUIPMENT SHALL BE DONE ABOVE THE ORDINARY HIGH WATER MARK (OHWM).</p> <p>MATERIAL STORAGE: STORE MATERIAL AND FUEL OUTSIDE OF THE ORDINARY HIGH WATER MARK OR ANY AREA LIKELY TO FLOOD.</p> <p>DEBRIS STORAGE: THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING ANY MATERIALS, DEBRIS, OR REFUSE WHICH HAS FALLEN INTO ANY STREAM OR RIVER CHANNELS RESULTING FROM THE EXECUTION OF THE PROJECT AS SOON AS POSSIBLE</p> <p style="text-align: center;">SEE NATIONWIDE PERMIT 14 IN THE CONTRACT</p> | | | | | | | | | | | | | | |
| <h2 style="text-align: center;">SPECIAL CONDITIONS</h2> <p><input type="checkbox"/> NAVIGABLE WATER OF THE U.S.</p> <p><input type="checkbox"/> ON-SITE MITIGATION</p> <p><input type="checkbox"/> ENDANGERED SPECIES PRESENT</p> <p><input type="checkbox"/> HISTORIC PROPERTIES PRESENT</p> <p><input type="checkbox"/> DESIGNATED CRITICAL RESOURCE WATERS</p> | <p style="text-align: center; color: red; font-size: 2em; opacity: 0.5;">PLACEHOLDER</p> | <h2 style="text-align: center;">401 CERTIFICATION CONDITIONS</h2> <p>THE CONTRACTOR SHALL BE RESPONSIBLE BUT NOT LIMITED TO THE FOLLOWING HIGHLIGHTS OF THE 401 CERTIFICATION (SEE CONTRACT FOR COMPLETE LIST):</p> <p><input type="checkbox"/> ALL SPILLS OF FUEL OR POLLUTANTS IN EXCESS OF FIVE GALLONS SHALL BE REPORTED TO ODEQ WITHIN 24 HRS AND REPORTED TO POLLUTION PREVENTION HOTLINE (1-800-522-0206)</p> <p><input type="checkbox"/> ALL FUELING AND SERVICING OF VEHICLES AND EQUIPMENT SHALL BE DONE OUTSIDE THE ORDINARY HIGH WATER MARK</p> <p><input type="checkbox"/> THE PERMITTEE SHALL PROVIDE ACCESS TO THE PROPERTY TO ODEQ FOR INSPECTIONS.</p> <p><input type="checkbox"/> ANY STOCKPILE SHALL BE ABOVE ORDINARY HIGH WATER MARK AND REMOVED FROM LIKELY FLOOD ZONE</p> <p><input type="checkbox"/> BEST MANAGEMENT PRACTICES SHOULD BE USED TO CONTROL SOIL EROSION AND MAINTAIN COMPLIANCE WITH WATER QUALITY STANDARDS.</p> <p><input type="checkbox"/> FOR ANY PROJECT THAT INVOLVES BANK STABILIZATION, THE PERMITTEE SHALL CONSIDER INSTALLING BIOENGINEERING PRACTICES IN PLACE OF STRUCTURAL PRACTICES (RIPRAP) TO MINIMIZE IMPACTS TO AQUATIC RESOURCES</p> | | | | | | | | | | | | | | |
| | | <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 80%; text-align: center; vertical-align: middle;"> <h3 style="margin: 0;">SECTION 404 PERMIT COMPLIANCE</h3> </td> <td style="width: 20%; text-align: center;"> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr><td style="font-size: 8px;">DETAIL</td><td style="width: 20px;"> </td></tr> <tr><td style="font-size: 8px;">REVIEW</td><td> </td></tr> <tr><td style="font-size: 8px;">APPROVED</td><td> </td></tr> <tr><td style="font-size: 8px;">ENVIRONMENTAL DIVISION</td><td> </td></tr> </table> </td> </tr> <tr> <td style="font-size: 8px;">STATE OF OKLAHOMA</td> <td style="font-size: 8px;">DEPARTMENT OF TRANSPORTATION JOB/PIECE NO. 24428(O4)</td> </tr> <tr> <td colspan="2" style="font-size: 8px; text-align: right;">SHEET NO. E001</td> </tr> </table> | <h3 style="margin: 0;">SECTION 404 PERMIT COMPLIANCE</h3> | <table border="1" style="width: 100%; border-collapse: collapse;"> <tr><td style="font-size: 8px;">DETAIL</td><td style="width: 20px;"> </td></tr> <tr><td style="font-size: 8px;">REVIEW</td><td> </td></tr> <tr><td style="font-size: 8px;">APPROVED</td><td> </td></tr> <tr><td style="font-size: 8px;">ENVIRONMENTAL DIVISION</td><td> </td></tr> </table> | DETAIL | | REVIEW | | APPROVED | | ENVIRONMENTAL DIVISION | | STATE OF OKLAHOMA | DEPARTMENT OF TRANSPORTATION JOB/PIECE NO. 24428(O4) | SHEET NO. E001 | |
| <h3 style="margin: 0;">SECTION 404 PERMIT COMPLIANCE</h3> | <table border="1" style="width: 100%; border-collapse: collapse;"> <tr><td style="font-size: 8px;">DETAIL</td><td style="width: 20px;"> </td></tr> <tr><td style="font-size: 8px;">REVIEW</td><td> </td></tr> <tr><td style="font-size: 8px;">APPROVED</td><td> </td></tr> <tr><td style="font-size: 8px;">ENVIRONMENTAL DIVISION</td><td> </td></tr> </table> | DETAIL | | REVIEW | | APPROVED | | ENVIRONMENTAL DIVISION | | | | | | | | |
| DETAIL | | | | | | | | | | | | | | | | |
| REVIEW | | | | | | | | | | | | | | | | |
| APPROVED | | | | | | | | | | | | | | | | |
| ENVIRONMENTAL DIVISION | | | | | | | | | | | | | | | | |
| STATE OF OKLAHOMA | DEPARTMENT OF TRANSPORTATION JOB/PIECE NO. 24428(O4) | | | | | | | | | | | | | | | |
| SHEET NO. E001 | | | | | | | | | | | | | | | | |

STORM WATER MANAGEMENT PLAN

FINAL FIELD MEETING
MARCH 2020

SITE DESCRIPTION

PROJECT LIMITS: US-81 REALIGNMENT FROM QUAIL RD EXT N 6.81 MI TO .22 MI N OF THE US 62/US 81 JCT

PROJECT DESCRIPTION: GRADE, DRAIN & BRIDGE

SUGGESTED SEQUENCE OF EROSION CONTROL ACTIVITIES: PRIOR TO INITIATING SOIL DISTURBING ACTIVITIES, THE CONTRACTOR WILL INSTALL ALL PERIMETER TEMPORARY SEDIMENT CONTROLS SPECIFIED, STRIP, STOCKPILE AND STABILIZE TOPSOIL. CLEAR AND GRUB ONLY IN NECESSARY AREAS, PRESERVING AS MUCH NATIVE VEGETATION AS POSSIBLE. INSTALL, MAINTAIN AND/OR MOVE TEMPORARY SEDIMENT ITEMS WITH CONSTRUCTION OPERATIONS AS PRACTICAL. IF DIRECTED BY THE ENGINEER, PLANT TEMPORARY SEEDING, REPLACE TOPSOIL AND DEVICES WHEN AN ACCEPTABLE VEGETATIVE COVER (AT LEAST 70%) HAS BEEN ATTAINED. AS SITE CONDITIONS WARRANT, THE CONTRACTOR MAY CHOOSE TO MODIFY THE TYPE OR ARRANGEMENT OF SPECIFIED PRACTICES TO IMPROVE THEIR EFFECTIVENESS AS APPROVED BY THE ENGINEER, THE CONTRACTOR WILL MAINTAIN A LOG OF THE DATES OF MAJOR SOIL DISTURBANCE ACTIVITIES, AND ALSO THE DATES OF INSTALLATION OF EROSION CONTROL MEASURES.

SOIL TYPE: SILT LOAM

TOTAL AREA OF THE CONSTRUCTION SITE: 599.57 ACRES

ESTIMATED AREA TO BE DISTURBED: 401.53 ACRES

OFFSITE AREA TO BE DISTURBED: (FOR CONTRACTOR USE)

TOTAL IMPERVIOUS AREA PRE-CONSTRUCTION: 24.89 ACRES

TOTAL IMPERVIOUS AREA POST-CONSTRUCTION: 25.92 ACRES

POST-CONSTRUCTION RUNOFF COEFFICIENT OF THE SITE: 0.52

LATITUDE & LONGITUDE OF CENTER OF PROJECT: 35.024638° N, 97.998482° W

PROJECT WILL DISCHARGE TO:
LINE CREEK, TRIBUTARY OF LINE CREEK, ROCK HOLLOW CREEK, TRIBUTARY OF ROCK HOLLOW

NAME OF RECEIVING WATERS: CREEK, UNNAMED CREEK.

SENSITIVE WATERS OR WATERSHEDS: YES NO

303(d) IMPAIRED WATERS: YES NO

IF YES, LIST IMPAIRMENT: _____

LOCATED IN A TMDL: YES NO

LAKE THUNDERBIRD TMDL: YES NO

MS4 ENTITY: YES NO

IF YES, LOCATION: _____

NOTE:
THIS SHEET SHOULD BE USED IN CONJUNCTION WITH A DRAINAGE MAP THAT ILLUSTRATES THE DRAINAGE PATTERNS/PATHWAYS AND RECEIVING WATERS FOR THIS PROJECT. THIS SHEET SHOULD ALSO BE USED WITH THE EROSION CONTROL SUMMARIES, PAY ITEMS, & NOTES.

EROSION AND SEDIMENT CONTROLS

SOIL STABILIZATION PRACTICES:

- TEMPORARY SEEDING
- PERMANENT SODDING, SPRIGGING OR SEEDING
- VEGETATIVE MULCHING
- SOIL RETENTION BLANKET
- PRESERVATION OF EXISTING VEGETATION

NOTE: TEMPORARY EROSION CONTROL METHODS MUST BE USED ON ALL DISTURBED AREAS WHERE CONSTRUCTION ACTIVITIES HAVE CEASED FOR OVER 14 DAYS. METHODS USED WILL BE AS SHOWN ON PLANS, OR AS DIRECTED BY THE ENGINEER.

STRUCTURAL PRACTICES:

- STABILIZED CONSTRUCTION EXIT
- TEMPORARY SILT FENCE
- TEMPORARY SILT DIKES
- TEMPORARY FIBER LOG
- DIVERSION, INTERCEPTOR OR PERIMETER DIKES
- DIVERSION, INTERCEPTOR OR PERIMETER SWALES
- ROCK FILTER DAMS
- TEMPORARY SLOPE DRAIN
- PAVED DITCH W/ DITCH LINER PROTECTION
- TEMPORARY DIVERSION CHANNELS
- TEMPORARY SEDIMENT BASINS
- TEMPORARY SEDIMENT TRAPS
- TEMPORARY SEDIMENT FILTERS
- TEMPORARY SEDIMENT REMOVAL
- RIP RAP
- INLET SEDIMENT FILTER
- TEMPORARY BRUSH SEDIMENT BARRIERS
- SANDBAG BERMS
- TEMPORARY STREAM CROSSINGS

OFFSITE VEHICLE TRACKING:

- HAUL ROADS DAMPENED FOR DUST CONTROL
- LOADED HAUL TRUCKS TO BE COVERED WITH TARPULIN
- EXCESS DIRT ON ROAD REMOVED DAILY

NOTES:

THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR THE FOLLOWING:

MAINTENANCE AND INSPECTION:

ALL EROSION AND SEDIMENT CONTROLS WILL BE MAINTAINED IN GOOD WORKING ORDER FROM THE BEGINNING OF CONSTRUCTION UNTIL AN ACCEPTABLE VEGETATIVE COVER IS ESTABLISHED. INSPECTION BY THE CONTRACTOR AND ANY NECESSARY REPAIRS SHALL BE PERFORMED ONCE EVERY 7 CALENDAR DAYS AND WITHIN 24 HOURS AFTER ANY STORM EVENT GREATER THAN 0.5 INCH AS RECORDED BY A NON-FREEZING RAIN GAUGE TO BE LOCATED ON SITE. POTENTIALLY ERODIBLE AREAS, DRAINAGEWAYS, MATERIAL STORAGE, STRUCTURAL DEVICES, CONSTRUCTION ENTRANCES AND EXITS ALONG WITH EROSION AND SEDIMENT CONTROL LOCATIONS ARE EXAMPLES OF SITES THAT NEED TO BE INSPECTED.

WASTE MATERIALS:

PROPER MANAGEMENT AND DISPOSAL OF CONSTRUCTION WASTE MATERIAL IS REQUIRED BY THE CONTRACTOR. MATERIALS INCLUDE STOCKPILES, SURPLUS, DEBRIS AND ALL OTHER BY-PRODUCTS FROM THE CONSTRUCTION PROCESS. PRACTICES INCLUDE DISPOSAL, PROPER MATERIALS HANDLING, SPILL PREVENTION AND CLEANUP MEASURES. CONTROLS AND PRACTICES SHALL MEET THE REQUIREMENTS OF ALL FEDERAL, STATE AND LOCAL AGENCIES.

HAZARDOUS MATERIALS:

PROPER MANAGEMENT AND DISPOSAL OF HAZARDOUS WASTE MATERIALS IS REQUIRED. THE CONTRACTOR IS RESPONSIBLE FOR FOLLOWING MANUFACTURER'S RECOMMENDATIONS, STATE AND FEDERAL REGULATIONS TO ENSURE CORRECT HANDLING, DISPOSAL, SPILL PREVENTION AND CLEANUP MEASURES. EXAMPLES INCLUDE BUT ARE NOT LIMITED TO: PAINTS, ACIDS, CLEANING SOLVENTS, CHEMICAL ADDITIVES, CONCRETE CURING COMPOUNDS AND CONTAMINATED SOILS.

GENERAL NOTES:

A STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IS REQUIRED TO COMPLY WITH THE OKLAHOMA POLLUTION DISCHARGE ELIMINATION SYSTEM (OPDES) REGULATIONS. THIS PLAN IS INITIATED DURING THE DESIGN PHASE, CONFIRMED IN THE PRE-WORK MEETINGS AND AVAILABLE ON THE JOB SITE ALONG WITH COPIES OF THE NOTICE OF INTENT (NOI) FORM AND PERMIT CERTIFICATE THAT HAVE BEEN FILED WITH THE OKLAHOMA DEPARTMENT OF ENVIRONMENTAL QUALITY (ODEQ). THE PLAN MUST BE KEPT CURRENT WITH UP-TO-DATE AMENDMENTS DURING THE PROGRESSION OF THE PROJECT. ALL CONTRACTOR OFF-SITE OPERATIONS ASSOCIATED WITH THE PROJECT MUST BE DOCUMENTED IN THE SWPPP, I.E., BORROW PITS, WORK ROADS, DISPOSAL SITES, ASPHALT/CONCRETE PLANTS, ETC. THE BASIC GOAL OF STORM WATER MANAGEMENT IS TO IMPROVE WATER QUALITY BY REDUCING POLLUTANTS IN STORM WATER DISCHARGES. RUNOFF FROM CONSTRUCTION SITES HAS A POTENTIAL FOR POLLUTION DUE TO EXPOSED SOILS AND THE PRESENCE OF HAZARDOUS MATERIALS USED IN THE CONSTRUCTION PROCESS. THE PREVENTION OF SOIL EROSION, CONTAINMENT OF HAZARDOUS MATERIALS AND/OR THE INTERCEPTION OF THESE POLLUTANTS BEFORE LEAVING THE CONSTRUCTION SITE ARE THE BEST PRACTICES FOR CONTROLLING STORM WATER POLLUTION.

THE FOLLOWING SECTIONS OF THE 2009 ODOT STANDARD SPECIFICATIONS SHOULD BE NOTED:

- 103.05 BONDING REQUIREMENTS
- 104.10 FINAL CLEANING UP
- 104.12 CONTRACTOR'S RESPONSIBILITY FOR WORK
- 104.13 ENVIRONMENTAL PROTECTION
- 106.08 STORAGE AND HANDLING OF MATERIAL
- 107.01 LAWS, RULES AND REGULATIONS TO BE OBSERVED
- 107.20 STORM WATER MANAGEMENT
- 220 MANAGEMENT OF EROSION, SEDIMENTATION AND STORM WATER POLLUTION PREVENTION AND CONTROL
- 221 TEMPORARY SEDIMENT CONTROL

IN ADDITION:

"ODEQ GENERAL PERMIT (OKR10) FOR STORM WATER DISCHARGES FROM CONSTRUCTION ACTIVITIES WITHIN THE STATE OF OKLAHOMA." ODEQ, WATER QUALITY DIVISION, SEPTEMBER 13, 2017.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

STORM WATER MANAGEMENT PLAN

DRAINAGE RECORD

| STR. NO. | ALIGNMENT | LOCATION (STATION) | LOCATION (OFFSET) | LT/RT | D.A. NO. | AREA (AC) | C | Tc (MIN) | I ₁₀ (IN/HR) | I ₅₀ (IN/HR) | TOTAL Q ₁₀ (CFS) | TOTAL Q ₅₀ (CFS) | Q _{INT0} (CFS) | Q _{INT50} (CFS) | INLET TYPE | DESIGN Q | ADJ. CA | ADD'N CA | FROM STR. NO. | CUMULATIVE TC (MIN) | I (IN/HR) | TOTAL Q IN CONDUIT (CFS) | PIPE DIA. (IN) | SLOPE (FT/FT) | VELOCITY (FT/S) | SLOPE LENGTH (FT/FT) | LENGTH (FT) | TIME IN PIPE (MIN) | TOTAL TC (MIN) | TO STR. NO. | FLOWLINE (FT) | D.S. FLOWLINE (FT) | TOP OF RIM/GRATE (FT) | SPREAD (FT) |
|----------|-----------|--------------------|-------------------|-------|----------|-----------|------|----------|-------------------------|-------------------------|-----------------------------|-----------------------------|-------------------------|--------------------------|------------------------|----------|----------|----------|------------------------|---------------------|-----------|--------------------------|----------------|---------------|-----------------|----------------------|-------------|--------------------|----------------|-------------|---------------|--------------------|-----------------------|-------------|
| 8 | US 81 | 143+15.00 | 0.00' | ε | 8 | 6.61 | 0.51 | 17.82 | 3.47 | 5.90 | 17.06 | 26.70 | 17.06 | 26.70 | RCP | Q50 | 4.0453 | 0.00 | DA 8 | 17.82 | 6.60 | 26.68 | 36" | 0.0026 | 6.62 | 334.70 | 336 | 0.84 | 18.66 | DITCH | 1170.46 | 1169.60 | | |
| 9 | US 81 | 149+50.00 | 0.00' | ε | 9 | 2.07 | 0.73 | 14.43 | 3.86 | 6.43 | 8.42 | 13.04 | 8.42 | 13.04 | SMD TYPE 2 | Q50 | 1.8133 | 0.00 | DA 9 | 14.43 | 7.19 | 13.04 | 24" | 0.0977 | 15.80 | 72.28 | 72 | 0.08 | 14.51 | STR 11 | 1168.00 | 1160.97 | 1188.40 | |
| 10 | US 81 | 149+70.00 | 244.75' | LT. | 10 | 4.60 | 0.51 | 17.85 | 3.46 | 5.89 | 11.87 | 18.55 | 11.87 | 18.55 | SMD TYPE 2 | Q50 | 2.8152 | 0.00 | DA 10 | 17.85 | 6.59 | 18.56 | 24" | 0.0440 | 14.04 | 120.71 | 122 | 0.14 | 17.99 | STR 11 | 1167.69 | 1162.39 | 1170.25 | |
| 11 | US 81 | 150+34.00 | 0.00' | ε | 11 | 407.43 | 0.50 | 70.51 | 1.45 | 2.77 | 462.43 | 757.82 | 462.43 | 757.82 | RCB | Q50 | 244.4580 | 4.63 | DA 11 & STR 9, 10 | 73.20 | 0.00 | 801.23 | 2-8"x6" | 0.0066 | 15.72 | 574.01 | 574 | 0.61 | 73.81 | CREEK | 1159.75 | 1155.98 | | |
| 12 | US 81 | 158+00.00 | 0.00' | ε | 12 | 1.62 | 0.76 | 14.90 | 3.80 | 6.35 | 6.76 | 10.49 | 6.76 | 10.49 | SMD TYPE 2 | Q50 | 1.4774 | 0.00 | DA 12 | 14.90 | 7.10 | 10.49 | 18" | 0.0365 | 11.51 | 142.09 | 142 | 0.21 | 15.11 | DITCH | 1174.00 | 1168.81 | 1181.67 | |
| 13 | US 81 | 159+03.00 | 0.00' | ε | 13 | 5.37 | 0.71 | 13.98 | 3.92 | 6.51 | 21.54 | 33.31 | 21.54 | 33.31 | SMD TYPE 2 | Q50 | 4.5752 | 0.00 | DA 13 | 13.98 | 7.28 | 33.30 | 24" | 0.0634 | 15.57 | 138.03 | 138 | 0.15 | 14.13 | DITCH | 1178.82 | 1170.09 | 1181.39 | |
| 14 | US 81 | 160+00.00 | 0.00' | ε | 14 | 2.89 | 0.71 | 13.62 | 3.96 | 6.57 | 11.72 | 18.10 | 11.72 | 18.10 | SMD TYPE 2 | Q50 | 2.4623 | 0.00 | DA 14 | 13.62 | 7.35 | 18.10 | 24" | 0.0274 | 11.16 | 134.94 | 136 | 0.20 | 13.82 | DITCH | 1175.00 | 1171.30 | 1181.63 | |
| 15 | US 81 | 172+00.00 | 67.08' | LT. | 15 | 0.37 | 0.86 | 10.00 | 4.54 | 7.31 | 4.05 | 7.09 | 1.97 | 2.98 | INLET LB TYPE I DES. 2 | Q10 | 0.3059 | 0.00 | DA 15 | 10.00 | 6.44 | 1.97 | 18" | 0.0590 | 9.30 | 63.45 | 64 | 0.11 | 10.11 | STR 16 | 1206.44 | 1202.70 | 1213.44 | |
| 16 | US 81 | 172+00.00 | 0.00' | ε | 16 | 2.06 | 0.53 | 12.56 | 4.12 | 6.77 | 6.44 | 9.92 | 6.44 | 9.92 | SMD TYPE 2 | Q50 | 1.3102 | 0.31 | DA 16 & STR 15 | 12.56 | 7.57 | 12.24 | 18" | 0.0676 | 13.57 | 63.48 | 64 | 0.08 | 12.64 | STR 17 | 1202.70 | 1198.42 | 1210.70 | 11.90 |
| 17 | US 81 | 172+00.00 | 67.08' | RT. | 17 | 0.37 | 0.86 | 10.00 | 4.54 | 7.31 | 3.99 | 6.90 | 1.98 | 2.93 | INLET LB TYPE I DES. 2 | Q10 | 0.3081 | 1.62 | DA 17 & STR 16 | 12.64 | 5.89 | 11.33 | 18" | 0.0565 | 13.84 | 107.87 | 108 | 0.13 | 12.77 | DITCH | 1198.42 | 1192.33 | 1213.44 | 9.71 |
| 18 | US 81 | 176+00.00 | 67.08' | LT. | 18 | 0.37 | 0.86 | 10.00 | 4.54 | 7.31 | 2.46 | 4.08 | 1.94 | 2.83 | INLET LB TYPE I DES. 2 | Q10 | 0.3008 | 0.00 | DA 18 | 10.00 | 6.44 | 1.94 | 18" | 0.1197 | 11.15 | 132.72 | 134 | 0.20 | 10.20 | DITCH | 1214.87 | 1199.10 | 1224.87 | 9.59 |
| 19 | US 81 | 176+00.00 | 67.08' | RT. | 19 | 0.37 | 0.86 | 10.00 | 4.54 | 7.31 | 3.88 | 6.62 | 1.94 | 2.85 | INLET LB TYPE I DES. 2 | Q10 | 0.3019 | 0.00 | DA 19 | 10.00 | 6.44 | 1.94 | 18" | 0.0940 | 10.12 | 116.55 | 118 | 0.19 | 10.19 | DITCH | 1214.85 | 1203.94 | 1224.87 | 9.61 |
| 20 | US 81 | 180+00.00 | 67.08' | LT. | 20 | 0.37 | 0.86 | 10.00 | 4.54 | 7.31 | 2.28 | 3.66 | 1.87 | 2.70 | INLET LB TYPE I DES. 2 | Q10 | 0.2905 | 0.00 | DA 20 | 10.00 | 6.44 | 1.87 | 18" | 0.1130 | 10.81 | 131.50 | 132 | 0.20 | 10.20 | DITCH | 1226.30 | 1211.53 | 1236.30 | 9.42 |
| 21 | US 81 | 180+00.00 | 67.08' | RT. | 21 | 0.37 | 0.86 | 10.00 | 4.54 | 7.31 | 3.72 | 6.22 | 1.89 | 2.72 | INLET LB TYPE I DES. 2 | Q10 | 0.2929 | 1.31 | DA 21 | 10.00 | 6.44 | 10.32 | 18" | 0.0855 | 9.72 | 130.69 | 132 | 0.22 | 10.22 | DITCH | 1226.28 | 1215.15 | 1236.28 | 9.46 |
| 22 | US 81 | 184+00.00 | 67.08' | LT. | 22 | 0.36 | 0.86 | 10.00 | 4.54 | 7.31 | 1.99 | 3.04 | 1.76 | 2.51 | INLET LB TYPE I DES. 2 | Q10 | 0.2736 | 0.00 | DA 22 | 10.00 | 6.44 | 1.76 | 18" | 0.0074 | 3.97 | 73.12 | 74 | 0.31 | 10.31 | DITCH | 1242.72 | 1242.18 | 1247.72 | 9.13 |
| 23 | US 81 | 184+00.00 | 67.08' | RT. | 23 | 0.36 | 0.86 | 10.00 | 4.54 | 7.31 | 3.46 | 5.64 | 1.79 | 2.54 | INLET LB TYPE I DES. 2 | Q10 | 0.2778 | 0.00 | DA 23 | 10.00 | 6.44 | 1.79 | 18" | 0.0595 | 8.46 | 79.66 | 80 | 0.16 | 10.16 | DITCH | 1242.70 | 1237.97 | 1247.70 | 9.20 |
| 24 | US 81 | 188+00.00 | 67.08' | LT. | 24 | 0.46 | 0.86 | 10.00 | 4.54 | 7.31 | 4.01 | 6.49 | 1.62 | 2.25 | INLET LB TYPE I DES. 2 | Q10 | 0.2514 | 0.00 | DA 24 | 10.00 | 6.44 | 1.62 | 18" | 0.0871 | 9.41 | 85.31 | 86 | 0.15 | 10.15 | DITCH | 1253.15 | 1245.75 | 1259.15 | |
| 25 | US 81 | 188+00.00 | 0.00' | ε | 25 | 0.81 | 0.53 | 10.00 | 4.54 | 7.31 | 2.76 | 4.21 | 2.76 | 4.21 | SMD TYPE 2 | Q50 | 0.5152 | 0.00 | DA 25 | 10.00 | 8.18 | 4.21 | 18" | 0.0360 | 9.32 | 63.38 | 64 | 0.11 | 10.11 | STR 26 | 1251.41 | 1249.13 | 1256.41 | 10.36 |
| 26 | US 81 | 188+00.00 | 67.08' | RT. | 26 | 0.47 | 0.86 | 10.00 | 4.54 | 7.31 | 3.12 | 4.90 | 1.66 | 2.30 | INLET LB TYPE I DES. 2 | Q10 | 0.2575 | 0.52 | DA 26 & STR 25 | 10.11 | 6.41 | 4.95 | 18" | 0.0662 | 11.54 | 93.86 | 94 | 0.14 | 10.25 | DITCH | 1249.13 | 1242.93 | 1259.13 | 8.85 |
| 27 | US 81 | 193+00.00 | 67.08' | LT. | 27 | 0.29 | 0.86 | 10.00 | 4.54 | 7.31 | 1.61 | 2.45 | 1.13 | 1.59 | INLET LB TYPE I DES. 2 | Q10 | 0.1761 | 0.00 | DA 27 | 10.00 | 6.44 | 1.13 | 18" | 0.0495 | 8.42 | 113.16 | 114 | 0.22 | 10.22 | DITCH | 1255.20 | 1249.61 | 1270.20 | 8.67 |
| 28 | US 81 | 193+00.00 | 67.08' | RT. | 28 | 0.31 | 0.86 | 10.00 | 4.54 | 7.31 | 1.72 | 2.62 | 1.20 | 1.68 | INLET LB TYPE I DES. 2 | Q10 | 0.1859 | 0.00 | DA 28 | 10.00 | 6.44 | 1.20 | 18" | 0.0916 | 8.90 | 140.52 | 142 | 0.26 | 10.26 | DITCH | 1255.18 | 1242.36 | 1270.18 | 8.89 |
| 29 | US 81 | 195+18.00 | 177.62' | LT. | 29 | 2.52 | 0.50 | 16.75 | 3.58 | 6.05 | 6.56 | 10.24 | 6.56 | 10.24 | SMD TYPE 2 | Q50 | 1.5120 | 0.34 | DA 29 | 16.75 | 6.77 | 12.52 | 24" | 0.0106 | 7.57 | 410.43 | 412 | 0.90 | 17.65 | DITCH | 1238.18 | 1233.82 | 1241.28 | |
| 30 | US 81 | 200+00.00 | 67.08' | LT. | 30 | 0.39 | 0.86 | 10.00 | 4.54 | 7.31 | 2.16 | 3.29 | 1.31 | 1.81 | INLET LB TYPE I DES. 2 | Q10 | 0.2037 | 0.00 | DA 30 | 10.00 | 6.44 | 1.31 | 18" | 0.0774 | 8.45 | 129.55 | 130 | 0.25 | 10.25 | STR 31 | 1263.97 | 1253.97 | 1269.00 | 8.37 |
| 31 | US 81 | 200+00.00 | 67.08' | RT. | 31 | 0.35 | 0.86 | 10.00 | 4.54 | 7.31 | 1.94 | 2.95 | 1.21 | 1.66 | INLET LB TYPE I DES. 2 | Q10 | 0.1876 | 0.20 | DA 31 & STR 30 | 10.25 | 6.38 | 2.50 | 18" | 0.1655 | 13.59 | 196.50 | 198 | 0.24 | 10.49 | DITCH | 1253.97 | 1221.89 | 1268.97 | 8.04 |
| 32 | US 81 | 204+75.00 | 67.08' | LT. | 32 | 0.45 | 0.50 | 10.00 | 4.54 | 7.31 | 2.30 | 3.70 | 1.74 | 2.42 | INLET LB TYPE I DES. 2 | Q10 | 0.2696 | 0.00 | DA 32 | 10.00 | 6.44 | 1.74 | 18" | 0.0963 | 10.00 | 127.65 | 128 | 0.21 | 10.21 | DITCH | 1242.22 | 1229.99 | 1257.22 | 9.00 |
| 33 | US 81 | 204+75.00 | 0.00' | ε | 33 | 0.76 | 0.95 | 10.00 | 4.54 | 7.31 | 4.65 | 7.09 | 4.65 | 7.09 | SMD TYPE 2 | Q50 | 0.8664 | 0.00 | DA 33 | 10.00 | 8.18 | 7.09 | 18" | 0.0674 | 12.11 | 63.62 | 64 | 0.09 | 10.09 | STR 34 | 1246.48 | 1242.20 | 1254.48 | |
| 34 | US 81 | 204+75.00 | 67.08' | RT. | 34 | 0.43 | 0.86 | 10.00 | 4.54 | 7.31 | 3.11 | 4.92 | 1.65 | 2.30 | INLET LB TYPE I DES. 2 | Q10 | 0.2559 | 0.87 | DA 34 & STR 33 | 10.09 | 6.42 | 7.20 | 18" | 0.1073 | 16.46 | 245.01 | 246 | 0.25 | 10.33 | DITCH | 1242.20 | 1216.06 | 1257.20 | 8.76 |
| 35 | US 81 | 208+75.00 | 67.08' | LT. | 35 | 0.36 | 0.86 | 10.00 | 4.54 | 7.31 | 2.55 | 4.32 | 1.83 | 2.62 | INLET LB TYPE I DES. 2 | Q10 | 0.2846 | 0.00 | DA 35 | 10.00 | 6.44 | 1.83 | 18" | 0.0100 | 4.48 | 74.30 | 74 | 0.28 | 10.28 | DITCH | 1236.58 | 1235.84 | 1245.22 | 9.26 |
| 36 | US 81 | 208+75.00 | 0.00' | ε | 36 | 0.51 | 0.53 | 10.00 | 4.54 | 7.31 | 1.74 | 2.65 | 1.74 | 2.65 | SMD TYPE 2 | Q50 | 0.3244 | 0.00 | DA 36 | 10.00 | 8.18 | 2.65 | 18" | 0.0164 | 5.94 | 63.35 | 64 | 0.18 | 10.18 | STR 37 | 1237.48 | 1236.44 | 1242.48 | |
| 37 | US 81 | 208+75.00 | 67.08' | RT. | 37 | 0.36 | 0.86 | 10.00 | 4.54 | 7.31 | 3.46 | 5.66 | 1.78 | 2.54 | INLET LB TYPE I DES. 2 | Q10 | 0.2766 | 0.32 | DA 37 & STR 36 | 10.18 | 6.40 | 3.84 | 18" | 0.1022 | 12.66 | 95.02 | 96 | 0.12 | 10.30 | DITCH | 1236.44 | 1226.78 | 1245.20 | 9.12 |
| 38 | US 81 | 213+00.00 | 67.08' | LT. | 38 | 0.38 | 0.86 | 10.00 | 4.54 | 7.31 | 2.10 | 3.21 | 1.93 | 2.80 | INLET LB TYPE I DES. 2 | Q10 | 0.3000 | 0.30 | DA 38 & STR 39 | 10.26 | 6.38 | 3.79 | 18" | 0.1319 | 13.94 | 175.44 | 176 | 0.21 | 10.47 | DITCH | 1217.47 | 1194.53 | 1232.47 | 9.51 |
| 39 | US 81 | 213+00.00 | 67.08' | RT. | 39 | 0.38 | 0.86 | 10.00 | 4.54 | 7.31 | 3.78 | 6.34 | 1.90 | 2.75 | INLET LB TYPE I DES. 2 | Q10 | 0.2950 | 0.00 | DA 39 | 10.00 | 6.44 | 1.90 | 18" | 0.0500 | 8.17 | 129.32 | 130 | 0.26 | 10.26 | STR 38 | 1223.93 | 1217.47 | 1232.45 | 9.43 |
| 40 | US 81 | 216+72.15 | 0.00' | ε | 40 | 398.20 | 0.50 | 61.77 | 1.59 | 3.01 | 493.77 | 805.16 | 493.77 | 805.16 | RCB | Q50 | 238.9200 | 58.64 | DA 40 & STR 38, 70, 71 | 52.20 | 0.00 | 1196.59 | 2-7"x8" | 0.0049 | 14.98 | 531.74 | 532 | 0.59 | 52.79 | CREEK | 1173.63 | 1171.00 | | |
| 41 | US 81 | 217+35.00 | 67.08' | LT. | 41 | 0.39 | 0.86 | 10.00 | 4.54 | 7.31 | 3.84 | 6.42 | 2.01 | 2.95 | INLET LB TYPE I DES. 2 | Q10 | 0.3127 | 0.00 | DA 41 | 10.00 | 6.44 | 2.01 | 18" | 0.0900 | 10.11 | 63.85 | 64 | 0.10 | 10.10 | STR 42 | 1214.40 | 1208.68 | 1219.42 | 9.72 |
| 42 | US 81 | 217+35.00 | 0.00' | ε | 42 | 1.11 | 0.53 | 10.00 | 4.54 | 7.31 | 3.79 | 5.77 | 3.79 | 5.77 | SMD TYPE 2 | Q50 | 0.7060 | 0.31 | DA 42 & STR 41 | 10.10 | 8.15 | 8.30 | 18" | 0.1459 | 16.42 | 64.26 | 64 | 0.06 | 10.17 | STR 43 | 1208.68 | 1199.40 | 1216.68 | |
| 43 | US 81 | 217+35.00 | 67.08' | RT. | 43 | 0.39 | 0.86 | 10.00 | 4.54 | 7.31 | 4 | | | | | | | | | | | | | | | | | | | | | | | |

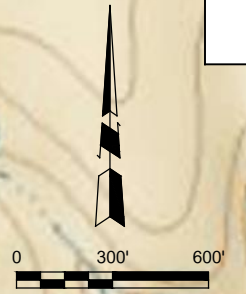
DRAINAGE RECORD

| STR. NO. | ALIGNMENT | LOCATION (STATION) | LOCATION (OFFSET) | LT/RT | D.A. NO. | AREA (AC) | C | T _C (MIN) | I ₀ (IN/HR) | I ₅₀ (IN/HR) | TOTAL Q ₀ (CFS) | TOTAL Q ₅₀ (CFS) | Q ₁₀₀ (CFS) | Q ₁₅₀ (CFS) | INLET TYPE | DESIGN Q (CFS) | ADJ. CA | ADD'N CA | FROM STR. NO. | CUMULATIVE T _C (MIN) | I (IN/HR) | TOTAL Q IN CONDUIT (CFS) | PIPE DIA. (IN) | SLOPE (FT/FT) | VELOCITY (FT/S) | SLOPE LENGTH (FT) | LENGTH (FT) | TIME IN PIPE (MIN) | TOTAL TC (MIN) | TO STR. NO. | FLOWLINE (FT) | D.S. FLOWLINE (FT) | TOP OF RIM/GRATE (FT) | SPREAD (FT) |
|----------|-------------|--------------------|-------------------|-------|----------|-----------|------|----------------------|------------------------|-------------------------|----------------------------|-----------------------------|------------------------|------------------------|------------------------|----------------|---------|----------|----------------------------|---------------------------------|-----------|--------------------------|----------------|---------------|-----------------|-------------------|-------------|--------------------|----------------|-------------|---------------|--------------------|-----------------------|-------------|
| 49 | US 81 | 224+08.72 | 0.00' | ε | 49 | 0.49 | 0.53 | 10.00 | 4.54 | 7.31 | 1.67 | 2.55 | 1.67 | 2.55 | SMD TYPE 2 | Q50 | 0.3116 | 0.00 | DA 49 | 10.00 | 8.18 | 2.55 | 24" | 0.0870 | 11.00 | 148.68 | 150 | 0.22 | 10.22 | STR 51 | 1202.12 | 1189.24 | 1205.22 | |
| 50 | US 81 | 225+60.00 | 149.64' | LT. | 50 | 14.40 | 0.50 | 25.13 | 2.86 | 5.03 | 30.67 | 48.64 | 30.67 | 48.64 | C.E.T. | Q50 | 8.6400 | 0.65 | DA 50 & STR. 44, 46 | 25.13 | 5.63 | 52.30 | 36" | 0.0191 | 12.50 | 147.87 | 148 | 0.20 | 25.33 | STR 51 | 1191.06 | 1188.24 | | |
| 51 | US 81 | 225+60.00 | 0.00' | ε | 51 | 1.02 | 0.53 | 11.48 | 4.29 | 6.99 | 3.30 | 5.07 | 3.30 | 5.07 | SMD TYPE 2B | Q50 | 0.6487 | 9.61 | DA 51 & STR 49, 50 | 25.33 | 5.61 | 57.48 | 36" | 0.0194 | 12.17 | 168.24 | 168 | 0.23 | 25.56 | DITCH | 1188.24 | 1184.98 | 1205.81 | |
| 52 | US 81 | 233+50.00 | 0.00' | ε | 52 | 0.84 | 0.53 | 10.00 | 4.54 | 7.31 | 2.87 | 4.37 | 2.87 | 4.37 | SMD TYPE 2 | Q50 | 0.5342 | 0.00 | DA 52 | 10.00 | 8.18 | 4.37 | 18" | 0.0052 | 4.56 | 109.30 | 110 | 0.40 | 10.40 | DITCH | 1219.63 | 1219.06 | 1222.19 | |
| 65 | Ramp G | 1182+20.00 | 22.74' | RT. | 65 | 0.33 | 0.71 | 10.00 | 4.54 | 7.31 | 1.51 | 2.30 | 1.51 | 2.30 | SMD TYPE 2 | Q50 | 0.2812 | 0.00 | DA 65 | 10.00 | 8.18 | 2.30 | 18" | 0.1180 | 11.56 | 110.51 | 112 | 0.16 | 10.16 | DITCH | 1231.26 | 1218.31 | 1241.42 | |
| 66 | Ramp G | 1185+50.00 | 67.95' | RT. | 66 | 4.01 | 0.58 | 12.07 | 4.19 | 6.87 | 13.95 | 21.43 | 13.95 | 21.43 | SMD TYPE 2A | Q50 | 2.7910 | 0.53 | DA 66 & STR 22, 24 | 12.07 | 7.68 | 25.47 | 30" | 0.0422 | 13.07 | 145.83 | 146 | 0.19 | 12.26 | DITCH | 1234.67 | 1228.52 | 1239.67 | |
| 67 | Ramp G | 1199+02.00 | 0.00' | ε | 67 | 4.04 | 0.60 | 12.84 | 4.08 | 6.72 | 14.18 | 21.85 | 14.18 | 21.85 | RCP | Q50 | 2.9088 | 0.17 | DA 67 | 12.84 | 7.51 | 23.15 | 30" | 0.0070 | 7.26 | 97.31 | 98 | 0.22 | 13.06 | STR 87 | 1242.80 | 1242.12 | | |
| 68 | Ramp H | 200+74.43 | 0.00' | ε | 68 | 3.37 | 0.60 | 18.51 | 3.40 | 5.80 | 10.05 | 15.75 | 10.05 | 15.75 | RCP | Q50 | 2.4264 | 0.18 | DA 68 & STR 27 | 18.51 | 6.49 | 16.89 | 30" | 0.0059 | 6.56 | 182.25 | 182 | 0.46 | 18.97 | DITCH | 1247.91 | 1246.84 | | |
| 69 | US 81 | 199+00.00 | 0.00' | ε | 69 | 5.79 | 0.54 | 14.28 | 3.88 | 6.45 | 17.48 | 27.09 | 17.48 | 27.09 | RCB | Q50 | 3.7519 | 0.34 | DA 69 | 14.28 | 7.22 | 29.52 | 3'x3' | 0.0218 | 10.53 | 464.24 | 465 | 0.73 | 15.01 | DITCH | 1233.84 | 1223.72 | | |
| 70 | Ramp J | 2191+00.00 | 68.16' | RT. | 70 | 4.61 | 0.50 | 20.97 | 3.17 | 5.49 | 10.79 | 16.98 | 10.79 | 16.98 | SMD TYPE 2A | Q50 | 2.7660 | 0.42 | DA 70 | 20.97 | 6.14 | 19.52 | 30" | 0.0448 | 13.42 | 169.30 | 170 | 0.21 | 21.18 | DITCH | 1224.37 | 1216.79 | 1236.37 | |
| 71 | Ramp J | 2203+80.00 | 0.00' | ε | 71 | 6.12 | 0.50 | 19.84 | 3.27 | 5.63 | 14.72 | 23.10 | 14.72 | 23.10 | RCP | Q50 | 3.6720 | 0.55 | DA 71 & STR 32, 35 | 19.84 | 6.29 | 26.59 | 30" | 0.0079 | 8.13 | 155.07 | 156 | 0.32 | 20.16 | DITCH | 1226.69 | 1225.46 | | |
| 72 | Ramp K | 3181+10.00 | 26.00' | LT. | 72 | 0.23 | 0.72 | 10.00 | 4.54 | 7.31 | 1.07 | 1.63 | 1.07 | 1.63 | SMD TYPE 2 | Q50 | 0.1987 | 0.00 | DA 72 | 10.00 | 8.18 | 1.63 | 18" | 0.1038 | 10.05 | 104.32 | 104 | 0.17 | 10.17 | DITCH | 1228.63 | 1217.86 | 1238.63 | |
| 73 | Ramp K | 3185+38.00 | 0.00' | ε | 73 | 4.71 | 0.50 | 16.19 | 3.64 | 6.14 | 12.46 | 19.41 | 12.46 | 19.41 | RCP | Q50 | 2.8260 | 1.05 | DA 73 & STR 23, 26 | 16.19 | 6.87 | 26.62 | 30" | 0.0120 | 9.59 | 115.97 | 116 | 0.20 | 16.39 | DITCH | 1235.09 | 1233.70 | | |
| 74 | Ramp K | 3199+00.00 | 49.00' | LT. | 74 | 2.52 | 0.50 | 14.51 | 3.85 | 6.42 | 7.01 | 10.84 | 7.01 | 10.84 | SMD TYPE 2A | Q50 | 1.5120 | 2.18 | DA 74 & STR 109 | 15.36 | 7.02 | 25.92 | 30" | 0.0057 | 7.07 | 103.52 | 104 | 0.24 | 15.60 | DITCH | 1227.09 | 1226.50 | 1233.93 | |
| 75 | Ramp K | 3201+75.00 | 41.00' | LT. | 75 | 1.15 | 0.64 | 10.13 | 4.52 | 7.28 | 4.72 | 7.19 | 4.72 | 7.19 | SMD TYPE 2 | Q50 | 0.8832 | 0.25 | DA 75 | 10.19 | 8.13 | 9.17 | 18" | 0.0085 | 6.26 | 77.26 | 78 | 0.21 | 10.40 | DITCH | 1225.58 | 1224.92 | 1228.14 | |
| 76 | Ramp M | 304+00.00 | 0.00' | ε | 76 | 5.89 | 0.50 | 5.95 | 5.47 | 8.41 | 22.26 | 33.22 | 22.26 | 33.22 | RCP | Q50 | 3.5340 | 10.72 | DA 76 & STR 31, 69 | 29.70 | 5.17 | 73.67 | 48" | 0.0050 | 8.82 | 256.55 | 258 | 0.48 | 30.19 | DITCH | 1220.42 | 1219.14 | | |
| 77 | Ramp N | 4192+00.00 | 0.00' | ε | 77 | 1.02 | 0.50 | 13.57 | 3.97 | 6.58 | 2.92 | 4.50 | 2.92 | 4.50 | RCP | Q50 | 0.6120 | 0.00 | DA 77 | 13.57 | 7.36 | 4.50 | 22"x13" | 0.0214 | 7.25 | 82.90 | 84 | 0.19 | 13.76 | DITCH | 1228.36 | 1226.59 | | |
| 78 | Ramp N | 4200+00.00 | 0.00' | ε | 78 | 11.83 | 0.58 | 1.75 | 7.03 | 10.03 | 64.02 | 92.38 | 64.02 | 92.38 | RCP | Q50 | 8.2337 | 17.06 | DA 78 & STR 34, 37, 76, 77 | 31.94 | 4.97 | 125.75 | 48" | 0.0133 | 13.65 | 181.45 | 182 | 0.22 | 32.16 | DITCH | 1216.06 | 1213.64 | | |
| 79 | Ramp N | 4206+35.22 | 26.92' | LT. | 79 | 0.30 | 0.71 | 10.00 | 4.54 | 7.31 | 1.37 | 2.09 | 1.37 | 2.09 | SMD TYPE 2 | Q50 | 0.2556 | 0.00 | DA 79 | 10.00 | 8.18 | 2.09 | 18" | 0.1832 | 13.09 | 170.50 | 172 | 0.21 | 10.21 | DITCH | 1223.99 | 1193.27 | 1233.99 | |
| 80 | Ramp B | 244+21.53 | 18.08' | RT. | 80 | 0.09 | 0.87 | 10.00 | 4.54 | 7.31 | 0.50 | 0.77 | 0.42 | 0.61 | INLET LB TYPE I DES. 2 | Q10 | 0.0649 | 0.00 | DA 80 | 10.00 | 6.44 | 0.42 | 18" | 0.0032 | 2.36 | 66.64 | 68 | 0.47 | 10.47 | DITCH | 1182.95 | 1182.74 | 1190.95 | |
| 87 | I-44 H.E. B | 268+26.00 | 0.00' | ε | 87 | 15.33 | 0.50 | 25.04 | 2.87 | 5.04 | 32.73 | 51.88 | 32.73 | 51.88 | C.E.T. | Q50 | 9.1980 | 3.80 | DA 61 & STR 67 | 25.04 | 5.64 | 73.30 | 36" | 0.0597 | 16.85 | 138.01 | 138 | 0.14 | 25.18 | STR 157 | 1232.89 | 1224.67 | | |
| 102 | I-44 H.E. B | 291+91.00 | 127.16' | RT. | 102 | 5.78 | 0.50 | 23.16 | 3.00 | 5.24 | 12.83 | 20.32 | 12.83 | 20.32 | RCP | Q50 | 3.4680 | 2.03 | DA 62 & STR 28, 29 | 23.16 | 5.86 | 32.22 | 30" | 0.0354 | 13.36 | 286.36 | 286 | 0.36 | 23.52 | DITCH | 1233.94 | 1223.82 | | |
| 109 | I-44 H.E. B | 300+36.55 | 106.00' | RT. | 109 | 2.75 | 0.59 | 15.11 | 3.77 | 6.31 | 8.86 | 13.75 | 8.86 | 13.75 | SMD TYPE 2A | Q50 | 1.9470 | 0.24 | DA 74 | 15.11 | 7.06 | 15.41 | 30" | 0.0031 | 5.85 | 87.22 | 88 | 0.25 | 15.36 | STR 74 | 1227.36 | 1227.09 | 1231.01 | |
| 122 | I-44 H.E. B | 261+25.00 | 0.00' | ε | 122 | 64.63 | 0.50 | 36.38 | 2.28 | 4.14 | 111.49 | 179.54 | 111.49 | 179.54 | RCB | Q50 | 38.7780 | 0.18 | DA 122 | 36.38 | 4.63 | 180.36 | 6'x7' | 0.0369 | 11.74 | 102.52 | 103 | 0.15 | 36.53 | CREEK | 1209.28 | 1205.50 | | |
| 123 | I-44 H.E. B | 327+99.00 | 0.00' | ε | 123 | 8.40 | 0.50 | 18.18 | 3.43 | 5.85 | 21.04 | 32.96 | 21.04 | 32.96 | RCB | Q50 | 5.0400 | 1.37 | DA 123 | 18.18 | 6.54 | 41.94 | 4'x2.5' | 0.0357 | 8.50 | 58.04 | 58 | 0.11 | 18.29 | DITCH | 1200.17 | 1198.10 | | |
| 124 | US 81 | 240+00.00 | 0.00' | ε | 124 | 1.22 | 0.53 | 10.00 | 4.54 | 7.31 | 4.16 | 6.35 | 4.16 | 6.35 | SMD TYPE 2 | Q50 | 0.7759 | 0.00 | DA 124 | 10.00 | 8.18 | 6.35 | 18" | 0.0857 | 13.77 | 168.39 | 170 | 0.20 | 10.20 | DITCH | 1225.21 | 1210.83 | 1237.77 | |
| 125 | US 81 | 242+50.00 | 0.00' | ε | 125 | 63.61 | 0.50 | 35.70 | 2.31 | 4.18 | 111.00 | 178.62 | 111.00 | 178.62 | RCP | Q50 | 38.1660 | 0.00 | DA 125 | 35.70 | 4.68 | 178.55 | 60" | 0.0147 | 15.77 | 449.31 | 450 | 0.47 | 36.17 | CREEK | 1199.77 | 1193.16 | | |
| 126 | US 81 | 265+00.00 | 0.00' | ε | 126 | 1.47 | 0.53 | 10.09 | 4.53 | 7.29 | 5.00 | 7.63 | 5.00 | 7.63 | SMD TYPE 2 | Q50 | 0.9349 | 0.00 | DA 126 | 10.09 | 8.16 | 7.62 | 18" | 0.0080 | 5.94 | 108.20 | 108 | 0.30 | 10.39 | DITCH | 1216.84 | 1215.97 | 1219.40 | |
| 127 | US 81 | 275+00.00 | 102.00' | RT. | 127 | 7.60 | 0.58 | 10.81 | 4.40 | 7.13 | 27.59 | 42.16 | 27.59 | 42.16 | GPI TYPE 2 DES. 10 | Q50 | 5.2896 | 0.00 | DA 173 | 10.81 | 7.97 | 42.18 | 36" | 0.0060 | 7.20 | 213.81 | 214 | 0.49 | 11.30 | DITCH | 1181.95 | 1180.66 | 1185.77 | |
| 128 | County Rd. | 303+00.00 | 0.00' | ε | 128 | 0.96 | 0.57 | 10.00 | 4.41 | 7.08 | 3.25 | 5.01 | 3.25 | 5.01 | RCP | Q10 | 0.5472 | 0.00 | DA 128 | 10.00 | 5.94 | 3.25 | 22"x13" | 0.0132 | 5.25 | 34.90 | 36 | 0.11 | 10.11 | DITCH | 1160.92 | 1160.46 | | |
| 129 | County Rd. | 309+65.00 | 0.00' | ε | 129 | 0.97 | 0.58 | 10.00 | 4.41 | 7.08 | 3.34 | 5.15 | 3.34 | 5.15 | RCP | Q10 | 0.5626 | 0.00 | DA 129 | 10.00 | 5.94 | 3.34 | 22"x13" | 0.0059 | 4.31 | 47.67 | 48 | 0.18 | 10.18 | DITCH | 1164.70 | 1164.42 | | |
| 130 | County Rd. | 311+98.00 | 24.90' | RT. | 130 | 0.20 | 0.53 | 10.00 | 4.41 | 7.08 | 0.63 | 0.97 | 0.63 | 0.97 | RCP | Q10 | 0.1060 | 0.00 | DA 130 | 10.00 | 5.94 | 0.63 | 22"x13" | 0.0194 | 3.79 | 44.23 | 44 | 0.19 | 10.19 | DITCH | 1169.83 | 1168.97 | | |
| 131 | County Rd. | 317+87.00 | 0.00' | ε | 131 | 1.50 | 0.57 | 13.13 | 3.89 | 6.35 | 4.51 | 7.03 | 4.51 | 7.03 | RCP | Q10 | 0.8550 | 0.00 | DA 131 | 13.13 | 5.28 | 4.52 | 22"x13" | 0.0059 | 4.72 | 47.70 | 48 | 0.17 | 13.30 | DITCH | 1164.23 | 1163.95 | | |
| 132 | County Rd. | 327+23.00 | 0.00' | ε | 132 | 2.55 | 0.50 | 28.46 | 2.54 | 4.34 | 4.53 | 7.19 | 4.53 | 7.19 | RCP | Q10 | 1.2750 | 0.00 | DA 132 | 28.46 | 3.55 | 4.52 | 22"x13" | 0.0031 | 3.57 | 35.50 | 36 | 0.17 | 28.63 | DITCH | 1174.10 | 1173.99 | | |
| 133 | US 81 | 321+00.00 | 0.82' | LT. | 133 | 1.38 | 0.65 | 10.00 | 4.41 | 7.08 | 5.33 | 8.21 | 5.33 | 8.21 | SMD TYPE 2 | Q50 | 1.0764 | 0.00 | DA 133 | 10.00 | 7.63 | 8.21 | 18" | 0.0100 | 6.58 | 119.07 | 120 | 0.30 | 10.30 | DITCH | 1135.96 | 1134.77 | 1138.52 | |
| 134 | US 81 | 322+00.00 | 0.50' | LT. | 134 | 2.24 | 0.62 | 10.35 | 4.34 | 6.99 | 8.14 | 12.55 | 8.14 | 12.55 | SMD TYPE 2 | Q50 | 1.6666 | 0.00 | DA 134 | 10.35 | 7.53 | 12.55 | 24" | 0.0063 | 6.19 | 117.63 | 118 | 0.32 | 10.67 | STR 231 | 1135.21 | 1134.47 | 1138.28 | |
| 135 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

DRAINAGE RECORD

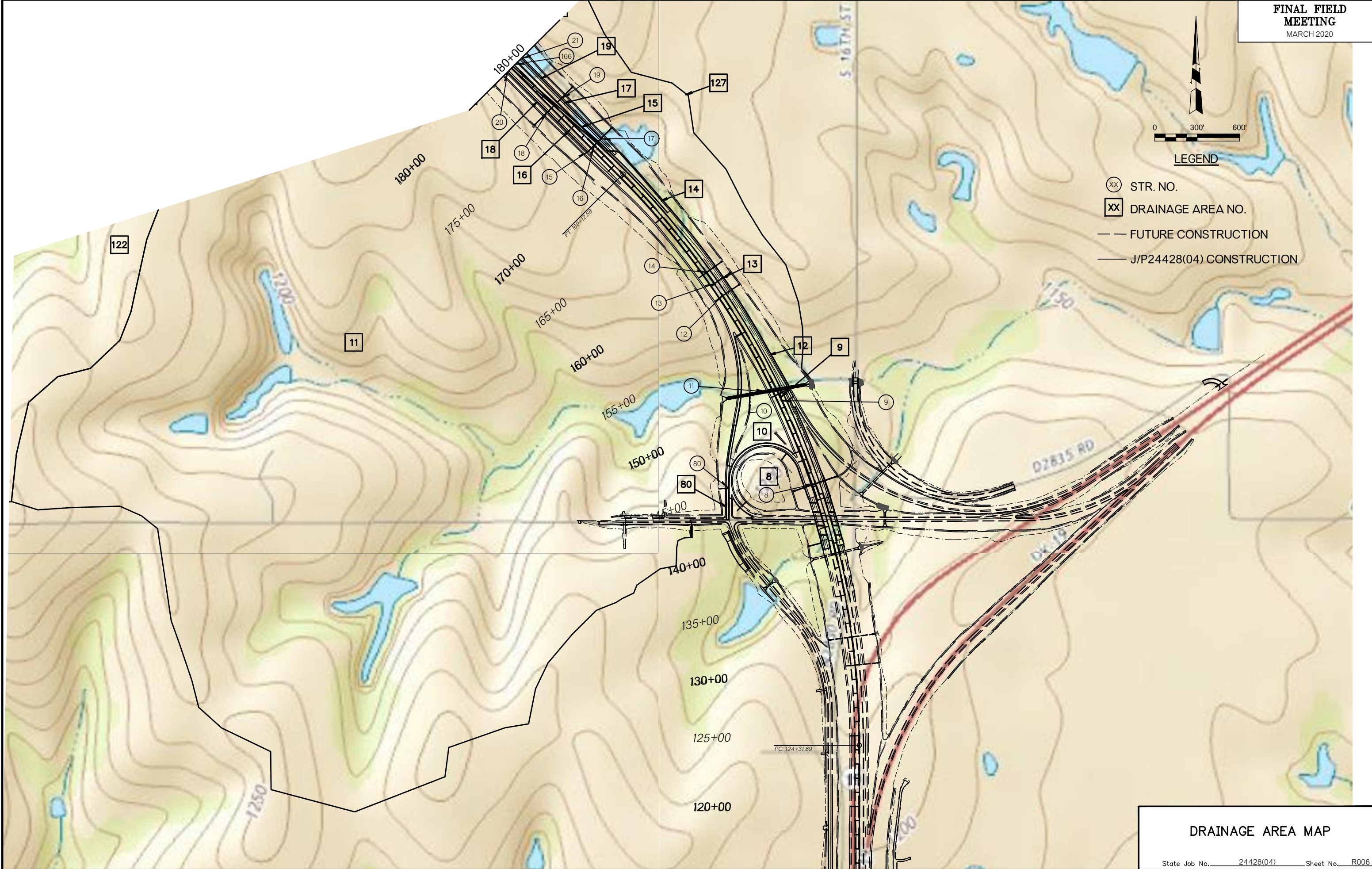
Table with columns: STR. NO., ALIGNMENT, LOCATION (STATION), LOCATION (OFFSET), LT/RT, D.A. NO., AREA (AC), C, Tc (MIN), I0 (IN/HR), I50 (IN/HR), TOTAL Q0 (CFS), TOTAL Q50 (CFS), QIN0 (CFS), QIN50 (CFS), INLET TYPE, DESIGN Q (CFS), ADJ. CA, ADD'N CA, FROM STR. NO., CUMULATIVE Tc (MIN), I (IN/HR), TOTAL Q IN CONDUIT (CFS), PIPE DIA. (IN), SLOPE (FT/FT), VELOCITY (FT/S), SLOPE LENGTH (FT), LENGTH (FT), TIME IN PIPE (MIN), TOTAL TC (MIN), TO STR. NO., FLOWLINE (FT), D.S. FLOWLINE (FT), TOP OF RIM/GRATE (FT), SPREAD (FT). Rows 194-231.

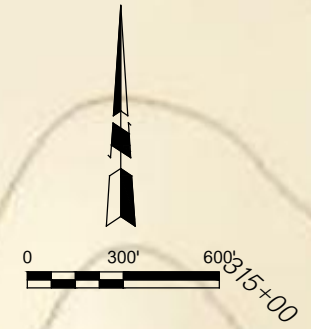
DRAINAGE RECORD



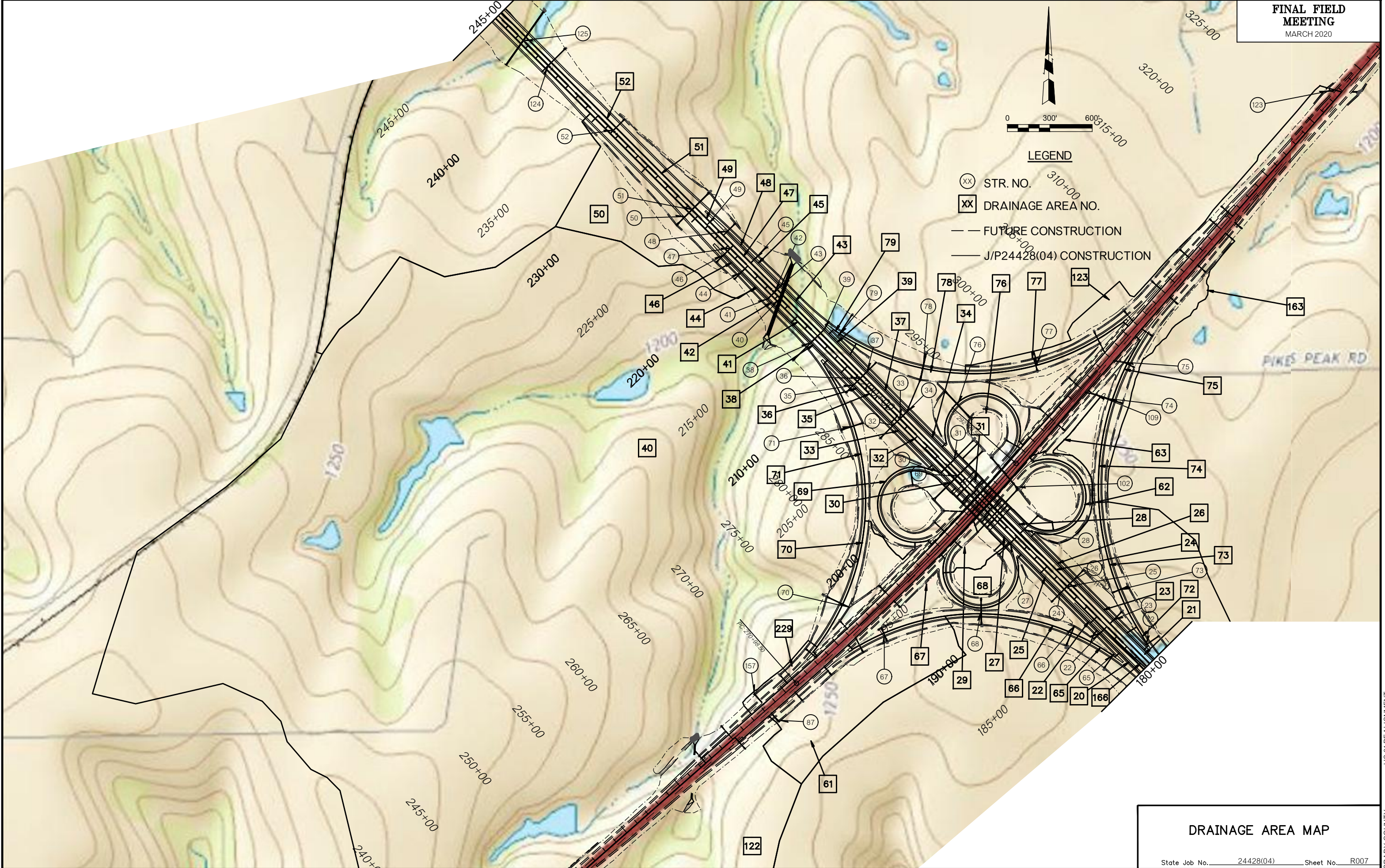
LEGEND

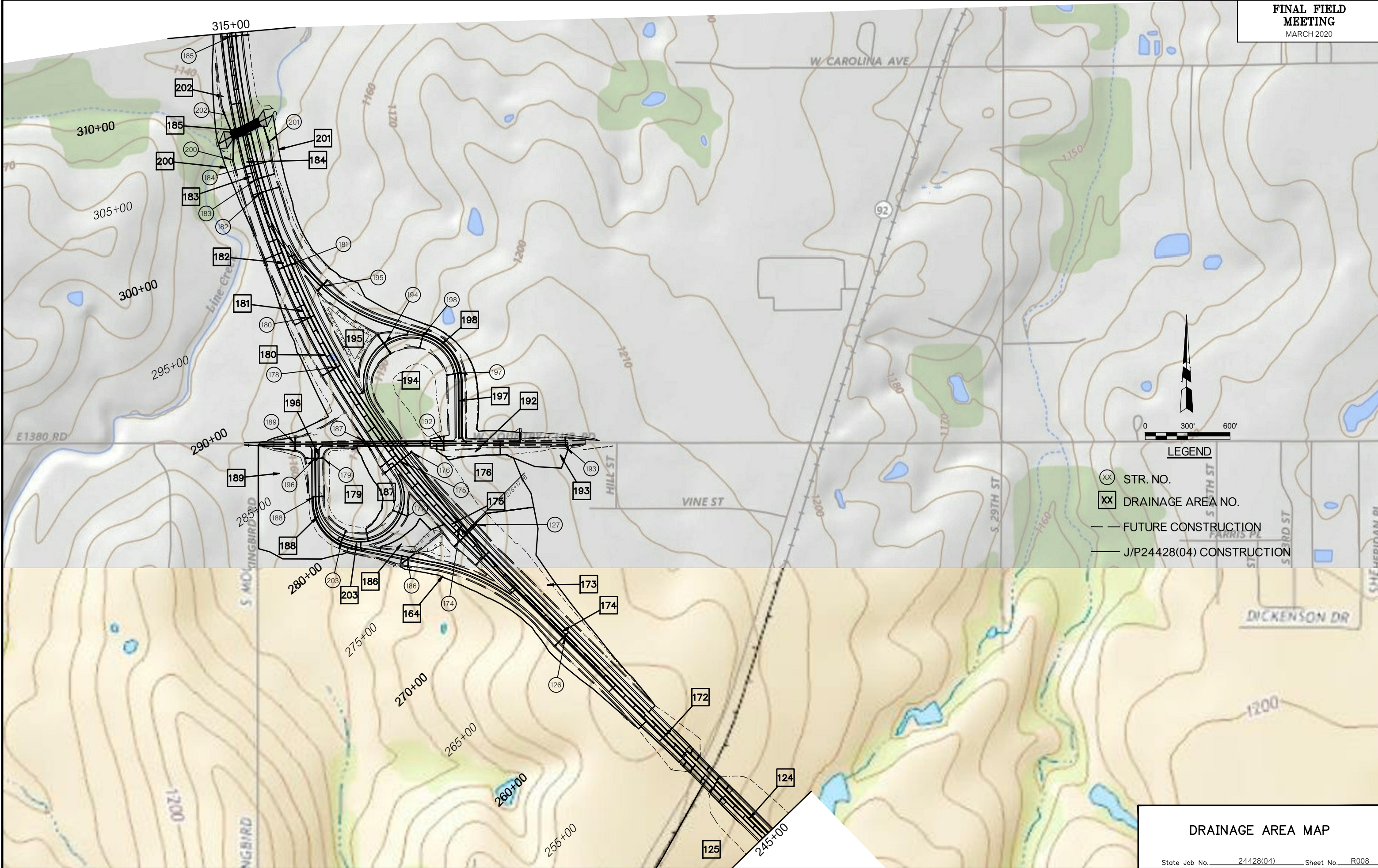
- ⊗ STR. NO.
- ⊠ DRAINAGE AREA NO.
- - - FUTURE CONSTRUCTION
- J/P24428(04) CONSTRUCTION



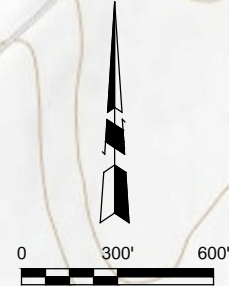
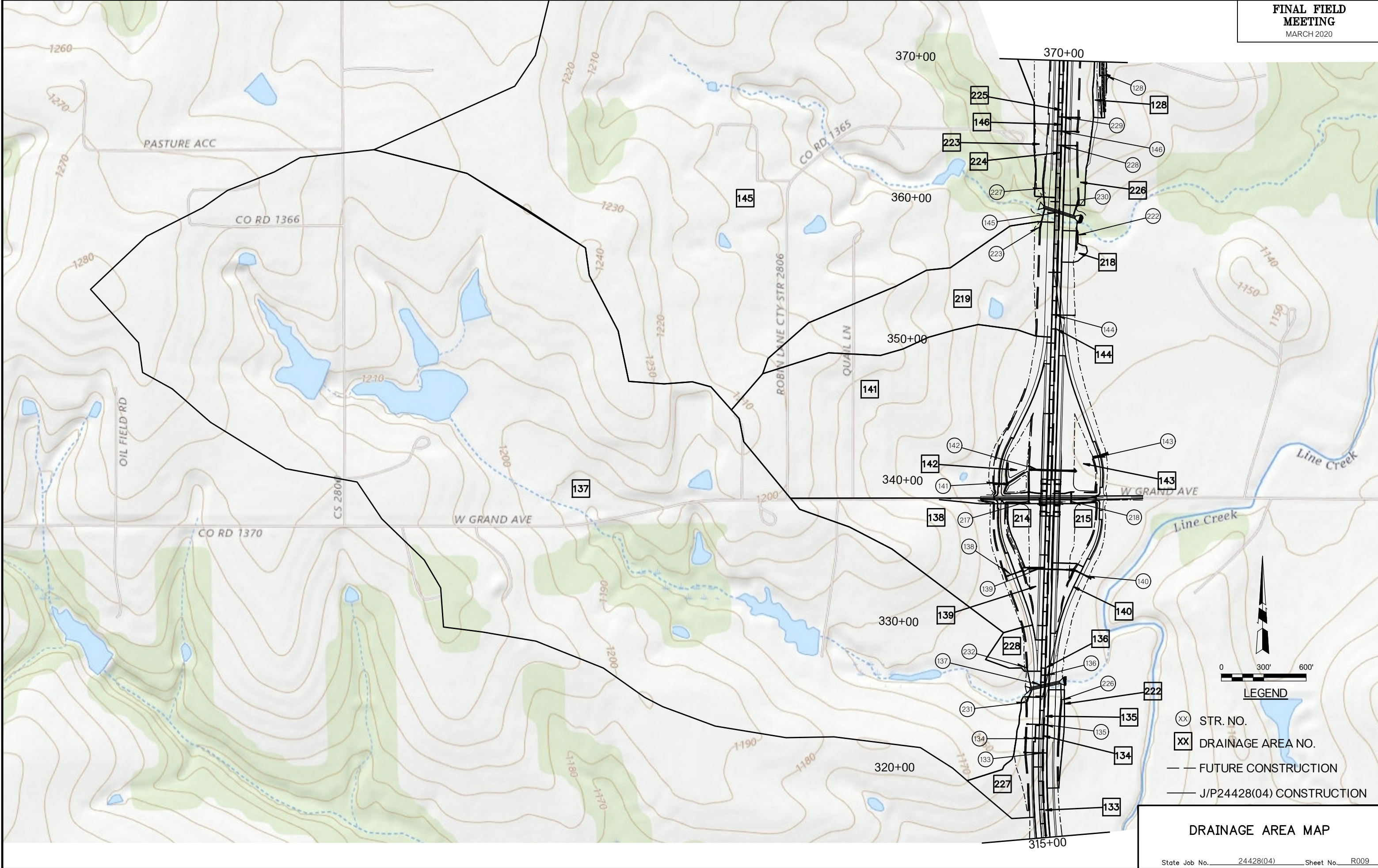


- LEGEND**
- XX STR. NO.
 - XX DRAINAGE AREA NO.
 - FUTURE CONSTRUCTION
 - J/P24428(04) CONSTRUCTION





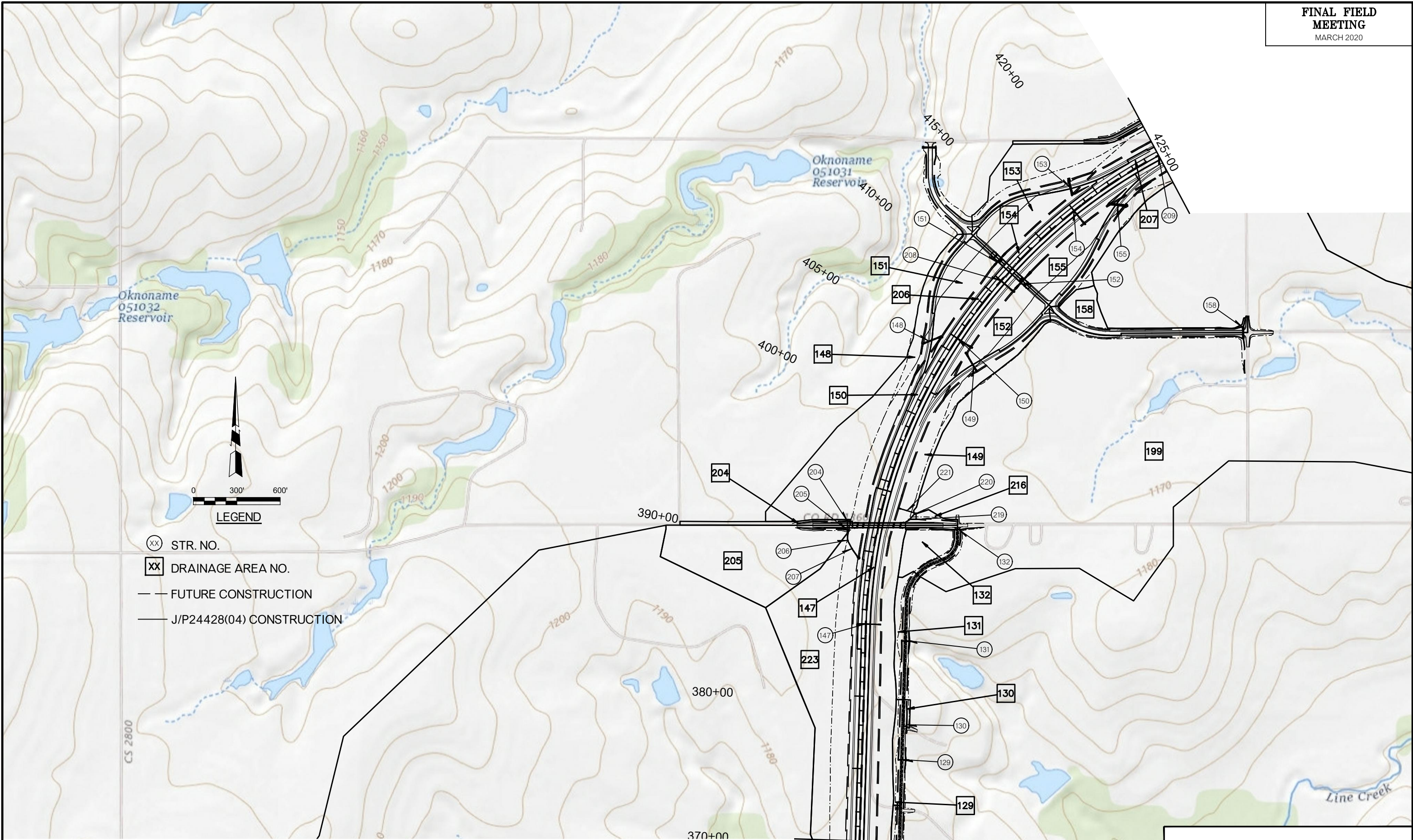
- LEGEND
- ⊗ STR. NO.
 - ⊠ DRAINAGE AREA NO.
 - - - FUTURE CONSTRUCTION
 - J/P24428(04) CONSTRUCTION



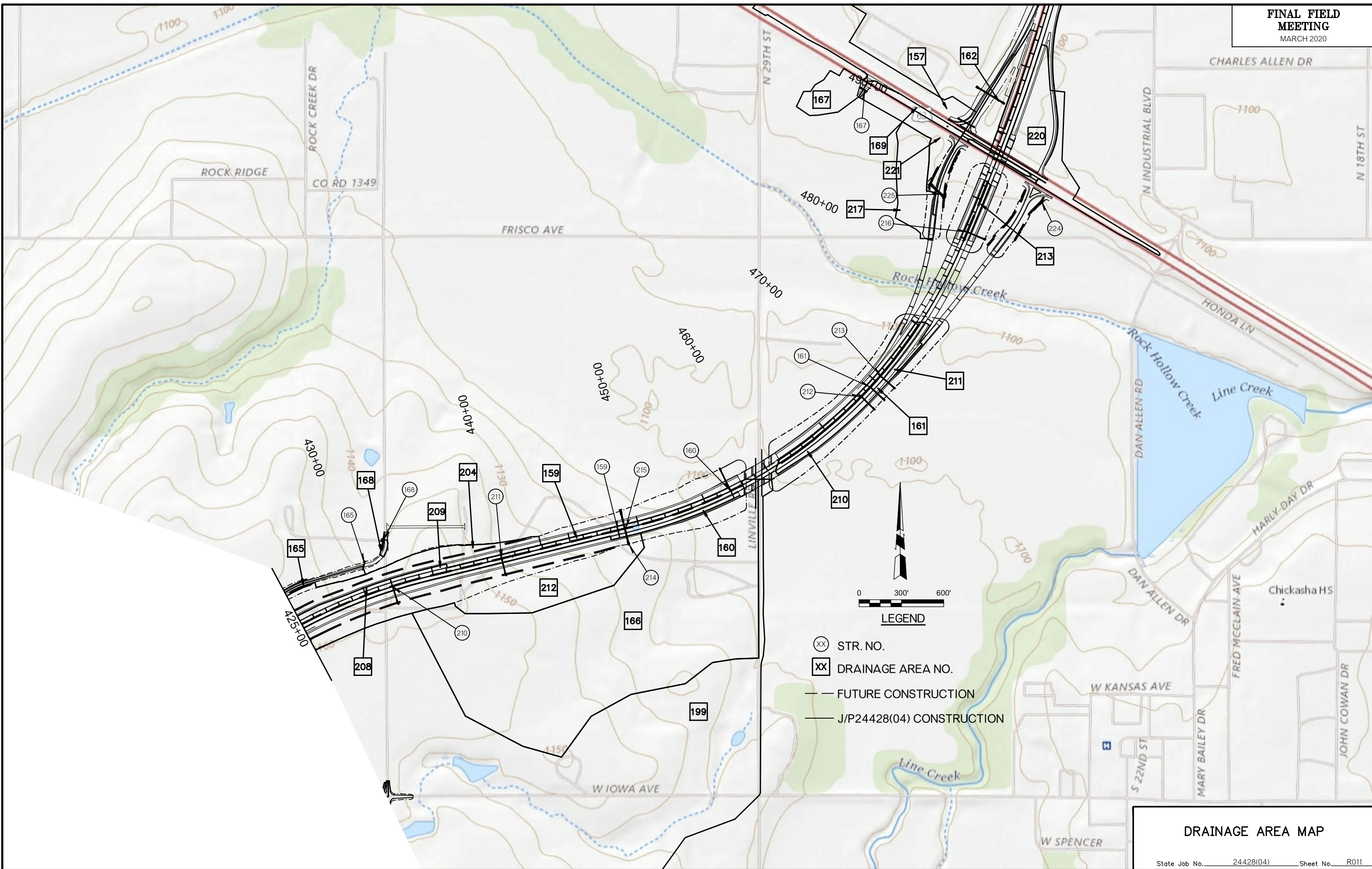
- LEGEND**
- ⓧ STR. NO.
 - ⓧ DRAINAGE AREA NO.
 - FUTURE CONSTRUCTION
 - J/P24428(04) CONSTRUCTION

DRAINAGE AREA MAP

GRADY COUNTY US 81 REALIGNMENT



- LEGEND**
- ⊗ STR. NO.
 - ⊠ DRAINAGE AREA NO.
 - FUTURE CONSTRUCTION
 - J/P24428(04) CONSTRUCTION



DRAINAGE AREA MAP

SUMMARY OF DISTURBED DRAINAGE AREAS

| ALIGNMENT | DISTURBED AREA NO. | OUTFLOW LOCATION (STATION) | DISTURBED AREA (STATION TO STATION) | DIRECTION TO OUTFALL | DESCRIPTION OF AREA | EROSION CONTROL MEASURES | DISTURBED AREA (AC.) |
|------------------|--------------------|----------------------------|-------------------------------------|----------------------|--|--|----------------------|
| US 81 | D1 | STA. 149+50 | STA. 141+22 TO STA. 149+50 | N | CROWN OF ROAD LT. TO RT. | FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.76 |
| RAMP A | D2 | STA. 143+00 | STA. 141+22 TO STA. 153+50 | E | CROWN OF ROAD RT. TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 4.03 |
| US 81 | D3 | STA. 149+50 | STA. 141+08 TO STA. 150+08 | E | EDGE OF SUPERELEVATED PAVEMENT TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT FENCE, SILT DIKE & SOLID SLAB SODDING | 4.00 |
| US 81 | D4 | STA. 149+70 | STA. 145+69 TO STA. 155+06 | N | CROWN OF ROAD LT. TO RT. | (2) SEDIMENT TRAP, FIBER LOG, SEDIMENT FILTER, SILT DIKE & SOLID SLAB SODDING | 4.60 |
| RAMP B | D5 | STA. 250+50 | STA. 243+00 TO STA. 250+83 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SEDIMENT FILTER, SILT DIKE & SOLID SLAB SODDING | 2.17 |
| US 81 | D6 | STA. 149+50 | STA. 151+50 TO STA. 180+00 | S | CROWN OF ROAD TO TOE OF SLOPE LT. | (3) SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 10.89 |
| US 81 | D7 | STA. 149+50 | STA. 159+03 TO STA. 172+00 | E | CROWN OF ROAD LT. TO RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 3.13 |
| US 81 | D8 | STA. 149+50 | STA. 149+50 TO STA. 159+03 | E | CROWN OF ROAD LT. TO RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 2.38 |
| RAMP D | D9 | STA. 249+50 | STA. 250+00 TO STA. 260+25 | SE | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.28 |
| US 81 | D10 | STA. 149+50 | STA. 160+18 TO STA. 180+00 | S | CROWN OF ROAD TO TOE OF SLOPE RT. | (2) SEDIMENT TRAP, FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 5.28 |
| US 81 | D11 | STA. 149+50 | STA. 184+00 TO STA. 191+20 | S | CROWN OF ROAD TO TOE OF SLOPE | FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 1.68 |
| RAMP K | D12 | STA. 3185+38 | STA. 3185+38 TO STA. 3191+72 | S | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 0.82 |
| US 81 | D13 | STA. 149+50 | STA. 176+24 TO STA. 184+30 | S | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 3.63 |
| RAMP L | D14 | STA. 212+57 | STA. 214+72 TO STA. 223+25 | W | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT FENCE, SILT DIKE & SOLID SLAB SODDING | 1.38 |
| US 81 | D15 | STA. 149+50 | STA. 172+96 TO STA. 188+00 | SE | CROWN OF ROAD TO TOE OF SLOPE RT. | (2) SEDIMENT TRAP, FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 5.98 |
| US 81 | D16 | STA. 185+39 | STA. 182+49 TO STA. 194+71 | E | CROWN OF ROAD RT. TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT FENCE, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 3.52 |
| RAMP G | D17 | STA. 1180+73 | STA. 1182+50 TO STA. 1194+00 | SE | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 3.48 |
| US 81 | D18 | TOE OF SLOPE | STA. 185+96 TO STA. 193+02 | S | CROWN OF ROAD TO TOE OF SLOPE | FIBER LOG, SILT FENCE, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 1.89 |
| RAMP H | D19 | STA. 200+74 | STA. 194+88 TO STA. 204+50 | S | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 2.24 |
| RAMP L | D20 | STA. 212+57 | STA. 223+25 TO STA. 226+77 | N | CROWN OF ROAD LT. TO TOE OF SLOPE RT. | FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.27 |
| US 81 | D21 | STA. 204+75 | STA. 198+33 TO STA. 204+75 | N | CROWN OF ROAD LT. TO RT. | FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 1.21 |
| US 81 | D22 | TOE OF SLOPE | STA. 215+08 TO STA. 220+00 | W | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 1.41 |
| RAMP G | D23 | STA. 260+04 | STA. 1194+00 TO STA. 1201+48 | W | CROWN OF ROAD RT. TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.57 |
| US 81 | D24 | STA. 282+50 | STA. 273+00 TO STA. 282+50 | N | CROWN OF ROAD LT. TO CROWN OF ROAD RT. | FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 1.59 |
| RAMP G | D25 | STA. 1199+02 | STA. 1192+00 TO STA. 1201+50 | N | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 0.90 |
| RAMP J | D26 | STA. 2191+00 | STA. 2187+07 TO STA. 2198+25 | W | CROWN OF ROAD RT. TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 0.68 |
| RAMP J | D27 | STA. 2191+00 | STA. 2187+50 TO STA. 2198+38 | W | CROWN OF ROAD RT TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 2.66 |
| RAMP L | D28 | STA. 215+41 | STA. 214+72 TO STA. 224+39 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.94 |
| RAMP K | D29 | STA. 3199+00 | STA. 3193+73 TO STA. 3199+00 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 0.90 |
| RAMP M | D30 | STA. 4200+00 | STA. 310+50 TO STA. 306+60 | W | CROWN OF ROAD LT TO TOE OF SLOPE LT. | SILT DIKE, FIBER LOG & SOLID SLAB SODDING | 0.58 |
| COUNTRY CLUB RD. | D31 | STA. 113+00 | STA. 111+57 TO STA. 117+00 | W | CROWN OF ROAD TO TOE OF SLOPE RT. | SILT DIKE & SOLID SLAB SODDING | 0.97 |
| RAMP K | D32 | STA. 3213+00 | STA. 3191+72 TO STA. 3198+80 | NE | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.45 |
| RAMP N | D33 | STA. 4192+00 | STA. 4191+90 TO STA. 4197+00 | E | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.02 |
| RAMP K | D34 | STA. 149+70 | STA. 3185+38 TO STA. 3191+72 | E | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.04 |
| OMITTED | D35 | | | | | | |
| RAMP M | D36 | STA. 304+00 | STA. 298+41 TO STA. 310+50 | N | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.53 |
| OMITTED | D37 | | | | | | |
| US 81 | D38 | STA. 200+00 | STA. 198+34 TO STA. 200+00 | N | CROWN OF ROAD TO LONGITUDINAL BARRIER | FIBER LOG, SEDIMENT FILTER & SOLID SLAB SODDING | 0.31 |
| US 81 | D39 | STA. 4200+00 | STA. 201+23 TO STA. 206+13 | N | CROWN OF ROAD TO CROWN OF ROAD | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 4.02 |
| US 81 | D40 | STA. 206+40 | STA. 197+46 TO STA. 211+30 | N & S | CROWN OF ROAD TO TOE OF SLOPE | (2) SEDIMENT TRAPS, FIBER LOG, SILT FENCE, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 5.68 |

NOTES:

1) PLACE SOLID SLAB SOD IMMEDIATELY UPON COMPLETION OF CONSTRUCTION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY OF DISTURBED DRAINAGE AREAS

SUMMARY OF DISTURBED DRAINAGE AREAS

| ALIGNMENT | DISTURBED AREA NO. | OUTFLOW LOCATION (STATION) | DISTURBED AREA (STATION TO STATION) | DIRECTION TO OUTFALL | DESCRIPTION OF AREA | EROSION CONTROL MEASURES | DISTURBED AREA (AC.) |
|------------------|--------------------|----------------------------|-------------------------------------|----------------------|---------------------------------------|---|----------------------|
| US 81 | D41 | STA. 4200+00 | STA. 204+75 TO STA. 210+70 | N | CROWN OF ROAD TO CROWN OF ROAD | SEDIMENT TRAP, FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 2.99 |
| US 81 | D42 | STA. 215+54 | STA. 202+30 TO STA. 215+54 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT FENCE, SILT DIKE & SOLID SLAB SODDING | 4.60 |
| RAMP N | D43 | TOE OF SLOPE | STA. 4197+00 TO STA. 4209+84 | N | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT FENCE, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 4.19 |
| US 81 | D44 | STA. 217+85 | STA. 208+75 TO STA. 217+35 | N | CROWN OF ROAD LT. TO RT. | FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 0.74 |
| US 81 | D45 | STA. 217+85 | STA. 215+50 TO STA. 222+25 | N | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT FENCE, SILT DIKE & SOLID SLAB SODDING | 2.79 |
| US 81 | D46 | STA. 225+60 | STA. 217+35 TO STA. 225+60 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 2.73 |
| US 81 | D47 | STA. 225+60 | STA. 220+00 TO STA. 235+00 | S | CROWN OF ROAD RT. TO TOE OF SLOPE RT. | (2) SEDIMENT TRAP, FIBER LOG, SILT DIKE, SILT FENCE, SEDIMENT FILTER & SOLID SLAB SODDING | 5.42 |
| US 81 | D48 | STA. 225+60 | STA. 225+60 TO STA. 236+00 | S | CROWN OF ROAD LT. TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.42 |
| US 81 | D49 | SA. 233+50 | STA. 233+50 TO STA. 240+00 | S | CROWN OF ROAD LT. TO RT. | FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 0.84 |
| US 81 | D50 | TOE OF SLOPE | STA. 235+00 TO STA. 248+90 | E | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT FENCE, SEDIMENT FILTER & SOLID SLAB SODDING | 3.41 |
| US 81 | D51 | STA. 242+14 | STA. 236+00 TO STA. 250+00 | S | CROWN OF ROAD LT. TO TOE OF SLOPE LT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 4.54 |
| US 81 | D52 | TOE OF SLOPE | STA. 242+55 TO STA. 250+14 | E | CROWN OF ROAD RT. TO TOE OF SLOPE RT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 3.26 |
| US 81 | D53 | TOE OF SLOPE | STA. 252+05 TO STA. 257+00 | E | CROWN OF ROAD RT. TO TOE OF SLOPE RT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 1.15 |
| US 81 | D54 | TOE OF SLOPE | STA. 252+05 TO STA. 257+01 | W | CROWN OF ROAD LT. TO TOE OF SLOPE LT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 1.12 |
| US 81 | D55 | STA. 275+00 | STA. 257+00 TO STA. 275+00 | N | CROWN OF ROAD TO TOE OF SLOPE RT. | (2) SEDIMENT TRAP, FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 5.47 |
| US 81 | D56 | STA. 276+64 | STA. 257+00 TO STA. 264+77 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.98 |
| US 81 | D57 | STA. 265+00 | STA. 252+90 TO STA. 265+00 | N | CROWN OF ROAD LT. TO RT. | FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 1.50 |
| US 81 | D58 | STA. 276+64 | STA. 265+00 TO STA. 279+31 | N | CROWN OF ROAD TO TOE OF SLOPE | SEDIMENT TRAP, FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 4.31 |
| RAMP O | D59 | STA. 1022+78 | STA. 1009+75 TO STA. 1024+50 | NW | CROWN OF ROAD TO TOE OF SLOPE LT. | (2) SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.37 |
| US 81 | D60 | STA. 278+75 | STA. 277+00 TO STA. 281+30 | W | CROWN OF ROAD LT. TO TOE OF SLOPE LT. | FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 1.72 |
| US 81 | D61 | STA. 282+00 | STA. 275+00 TO STA. 283+30 | N | CROWN OF ROAD RT. TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 3.52 |
| US 81 | D62 | STA. 283+85 | STA. 279+19 TO STA. 284+60 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 2.50 |
| RAMP O | D63 | TOE OF SLOPE | STA. 1024+50 TO STA. 1031+50 | W | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT FENCE, SILT DIKE & SOLID SLAB SODDING | 0.65 |
| RAMP P | D64 | STA. 1051+00 | STA. 1050+00 TO STA. 1059+00 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 2.51 |
| COUNTRY CLUB RD. | D65 | STA. 102+50 | STA. 100+16 TO STA. 104+43 | N | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.08 |
| COUNTRY CLUB RD. | D66 | STA. 124+00 | STA. 117+00 TO STA. 124+00 | E | CROWN OF ROAD TO TOE OF SLOPE | SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 1.36 |
| COUNTRY CLUB RD. | D67 | STA. 123+00 | STA. 121+78 TO STA. 123+00 | E | CROWN OF ROAD TO TOE OF SLOPE RT. | SILT DIKE & SOLID SLAB SODDING | 0.09 |
| RAMP R | D68 | STA. 308+41 | STA. 1150+00 TO STA. 1167+00 | N | CROWN OF ROAD TO TOE OF SLOPE RT. | (2) SEDIMENT TRAP, FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 4.64 |
| RAMP Q | D69 | STA. 1109+37 | STA. 1109+37 TO STA. 1120+65 | N | CROWN OF ROAD TO TOE OF SLOPE RT. | (2) SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 4.32 |
| US 81 | D70 | STA. 288+85 | STA. 281+80 TO STA. 289+80 | N | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.76 |
| US 81 | D71 | STA. 290+00 | STA. 282+50 TO STA. 290+00 | N | CROWN OF ROAD LT. TO RT. | FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 1.71 |
| US 81 | D72 | STA. 307+85 | STA. 284+05 TO STA. 294+00 | N | CROWN OF ROAD LT. TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 4.08 |
| US 81 | D73 | STA. 295+50 | STA. 287+62 TO STA. 298+54 | N | CROWN OF ROAD TO TOE OF SLOPE | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 4.39 |
| US 81 | D74 | STA. 307+85 | STA. 290+00 TO STA. 306+63 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 4.51 |
| US 81 | D75 | STA. 308+41 | STA. 295+65 TO STA. 306+95 | N | CROWN OF ROAD RT. TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.17 |
| US 81 | D76 | STA. 304+50 | STA. 299+00 TO STA. 307+71 | N | CROWN OF ROAD LT. TO RT. | FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 1.93 |
| US 81 | D77 | STA. 308+41 | STA. 306+95 TO STA. 315+00 | E | CROWN OF ROAD RT. TO TOE OF SLOPE RT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 2.07 |
| US 81 | D78 | STA. 314+75 | STA. 307+71 TO STA. 314+75 | N | CROWN OF ROAD LT. TO RT. | FIBER LOG, SILT DIKE, SEDIMENT FILTER & SOLID SLAB SODDING | 1.56 |
| US 81 | D79 | STA. 307+85 | STA. 306+63 TO STA. 315+00 | N | CROWN OF ROAD LT. TO TOE OF SLOPE LT. | FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 1.70 |

NOTES:

1) PLACE SOLID SLAB SOD IMMEDIATELY UPON COMPLETION OF CONSTRUCTION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY OF DISTURBED DRAINAGE AREAS

SUMMARY OF DISTURBED DRAINAGE AREAS

| ALIGNMENT | DISTURBED AREA NO. | OUTFLOW LOCATION (STATION) | DISTURBED AREA (STATION TO STATION) | DIRECTION TO OUTFALL | DESCRIPTION OF AREA | EROSION CONTROL MEASURES | DISTURBED AREA (AC.) |
|-------------|--------------------|----------------------------|-------------------------------------|----------------------|-----------------------------------|---|----------------------|
| US 81 | D80 | STA. 325+75 | STA. 315+00 TO STA. 325+50 | N | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 2.33 |
| US 81 | D81 | STA. 325+75 | STA. 315+00 TO STA. 326+50 | N & S | CROWN OF ROAD LT. TO RT. | SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 2.18 |
| US 81 | D82 | STA. 325+75 | STA. 315+00 TO STA. 324+93 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 2.26 |
| US 81 | D83 | STA. 325+75 | STA. 324+93 TO STA. 326+73 | W | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 0.45 |
| RAMP S | D84 | STA. 1212+00 | STA. 1212+74 TO STA. 1216+00 | S | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 0.82 |
| US 81 | D85 | STA. 325+75 | STA. 326+50 TO STA. 337+79 | S | CROWN OF ROAD LT. TO RT. | SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.39 |
| RAMP U | D86 | TOE OF SLOPE | STA. 1302+49 TO STA. 1313+01 | E | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 2.01 |
| RAMP S | D87 | STA. 1217+50 | STA. 1216+00 TO STA. 1225+02 | E | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.69 |
| RAMP S | D88 | STA. 1217+50 | STA. 1214+95 TO STA. 1225+02 | E | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 3.39 |
| US 81 | D89 | STA. 333+25 | STA. 329+48 TO STA. 338+00 | E | CROWN OF US 81 TO CROWN OF RAMP U | SEDIMENT TRAP, FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 2.52 |
| RAMP U | D90 | TOE OF SLOPE | STA. 1313+01 TO STA. 1316+29 | E | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 0.29 |
| RAMP U | D91 | STA. 1316+66 | STA. 1311+48 TO STA. 1316+29 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 0.43 |
| US 81 | D92 | TOE OF SLOPE | STA. 337+79 TO STA. 338+71 | N | BRIDGE GRADING LT. TO RT. | SILT FENCE & SOLID SLAB SODDING | 0.38 |
| RAMP T | D93 | STA. 1256+50 | STA. 1250+20 TO STA. 1262+00 | E | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 2.26 |
| US 81 | D94 | STA. 342+30 | STA. 339+41 TO STA. 348+39 | E | CROWN OF US 81 TO CROWN OF RAMP T | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.40 |
| US 81 | D95 | STA. 358+75 | STA. 340+32 TO STA. 352+00 | E | CROWN OF ROAD LT. TO RT. | SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.46 |
| US 81 | D96 | STA. 342+30 | STA. 339+42 TO STA. 349+12 | E | CROWN OF US 81 TO CROWN OF RAMP V | FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 3.16 |
| RAMP V | D97 | STA. 1353+00 | STA. 1350+40 TO STA. 1352+88 | E | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 0.43 |
| RAMP V | D98 | TOE OF SLOPE | STA. 1350+38 TO STA. 1367+94 | E | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 3.21 |
| RAMP T | D99 | STA. 1270+30 | STA. 1262+00 TO STA. 1270+12 | E | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 2.41 |
| US 81 | D100 | STA. 358+75 | STA. 352+00 TO STA. 365+00 | E | CROWN OF ROAD LT. TO RT. | SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.68 |
| US 81 | D101 | STA. 358+75 | STA. 356+62 TO STA. 358+00 | E | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT FILTER, FIBER LOG, & SOLID SLAB SODDING | 0.36 |
| US 81 | D102 | STA. 358+75 | STA. 358+00 TO STA. 359+80 | E | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 0.45 |
| US 81 | D103 | STA. 358+75 | STA. 358+55 TO STA. 360+29 | E | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 0.52 |
| US 81 | D104 | STA. 358+75 | STA. 359+80 TO STA. 365+00 | E | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.91 |
| US 81 | D105 | STA. 358+75 | STA. 360+29 TO STA. 373+00 | E | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 4.76 |
| US 81 | D106 | STA. 358+75 | STA. 365+00 TO STA. 385+00 | S | CROWN OF ROAD LT. TO RT. | SEDIMENT TRAP, SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 2.68 |
| US 81 | D107 | STA. 358+75 | STA. 365+00 TO STA. 376+00 | E | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 4.82 |
| COUNTY ROAD | D108 | STA. 303+00 | STA. 300+00 TO STA. 306+75 | E | CROWN OF ROAD TO TOE OF SLOPE LT. | SILT DIKE & SOLID SLAB SODDING | 0.54 |
| COUNTY ROAD | D109 | STA. 303+00 | STA. 300+00 TO STA. 306+28 | E | CROWN OF ROAD TO TOE OF SLOPE RT. | SILT DIKE, SILT FENCE, & SOLID SLAB SODDING | 0.37 |
| US 81 | D110 | STA. 358+75 | STA. 373+00 TO STA. 334+00 | S | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 4.70 |
| COUNTY ROAD | D111 | STA. 309+65 | STA. 306+75 TO STA. 313+82 | N & S | CROWN OF ROAD TO TOE OF SLOPE LT. | SILT DIKE & SOLID SLAB SODDING | 0.57 |
| COUNTY ROAD | D112 | STA. 309+65 | STA. 306+28 TO STA. 313+82 | N & S | CROWN OF ROAD TO TOE OF SLOPE RT. | SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 0.62 |
| US 81 | D113 | STA. 358+75 | STA. 376+00 TO STA. 385+00 | S | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.79 |
| COUNTY ROAD | D114 | STA. 317+87 | STA. 313+82 TO STA. 322+64 | N & S | CROWN OF ROAD TO TOE OF SLOPE LT. | SILT DIKE & SOLID SLAB SODDING | 0.76 |

NOTES:

1) PLACE SOLID SLAB SOD IMMEDIATELY UPON COMPLETION OF CONSTRUCTION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY OF DISTURBED DRAINAGE AREA

SUMMARY OF DISTURBED DRAINAGE AREAS

| ALIGNMENT | DISTURBED AREA NO. | OUTFLOW LOCATION (STATION) | DISTURBED AREA (STATION TO STATION) | DIRECTION TO OUTFALL | DESCRIPTION OF AREA | EROSION CONTROL MEASURES | DISTURBED AREA (AC.) |
|-------------|--------------------|----------------------------|-------------------------------------|----------------------|---|--|----------------------|
| COUNTY ROAD | D115 | STA. 317+87 | STA. 313+82 TO STA. 319+00 | N & S | CROWN OF ROAD TO TOE OF SLOPE RT. | SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 0.35 |
| US 81 | D116 | STA. 358+75 | STA. 384+00 TO STA. 381+92 | S | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 2.56 |
| US 81 | D117 | STA. 358+75 | STA. 385+00 TO STA. 391+95 | S | CROWN OF ROAD LT. TO RT. | SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.39 |
| US 81 | D118 | STA. 358+75 | STA. 385+00 TO STA. 392+35 | S | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 2.62 |
| COUNTY ROAD | D119 | TOE OF SLOPE | STA. 319+00 TO STA. 322+50 | S & E | CROWN OF ROAD TO TOE OF SLOPE RT. | SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 0.25 |
| COUNTY ROAD | D120 | STA. 327+15 | STA. 322+64 TO STA. 327+15 | N & E | CROWN OF ROAD TO TOE OF SLOPE LT. | SILT DIKE & SOLID SLAB SODDING | 0.33 |
| COUNTY ROAD | D121 | STA. 327+15 | STA. 322+50 TO STA. 327+15 | N | CROWN OF ROAD TO TOE OF SLOPE RT. | SILT DIKE & SOLID SLAB SODDING | 0.41 |
| US 81 | D122 | STA. 442+00 | STA. 391+50 TO STA. 405+00 | NE | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.59 |
| US 81 | D123 | STA. 449+00 | STA. 391+77 TO STA. 406+00 | NE | CROWN OF ROAD LT. TO RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.17 |
| US 81 | D124 | STA. 449+00 | STA. 391+95 TO STA. 405+00 | NE | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 4.73 |
| US 81 | D125 | STA. 449+00 | STA. 405+00 TO STA. 412+00 | NE | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.35 |
| US 81 | D126 | STA. 442+00 | STA. 405+00 TO STA. 412+00 | NE | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.06 |
| US 81 | D127 | STA. 449+00 | STA. 406+00 TO STA. 411+00 | NE | CROWN OF ROAD LT. TO RT. | SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 0.97 |
| US 81 | D128 | STA. 442+00 | STA. 412+00 TO STA. 417+90 | NE | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 2.61 |
| US 81 | D129 | STA. 449+00 | STA. 411+00 TO STA. 418+00 | NE | CROWN OF ROAD LT. TO RT. | SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.35 |
| US 81 | D130 | STA. 449+00 | STA. 412+00 TO STA. 417+40 | NE | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 2.69 |
| US 81 | D131 | STA. 442+00 | STA. 417+90 TO STA. 429+00 | NE | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.01 |
| US 81 | D132 | STA. 449+00 | STA. 418+00 TO STA. 425+00 | NE | CROWN OF ROAD LT. TO RT. | SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.51 |
| US 81 | D133 | STA. 449+00 | STA. 417+40 TO STA. 424+50 | NE | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 2.51 |
| EW 135.5 | D134 | STA. 22+40 | STA. 11+97 TO STA. 22+29 | NE | CROWN OF ROAD TO TOE OF SLOPE LT. | SILT FENCE, SILT DIKE, & SOLID SLAB SODDING | 0.57 |
| COUNTY ROAD | D135 | STA. 22+40 | STA. 11+97 TO STA. 22+10 | NE | CROWN OF ROAD TO TOE OF SLOPE RT. | SILT FENCE, SILT DIKE, & SOLID SLAB SODDING | 0.56 |
| US 81 | D136 | STA. 449+00 | STA. 425+00 TO STA. 432+00 | NE | CROWN OF ROAD LT. TO RT. | SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.43 |
| US 81 | D137 | STA. 449+00 | STA. 424+50 TO STA. 431+35 | NE | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 2.89 |
| EW 135.5 | D138 | STA. 25+40 | STA. 22+29 TO STA. 25+40 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | SILT FENCE & SOLID SLAB SODDING | 0.22 |
| EW 135.5 | D139 | STA. 26+04 | STA. 22+10 TO STA. 26+04 | N | CROWN OF ROAD TO TOE OF SLOPE RT. & DWY WIDTH | SILT FENCE & SOLID SLAB SODDING | 0.27 |
| US 81 | D140 | STA. 442+00 | STA. 429+00 TO STA. 442+00 | NE | CROWN OF ROAD TO TOE OF SLOPE LT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.28 |
| US 81 | D141 | STA. 449+00 | STA. 432+00 TO STA. 440+00 | NE | CROWN OF ROAD LT. TO RT. | FIBER LOG, SEDIMENT FILTER, SILT DIKE & SOLID SLAB SODDING | 1.29 |
| US 81 | D142 | STA. 449+00 | STA. 431+35 TO STA. 439+35 | NE | CROWN OF ROAD TO TOE OF SLOPE RT. | SEDIMENT TRAP, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 3.40 |
| US 81 | D143 | STA. 449+00 | STA. 440+00 TO STA. 448+50 | NE | CROWN OF ROAD LT. TO RT. | SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.13 |
| US 81 | D144 | STA. 449+00 | STA. 439+35 TO STA. 450+00 | N | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT FENCE, SILT DIKE, & SOLID SLAB SODDING | 2.68 |
| US 81 | D145 | TOE OF SLOPE | STA. 442+00 TO STA. 458+84 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 4.27 |
| US 81 | D146 | STA. 457+00 | STA. 448+50 TO STA. 457+44 | N | CROWN OF ROAD LT. TO RT. | SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.73 |
| US 81 | D147 | TOE OF SLOPE | STA. 450+00 TO STA. 458+24 | N | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 1.70 |
| US 81 | D148 | TOE OF SLOPE | STA. 460+12 TO STA. 475+76 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 3.70 |
| US 81 | D149 | STA. 469+43 | STA. 460+45 TO STA. 469+43 | NE | CROWN OF ROAD LT. TO RT. | SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.84 |

NOTES:

1) PLACE SOLID SLAB SOD IMMEDIATELY UPON COMPLETION OF CONSTRUCTION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY OF DISTURBED DRAINAGE AREA

SUMMARY OF DISTURBED DRAINAGE AREAS

| ALIGNMENT | DISTURBED AREA NO. | OUTFLOW LOCATION (STATION) | DISTURBED AREA (STATION TO STATION) | DIRECTION TO OUTFALL | DESCRIPTION OF AREA | EROSION CONTROL MEASURES | DISTURBED AREA (AC.) |
|-----------------------|--------------------|----------------------------|-------------------------------------|----------------------|--|---|----------------------|
| US 81 | D150 | TOE OF SLOPE | STA. 459+54 TO STA. 476+30 | S | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 3.37 |
| US 81 | D151 | STA. 469+43 | STA. 469+43 TO STA. 475+43 | SW | CROWN OF ROAD LT. TO RT. | SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 1.42 |
| US 81 | D152 | TOE OF SLOPE | STA. 480+95 TO STA. 488+34 | S | CROWN OF US 81 TO CROWN OF RAMP AA | FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 2.62 |
| US 81 | D153 | TOE OF SLOPE | STA. 481+21 TO STA. 487+43 | S | CROWN OF US 81 TO CROWN OF RAMP CC | FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 2.44 |
| RAMP CC | D154 | TOE OF SLOPE | STA. 482+32 TO STA. 488+11 | S | CROWN OF ROAD TO TOE OF SLOPE RT. | FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 0.93 |
| US 81 | D155 | STA. 482+50 | STA. 482+50 TO STA. 486+30 | S | CROWN OF ROAD LT. TO RT. | SEDIMENT FILTER, FIBER LOG, SILT DIKE & SOLID SLAB SODDING | 0.52 |
| RAMP AA | D156 | STA. 484+20 | STA. 484+00 TO STA. 488+06 | S | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 0.43 |
| I-44 | D157 | STA. 263+63 | STA. 245+65 TO STA. 258+20 | E | EDGE OF EXISTING RDWY. LT. TO TOE OF SLOPE LT. | SEDIMENT FILTER, SILT DIKE, SILT FENCE & SOLID SLAB SODDING | 1.23 |
| I-44 | D158 | STA. 263+63 | STA. 251+85 TO STA. 275+53 | W & E | EDGE OF EXISTING RDWY. RT. TO TOE OF SLOPE RT. | SEDIMENT TRAP, SEDIMENT FILTER, SILT DIKE, SILT FENCE, FIBER LOG & SOLID SLAB SODDING | 3.85 |
| I-44 | D159 | STA. 263+63 | STA. 258+20 TO STA. 263+43 | N | EDGE OF EXISTING RDWY. LT. TO TOE OF SLOPE LT. | SILT FENCE & SOLID SLAB SODDING | 1.84 |
| I-44 | D160 | TOE OF SLOPE | STA. 263+01 TO STA. 268+27 | N | EDGE OF EXISTING RDWY. LT. TO TOE OF SLOPE LT. | SILT FENCE, FIBER LOG & SOLID SLAB SODDING | 1.03 |
| I-44 | D161 | STA. 267+68 | STA. 268+27 TO STA. 273+80 | N | EDGE OF EXISTING RDWY. LT. TO TOE OF SLOPE LT. | SEDIMENT FILTER, SILT DIKE, SILT FENCE, FIBER LOG & SOLID SLAB SODDING | 1.31 |
| I-44 | D162 | STA. 276+64 | STA. 273+36 TO STA. 284+72 | W | EDGE OF EXISTING RDWY. LT. TO TOE OF SLOPE LT. | SEDIMENT TRAP, SEDIMENT FILTER, SILT DIKE, FIBER LOG & SOLID SLAB SODDING | 2.27 |
| I-44 | D163 | STA. 263+63 | STA. 275+53 TO STA. 284+65 | W | EDGE OF EXISTING RDWY. RT. TO TOE OF SLOPE RT. | SILT DIKE, SILT FENCE, FIBER LOG & SOLID SLAB SODDING | 1.88 |
| I-44 | D164 | STA. 294+74 | STA. 283+25 TO STA. 288+80 | NE | EDGE OF EXISTING RDWY. LT. TO TOE OF SLOPE LT. | (2) SEDIMENT TRAP, SILT DIKE, FIBER LOG & SOLID SLAB SODDING | 4.17 |
| I-44 | D165 | STA. 294+74 | STA. 283+85 TO STA. 288+26 | NE | EDGE OF EXISTING RDWY. RT. TO TOE OF SLOPE RT. | SEDIMENT FILTER, SILT DIKE, SILT FENCE, FIBER LOG & SOLID SLAB SODDING | 1.19 |
| I-44 | D166 | STA. 294+74 | STA. 288+30 TO STA. 294+30 | N | EDGE OF EXISTING RDWY. LT. TO TOE OF SLOPE LT. | SILT DIKE & SOLID SLAB SODDING | 1.86 |
| I-44 | D167 | STA. 294+74 | STA. 288+19 TO STA. 293+93 | N | EDGE OF EXISTING RDWY. RT. TO TOE OF SLOPE RT. | SILT DIKE & SOLID SLAB SODDING | 1.76 |
| I-44 | D168 | STA. 294+74 | STA. 294+30 TO STA. 303+33 | N | EDGE OF EXISTING RDWY. LT. TO TOE OF SLOPE LT. | SEDIMENT TRAP, SILT DIKE & SOLID SLAB SODDING | 2.60 |
| I-44 | D169 | STA. 329+00 | STA. 293+92 TO STA. 300+36 | E | EDGE OF EXISTING RDWY. RT. TO TOE OF SLOPE RT. | SEDIMENT FILTER, SILT DIKE & SOLID SLAB SODDING | 1.59 |
| I-44 | D170 | STA. 294+74 | STA. 296+40 TO STA. 305+51 | NW | EDGE OF EXISTING RDWY. LT. TO TOE OF SLOPE LT. | SILT DIKE, FIBER LOG & SOLID SLAB SODDING | 1.61 |
| I-44 | D171 | STA. 329+00 | STA. 300+36 TO STA. 305+33 | E | EDGE OF EXISTING RDWY. RT. TO TOE OF SLOPE RT. | SEDIMENT FILTER, SILT DIKE & SOLID SLAB SODDING | 1.18 |
| I-44 | D172 | STA. 329+00 | STA. 299+21 TO STA. 329+40 | SE | EDGE OF EXISTING RDWY. LT. TO TOE OF SLOPE LT. | (2) SEDIMENT TRAP, SILT DIKE, SILT FENCE, FIBER LOG & SOLID SLAB SODDING | 5.22 |
| I-44 | D173 | STA. 329+00 | STA. 300+65 TO STA. 333+30 | S & E | EDGE OF EXISTING RDWY. RT. TO TOE OF SLOPE RT. | (2) SEDIMENT TRAP, SEDIMENT FILTER, SILT DIKE, SILT FENCE, FIBER LOG & SOLID SLAB SODDING | 5.22 |
| IOWA | D174 | STA. 60+57 | STA. 53+80 TO STA. 60+89 | S | CROWN OF ROAD LT. TO RT. | SILT DIKE & SOLID SLAB SODDING | 1.35 |
| IOWA | D175 | TOE OF SLOPE | STA. 60+89 TO STA. 62+65 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | SILT FENCE & SOLID SLAB SODDING | 0.11 |
| IOWA | D176 | TOE OF SLOPE | STA. 60+89 TO STA. 62+65 | S | CROWN OF ROAD TO TOE OF SLOPE RT. | SILT FENCE & SOLID SLAB SODDING | 0.12 |
| US 62 | D177 | TOE OF SLOPE | STA. 394+34 TO STA. 395+20 | E | TOE OF DRIVEWAY LT. TO RT. | SILT FENCE & SOLID SLAB SODDING | 0.11 |
| US 62 | D178 | TOE OF SLOPE | STA. 394+05 TO STA. 397+02 | S | EXISTING SHOULDER TO TOE OF WIDENING | SOLID SLAB SODDING | 0.10 |
| RAMP AA | D179 | TOE OF SLOPE | STA. 480+60 TO STA. 484+00 | W | CROWN OF ROAD TO TOE OF SLOPE LT. | FIBER LOG, SILT FENCE & SOLID SLAB SODDING | 0.49 |
| US 81 INTERIM CONN. 2 | D180 | STA. 489+60 | STA. 489+08 TO STA. 493+11 | S | CROWN OF ROAD TO TOE OF SLOPE LT. | ROCK FILTER DAM, SILT DIKE, SILT FENCE, & SOLID SLAB SODDING | 0.63 |
| US 81 INTERIM CONN. 2 | D181 | STA. 489+60 | STA. 489+08 TO STA. 493+11 | S | CROWN OF ROAD TO TOE OF SLOPE RT. | ROCK FILTER DAM, SILT FENCE, & SOLID SLAB SODDING | 0.45 |
| US 81 INTERIM CONN. 2 | D182 | STA. 507+00 | STA. 493+11 TO STA. 506+89 | N | CROWN OF ROAD TO TOE OF SLOPE LT. | ROCK FILTER DAM, SILT FENCE, & SOLID SLAB SODDING | 1.65 |
| US 81 INTERIM CONN. 2 | D183 | STA. 507+00 | STA. 492+75 TO STA. 499+40 | N | CROWN OF ROAD TO TOE OF SLOPE RT. | ROCK FILTER DAM, SILT DIKE, SILT FENCE, & SOLID SLAB SODDING | 0.59 |

NOTES:

1) PLACE SOLID SLAB SOD IMMEDIATELY UPON COMPLETION OF CONSTRUCTION.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUMMARY OF DISTURBED DRAINAGE AREA

SEC. 20 T6N R7W

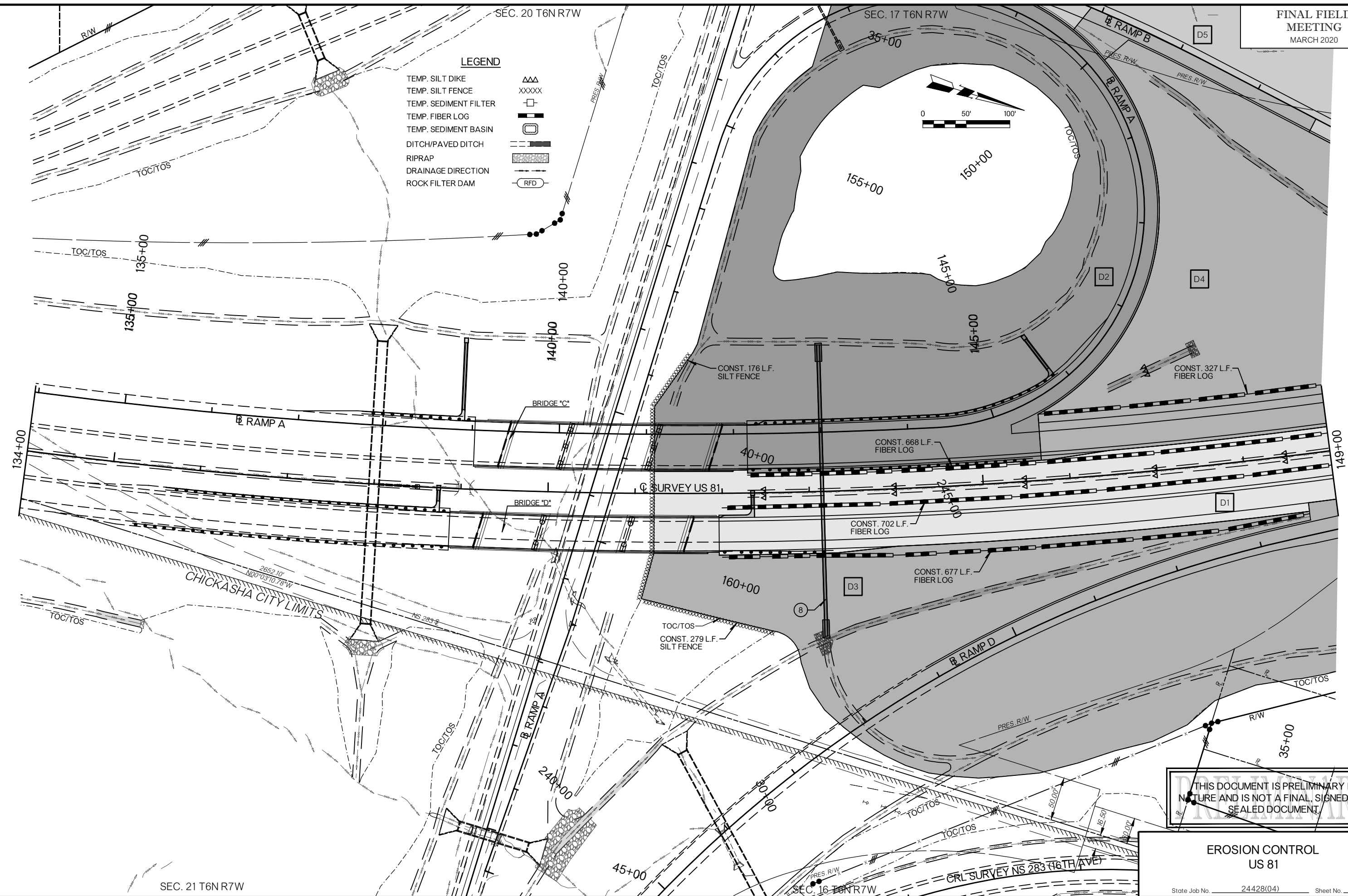
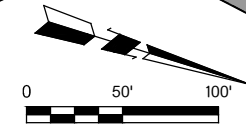
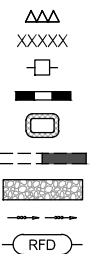
SEC. 17 T6N R7W

SEC. 21 T6N R7W

SEC. 16 T6N R7W

LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM



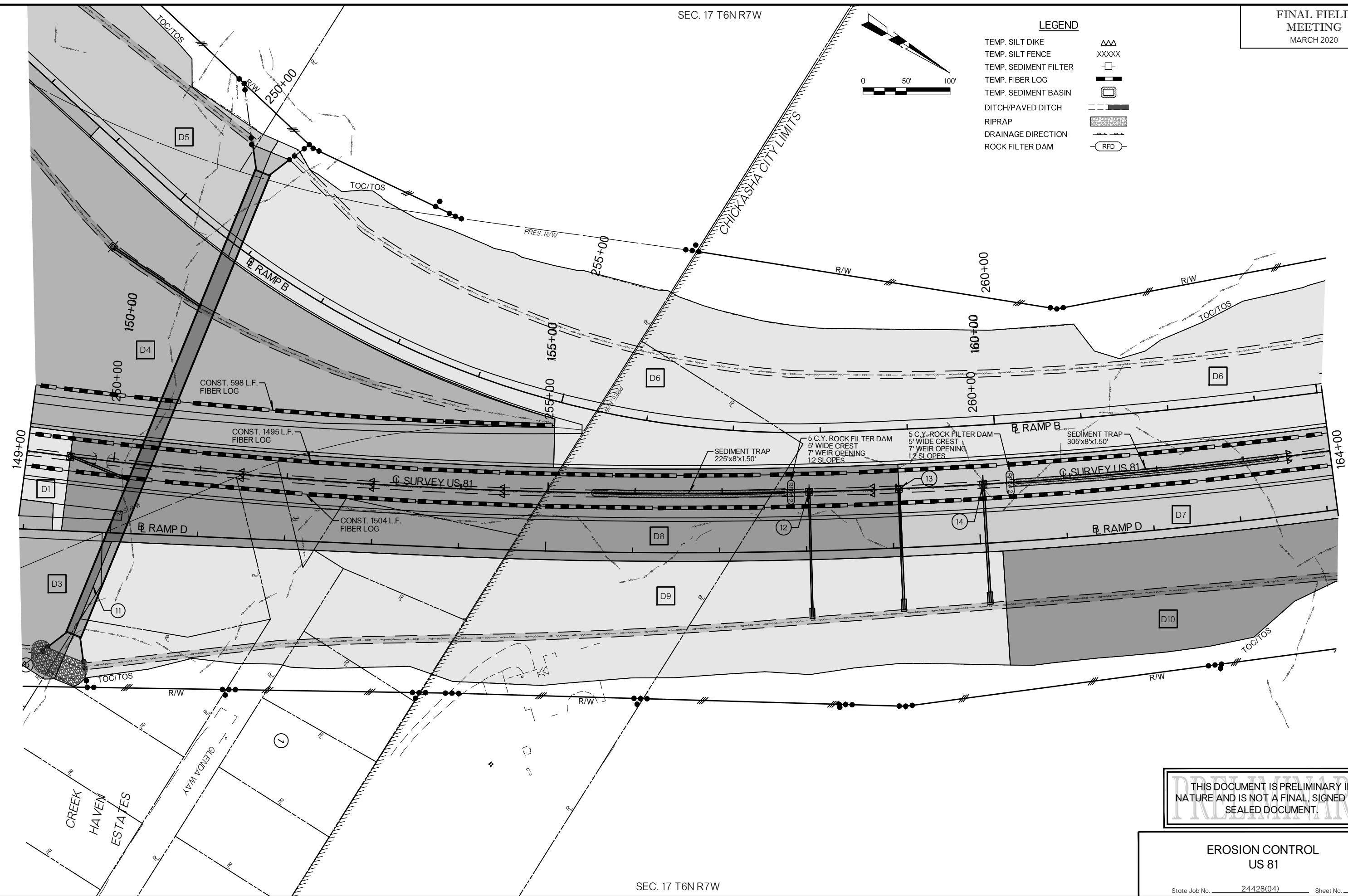
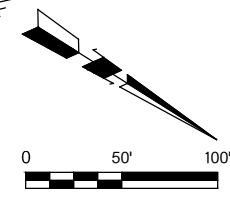
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

**EROSION CONTROL
US 81**

GRADY COUNTY US 81 REALIGNMENT

LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM

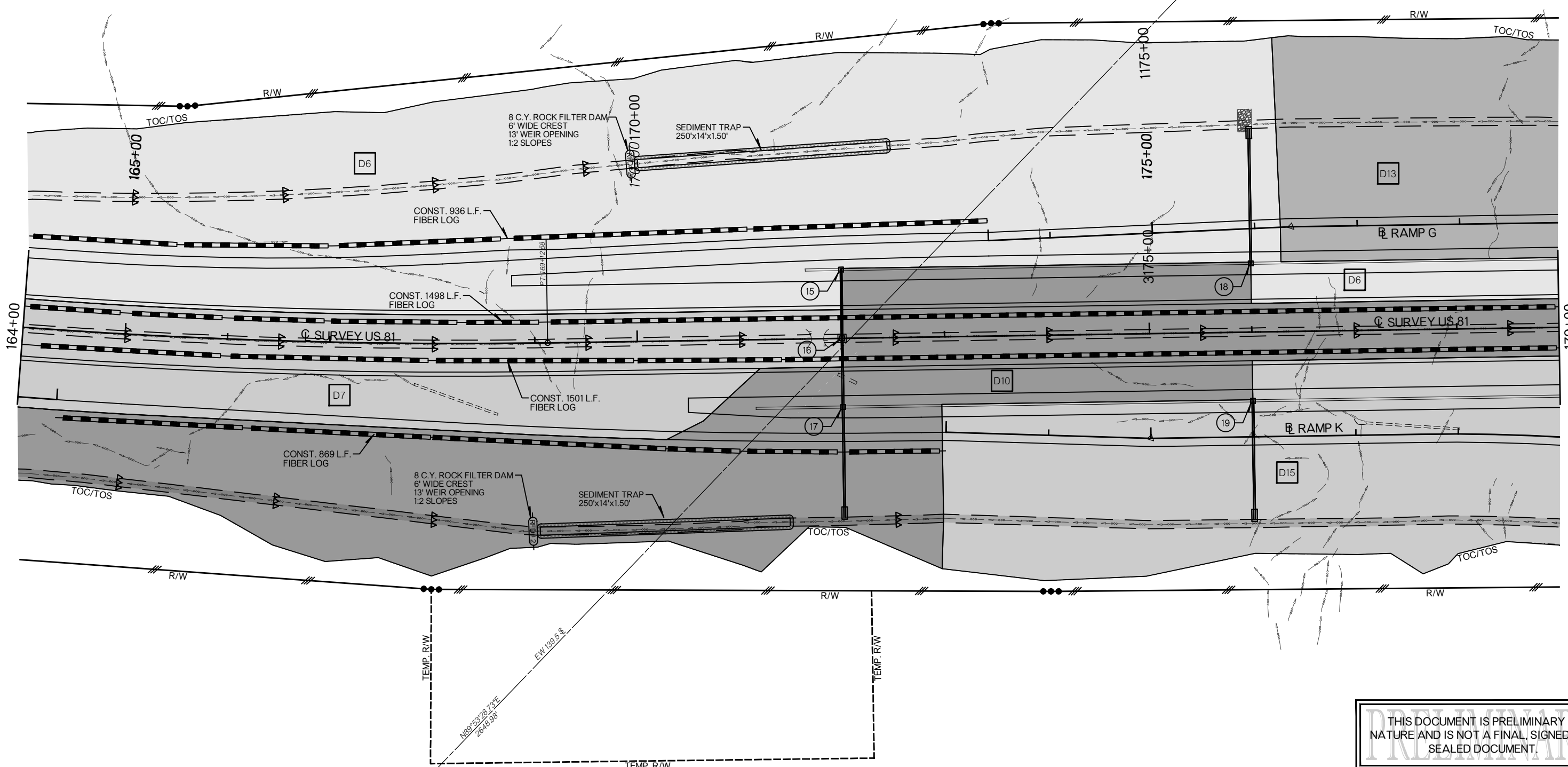
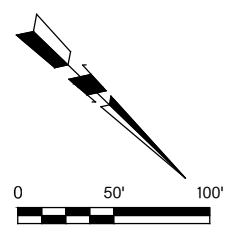


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

**EROSION CONTROL
US 81**

LEGEND

- TEMP. SILT DIKE △△△
- TEMP. SILT FENCE XXXXX
- TEMP. SEDIMENT FILTER □
- TEMP. FIBER LOG ▬
- TEMP. SEDIMENT BASIN ○
- DITCH/PAVED DITCH ▬▬▬
- RIPRAP ▨
- DRAINAGE DIRECTION ▬▬▬▬▬
- ROCK FILTER DAM (RFD)

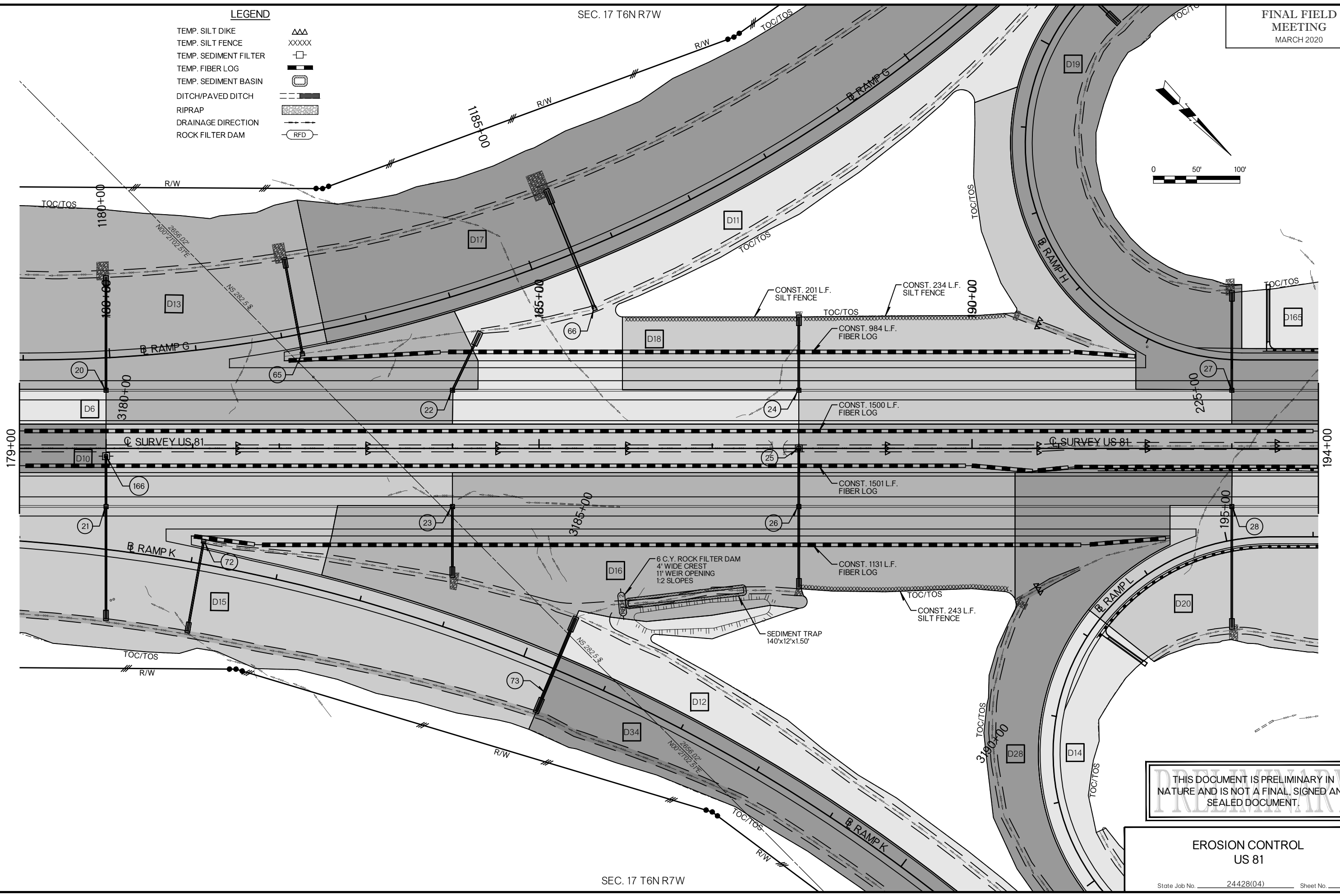
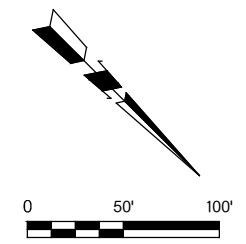


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

**EROSION CONTROL
US 81**

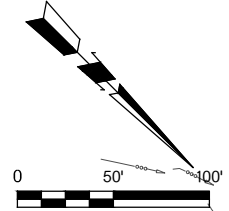
LEGEND

| | |
|-----------------------|-------|
| TEMP. SILT DIKE | △△ |
| TEMP. SILT FENCE | XXXXX |
| TEMP. SEDIMENT FILTER | □ |
| TEMP. FIBER LOG | ▬ |
| TEMP. SEDIMENT BASIN | ○ |
| DITCH/PAVED DITCH | ▬▬ |
| RIPRAP | ▨ |
| DRAINAGE DIRECTION | → |
| ROCK FILTER DAM | RFD |



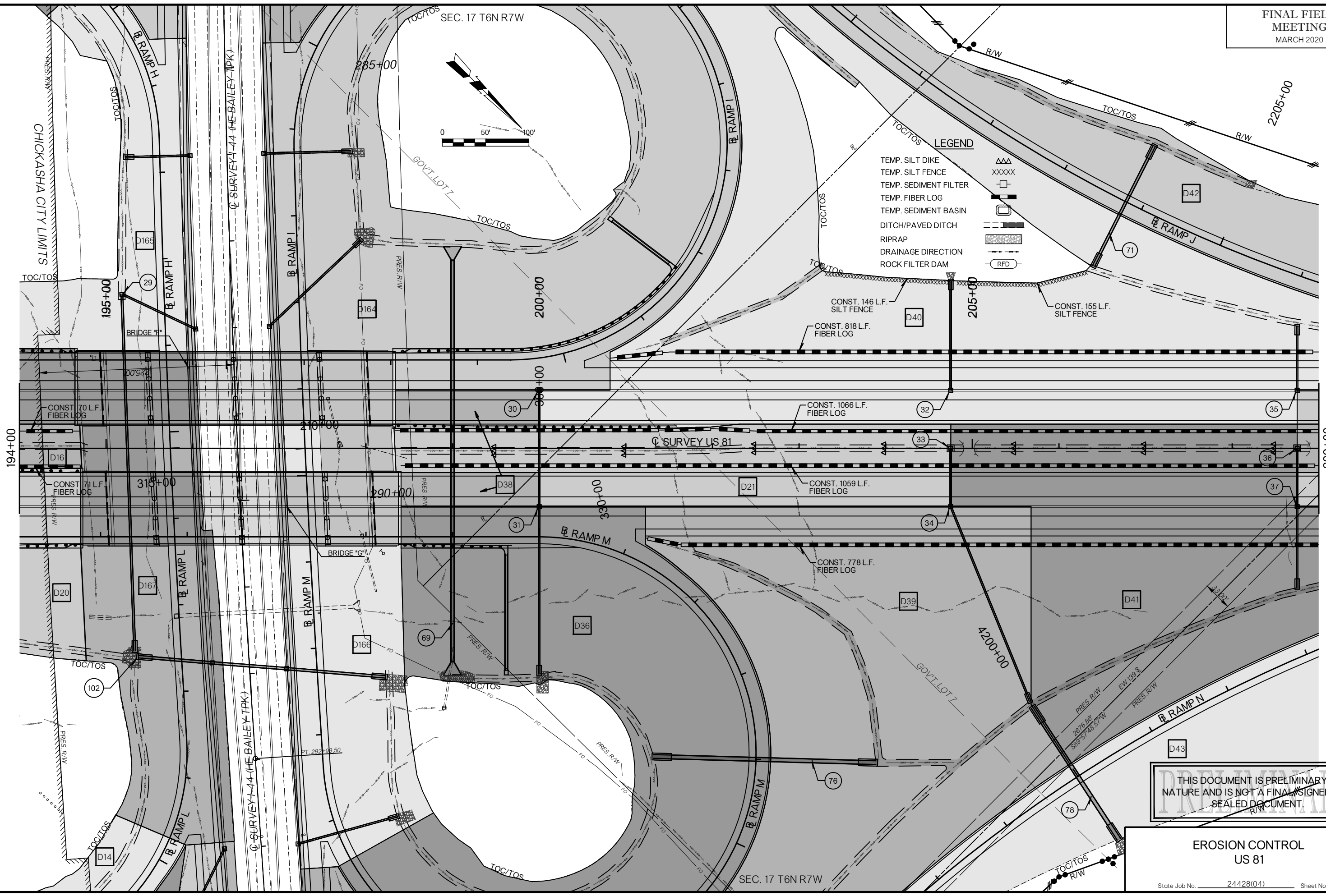
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

**EROSION CONTROL
US 81**



LEGEND

| | |
|-----------------------|-------|
| TEMP. SILT DIKE | △△△ |
| TEMP. SILT FENCE | XXXXX |
| TEMP. SEDIMENT FILTER | □ |
| TEMP. FIBER LOG | — |
| TEMP. SEDIMENT BASIN | ○ |
| DITCH/PAVED DITCH | — |
| RIPRAP | ▨ |
| DRAINAGE DIRECTION | → |
| ROCK FILTER DAM | RFD |



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

**EROSION CONTROL
US 81**

State Job No. 24428(04) Sheet No. R021

US 81 REALIGNMENT
GRADY COUNTY

SEC. 17 T6N R7W

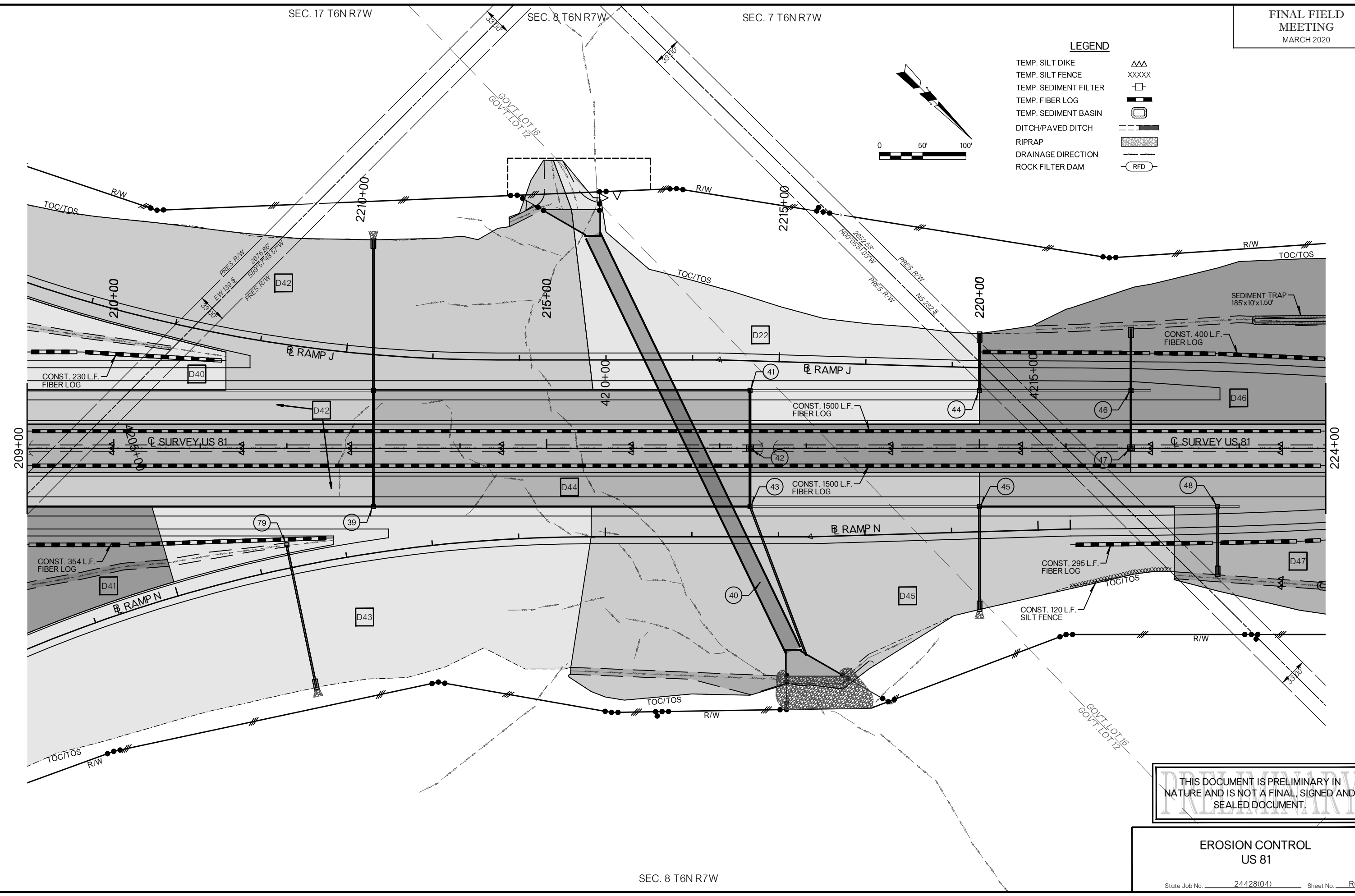
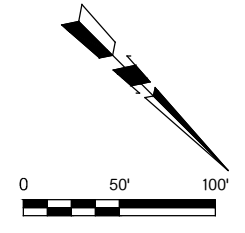
SEC. 8 T6N R7W

SEC. 7 T6N R7W

FINAL FIELD MEETING
MARCH 2020

LEGEND

- TEMP. SILT DIKE △△△
- TEMP. SILT FENCE XXXXX
- TEMP. SEDIMENT FILTER □
- TEMP. FIBER LOG ▬
- TEMP. SEDIMENT BASIN ○
- DITCH/PAVED DITCH ▬▬▬
- RIPRAP ▨
- DRAINAGE DIRECTION →
- ROCK FILTER DAM RFD



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL
US 81

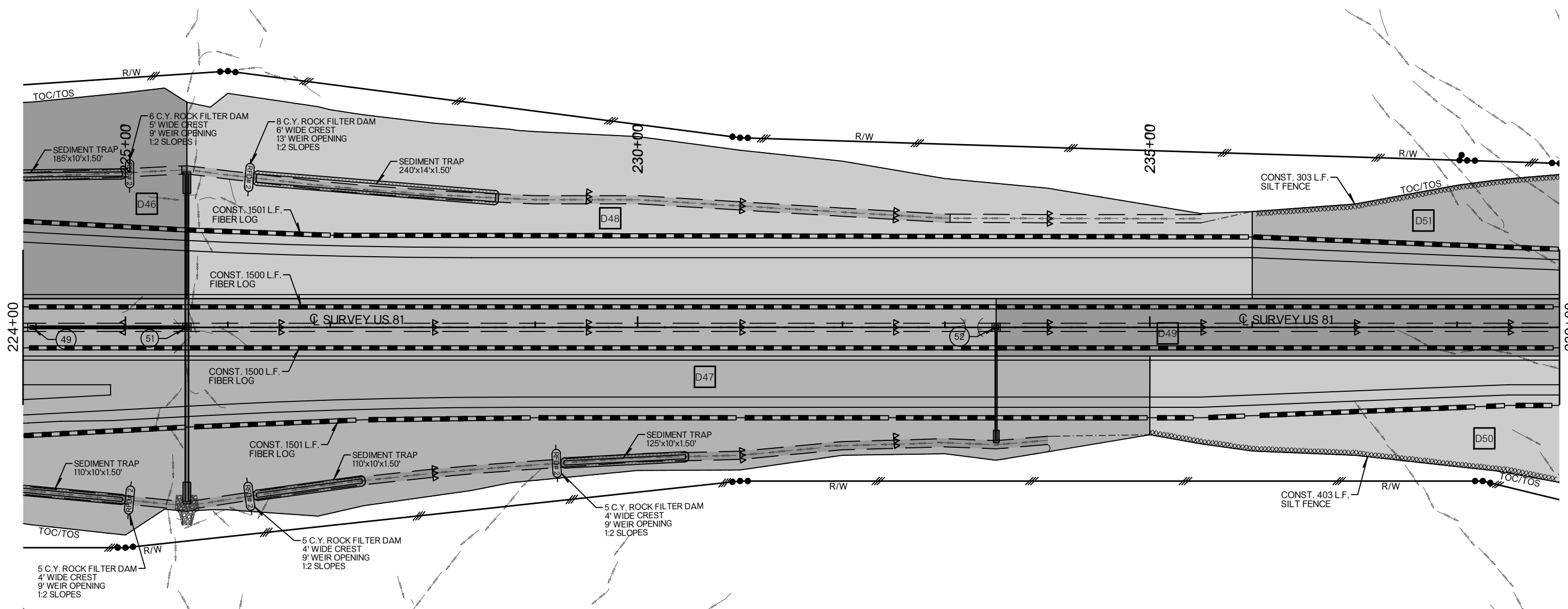
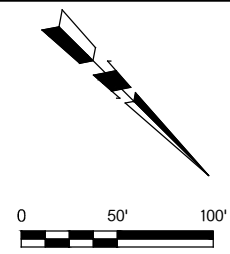
SEC. 8 T6N R7W

GRADY COUNTY
US 81 REALIGNMENT

SEC. 7 T6N R7W

LEGEND

- TEMP. SILT DIKE ▲▲▲
- TEMP. SILT FENCE XXXXX
- TEMP. SEDIMENT FILTER □
- TEMP. FIBER LOG ▬
- TEMP. SEDIMENT BASIN ○
- DITCH/PAVED DITCH - - -
- RIPRAP ▨
- DRAINAGE DIRECTION →
- ROCK FILTER DAM (RFD)



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

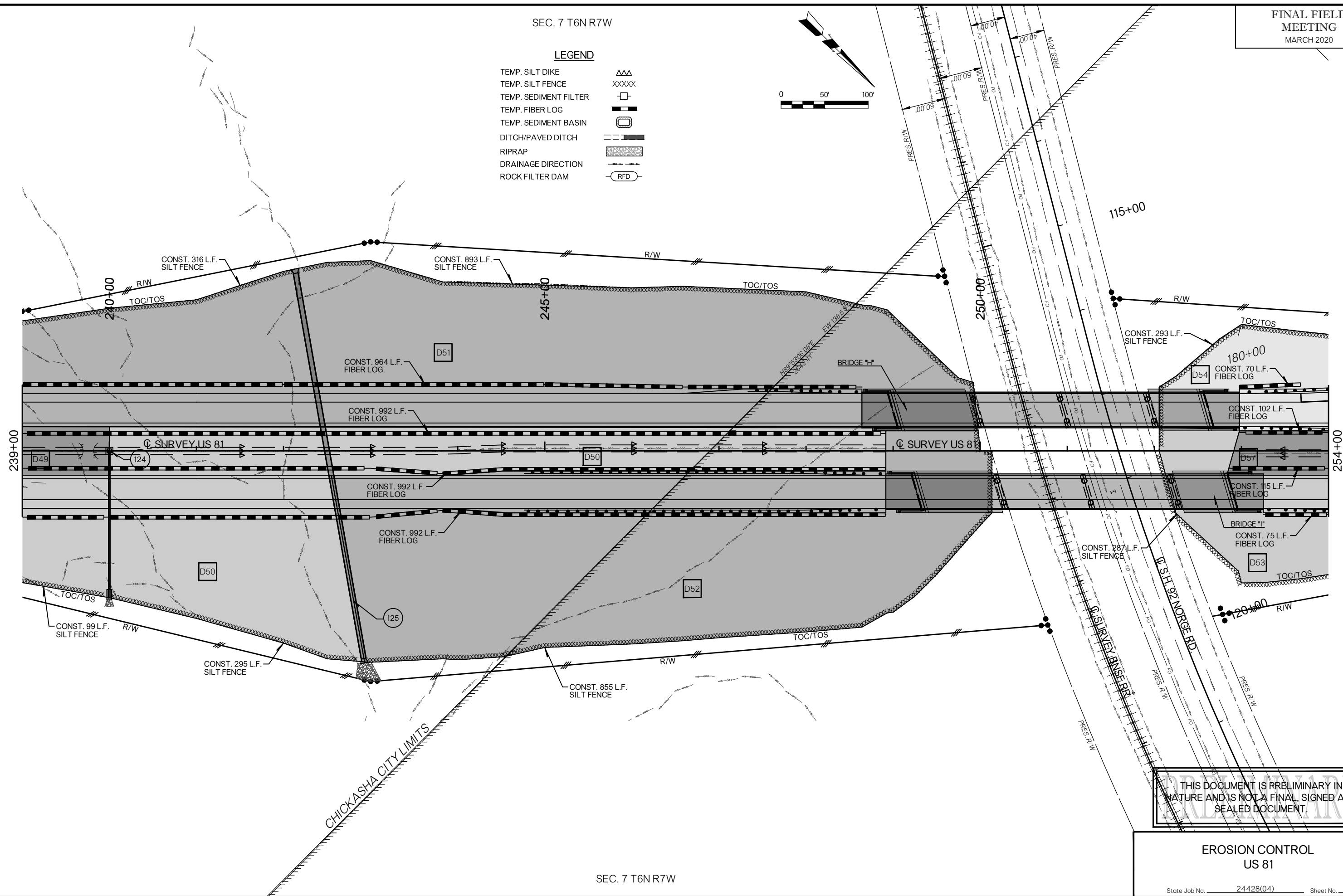
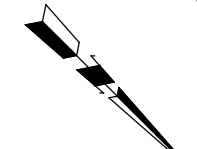
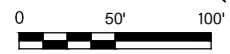
**EROSION CONTROL
US 81**

State Job No. 24428(04) Sheet No. R023

SEC. 7 T6N R7W

LEGEND

- TEMP. SILT DIKE △△△
- TEMP. SILT FENCE XXXXX
- TEMP. SEDIMENT FILTER □
- TEMP. FIBER LOG —
- TEMP. SEDIMENT BASIN ○
- DITCH/PAVED DITCH ---
- RIPRAP ▨
- DRAINAGE DIRECTION →
- ROCK FILTER DAM (RFD)



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

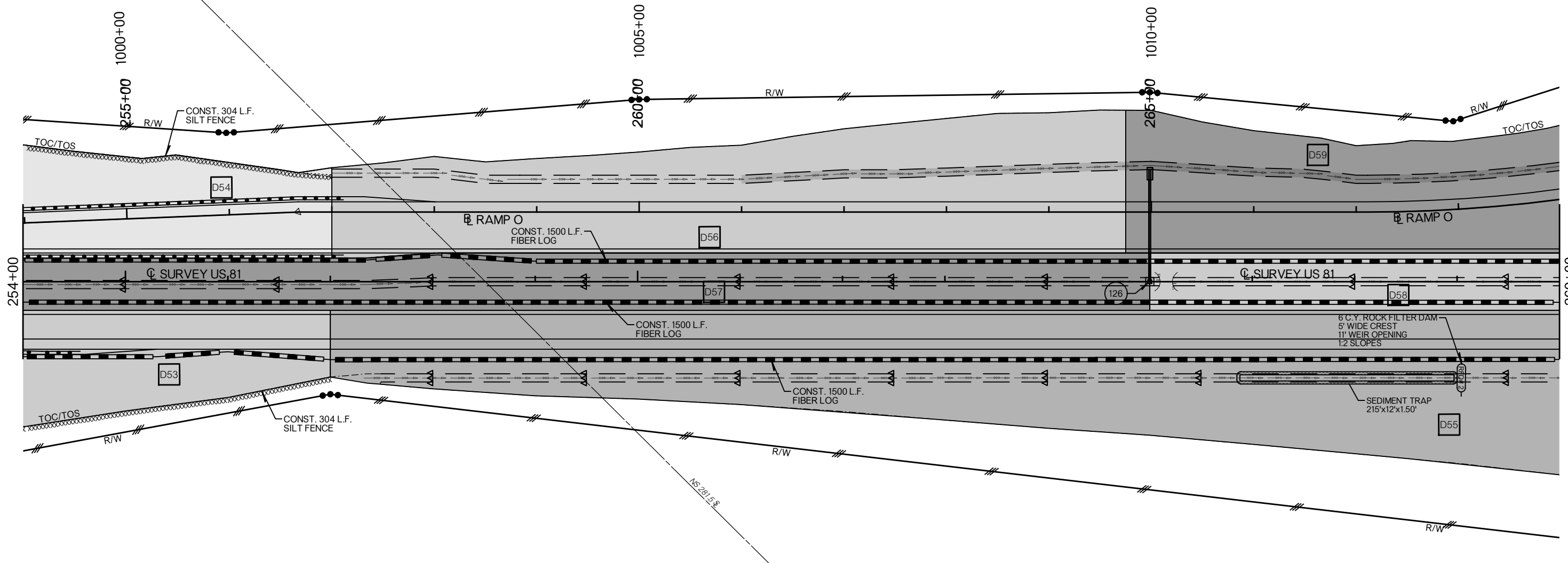
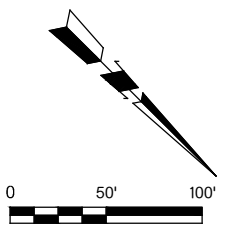
EROSION CONTROL
US 81

SEC. 7 T6N R7W

SEC. 7 T6N R7W

LEGEND

- TEMP. SILT DIKE △△△
- TEMP. SILT FENCE XXXXX
- TEMP. SEDIMENT FILTER □
- TEMP. FIBER LOG ▬
- TEMP. SEDIMENT BASIN ○
- DITCH/PAVED DITCH ▬▬▬
- RIPRAP ▨
- DRAINAGE DIRECTION →
- ROCK FILTER DAM (RFD)



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

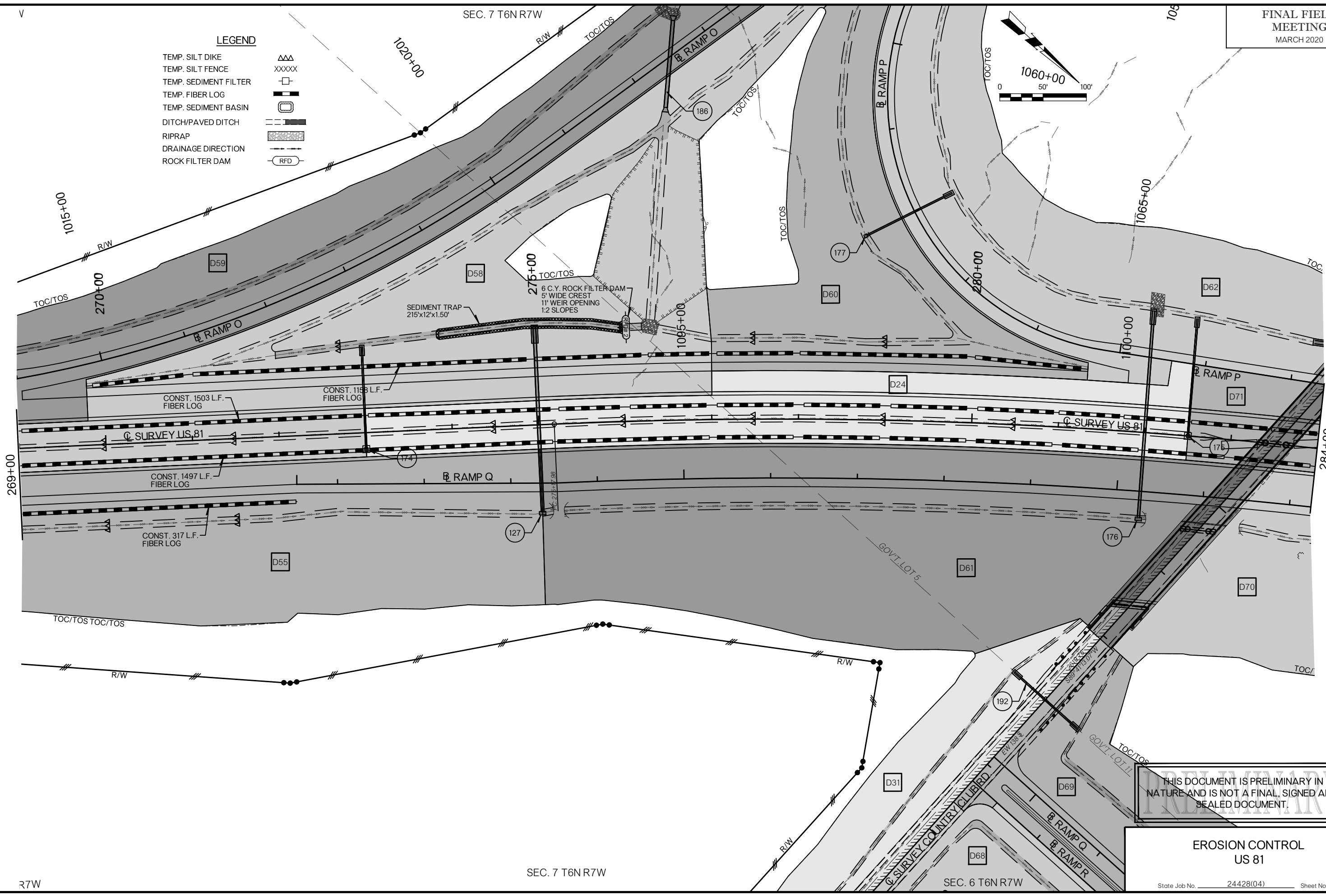
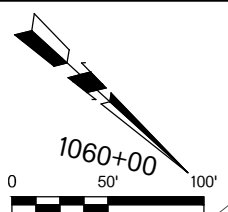
**EROSION CONTROL
US 81**

SEC. 7 T6N R7W

GRADY COUNTY US 81 REALIGNMENT

LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

**EROSION CONTROL
US 81**

SEC. 7 T6N R7W

SEC. 7 T6N R7W

SEC. 6 T6N R7W

R7W

US 81 REALIGNMENT
GRADY COUNTY

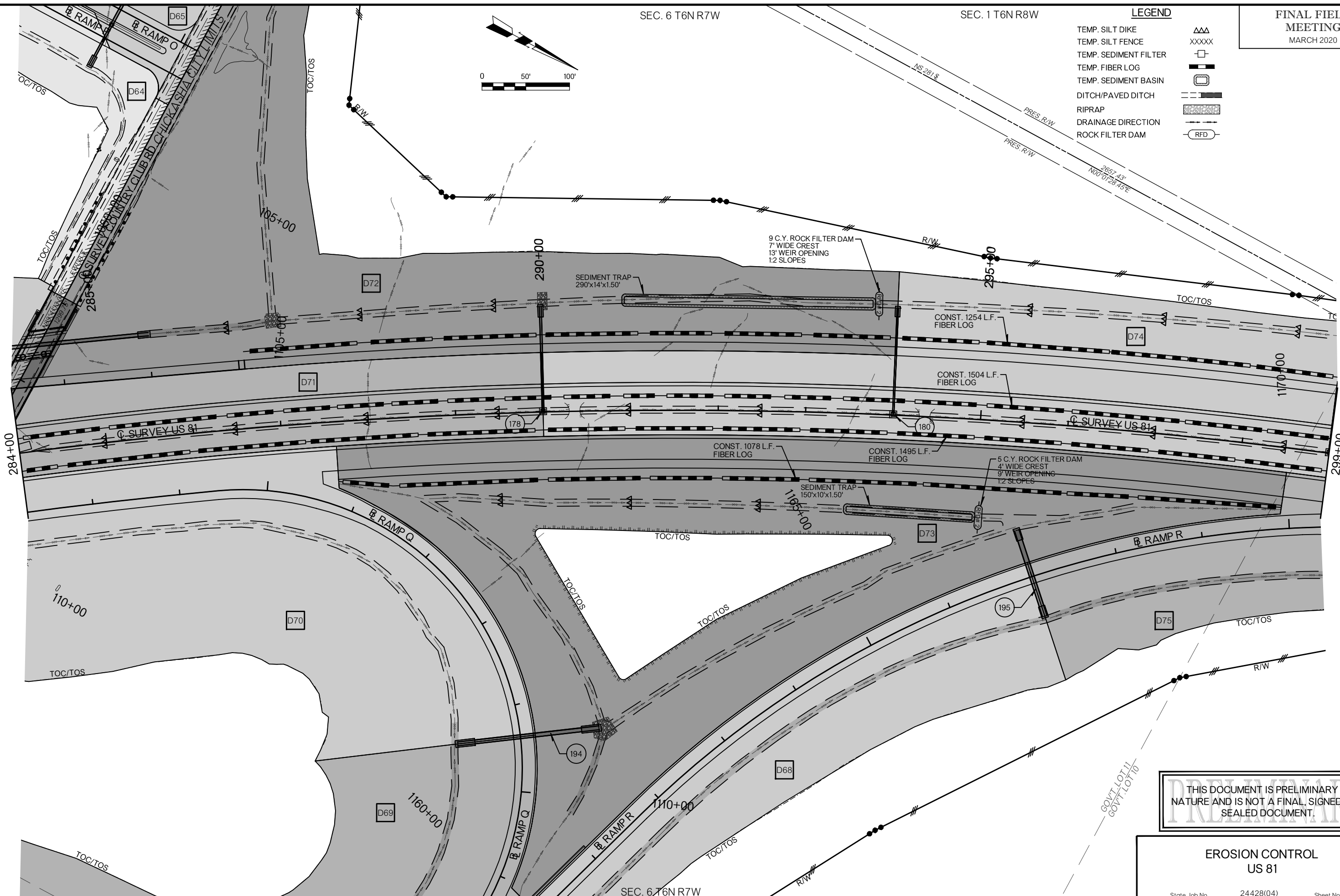
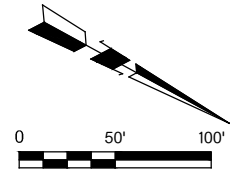
SEC. 6 T6N R7W

SEC. 1 T6N R8W

LEGEND

- TEMP. SILT DIKE △△△
- TEMP. SILT FENCE XXXXX
- TEMP. SEDIMENT FILTER □
- TEMP. FIBER LOG ▬
- TEMP. SEDIMENT BASIN ○
- DITCH/PAVED DITCH ▬▬▬
- RIPRAP ▨
- DRAINAGE DIRECTION →
- ROCK FILTER DAM RFD

FINAL FIELD MEETING
MARCH 2020



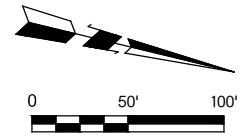
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

**EROSION CONTROL
US 81**

State Job No. 24428(04) Sheet No. R027

GRADY COUNTY US 81 REALIGNMENT

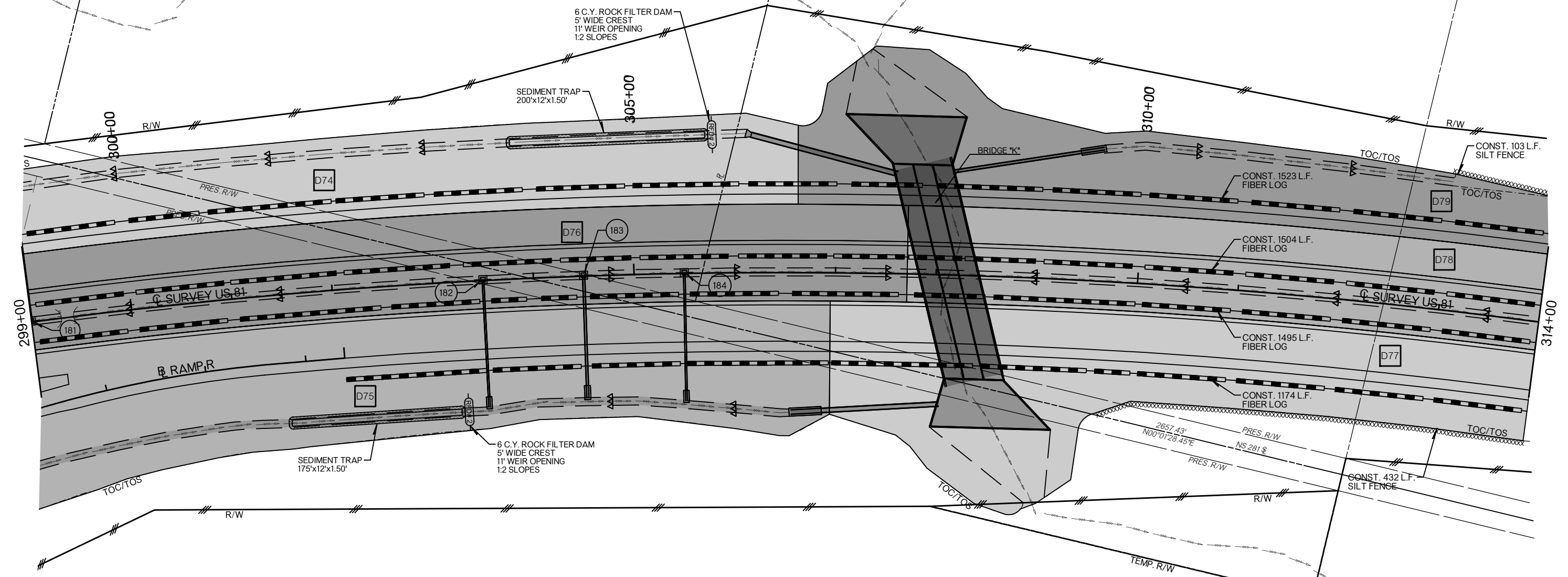
SEC. 1 T6N R8W



FINAL FIELD MEETING
MARCH 2020

LEGEND

| | |
|-----------------------|-------|
| TEMP. SILT DIKE | △△△ |
| TEMP. SILT FENCE | XXXXX |
| TEMP. SEDIMENT FILTER | □ |
| TEMP. FIBER LOG | ▬ |
| TEMP. SEDIMENT BASIN | ○ |
| DITCH/PAVED DITCH | ▬▬▬ |
| RIPRAP | ▨ |
| DRAINAGE DIRECTION | → |
| ROCK FILTER DAM | RFD |



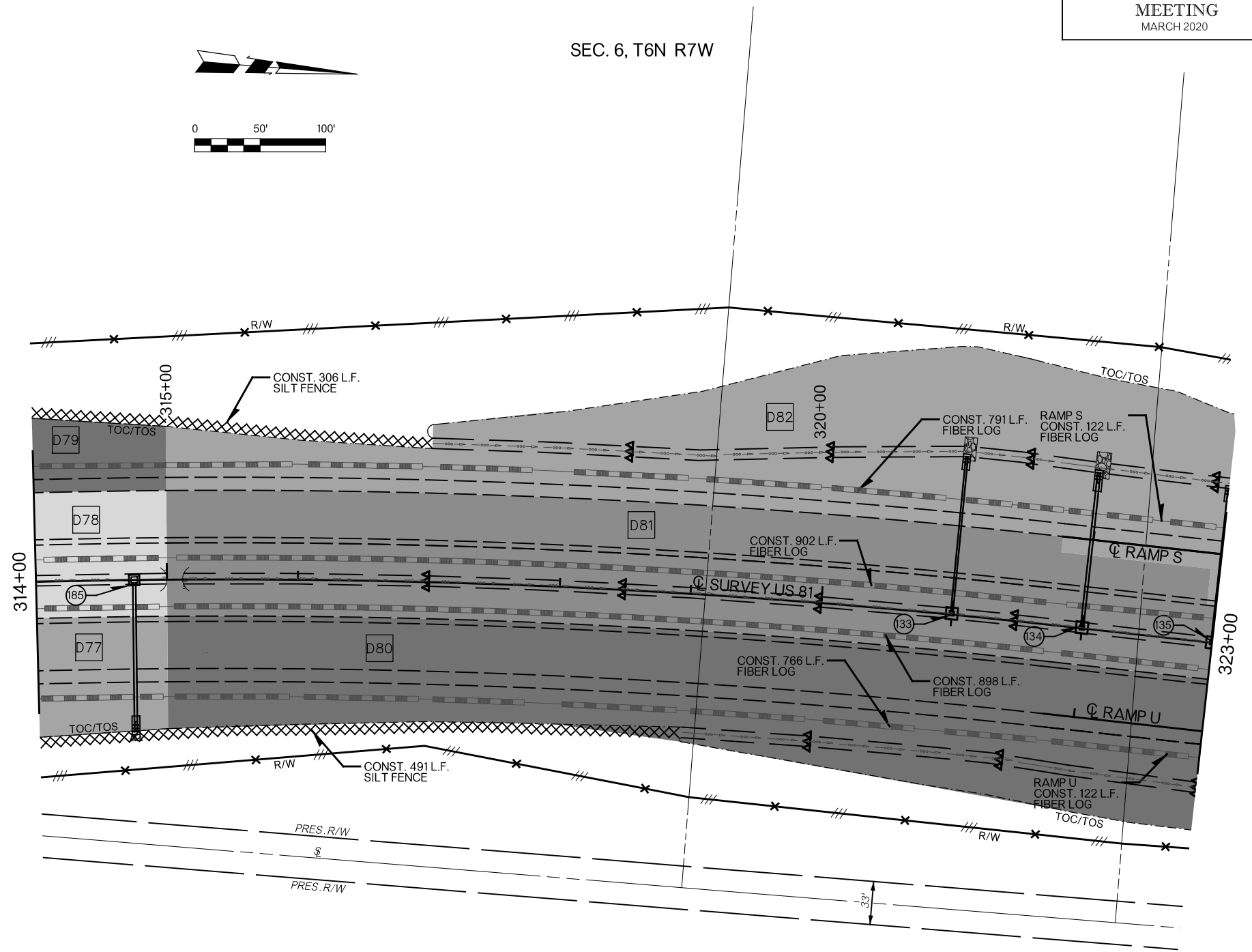
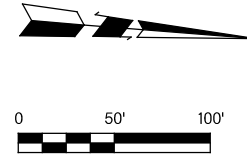
SEC. 6 T6N R7W

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

**EROSION CONTROL
US 81**

US 81 REALIGNMENT
GRADY COUNTY

SEC. 6, T6N R7W



LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM

NOTE:
ALL QUANTITIES, STATIONS AND
OFFSETS ARE BASED OF U.S. 81
ALIGNMENT UNLESS OTHERWISE
NOTED.

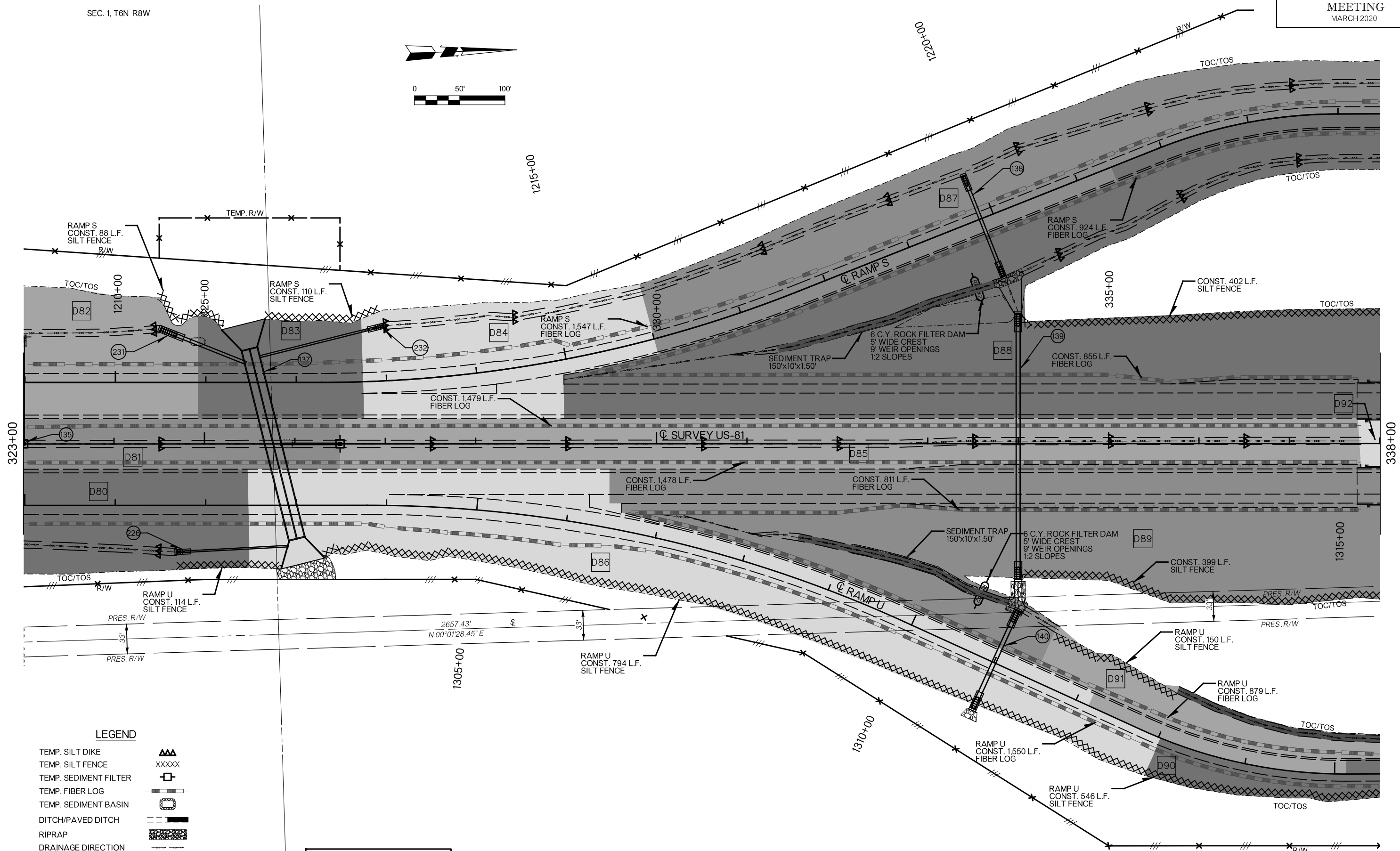
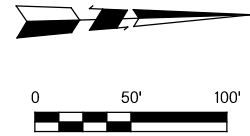
PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

**EROSION CONTROL
US 81**

State Job No. 24428(04) Sheet No. R029

US 81 REALIGNMENT
GRADY COUNTY

SEC. 1, T6N R8W



LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM

NOTE:
ALL QUANTITIES, STATIONS AND OFFSETS ARE BASED OF U.S. 81 ALIGNMENT UNLESS OTHERWISE NOTED.

SEC. 6, T6N R7W

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

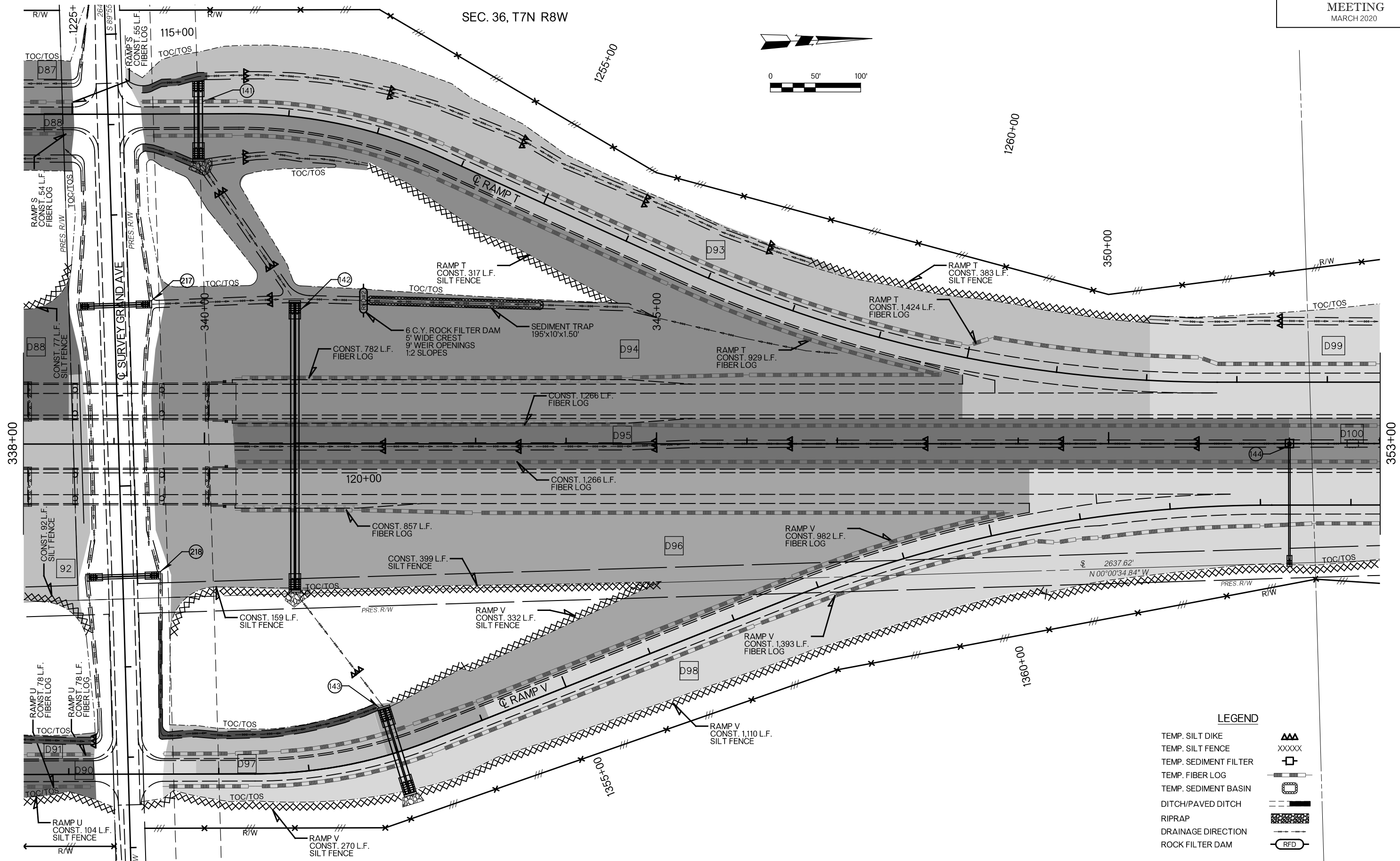
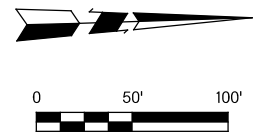
**EROSION CONTROL
US 81**

State Job No. 24428(04) Sheet No. R030

GRADY COUNTY

SEC. 36, T7N R8W

SEC. 31, T7N R7W



LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM

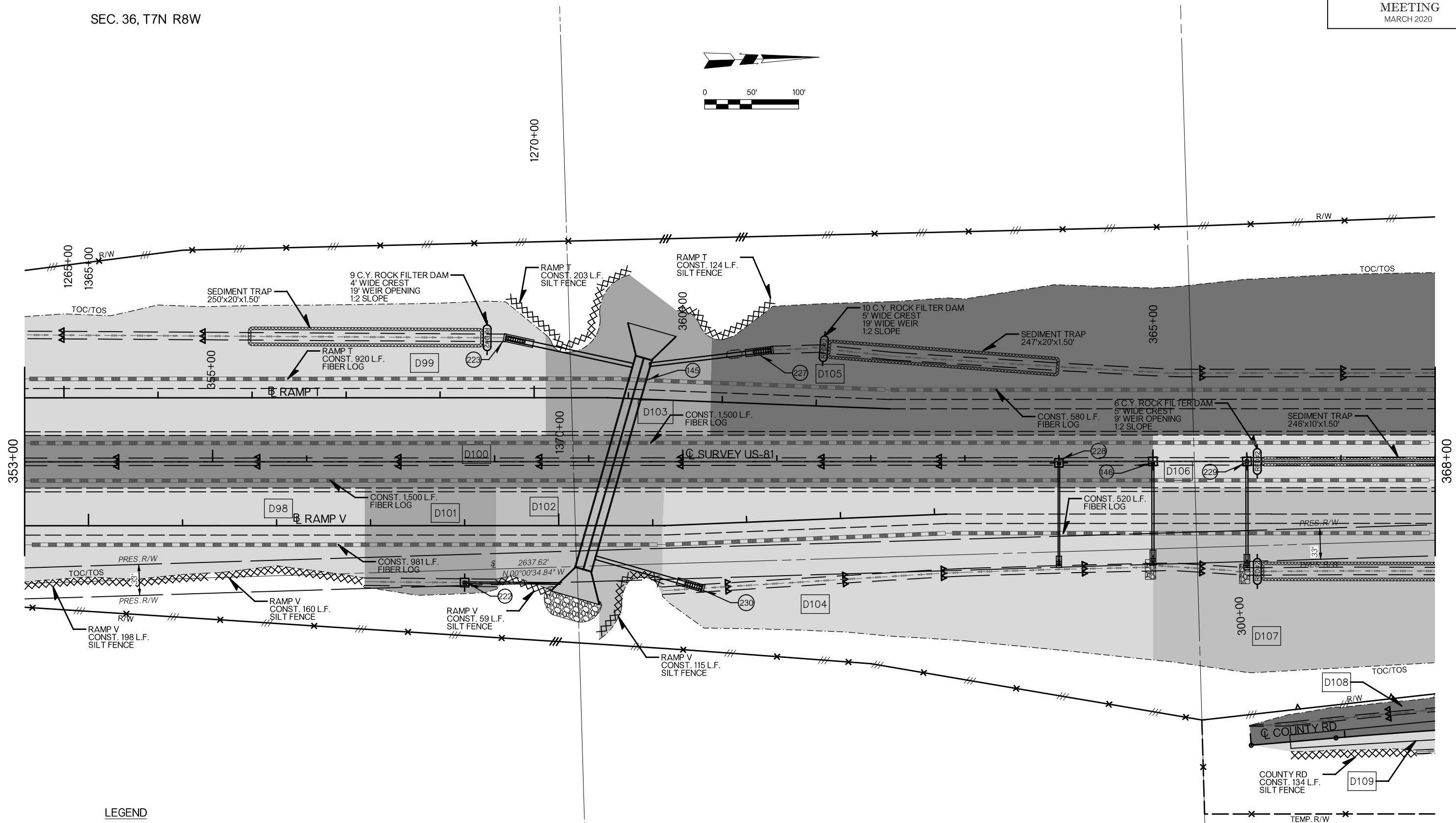
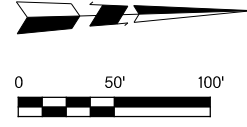
NOTE:
ALL QUANTITIES, STATIONS AND OFFSETS ARE BASED OF U.S. 81 ALIGNMENT UNLESS OTHERWISE NOTED.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

**EROSION CONTROL
US 81**

State Job No. 24428(04) Sheet No. R031

US 81 REALIGNMENT
GRADY COUNTY



LEGEND

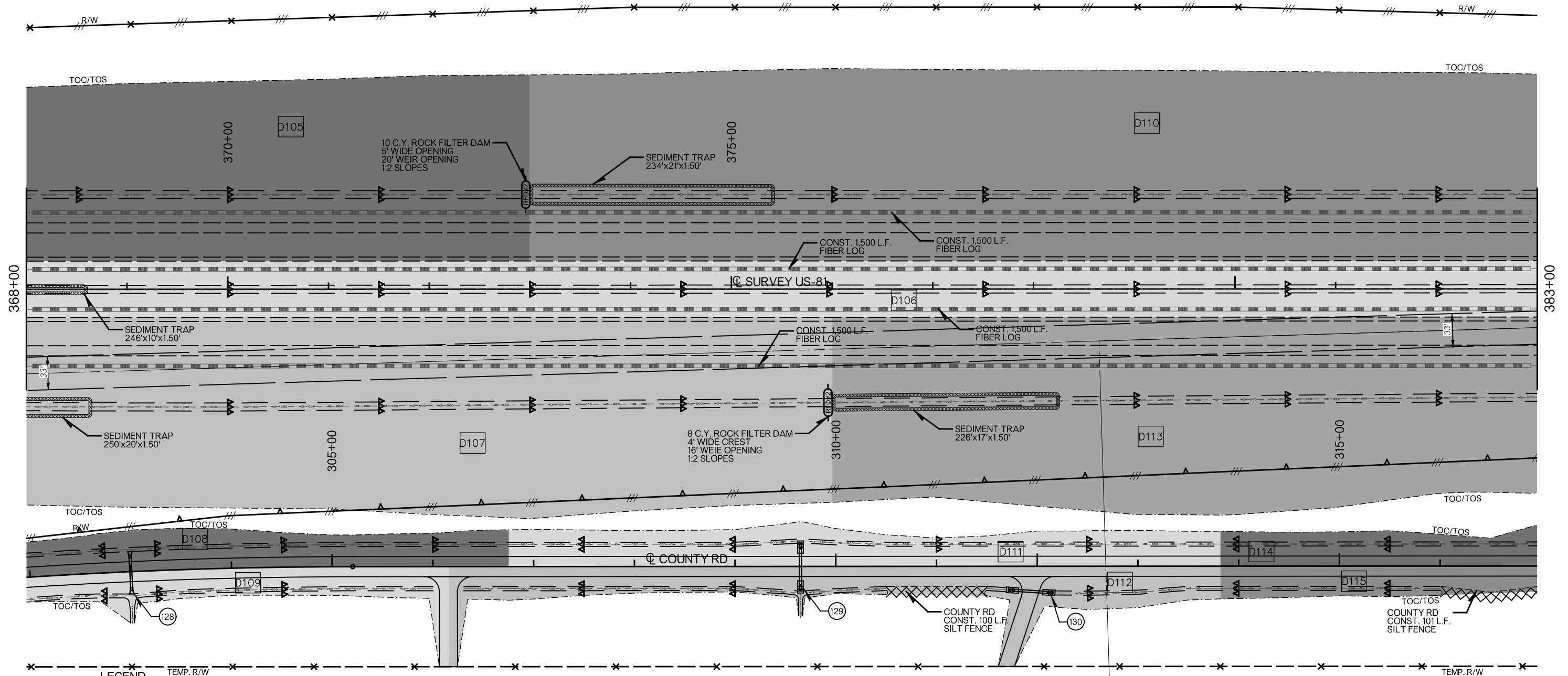
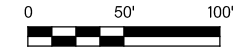
- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM

NOTE:
ALL QUANTITIES, STATIONS AND
OFFSETS ARE BASED OF U.S. 81
ALIGNMENT UNLESS OTHERWISE
NOTED.

THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

EROSION CONTROL
US 81

State Job No. 24428(04) Sheet No. R032



LEGEND

| | |
|-----------------------|--|
| TEMP. SILT DIKE | |
| TEMP. SILT FENCE | |
| TEMP. SEDIMENT FILTER | |
| TEMP. FIBER LOG | |
| TEMP. SEDIMENT BASIN | |
| DITCH/PAVED DITCH | |
| RIPRAP | |
| DRAINAGE DIRECTION | |
| ROCK FILTER DAM | |

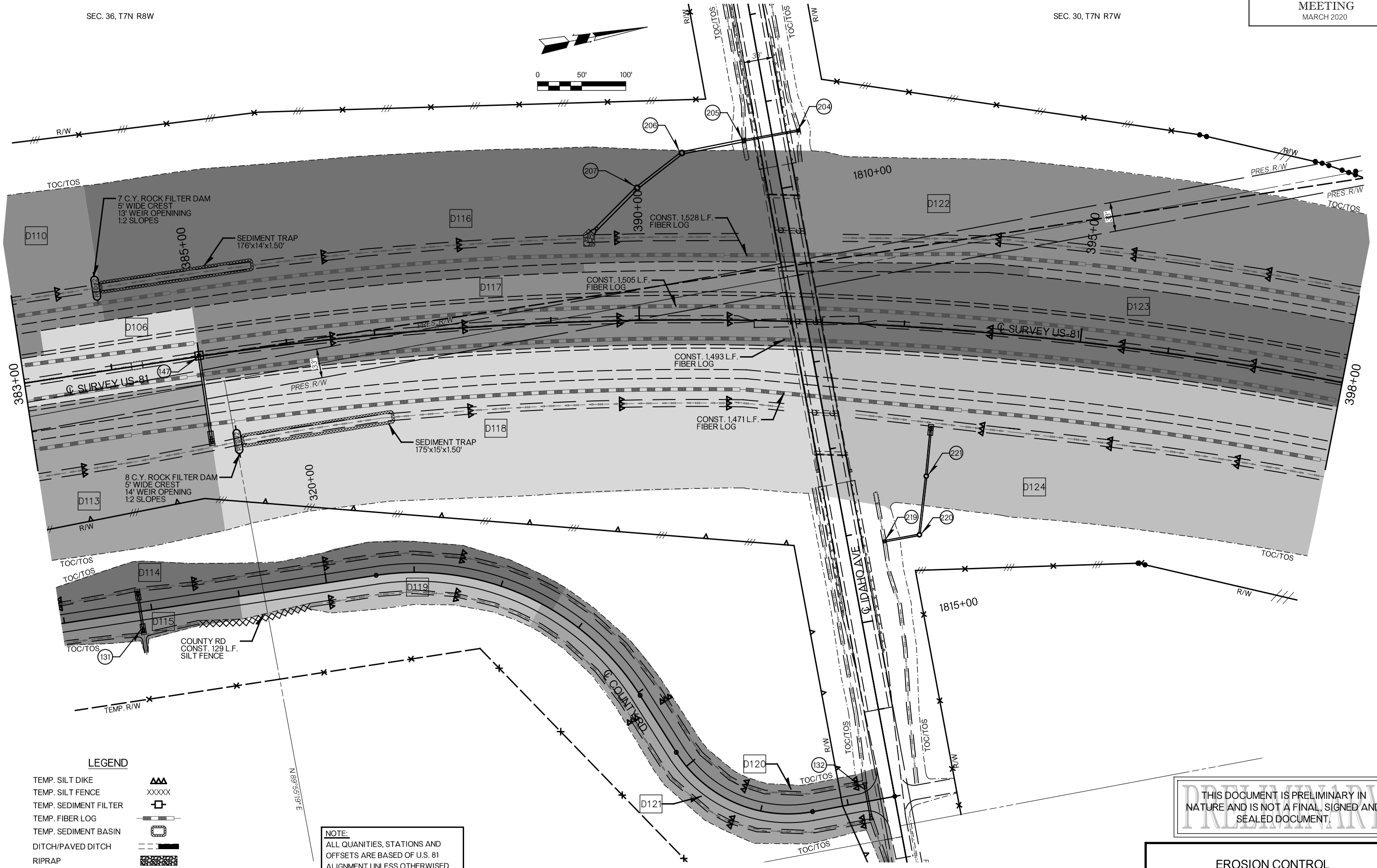
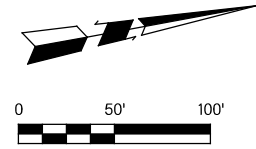
NOTE:
ALL QUANTITIES, STATIONS AND OFFSETS ARE BASED OF U.S. 81 ALIGNMENT UNLESS OTHERWISE NOTED.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL US 81

State Job No. 24428(04) Sheet No. R033

US 81 REALIGNMENT GRADY COUNTY



LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM

NOTE:
ALL QUANTITIES, STATIONS AND
OFFSETS ARE BASED OF U.S. 81
ALIGNMENT UNLESS OTHERWISE
NOTED.

THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

EROSION CONTROL
US 81

SEC. 25, T7N R8W

SEC. 30, T7N R7W

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL US 81

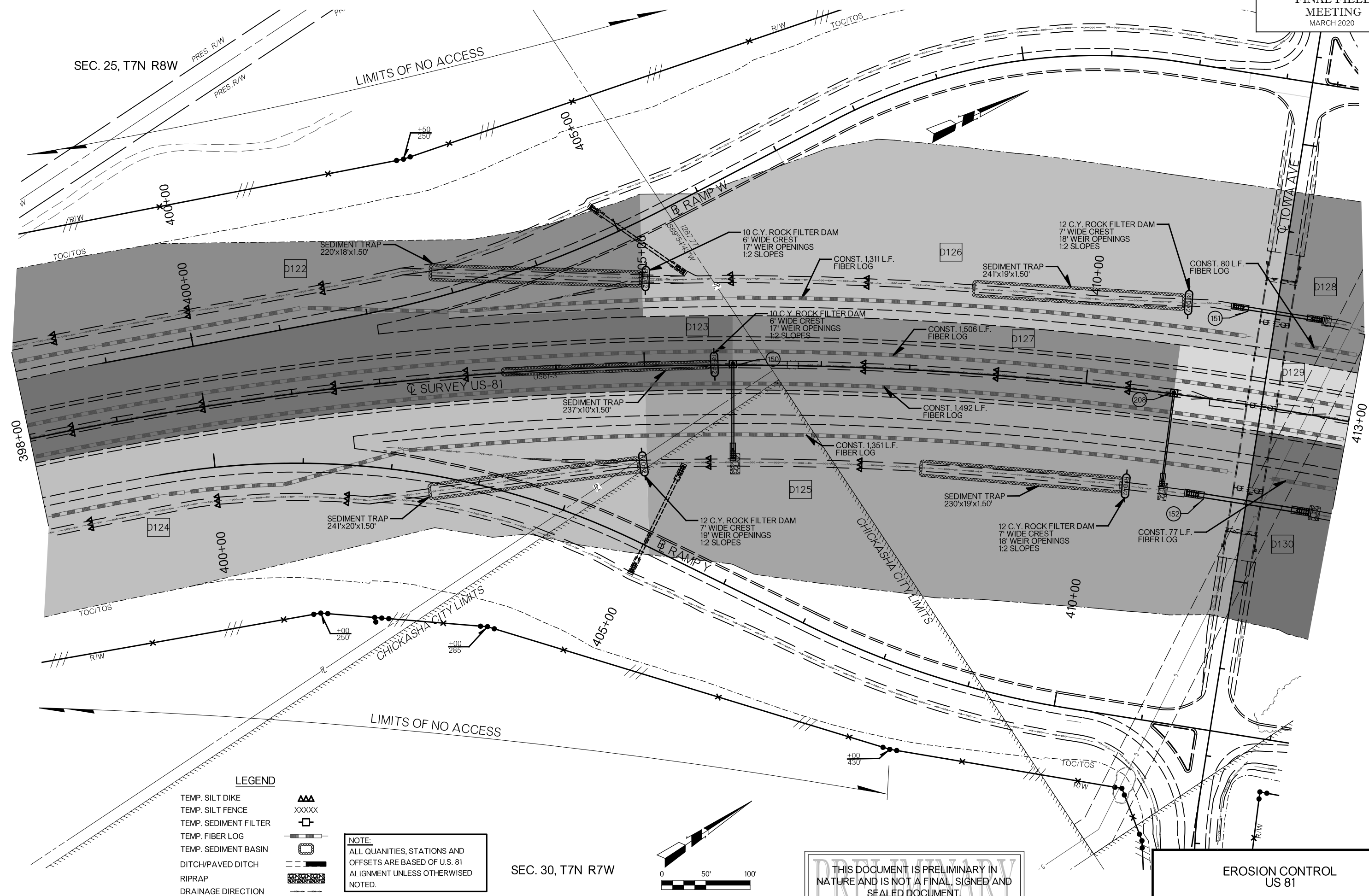
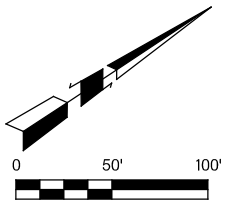
LIMITS OF NO ACCESS

LIMITS OF NO ACCESS

LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM

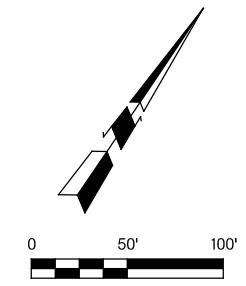
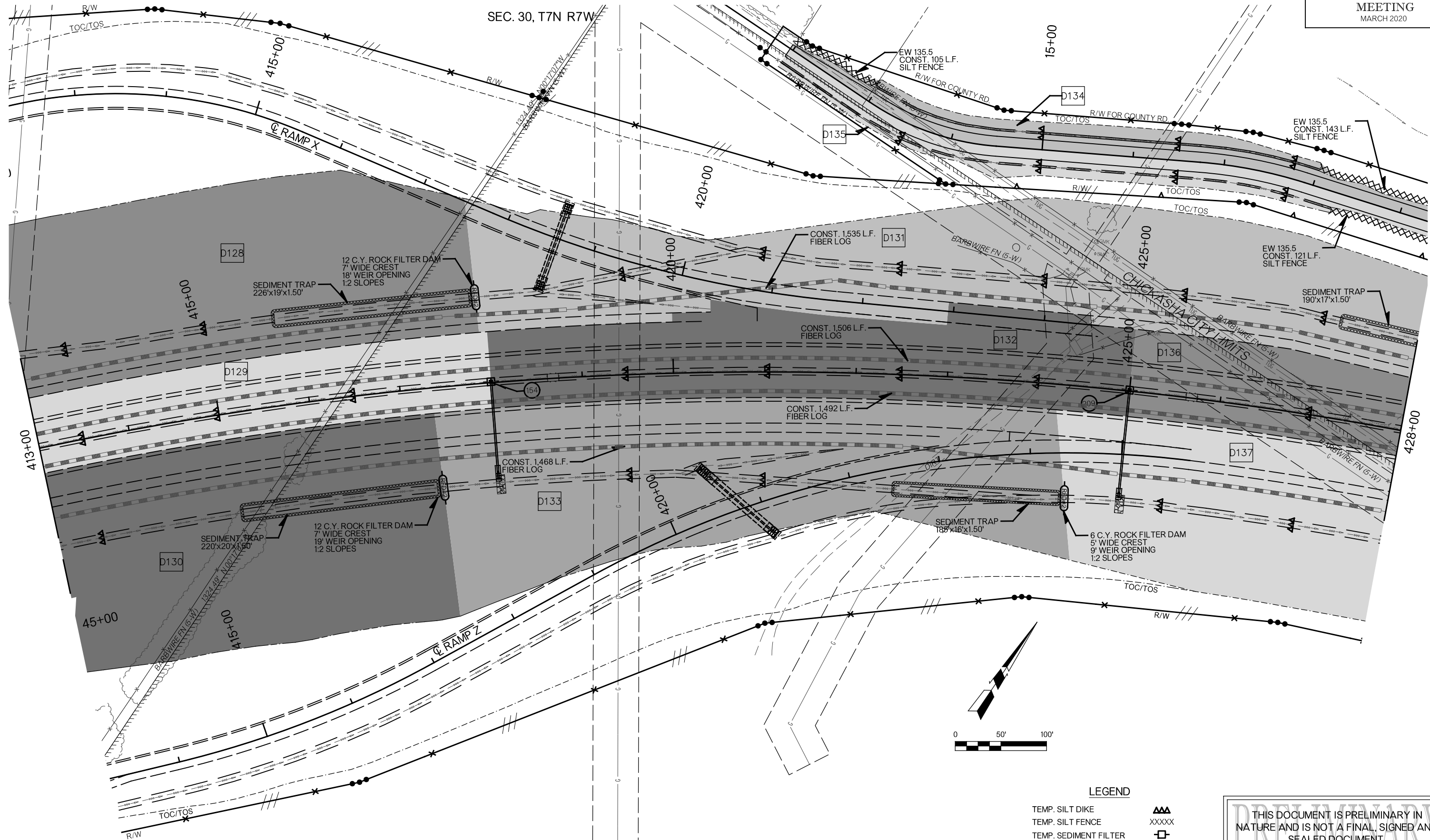
NOTE:
ALL QUANTITIES, STATIONS AND OFFSETS ARE BASED OF U.S. 81 ALIGNMENT UNLESS OTHERWISE NOTED.



GRADY COUNTY US 81 REALIGNMENT

SEC. 30, T7N R7W

SEC. 30, T7N R7W



NOTE:
ALL QUANTITIES, STATIONS AND OFFSETS ARE BASED OF U.S. 81 ALIGNMENT UNLESS OTHERWISE NOTED.

LEGEND

| | |
|-----------------------|-------|
| TEMP. SILT DIKE | ▲▲▲ |
| TEMP. SILT FENCE | XXXXX |
| TEMP. SEDIMENT FILTER | □ |
| TEMP. FIBER LOG | ▬▬▬ |
| TEMP. SEDIMENT BASIN | ○ |
| DITCH/PAVED DITCH | ▬▬▬▬ |
| RIPRAP | ▨▨▨▨ |
| DRAINAGE DIRECTION | ▬▬▬▬ |
| ROCK FILTER DAM | RFD |

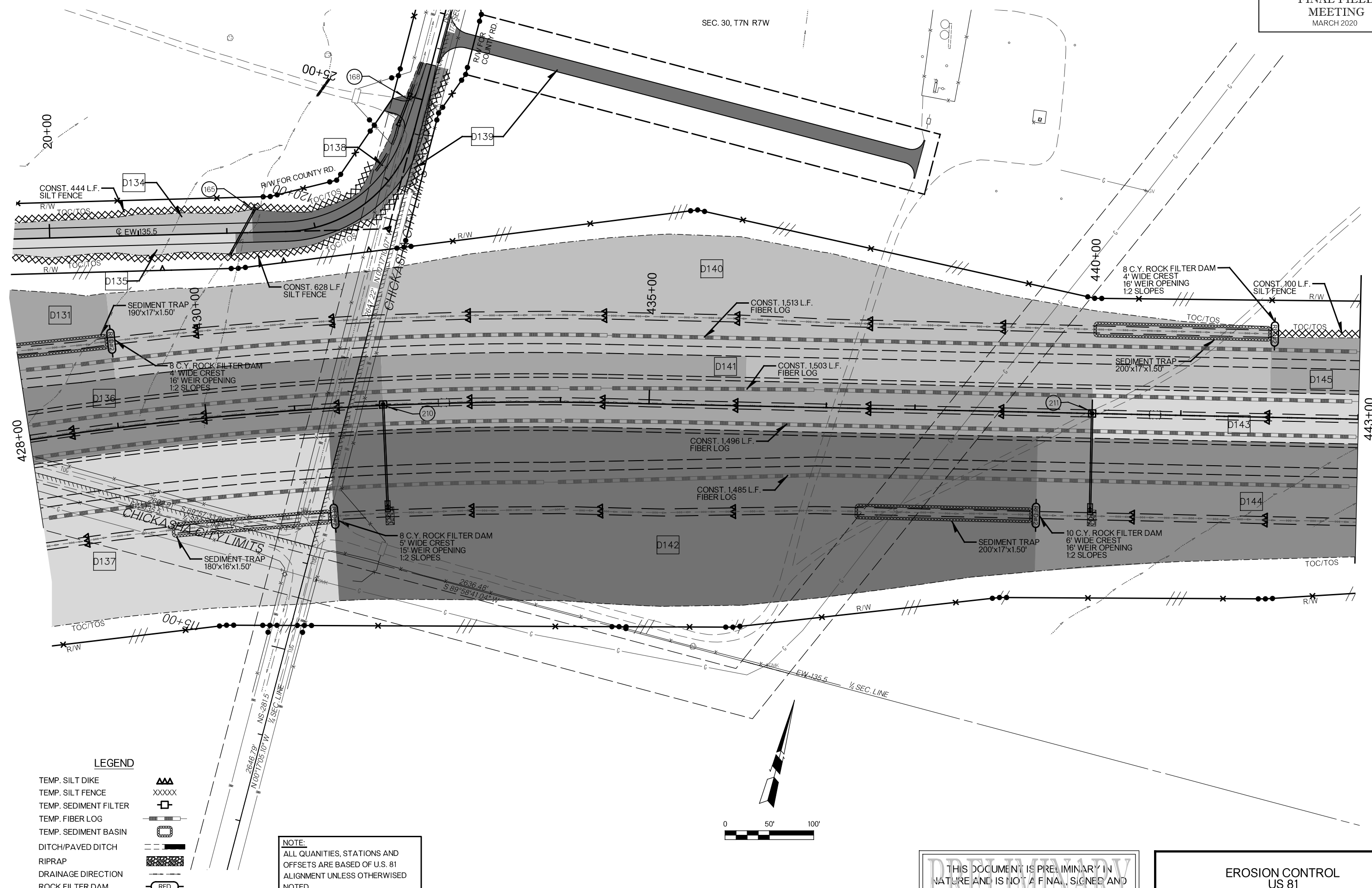
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL US 81

State Job No. 24428(04) Sheet No. R036

US 81 REALIGNMENT GRADY COUNTY

SEC. 30, T7N R7W



LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM

NOTE:
ALL QUANTITIES, STATIONS AND OFFSETS ARE BASED OF U.S. 81 ALIGNMENT UNLESS OTHERWISE NOTED.

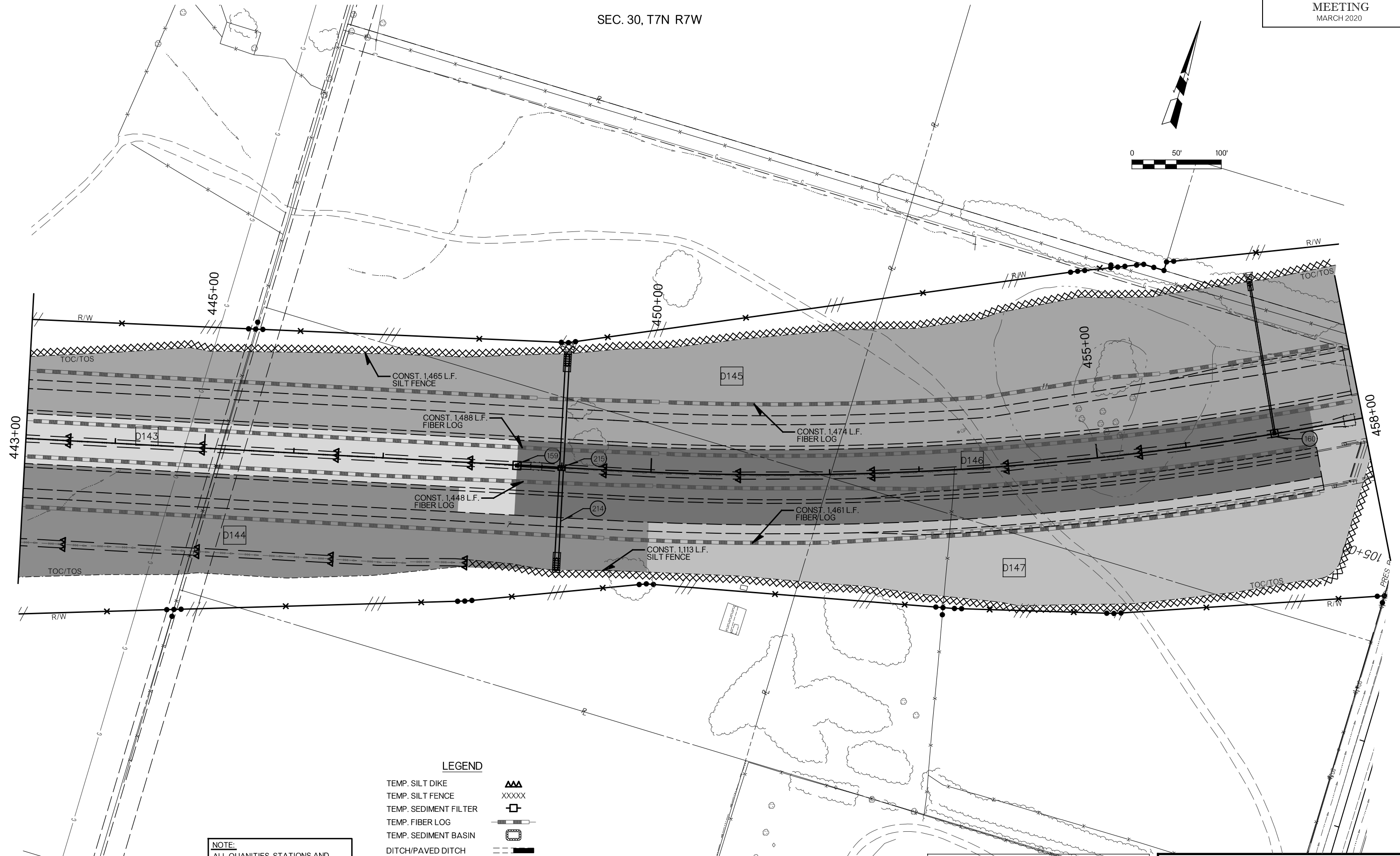
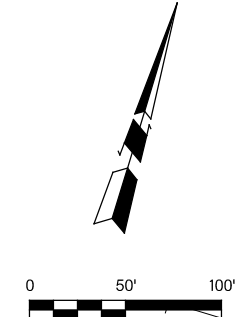
PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL
US 81
State Job No. 24428(04) Sheet No. R037

SEC. 30, T7N R7W

US 81 REALIGNMENT
GRADY COUNTY

SEC. 30, T7N R7W



NOTE:
ALL QUANTITIES, STATIONS AND OFFSETS ARE BASED OF U.S. 81 ALIGNMENT UNLESS OTHERWISE NOTED.

LEGEND

| | |
|-----------------------|-------|
| TEMP. SILT DIKE | ▲▲▲ |
| TEMP. SILT FENCE | XXXXX |
| TEMP. SEDIMENT FILTER | □ |
| TEMP. FIBER LOG | ▬ |
| TEMP. SEDIMENT BASIN | ○ |
| DITCH/PAVED DITCH | ▬▬ |
| RIPRAP | ▨▨▨ |
| DRAINAGE DIRECTION | → |
| ROCK FILTER DAM | ○ RFD |

SEC. 30, T7N R7W

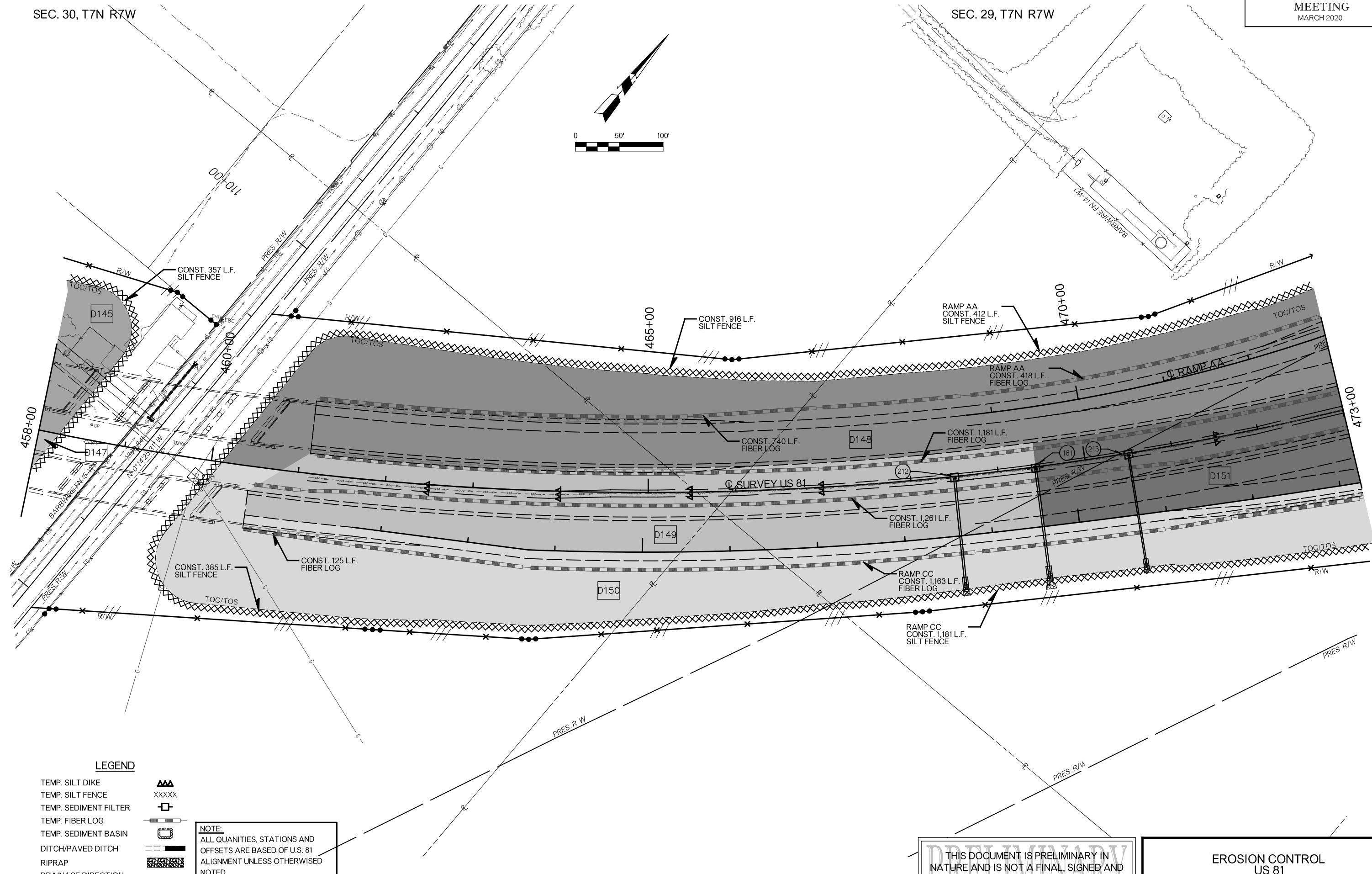
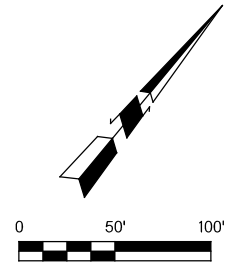
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL US 81
State Job No. 24428(04) Sheet No. R038

US 81 REALIGNMENT GRADY COUNTY

SEC. 30, T7N R7W

SEC. 29, T7N R7W



- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM

NOTE:
ALL QUANTITIES, STATIONS AND
OFFSETS ARE BASED OF U.S. 81
ALIGNMENT UNLESS OTHERWISE
NOTED.

THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

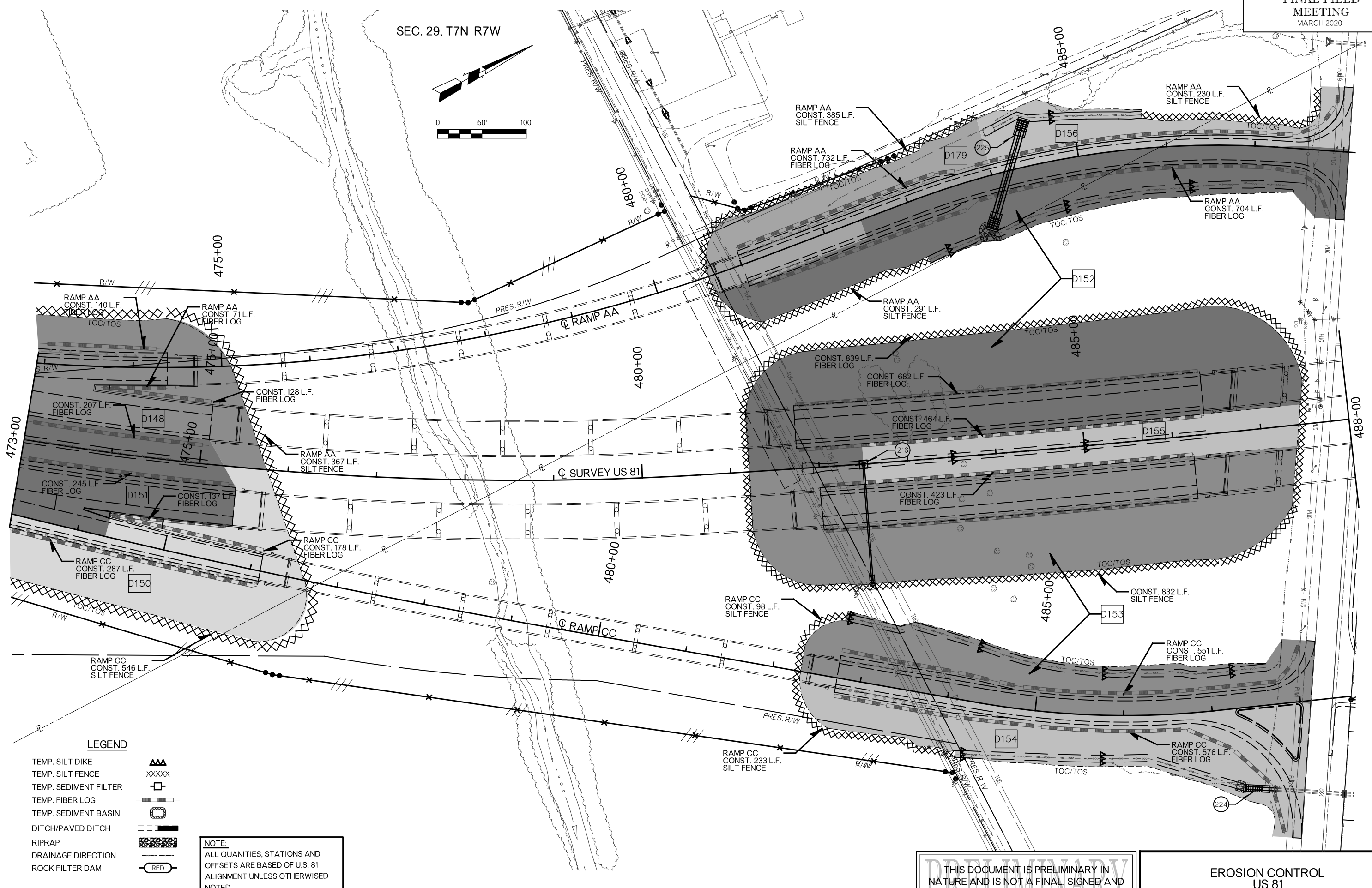
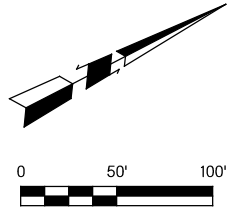
EROSION CONTROL
US 81

State Job No. 24428(04) Sheet No. R039

SEC. 29, T7N R7W

US 81 REALIGNMENT
GRADY COUNTY

SEC. 29, T7N R7W



LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM

NOTE:
ALL QUANTITIES, STATIONS AND OFFSETS ARE BASED OF U.S. 81 ALIGNMENT UNLESS OTHERWISE NOTED.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL US 81

SEC. 30, T7N R7W

US 81 REALIGNMENT GRADY COUNTY

SEC. 18 T6N R7W

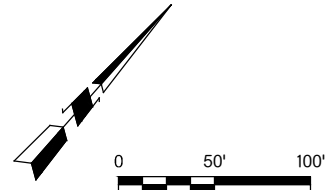
240+00

245+00

250+00

PRES. R/W

PRES. R/W



ASPHALT
CONCRETE
ASPHALT
ASPHALT
CONCRETE
ASPHALT

ASPHALT
CONCRETE
ASPHALT
ASPHALT
CONCRETE
ASPHALT

☉ SURVEY I-44 (HE BAILEY TPK)

252+00

LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM

PRES. R/W

PRES. R/W

CONST. 15 L.F. SILT FENCE

SEC. 18 T6N R7W

SEC. 18 T6N R7W

PRES. R/W

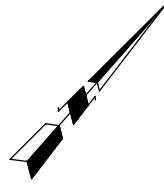
PRES. R/W

PRES. R/W

255+00

260+00

265+00



CONST. 668 L.F. SILT FENCE

CONST. 103 L.F. SILT FENCE

D157

D159

D160

☉ SURVEY I-44 (HE BAILEY TPK)

252+00

267+00

CONST. 741 L.F. SILT FENCE

D158

122

8 C.Y. ROCK FILTER DAM
6' WIDE CREST
13' WEIR OPENING
1:2 SLOPES

SEDIMENT TRAP
245'x10'x1.50'

CONST. 861 L.F. SILT FENCE

RAMP G

CONST. 861 L.F. SILT FENCE

PRES. R/W

PRES. R/W

PRES. R/W

SEC. 18 T6N R7W

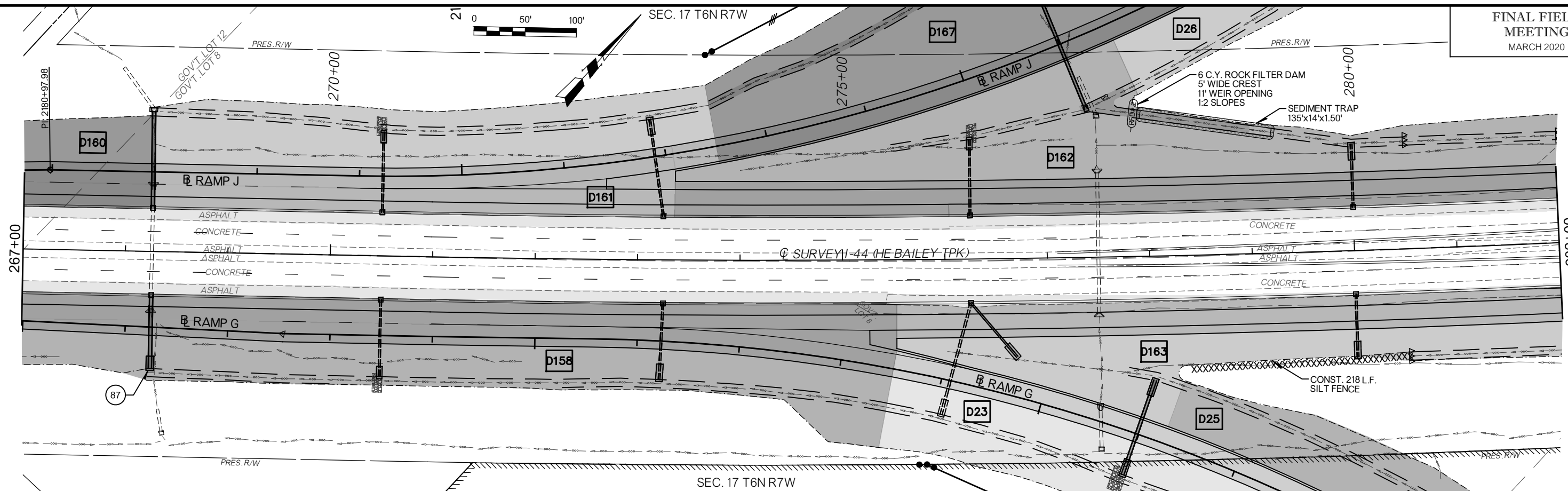
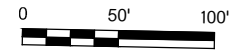
46

00

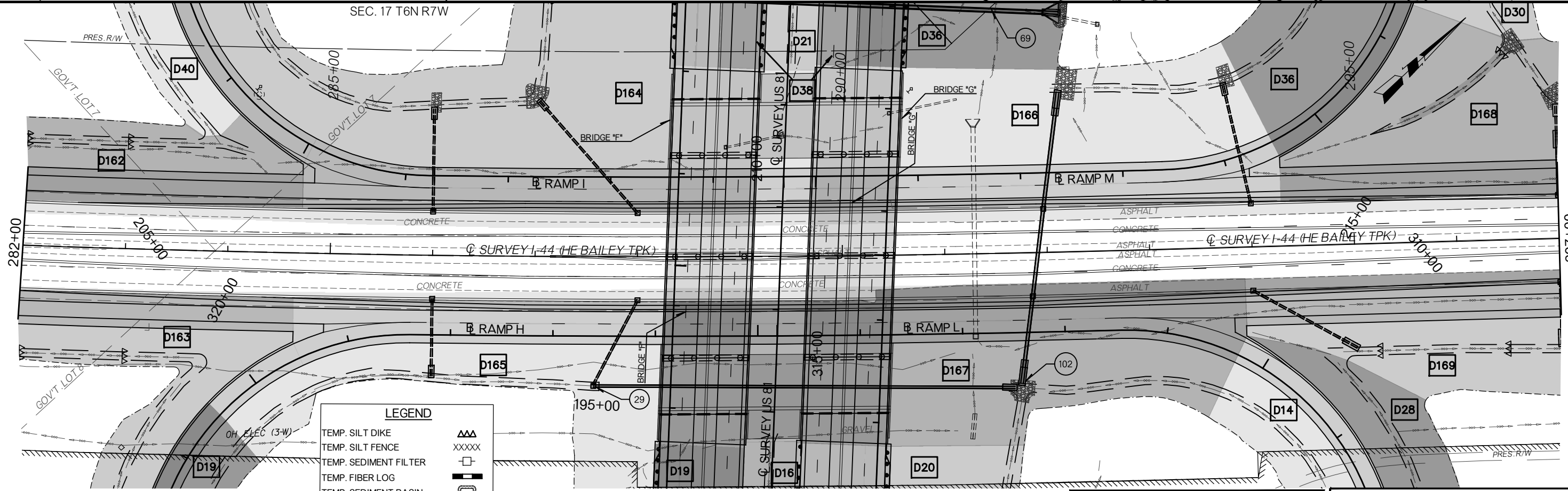
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL
I-44

SEC. 17 T6N R7W



SEC. 17 T6N R7W

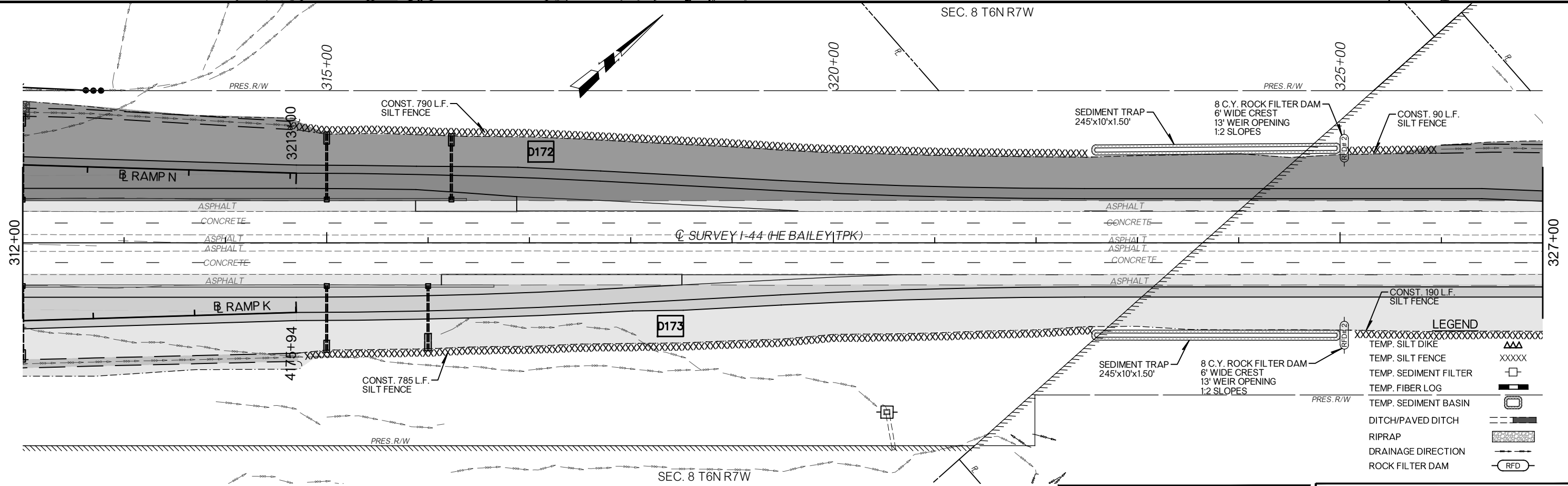
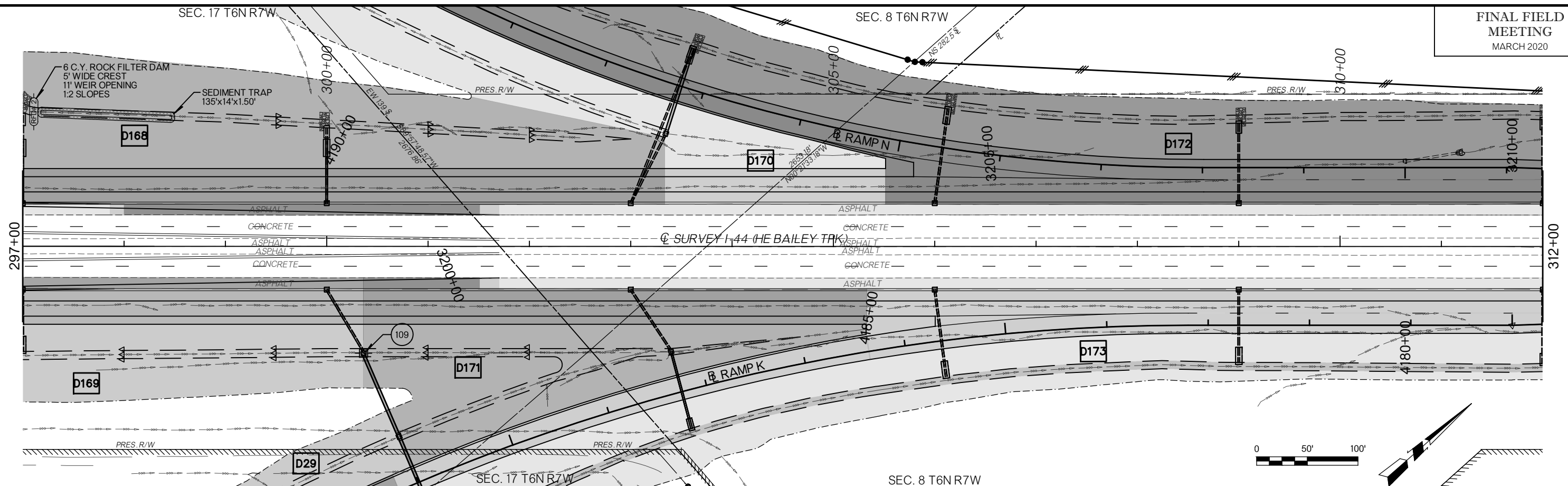


LEGEND

| | |
|-----------------------|-------|
| TEMP. SILT DIKE | ▲▲▲ |
| TEMP. SILT FENCE | XXXXX |
| TEMP. SEDIMENT FILTER | □ |
| TEMP. FIBER LOG | ▬ |
| TEMP. SEDIMENT BASIN | ○ |
| DITCH/PAVED DITCH | ▬▬▬ |
| RIPRAP | ▨ |
| DRAINAGE DIRECTION | ▬▬▬ |
| ROCK FILTER DAM | RFD |

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL
I-44

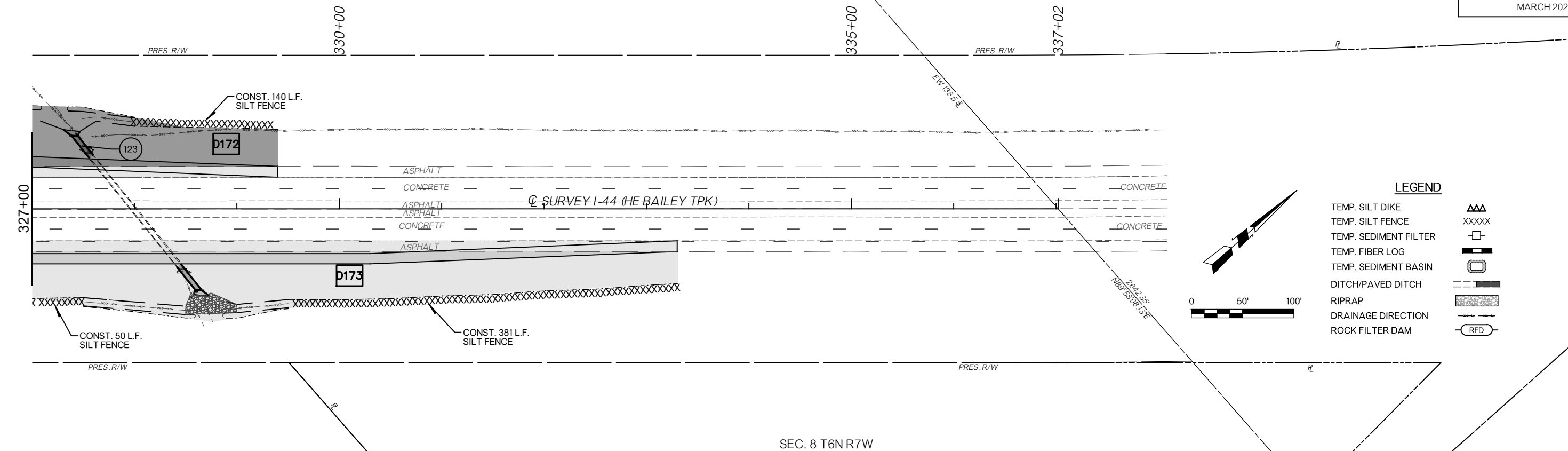


| LEGEND | |
|-----------------------|-------|
| TEMP. SILT DIKE | ▲▲▲ |
| TEMP. SILT FENCE | XXXXX |
| TEMP. SEDIMENT FILTER | □ |
| TEMP. FIBER LOG | — |
| TEMP. SEDIMENT BASIN | ○ |
| DITCH/PAVED DITCH | — |
| RIPRAP | ▨ |
| DRAINAGE DIRECTION | → |
| ROCK FILTER DAM | RFD |

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

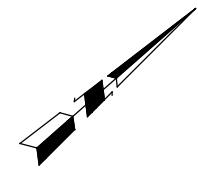
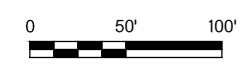
EROSION CONTROL
I-44

SEC. 8 T6N R7W



LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM

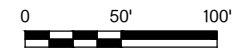


SEC. 8 T6N R7W

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

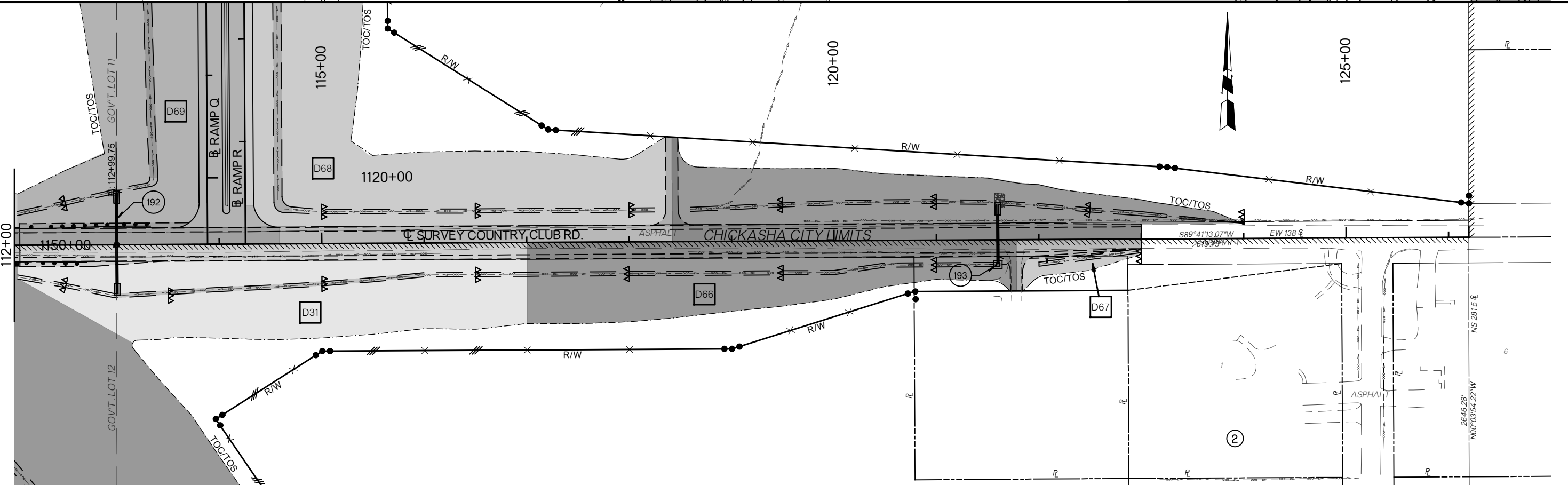
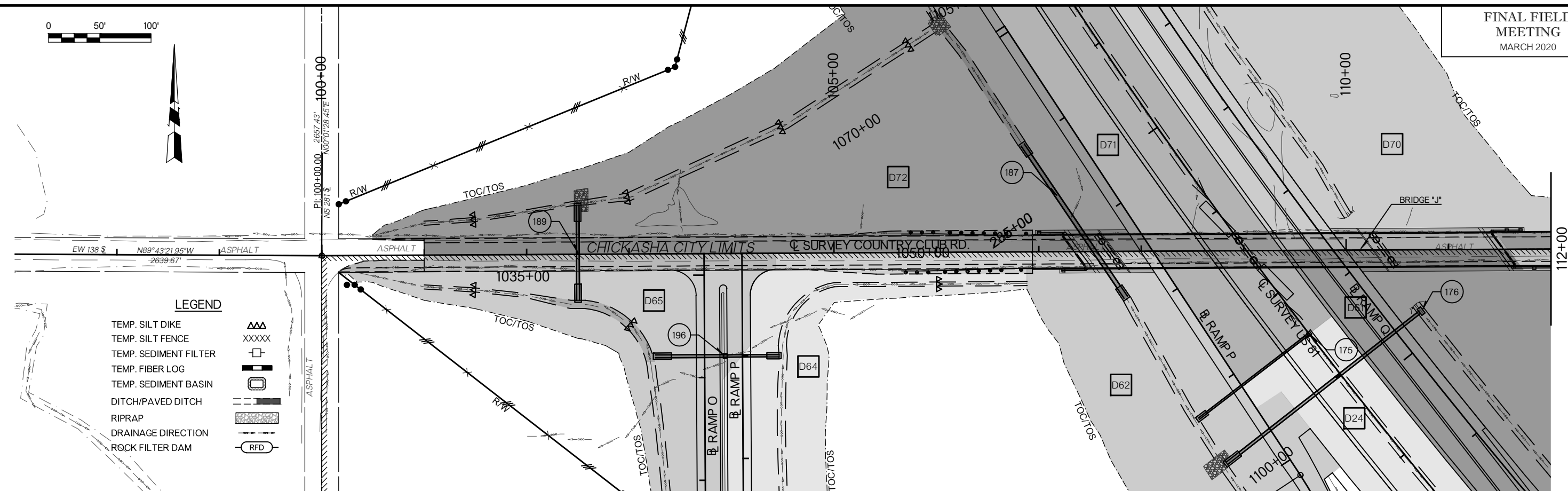
EROSION CONTROL I-44
State Job No. 24428(04) Sheet No. R044

US 81 REALIGNMENT
GRADY COUNTY



LEGEND

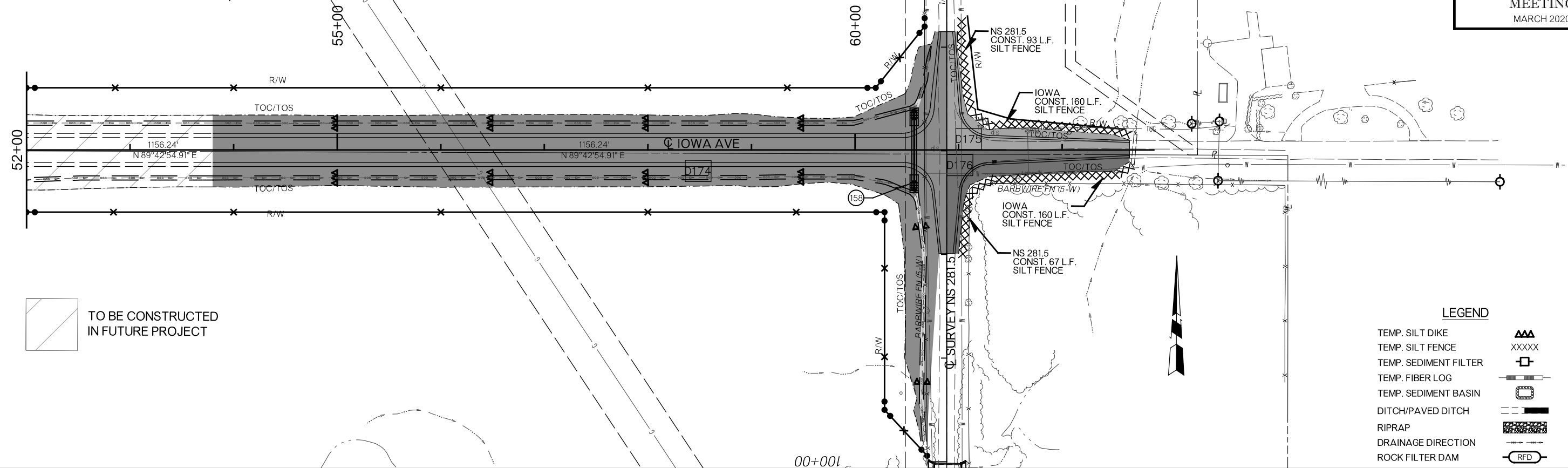
- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

**EROSION CONTROL
COUNTRY CLUB RD**

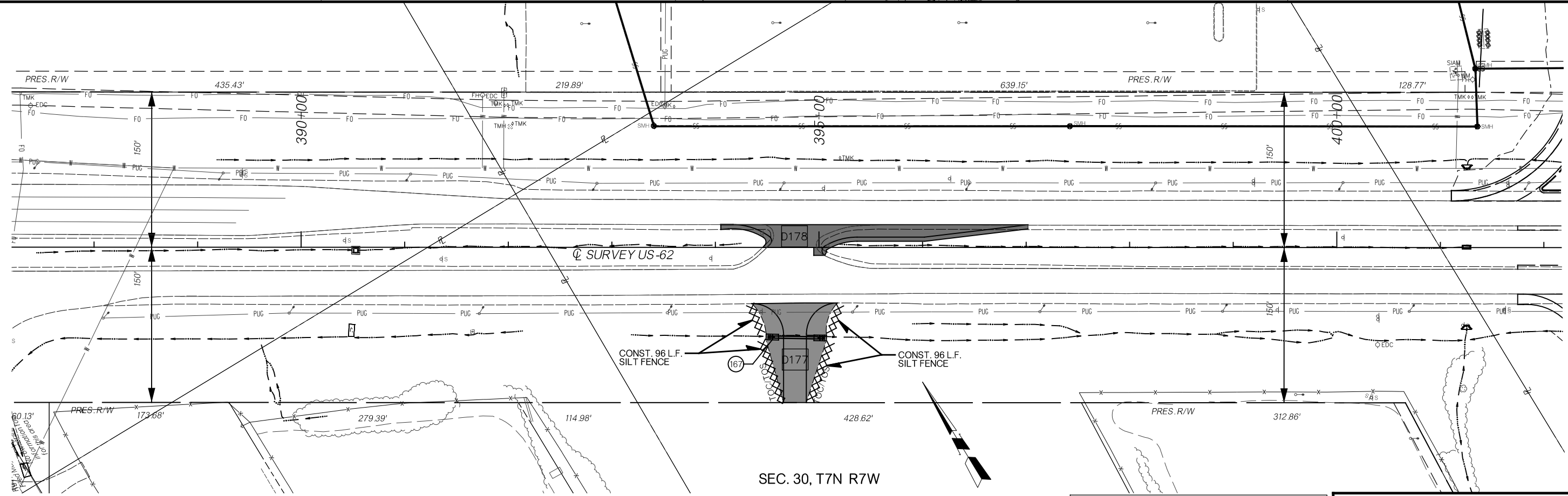
SEC. 25, T7N R8W



TO BE CONSTRUCTED
IN FUTURE PROJECT

LEGEND

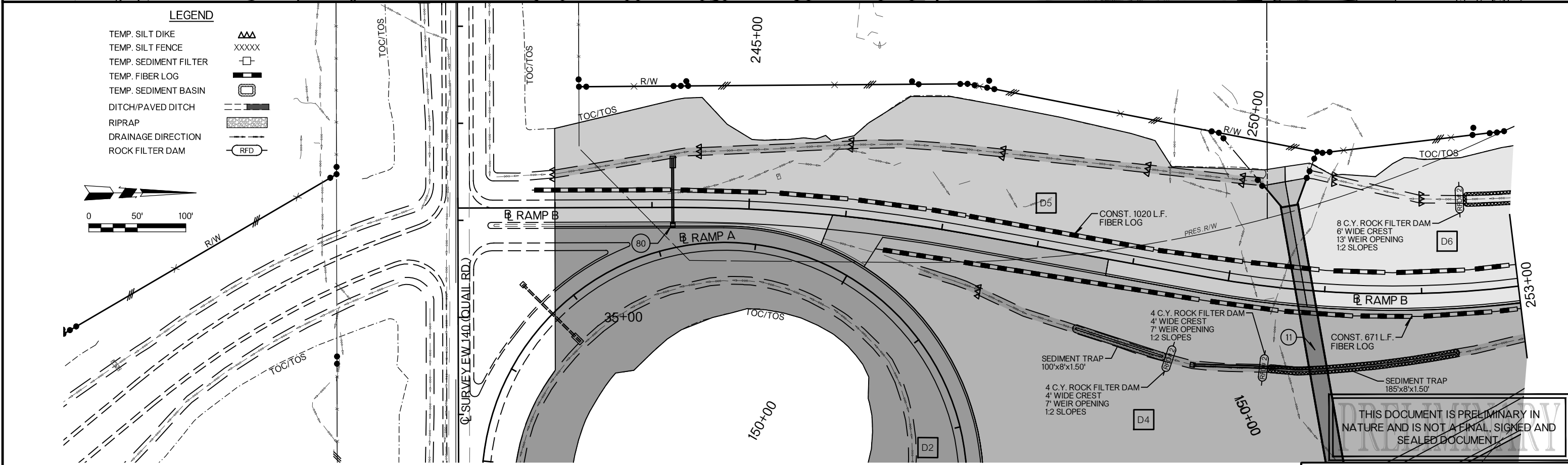
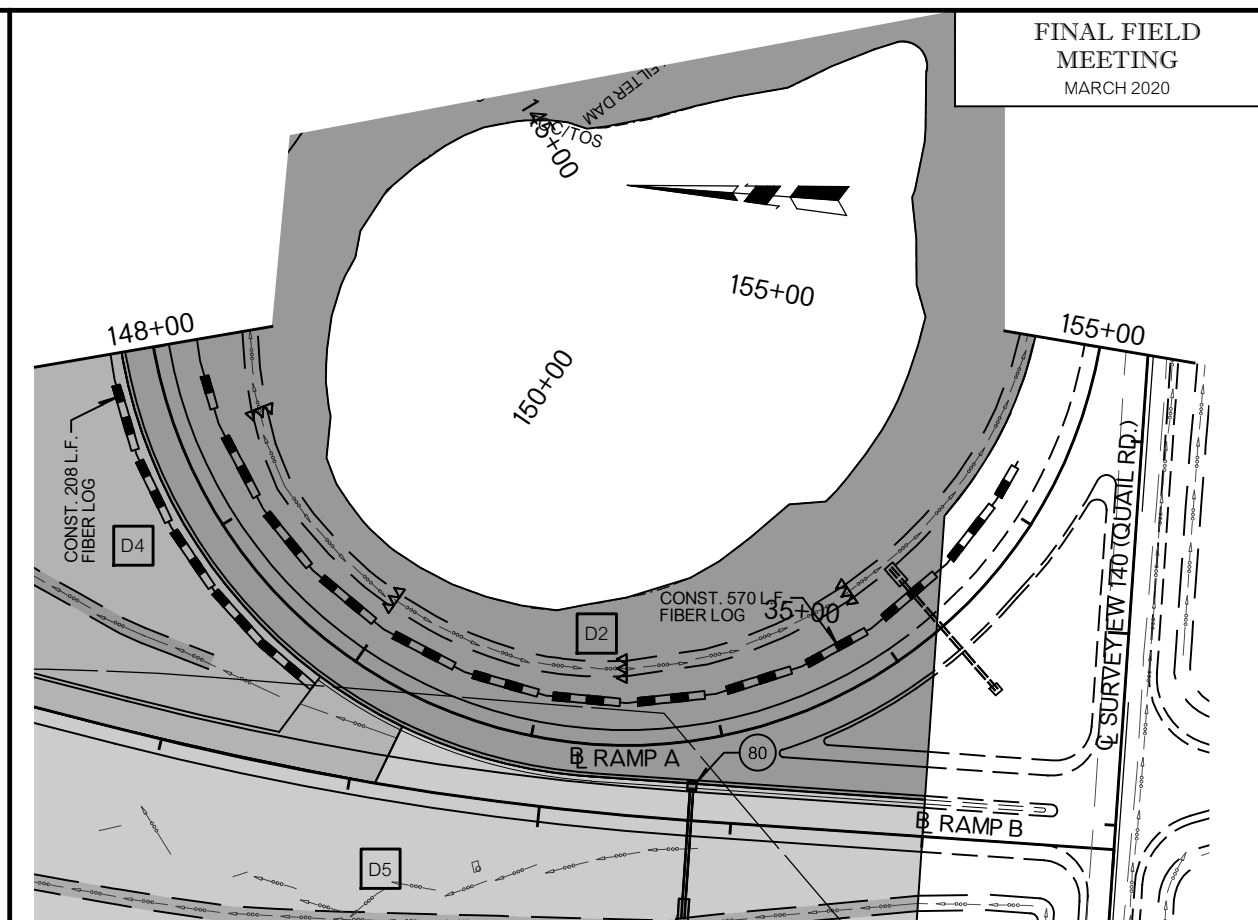
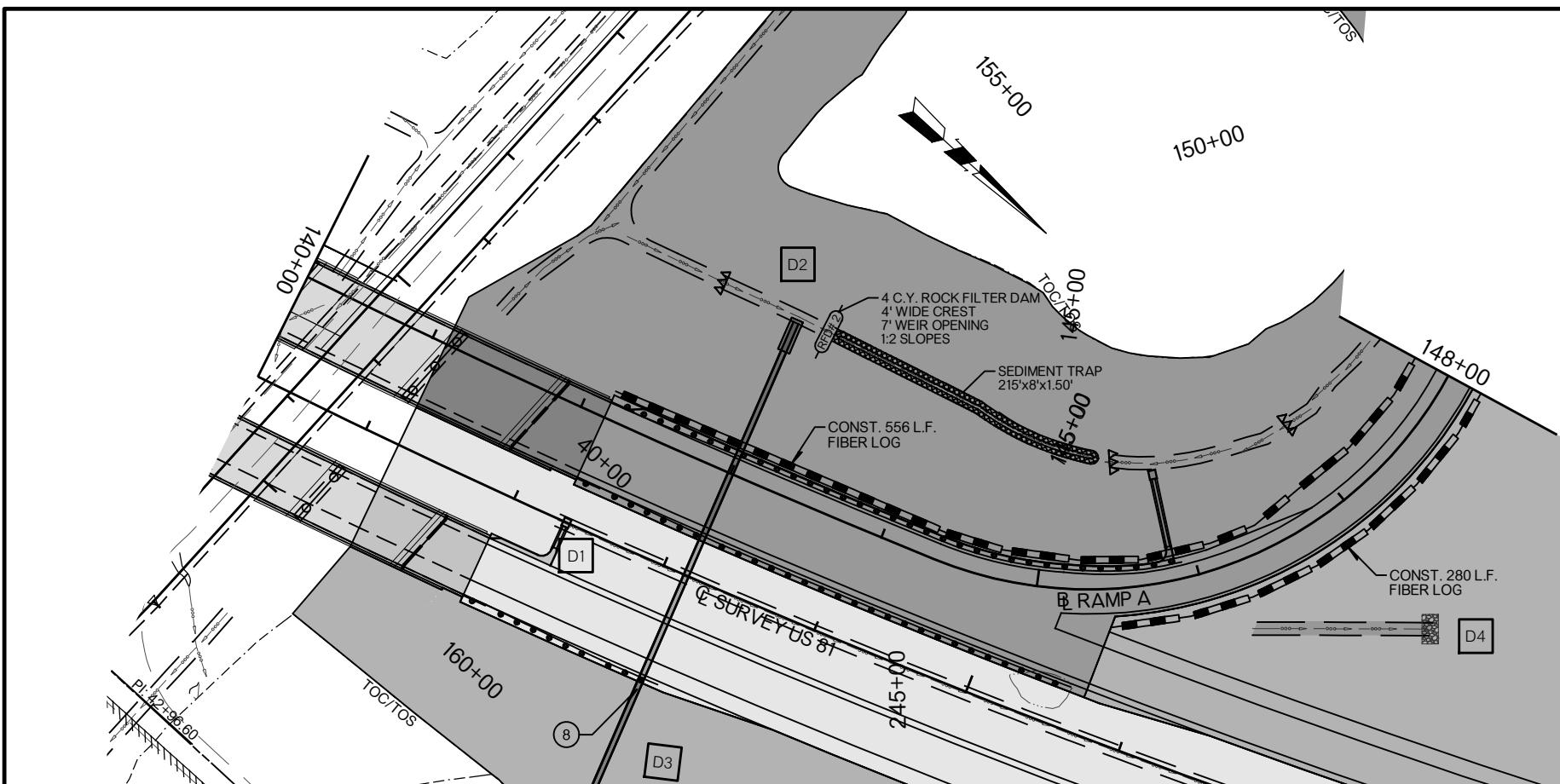
- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM



SEC. 30, T7N R7W

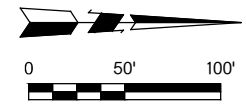
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL
IOWA AVE. AND US 62

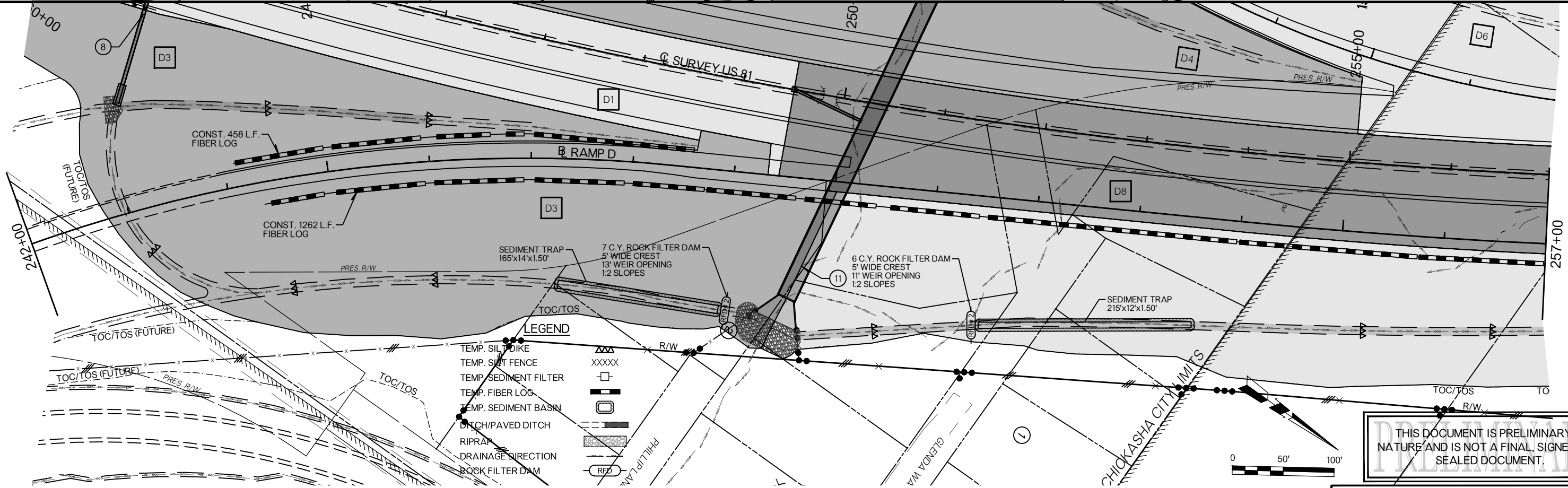
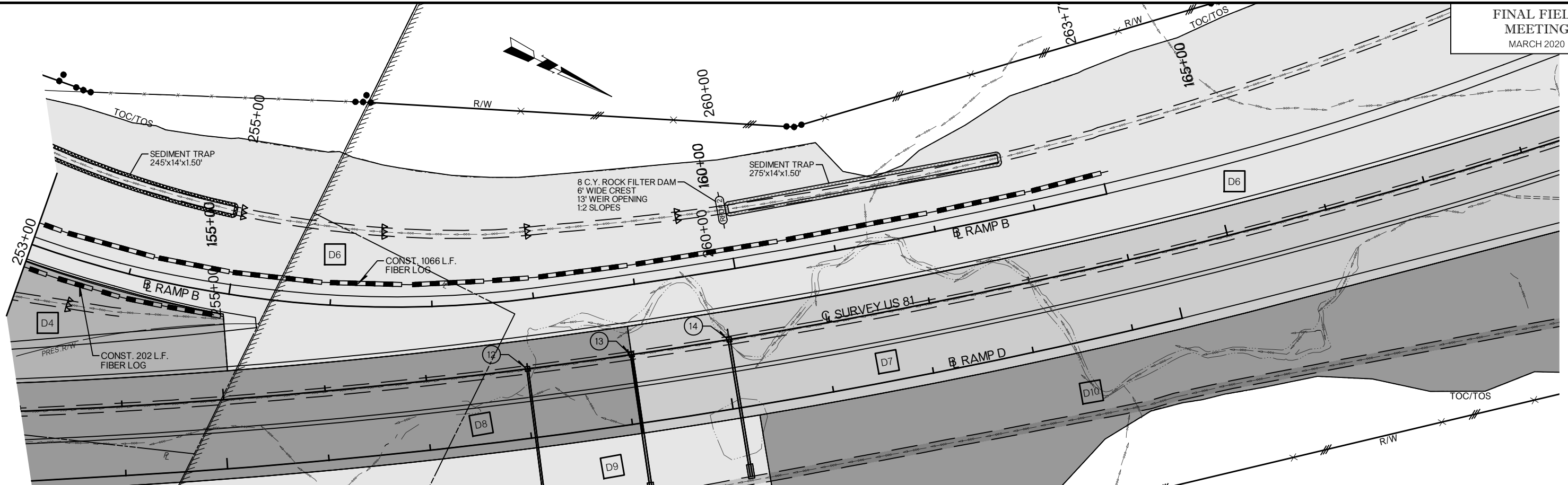


LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

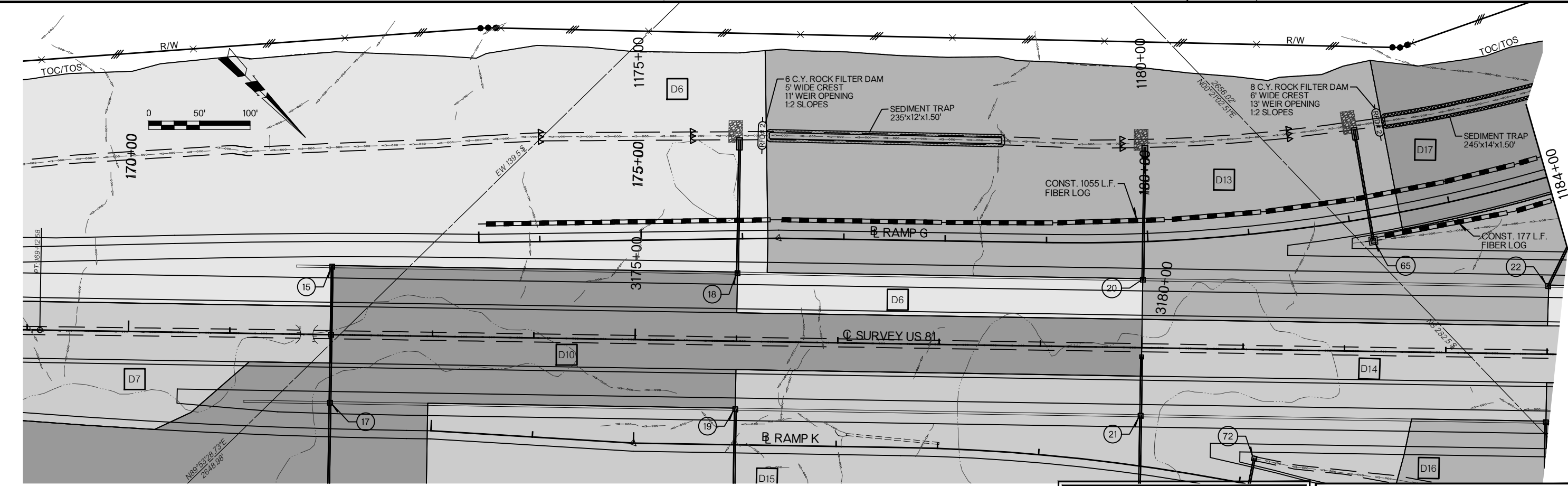
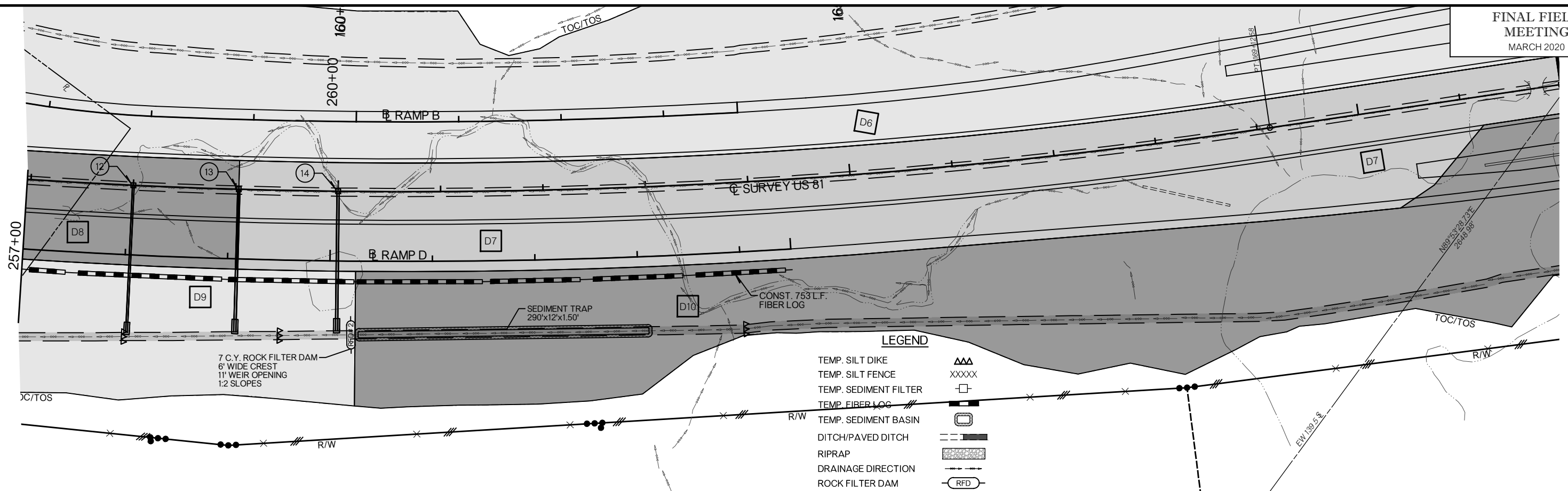


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL RAMPS

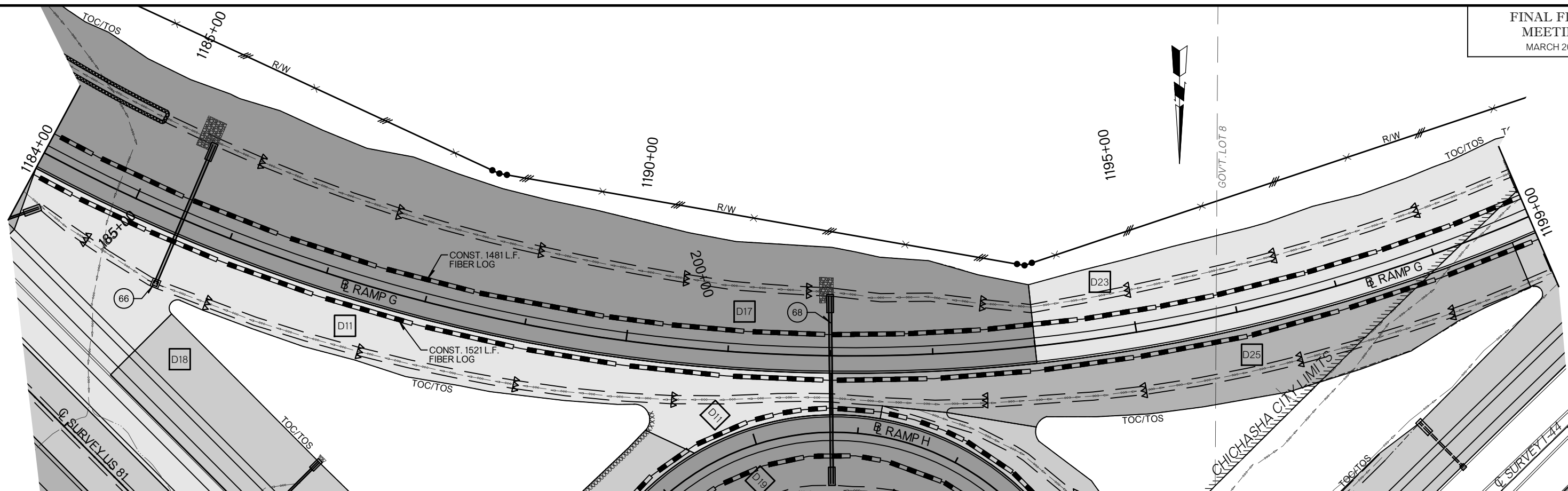
State Job No. 24428(04) Sheet No. R048

US 81 REALIGNMENT GRADY COUNTY



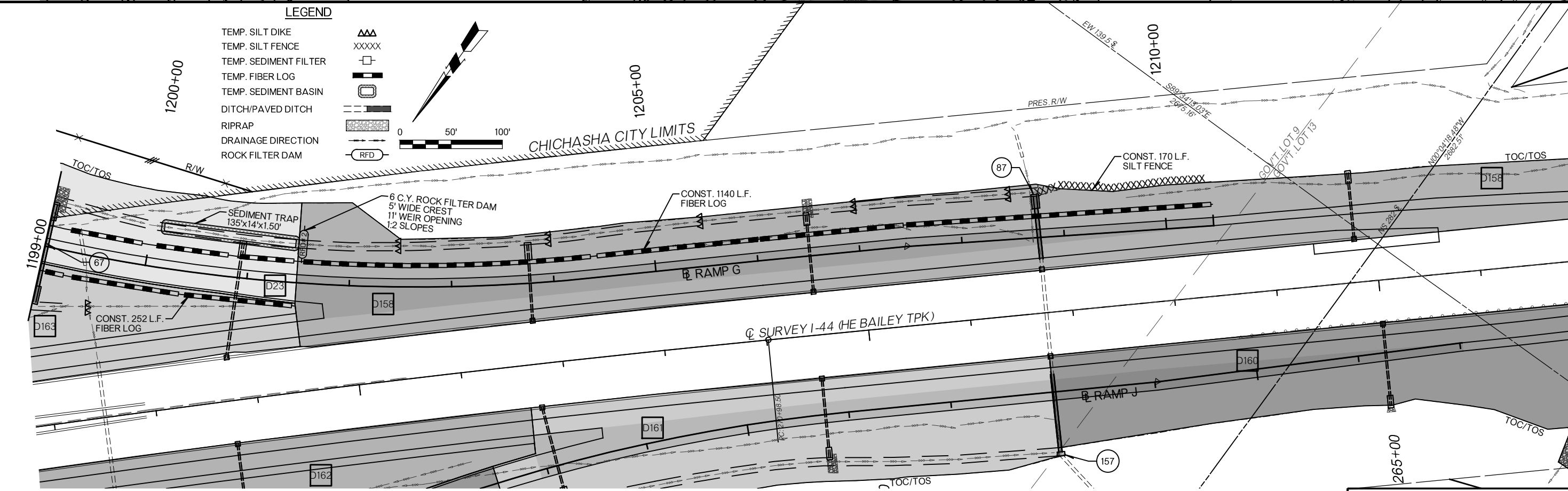
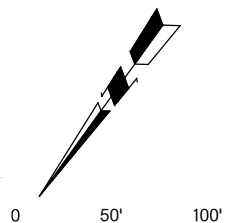
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL RAMPS



LEGEND

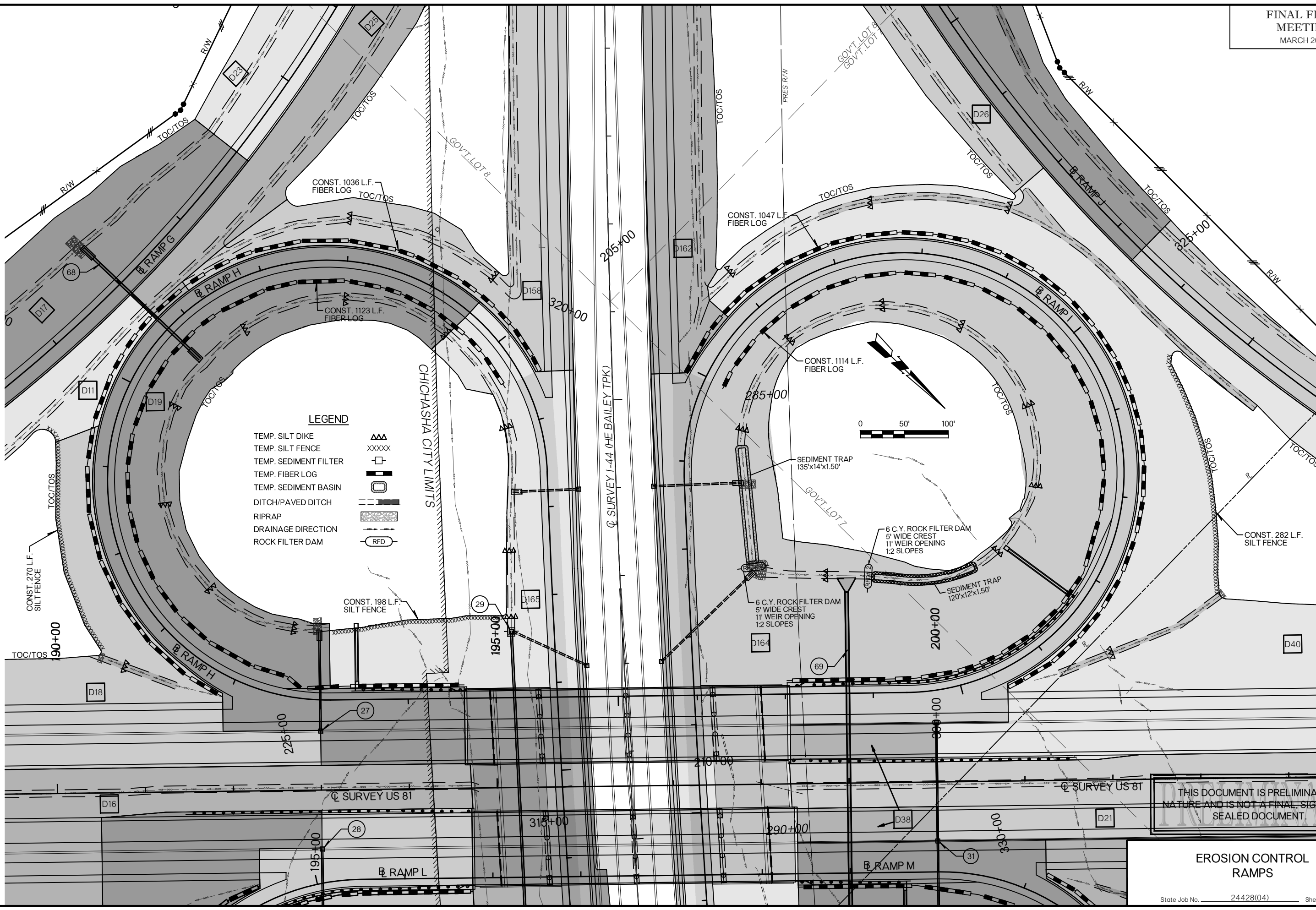
- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

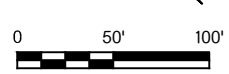
EROSION CONTROL RAMPS

State Job No. 24428(04) Sheet No. R050



LEGEND

| | |
|-----------------------|-------|
| TEMP. SILT DIKE | ▲▲▲ |
| TEMP. SILT FENCE | XXXXX |
| TEMP. SEDIMENT FILTER | □ |
| TEMP. FIBER LOG | ▬ |
| TEMP. SEDIMENT BASIN | ○ |
| DITCH/PAVED DITCH | ▬▬▬ |
| RIPRAP | ▨ |
| DRAINAGE DIRECTION | → |
| ROCK FILTER DAM | RFD |












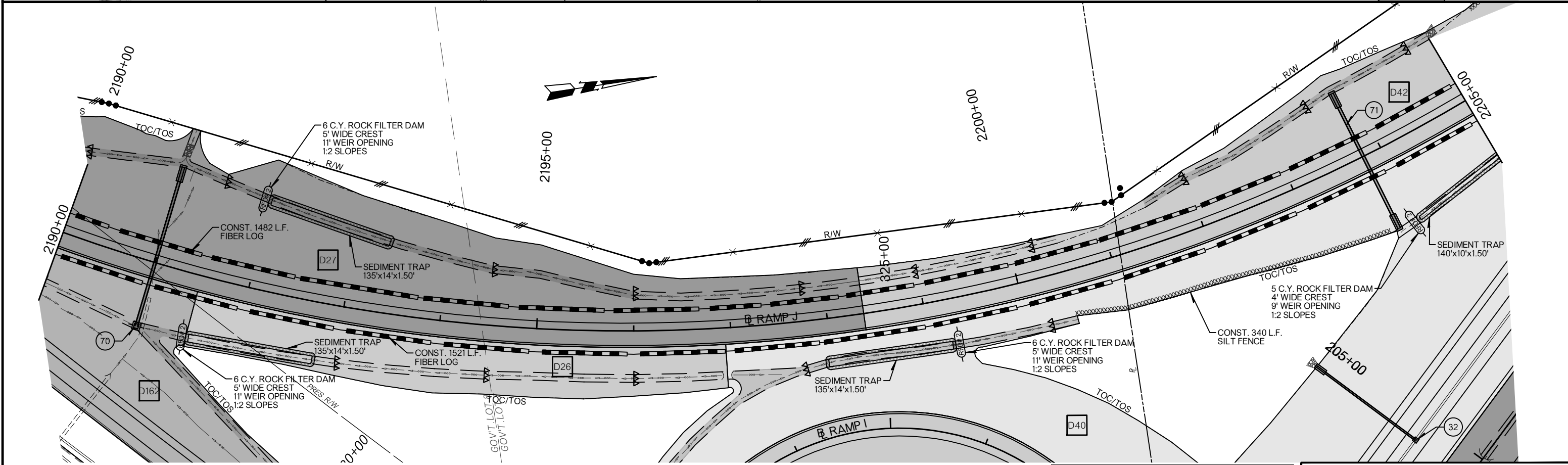
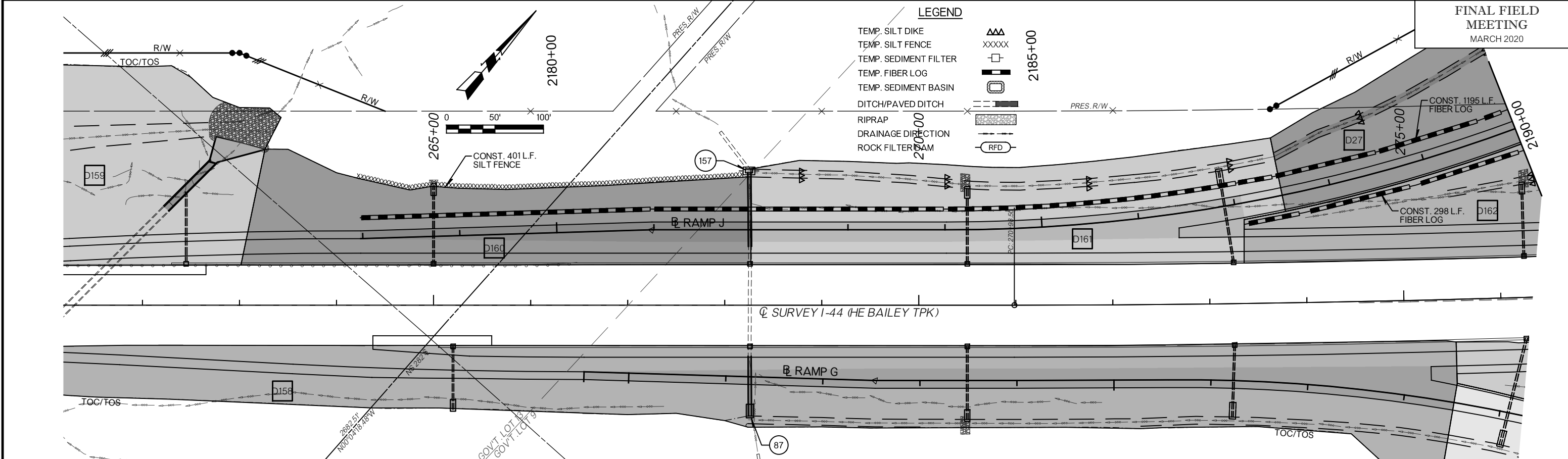
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL RAMPS

US 81 REALIGNMENT GRADY COUNTY

LEGEND

- TEMP. SILT DIKE 
- TEMP. SILT FENCE 
- TEMP. SEDIMENT FILTER 
- TEMP. FIBER LOG 
- TEMP. SEDIMENT BASIN 
- DITCH/PAVED DITCH 
- RIPRAP 
- DRAINAGE DIRECTION 
- ROCK FILTER DAM 



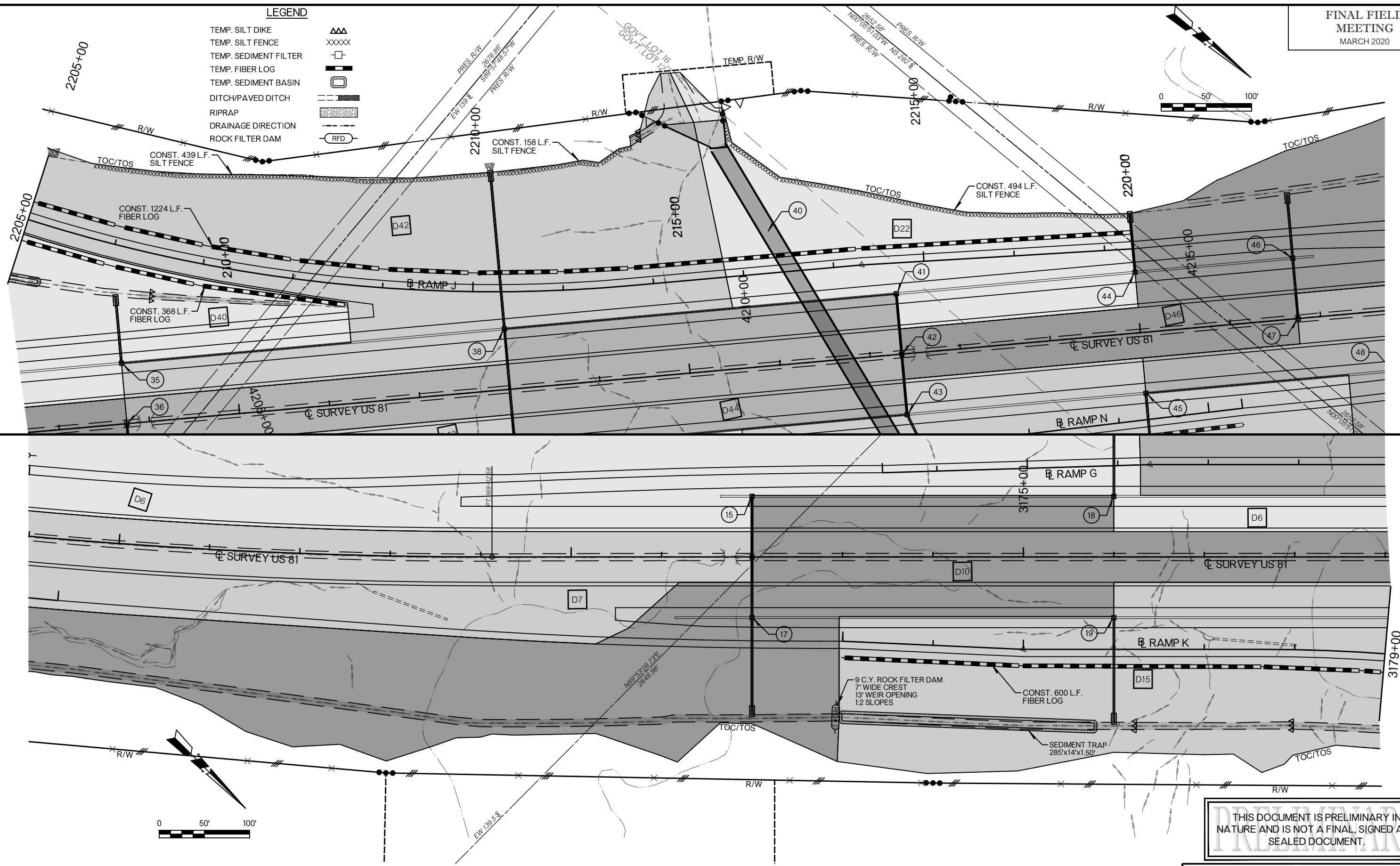
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL RAMP

LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM

- RFD

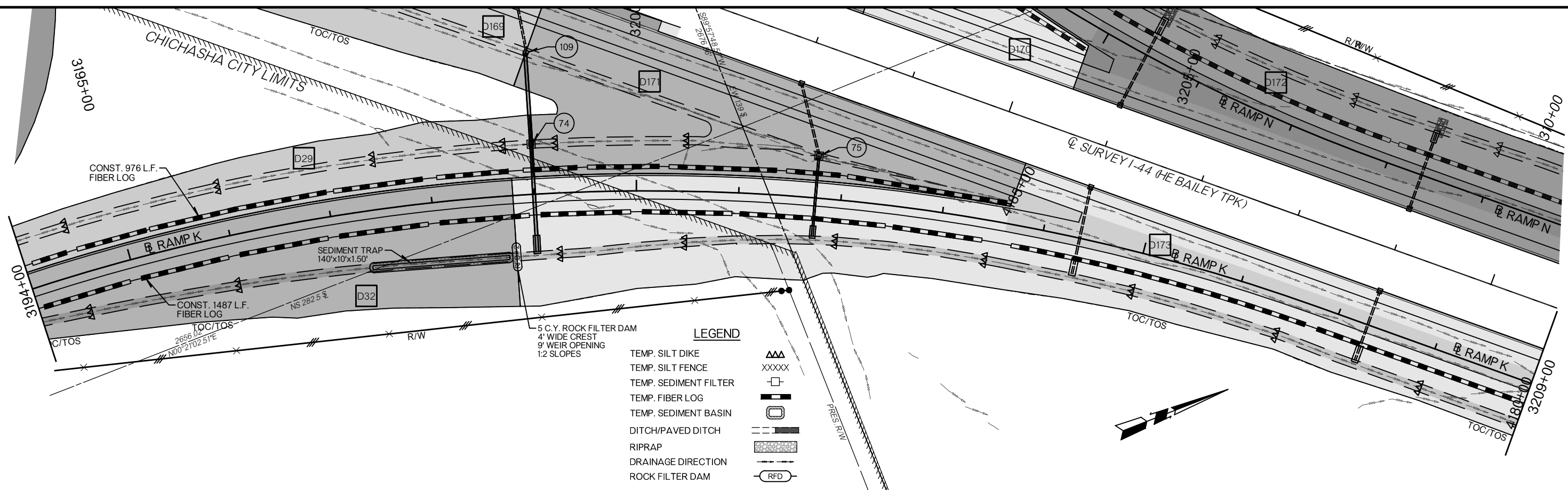
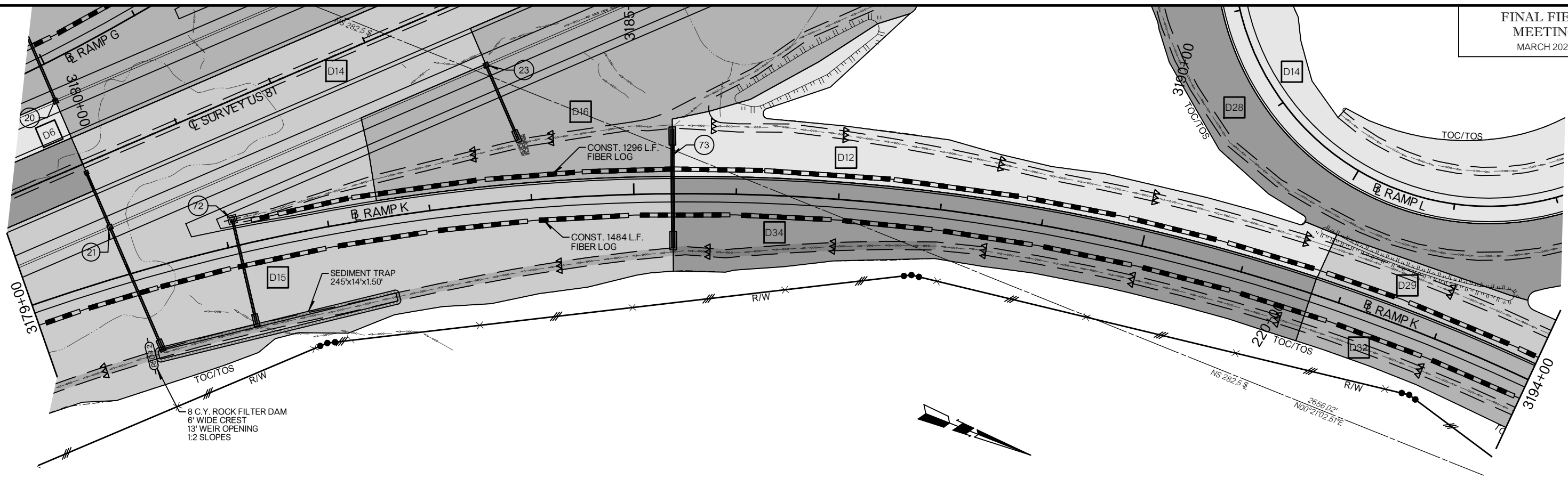


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL RAMPS

State Job No. 24428(04) Sheet No. R053

GRADY COUNTY US 81 REALIGNMENT



LEGEND

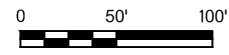
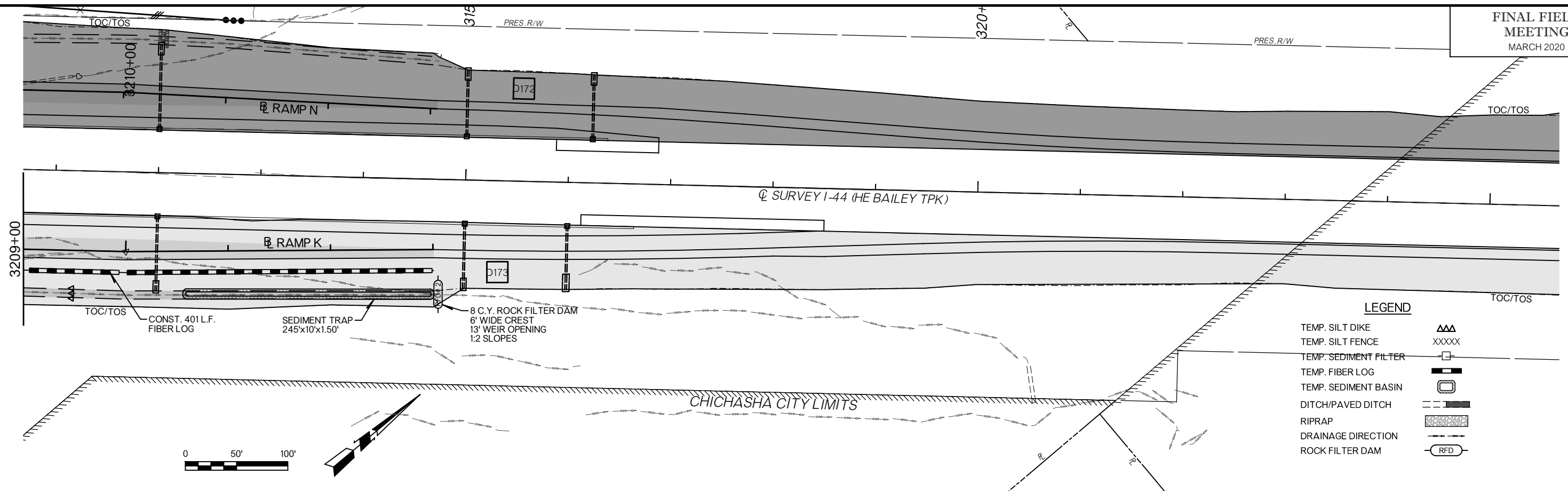
| | |
|-----------------------|-------|
| TEMP. SILT DIKE | ▲▲▲ |
| TEMP. SILT FENCE | XXXXX |
| TEMP. SEDIMENT FILTER | □ |
| TEMP. FIBER LOG | ▬ |
| TEMP. SEDIMENT BASIN | ○ |
| DITCH/PAVED DITCH | ▬▬▬ |
| RIPRAP | ▨ |
| DRAINAGE DIRECTION | → |
| ROCK FILTER DAM | RFD |

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL RAMPS

State Job No. 24428(04) Sheet No. R054

GRADY COUNTY US 81 REALIGNMENT

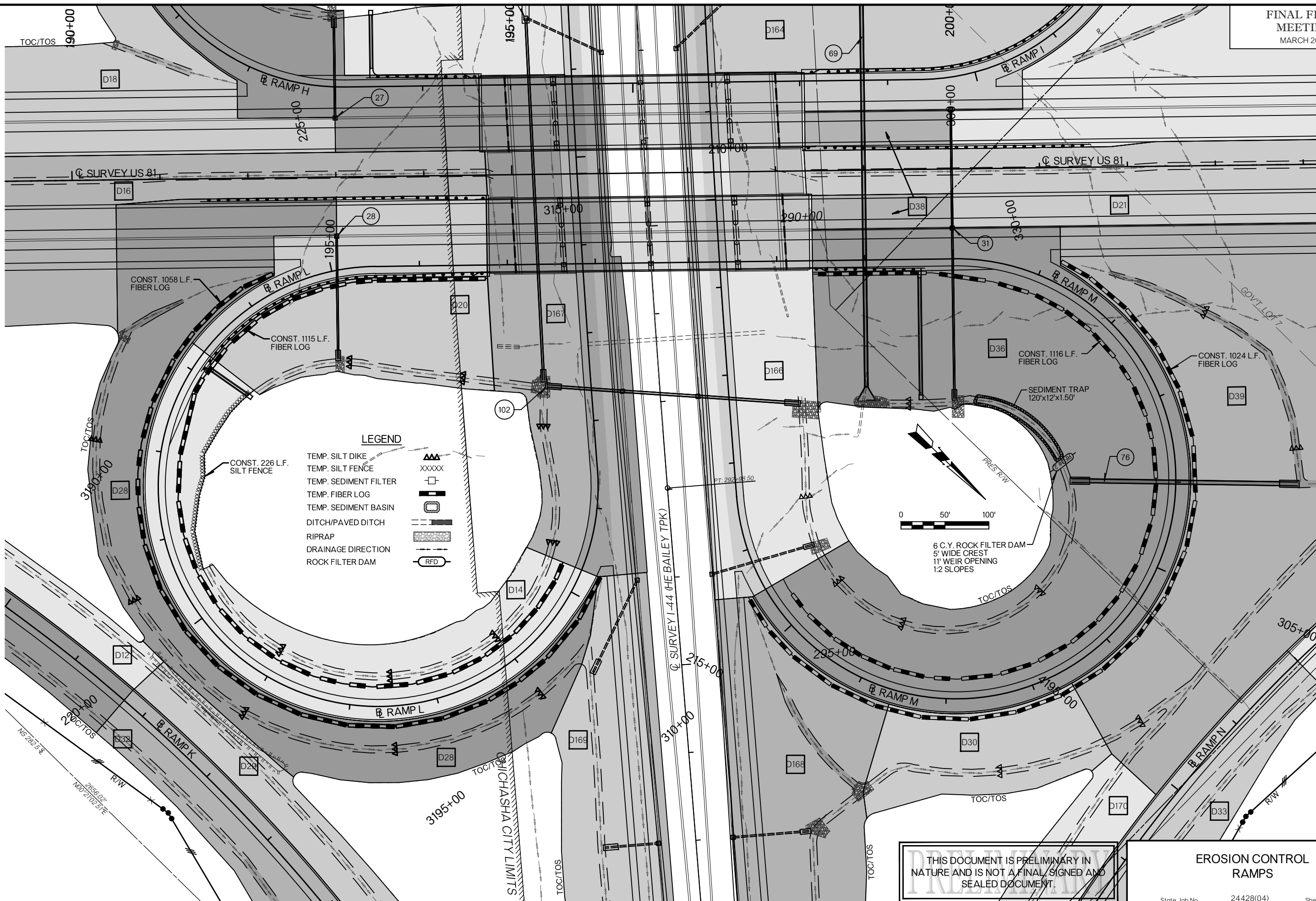


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL RAMPS

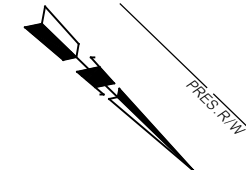
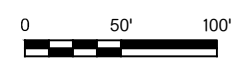
State Job No. 24428(04) Sheet No. R055

US 81 REALIGNMENT
GRADY COUNTY



LEGEND

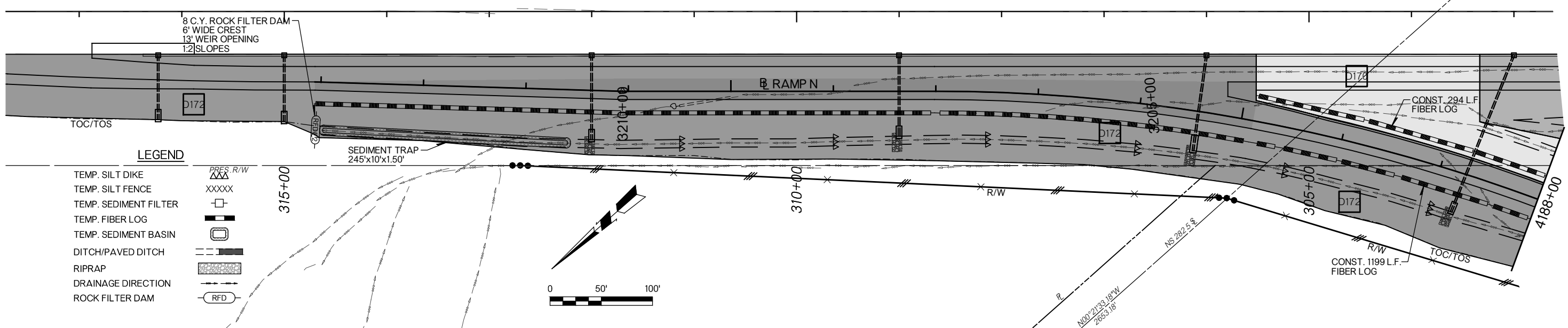
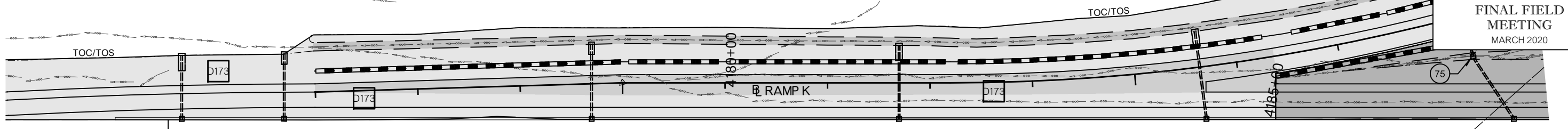
| | |
|-----------------------|--|
| TEMP. SILT DIKE | |
| TEMP. SILT FENCE | |
| TEMP. SEDIMENT FILTER | |
| TEMP. FIBER LOG | |
| TEMP. SEDIMENT BASIN | |
| DITCH/PAVED DITCH | |
| RIPRAP | |
| DRAINAGE DIRECTION | |
| ROCK FILTER DAM | |



6 C.Y. ROCK FILTER DAM
5' WIDE CREST
11' WEIR OPENING
1:2 SLOPES

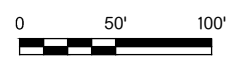
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL RAMP



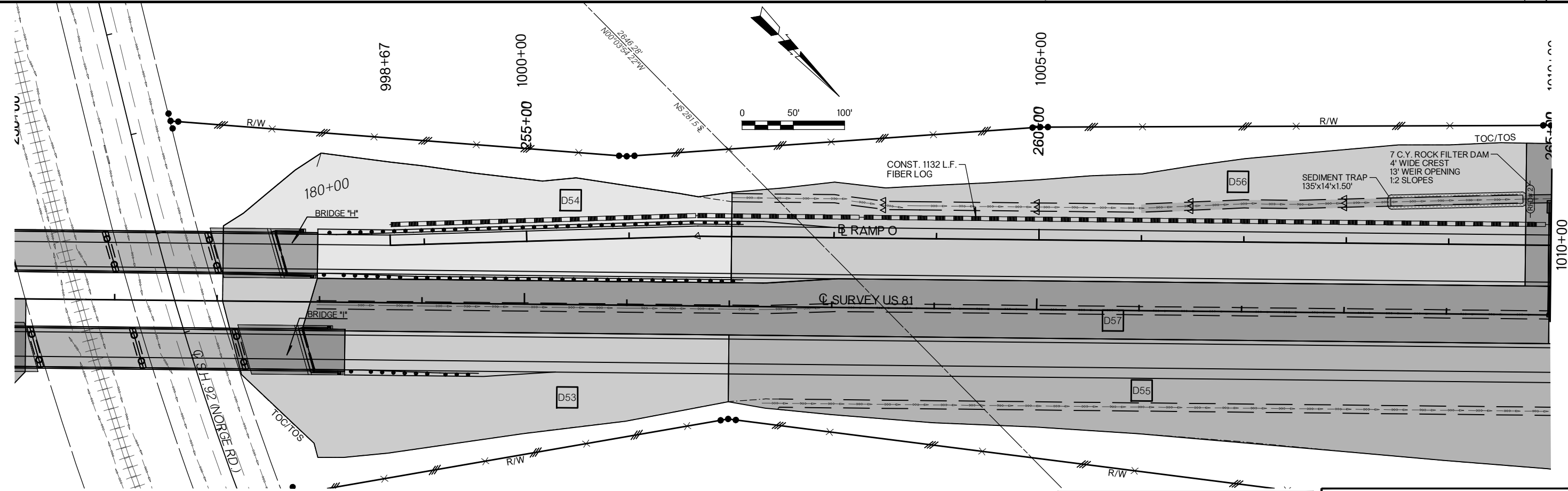
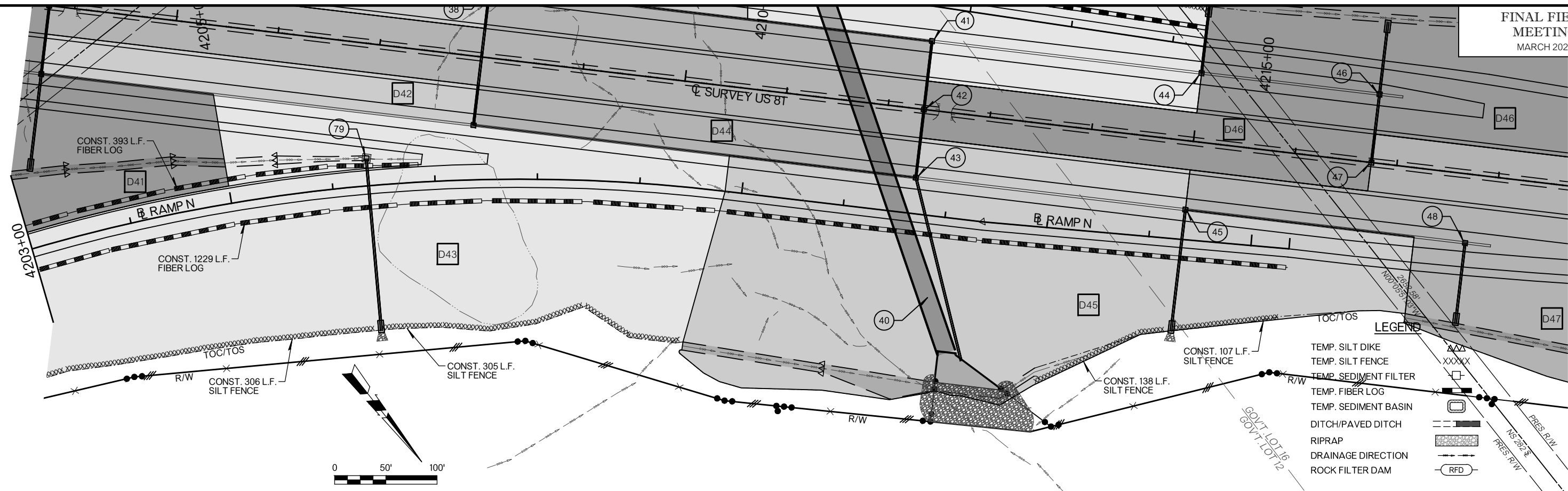
LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL RAMPS



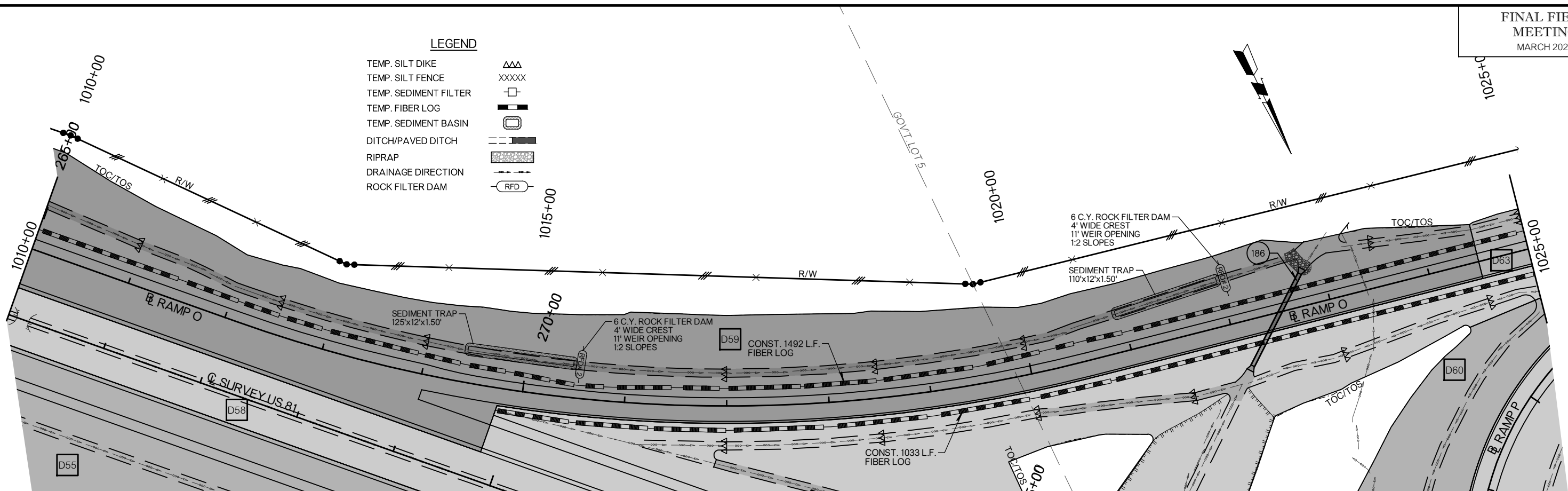
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL RAMPS

State Job No. 24428(04) Sheet No. R058

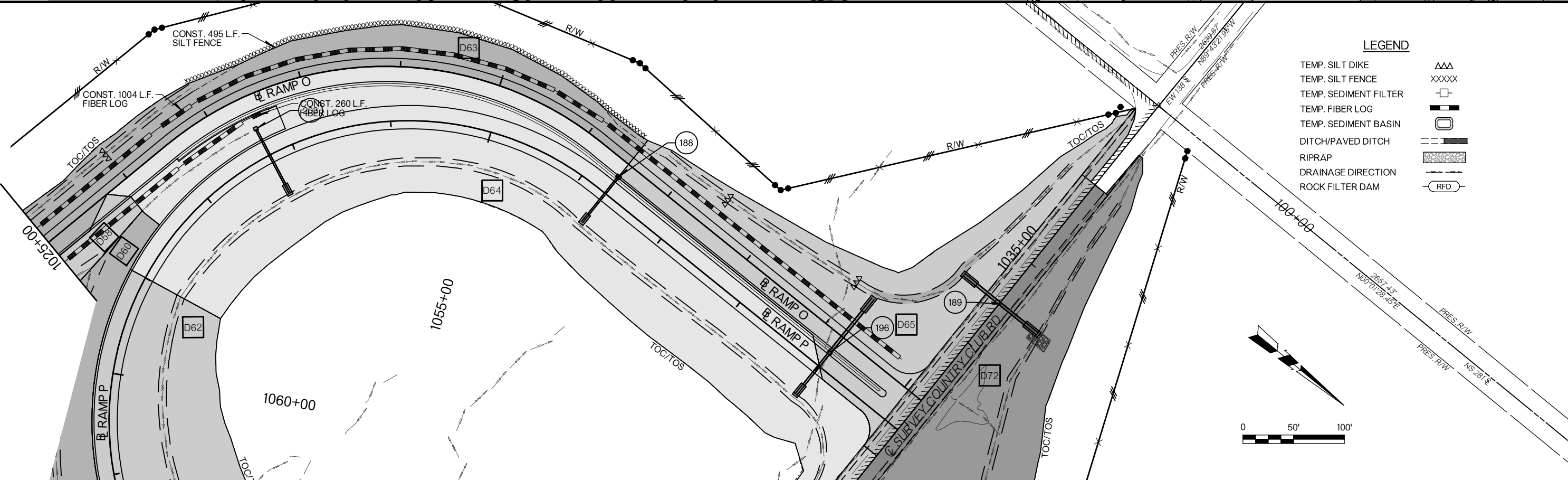
LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM



LEGEND

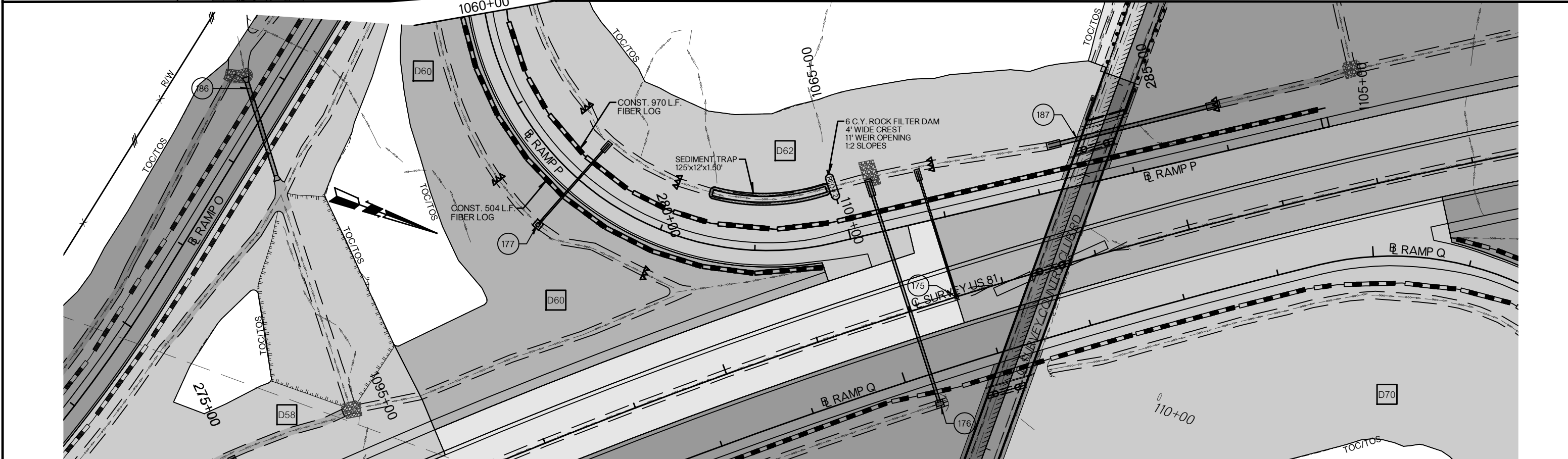
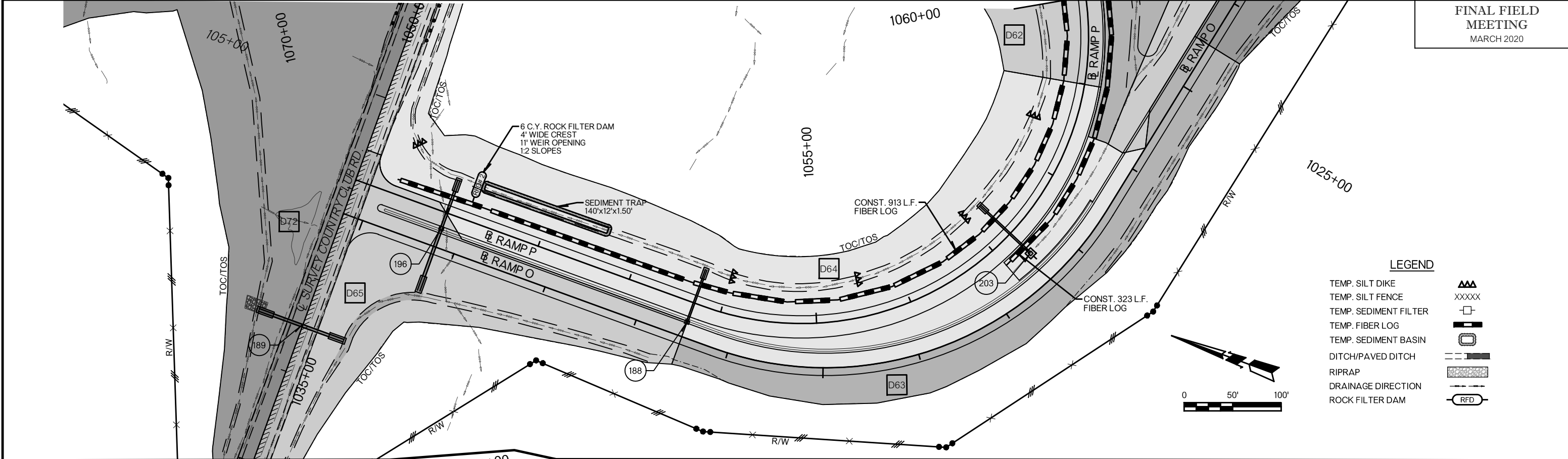
- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL RAMPS
State Job No. 24428(04) Sheet No. R059

GRADY COUNTY US 81 REALIGNMENT

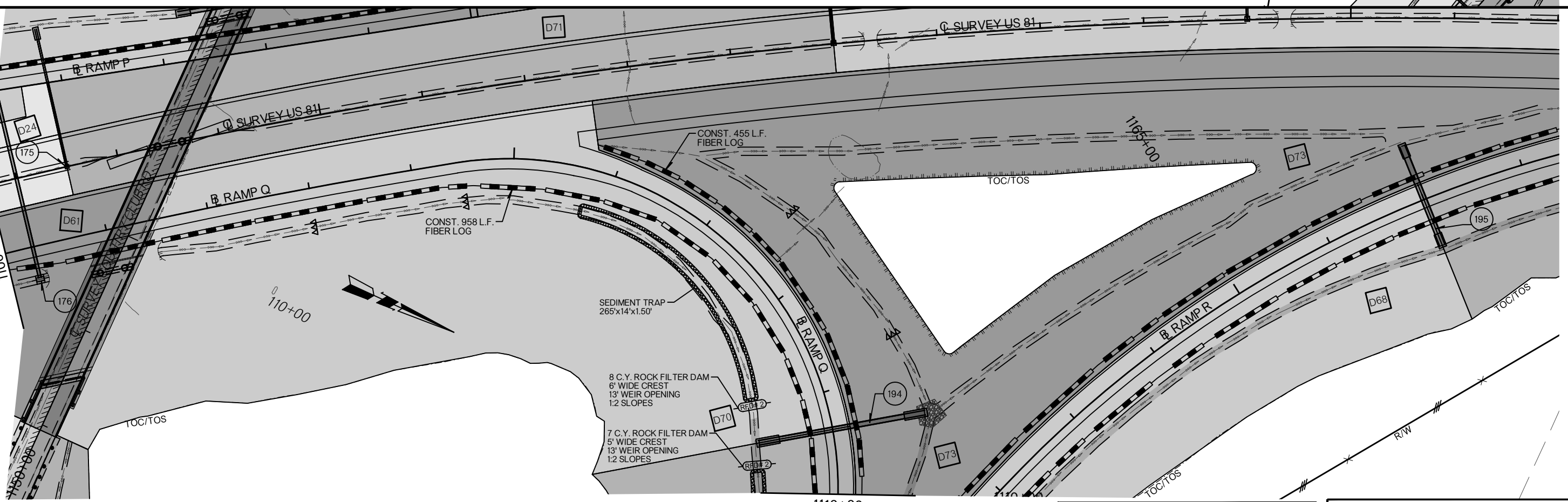
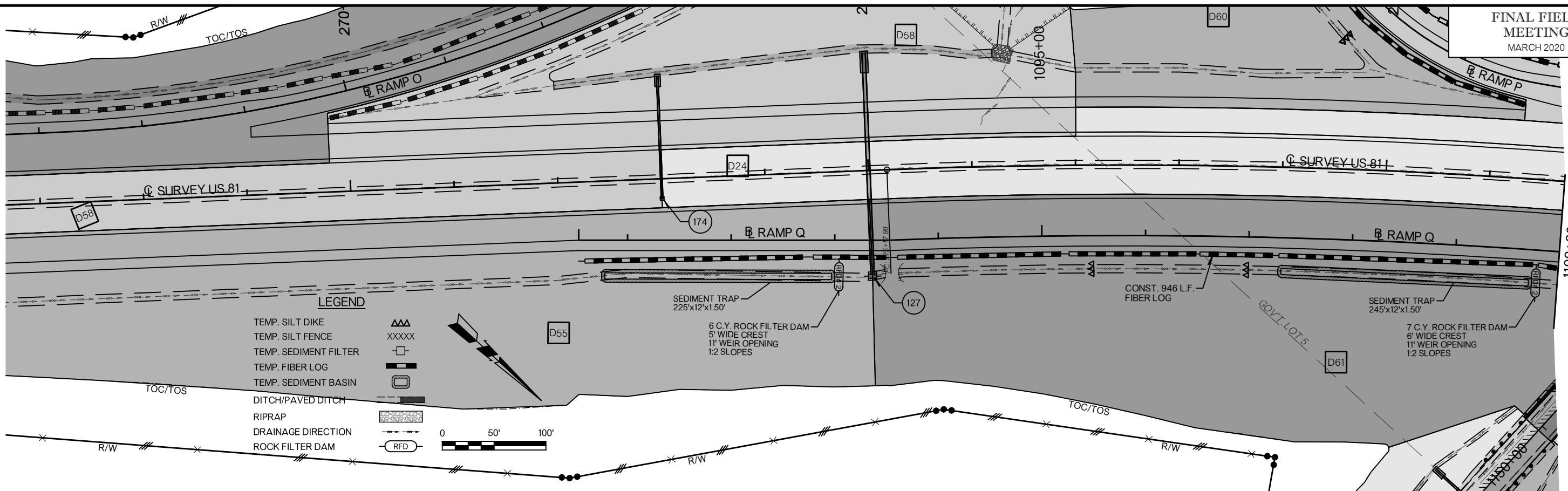


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL RAMPS

State Job No. 24428(04) Sheet No. R060

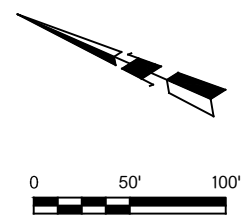
GRADY COUNTY US 81 REALIGNMENT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

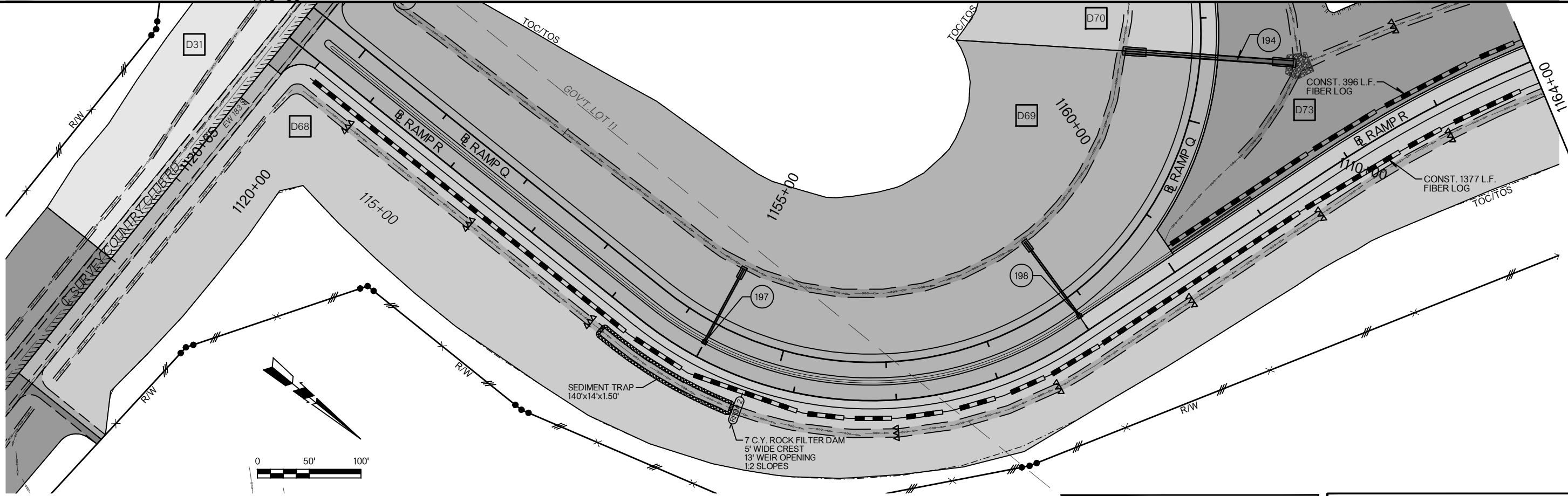
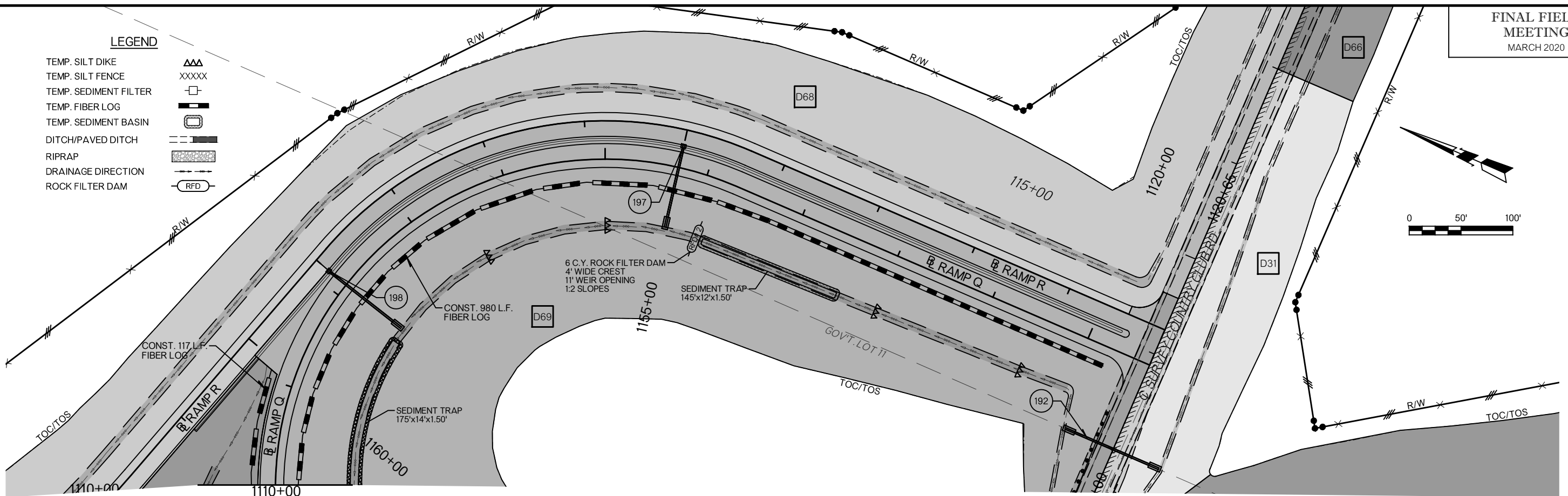
EROSION CONTROL RAMPS
State Job No. 24428(04) Sheet No. R061

GRADY COUNTY US 81 REALIGNMENT



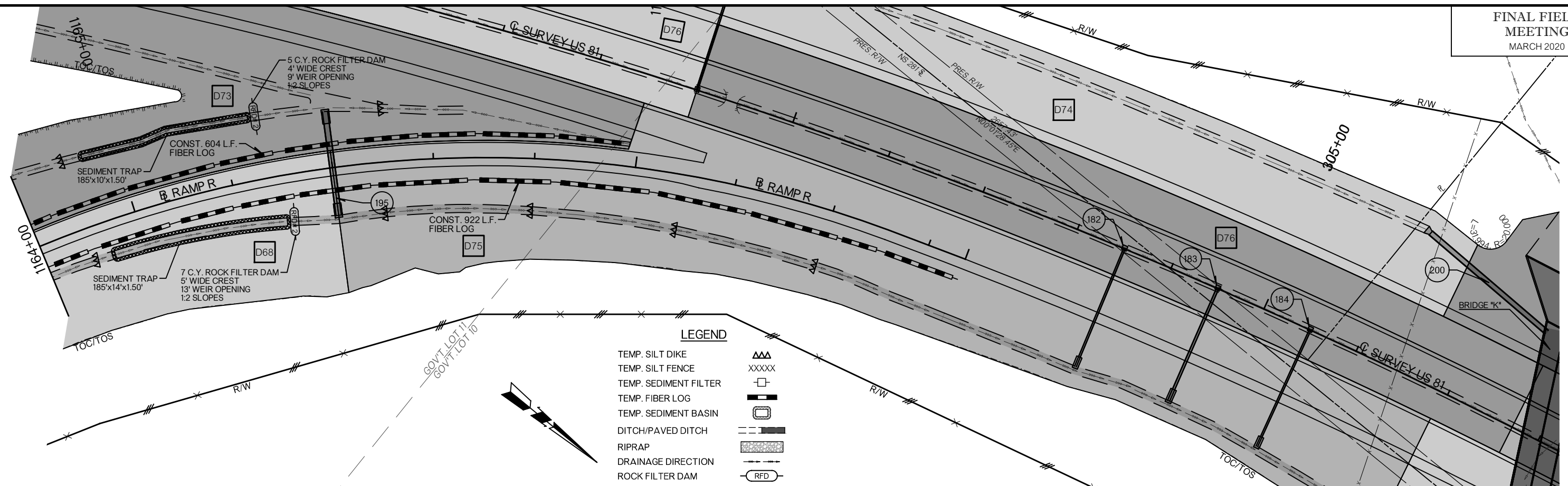
LEGEND

- TEMP. SILT DIKE
- TEMP. SILT FENCE
- TEMP. SEDIMENT FILTER
- TEMP. FIBER LOG
- TEMP. SEDIMENT BASIN
- DITCH/PAVED DITCH
- RIPRAP
- DRAINAGE DIRECTION
- ROCK FILTER DAM












THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL RAMPS
State Job No. 24428(04) Sheet No. R062



LEGEND

- TEMP. SILT DIKE 
- TEMP. SILT FENCE 
- TEMP. SEDIMENT FILTER 
- TEMP. FIBER LOG 
- TEMP. SEDIMENT BASIN 
- DITCH/PAVED DITCH 
- RIPRAP 
- DRAINAGE DIRECTION 
- ROCK FILTER DAM 

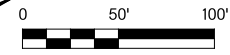
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

EROSION CONTROL RAMP

State Job No. 24428(04) Sheet No. R063

SEC. 20, T7N R7W

FINAL FIELD MEETING MARCH 2020

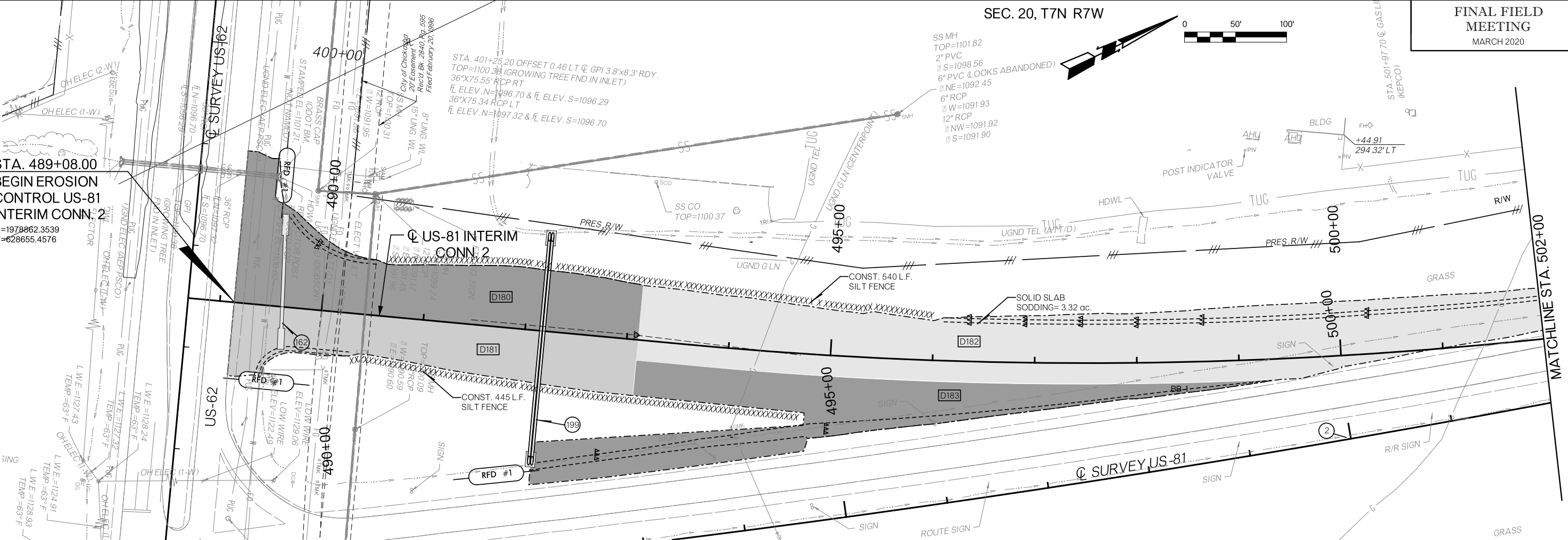


STA. 489+08.00
BEGIN EROSION CONTROL US-81 INTERIM CONN. 2
X=1978862.3539
Y=628655.4576

STA. 401+25.20 OFFSET 0.46 LT @ GPI 3.8'x8.3' RDY.
TOP=1100.38 (GROWING TREE FND IN INLET)
36"X75.55' RCP RT
FL ELEV. N=1096.70 & FL ELEV. S=1096.29
36"X75.34 RCP LT
FL ELEV. N=1097.32 & FL ELEV. S=1096.70

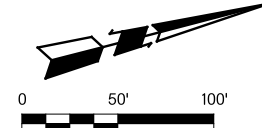
SS MH
TOP=1101.82
2" PVC
S=1098.56
6" PVC (LOOKS ABANDONED)
NE=1092.45
6" RCP
W=1091.93
12" RCP
NW=1091.92
S=1091.90

STA. 501+97.70 @ GAS LN (KEPCO)



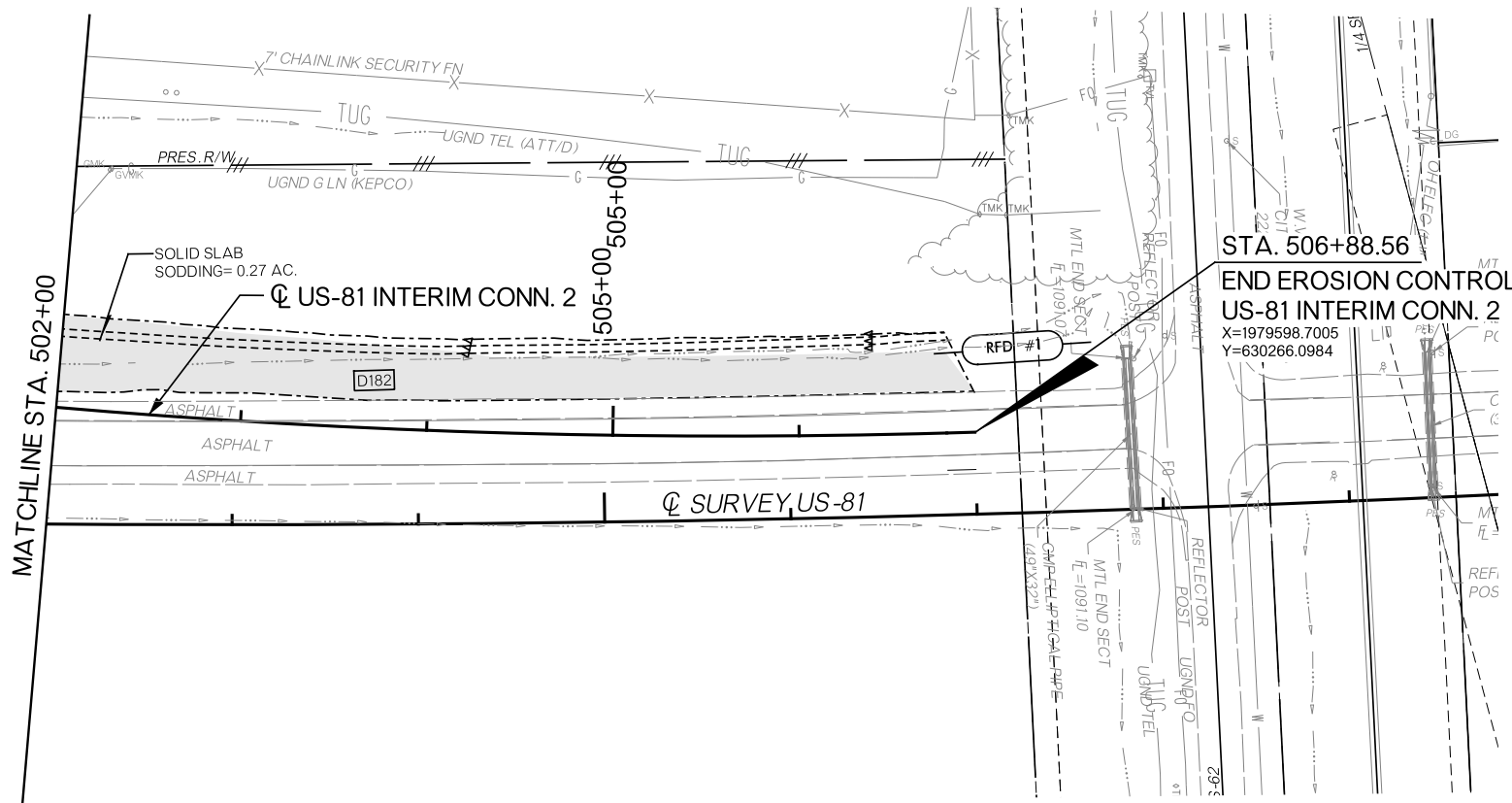
MATCHLINE STA. 502+00

STA. 506+88.56
END EROSION CONTROL US-81 INTERIM CONN. 2
X=1979598.7005
Y=630266.0984



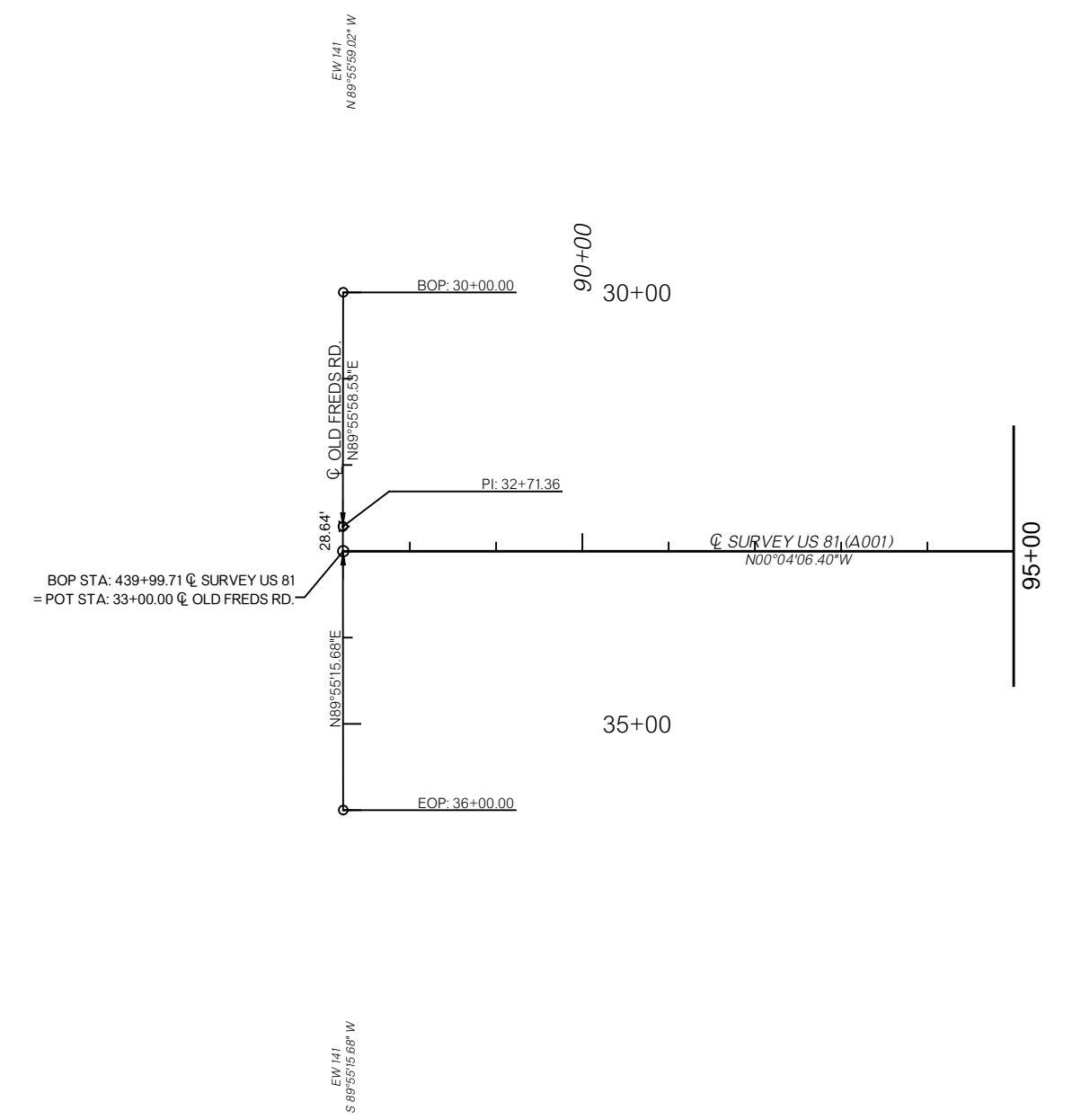
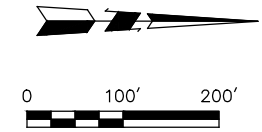
LEGEND:

- SOLID SLAB SODDING
- TEMP. SILT DIKE
- ROCK FILTER DAM
- TEMP. SILT FENCE



PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

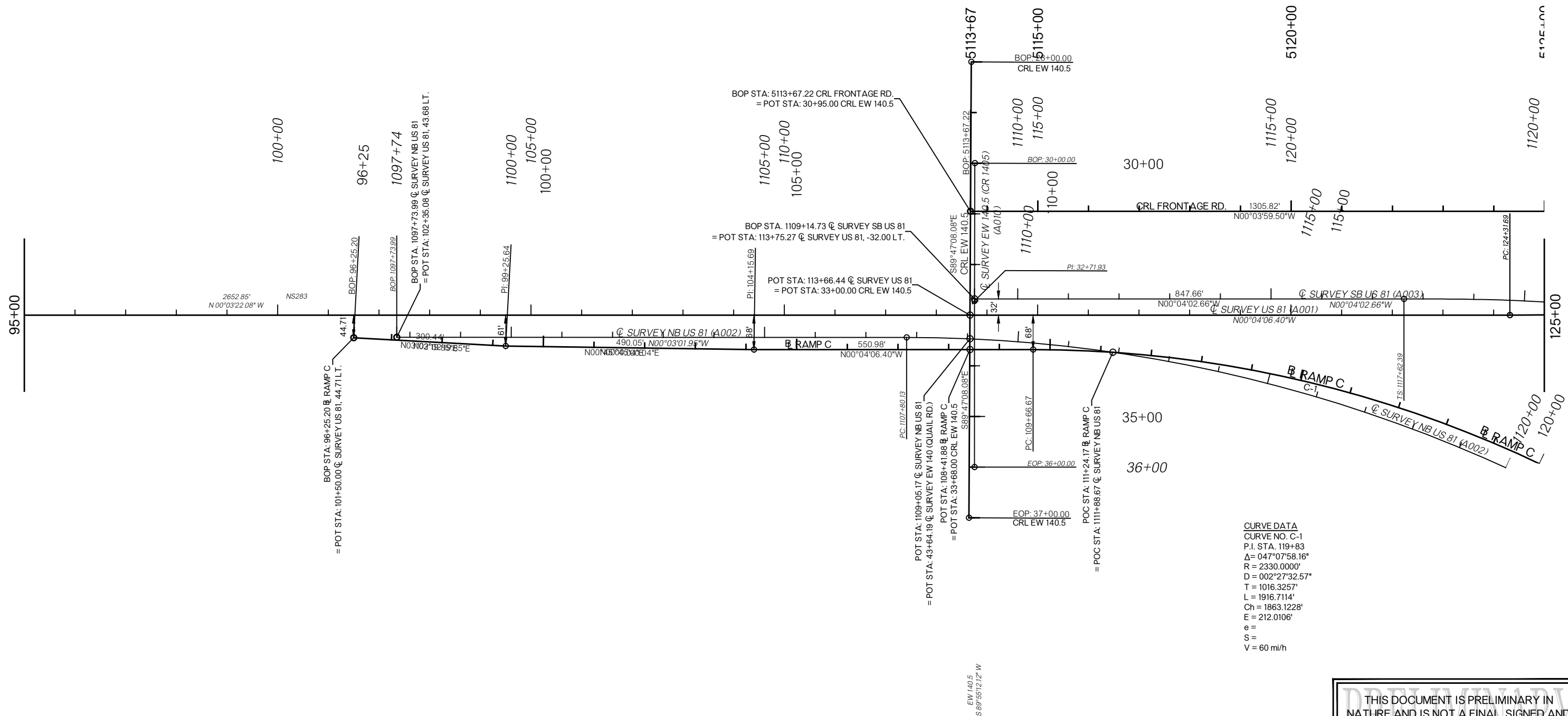
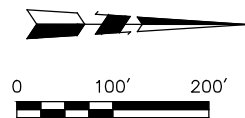
EROSION CONTROL



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019

GEOMETRIC LAYOUT



CURVE DATA
CURVE NO. C-1
P.I. STA. 119+83
 $\Delta = 047^{\circ}07'58.16''$
 $R = 2330.0000'$
 $D = 002^{\circ}27'32.57''$
 $T = 1016.3257'$
 $L = 1916.7114'$
 $Ch = 1863.1228'$
 $E = 212.0106'$
 $e =$
 $S =$
 $V = 60 \text{ mi/h}$

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GEOMETRIC LAYOUT

CURVE DATA
 CURVE NO. US 81-1
 P.I. STA. 147+93.78
 $\Delta = 044^\circ 48' 32.01''$
 R = 5729.5801'
 D = 001°00'00.00"
 T = 2362.0826'
 L = 4480.8909'
 Ch = 4367.5686'
 E = 467.8002'
 e = .06
 S = .034
 V = 70 mi/h

CURVE DATA
 CURVE NO. A-1
 P.I. STA. 137+46.08
 $\Delta = 014^\circ 01' 06.35''$
 R = 5661.5801'
 D = 001°00'43.24"
 T = 696.0790'
 L = 1385.2062'
 Ch = 1381.7537'
 E = 42.6302'
 e = 0.06'/
 S = 0.034'/
 V = 70 mi/h

CURVE DATA
 CURVE NO. A-2
 P.I. STA. 140+77.40
 $\Delta = 249^\circ 53' 03.76''$
 R = 250.0000'
 D = 022°55'05.92"
 T = 357.8048'
 L = 1090.3263'
 Ch = 409.8652'
 E = 686.4909'
 e = 0.06'/
 S = 0.06'/
 V = 30 mi/h

CURVE DATA
 CURVE NO. FR-2
 P.I. STA. 5140+16.66
 $\Delta = 019^\circ 27' 21.73''$
 R = 868.0000'
 D = 006°36'03.23"
 T = 148.8069'
 L = 294.7484'
 Ch = 293.3343'
 E = 12.6631'
 e = 0.06'/
 S = 0.044'/
 V = 35 mi/h

CURVE DATA
 CURVE NO. A-1EXT.
 P.I. STA. 245+52.42
 $\Delta = 032^\circ 44' 07.81''$
 R = 267.0000'
 D = 021°27'32.74"
 T = 78.4191'
 L = 152.5484'
 Ch = 150.4820'
 E = 11.2778'
 e = 0.06'/
 S = 0.06'/
 V = 30 mi/h

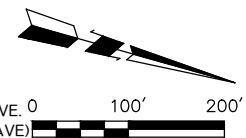
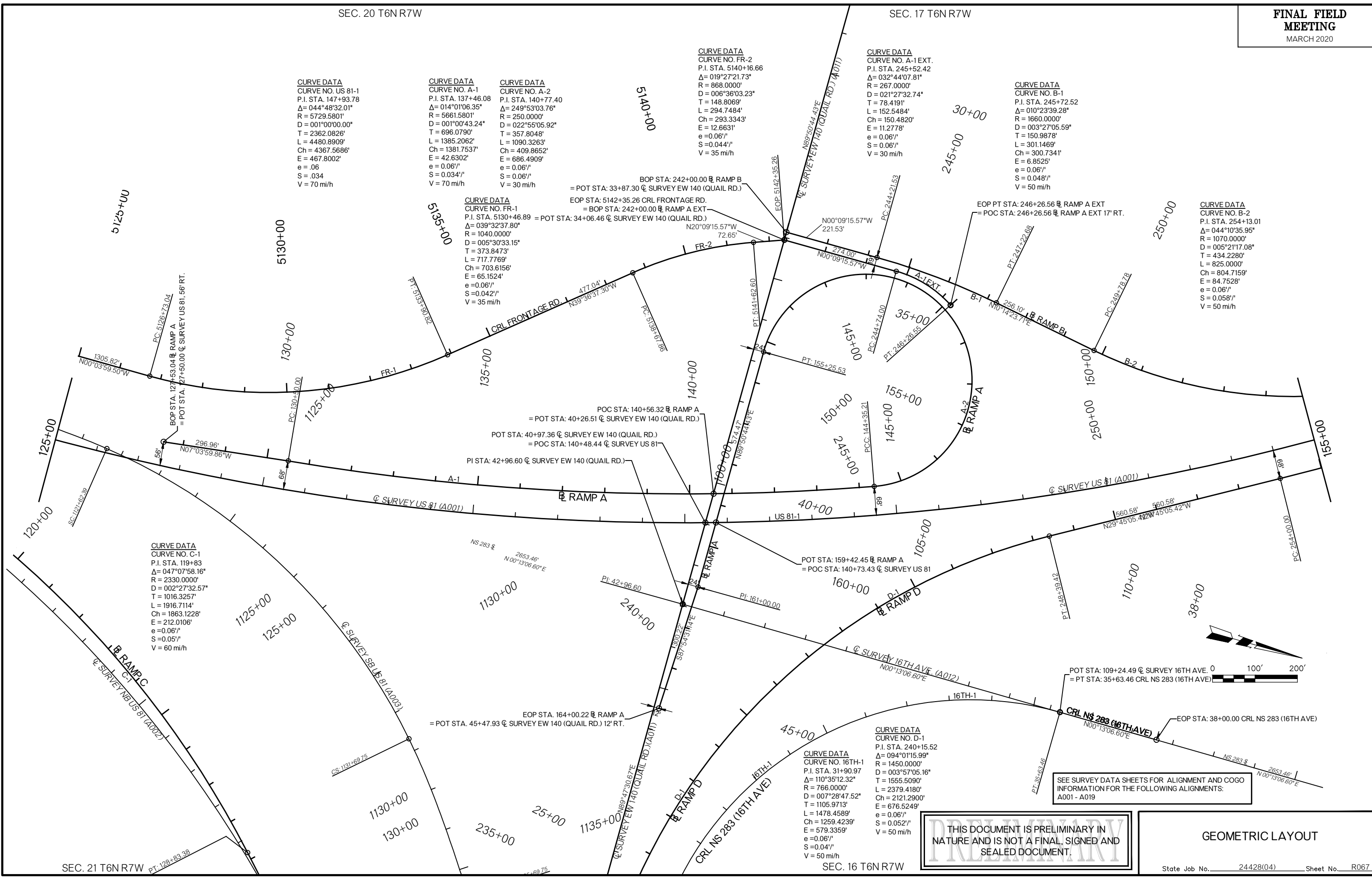
CURVE DATA
 CURVE NO. B-1
 P.I. STA. 245+72.52
 $\Delta = 010^\circ 23' 39.28''$
 R = 1660.0000'
 D = 003°27'05.59"
 T = 150.9878'
 L = 301.1469'
 Ch = 300.7341'
 E = 6.8525'
 e = 0.06'/
 S = 0.048'/
 V = 50 mi/h

CURVE DATA
 CURVE NO. B-2
 P.I. STA. 254+13.01
 $\Delta = 044^\circ 10' 35.95''$
 R = 1070.0000'
 D = 005°21'17.08"
 T = 434.2280'
 L = 825.0000'
 Ch = 804.7159'
 E = 84.7528'
 e = 0.06'/
 S = 0.058'/
 V = 50 mi/h

CURVE DATA
 CURVE NO. C-1
 P.I. STA. 119+83
 $\Delta = 047^\circ 07' 58.16''$
 R = 2330.0000'
 D = 002°27'32.57"
 T = 1016.3257'
 L = 1916.7114'
 Ch = 1863.1228'
 E = 212.0106'
 e = 0.06'/
 S = 0.05'/
 V = 60 mi/h

CURVE DATA
 CURVE NO. 16TH-1
 P.I. STA. 31+90.97
 $\Delta = 110^\circ 35' 12.32''$
 R = 766.0000'
 D = 007°28'47.52"
 T = 1105.9713'
 L = 1478.4589'
 Ch = 1259.4239'
 E = 579.3359'
 e = 0.06'/
 S = 0.04'/
 V = 50 mi/h

CURVE DATA
 CURVE NO. D-1
 P.I. STA. 240+15.52
 $\Delta = 094^\circ 01' 15.99''$
 R = 1450.0000'
 D = 003°57'05.16"
 T = 1555.5090'
 L = 2379.4180'
 Ch = 2121.2900'
 E = 676.5249'
 e = 0.06'/
 S = 0.052'/
 V = 50 mi/h



SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GEOMETRIC LAYOUT

CURVE DATA CURVE NO. B-2 P.I. STA. 254+13.01 Δ= 044°10'35.95" R = 1070.0000' D = 005°21'17.08" T = 434.2280' L = 825.0000' Ch = 804.7159' E = 84.7528' e = 0.06'/ S = 0.058'/ V = 50 mi/h

CURVE DATA CURVE NO. B-3 P.I. STA. 260+88.29 Δ= 005°45'13.21" R = 5661.5801' D = 001°00'43.24" T = 284.5084' L = 568.5386' Ch = 568.2998' E = 7.1441' e = 0.06'/ S = 0.034'/ V = 70 mi/h

CURVE DATA CURVE NO. D-2 P.I. STA. 259+26.44 Δ= 010°22'36.62" R = 5797.5801' D = 000°59'17.77" T = 526.4396' L = 1049.9997' Ch = 1048.5653' E = 23.8522' e = 0.06'/ S = 0.034'/ V = 70 mi/h

CURVE DATA CURVE NO. SB CD-1 P.I. STA. 264+05.84 Δ= 000°40'57.00" R = 5661.5801' D = 001°00'43.24" T = 33.7204' L = 67.4400' Ch = 67.4396' E = 0.1004' e = 0.06'/ S = 0.034'/ V = 70 mi/h

CURVE DATA CURVE NO. SB CD-2 P.I. STA. 266+11.36 Δ= 006°41'18.66" R = 2940.0000' D = 001°56'55.81" T = 171.7980' L = 343.2057' Ch = 343.0108' E = 5.0152' e = 0.06'/ S = 0.034'/ V = 50 mi/h

CURVE DATA CURVE NO. SB CD-3 P.I. STA. 271+66.51 Δ= 002°11'02.70" R = 7870.0000' D = 000°43'40.90" T = 150.0182' L = 300.0000' Ch = 299.9818' E = 1.4297' e = 0.06'/ S = NC V = 50 mi/h

CURVE DATA CURVE NO. G-1 P.I. STA. 1194+92.65 Δ= 087°03'00.25" R = 1670.0000' D = 003°25'51.19" T = 1586.1582' L = 2537.2483' Ch = 2300.1627' E = 633.2147' e = 0.06'/ S = 0.048'/ V = 50 mi/h

255+00

260+00 160+00 260+00

265+00 165+00 265+00

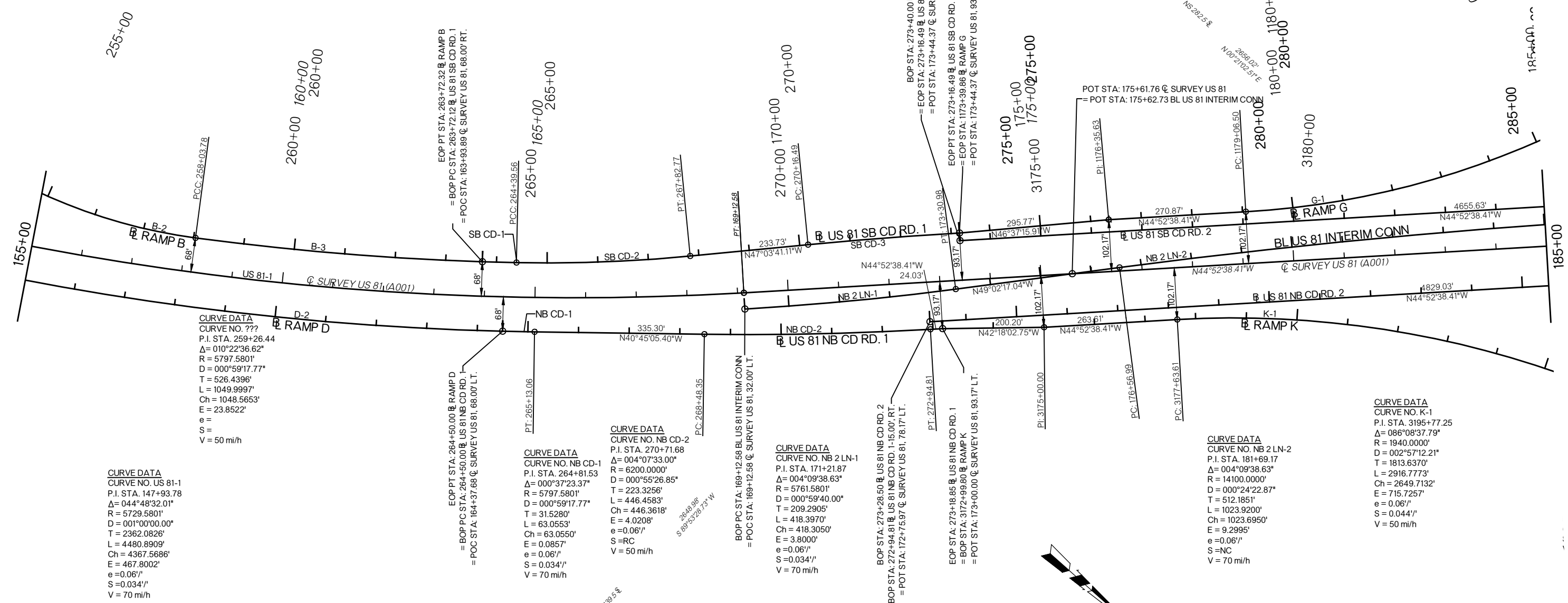
270+00 170+00 270+00

275+00 175+00 3175+00 175+00 275+00

280+00 180+00 1180+00 280+00

1185+00

185+00



CURVE DATA CURVE NO. US 81-1 P.I. STA. 147+93.78 Δ= 044°48'32.01" R = 5729.5801' D = 001°00'00.00" T = 2362.0826' L = 4480.8909' Ch = 4367.5686' E = 467.8002' e = 0.06'/ S = 0.034'/ V = 70 mi/h

CURVE DATA CURVE NO. ??? P.I. STA. 259+26.44 Δ= 010°22'36.62" R = 5797.5801' D = 000°59'17.77" T = 526.4396' L = 1049.9997' Ch = 1048.5653' E = 23.8522' e = 0.06'/ S = 0.034'/ V = 50 mi/h

EOP PT STA: 264+50.00 RAMP D = BOP STA: 264+50.00 US 81 NB CD RD. 1 = POC STA: 164+37.88 SURVEY US 81, 68.00' RT.

CURVE DATA CURVE NO. NB CD-1 P.I. STA. 264+81.53 Δ= 000°37'23.37" R = 5797.5801' D = 000°59'17.77" T = 31.5280' L = 63.0553' Ch = 63.0550' E = 0.0857' e = 0.06'/ S = 0.034'/ V = 70 mi/h

CURVE DATA CURVE NO. NB CD-2 P.I. STA. 270+71.68 Δ= 004°07'33.00" R = 6200.0000' D = 000°55'26.85" T = 223.3256' L = 446.4583' Ch = 446.3618' E = 4.0208' e = 0.06'/ S = RC V = 50 mi/h

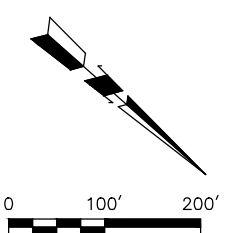
CURVE DATA CURVE NO. NB LN-1 P.I. STA. 171+21.87 Δ= 004°09'38.63" R = 5761.5801' D = 000°59'40.00" T = 209.2905' L = 418.3970' Ch = 418.3050' E = 3.8000' e = 0.06'/ S = 0.034'/ V = 70 mi/h

BOP STA: 273+28.50 US 81 NB CD RD. 2 = BOP STA: 272+94.81 US 81 NB CD RD. 1 = POT STA: 172+75.97 SURVEY US 81, 78.17' LT.

EOP STA: 273+18.85 US 81 NB CD RD. 1 = BOP STA: 3172+99.80 RAMP K = POT STA: 173+00.00 SURVEY US 81, 93.17' LT.

CURVE DATA CURVE NO. NB LN-2 P.I. STA. 181+69.17 Δ= 004°09'38.63" R = 14100.0000' D = 000°24'22.87" T = 512.1851' L = 1023.9200' Ch = 1023.6950' E = 9.2995' e = 0.06'/ S = NC V = 70 mi/h

CURVE DATA CURVE NO. K-1 P.I. STA. 3195+77.25 Δ= 086°08'37.79" R = 1940.0000' D = 002°57'12.21" T = 1813.6370' L = 2916.7773' Ch = 2649.7132' E = 715.7257' e = 0.06'/ S = 0.044'/ V = 50 mi/h



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019

GEOMETRIC LAYOUT

CURVE DATA
 CURVE NO. G-1
 P.I. STA. 1194+92.65
 $\Delta = 087^{\circ}03'00.25''$
 $R = 1670.0000'$
 $D = 003^{\circ}25'51.19''$
 $T = 1586.1582'$
 $L = 2537.2483'$
 $Ch = 2300.1627'$
 $E = 633.2147'$
 $e = 0.06''$
 $S = 0.048''$
 $V = 50$ mi/h

CURVE DATA
 CURVE NO. H-1
 P.I. STA. 192+32.12
 $\Delta = 268^{\circ}32'33.78''$
 $R = 250.0000'$
 $D = 022^{\circ}55'05.92''$
 $T = 256.4409'$
 $L = 1171.7386'$
 $Ch = 358.0209'$
 $E = 608.1367'$
 $e = 0.06''$
 $S = 0.06''$
 $V = 30$ mi/h

CURVE DATA
 CURVE NO. EB CD-3
 P.I. STA. 282+42.46
 $\Delta = 007^{\circ}20'00.00''$
 $R = 17242.2340'$
 $D = 000^{\circ}19'56.28''$
 $T = 1104.9326'$
 $L = 2206.8475'$
 $Ch = 2205.3415'$
 $E = 35.3674'$
 $e = 0.06''$
 $S = NC$
 $V = 50$ mi/h

CURVE DATA
 CURVE NO. WB CD-3
 P.I. STA. 282+36.79
 $\Delta = 007^{\circ}20'00.00''$
 $R = 17135.2340'$
 $D = 000^{\circ}20'03.75''$
 $T = 1098.0757'$
 $L = 2193.1525'$
 $Ch = 2191.6558'$
 $E = 35.1479'$
 $e = 0.06''$
 $S = NC$
 $V = 50$ mi/h

CURVE DATA
 CURVE NO. J-1
 P.I. STA. 2202+07.97
 $\Delta = 093^{\circ}28'40.68''$
 $R = 1700.0000'$
 $D = 003^{\circ}22'13.22''$
 $T = 1806.4572'$
 $L = 2773.5471'$
 $Ch = 2476.0133'$
 $E = 780.5821'$
 $e = 0.06''$
 $S = 0.048''$
 $V = 50$ mi/h

CURVE DATA
 CURVE NO. I-1
 P.I. STA. 317+04.12
 $\Delta = 001^{\circ}09'28.41''$
 $R = 17111.0673'$
 $D = 000^{\circ}20'05.45''$
 $T = 172.9050'$
 $L = 345.7982'$
 $Ch = 345.7923'$
 $E = 0.8736'$
 $e = 0.06''$
 $S = NC$
 $V = 30$ mi/h

CURVE DATA
 CURVE NO. I-2
 P.I. STA. 316+33.54
 $\Delta = 271^{\circ}30'50.17''$
 $R = 250.0000'$
 $D = 022^{\circ}55'05.92''$
 $T = 243.4800'$
 $L = 1184.7030'$
 $Ch = 348.8517'$
 $E = 598.9735'$
 $e = 0.06''$
 $S = 0.06''$
 $V = 30$ mi/h

BL US 81 INTERIM CONN
 POT STA: 186+80.91 BL US 81 INTERIM CONN
 = POT STA: 186+78.79 \odot SURVEY US 81 44.00' RT.

\odot SURVEY US 81 (A001)
 $N44^{\circ}52'38.41''W$

POT STA: 196+51.96 \odot SURVEY US 81
 = POC STA: 289+39.57 \odot SURVEY I-44 (HE BAILEY TPK)

\odot SURVEY US 81 (A001)
 $N44^{\circ}52'38.41''W$

BOP POC STA: 315+31.22 \odot RAMP I
 = EOP POC STA: 315+31.22 \odot RAMP M
 = POT STA: 197+29.71 \odot SURVEY US 81

\odot SURVEY US 81 (A001)
 $N44^{\circ}52'38.41''W$

CURVE DATA
 CURVE NO. L-2
 P.I. STA. 211+21.25
 $\Delta = 273^{\circ}45'49.30''$
 $R = 250.0000'$
 $D = 022^{\circ}55'05.92''$
 $T = 234.0945'$
 $L = 1194.5195'$
 $Ch = 341.7525'$
 $E = 592.4912'$
 $e = 0.06''$
 $S = 0.06''$
 $V = 30$ mi/h

CURVE DATA
 CURVE NO. K-1
 P.I. STA. 3195+77.25
 $\Delta = 086^{\circ}08'37.79''$
 $R = 1940.0000'$
 $D = 002^{\circ}57'12.21''$
 $T = 1813.6370'$
 $L = 2916.7773'$
 $Ch = 2649.7132'$
 $E = 715.7257'$
 $e = 0.06''$
 $S = 0.044''$
 $V = 50$ mi/h

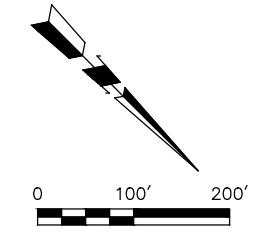
CURVE DATA
 CURVE NO. NB 2 LN-2
 P.I. STA. NB 2 LN-2
 $\Delta = 004^{\circ}09'38.63''$
 $R = 14100.0000'$
 $D = 000^{\circ}24'22.87''$
 $T = 512.1851'$
 $L = 1023.9200'$
 $Ch = 1023.6950'$
 $E = 9.2995'$
 $e = 0.06''$
 $S = 0.034''$
 $V = 70$ mi/h

CURVE DATA
 CURVE NO. L-1
 P.I. STA. 211+87.21
 $\Delta = 001^{\circ}06'57.30''$
 $R = 17266.4007'$
 $D = 000^{\circ}19'54.60''$
 $T = 168.1492'$
 $L = 336.2878'$
 $Ch = 336.2825'$
 $E = 0.8187'$
 $e = 0.06''$
 $S = NC$
 $V = 30$ mi/h

CURVE DATA
 CURVE NO. M-1
 P.I. STA. 297+32.93
 $\Delta = 266^{\circ}08'37.79''$
 $R = 250.0000'$
 $D = 022^{\circ}55'05.92''$
 $T = 267.4184'$
 $L = 1161.2715'$
 $Ch = 365.2486'$
 $E = 616.0774'$
 $e = 0.06''$
 $S = 0.06''$
 $V = 30$ mi/h

CURVE DATA
 CURVE NO. M-2
 P.I. STA. 313+54.38
 $\Delta = 001^{\circ}11'03.63''$
 $R = 17111.0672'$
 $D = 000^{\circ}20'05.45''$
 $T = 176.8547'$
 $L = 353.6968'$
 $Ch = 353.6905'$
 $E = 0.9139'$
 $e = 0.06''$
 $S = NC$
 $V = 30$ mi/h

CURVE DATA
 CURVE NO. N-1
 P.I. STA. 4200+17.46
 $\Delta = 093^{\circ}51'48.47''$
 $R = 1700.0000'$
 $D = 003^{\circ}22'13.22''$
 $T = 1818.6774'$
 $L = 2784.9849'$
 $Ch = 2483.8379'$
 $E = 789.4954'$
 $e = 0.06''$
 $S = 0.048''$
 $V = 50$ mi/h

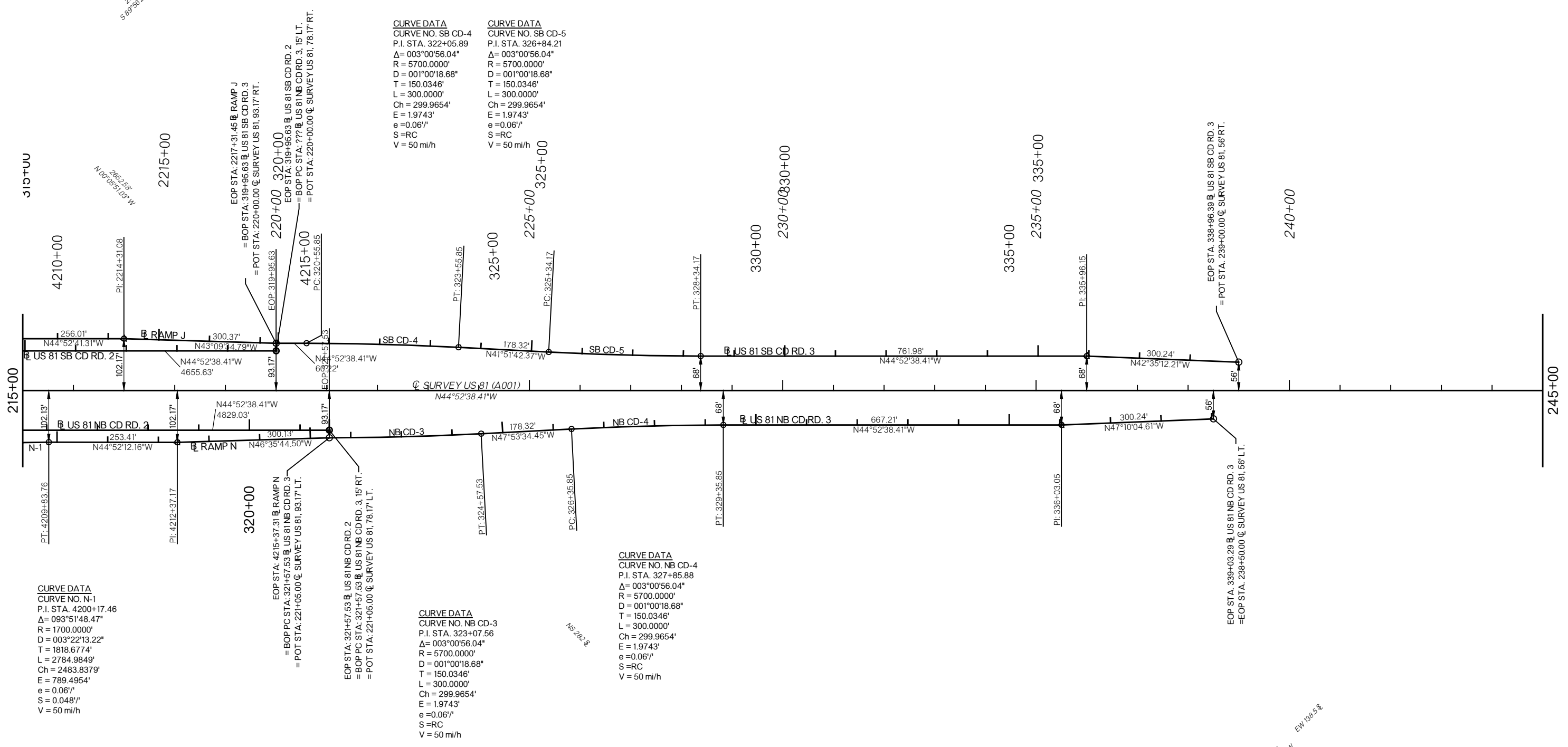


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019

GEOMETRIC LAYOUT

US 81 REALIGNMENT
GRADY COUNTY



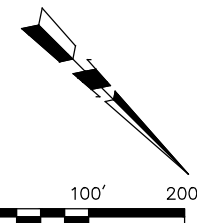
CURVE DATA
 CURVE NO. N-1
 P.I. STA. 4200+17.46
 $\Delta = 093^{\circ}51'48.47''$
 $R = 1700.0000'$
 $D = 003^{\circ}22'13.22''$
 $T = 1818.6774'$
 $L = 2784.9849'$
 $Ch = 2483.8379'$
 $E = 789.4954'$
 $e = 0.061''$
 $S = 0.048''$
 $V = 50 \text{ mi/h}$

EOP STA: 4215+37.31 RAMP N
 = BOP STA: 321+57.53 US 81 NB CD RD. 3
 = POT STA: 221+05.00 SURVEY US 81, 93.17' LT.

EOP STA: 321+57.53 US 81 NB CD RD. 2
 = BOP STA: 321+57.53 US 81 NB CD RD. 3, 15' RT.
 = POT STA: 221+05.00 SURVEY US 81, 78.17' LT.

CURVE DATA
 CURVE NO. NB CD-3
 P.I. STA. 323+07.56
 $\Delta = 003^{\circ}00'56.04''$
 $R = 5700.0000'$
 $D = 001^{\circ}00'18.68''$
 $T = 150.0346'$
 $L = 300.0000'$
 $Ch = 299.9654'$
 $E = 1.9743'$
 $e = 0.061''$
 $S = RC$
 $V = 50 \text{ mi/h}$

NS-202-3

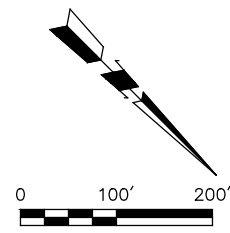


SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GEOMETRIC LAYOUT

SEC. 7 T6N R7W



CURVE DATA
 CURVE NO. O-1
 P.I. STA. 1016+45.95
 $\Delta = 033^{\circ}46'51.15''$
 R = 1432.3900'
 D = 004^{\circ}00'00.05"
 T = 434.9323'
 L = 844.5193'
 Ch = 832.3404'
 E = 64.5760'
 e = 0.06'/'
 S = 0.052'/'
 V = 50 mi/h

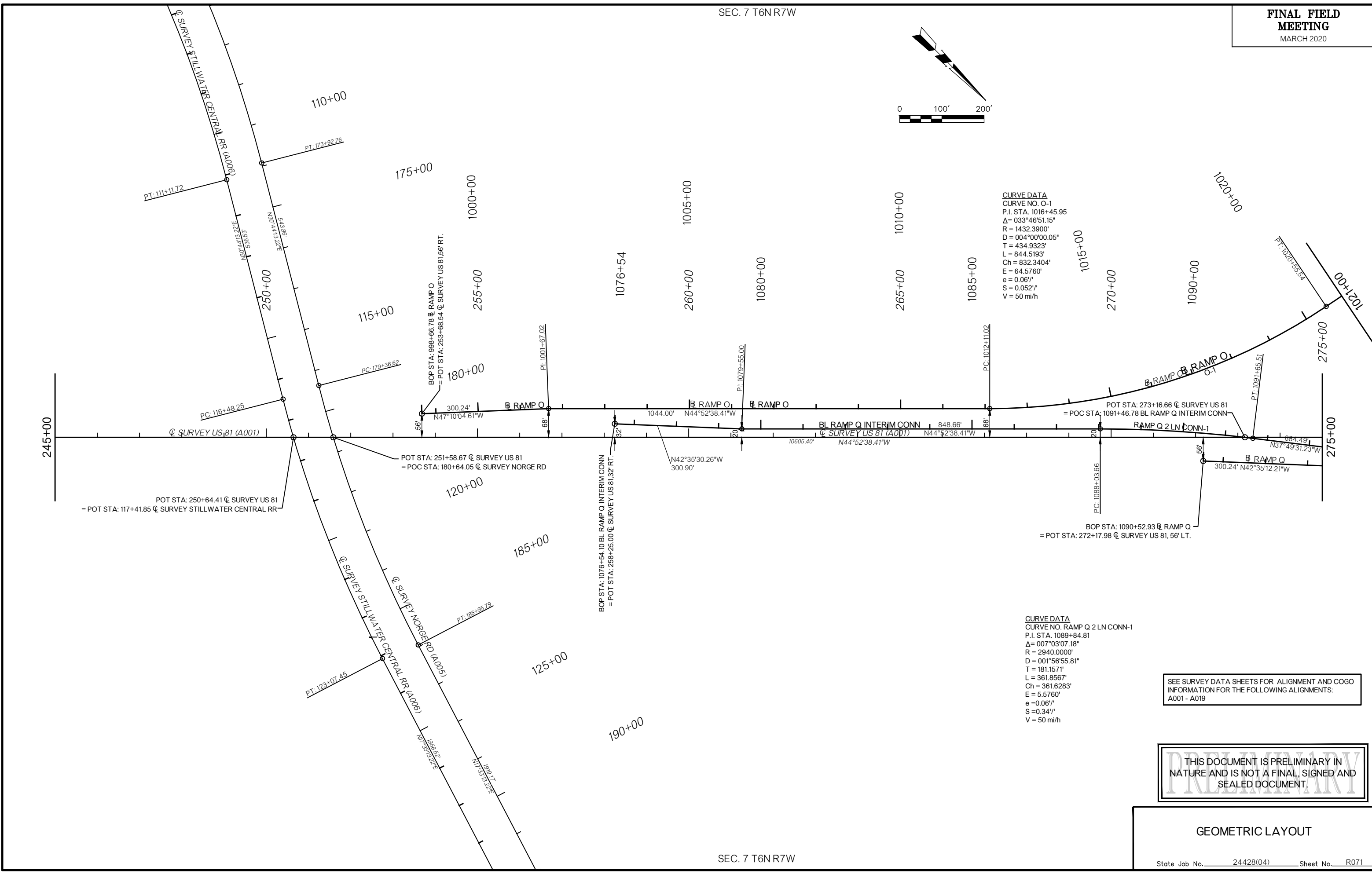
CURVE DATA
 CURVE NO. RAMP Q 2 LN CONN-1
 P.I. STA. 1089+84.81
 $\Delta = 007^{\circ}03'07.18''$
 R = 2940.0000'
 D = 001^{\circ}56'55.81"
 T = 181.1571'
 L = 361.8567'
 Ch = 361.6283'
 E = 5.5760'
 e = 0.06'/'
 S = 0.34'/'
 V = 50 mi/h

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS:
A001 - A019

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GEOMETRIC LAYOUT

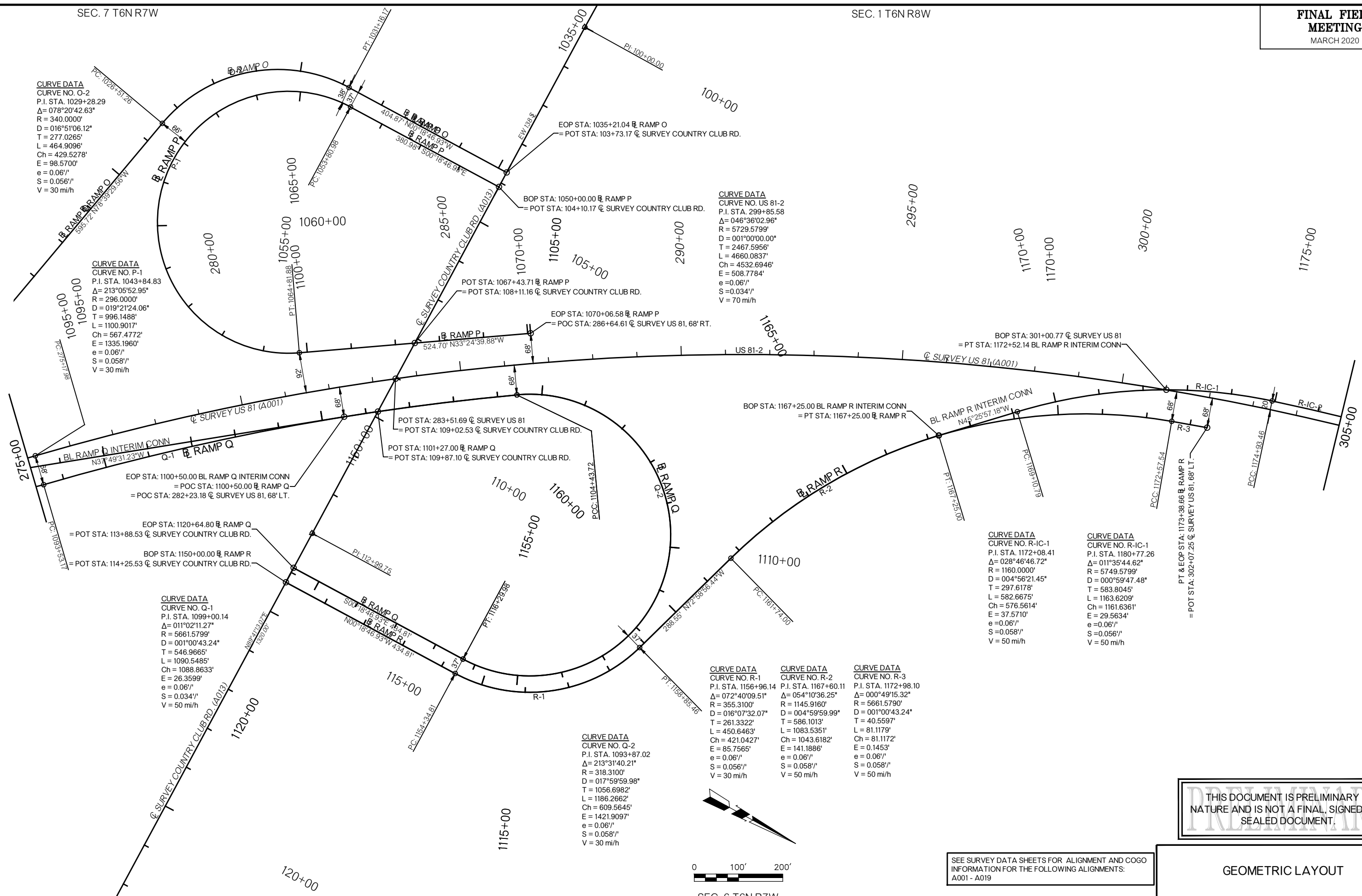
SEC. 7 T6N R7W



US 81 REALIGNMENT GRADY COUNTY

SEC. 7 T6N R7W

SEC. 1 T6N R8W



CURVE DATA
CURVE NO. Q-2
P.I. STA. 1029+28.29
 $\Delta = 078^{\circ}20'42.63''$
R = 340.0000'
D = 016^{\circ}51'06.12"
T = 277.0265'
L = 464.9096'
Ch = 429.5278'
E = 98.5700'
e = 0.061'
S = 0.0561'
V = 30 mi/h

CURVE DATA
CURVE NO. P-1
P.I. STA. 1043+84.83
 $\Delta = 213^{\circ}05'52.95''$
R = 296.0000'
D = 019^{\circ}21'24.06"
T = 996.1488'
L = 1100.9017'
Ch = 567.4772'
E = 1335.1960'
e = 0.061'
S = 0.0581'
V = 30 mi/h

CURVE DATA
CURVE NO. US 81-2
P.I. STA. 299+85.58
 $\Delta = 046^{\circ}36'02.96''$
R = 5729.5799'
D = 001^{\circ}00'00.00"
T = 2467.5956'
L = 4660.0837'
Ch = 4532.6946'
E = 508.7784'
e = 0.061'
S = 0.0341'
V = 70 mi/h

CURVE DATA
CURVE NO. Q-1
P.I. STA. 1099+00.14
 $\Delta = 011^{\circ}02'11.27''$
R = 5661.5799'
D = 001^{\circ}00'43.24"
T = 546.9665'
L = 1090.5485'
Ch = 1088.8633'
E = 26.3599'
e = 0.061'
S = 0.0341'
V = 50 mi/h

CURVE DATA
CURVE NO. Q-2
P.I. STA. 1093+87.02
 $\Delta = 213^{\circ}31'40.21''$
R = 318.3100'
D = 017^{\circ}59'59.98"
T = 1056.6982'
L = 1186.2662'
Ch = 609.5645'
E = 1421.9097'
e = 0.061'
S = 0.0581'
V = 30 mi/h

CURVE DATA
CURVE NO. R-IC-1
P.I. STA. 1172+08.41
 $\Delta = 028^{\circ}46'46.72''$
R = 1160.0000'
D = 004^{\circ}56'21.45"
T = 297.6178'
L = 582.6675'
Ch = 576.5614'
E = 37.5710'
e = 0.061'
S = 0.0581'
V = 50 mi/h

CURVE DATA
CURVE NO. R-IC-1
P.I. STA. 1180+77.26
 $\Delta = 011^{\circ}35'44.62''$
R = 5749.5799'
D = 000^{\circ}59'47.48"
T = 583.8045'
L = 1163.6209'
Ch = 1161.6361'
E = 29.5634'
e = 0.061'
S = 0.0581'
V = 50 mi/h

CURVE DATA
CURVE NO. R-1
P.I. STA. 1156+96.14
 $\Delta = 072^{\circ}40'09.51''$
R = 355.3100'
D = 016^{\circ}07'32.07"
T = 261.3322'
L = 450.6463'
Ch = 421.0427'
E = 85.7565'
e = 0.061'
S = 0.0561'
V = 30 mi/h

CURVE DATA
CURVE NO. R-2
P.I. STA. 1167+60.11
 $\Delta = 054^{\circ}10'36.25''$
R = 1145.9160'
D = 004^{\circ}59'59.99"
T = 586.1013'
L = 1083.5351'
Ch = 1043.6182'
E = 141.1886'
e = 0.061'
S = 0.0581'
V = 50 mi/h

CURVE DATA
CURVE NO. R-3
P.I. STA. 1172+98.10
 $\Delta = 000^{\circ}49'15.32''$
R = 5661.5790'
D = 001^{\circ}00'43.24"
T = 40.5597'
L = 81.1179'
Ch = 81.1172'
E = 0.1453'
e = 0.061'
S = 0.0581'
V = 50 mi/h

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

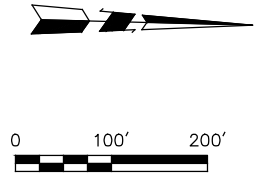
SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019

GEOMETRIC LAYOUT

US 81 REALIGNMENT GRADY COUNTY

SEC. 1 T6N R8W

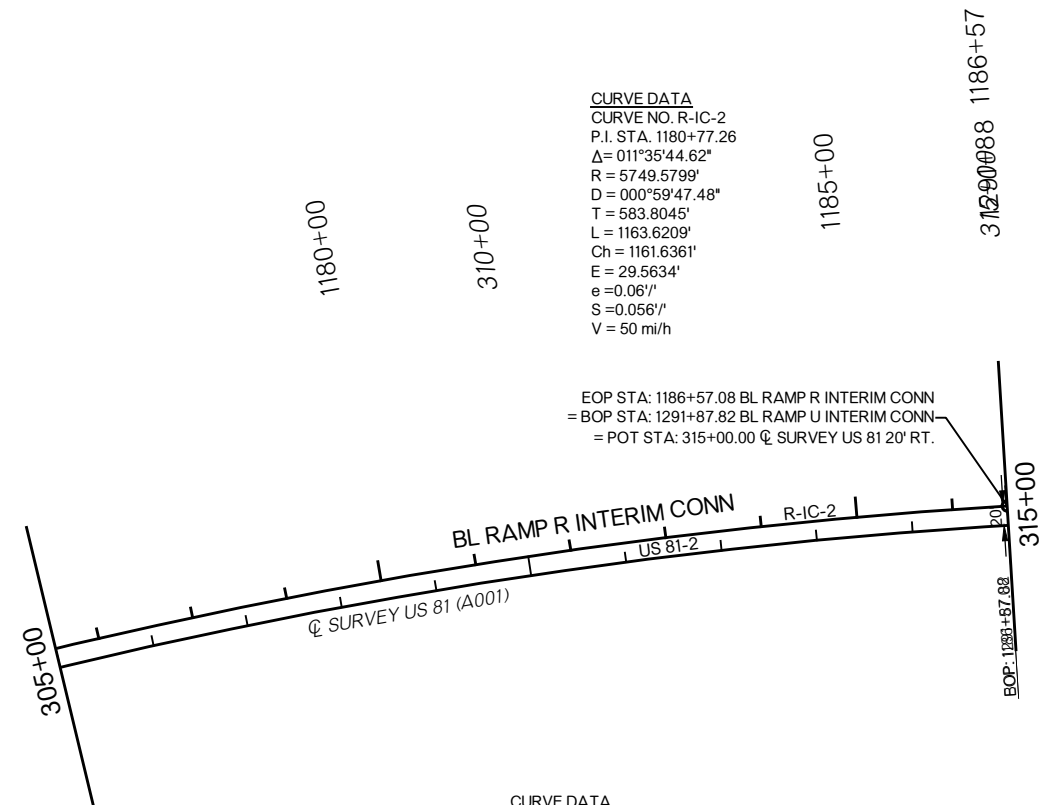
SEC. 1 T6N R8W



CURVE DATA
 CURVE NO. R-IC-2
 P.I. STA. 1180+77.26
 $\Delta = 011^{\circ}35'44.62''$
 $R = 5749.5799'$
 $D = 000^{\circ}59'47.48''$
 $T = 583.8045'$
 $L = 1163.6209'$
 $Ch = 1161.6361'$
 $E = 29.5634'$
 $e = 0.061''$
 $S = 0.0561''$
 $V = 50 \text{ mi/h}$

EOP STA: 1186+57.08 BL RAMP R INTERIM CONN
 = BOP STA: 1291+87.82 BL RAMP U INTERIM CONN
 = POT STA: 315+00.00 @ SURVEY US 81 20' RT.

CURVE DATA
 CURVE NO. US 81-2
 P.I. STA. 299+85.58
 $\Delta = 046^{\circ}36'02.96''$
 $R = 5729.5799'$
 $D = 001^{\circ}00'00.00''$
 $T = 2467.5956'$
 $L = 4660.0837'$
 $Ch = 4532.6946'$
 $E = 508.7784'$
 $e = 0.061''$
 $S = 0.0541''$
 $V = 70 \text{ mi/h}$



PRELIMINARY
 THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

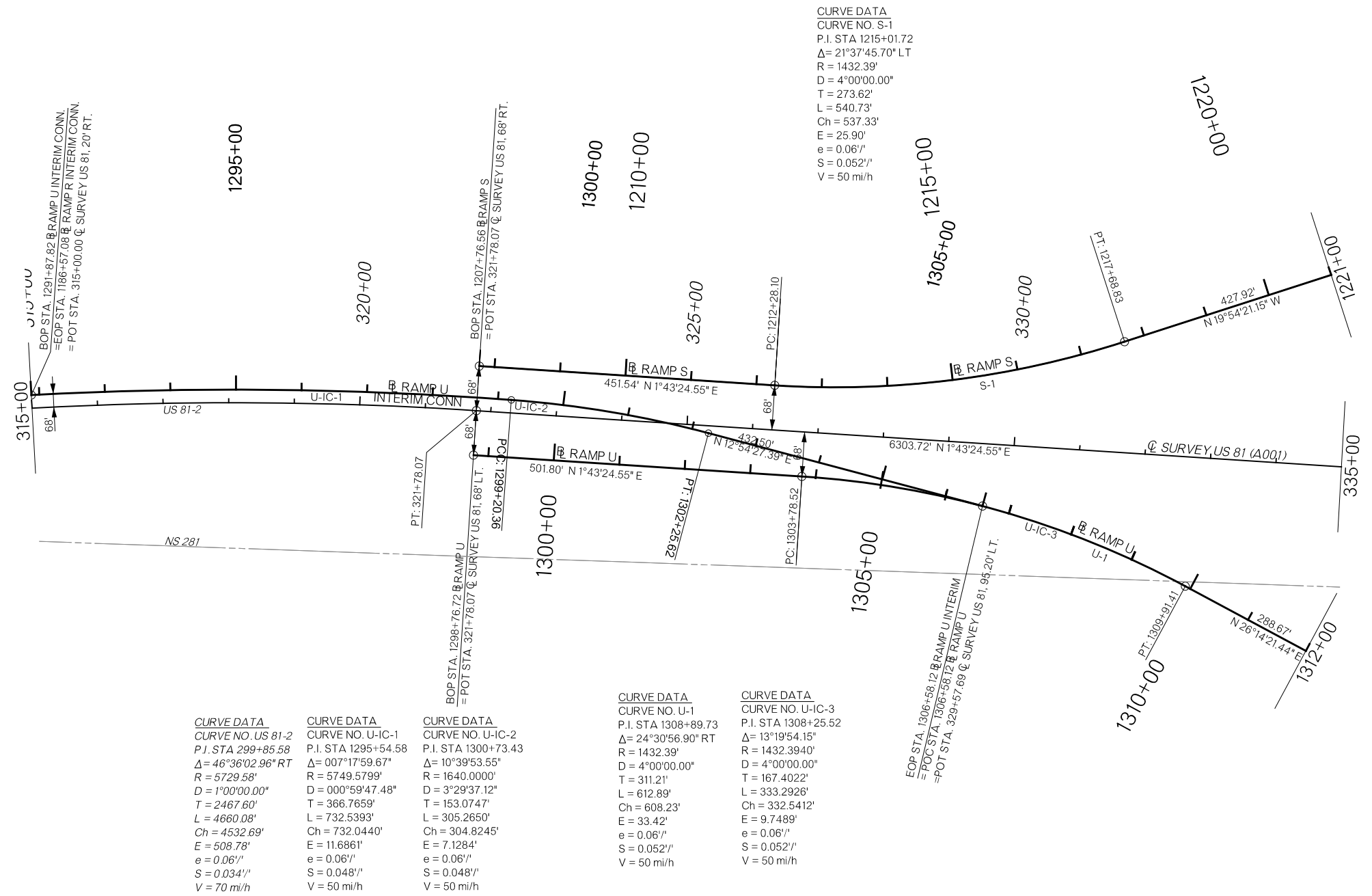
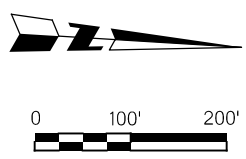
SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS:
 A001 - A019

GEOMETRIC LAYOUT

SEC. 6 T6N R7W

SEC. 6 T6N R7W

SEC. 1 T6N R8W



CURVE DATA
 CURVE NO. S-1
 P.I. STA 1215+01.72
 $\Delta = 21^{\circ}37'45.70''$ LT
 R = 1432.39'
 D = $4^{\circ}00'00.00''$
 T = 273.62'
 L = 540.73'
 Ch = 537.33'
 E = 25.90'
 e = 0.06'/'
 S = 0.052'/'
 V = 50 mi/h

CURVE DATA
 CURVE NO. US 81-2
 P.I. STA 299+85.58
 $\Delta = 46^{\circ}36'02.96''$ RT
 R = 5729.58'
 D = $1^{\circ}00'00.00''$
 T = 2467.60'
 L = 4660.08'
 Ch = 4532.69'
 E = 508.78'
 e = 0.06'/'
 S = 0.034'/'
 V = 70 mi/h

CURVE DATA
 CURVE NO. U-IC-1
 P.I. STA 1295+54.58
 $\Delta = 007^{\circ}17'59.67''$
 R = 5749.5799'
 D = $000^{\circ}59'47.48''$
 T = 366.7659'
 L = 732.5393'
 Ch = 732.0440'
 E = 11.6861'
 e = 0.06'/'
 S = 0.048'/'
 V = 50 mi/h

CURVE DATA
 CURVE NO. U-IC-2
 P.I. STA 1300+73.43
 $\Delta = 10^{\circ}39'53.55''$
 R = 1640.0000'
 D = $3^{\circ}29'37.12''$
 T = 153.0747'
 L = 305.2650'
 Ch = 304.8245'
 E = 7.1284'
 e = 0.06'/'
 S = 0.048'/'
 V = 50 mi/h

CURVE DATA
 CURVE NO. U-1
 P.I. STA 1308+89.73
 $\Delta = 24^{\circ}30'56.90''$ RT
 R = 1432.39'
 D = $4^{\circ}00'00.00''$
 T = 311.21'
 L = 612.89'
 Ch = 608.23'
 E = 33.42'
 e = 0.06'/'
 S = 0.052'/'
 V = 50 mi/h

CURVE DATA
 CURVE NO. U-IC-3
 P.I. STA 1308+25.52
 $\Delta = 13^{\circ}19'54.15''$
 R = 1432.3940'
 D = $4^{\circ}00'00.00''$
 T = 167.4022'
 L = 333.2926'
 Ch = 332.5412'
 E = 9.7489'
 e = 0.06'/'
 S = 0.052'/'
 V = 50 mi/h

EOP STA. 1306+58.12 RAMP U INTERIM
 = POT STA. 1306+58.12 RAMP U
 = POT STA. 329+57.69 SURVEY US 81, 20' LT.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

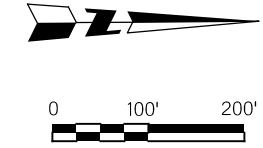
SEC. 6 T6N R7W

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS:
A001-A019

GEOMETRIC LAYOUT

State Job No. 24428(04) Sheet No. R074

US 81 REALIGNMENT
GRADY COUNTY



SEC. 1 T6N R8W

SEC. 36 T7N R8W

CURVE DATA
CURVE NO. S-2
P.I. STA 1222+99.41
 $\Delta = 21^\circ 31' 41.96''$ RT
R = 540.00'
D = $10^\circ 36' 37.19''$
T = 102.66'
L = 202.90'
Ch = 201.71'
E = 9.67'
e = 0.061'
S = 0.0481'
V = 30 mi/h

CURVE DATA
CURVE NO. T-1
P.I. STA 1252+62.91
 $\Delta = 24^\circ 40' 54.29''$ RT
R = 540.00'
D = $10^\circ 36' 37.19''$
T = 118.14'
L = 232.62'
Ch = 230.83'
E = 12.77'
e = 0.061'
S = 0.0481'
V = 30 mi/h

CURVE DATA
CURVE NO. T-2
P.I. STA 1259+76.50
 $\Delta = 24^\circ 30' 56.90''$ LT
R = 1432.39'
D = $4^\circ 00' 00.00''$
T = 311.21'
L = 612.89'
Ch = 608.23'
E = 33.42'
e = 0.061'
S = 0.0521'
V = 50 mi/h

CURVE DATA
CURVE NO. V-IC-1
P.I. STA 1367+27.33
 $\Delta = 9^\circ 29' 49.40''$
R = 1820.00'
D = $3^\circ 08' 53.23''$
T = 151.1833'
L = 301.6739'
Ch = 301.3287'
E = 6.2684'
e = 0.061'
S = 0.0461'
V = 50 mi/h

CURVE DATA
CURVE NO. U-2
P.I. STA 1313+98.22
 $\Delta = 24^\circ 40' 54.29''$ LT
R = 540.00'
D = $10^\circ 36' 37.19''$
T = 118.14'
L = 232.62'
Ch = 230.83'
E = 12.77'
e = 0.061'
S = 0.0481'
V = 30 mi/h

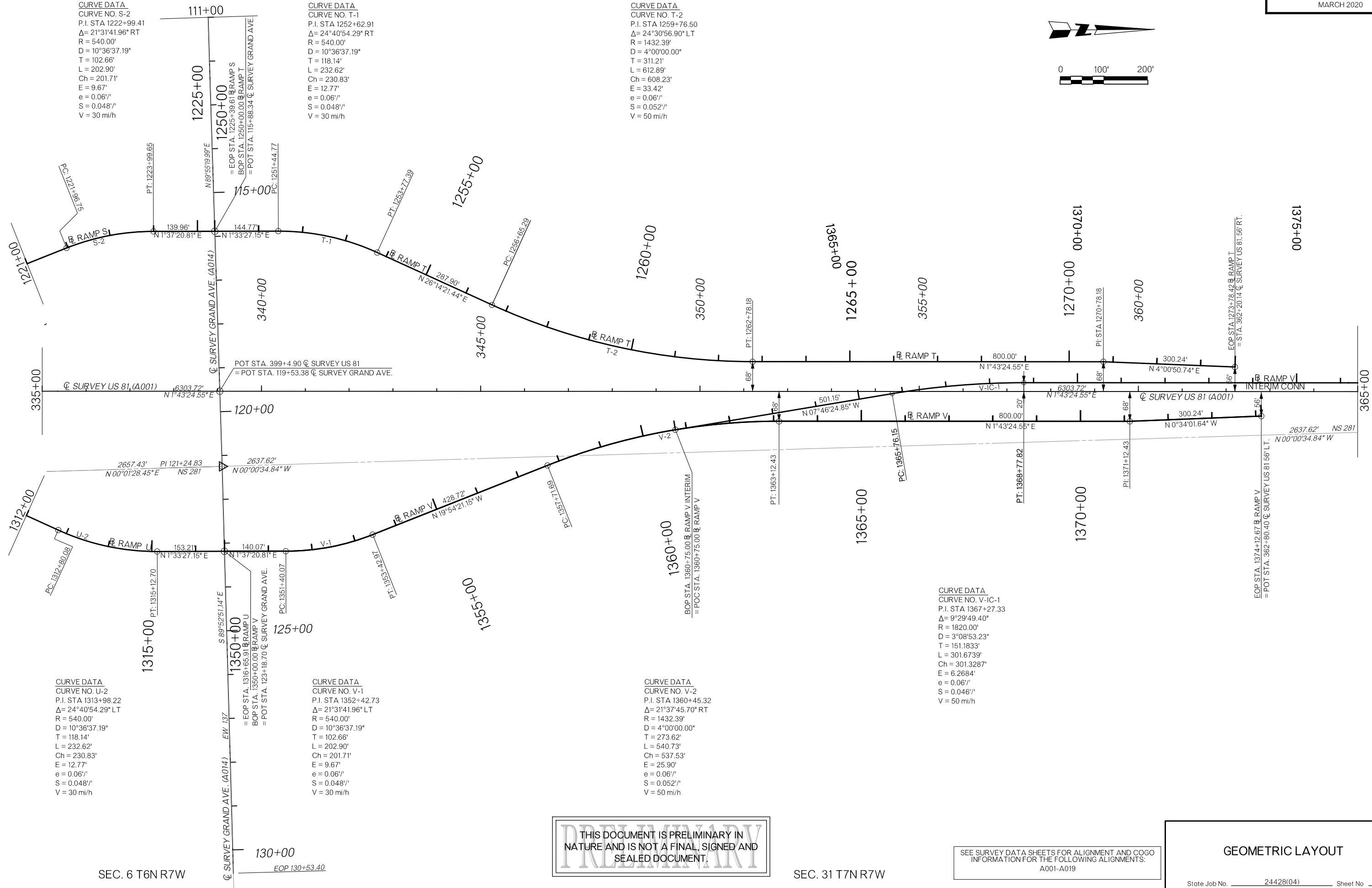
CURVE DATA
CURVE NO. V-1
P.I. STA 1352+42.73
 $\Delta = 21^\circ 31' 41.96''$ LT
R = 540.00'
D = $10^\circ 36' 37.19''$
T = 102.66'
L = 202.90'
Ch = 201.71'
E = 9.67'
e = 0.061'
S = 0.0481'
V = 30 mi/h

CURVE DATA
CURVE NO. V-2
P.I. STA 1360+45.32
 $\Delta = 21^\circ 37' 45.70''$ RT
R = 1432.39'
D = $4^\circ 00' 00.00''$
T = 273.62'
L = 540.73'
Ch = 537.53'
E = 25.90'
e = 0.061'
S = 0.0521'
V = 50 mi/h

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS:
A001-A019

GEOMETRIC LAYOUT
State Job No. 24428(04) Sheet No. R075



US 81 REALIGNMENT
GRADY COUNTY

SEC. 36 T7N R8W

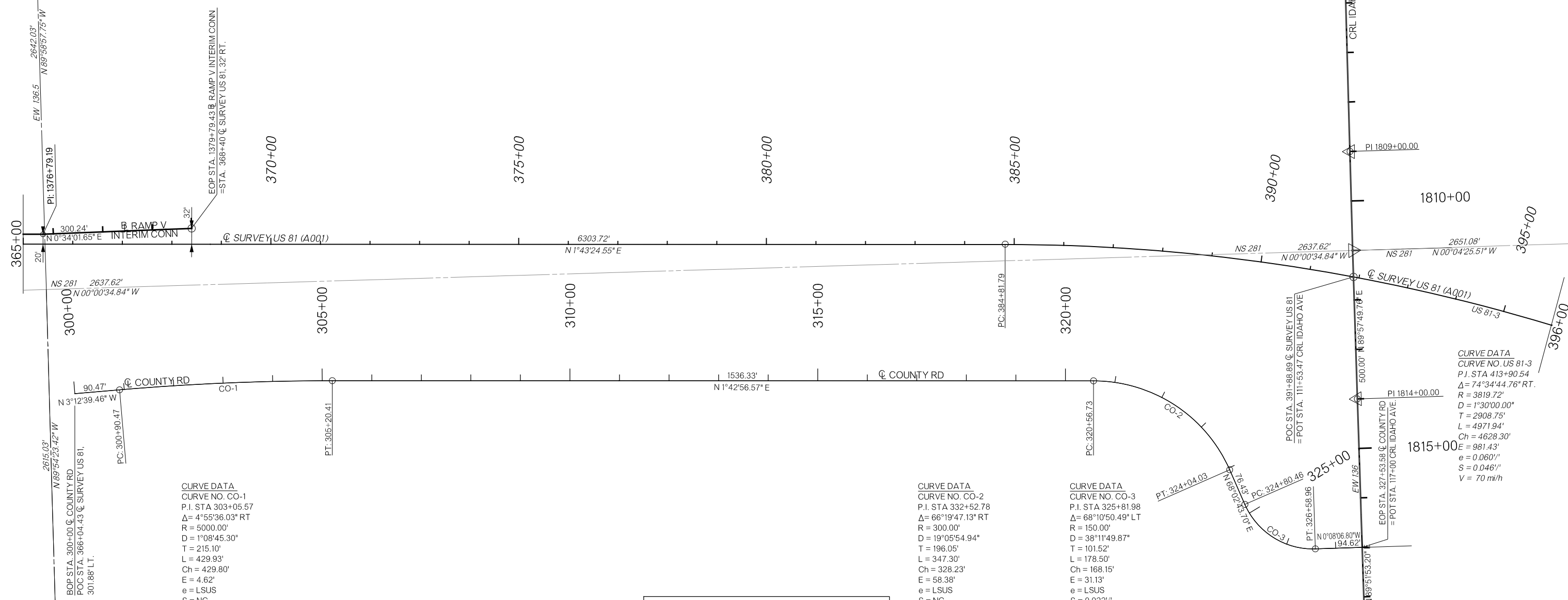
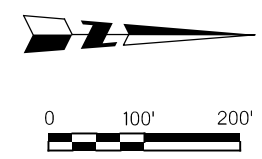
1800+00

SEC. 30 T7N R7W

1805+00

1810+00

1815+00



CURVE DATA
 CURVE NO. CO-1
 P.I. STA 303+05.57
 $\Delta = 4^{\circ}55'36.03''$ RT
 R = 5000.00'
 D = $1^{\circ}08'45.30''$
 T = 215.10'
 L = 429.93'
 Ch = 429.80'
 E = 4.62'
 e = LSUS
 S = NC
 V = 45 mi/h

CURVE DATA
 CURVE NO. CO-2
 P.I. STA 332+52.78
 $\Delta = 66^{\circ}19'47.13''$ RT
 R = 300.00'
 D = $19^{\circ}05'54.94''$
 T = 196.05'
 L = 347.30'
 Ch = 328.23'
 E = 58.38'
 e = LSUS
 S = NC
 V = 25 mi/h

CURVE DATA
 CURVE NO. CO-3
 P.I. STA 325+81.98
 $\Delta = 68^{\circ}10'50.49''$ LT
 R = 150.00'
 D = $38^{\circ}11'49.87''$
 T = 101.52'
 L = 178.50'
 Ch = 168.15'
 E = 31.13'
 e = LSUS
 S = NC
 V = 25 mi/h

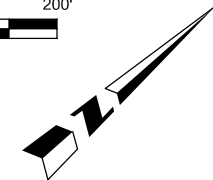
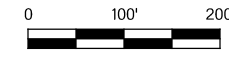
CURVE DATA
 CURVE NO. US 81-3
 P.I. STA 413+90.54
 $\Delta = 74^{\circ}34'44.76''$ RT.
 R = 3819.72'
 D = $1^{\circ}30'00.00''$
 T = 2908.75'
 L = 4971.94'
 Ch = 4628.30'
 E = 981.43'
 e = 0.0601''
 S = 0.0461''
 V = 70 mi/h

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS:
A001-A019

GEOMETRIC LAYOUT

SEC. 25, T7N R8W



CURVE DATA

CURVE IOWA-1
PI STA. 35+62.55
 $\Delta = 47^{\circ}30'12.04''$
R = 341.00'
D = $16^{\circ}48'08.21''$
T = 150.06'
L = 282.72'
Ch = 274.69'
E = 31.56'
e = 0.060'
S = 0.060'
V = 35 MPH

CURVE DATA

CURVE NO. W-1
PI STA. 403+71.09
 $\Delta = 16^{\circ}58'34.55''$
R = 1288.91'
D = $4^{\circ}26'43.08''$
T = 192.36'
L = 381.89'
Ch = 380.50'
E = 14.27'
e = 0.060'/'
S = 0.054'/'
V = 50 MPH

CURVE DATA

CURVE NO. W-2
PI STA. 409+86.99
 $\Delta = 36^{\circ}31'56.87''$
R = 637.34'
D = $8^{\circ}59'23.35''$
T = 210.36'
L = 406.36'
Ch = 399.53'
E = 33.82'
e = 0.060'/'
S = 0.052'/'
V = 35 MPH

CURVE DATA

CURVE NO. X-1
PI STA. 413+94.53
 $\Delta = 36^{\circ}38'02.19''$
R = 520.98'
D = $10^{\circ}59'51.76''$
T = 172.47'
L = 333.11'
Ch = 327.46'
E = 27.81'
e = 0.060'/'
S = 0.056'/'
V = 35 MPH

CURVE DATA

CURVE NO. X-2
PI STA. 419+78.69
 $\Delta = 17^{\circ}43'47.07''$
R = 1259.64'
D = $4^{\circ}32'54.85''$
T = 196.46'
L = 389.79'
Ch = 388.23'
E = 15.23'
e = 0.060'/'
S = 0.056'/'
V = 50 MPH

CURVE DATA

CURVE NO. CR-1
PI STA. 13+77.17
 $\Delta = 29^{\circ}27'45.84''$
R = 288.00'
D = $19^{\circ}53'39.72''$
T = 75.72'
L = 148.10'
Ch = 146.47'
E = 9.79'
e = LSUS
S = NC
V = 25 MPH

CURVE DATA

CURVE NO. US 81-3
P.I. STA. 413+90.54
 $\Delta = 74^{\circ}34'44.76''$
R = 3819.72'
D = $1^{\circ}30'00.00''$
T = 2908.75'
L = 4971.94'
Ch = 4628.30'
E = 981.43'
e = 0.060'/'
S = 0.046'/'
V = 70 mi/h

CURVE DATA

CURVE NO. Y-1
PI STA. 402+26.93
 $\Delta = 36^{\circ}07'45.02''$
R = 887.93'
D = $6^{\circ}27'09.89''$
T = 289.61'
L = 559.90'
Ch = 550.67'
E = 46.04'
e = 0.060'/'
S = 0.060'/'
V = 50 MPH

CURVE DATA

CURVE NO. Y-2
PI STA. 408+63.72
 $\Delta = 17^{\circ}11'20.16''$
R = 1025.56'
D = $5^{\circ}35'12.41''$
T = 155.00'
L = 307.67'
Ch = 306.52'
E = 11.65'
e = 0.060'/'
S = 0.042'/'
V = 35 MPH

CURVE DATA

CURVE IOWA-2
PI STA. 49+47.37
 $\Delta = 42^{\circ}46'53.06''$
R = 520.87'
D = $11^{\circ}00'00.05''$
T = 204.03'
L = 388.92'
Ch = 379.95'
E = 38.53'
e = 0.060'/'
S = 0.056'/'
V = 35 MPH

CURVE DATA

CURVE NO. Z-1
PI STA. 415+48.00
 $\Delta = 15^{\circ}17'37.10''$
R = 1125.00'
D = $5^{\circ}05'34.65''$
T = 151.04'
L = 300.29'
Ch = 299.40'
E = 10.09'
e = 0.060'/'
S = 0.040'/'
V = 35 MPH

CURVE DATA

CURVE NO. Z-2
PI STA. 422+49.17
 $\Delta = 35^{\circ}54'58.57''$
R = 1075.77'
D = $5^{\circ}19'33.67''$
T = 348.67'
L = 674.35'
Ch = 663.37'
E = 55.09'
e = 0.060'/'
S = 0.058'/'
V = 50 MPH

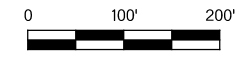
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEC. 30, T7N R7W

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING SEGMENTS:
A001-A019

GEOMETRIC LAYOUT

US 81 REALIGNMENT
GRADY COUNTY

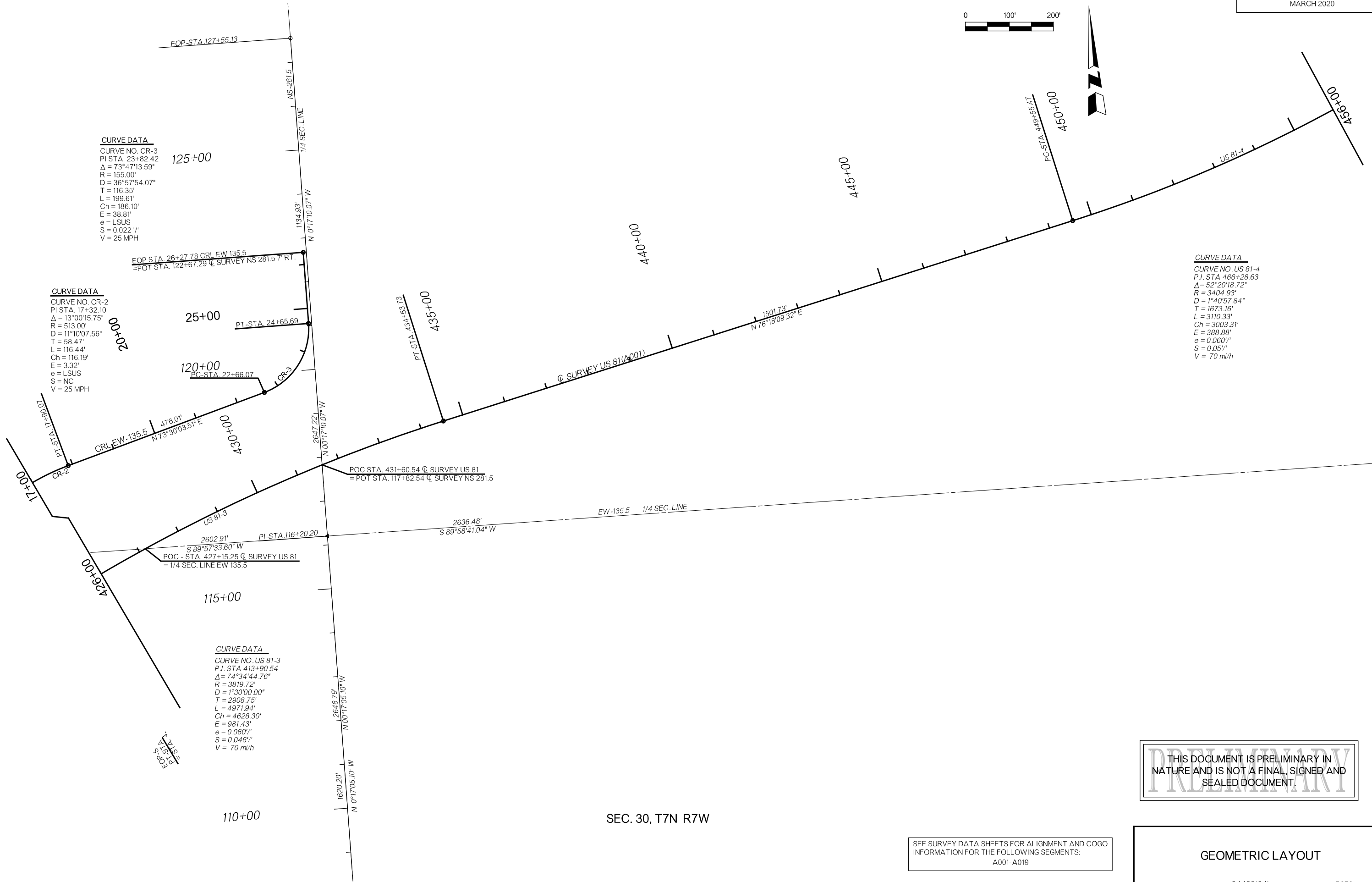


CURVE DATA
 CURVE NO. CR-3
 P.I. STA. 23+82.42
 $\Delta = 73^\circ 47' 13.59''$
 R = 155.00'
 $D = 36^\circ 57' 54.07''$
 T = 116.35'
 L = 199.61'
 Ch = 186.10'
 E = 38.81'
 e = LSUS
 S = 0.022 1/1
 V = 25 MPH

CURVE DATA
 CURVE NO. CR-2
 P.I. STA. 17+32.10
 $\Delta = 13^\circ 00' 15.75''$
 R = 513.00'
 $D = 11^\circ 10' 07.56''$
 T = 58.47'
 L = 116.44'
 Ch = 116.19'
 E = 3.32'
 e = LSUS
 S = NC
 V = 25 MPH

CURVE DATA
 CURVE NO. US 81-4
 P.I. STA 466+28.63
 $\Delta = 52^\circ 20' 18.72''$
 R = 3404.93'
 $D = 1^\circ 40' 57.84''$
 T = 1673.16'
 L = 3110.33'
 Ch = 3003.31'
 E = 388.88'
 e = 0.060 1/1
 S = 0.05 1/1
 V = 70 mi/h

CURVE DATA
 CURVE NO. US 81-3
 P.I. STA 413+90.54
 $\Delta = 74^\circ 34' 44.76''$
 R = 3819.72'
 $D = 1^\circ 30' 00.00''$
 T = 2908.75'
 L = 4971.94'
 Ch = 4628.30'
 E = 981.43'
 e = 0.060 1/1
 S = 0.046 1/1
 V = 70 mi/h



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEC. 30, T7N R7W

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING SEGMENTS:
A001-A019

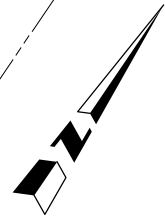
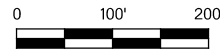
GEOMETRIC LAYOUT
 State Job No. 24428(04) Sheet No. R078

US 81 REALIGNMENT
GRADY COUNTY

SEC. 20, T7N R7W

SEC. 30, T7N R7W

SEC. 29, T7N R7W



CURVE DATA

CURVE NO. AA-2
PI STA. 485+17.91
 $\Delta = 24^\circ 38' 57.30''$
R = 978.46'
D = 5°51'20.65"
T = 213.78'
L = 420.94'
Ch = 417.70'
E = 23.08'
e = 0.060'/'
S = 0.042'/'
V = 35 MPH

CURVE DATA

CURVE NO. AA-1
PI STA. 475+20.40
 $\Delta = 37^\circ 22' 01.79''$
R = 1920.91'
D = 2°58'57.86"
T = 649.58'
L = 1252.78'
Ch = 1230.70'
E = 106.86'
e = 0.060'/'
S = 0.046'/'
V = 50 MPH

CURVE DATA

CURVE NO. CC-3
PI STA. 485+69.88
D = 6°01'52.08"
R = 950.00'
T = 133.57'
L = 265.39'
Ch = 264.53'
E = 9.34'
e = 0.060'/'
S = 0.044'/'
V = 35 MPH

CURVE DATA

CURVE NO. US 81-5
P.I. STA 510+85.30
 $\Delta = 23^\circ 48' 44.87''$
R = 14320.85'
D = 000°24'00.31"
T = 3019.50'
L = 5951.83'
Ch = 5909.09'
E = 314.86'
e = 0.060'/'
S = NC
V = 70 mi/h

CURVE DATA

CURVE NO. CC-2
PI STA. 474+12.91
R = 9000.00'
D = 0°38'11.83"
T = 530.78'
L = 1060.33'
Ch = 1059.71'
E = 15.64'
e = 0.060'/'
S = 0.024'/'
V = 70 MPH

CURVE DATA

CURVE NO. US 81-4
P.I. STA 466+28.63
 $\Delta = 52^\circ 20' 18.72''$
R = 3404.93'
D = 1°40'57.84"
T = 1673.16'
L = 3110.33'
Ch = 3003.31'
E = 388.88'
e = 0.060'/'
S = 0.05'/'
V = 70 mi/h

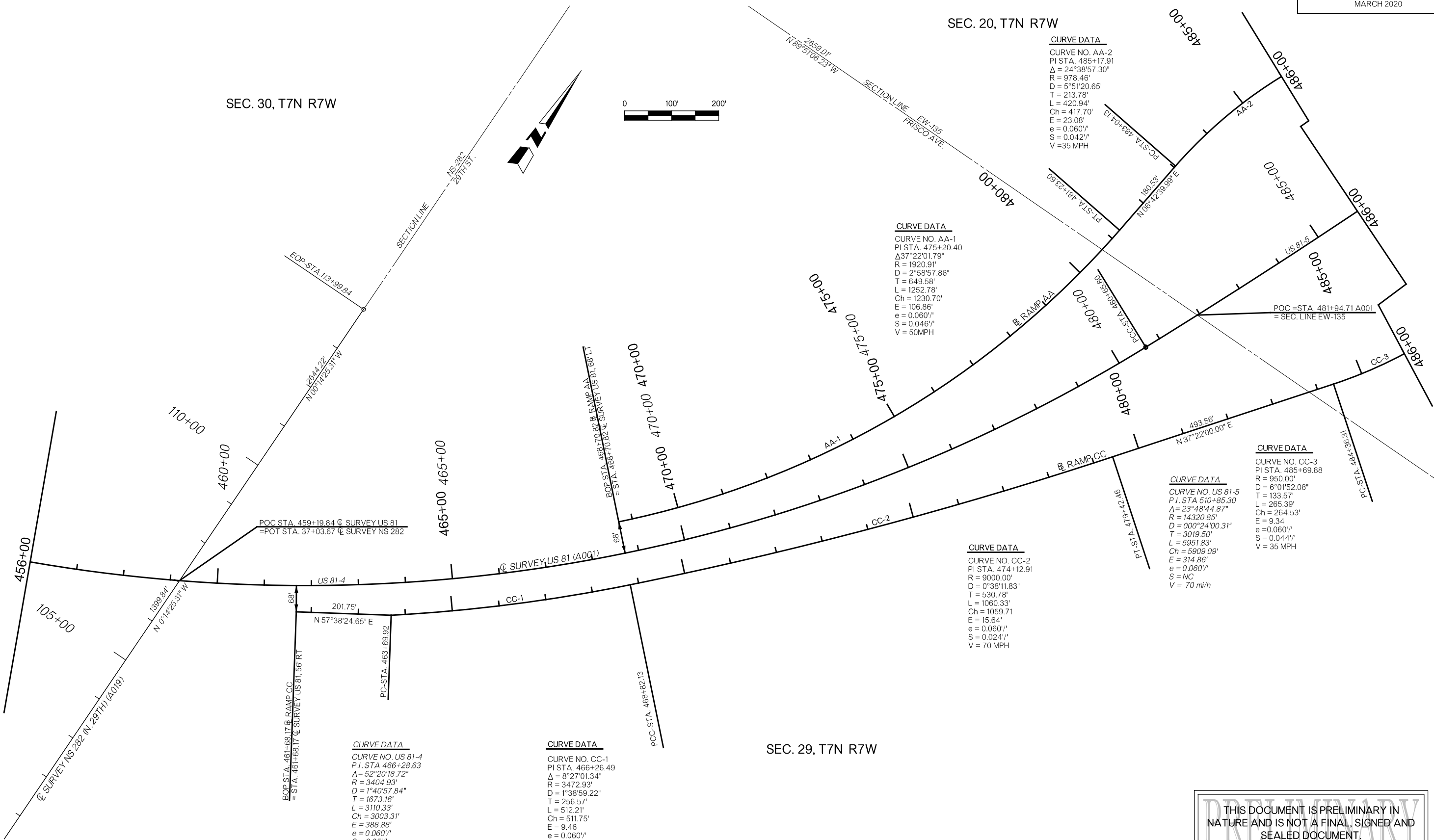
CURVE DATA

CURVE NO. CC-1
PI STA. 466+26.49
 $\Delta = 8^\circ 27' 01.34''$
R = 3472.93'
D = 1°38'59.22"
T = 256.57'
L = 512.21'
Ch = 511.75'
E = 9.46'
e = 0.060'/'
S = 0.05'/'
V = 70 MPH

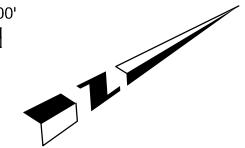
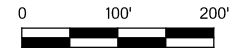
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING SEGMENTS:
A001-A019

GEOMETRIC LAYOUT
State Job No. 24428(04) Sheet No. R079



US 81 REALIGNMENT GRADY COUNTY



CURVE DATA

CURVE NO. AA-2
P.I. STA. 485+17.91
 $\Delta = 24^\circ 38' 57.30''$
R = 978.46'
D = $5^\circ 51' 20.65''$
T = 213.78'
L = 420.94'
Ch = 417.70'
E = 23.08'
e = 0.060'/'
S = 0.042'/'
V = 35 MPH

CURVE DATA

CURVE NO. BB-1
P.I. STA. 493+08.77
 $\Delta = 1^\circ 38' 21.55''$
R = 11321.10
D = $0^\circ 30' 21.95''$
T = 161.97'
L = 323.91'
Ch = 323.90'
E = 1.16
e = 0.060'/'
S = NC
V = 35 MPH

CURVE DATA

CURVE NO. BB-2
P.I. = 498+34.84
 $\Delta = 16^\circ 55' 40.57''$
R = 1388.50'
D = $4^\circ 07' 35.21''$
T = 206.62'
L = 410.23'
Ch = 408.74'
E = 15.29'
e = 0.060'/'
S = 0.054'/'
V = 50 MPH

CURVE DATA

CURVE NO. BB-1
P.I. STA. 499+92.99
 $\Delta = 017^\circ 41' 58.94''$
R = 4260.0000'
D = $001^\circ 20' 41.90''$
T = 663.2788'
L = 1315.9914'
Ch = 1310.7649'
E = 51.3268'
e = 0.060'/'
S =
V = 720 mi/h

CURVE DATA

CURVE NO. DD-2
P.I. STA. 496+96.73
 $\Delta = 22^\circ 54' 57.98''$
R = 1420.28'
D = $4^\circ 02' 02.78''$
T = 287.88'
L = 568.06'
Ch = 564.28'
E = 28.88'
e = 0.060'/'
S = 0.052'/'
V = 50 MPH

CURVE DATA

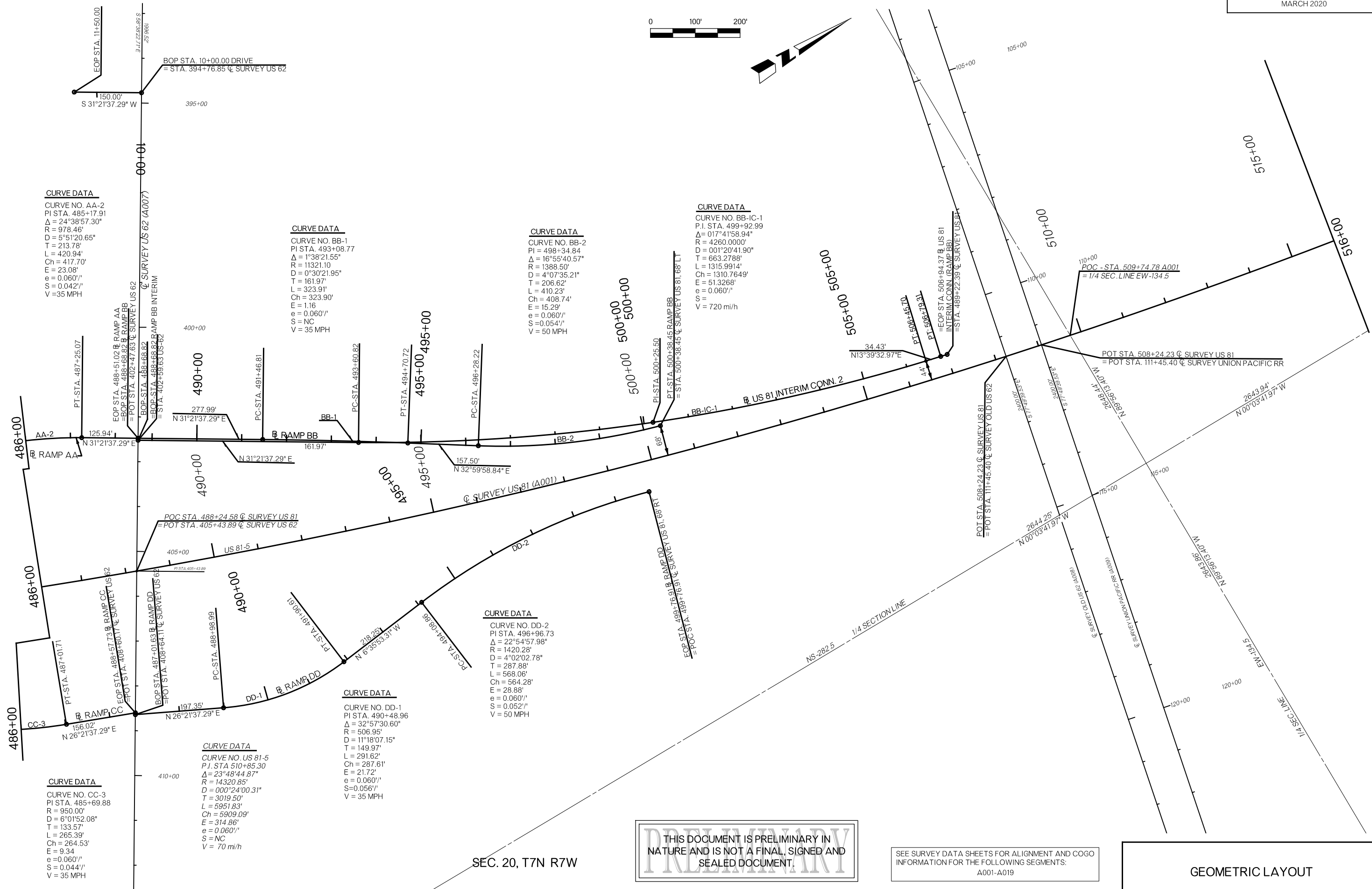
CURVE NO. DD-1
P.I. STA. 490+48.96
 $\Delta = 32^\circ 57' 30.60''$
R = 506.95'
D = $11^\circ 18' 07.15''$
T = 149.97'
L = 291.62'
Ch = 287.61'
E = 21.72'
e = 0.060'/'
S = 0.056'/'
V = 35 MPH

CURVE DATA

CURVE NO. US 81-5
P.I. STA 510+85.30
 $\Delta = 23^\circ 48' 44.87''$
R = 14320.85'
D = $000^\circ 24' 00.31''$
T = 3019.50'
L = 5951.83'
Ch = 5909.09'
E = 314.86'
e = 0.060'/'
S = NC
V = 70 mi/h

CURVE DATA

CURVE NO. CC-3
P.I. STA. 485+69.88
R = 950.00'
D = $6^\circ 01' 52.08''$
T = 133.57'
L = 265.39'
Ch = 264.53'
E = 9.34'
e = 0.060'/'
S = 0.044'/'
V = 35 MPH



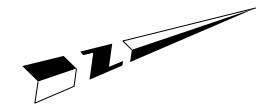
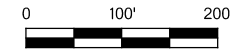
SEC. 20, T7N R7W

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

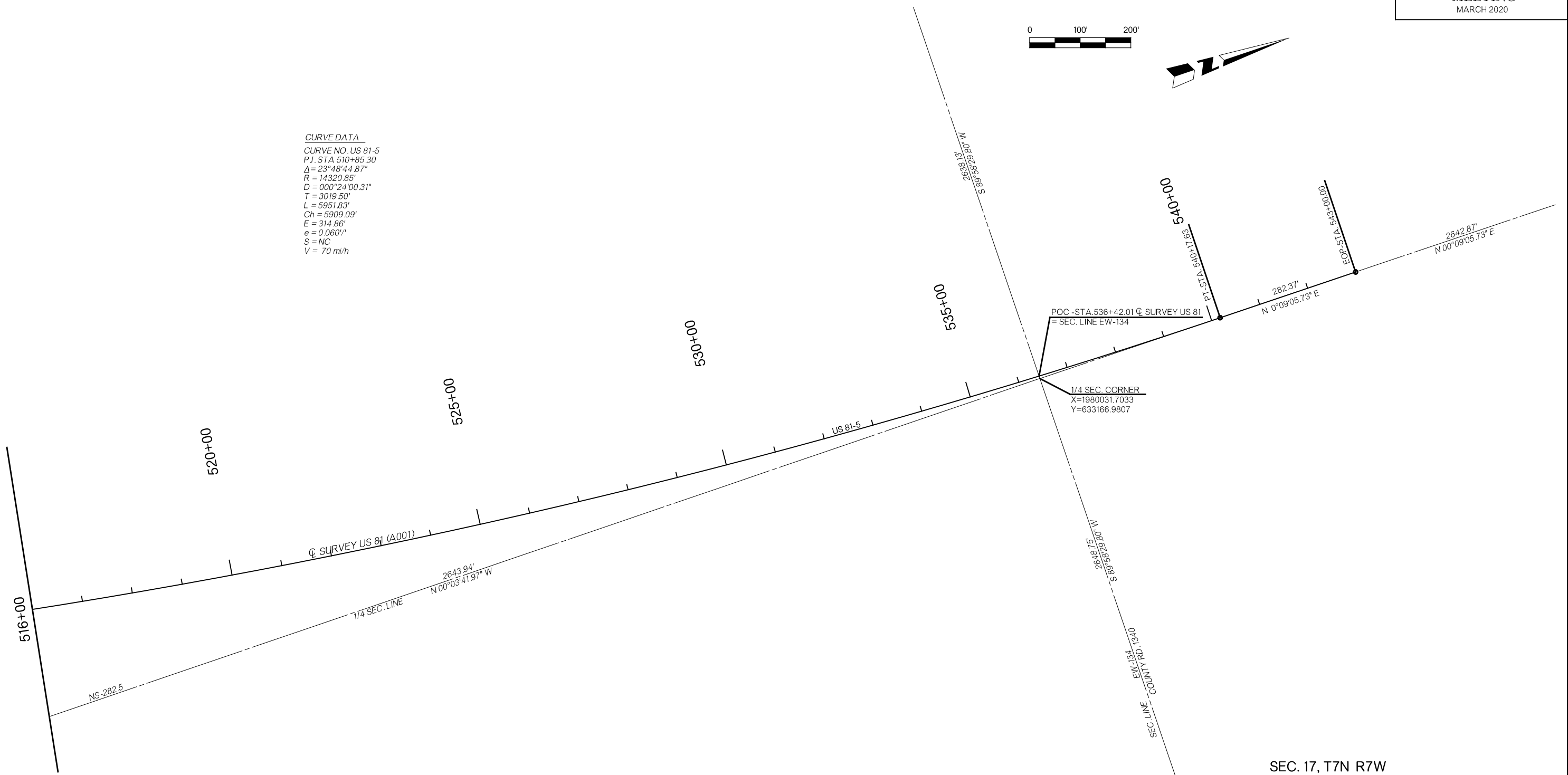
SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING SEGMENTS:
A001-A019

GEOMETRIC LAYOUT

GRADY COUNTY US 81 REALIGNMENT



CURVE DATA
 CURVE NO. US 81-5
 P.I. STA 510+85.30
 $\Delta = 23^\circ 48' 44.87''$
 $R = 14320.85'$
 $D = 000^\circ 24' 00.31''$
 $T = 3019.50'$
 $L = 5951.83'$
 $Ch = 5909.09'$
 $E = 314.86'$
 $e = 0.0601'$
 $S = NC$
 $V = 70 \text{ mi/h}$



SEC. 20, T7N R7W

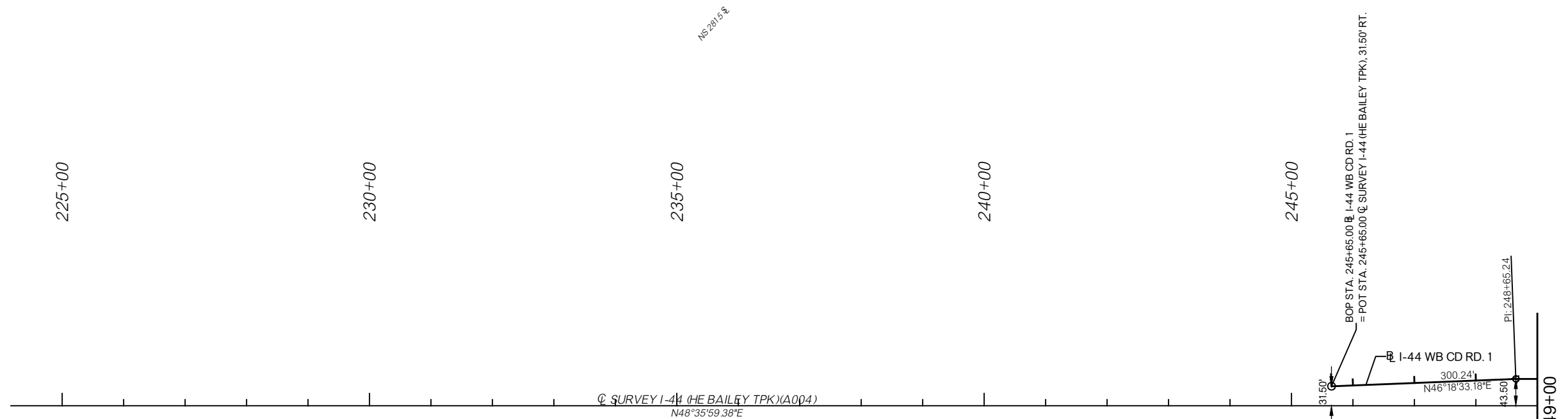
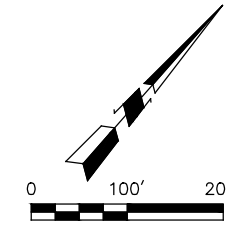
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING SEGMENTS:
A001-A019

GEOMETRIC LAYOUT
 State Job No. 24428(04) Sheet No. R081

SEC. 17, T7N R7W

US 81 REALIGNMENT
GRADY COUNTY



2621.54
N 89° 48' 53.22\"/>

225+00

230+00

235+00

240+00

245+00

249+00

☉ SURVEY I-44 (HE BAILEY TPK) (A004)
N48°35'59.38\"/>

2687.23
N 00° 07' 34.53\"/>

2637.32
N 89° 48' 57.22\"/>

EW 140.8

BOP STA. 245+65.00 @ I-44 WB CD RD. 1
= POT STA. 245+65.00 @ SURVEY I-44 (HE BAILEY TPK), 31.50' RT.

31.50'
300.24'
N46°18'33.18\"/>

PI: 248+65.24

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEC. 18 T6N R7W
SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS:
A001 - A019

GEOMETRIC LAYOUT

CURVE DATA
 CURVE NO. WB CD-1
 P.I. STA. 257+97.48
 $\Delta = 003^{\circ}00'56.04''$
 R = 5700.0000'
 D = 001^{\circ}00'18.68"
 T = 150.0346'
 L = 300.0000'
 Ch = 299.9654'
 E = 1.9743'
 e = 0.061'
 S = 0.0341'
 V = 70 mi/h

CURVE DATA
 CURVE NO. WB CD-2
 P.I. STA. 262+75.80
 $\Delta = 003^{\circ}00'56.04''$
 R = 5700.0000'
 D = 001^{\circ}00'18.68"
 T = 150.0346'
 L = 300.0000'
 Ch = 299.9654'
 E = 1.9743'
 e = 0.061'
 S = RC
 V = 50 mi/h

CURVE DATA
 CURVE NO. WB CD-3
 P.I. STA. 282+36.79
 $\Delta = 007^{\circ}20'00.00''$
 R = 17135.2340'
 D = 000^{\circ}20'03.75"
 T = 1098.0757'
 L = 2193.1525'
 Ch = 2191.6558'
 E = 35.1479'
 e = 0.061'
 S = NC
 V = 50 mi/h

CURVE DATA
 CURVE NO. J-1
 P.I. STA. 2202+07.97
 $\Delta = 093^{\circ}28'40.68''$
 R = 1700.0000'
 D = 003^{\circ}22'13.22"
 T = 1806.4572'
 L = 2773.5471'
 Ch = 2476.0133'
 E = 780.5821'
 e = 0.061'
 S = 0.0481'
 V = 50 mi/h

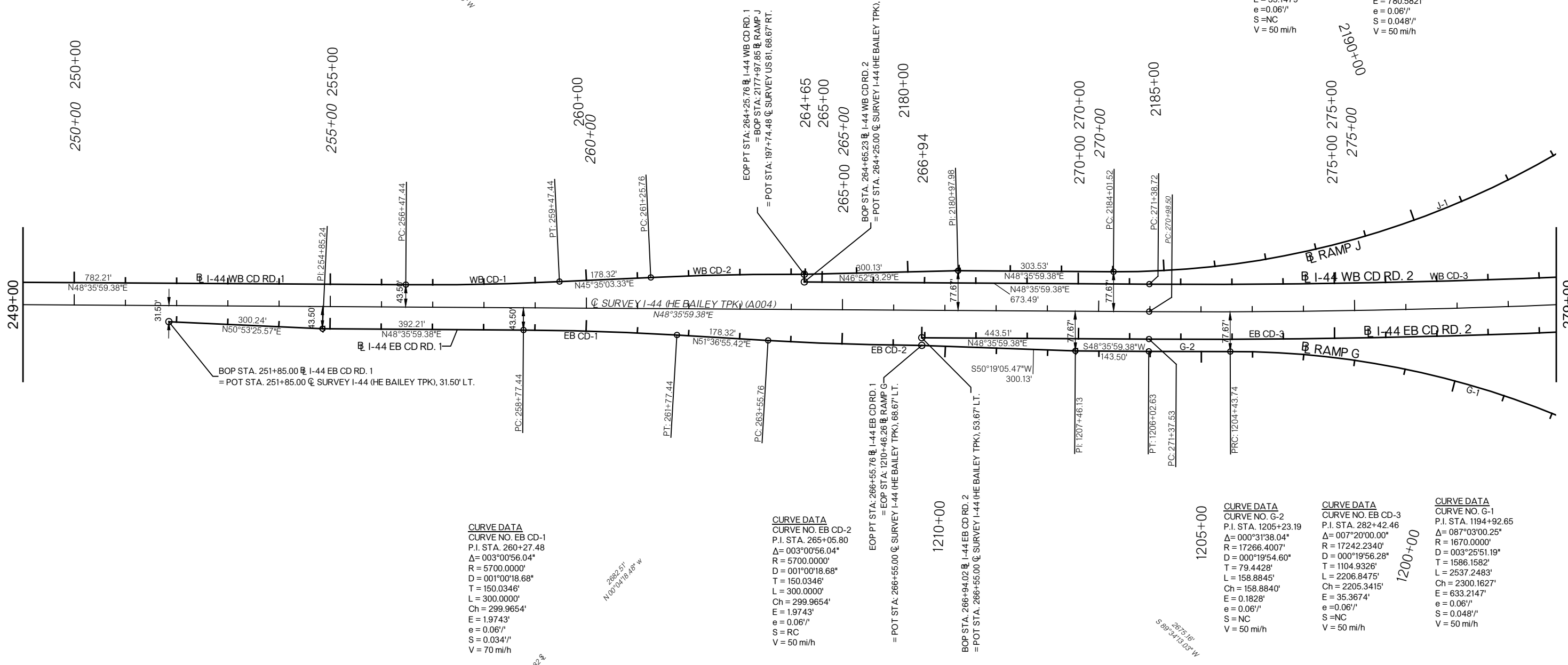
CURVE DATA
 CURVE NO. EB CD-1
 P.I. STA. 260+27.48
 $\Delta = 003^{\circ}00'56.04''$
 R = 5700.0000'
 D = 001^{\circ}00'18.68"
 T = 150.0346'
 L = 300.0000'
 Ch = 299.9654'
 E = 1.9743'
 e = 0.061'
 S = 0.0341'
 V = 70 mi/h

CURVE DATA
 CURVE NO. EB CD-2
 P.I. STA. 265+05.80
 $\Delta = 003^{\circ}00'56.04''$
 R = 5700.0000'
 D = 001^{\circ}00'18.68"
 T = 150.0346'
 L = 300.0000'
 Ch = 299.9654'
 E = 1.9743'
 e = 0.061'
 S = RC
 V = 50 mi/h

CURVE DATA
 CURVE NO. G-2
 P.I. STA. 1205+23.19
 $\Delta = 000^{\circ}31'38.04''$
 R = 17266.4007'
 D = 000^{\circ}19'54.60"
 T = 79.4428'
 L = 158.8845'
 Ch = 158.8840'
 E = 0.1828'
 e = 0.061'
 S = NC
 V = 50 mi/h

CURVE DATA
 CURVE NO. EB CD-3
 P.I. STA. 282+42.46
 $\Delta = 007^{\circ}20'00.00''$
 R = 17242.2340'
 D = 000^{\circ}19'56.28"
 T = 1104.9326'
 L = 2206.8475'
 Ch = 2205.3415'
 E = 35.3674'
 e = 0.061'
 S = NC
 V = 50 mi/h

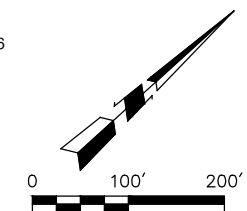
CURVE DATA
 CURVE NO. G-1
 P.I. STA. 1194+92.65
 $\Delta = 087^{\circ}03'00.25''$
 R = 1670.0000'
 D = 003^{\circ}25'51.19"
 T = 1586.1582'
 L = 2537.2483'
 Ch = 2300.1627'
 E = 633.2147'
 e = 0.061'
 S = 0.0481'
 V = 50 mi/h



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEC. 17 T6N R7W
 SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019

GEOMETRIC LAYOUT



CURVE DATA
CURVE NO. I-1
P.I. STA. 317+04.12
 $\Delta = 001^{\circ}09'28.41''$
R = 17111.0673'
D = 000^{\circ}20'05.45"
T = 172.9050'
L = 345.7982'
Ch = 345.7923'
E = 0.8736'
e = 0.061'
S = NC
V = 30 mi/h

CURVE DATA
CURVE NO. I-2
P.I. STA. 316+33.54
 $\Delta = 271^{\circ}30'50.17''$
R = 250.0000'
D = 022^{\circ}55'05.92"
T = 243.4800'
L = 1184.7030'
Ch = 348.8517'
E = 598.9735'
e = 0.061'
S = 0.061'
V = 30 mi/h

CURVE DATA
CURVE NO. J-1
P.I. STA. 2202+07.97
 $\Delta = 093^{\circ}28'40.68''$
R = 1700.0000'
D = 003^{\circ}22'13.22"
T = 1806.4572'
L = 2773.5471'
Ch = 2476.0133'
E = 780.5821'
e = 0.061'
S = 0.0481'
V = 50 mi/h

CURVE DATA
CURVE NO. M-1
P.I. STA. 297+32.93
 $\Delta = 266^{\circ}08'37.79''$
R = 250.0000'
D = 022^{\circ}55'05.92"
T = 267.4184'
L = 1161.2715'
Ch = 365.2486'
E = 616.0774'
e = 0.061'
S = 0.061'
V = 30 mi/h

CURVE DATA
CURVE NO. M-2
P.I. STA. 313+54.38
 $\Delta = 001^{\circ}11'03.63''$
R = 17111.0672'
D = 000^{\circ}20'05.45"
T = 176.8547'
L = 353.6968'
Ch = 353.6905'
E = 0.9139'
e = 0.061'
S = NC
V = 30 mi/h

CURVE DATA
CURVE NO. N-1
P.I. STA. 4200+17.46
 $\Delta = 093^{\circ}51'48.47''$
R = 1700.0000'
D = 003^{\circ}22'13.22"
T = 1818.6774'
L = 2784.9849'
Ch = 2483.8379'
E = 789.4954'
e = 0.061'
S = 0.0481'
V = 50 mi/h

CURVE DATA
CURVE NO. EB CD-3
P.I. STA. 282+42.46
 $\Delta = 007^{\circ}20'00.00''$
R = 17242.2340'
D = 000^{\circ}19'56.28"
T = 1104.9326'
L = 2206.8475'
Ch = 2205.3415'
E = 35.3674'
e = 0.061'
S = NC
V = 50 mi/h

CURVE DATA
CURVE NO. H-1
P.I. STA. 192+00.00
 $\Delta = 268^{\circ}32'33.78''$
R = 250.0000'
D = 022^{\circ}55'05.92"
T = 256.4409'
L = 1171.7386'
Ch = 358.0209'
E = 608.1367'
e = 0.061'
S = 0.061'
V = 30 mi/h

CURVE DATA
CURVE NO. H-2
P.I. STA. 208+39.69
 $\Delta = 001^{\circ}11'25.77''$
R = 17266.4007'
D = 000^{\circ}19'54.60"
T = 179.3871'
L = 358.7512'
Ch = 358.7548'
E = 0.9318'
e = 0.061'
S = NC
V = 30 mi/h

CURVE DATA
CURVE NO. K-1
P.I. STA. 3195+77.25
 $\Delta = 086^{\circ}08'37.79''$
R = 1940.0000'
D = 002^{\circ}57'12.21"
T = 1813.6370'
L = 2916.7773'
Ch = 2649.7132'
E = 715.7257'
e = 0.061'
S = 0.0441'
V = 50 mi/h

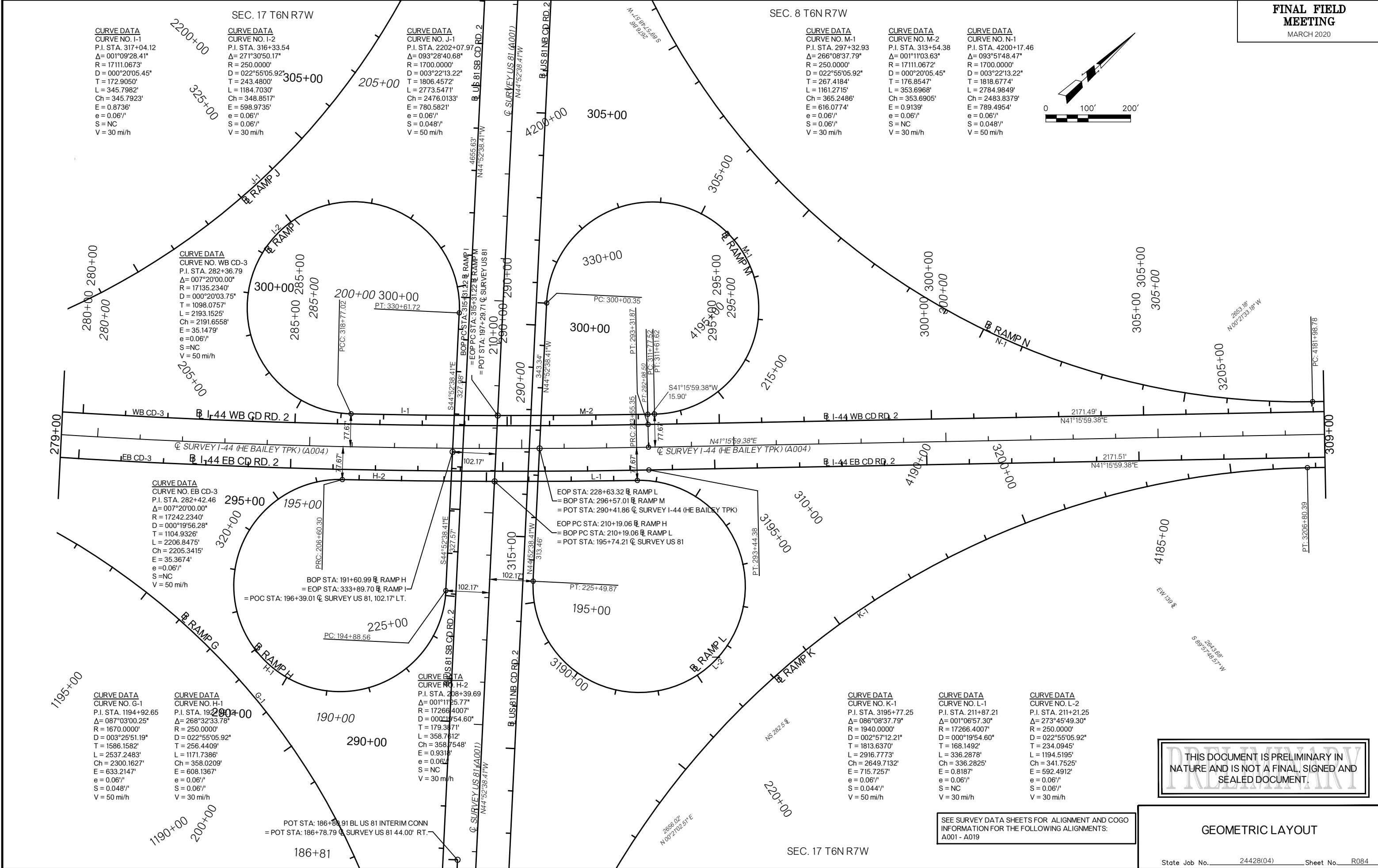
CURVE DATA
CURVE NO. L-1
P.I. STA. 211+87.21
 $\Delta = 001^{\circ}06'57.30''$
R = 17266.4007'
D = 000^{\circ}19'54.60"
T = 168.1492'
L = 336.2878'
Ch = 336.2825'
E = 0.8187'
e = 0.061'
S = NC
V = 30 mi/h

CURVE DATA
CURVE NO. L-2
P.I. STA. 211+21.25
 $\Delta = 273^{\circ}45'49.30''$
R = 250.0000'
D = 022^{\circ}55'05.92"
T = 234.0945'
L = 1194.5195'
Ch = 341.7525'
E = 592.4912'
e = 0.061'
S = 0.061'
V = 30 mi/h

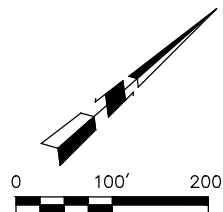
SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GEOMETRIC LAYOUT



GRADY COUNTY US 81 REALIGNMENT

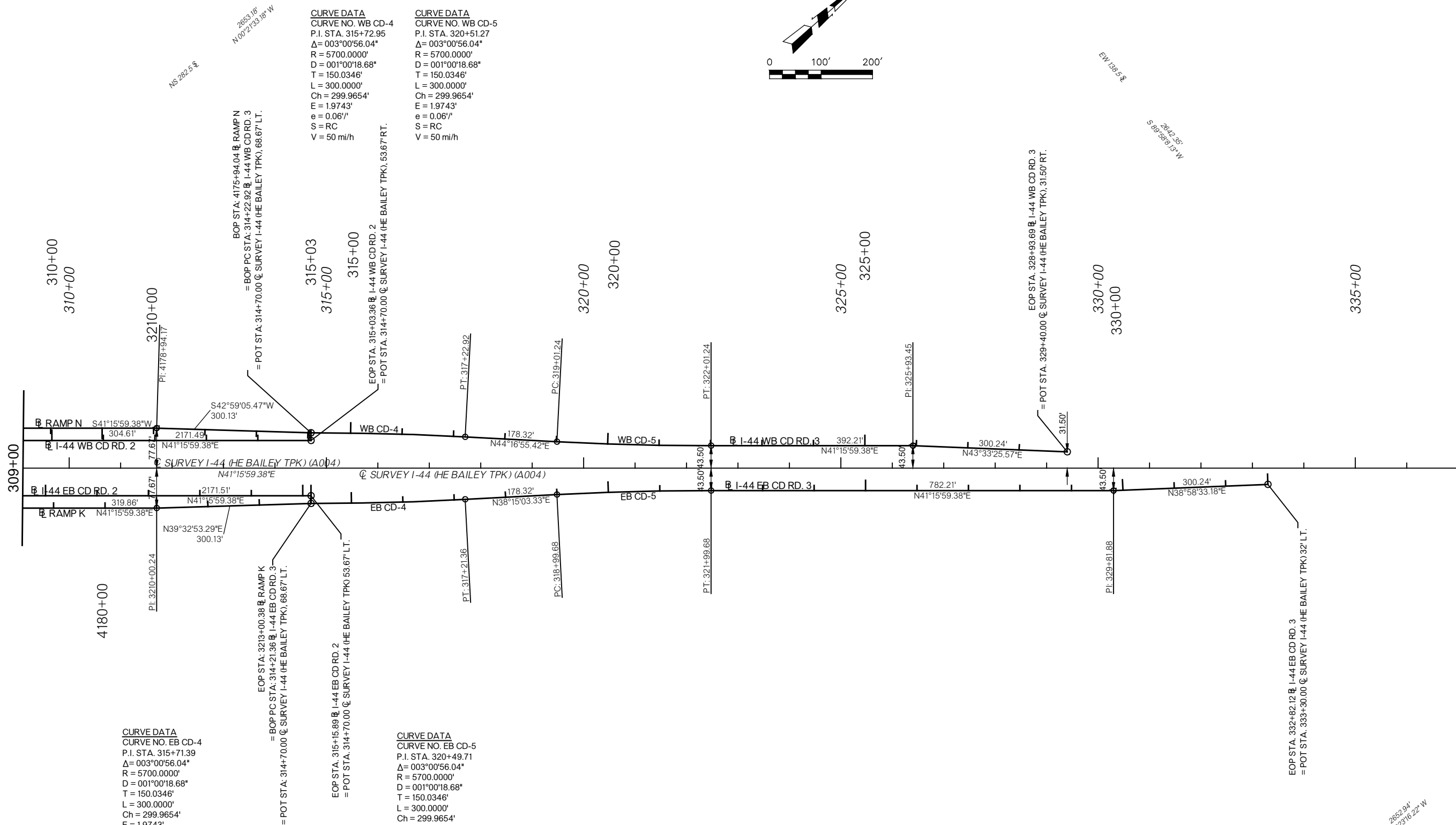


CURVE DATA
 CURVE NO. WB CD-4
 P.I. STA. 315+72.95
 $\Delta = 003^{\circ}00'56.04''$
 R = 5700.0000'
 D = 001^{\circ}00'18.68''
 T = 150.0346'
 L = 300.0000'
 Ch = 299.9654'
 E = 1.9743'
 e = 0.061'
 S = RC
 V = 50 mi/h

CURVE DATA
 CURVE NO. WB CD-5
 P.I. STA. 320+51.27
 $\Delta = 003^{\circ}00'56.04''$
 R = 5700.0000'
 D = 001^{\circ}00'18.68''
 T = 150.0346'
 L = 300.0000'
 Ch = 299.9654'
 E = 1.9743'
 e = 0.061'
 S = RC
 V = 50 mi/h

CURVE DATA
 CURVE NO. EB CD-4
 P.I. STA. 315+71.39
 $\Delta = 003^{\circ}00'56.04''$
 R = 5700.0000'
 D = 001^{\circ}00'18.68''
 T = 150.0346'
 L = 300.0000'
 Ch = 299.9654'
 E = 1.9743'
 e = 0.061'
 S = RC
 V = 50 mi/h

CURVE DATA
 CURVE NO. EB CD-5
 P.I. STA. 320+49.71
 $\Delta = 003^{\circ}00'56.04''$
 R = 5700.0000'
 D = 001^{\circ}00'18.68''
 T = 150.0346'
 L = 300.0000'
 Ch = 299.9654'
 E = 1.9743'
 e = 0.061'
 S = RC
 V = 50 mi/h



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS:
A001 - A019

GEOMETRIC LAYOUT

| CURVE DATA | CURVE DATA | CURVE DATA | CURVE DATA |
|---|---|--|---|
| CURVE NO. A-1 P.I. STA. 137+46.08 Δ= 014°01'06.35" R = 5661.5801' D = 001°00'43.24" T = 696.0790' L = 1385.2062' Ch = 1381.7537' E = 42.6302' e = 0.06'/' S = 0.034'/' V = 70 mi/h | CURVE NO. A-2 P.I. STA. 140+77.40 Δ= 249°53'03.76" R = 250.0000' D = 022°55'05.92" T = 357.8048' L = 1090.3263' Ch = 409.8652' E = 686.4909' e = 0.06'/' S = 0.06'/' V = 30 mi/h | CURVE NO. B-1 P.I. STA. 245+72.52 Δ= 010°23'39.28" R = 1660.0000' D = 003°27'05.59" T = 150.9878' L = 301.1469' Ch = 300.7341' E = 6.8525' e = 0.06'/' S = 0.048'/' V = 50 mi/h | CURVE NO. B-2 P.I. STA. 254+13.01 Δ= 044°10'35.95" R = 1070.0000' D = 005°21'17.08" T = 434.2280' L = 825.0000' Ch = 804.7159' E = 84.7528' e = 0.06'/' S = 0.058'/' V = 50 mi/h |

| CURVE DATA | CURVE DATA | CURVE DATA |
|---|--|---|
| CURVE NO. D-1 P.I. STA. 240+15.52 Δ= 094°01'15.99" R = 1450.0000' D = 003°57'05.16" T = 1555.5090' L = 2379.4180' Ch = 2121.2900' E = 676.5249' e = 0.06'/' S = 0.052'/' V = 50 mi/h | CURVE NO. US 81-1 P.I. STA. 147+93.78 Δ= 044°48'32.01" R = 5729.5801' D = 001°00'00.00" T = 2362.0826' L = 4480.8909' Ch = 4367.5686' E = 467.8002' e = 0.06'/' S = 0.06'/' V = 70 mi/h | CURVE NO. A-1EXT P.I. STA. 245+52.42 Δ= 032°44'07.81" R = 267.0000' D = 021°27'32.74" T = 78.4191' L = 152.5484' Ch = 150.4820' E = 11.2778' e = 0.06'/' S = 0.06'/' V = 30 mi/h |

EOP PT STA: 246+26.56 @ RAMP A EXT
= POC STA: 246+26.56 @ RAMP A EXT 17' RT.

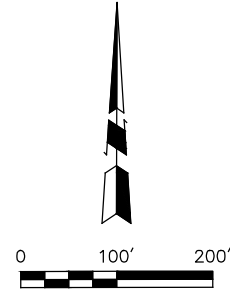
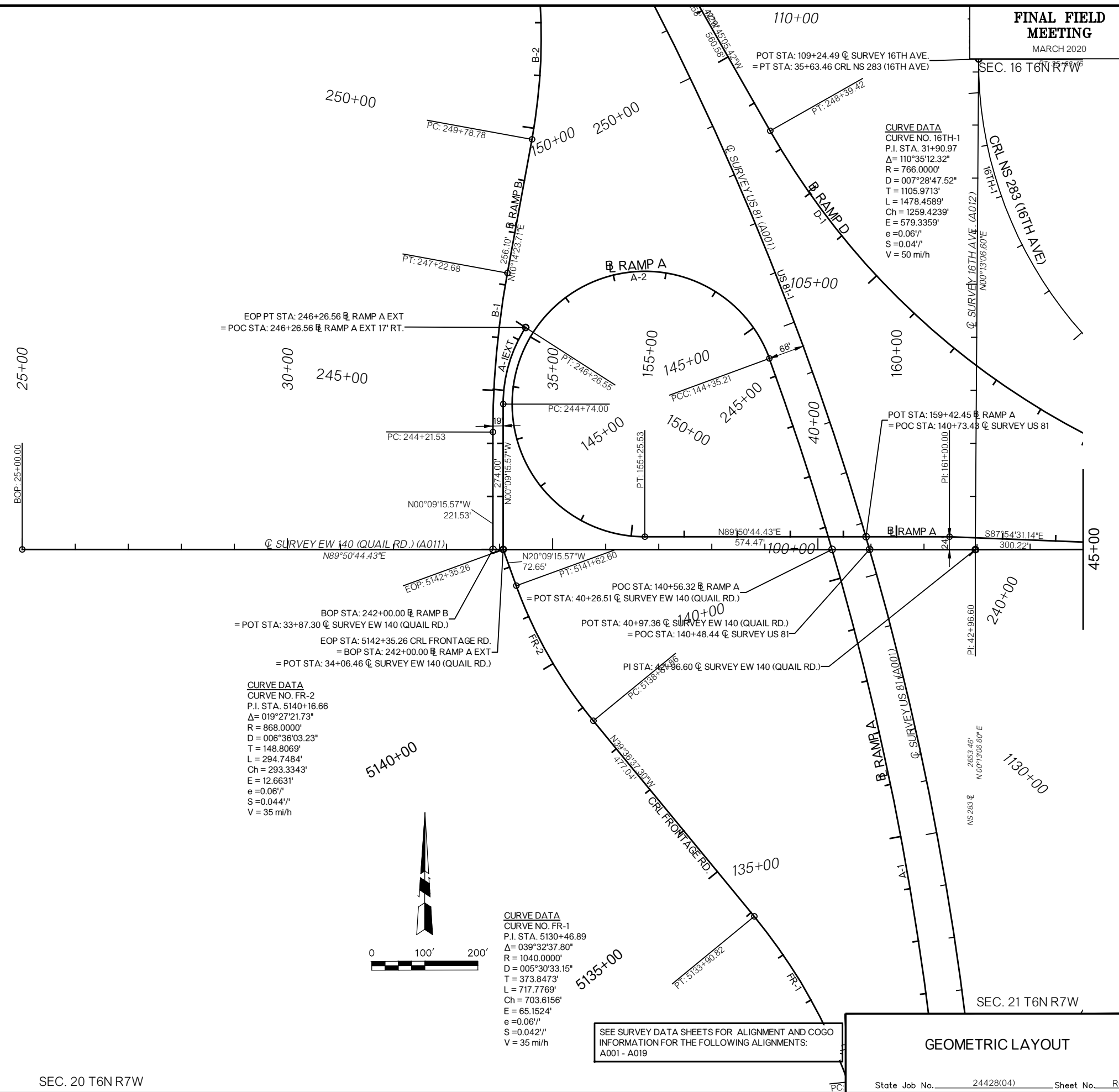
BOP STA: 242+00.00 @ RAMP B
= POT STA: 33+87.30 @ SURVEY EW 140 (QUAIL RD.)
EOP STA: 5142+35.26 @ CRL FRONTAGE RD.
= BOP STA: 242+00.00 @ RAMP A EXT
= POT STA: 34+06.46 @ SURVEY EW 140 (QUAIL RD.)

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019

| CURVE DATA |
|---|
| CURVE NO. 16TH-1 P.I. STA. 31+90.97 Δ= 110°35'12.32" R = 766.0000' D = 007°28'47.52" T = 1105.9713' L = 1478.4589' Ch = 1259.4239' E = 579.3359' e = 0.06'/' S = 0.04'/' V = 50 mi/h |

| CURVE DATA |
|--|
| CURVE NO. FR-2 P.I. STA. 5140+16.66 Δ= 019°27'21.73" R = 868.0000' D = 006°36'03.23" T = 148.8069' L = 294.7484' Ch = 293.3343' E = 12.6631' e = 0.06'/' S = 0.044'/' V = 35 mi/h |

| CURVE DATA |
|---|
| CURVE NO. FR-1 P.I. STA. 5130+46.89 Δ= 039°32'37.80" R = 1040.0000' D = 005°30'33.15" T = 373.8473' L = 717.7769' Ch = 703.6156' E = 65.1524' e = 0.06'/' S = 0.042'/' V = 35 mi/h |

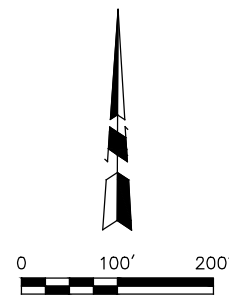
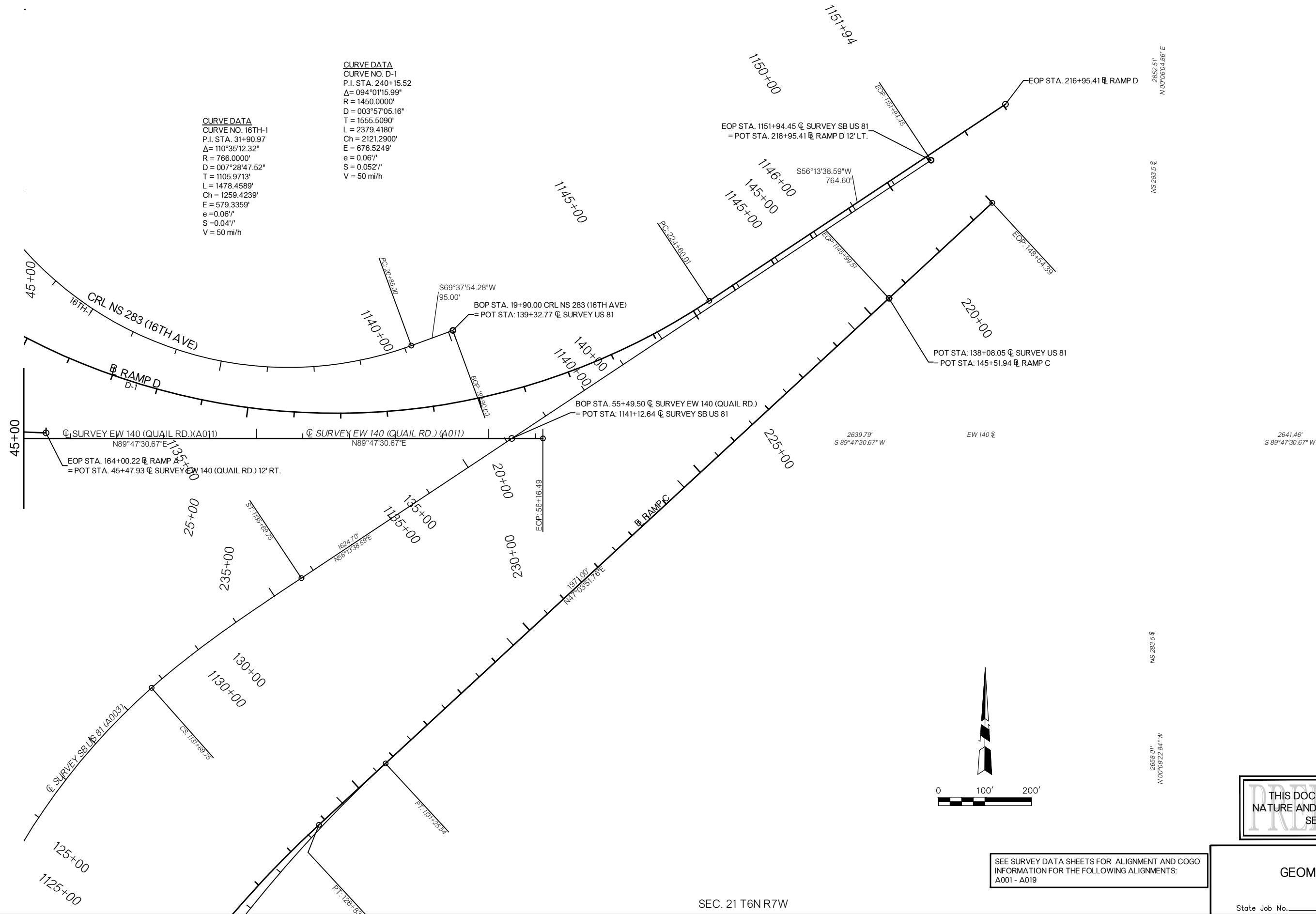


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GEOMETRIC LAYOUT

CURVE DATA
 CURVE NO. 16TH-1
 P.I. STA. 31+90.97
 $\Delta = 110^\circ 35' 12.32''$
 R = 766.0000'
 $D = 007^\circ 28' 47.52''$
 T = 1105.9713'
 L = 1478.4589'
 Ch = 1259.4239'
 E = 579.3359'
 e = 0.061'
 S = 0.041'
 V = 50 mi/h

CURVE DATA
 CURVE NO. D-1
 P.I. STA. 240+15.52
 $\Delta = 094^\circ 01' 15.99''$
 R = 1450.0000'
 $D = 003^\circ 57' 05.16''$
 T = 1555.5090'
 L = 2379.4180'
 Ch = 2121.2900'
 E = 676.5249'
 e = 0.061'
 S = 0.0521'
 V = 50 mi/h



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019

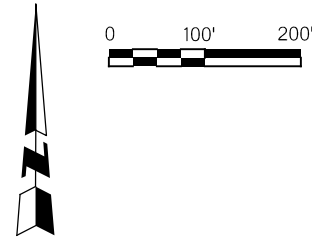
GEOMETRIC LAYOUT

SEC. 1 T6N R8W

SEC. 36 T7N R8W
1260+00

1360+00

FINAL FIELD MEETING
MARCH 2020



100+00

BOP 100+00.00

Q SURVEY GRAND AVE. (A014)

2124.83' N 89°55'19.99" E

1225+00

2124.83' N 89°55'19.99" E

120+00

S 89°55'19.99" W

Q SURVEY US 81 (A001)

N 89°52'51.14" W

1350+00

928.57' S 89°52'51.14" E

EW 137 Q SURVEY GRAND AVE. (A014)

130+00

EOP 130+53.40

CURVE DATA
CURVE NO. T-1
P.I. STA 1252+62.91
Δ = 24°40'54.29" RT
R = 540.00'
D = 10°36'37.19"
T = 118.14'
L = 232.62'
Ch = 230.83'
E = 12.77'
e = 0.06'/'
S = 0.048'/'
V = 30 mi/h

CURVE DATA
CURVE NO. T-2
P.I. STA 1259+76.50
Δ = 24°30'56.90" LT
R = 1432.39'
D = 4°00'00.00"
T = 311.21'
L = 612.89'
Ch = 608.23'
E = 33.42'
e = 0.06'/'
S = 0.052'/'
V = 50 mi/h

CURVE DATA
CURVE NO. V-1
P.I. STA 1352+42.73
Δ = 21°31'41.96" LT
R = 540.00'
D = 10°36'37.19"
T = 102.66'
L = 202.90'
Ch = 201.71'
E = 9.67'
e = 0.06'/'
S = 0.048'/'
V = 30 mi/h

CURVE DATA
CURVE NO. V-2
P.I. STA 1360+45.32
Δ = 21°37'45.70" RT
R = 1432.39'
D = 4°00'00.00"
T = 273.62'
L = 540.73'
Ch = 537.53'
E = 25.90'
e = 0.06'/'
S = 0.052'/'
V = 50 mi/h

= POT STA. 115+88.34 Q SURVEY GRAND AVE
= EOP STA. 1225+39.61 RAMP S
= BOP STA. 1250+00.00 RAMP T

= POT STA. 123+18.70 Q SURVEY GRAND AVE
= EOP STA. 1316+65.91 RAMP U
= BOP STA. 1350+00.00 RAMP V

CURVE DATA
CURVE NO. S-1
P.I. STA 1215+01.72
Δ = 21°37'45.70" LT
R = 1432.39'
D = 4°00'00.00"
T = 273.62'
L = 540.73'
Ch = 537.33'
E = 25.90'
e = 0.06'/'
S = 0.052'/'
V = 50 mi/h

CURVE DATA
CURVE NO. S-2
P.I. STA 1222+99.41
Δ = 21°31'41.96" RT
R = 540.00'
D = 10°36'37.19"
T = 102.66'
L = 202.90'
Ch = 201.71'
E = 9.67'
e = 0.06'/'
S = 0.048'/'
V = 30 mi/h

CURVE DATA
CURVE NO. U-1
P.I. STA 1308+89.73
Δ = 24°30'56.90" RT
R = 1432.39'
D = 4°00'00.00"
T = 311.21'
L = 612.89'
Ch = 608.23'
E = 33.42'
e = 0.06'/'
S = 0.052'/'
V = 50 mi/h

CURVE DATA
CURVE NO. U-2
P.I. STA 1313+98.22
Δ = 24°40'54.29" LT
R = 540.00'
D = 10°36'37.19"
T = 118.14'
L = 232.62'
Ch = 230.83'
E = 12.77'
e = 0.06'/'
S = 0.048'/'
V = 30 mi/h

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEC. 6 T6N R7W

SEC. 31 T7N R7W

1215+00 00+90.81

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS:
A001-A019

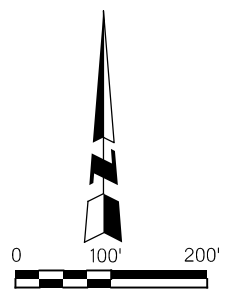
GEOMETRIC LAYOUT
GRAND AVENUE

State Job No. 24428(04) Sheet No. R088

US 81 REALIGNMENT
GRADY COUNTY

SEC. 30 T7N R7W

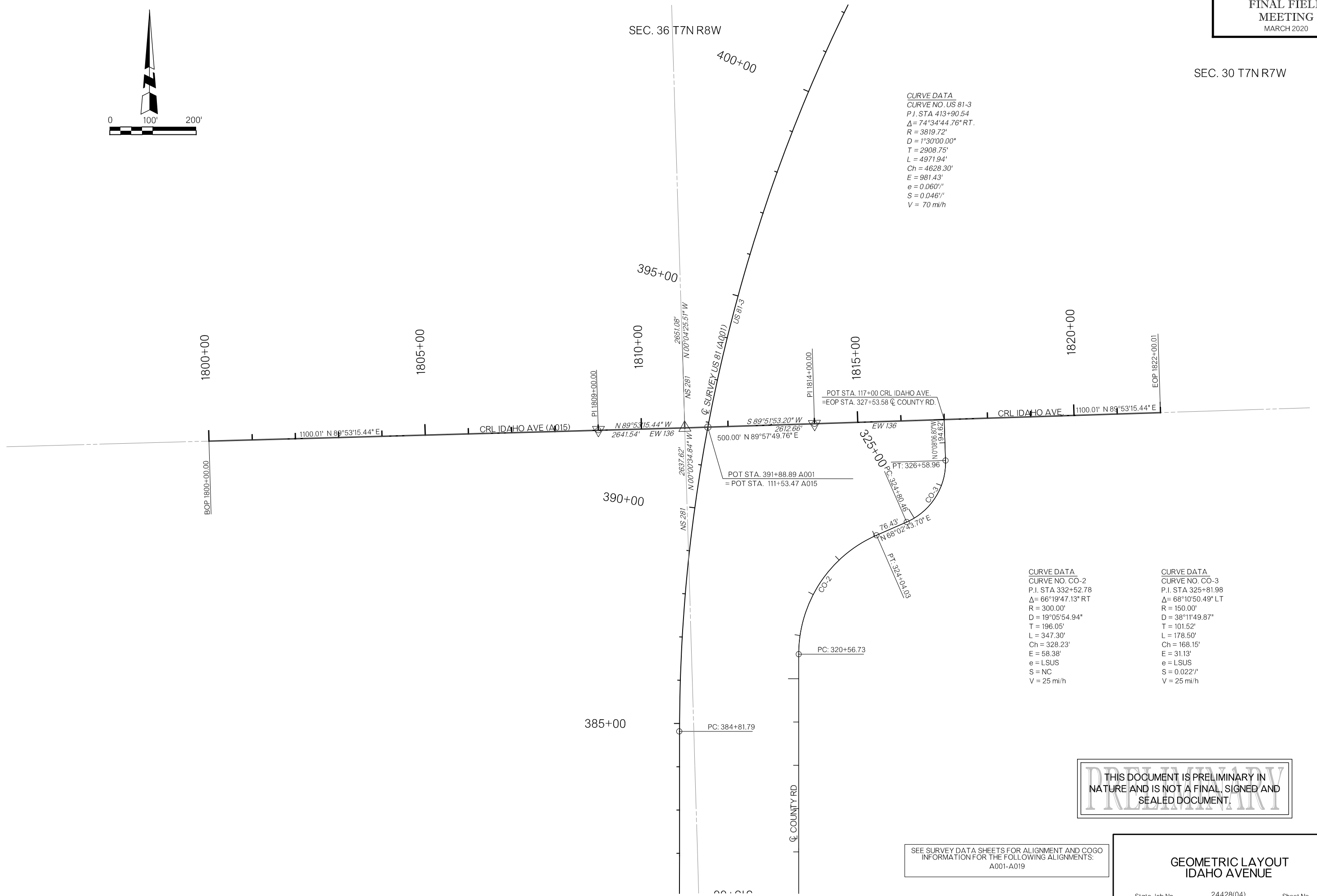
SEC. 36 T7N R8W



CURVE DATA
 CURVE NO. US 81-3
 P.I. STA 413+90.54
 $\Delta = 74^\circ 34' 44.76''$ RT.
 R = 3819.72'
 D = $1^\circ 30' 00.00''$
 T = 2908.75'
 L = 4971.94'
 Ch = 4628.30'
 E = 981.43'
 e = 0.0601'
 S = 0.0461'
 V = 70 mi/h

CURVE DATA
 CURVE NO. CO-2
 P.I. STA 332+52.78
 $\Delta = 66^\circ 19' 47.13''$ RT
 R = 300.00'
 D = $19^\circ 05' 54.94''$
 T = 196.05'
 L = 347.30'
 Ch = 328.23'
 E = 58.38'
 e = LSUS
 S = NC
 V = 25 mi/h

CURVE DATA
 CURVE NO. CO-3
 P.I. STA 325+81.98
 $\Delta = 68^\circ 10' 50.49''$ LT
 R = 150.00'
 D = $38^\circ 11' 49.87''$
 T = 101.52'
 L = 178.50'
 Ch = 168.15'
 E = 31.13'
 e = LSUS
 S = 0.0221'
 V = 25 mi/h



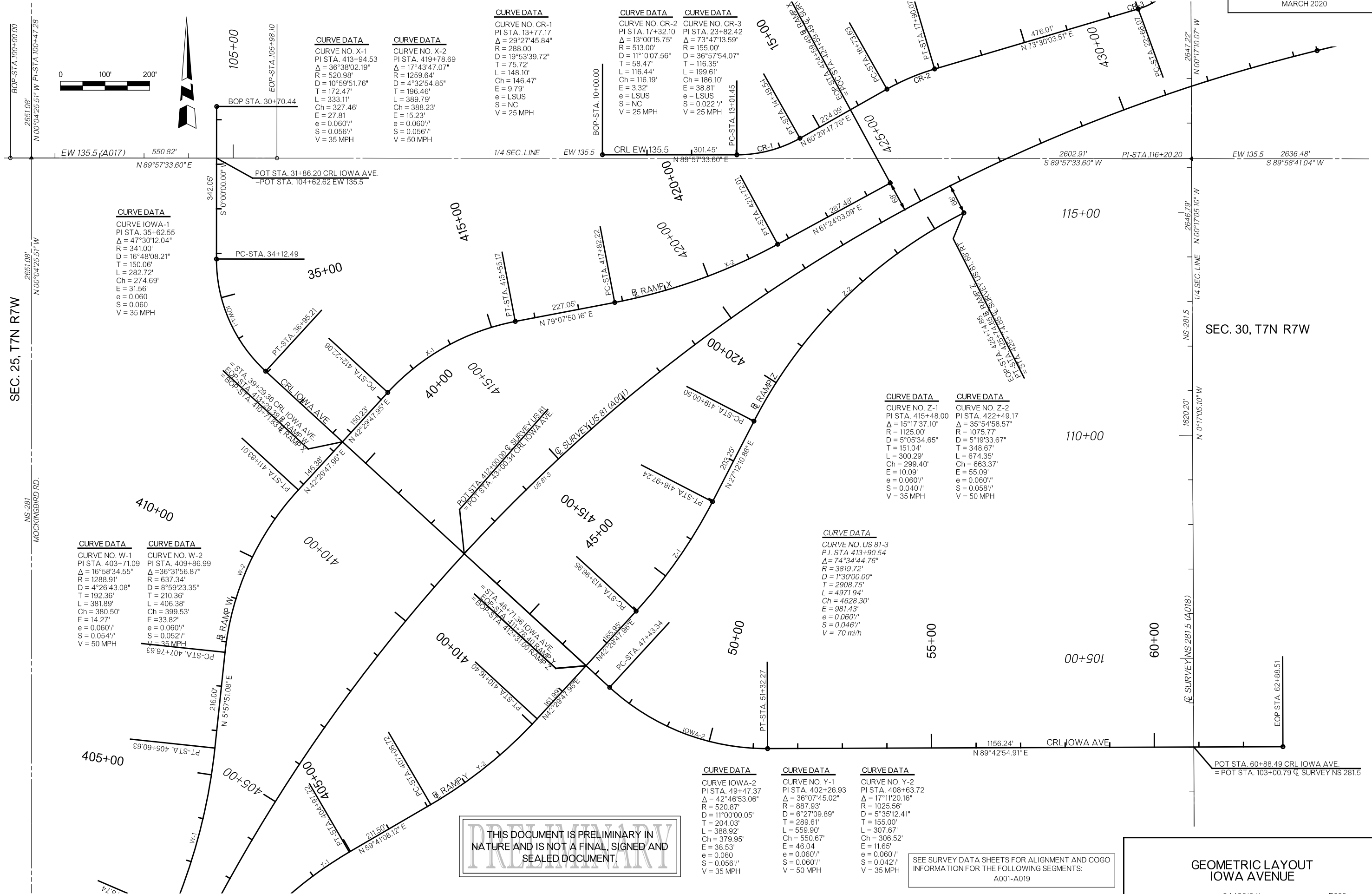
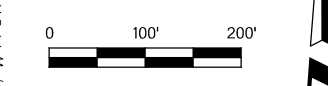
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001-A019

GEOMETRIC LAYOUT
IDAHO AVENUE
 State Job No. 24428(04) Sheet No. R089

US 81 REALIGNMENT
GRADY COUNTY

SEC. 30, T7N R7W



CURVE DATA
 CURVE NO. X-1
 PI STA. 413+94.53
 $\Delta = 36^\circ 38' 02.19''$
 $R = 520.98'$
 $D = 10^\circ 59' 51.76''$
 $T = 172.47'$
 $L = 333.11'$
 $Ch = 327.46'$
 $E = 27.81'$
 $e = 0.060''$
 $S = 0.056''$
 $V = 35$ MPH

CURVE DATA
 CURVE NO. X-2
 PI STA. 419+78.69
 $\Delta = 17^\circ 43' 47.07''$
 $R = 1259.64'$
 $D = 4^\circ 32' 54.85''$
 $T = 196.46'$
 $L = 389.79'$
 $Ch = 388.23'$
 $E = 15.23'$
 $e = 0.060''$
 $S = 0.056''$
 $V = 50$ MPH

CURVE DATA
 CURVE NO. CR-1
 PI STA. 13+77.17
 $\Delta = 29^\circ 27' 45.84''$
 $R = 288.00'$
 $D = 19^\circ 53' 39.72''$
 $T = 75.72'$
 $L = 148.10'$
 $Ch = 146.47'$
 $E = 9.79'$
 $e = LSUS$
 $S = NC$
 $V = 25$ MPH

CURVE DATA
 CURVE NO. CR-2
 PI STA. 17+32.10
 $\Delta = 13^\circ 00' 15.75''$
 $R = 513.00'$
 $D = 11^\circ 10' 07.56''$
 $T = 58.47'$
 $L = 116.44'$
 $Ch = 116.19'$
 $E = 3.32'$
 $e = LSUS$
 $S = NC$
 $V = 25$ MPH

CURVE DATA
 CURVE NO. CR-3
 PI STA. 23+82.42
 $\Delta = 73^\circ 47' 13.59''$
 $R = 155.00'$
 $D = 36^\circ 57' 54.07''$
 $T = 116.35'$
 $L = 199.61'$
 $Ch = 186.10'$
 $E = 38.81'$
 $e = LSUS$
 $S = 0.022''$
 $V = 25$ MPH

CURVE DATA
 CURVE IOWA-1
 PI STA. 35+62.55
 $\Delta = 47^\circ 30' 12.04''$
 $R = 341.00'$
 $D = 16^\circ 48' 08.21''$
 $T = 150.06'$
 $L = 282.72'$
 $Ch = 274.69'$
 $E = 31.56'$
 $e = 0.060''$
 $S = 0.060''$
 $V = 35$ MPH

CURVE DATA
 CURVE NO. Z-1
 PI STA. 415+48.00
 $\Delta = 15^\circ 17' 37.10''$
 $R = 1125.00'$
 $D = 5^\circ 05' 34.65''$
 $T = 151.04'$
 $L = 300.29'$
 $Ch = 299.40'$
 $E = 10.09'$
 $e = 0.060''$
 $S = 0.040''$
 $V = 35$ MPH

CURVE DATA
 CURVE NO. Z-2
 PI STA. 422+49.17
 $\Delta = 35^\circ 54' 58.57''$
 $R = 1075.77'$
 $D = 5^\circ 19' 33.67''$
 $T = 348.67'$
 $L = 674.35'$
 $Ch = 663.37'$
 $E = 55.09'$
 $e = 0.060''$
 $S = 0.058''$
 $V = 50$ MPH

CURVE DATA
 CURVE NO. W-1
 PI STA. 403+71.09
 $\Delta = 16^\circ 58' 34.55''$
 $R = 1288.91'$
 $D = 4^\circ 26' 43.08''$
 $T = 192.36'$
 $L = 381.89'$
 $Ch = 380.50'$
 $E = 14.27'$
 $e = 0.060''$
 $S = 0.054''$
 $V = 50$ MPH

CURVE DATA
 CURVE NO. W-2
 PI STA. 409+86.99
 $\Delta = 36^\circ 31' 56.87''$
 $R = 637.34'$
 $D = 8^\circ 59' 23.35''$
 $T = 210.36'$
 $L = 406.38'$
 $Ch = 399.53'$
 $E = 33.82'$
 $e = 0.060''$
 $S = 0.052''$
 $V = 35$ MPH

CURVE DATA
 CURVE NO. US 81-3
 PI STA. 413+90.54
 $\Delta = 74^\circ 34' 44.76''$
 $R = 3819.72'$
 $D = 1^\circ 30' 00.00''$
 $T = 2908.75'$
 $L = 4971.94'$
 $Ch = 4628.30'$
 $E = 981.43'$
 $e = 0.060''$
 $S = 0.046''$
 $V = 70$ mi/h

CURVE DATA
 CURVE IOWA-2
 PI STA. 49+47.37
 $\Delta = 42^\circ 46' 53.06''$
 $R = 520.87'$
 $D = 11^\circ 00' 00.05''$
 $T = 204.03'$
 $L = 388.92'$
 $Ch = 379.95'$
 $E = 38.53'$
 $e = 0.060''$
 $S = 0.056''$
 $V = 35$ MPH

CURVE DATA
 CURVE NO. Y-1
 PI STA. 402+26.93
 $\Delta = 36^\circ 07' 45.02''$
 $R = 887.93'$
 $D = 6^\circ 27' 09.89''$
 $T = 289.61'$
 $L = 559.90'$
 $Ch = 550.67'$
 $E = 46.04'$
 $e = 0.060''$
 $S = 0.060''$
 $V = 50$ MPH

CURVE DATA
 CURVE NO. Y-2
 PI STA. 408+63.72
 $\Delta = 17^\circ 11' 20.16''$
 $R = 1025.56'$
 $D = 5^\circ 35' 12.41''$
 $T = 155.00'$
 $L = 307.67'$
 $Ch = 306.52'$
 $E = 11.65'$
 $e = 0.060''$
 $S = 0.042''$
 $V = 35$ MPH

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING SEGMENTS:
A001-A019

GEOMETRIC LAYOUT
IOWA AVENUE

Alignment: **Q** SURVEY US 81
Description: **A001**

Table with columns: Description, PT Station, Northing, Easting, Value, Parameter, Type, Course. Includes sections for Tangent Data, Curve Point Data, and Circular Curve Data.

Alignment: **R** US 81 NB CD RD. 1
Description: **R** US 81 NB CD RD. 1

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course. Includes sections for Curve Point Data, Tangent Data, and Circular Curve Data.

Alignment: **R** US 81 NB CD RD. 2
Description: **R** US 81 NB CD RD. 2

Table with columns: Description, PT Station, Northing, Easting, Value, Parameter, Type, Course.

Alignment: **R** US 81 SB CD RD. 2
Description: **R** US 81 SB CD RD. 2

Table with columns: Description, PT Station, Northing, Easting, Value, Parameter, Type, Course.

Alignment: **R** US 81 NB CD RD. 3
Description: **R** US 81 NB CD RD. 3

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, PT Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, PT Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, PT Station, Northing, Easting, Value, Parameter, Type, Course.

Alignment: **R** US 81 SB CD RD. 3
Description: **R** US 81 SB CD RD. 3

Table with columns: Description, PT Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, PT Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, PT Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, PT Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, PT Station, Northing, Easting, Value, Parameter, Type, Course.

Alignment: **R** US 81 SB CD RD. 1
Description: **R** US 81 SB CD RD. 1

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, PT Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Alignment: **R** US 81 INTERIM CONN
Description: **R** US 81 INTERIM CONN

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, PT Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

Table with columns: Description, Station, Northing, Easting, Value, Parameter, Type, Course.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019

GEOMETRIC LAYOUT

Alignment: RAMP G Description: I-44 NB to US 81 SB. Includes Tangent Data, Curve Point Data, and Circular Curve Data tables.

Alignment: RAMP I Description: WB I-44 to SB US 81. Includes Curve Point Data, Tangent Data, and Circular Curve Data tables.

Alignment: RAMP K Description: NB US 81 to EB I-44. Includes Tangent Data, Curve Point Data, and Circular Curve Data tables.

Alignment: RAMP M Description: NB US 81 to WB I-44. Includes Tangent Data, Curve Point Data, and Circular Curve Data tables.

Alignment: RAMP J Description: SB US 81 to WB I-44. Includes Tangent Data, Curve Point Data, and Circular Curve Data tables.

Alignment: RAMP L Description: EB I-44 to NB US 81. Includes Curve Point Data, Tangent Data, and Circular Curve Data tables.

Alignment: RAMP N Description: WB I-44 to NB US 81. Includes Tangent Data, Curve Point Data, and Circular Curve Data tables.

Alignment: RAMP H Description: US 81 SB to I-44 NB. Includes Tangent Data, Curve Point Data, and Circular Curve Data tables.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019

GEOMETRIC LAYOUT

Alignment: RAMP T Description: SB OFF-RAMP AT GRAND AVE. Tangent Data Description Station Northing Easting PC: 1251+44.77 617311.5707130 1971617.9196863 End: 1251+44.77 617456.2893484 1971621.8547233 Parameter Value Parameter Value Length: 144.77 Course: N 01° 33' 27.15" E Curve Point Data Description Station Northing Easting PC: 1251+44.77 617456.2893484 1971621.8547233 RP: 1251+77.39 617441.6116608 1972161.6552101 PT: 1253+77.39 617680.3570199 1971677.2992939 Circular Curve Data Parameter Value Parameter Value Delta: 24° 40' 54.29" Type: RIGHT Radius: 540 Tangent: 118.14 Length: 232.62 External: 12.77 Mid-Ord: 12.48 Chord: 230.83 Course: N 13° 53' 54.30" E Tangent Data Description PT Station Northing Easting Start: 1253+77.39 617680.3570199 1971677.2992939 End: 1256+65.29 617938.5884017 1971804.5849158 Parameter Value Parameter Value Length: 287.9 Course: N 26° 14' 21.44" E Curve Point Data Description Station Northing Easting PC: 1256+65.29 617938.5884017 1971804.5849158 RP: 1256+65.29 618571.8799200 1970519.7913821 PT: 1262+78.18 618528.7992877 1971951.5373897 Circular Curve Data Parameter Value Parameter Value Delta: 24° 30' 56.90" Type: LEFT Radius: 1432.39 Tangent: 311.21 Length: 612.89 External: 33.42 Mid-Ord: 32.66 Chord: 608.23 Course: N 13° 58' 53.00" E Tangent Data Description PT Station Northing Easting Start: 1262+78.18 618528.7992877 1971951.5373897 End: 1270+78.18 619328.4373803 1971975.5981620 Parameter Value Parameter Value Length: 800 Course: N 01° 43' 24.55" E Tangent Data Description PT Station Northing Easting Start: 1270+78.18 619328.4373803 1971975.5981620 End: 1273+78.42 619627.9407535 1971996.6155230 Parameter Value Parameter Value Length: 300.24 Course: N 04° 00' 50.74" E

Alignment: RAMP U INTERIM CONN Description: RAMP U INTERIM CONN Curve Point Data Description Station Northing Easting PC: 1291+87.82 614906.8669100 1971930.8108280 RP: 1291+87.82 615413.6912974 1977658.0089009 PT: 1299+20.36 615638.6900928 1971912.8331623 Circular Curve Data Parameter Value Parameter Value Delta: 07° 17' 59.67" Type: RIGHT Radius: 5749.58 Tangent: 366.77 Length: 732.54 External: 11.69 Mid-Ord: 11.66 Chord: 732.04 Course: N 01° 24' 26.00" W Curve Point Data Description Station Northing Easting PC: 1299+20.36 615638.6900928 1971912.8331623 RP: 1302+25.62 615574.5118341 1973551.5769345 PT: 1302+25.62 615940.8542708 1971953.0172018 Circular Curve Data Parameter Value Parameter Value Delta: 10° 39' 53.55" Type: RIGHT Radius: 1640 Tangent: 153.07 Length: 305.26 External: 7.13 Mid-Ord: 7.1 Chord: 304.82 Course: N 07° 34' 30.61" E Tangent Data Description PT Station Northing Easting Start: 1302+25.62 615940.8542708 1971953.0172018 End: 1306+58.12 616362.4240980 1972049.6284917 Parameter Value Parameter Value Length: 432.5 Course: N 12° 54' 27.39" E

Alignment: RAMP V INTERIM CONN Description: RAMP V INTERIM CONN Tangent Data Description PT Station Northing Easting Start: 1360+75.00 618348.1163556 1972101.8034291 End: 1365+76.15 618844.6567333 1972034.0191919 Parameter Value Parameter Value Length: 501.15 Course: N 07° 46' 24.85" W Curve Point Data Description Station Northing Easting PC: 1365+76.15 618844.6567333 1972034.0191919 RP: 1365+76.15 619090.8272634 1973837.2940101 PT: 1368+77.82 619145.5655204 1972018.1173493 Circular Curve Data Parameter Value Parameter Value Delta: 09° 29' 49.40" Type: RIGHT Radius: 1820 Tangent: 151.18 Length: 301.67 External: 6.27 Mid-Ord: 6.25 Chord: 301.33 Course: N 03° 01' 30.15" W Tangent Data Description PT Station Northing Easting Start: 1368+77.82 619145.5655204 1972018.1173493 End: 1376+79.19 619946.5757805 1972042.2194096 Parameter Value Parameter Value Length: 801.37 Course: N 01° 43' 24.55" E Tangent Data Description PT Station Northing Easting Start: 1376+79.19 619946.5757805 1972042.2194096 End: 1379+79.43 620246.8009768 1972039.2476278 Parameter Value Parameter Value Length: 300.24 Course: N 00° 34' 01.65" W

Alignment: CRL IDAHO AVE Description: COUNTY ROAD EW 136 Tangent Data Description PT Station Northing Easting Start: 1800+00.00 622589.7015111 1971053.5068602 End: 1809+00.00 622587.9362912 1971953.5051290 Parameter Value Parameter Value Length: 900.00 Course: S 89° 53' 15.44" E Tangent Data Description PT Station Northing Easting Start: 1809+00.00 622587.9362912 1971953.5051290 End: 1814+00.00 622588.2519903 1972453.5039089 Parameter Value Parameter Value Length: 500.00 Course: N 89° 57' 49.76" E Tangent Data Description PT Station Northing Easting Start: 1814+00.00 622588.2519903 1972453.5039089 End: 1822+00.01 622590.1400532 1973253.5119214 Parameter Value Parameter Value Length: 800.01 Course: N 89° 51' 53.20" E

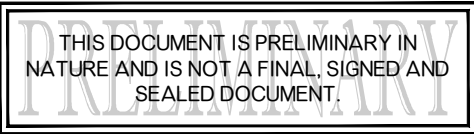
Alignment: CRL IOWA AVE Description: CRL IOWA AVE Tangent Data Description PT Station Northing Easting Start: 30+70.44 625354.6716322 1972565.4484033 End: 34+12.49 625012.6253460 1972565.4484033 Parameter Value Parameter Value Length: 342.05 Course: S 00° 00' 00.00" W Curve Point Data Description Station Northing Easting PC: 34+12.49 625012.6253460 1972565.4484033 RP: 34+12.49 625012.6253460 1972906.4484033 PT: 36+95.21 624761.2003321 1972676.0868127 Circular Curve Data Parameter Value Parameter Value Delta: 47° 30' 12.04" Type: LEFT Radius: 341 Tangent: 150.06 Length: 282.72 External: 31.56 Mid-Ord: 28.88 Chord: 274.69 Course: S 23° 45' 06.02" E Tangent Data Description PT Station Northing Easting Start: 36+95.21 624761.2003321 1972676.0868127 End: 47+43.34 624053.1366677 1973448.8932349 Parameter Value Parameter Value Length: 1048.13 Course: S 47° 30' 12.04" E Curve Point Data Description Station Northing Easting PC: 47+43.34 624053.1366677 1973448.8932349 RP: 47+43.34 624437.1828465 1973800.7654979 PT: 51+32.27 623916.3192789 1973803.3541036 Circular Curve Data Parameter Value Parameter Value Delta: 42° 46' 53.06" Type: LEFT Radius: 520.87 Tangent: 204.03 Length: 388.92 External: 38.53 Mid-Ord: 35.88 Chord: 379.95 Course: S 68° 53' 38.56" E Tangent Data Description PT Station Northing Easting Start: 51+32.27 623916.3192789 1973803.3541036 End: 62+88.51 623922.0655352 1974959.5809989 Parameter Value Parameter Value Length: 1156.24 Course: N 89° 42' 54.91" E

Alignment: RAMP U Description: NB OFF-RAMP AT GRAND AVE Tangent Data Description PT Station Northing Easting Start: 1298+76.72 615983.9687777 1971998.9902362 End: 1303+78.52 616085.5378232 1972014.0822368 Parameter Value Parameter Value Length: 501.8 Course: N 01° 43' 24.55" E Curve Point Data Description Station Northing Easting PC: 1303+78.52 616085.5378232 1972014.0822368 RP: 1303+78.52 616042.4571908 1973445.8282444 PT: 1309+91.41 616675.7487091 1972161.0347108 Circular Curve Data Parameter Value Parameter Value Delta: 24° 30' 56.90" Type: RIGHT Radius: 1432.39 Tangent: 311.21 Length: 612.89 External: 33.42 Mid-Ord: 32.66 Chord: 608.23 Course: N 13° 58' 53.00" E Tangent Data Description PT Station Northing Easting Start: 1309+91.41 616675.7487091 1972161.0347108 End: 1312+80.08 616934.6730648 1972288.6619085 Parameter Value Parameter Value Length: 288.67 Course: N 26° 14' 21.44" E Curve Point Data Description Station Northing Easting PC: 1312+80.08 616934.6730648 1972288.6619085 RP: 1312+80.08 617173.4184239 1971804.3059923 PT: 1315+12.70 617158.7407364 1972344.1064791 Circular Curve Data Parameter Value Parameter Value Delta: 24° 40' 54.29" Type: LEFT Radius: 540 Tangent: 118.14 Length: 232.62 External: 12.77 Mid-Ord: 12.48 Chord: 230.83 Course: N 13° 53' 54.30" E Tangent Data Description PT Station Northing Easting Start: 1315+12.70 617158.7407364 1972344.1064791 End: 1316+65.91 617311.8959232 1972348.2709140 Parameter Value Parameter Value Length: 133.21 Course: N 01° 33' 27.15" E

Alignment: RAMP V Description: RAMP V Tangent Data Description PT Station Northing Easting Start: 1350+00.00 617311.8959234 1972348.2707940 End: 1351+40.07 617451.9127183 1972352.2367186 Parameter Value Parameter Value Length: 140.07 Course: N 01° 37' 20.81" E Curve Point Data Description Station Northing Easting PC: 1351+40.07 617451.9127183 1972352.2367186 RP: 1351+40.07 617467.2018894 1971812.4532053 PT: 1353+42.97 617651.0589019 1972320.1899471 Circular Curve Data Parameter Value Parameter Value Delta: 21° 31' 41.96" Type: LEFT Radius: 540 Tangent: 102.66 Length: 202.9 Tangent: 102.66 Mid-Ord: 9.5 External: 9.67 Chord: 201.71 Course: N 09° 08' 30.17" W Tangent Data Description PT Station Northing Easting Start: 1353+42.97 617651.0589019 1972320.1899471 End: 1357+71.69 618054.1641601 1972174.2211361 Parameter Value Parameter Value Length: 428.72 Course: N 19° 54' 21.15" W Curve Point Data Description Station Northing Easting PC: 1357+71.69 618054.1641601 1972174.2211361 RP: 1357+71.69 618541.8598668 1973521.0342148 PT: 1363+12.43 618584.9404991 1972089.2882072 Circular Curve Data Parameter Value Parameter Value Delta: 21° 37' 45.70" Type: RIGHT Radius: 1432.39 Tangent: 273.62 Length: 540.73 External: 25.9 Mid-Ord: 25.44 Chord: 537.53 Course: N 09° 05' 28.30" W Tangent Data Description PT Station Northing Easting Start: 1363+12.43 618584.9404991 1972089.2882072 End: 1371+12.43 619384.5785918 1972113.3489795 Parameter Value Parameter Value Length: 800 Course: N 01° 43' 24.55" E Tangent Data Description PT Station Northing Easting Start: 1371+12.43 619384.5785918 1972113.3489795 End: 1374+12.67 619684.8037881 1972110.3771977 Parameter Value Parameter Value Length: 300.24 Course: N 00° 34' 01.65" W

Alignment: COUNTY ROAD Description: COUNTY ROAD Tangent Data Description PT Station Northing Easting Start: 300+00.00 620001.2987043 1972365.8961659 End: 300+90.47 620091.6300133 1972360.8285258 Parameter Value Parameter Value Length: 90.47 Course: N 03° 12' 39.46" W Curve Point Data Description Station Northing Easting PC: 300+90.47 620091.6300133 1972360.8285258 RP: 300+90.47 620371.6926173 1977352.9788578 PT: 305+20.41 620521.3944345 1972355.2204237 Circular Curve Data Parameter Value Parameter Value Delta: 04° 55' 36.03" Type: RIGHT Radius: 5000.00 Tangent: 215.1 Length: 429.93 External: 4.62 Mid-Ord: 4.62 Chord: 429.80 Course: N 00° 44' 51.45" W Tangent Data Description PT Station Northing Easting Start: 305+20.41 620521.3944345 1972355.2204237 End: 320+56.73 622057.0315824 1972401.2185796 Parameter Value Parameter Value Length: 1536.33 Course: N 01° 42' 56.57" E Curve Point Data Description Station Northing Easting PC: 320+56.73 622057.0315824 1972401.2185796 RP: 320+56.73 622048.0494734 1972701.0840856 PT: 324+04.03 622326.2937309 1972588.9228931 Circular Curve Data Parameter Value Parameter Value Delta: 66° 19' 47.13" Type: RIGHT Radius: 300 Tangent: 196.05 Length: 347.3 External: 58.38 Mid-Ord: 48.87 Chord: 328.23 Course: N 34° 52' 50.13" E Tangent Data Description PT Station Northing Easting Start: 324+04.03 622326.2937309 1972588.9228931 End: 324+80.46 622354.8679593 1972659.8085067 Parameter Value Parameter Value Length: 76.43 Course: N 68° 02' 43.70" E Curve Point Data Description Station Northing Easting PC: 324+80.46 622354.8679593 1972659.8085067 RP: 324+80.46 622493.9900881 1972603.7279104 PT: 326+58.96 622494.3440954 1972753.7274927 Circular Curve Data Parameter Value Parameter Value Delta: 68° 10' 50.49" Type: LEFT Radius: 150 Tangent: 101.52 Length: 178.5 External: 31.13 Mid-Ord: 25.78 Chord: 168.15 Course: N 33° 57' 18.45" E Tangent Data Description PT Station Northing Easting Start: 326+58.96 622494.3440954 1972753.7274927 End: 327+53.58 622588.9600075 1972753.5041939 Parameter Value Parameter Value Length: 94.62 Course: N 00° 08' 06.80" W

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019 GEOMETRIC LAYOUT State Job No. 24428(04) Sheet No. R096



Alignment: **RAMP AA**
Description: US-62 TO US-81 SB ENTRANCE

| Curve Point Data | | | |
|---------------------|----------------|----------------|--------------------|
| Description | Station | Northing | Easting |
| PC: | 468+70.82 | 626825.7344287 | 1978084.3320748 |
| RP: | | 628161.9977067 | 1976704.3686196 |
| PT: | 481+23.60 | 627937.5137502 | 1978612.1174603 |
| Circular Curve Data | | | |
| Parameter | Value | Parameter | Value |
| Delta: | 37° 22' 01.79" | Type: | LEFT |
| Radius: | 1920.91 | | |
| Length: | 1252.78 | Tangent: | 649.58 |
| Mid-Ord: | 101.23 | External: | 106.86 |
| Chord: | 1230.7 | Course: | N 25° 23' 40.89" E |
| Tangent Data | | | |
| Description | PT Station | Northing | Easting |
| Start: | 481+23.60 | 627937.5137502 | 1978612.1174603 |
| End: | 483+04.13 | 628116.8086867 | 1978633.2150156 |
| Parameter | Value | Parameter | Value |
| Length: | 180.53 | Course: | N 06° 42' 39.99" E |
| Curve Point Data | | | |
| Description | Station | Northing | Easting |
| PC: | 483+04.13 | 628116.8086867 | 1978633.2150156 |
| RP: | | 628002.4631518 | 1979604.9662491 |
| PT: | 487+25.07 | 628511.6700022 | 1978769.4521890 |
| Circular Curve Data | | | |
| Parameter | Value | Parameter | Value |
| Delta: | 24° 38' 57.30" | Type: | RIGHT |
| Radius: | 978.46 | | |
| Length: | 420.94 | Tangent: | 213.78 |
| Mid-Ord: | 22.55 | External: | 23.08 |
| Chord: | 417.7 | Course: | N 19° 02' 08.64" E |
| Tangent Data | | | |
| Description | PT Station | Northing | Easting |
| Start: | 487+25.07 | 628511.6700022 | 1978769.4521890 |
| End: | 488+51.02 | 628619.2197929 | 1978834.9987702 |
| Parameter | Value | Parameter | Value |
| Length: | 125.95 | Course: | N 31° 21' 37.29" E |

Alignment: **US 81 INTERIM CONN 2**
Description: INTERIM US-62 TO EXISTING US-81

| Tangent Data | | | |
|---------------------|------------------|----------------|----------------------|
| Description | PT Station | Northing | Easting |
| Start: | 489+13.82 | 628651.5989739 | 1978868.7853525 |
| End: | 493+29.71 | 629006.7291372 | 1979085.2206235 |
| Parameter | Value | Parameter | Value |
| Length: | 492 | Course: | N 31° 21' 37.2925" E |
| Curve Point Data | | | |
| Description | Station | Northing | Easting |
| PC: | 493+60.82 | 629037.0020794 | 1979094.8874577 |
| RP: | | 631229.0067591 | 1975498.2140663 |
| PT: | 506+79.31 | 630251.0990479 | 1979595.1202781 |
| Circular Curve Data | | | |
| Parameter | Value | Parameter | Value |
| Delta: | 17° 56' 07.4014" | Type: | LEFT |
| Radius: | 4212 | | |
| Length: | 1318.49 | Tangent: | 664.68 |
| Mid-Ord: | 51.49 | External: | 52.12 |
| Chord: | 1313.11 | Course: | N 22° 23' 33.5918" E |
| Tangent Data | | | |
| Description | PT Station | Northing | Easting |
| Start: | 506+79.31 | 630251.0990479 | 1979595.1202781 |
| End: | 506+94.37 | 630265.7527835 | 1979598.6180397 |
| Parameter | Value | Parameter | Value |
| Length: | 15.07 | Course: | N 13° 25' 29.8911" E |

Alignment: **RAMP DD**
Description: US-62 TO US-81 NB ENTRANCE

| Tangent Data | | | |
|---------------------|----------------|----------------|--------------------|
| Description | PT Station | Northing | Easting |
| Start: | 487+01.63 | 628298.3951719 | 1979361.4125123 |
| End: | 488+98.99 | 628475.2290089 | 1979449.0411489 |
| Parameter | Value | Parameter | Value |
| Length: | 197.35 | Course: | N 26° 21' 37.29" E |
| Curve Point Data | | | |
| Description | Station | Northing | Easting |
| PC: | 488+98.99 | 628475.2290089 | 1979449.0411489 |
| RP: | | 628700.3240139 | 1978994.8013601 |
| PT: | 491+90.61 | 628758.5754325 | 1979498.3967525 |
| Circular Curve Data | | | |
| Parameter | Value | Parameter | Value |
| Delta: | 32° 57' 30.60" | Type: | LEFT |
| Radius: | 506.95 | | |
| Length: | 291.62 | Tangent: | 149.97 |
| Mid-Ord: | 20.82 | External: | 21.72 |
| Chord: | 287.61 | Course: | N 09° 52' 51.99" E |
| Tangent Data | | | |
| Description | PT Station | Northing | Easting |
| Start: | 491+90.61 | 628758.5754325 | 1979498.3967525 |
| End: | 494+08.86 | 628975.3798548 | 1979473.3187527 |
| Parameter | Value | Parameter | Value |
| Length: | 218.25 | Course: | N 06° 35' 53.31" W |
| Curve Point Data | | | |
| Description | Station | Northing | Easting |
| PC: | 494+08.86 | 628975.3798548 | 1979473.3187527 |
| RP: | | 629138.5775134 | 1980884.1957909 |
| PT: | 499+76.91 | 629537.6314074 | 1979521.1244493 |
| Circular Curve Data | | | |
| Parameter | Value | Parameter | Value |
| Delta: | 22° 54' 57.98" | Type: | RIGHT |
| Radius: | 1420.28 | | |
| Length: | 568.06 | Tangent: | 287.88 |
| Mid-Ord: | 28.31 | External: | 28.88 |
| Chord: | 564.28 | Course: | N 04° 51' 35.68" E |

Alignment: **RAMP BB**
Description: US-81 SB TO US-62 EXIT

| Tangent Data | | | |
|---------------------|----------------|----------------|--------------------|
| Description | PT Station | Northing | Easting |
| Start: | 488+68.82 | 628619.2197929 | 1978834.9987702 |
| End: | 491+46.81 | 628856.5956478 | 1978979.6682823 |
| Parameter | Value | Parameter | Value |
| Length: | 277.99 | Course: | N 31° 21' 37.29" E |
| Curve Point Data | | | |
| Description | Station | Northing | Easting |
| PC: | 491+46.81 | 628856.5956478 | 1978979.6682823 |
| RP: | | 622964.8817024 | 1988646.8788376 |
| PT: | 494+70.72 | 629130.7402033 | 1979152.1728347 |
| Circular Curve Data | | | |
| Parameter | Value | Parameter | Value |
| Delta: | 01° 38' 21.55" | Type: | RIGHT |
| Radius: | 11321.1 | | |
| Length: | 323.91 | Tangent: | 161.97 |
| Mid-Ord: | 1.16 | External: | 1.16 |
| Chord: | 323.9 | Course: | N 32° 10' 48.07" E |
| Tangent Data | | | |
| Description | PT Station | Northing | Easting |
| Start: | 494+70.72 | 629130.7402033 | 1979152.1728347 |
| End: | 496+28.22 | 629262.8305726 | 1979237.9522708 |
| Parameter | Value | Parameter | Value |
| Length: | 157.5 | Course: | N 32° 59' 58.84" E |
| Curve Point Data | | | |
| Description | Station | Northing | Easting |
| PC: | 496+28.22 | 629262.8305726 | 1979237.9522708 |
| RP: | | 630019.0563796 | 1978073.4523544 |
| PT: | 500+38.45 | 629634.6624548 | 1979407.6857811 |
| Circular Curve Data | | | |
| Parameter | Value | Parameter | Value |
| Delta: | 16° 55' 40.57" | Type: | LEFT |
| Radius: | 1388.5 | | |
| Length: | 410.23 | Tangent: | 206.62 |
| Mid-Ord: | 15.12 | External: | 15.29 |
| Chord: | 408.74 | Course: | N 24° 32' 08.56" E |

Alignment: **RAMP CC**
Description: US-81 NB TO US-62 EXIT

| Tangent Data | | | |
|---------------------|----------------|----------------|--------------------|
| Description | PT Station | Northing | Easting |
| Start: | 461+68.17 | 626281.0613940 | 1977627.4012870 |
| End: | 463+69.92 | 626389.0443773 | 1977797.8192878 |
| Parameter | Value | Parameter | Value |
| Length: | 201.75 | Course: | N 57° 38' 24.65" E |
| Curve Point Data | | | |
| Description | Station | Northing | Easting |
| PC: | 463+69.92 | 626389.0443773 | 1977797.8192878 |
| RP: | | 629146.7859314 | 1975686.8706135 |
| PT: | 468+82.13 | 626729.1937846 | 1978180.1598058 |
| Circular Curve Data | | | |
| Parameter | Value | Parameter | Value |
| Delta: | 08° 27' 01.38" | Type: | LEFT |
| Radius: | 3472.93 | | |
| Length: | 512.21 | Tangent: | 256.57 |
| Mid-Ord: | 9.44 | External: | 9.46 |
| Chord: | 511.75 | Course: | N 48° 20' 31.54" E |
| Tangent Data | | | |
| Description | PT Station | Northing | Easting |
| Start: | 468+82.13 | 626729.1937846 | 1978180.1598058 |
| End: | 479+42.46 | 627532.0934586 | 1978871.7818750 |
| Parameter | Value | Parameter | Value |
| Length: | 493.86 | Course: | N 37° 22' 00.00" E |
| Curve Point Data | | | |
| Description | Station | Northing | Easting |
| PC: | 479+42.46 | 627532.0934586 | 1978871.7818750 |
| RP: | | 632994.3155466 | 1971718.8712883 |
| PT: | 484+36.31 | 627924.5946519 | 1979171.5100293 |
| Circular Curve Data | | | |
| Parameter | Value | Parameter | Value |
| Delta: | 06° 45' 00.86" | Type: | LEFT |
| Radius: | 9000 | | |
| Length: | 1060.32 | Tangent: | 530.78 |
| Mid-Ord: | 15.61 | External: | 15.64 |
| Chord: | 1059.71 | Course: | N 40° 44' 30.43" E |
| Tangent Data | | | |
| Description | PT Station | Northing | Easting |
| Start: | 484+36.31 | 627924.5946519 | 1979171.5100293 |
| End: | 487+01.71 | 628155.1416385 | 1979301.2232170 |
| Parameter | Value | Parameter | Value |
| Length: | 493.86 | Course: | N 37° 22' 00.00" E |
| Curve Point Data | | | |
| Description | Station | Northing | Easting |
| PC: | 484+36.31 | 627924.5946519 | 1979171.5100293 |
| RP: | | 628501.1625390 | 1978416.4805784 |
| PT: | 487+01.71 | 628155.1416385 | 1979301.2232170 |
| Circular Curve Data | | | |
| Parameter | Value | Parameter | Value |
| Delta: | 16° 00' 22.71" | Type: | LEFT |
| Radius: | 950 | | |
| Length: | 265.39 | Tangent: | 133.57 |
| Mid-Ord: | 9.25 | External: | 9.34 |
| Chord: | 264.53 | Course: | N 29° 21' 48.65" E |
| Tangent Data | | | |
| Description | PT Station | Northing | Easting |
| Start: | 487+01.71 | 628155.1416385 | 1979301.2232170 |
| End: | 488+57.73 | 628300.4440561 | 1979358.0506730 |
| Parameter | Value | Parameter | Value |
| Length: | 156.02 | Course: | N 21° 21' 37.29" E |

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEE SURVEY DATA SHEETS FOR ALIGNMENT AND COGO INFORMATION FOR THE FOLLOWING ALIGNMENTS: A001 - A019

GEOMETRIC LAYOUT

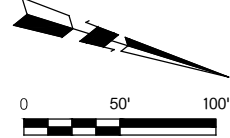
US 81 REALIGNMENT GRADY COUNTY

BM#11 - 1000 NAIL SET
S.W. FACE 8" ELM
@ SURVEY U.S. 81
STA. 134+50.38, 58.92' RT.
ELEV. = 1183.356

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE @ SURVEY U.S. 81
UNLESS OTHERWISE NOTED.

BM#12 - 1000 NAIL SET
W. FACE 18" ELM
@ SURVEY U.S. 81
STA. 141+72.09, 306.34' LT.
ELEV. = 1191.811



THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

TO BE CONSTRUCTED
IN FUTURE PROJECT

STA. 146+50 TO 147+55
CONST. 8' BOTTOM PAVED DITCH LT.

CONST. RIPRAP W/
FILTER BLANKET

CAUTION:
TUG

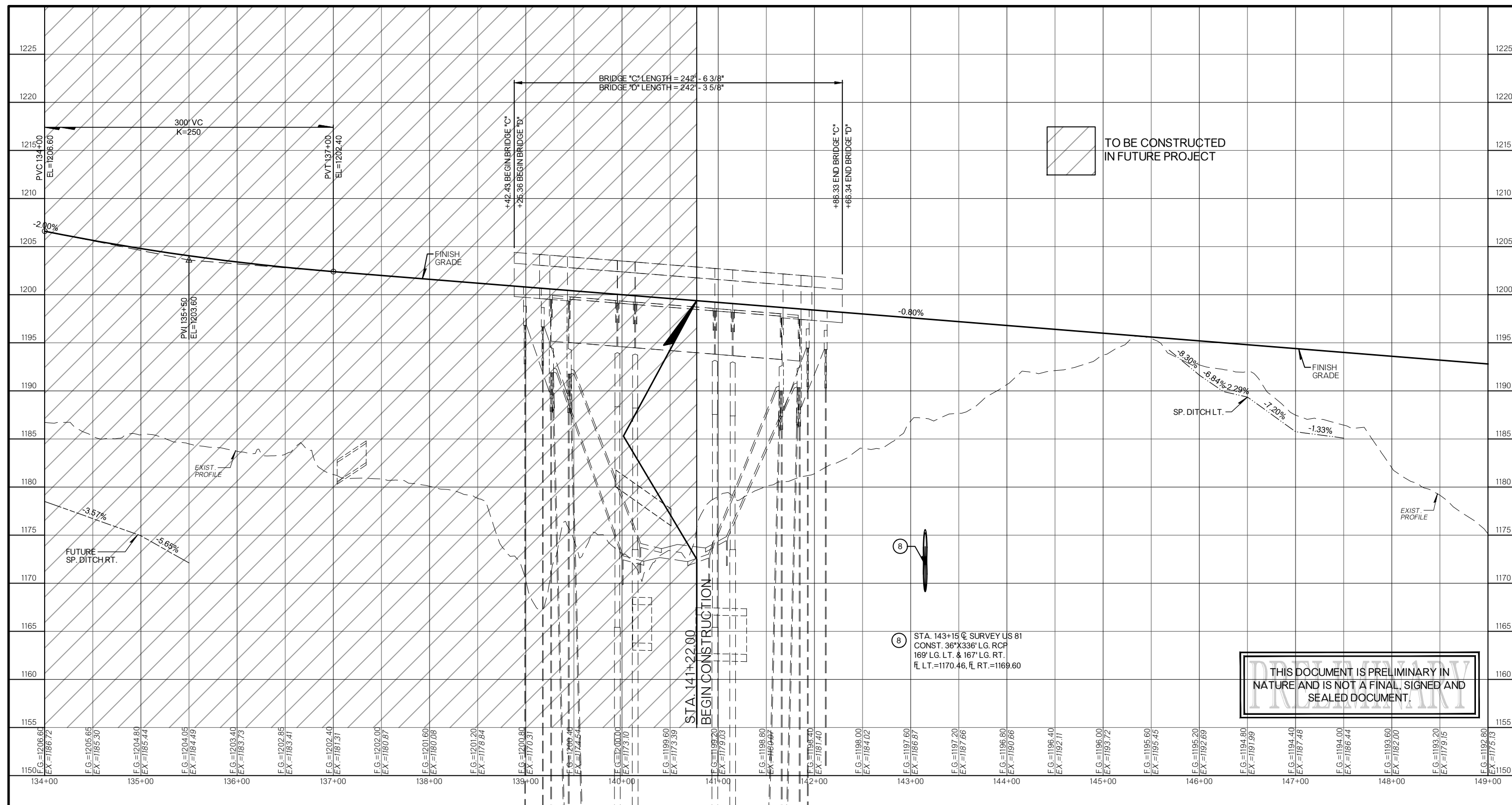
BRIDGE "C"

BRIDGE "D"

UTILITY OWNERS

| | |
|--------------------------------|----------------|
| MEDICINE PARK TELEPHONE CO. | (580) 529-2700 |
| CHICKASAW TELEPHONE CO. | (580) 618-5455 |
| SOUTHWESTERN BELL | (800) 522-6543 |
| DOBSON TECHNOLOGIES | (800) 778-9140 |
| AT&T | (800) 778-9140 |
| INTELLEQ COMMUNICATIONS | (800) 335-4343 |
| PUBLIC SERVICE CO. OF OKLAHOMA | (888) 216-3523 |
| OEC | (405) 321-2024 |
| SUNOCO LOGISTICS | (800) 753-5531 |
| UNIT PETROLEUM | (918) 493-7700 |
| CONTINUUM ENERGY | (877) 587-0026 |
| DCP MIDSTREAM | (800) 435-1679 |
| ENABLE | (800) 522-8048 |
| KEPCO OPERATING INC. | (855) 421-2088 |
| TECHNICAL ENERGY SERVICES | (405) 329-8196 |
| CITY OF CHICKASHA PUBLIC WORKS | (405) 222-6080 |
| RURAL WATER DISTRICT #6 | (405) 459-6626 |
| RURAL WATER DISTRICT #7 | (405) 779-6224 |
| CENTERPOINT ENERGY | (866) 275-5265 |

PLAN
US 81 REALIGNMENT
134+00 TO 149+00

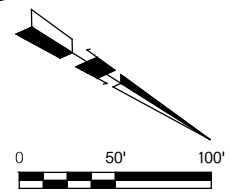


PROFILE
US 81 REALIGNMENT
134+00 TO 149+00

SEC. 17 T6N R7W

BM#13 - 100D NAIL SET N. FACE 24" COTTONWOOD
NEAR S.E. CORNER OF POND DAM
Q SURVEY U.S. 81
STA. 151+40.21, 517.03' LT.
ELEV. = 1173.321

BM#215 - 80D SPIKE IN FENCE POST
Q SURVEY U.S. 81
STA. 155+15.31, 300.60' LT.
ELEV. = 1186.551



STA. 151+08 TO STA. 151+25
CONST. 72 LF FENCE
STYLE WWF

STA. 151+72 TO STA. 151+95
CONST. 30 LF FENCE
STYLE WWF

CAUTION:
WATERLINE

B RAMP D
N29°45'05.42"W

STA. 149+92 TO STA. 149+99
CONST. 30 LF FENCE
STYLE WWF

STA. 149+96 TO 187+33
CONST. 8' BOTTOM PAVED DITCH RT.

STA. 149+00 TO STA. 149+50
CONST. 73 LF FENCE
STYLE WWF

CREEK HAVEN
ESTATES

SEC. 17 T6N R7W

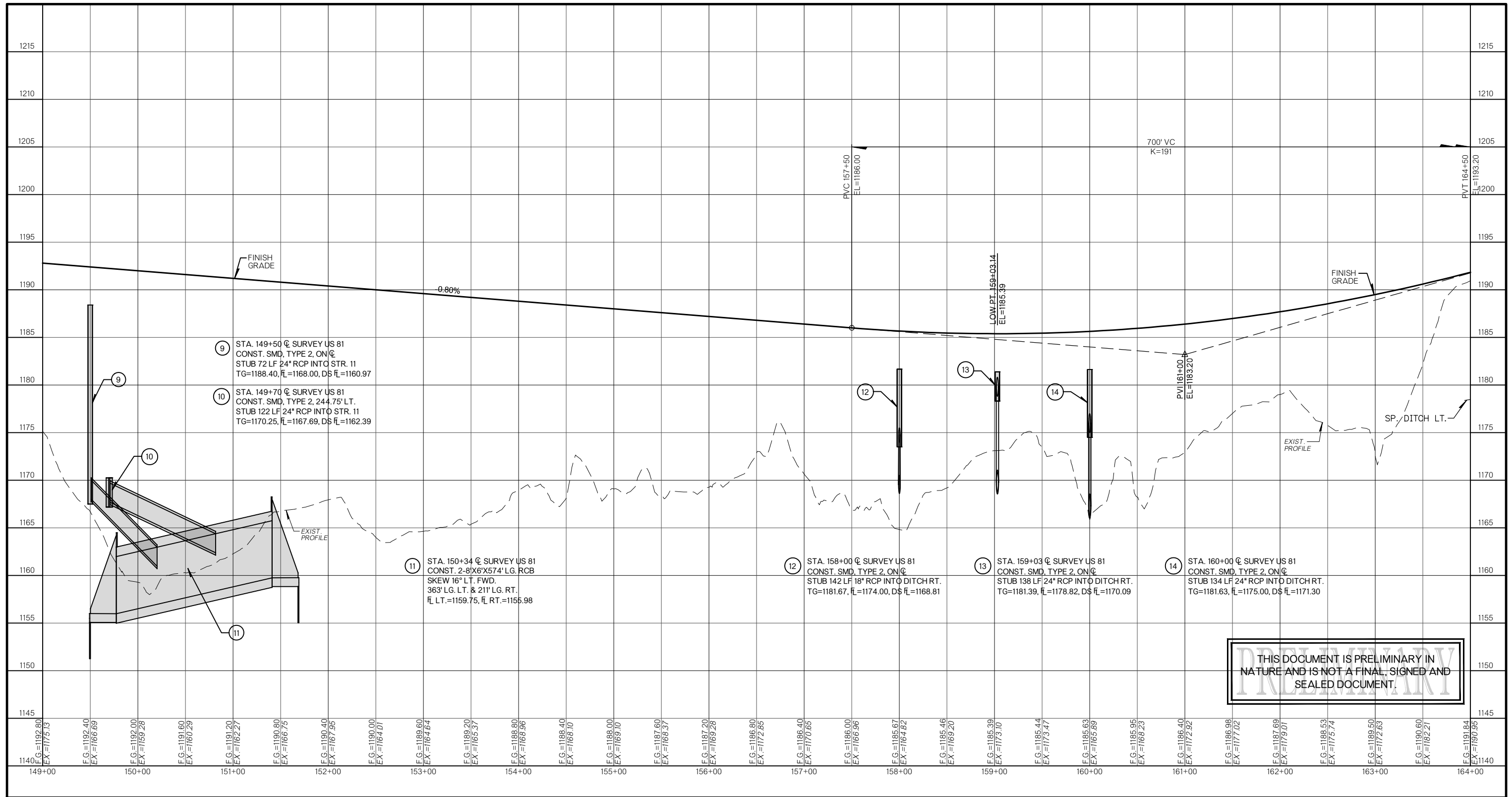
NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY U.S. 81
UNLESS OTHERWISE NOTED.

THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

PLAN
US 81 REALIGNMENT
149+00 TO 164+00

US 81 REALIGNMENT
GRADY COUNTY



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

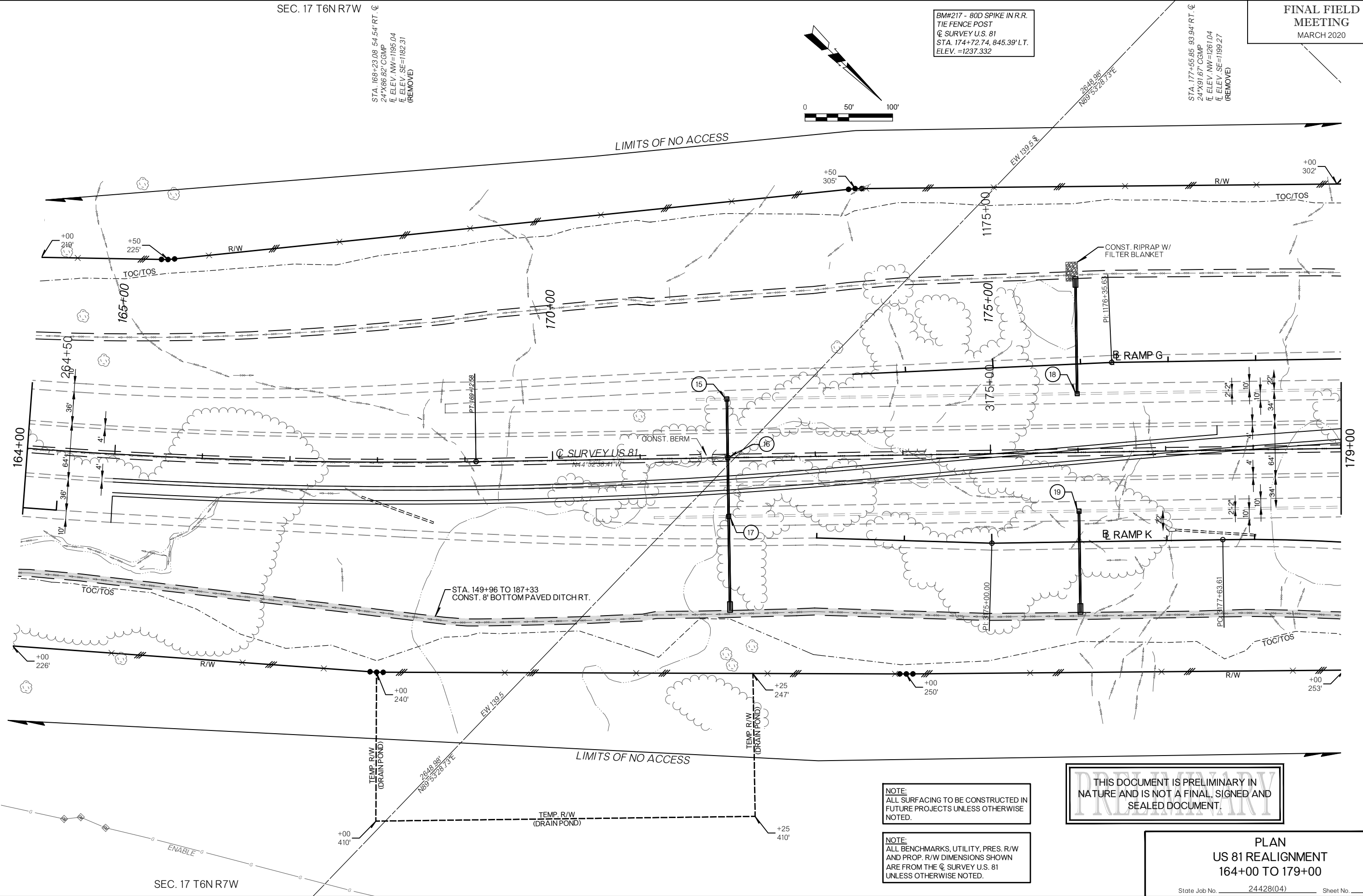
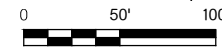
SEC. 17 T6N R7W

STA. 168+23.08 54.54' RT. @
24"X86.82" CGMP
I. ELEV. NW=1195.04
II. ELEV. SE=1182.31
(REMOVE)

BM#217 - 80D SPIKE IN R.R.
TIE FENCE POST
@ SURVEY U.S. 81
STA. 174+72.74, 845.39' LT.
ELEV. =1237.332

STA. 177+55.85 93.94' RT. @
24"X91.67" CGMP
I. ELEV. NW=1261.04
II. ELEV. SE=1199.27
(REMOVE)

FINAL FIELD MEETING
MARCH 2020



NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

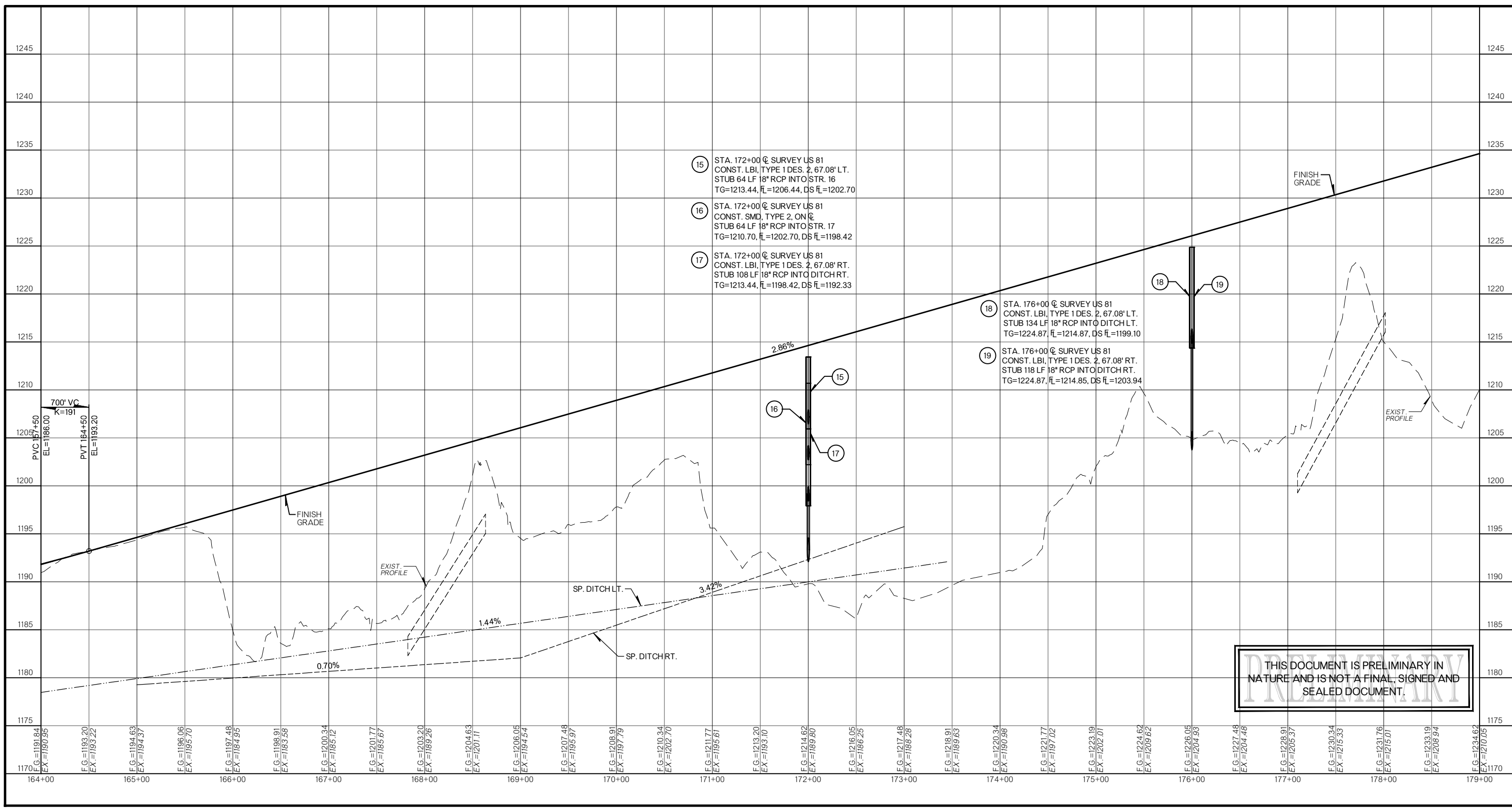
NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE @ SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PLAN
US 81 REALIGNMENT
164+00 TO 179+00
State Job No. 24428(04) Sheet No. R103

SEC. 17 T6N R7W

US 81 REALIGNMENT
GRADY COUNTY

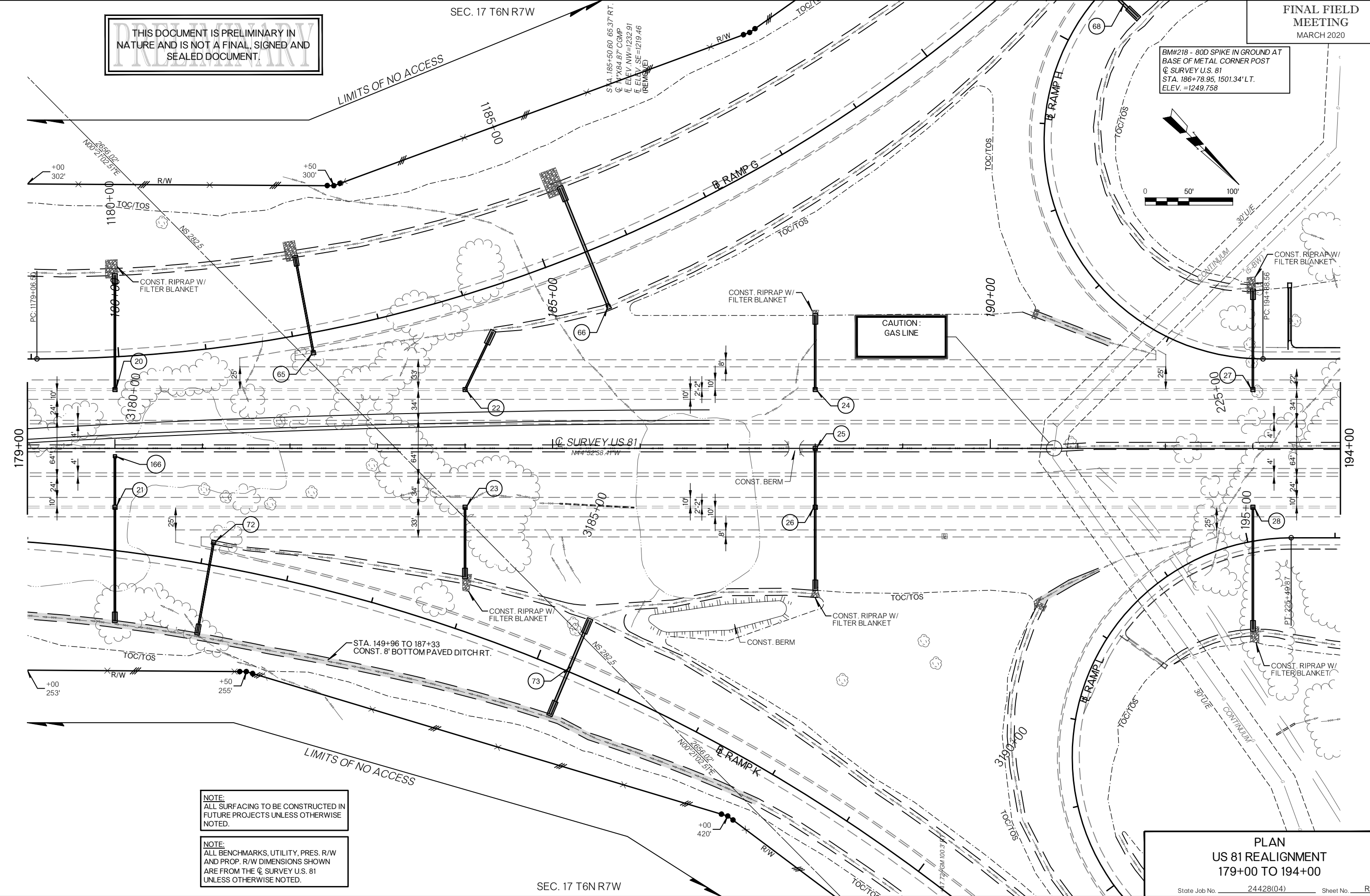
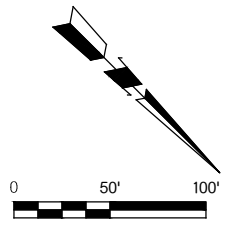


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

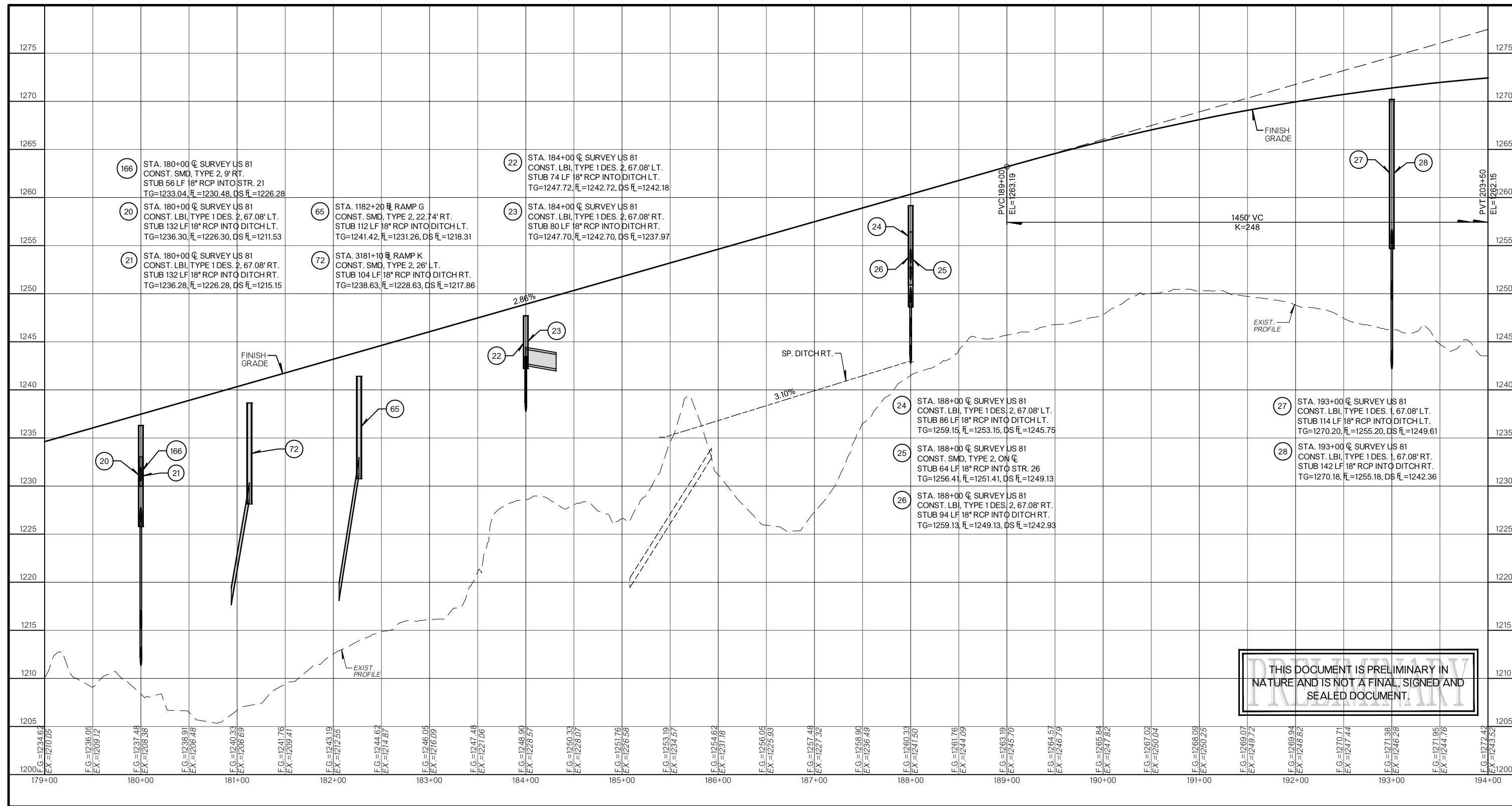
BM#218 - 80D SPIKE IN GROUND AT BASE OF METAL CORNER POST
@ SURVEY U.S. 81
STA. 186+78.95, 1501.34'L.T.
ELEV. =1249.758



NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE @ SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

PLAN
US 81 REALIGNMENT
179+00 TO 194+00

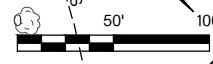


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

BM#2 - 100D NAIL SET IN
E. FACE 12" ELM
Q SURVEY U.S. 81
STA. 206+56.29, 312.42' LT.
ELEV. = 1230.502

SEC. 17 T6N R7W

BM#1 - 100D NAIL SET IN E. FACE
8" WOOD POST, N. SIDE OF I-44
Q SURVEY U.S. 81
STA. 197+99.60, 286.18' LT.
ELEV. = 1249.874



CAUTION:
WATERLINE
OR POWER
FIBER

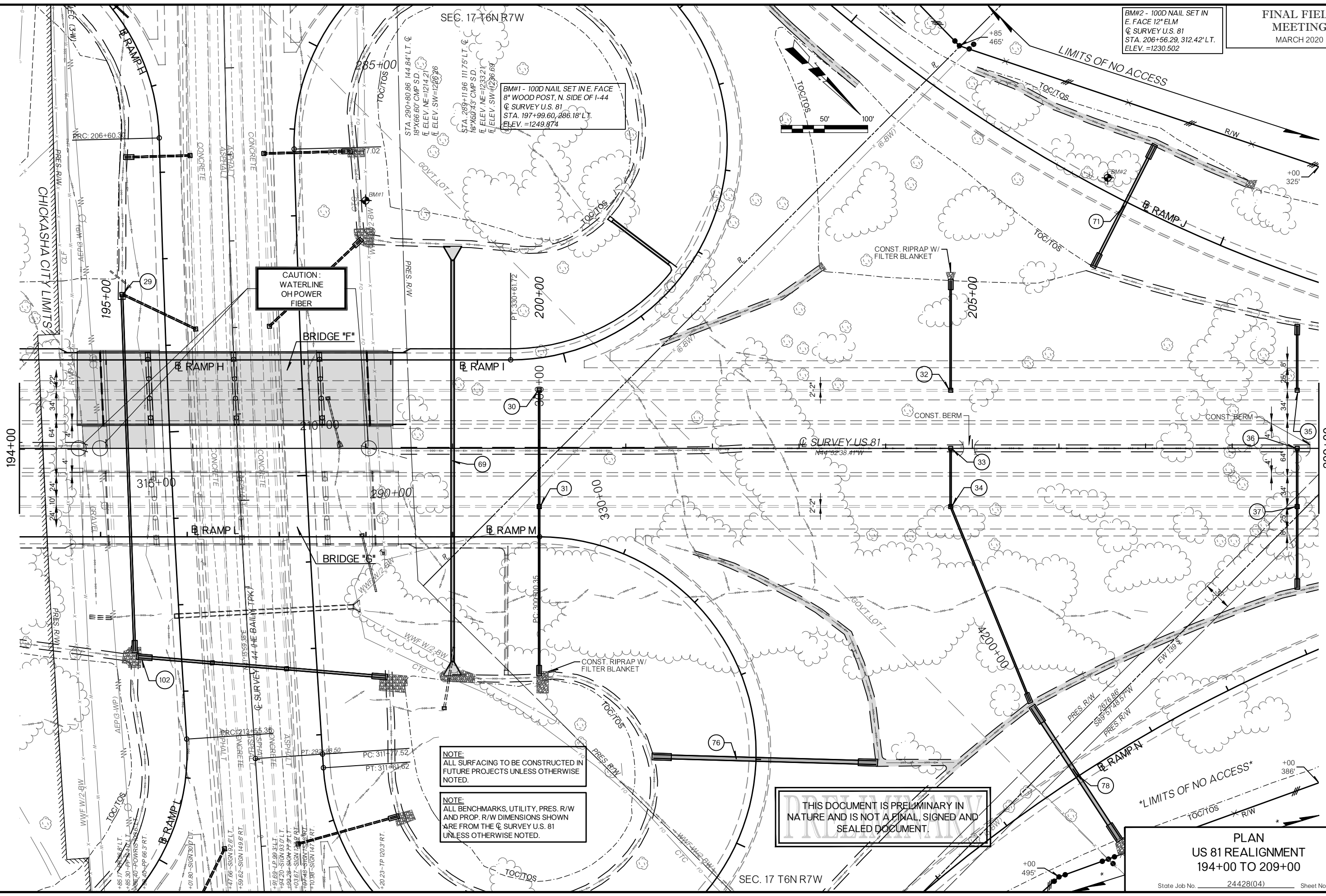
NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY U.S. 81
UNLESS OTHERWISE NOTED.

THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

LIMITS OF NO ACCESS

PLAN
US 81 REALIGNMENT
194+00 TO 209+00



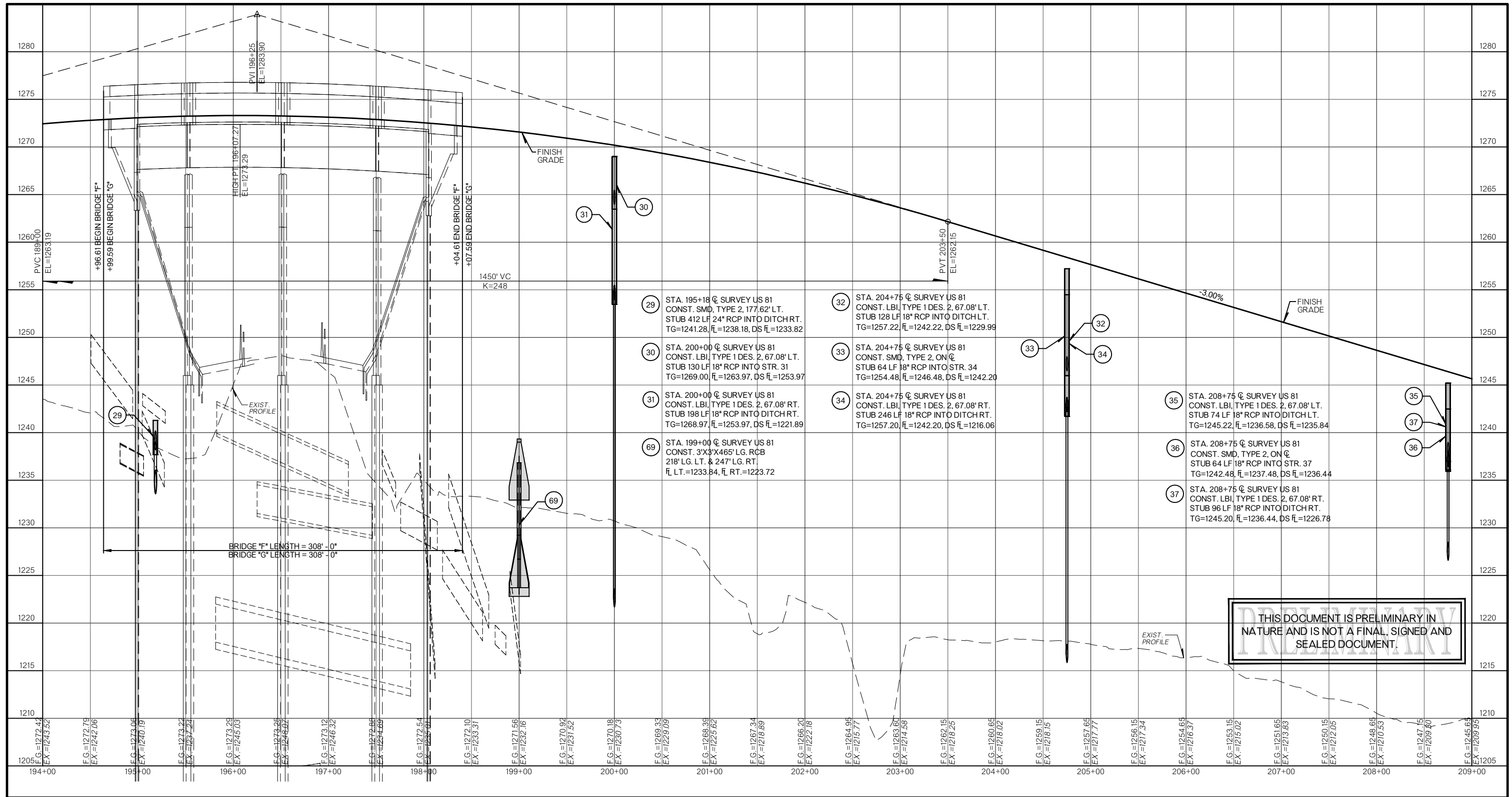
194+00

209+00

SEC. 17 T6N R7W

US 81 REALIGNMENT

GRADY COUNTY

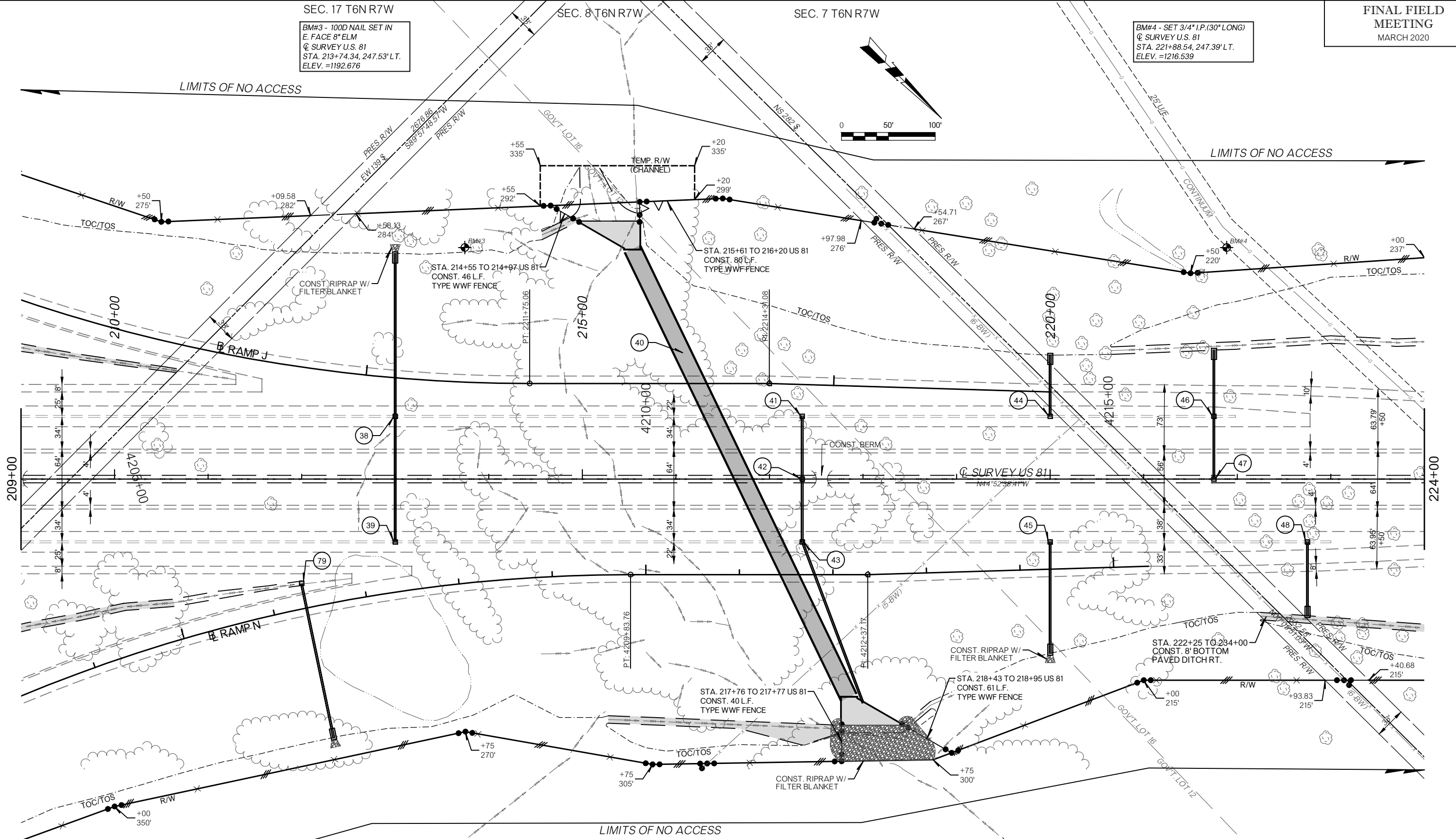
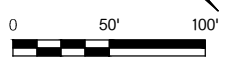


- 29 STA. 195+18 @ SURVEY US 81 CONST. SMD, TYPE 2, 177.62' LT. STUB 412 LF 24" RCP INTO DITCH RT. TG=1241.28, f_L=1238.18, DS f_L=1233.82
- 30 STA. 200+00 @ SURVEY US 81 CONST. LBI, TYPE 1 DES. 2, 67.08' LT. STUB 130 LF 18" RCP INTO STR. 31 TG=1269.00, f_L=1263.97, DS f_L=1253.97
- 31 STA. 200+00 @ SURVEY US 81 CONST. LBI, TYPE 1 DES. 2, 67.08' RT. STUB 198 LF 18" RCP INTO DITCH RT. TG=1268.97, f_L=1253.97, DS f_L=1221.89
- 69 STA. 199+00 @ SURVEY US 81 CONST. 3'X3'X465' LG. RCB 218' LG. LT. & 247' LG. RT. f_L LT.=1233.84, f_L RT.=1223.72
- 32 STA. 204+75 @ SURVEY US 81 CONST. LBI, TYPE 1 DES. 2, 67.08' LT. STUB 128 LF 18" RCP INTO DITCH LT. TG=1257.22, f_L=1242.22, DS f_L=1229.99
- 33 STA. 204+75 @ SURVEY US 81 CONST. SMD, TYPE 2, ON @ STUB 64 LF 18" RCP INTO STR. 34 TG=1254.48, f_L=1246.48, DS f_L=1242.20
- 34 STA. 204+75 @ SURVEY US 81 CONST. LBI, TYPE 1 DES. 2, 67.08' RT. STUB 246 LF 18" RCP INTO DITCH RT. TG=1257.20, f_L=1242.20, DS f_L=1216.06
- 35 STA. 208+75 @ SURVEY US 81 CONST. LBI, TYPE 1 DES. 2, 67.08' LT. STUB 74 LF 18" RCP INTO DITCH LT. TG=1245.22, f_L=1236.58, DS f_L=1235.84
- 36 STA. 208+75 @ SURVEY US 81 CONST. SMD, TYPE 2, ON @ STUB 64 LF 18" RCP INTO STR. 37 TG=1242.48, f_L=1237.48, DS f_L=1236.44
- 37 STA. 208+75 @ SURVEY US 81 CONST. LBI, TYPE 1 DES. 2, 67.08' RT. STUB 96 LF 18" RCP INTO DITCH RT. TG=1245.20, f_L=1236.44, DS f_L=1226.78

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEC. 17 T6N R7W
BM#3 - 100D NAIL SET IN E. FACE 8" ELM
Q SURVEY U.S. 81
STA. 213+74.34, 247.53' LT.
ELEV. =1192.676

BM#4 - SET 3/4" I.P. (30" LONG)
Q SURVEY U.S. 81
STA. 221+88.54, 247.39' LT.
ELEV. =1216.539



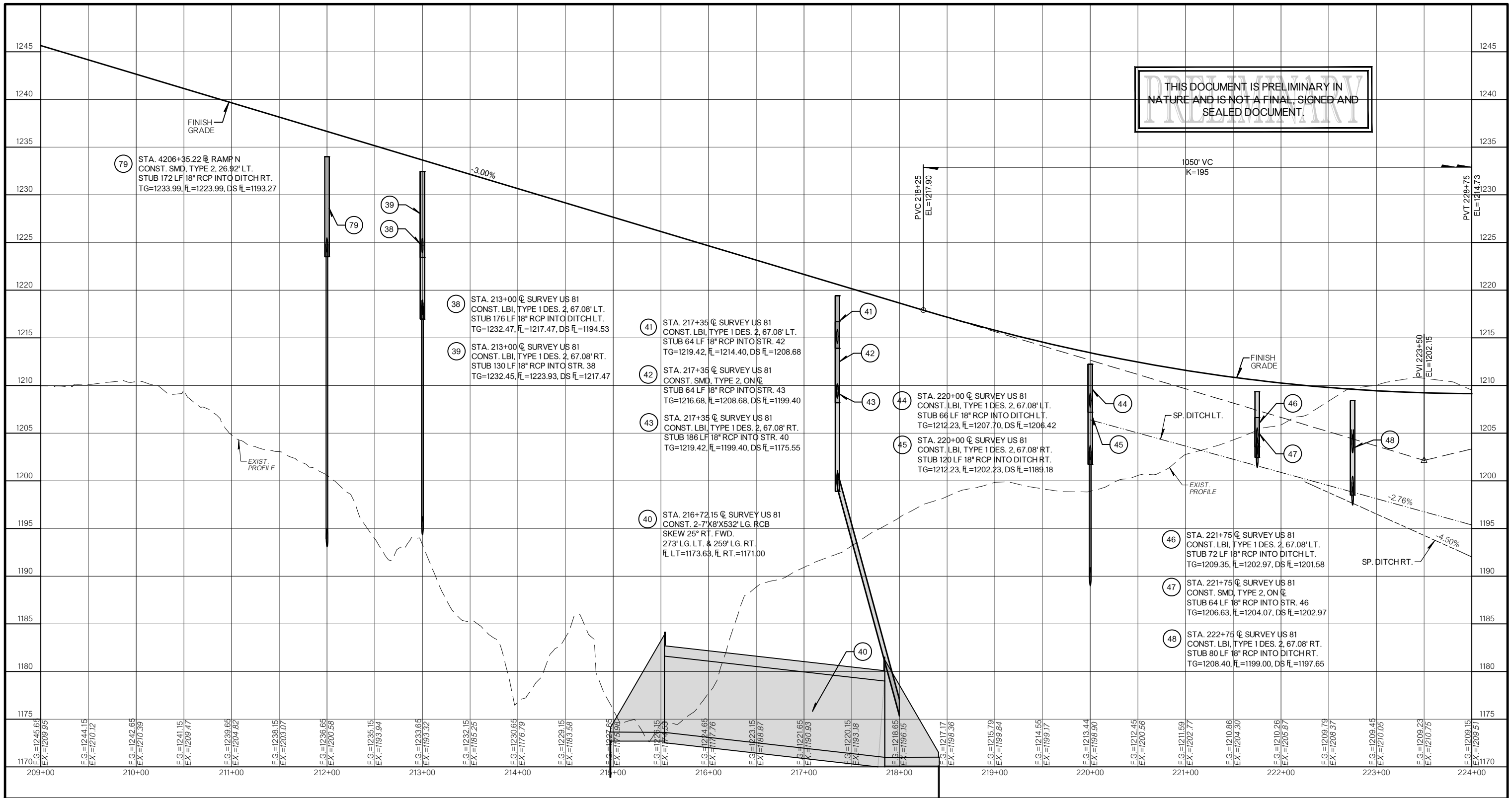
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PLAN
US 81 REALIGNMENT
209+00 TO 224+00

SEC. 8 T6N R7W

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



| | | | | | | | | | | | | | | | |
|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|--------------------------|
| 209+00 | 210+00 | 211+00 | 212+00 | 213+00 | 214+00 | 215+00 | 216+00 | 217+00 | 218+00 | 219+00 | 220+00 | 221+00 | 222+00 | 223+00 | 224+00 |
| FG=1245.64 EX=1209.95 | FG=1244.15 EX=1210.12 | FG=1242.65 EX=1210.39 | FG=1241.15 EX=1209.47 | FG=1239.65 EX=1204.82 | FG=1238.15 EX=1203.07 | FG=1236.65 EX=1200.59 | FG=1235.15 EX=1193.94 | FG=1233.65 EX=1193.32 | FG=1232.15 EX=1185.25 | FG=1230.65 EX=1176.79 | FG=1229.15 EX=1183.58 | FG=1227.65 EX=1175.00 | FG=1226.15 EX=1173.63 | FG=1224.65 EX=1171.00 | FG=1223.15 EX=1170.00 |

PROFILE
US 81 REALIGNMENT
209+00 TO 224+00
State Job No. 24428(04) Sheet No. R110

US 81 REALIGNMENT
GRADY COUNTY

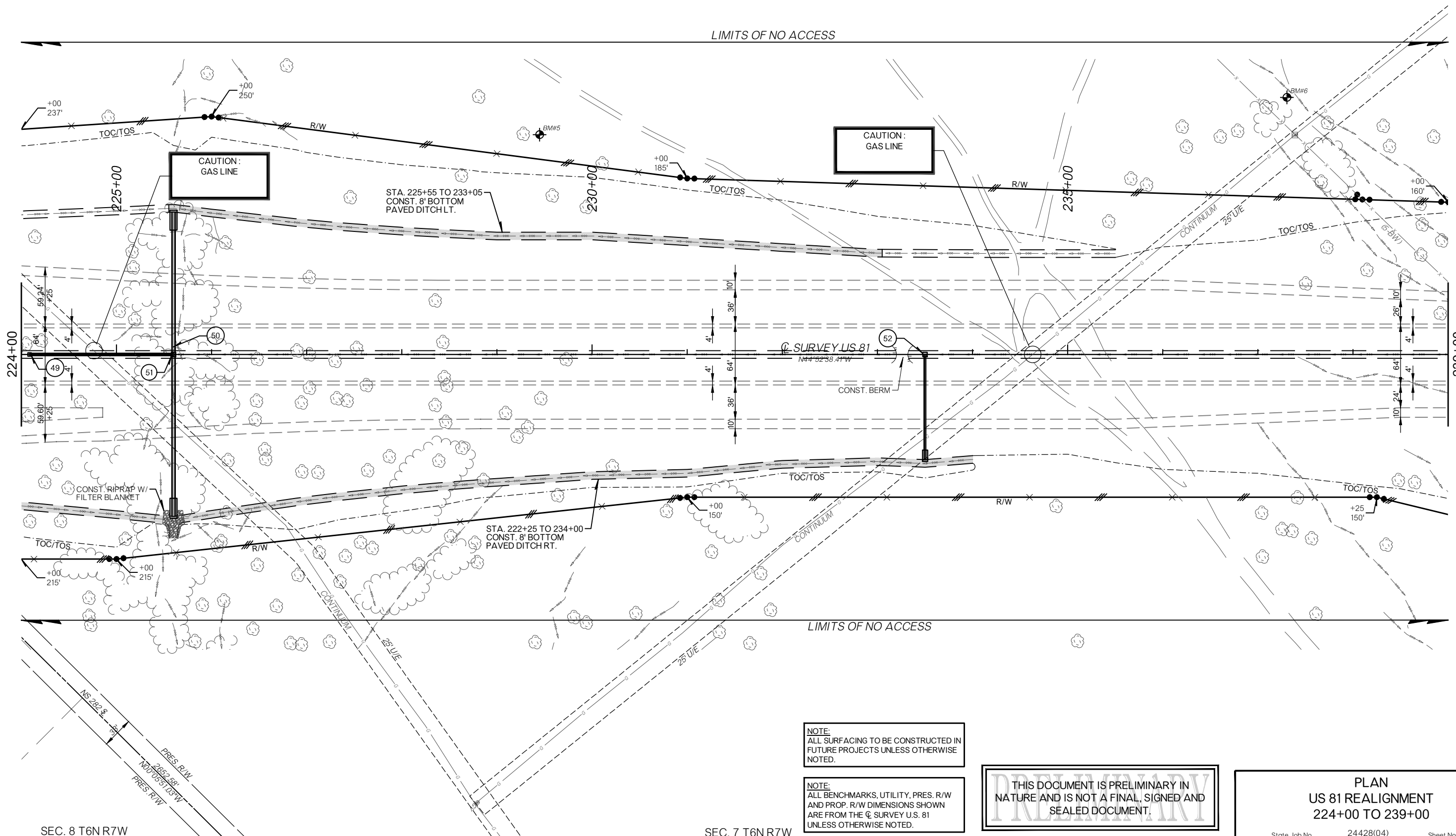
BM#5 - SET 3/4" I.P. (30" LONG)
@ SURVEY U.S. 81
STA. 229+44.85, 230.97' LT.
ELEV. =1227.592

SEC. 7 T6N R7W

BM#6 - SET 3/4" I.P. (30" LONG)
@ SURVEY U.S. 81
STA. 237+30.46, 270.08' LT.
ELEV. =1220.146

FINAL FIELD
MEETING
MARCH 2020

LIMITS OF NO ACCESS



NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE @ SURVEY U.S. 81
UNLESS OTHERWISE NOTED.

THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

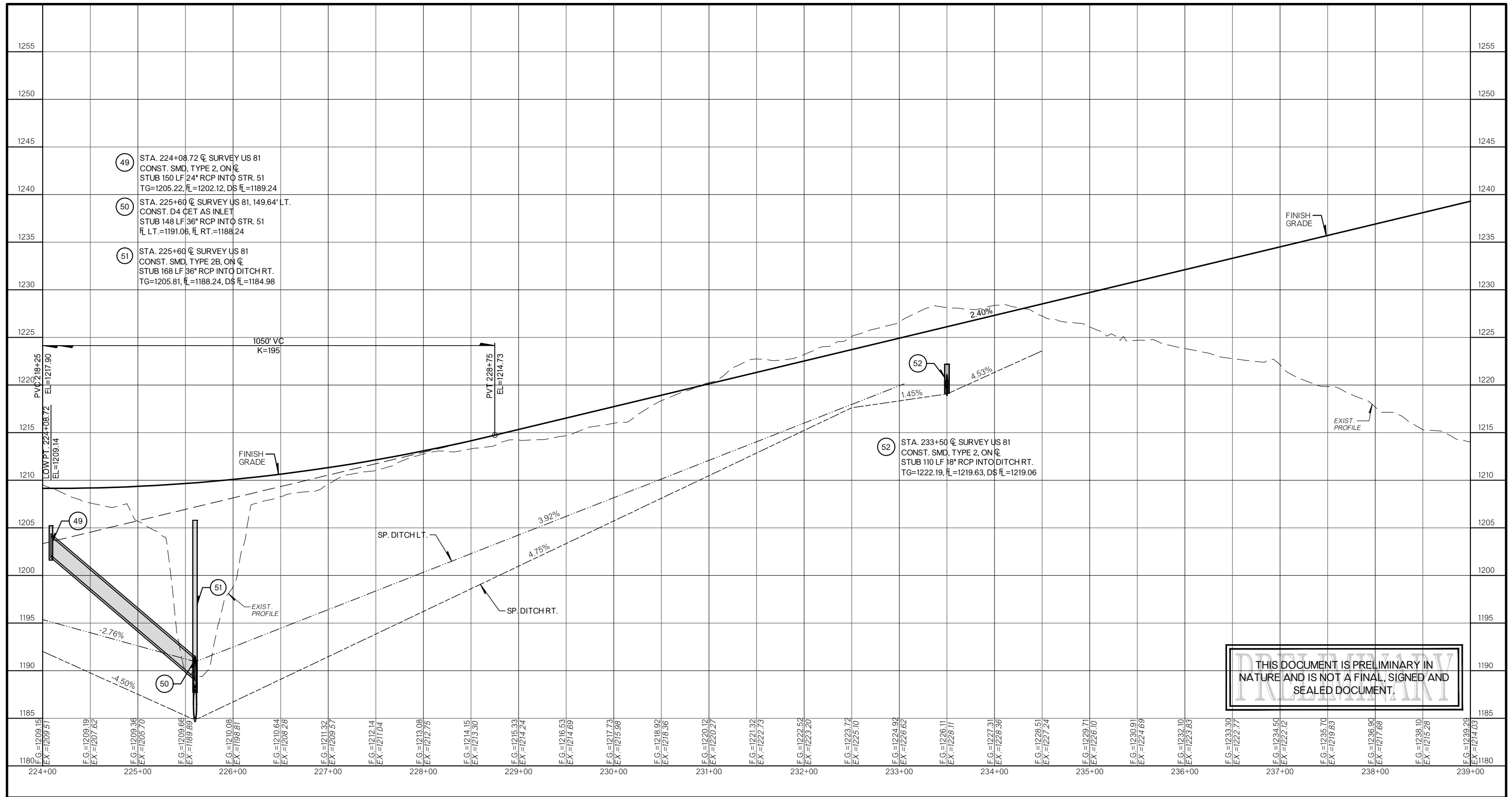
PLAN
US 81 REALIGNMENT
224+00 TO 239+00

State Job No. 24428(04) Sheet No. R11

SEC. 8 T6N R7W

SEC. 7 T6N R7W

US 81 REALIGNMENT
GRADY COUNTY



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL SIGNED AND SEALED DOCUMENT.

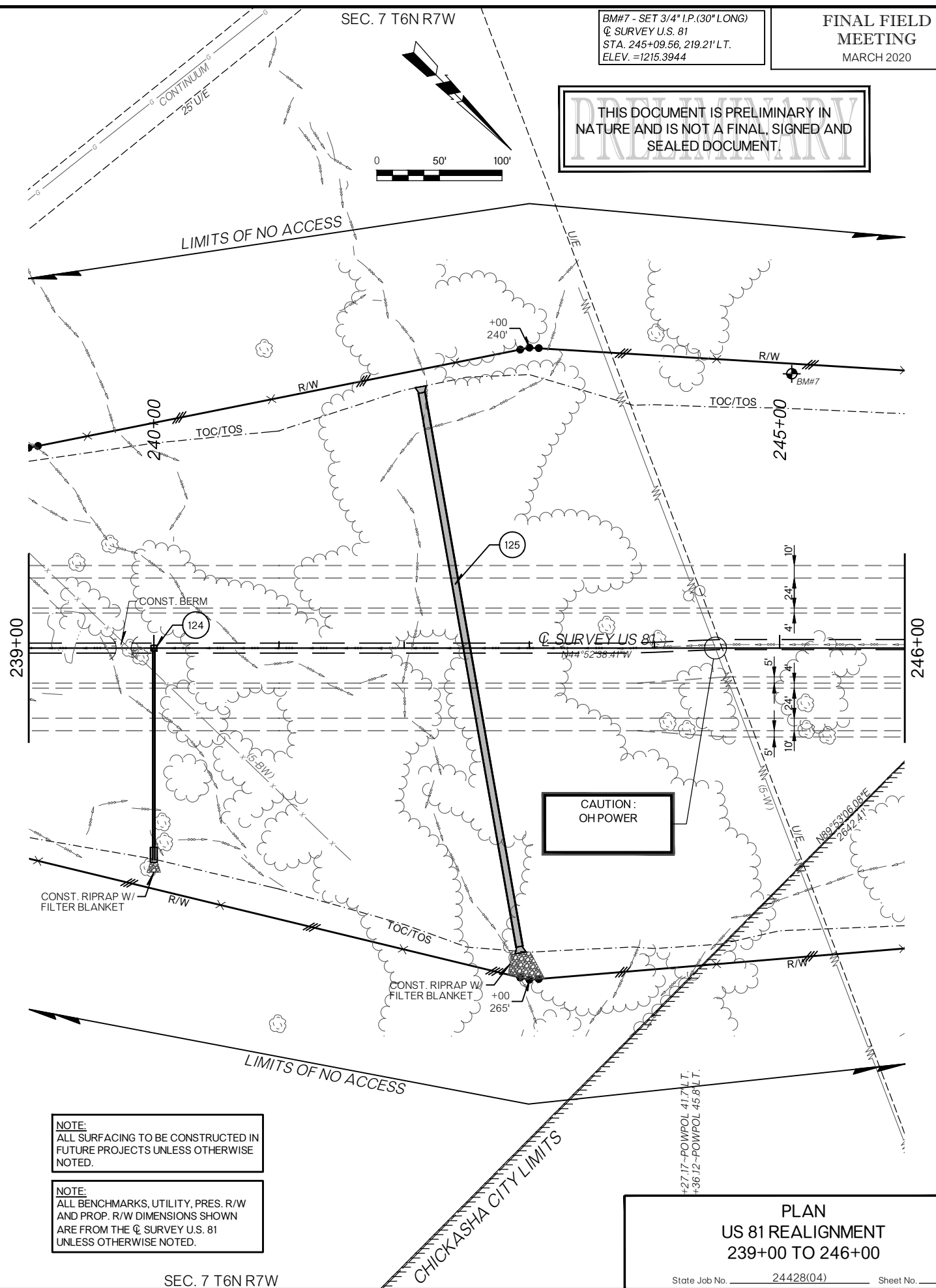
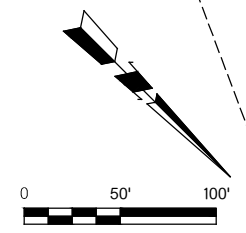
PROFILE
US 81 REALIGNMENT
224+00 TO 239+00
State Job No. 24428(04) Sheet No. R112

SEC. 7 T6N R7W

BM#7 - SET 3/4" I.P. (30" LONG)
@ SURVEY U.S. 81
STA. 245+09.56, 219.21' LT.
ELEV. = 1215.3944

FINAL FIELD MEETING
MARCH 2020

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

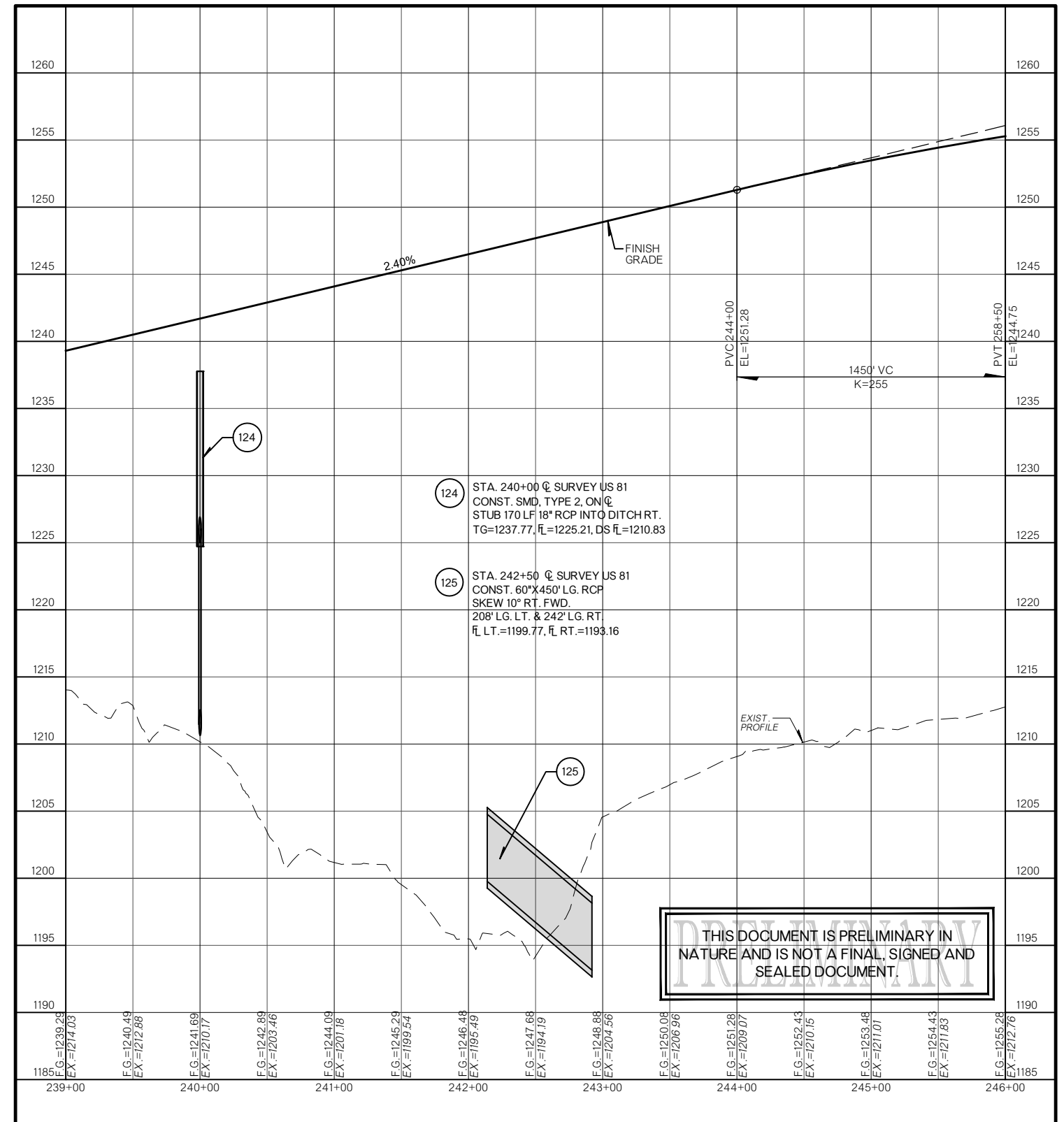
NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

PLAN
US 81 REALIGNMENT
239+00 TO 246+00

State Job No. 24428(04) Sheet No. R113

SEC. 7 T6N R7W

US 81 REALIGNMENT
GRADY COUNTY

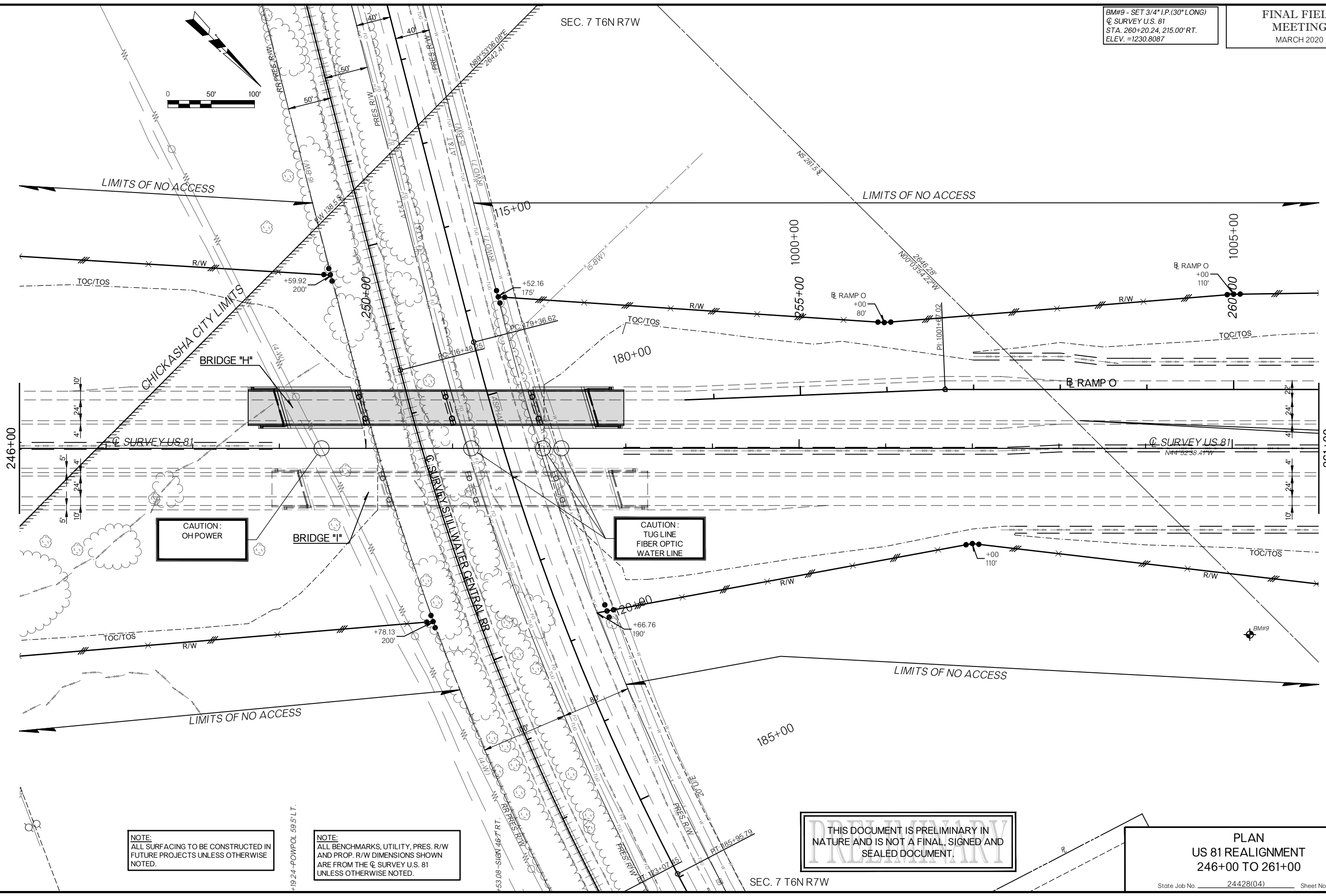
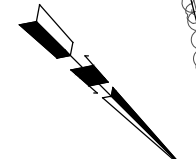
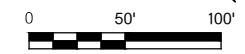


PROFILE
US 81 REALIGNMENT
239+00 TO 246+00

BM#9 - SET 3/4" I.P. (30" LONG)
Q SURVEY U.S. 81
STA. 260+20.24, 215.00' RT.
ELEV. =1230.8087

FINAL FIELD MEETING
MARCH 2020

SEC. 7 T6N R7W



CAUTION:
OH POWER

CAUTION:
TUG LINE
FIBER OPTIC
WATER LINE

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY U.S. 81
UNLESS OTHERWISE NOTED.

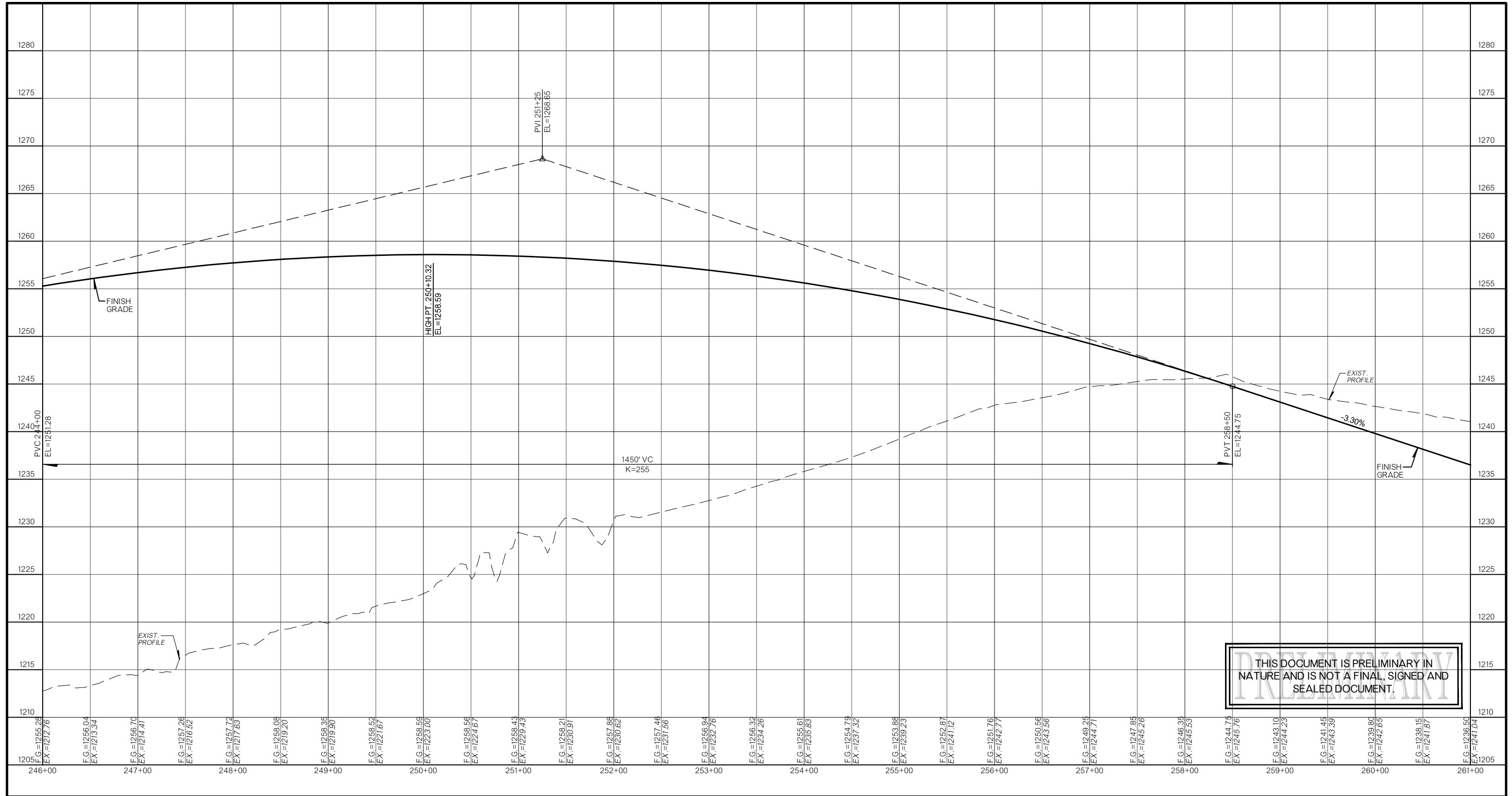
THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

PLAN
US 81 REALIGNMENT
246+00 TO 261+00

State Job No. 24428(04) Sheet No. R115

SEC. 7 T6N R7W

US 81 REALIGNMENT
GRADY COUNTY



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

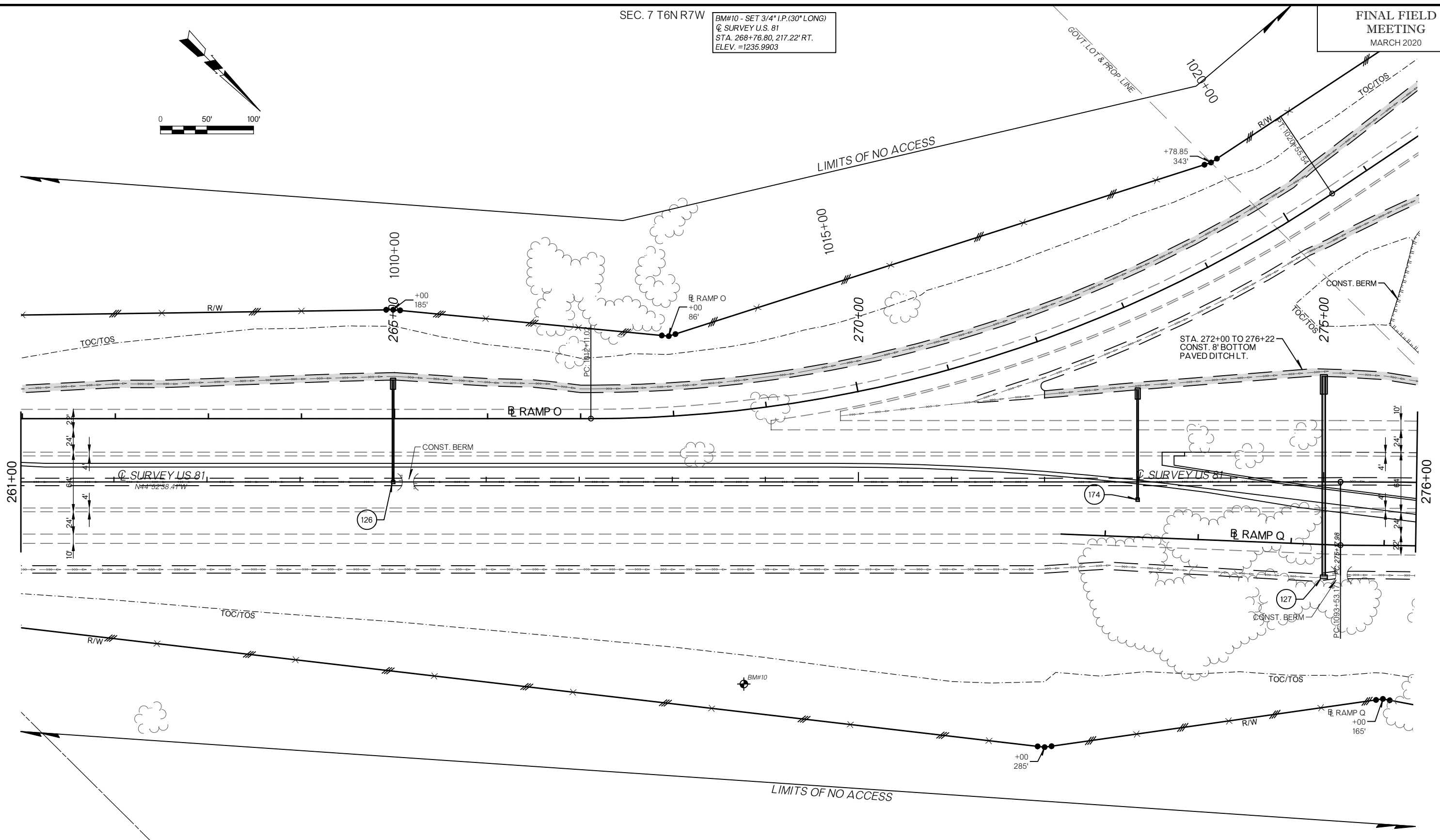
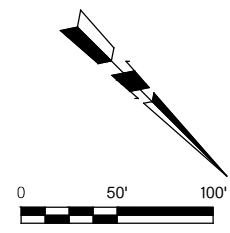
PROFILE
US 81 REALIGNMENT
246+00 TO 261+00
State Job No. 24428(04) Sheet No. R116

US 81 REALIGNMENT
GRADY COUNTY

SEC. 7 T6N R7W

BM#10 - SET 3/4" I.P. (30" LONG)
Q SURVEY U.S. 81
STA. 268+76.80, 217.22' RT.
ELEV. = 1235.9903

FINAL FIELD MEETING
MARCH 2020



NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY U.S. 81
UNLESS OTHERWISE NOTED.

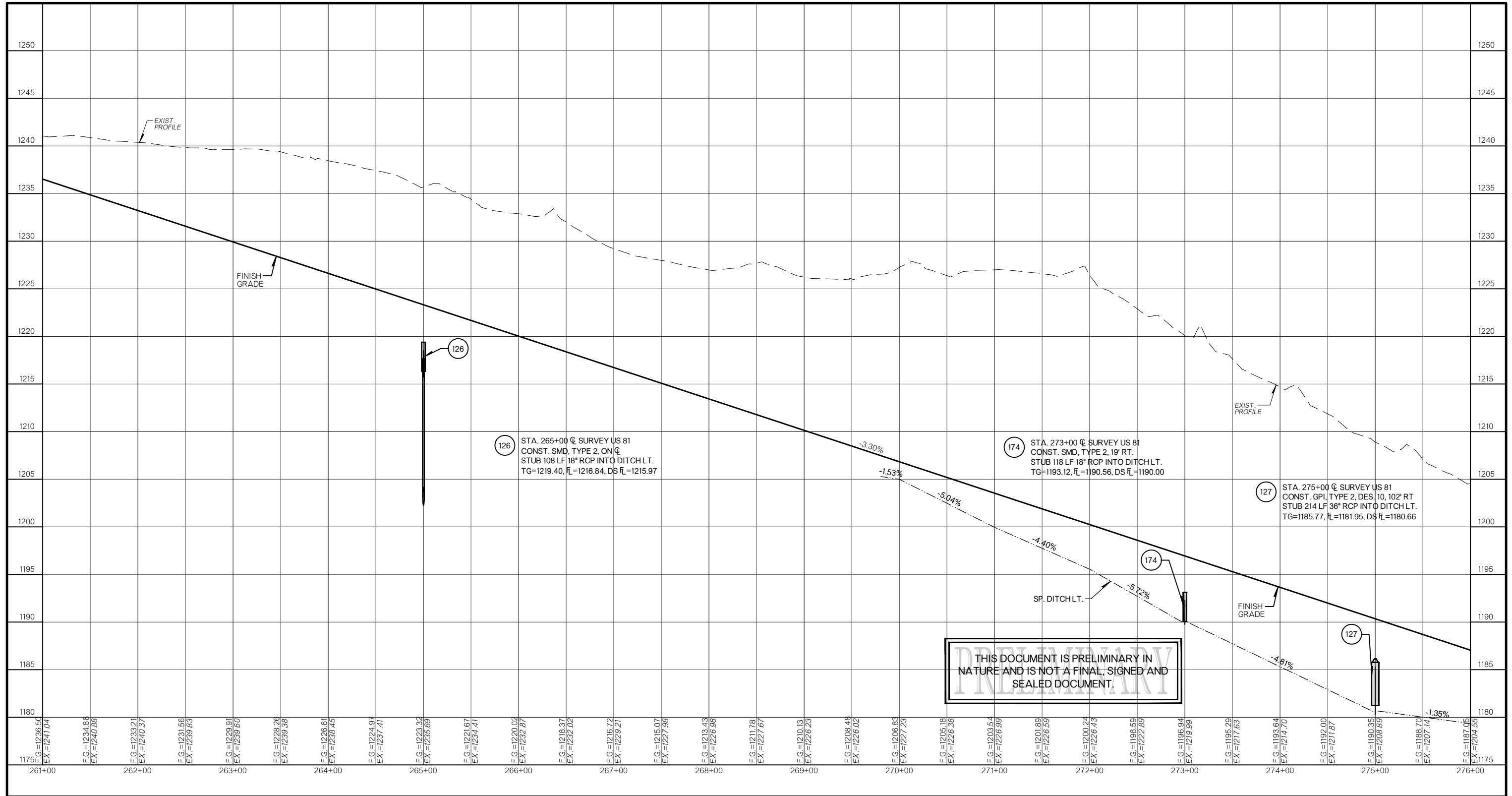
THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

PLAN
US 81 REALIGNMENT
261+00 TO 276+00

SEC. 7 T6N R7W

State Job No. 24428(04) Sheet No. R117

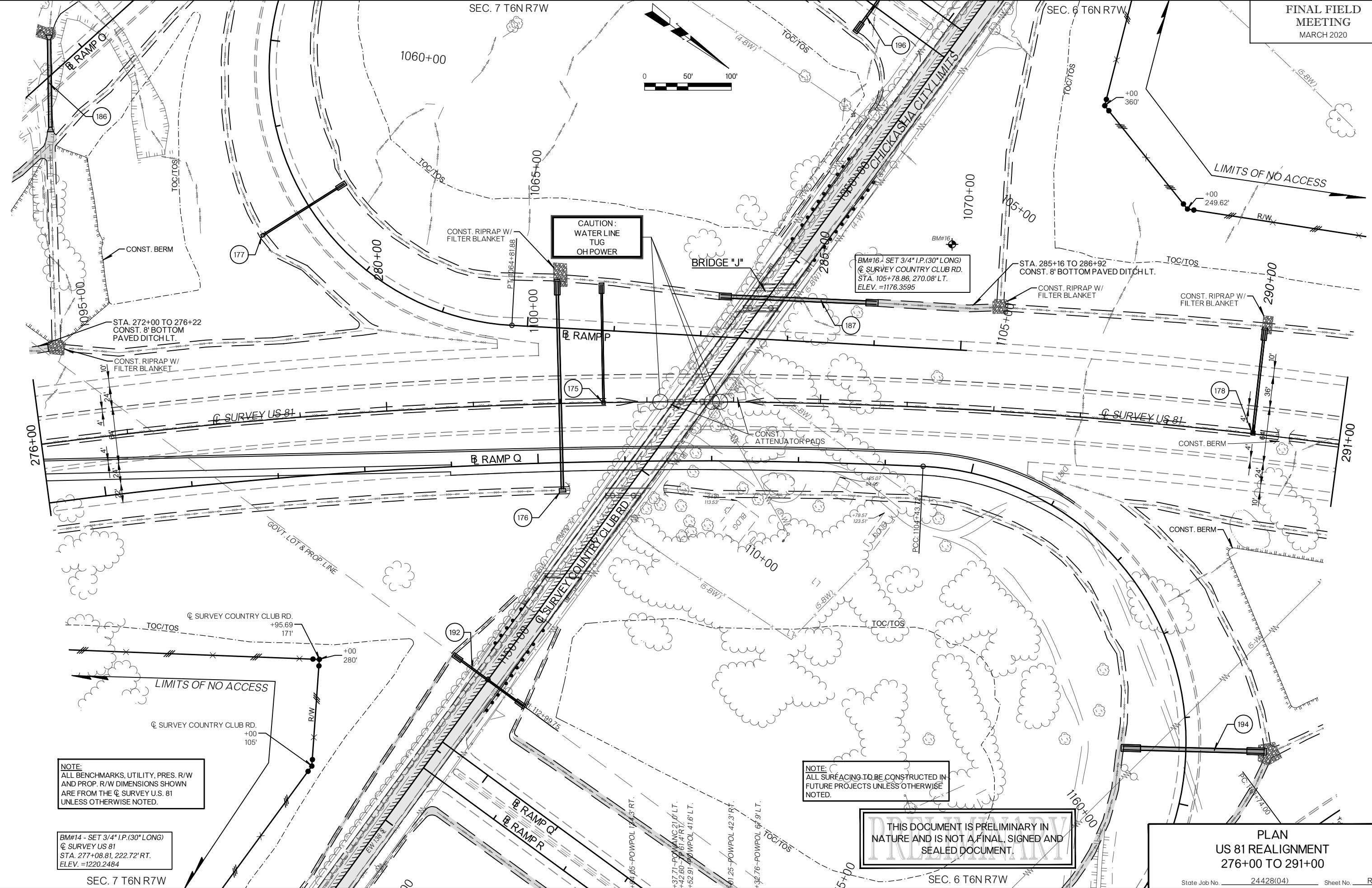
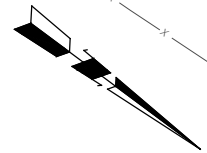
US 81 REALIGNMENT
GRADY COUNTY



**PROFILE
US 81 REALIGNMENT
261+00 TO 276+00**

State Job No. 24428(04) Sheet No. R118

US 81 REALIGNMENT
GRADY COUNTY



NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY U.S. 81
UNLESS OTHERWISE NOTED.

BM#14 - SET 3/4" I.P. (30" LONG)
Q SURVEY US 81
STA. 277+08.81, 222.72' RT.
ELEV. = 1220.2484

CAUTION:
WATER LINE
TUG
OH POWER

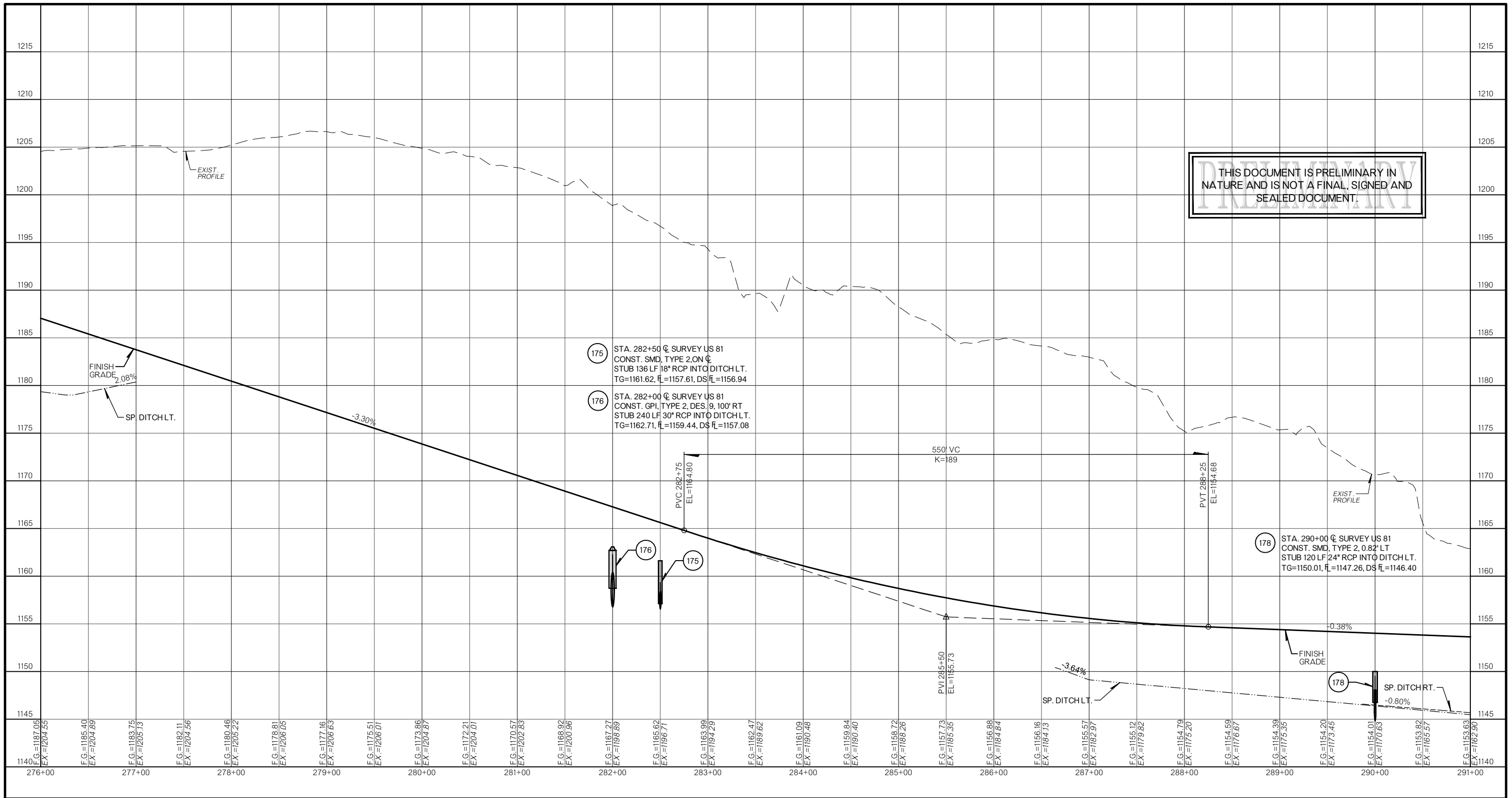
BM#16 - SET 3/4" I.P. (30" LONG)
Q SURVEY COUNTRY CLUB RD.
STA. 105+78.86, 270.08' LT.
ELEV. = 1176.3595

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

PLAN
US 81 REALIGNMENT
276+00 TO 291+00

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



**PROFILE
US 81 REALIGNMENT
276+00 TO 291+00**

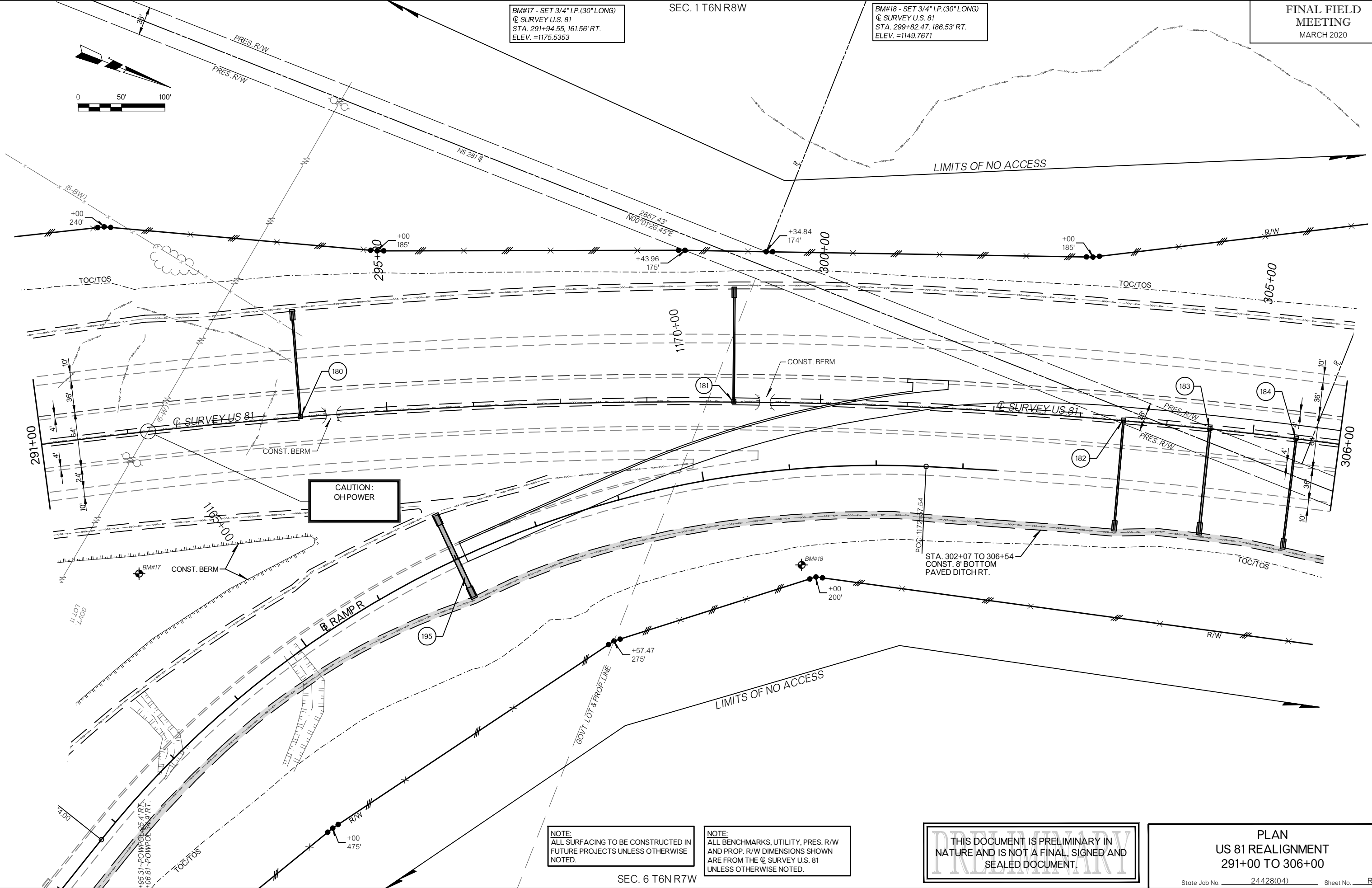
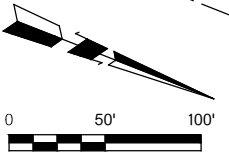
State Job No. 24428(04) Sheet No. R120

US 81 REALIGNMENT
GRADY COUNTY

SEC. 1 T6N R8W

BM#17 - SET 3/4" I.P. (30" LONG)
@ SURVEY U.S. 81
STA. 291+94.55, 161.56' RT.
ELEV. = 1175.5353

BM#18 - SET 3/4" I.P. (30" LONG)
@ SURVEY U.S. 81
STA. 299+82.47, 186.53' RT.
ELEV. = 1149.7671



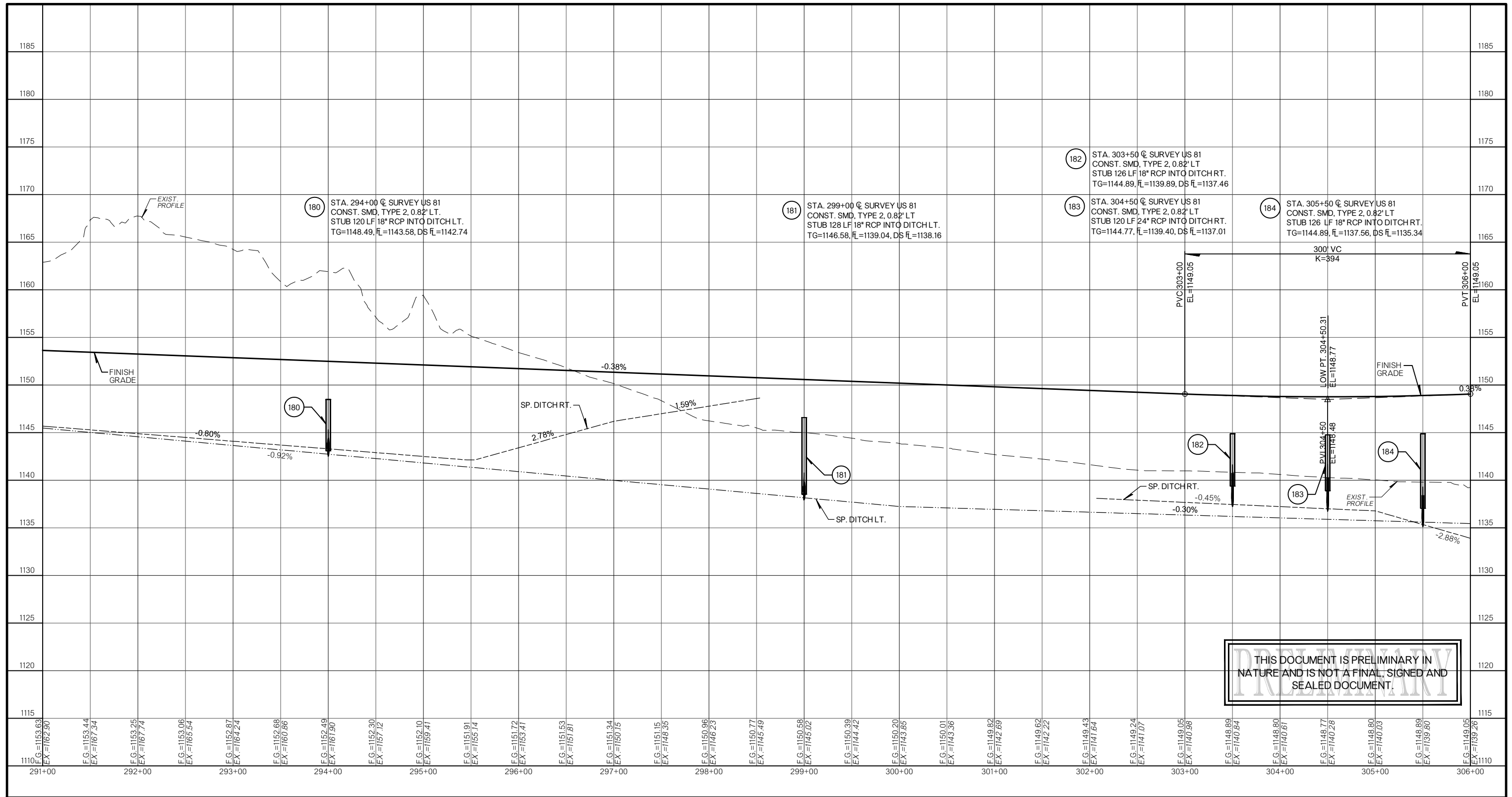
NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE @ SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PLAN
US 81 REALIGNMENT
291+00 TO 306+00

SEC. 6 T6N R7W



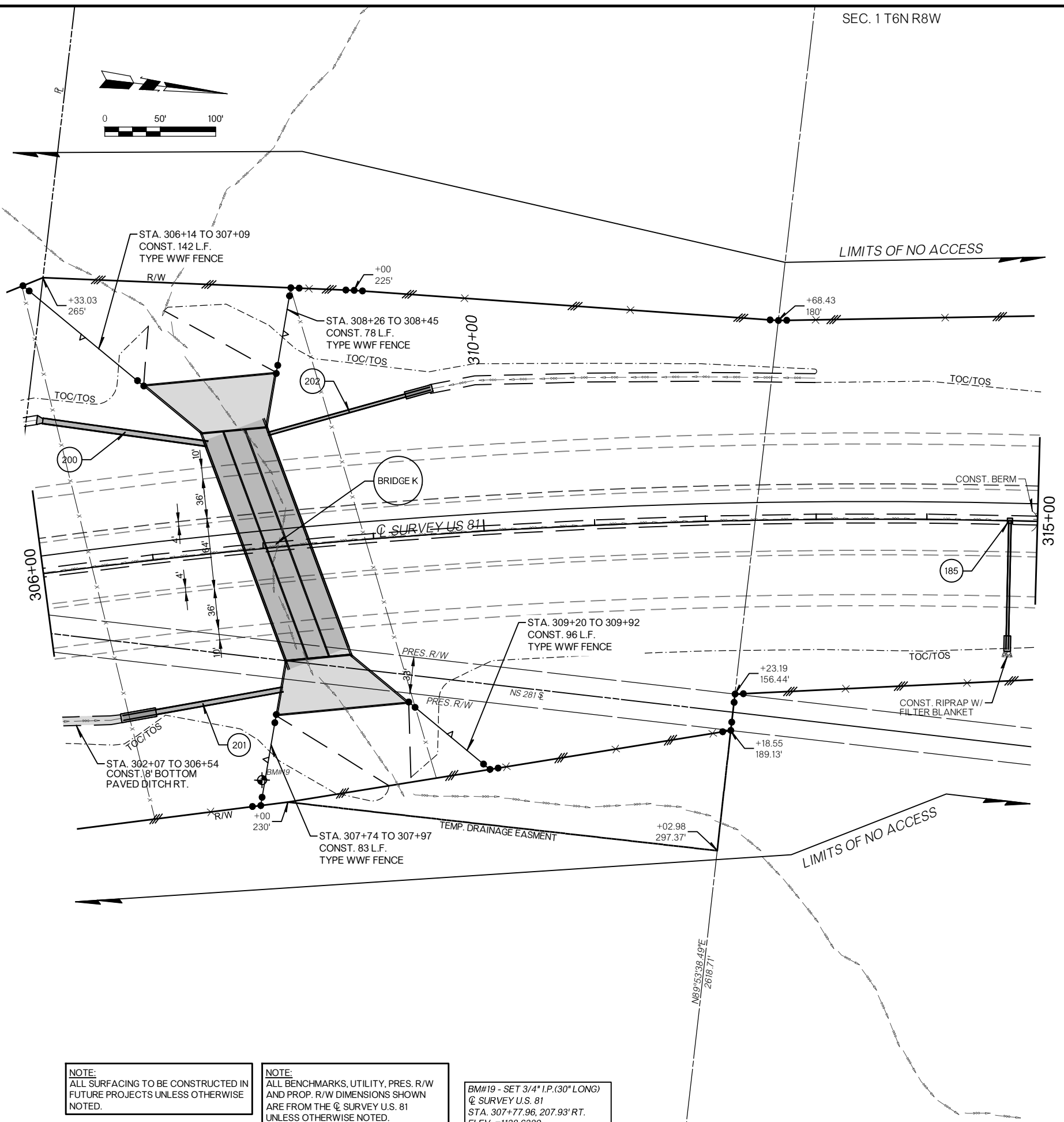
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROFILE
US 81 REALIGNMENT
291+00 TO 306+00
State Job No. 24428(04) Sheet No. R122

US 81 REALIGNMENT
GRADY COUNTY

SEC. 1 T6N R8W

SEC. 6 T6N R7W



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

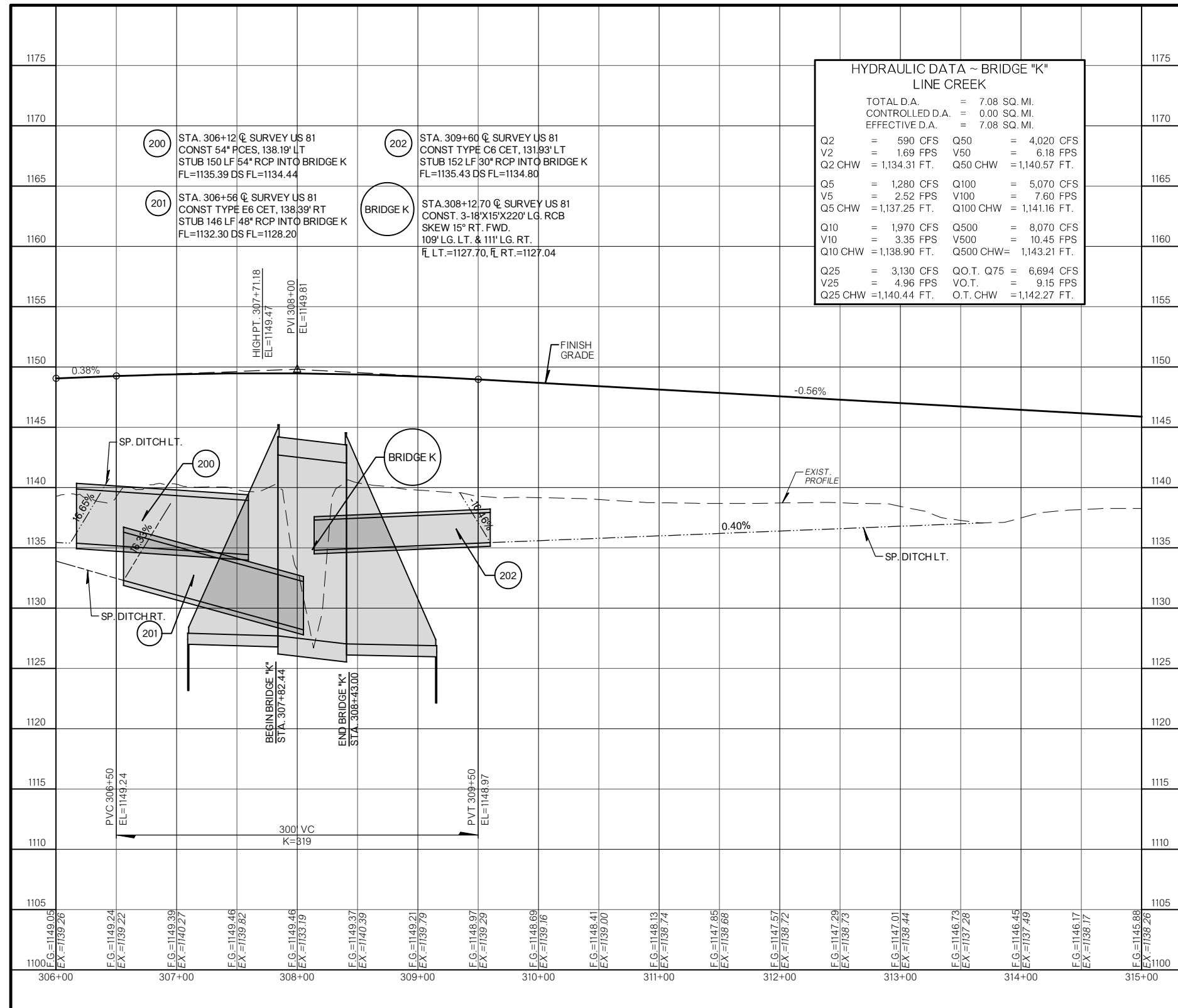
NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE \bar{C} SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

BM#19 - SET 3/4\" I.P. (30\" LONG)
 \bar{C} SURVEY U.S. 81
STA. 307+77.96, 207.93' RT.
ELEV. = 1138.6389

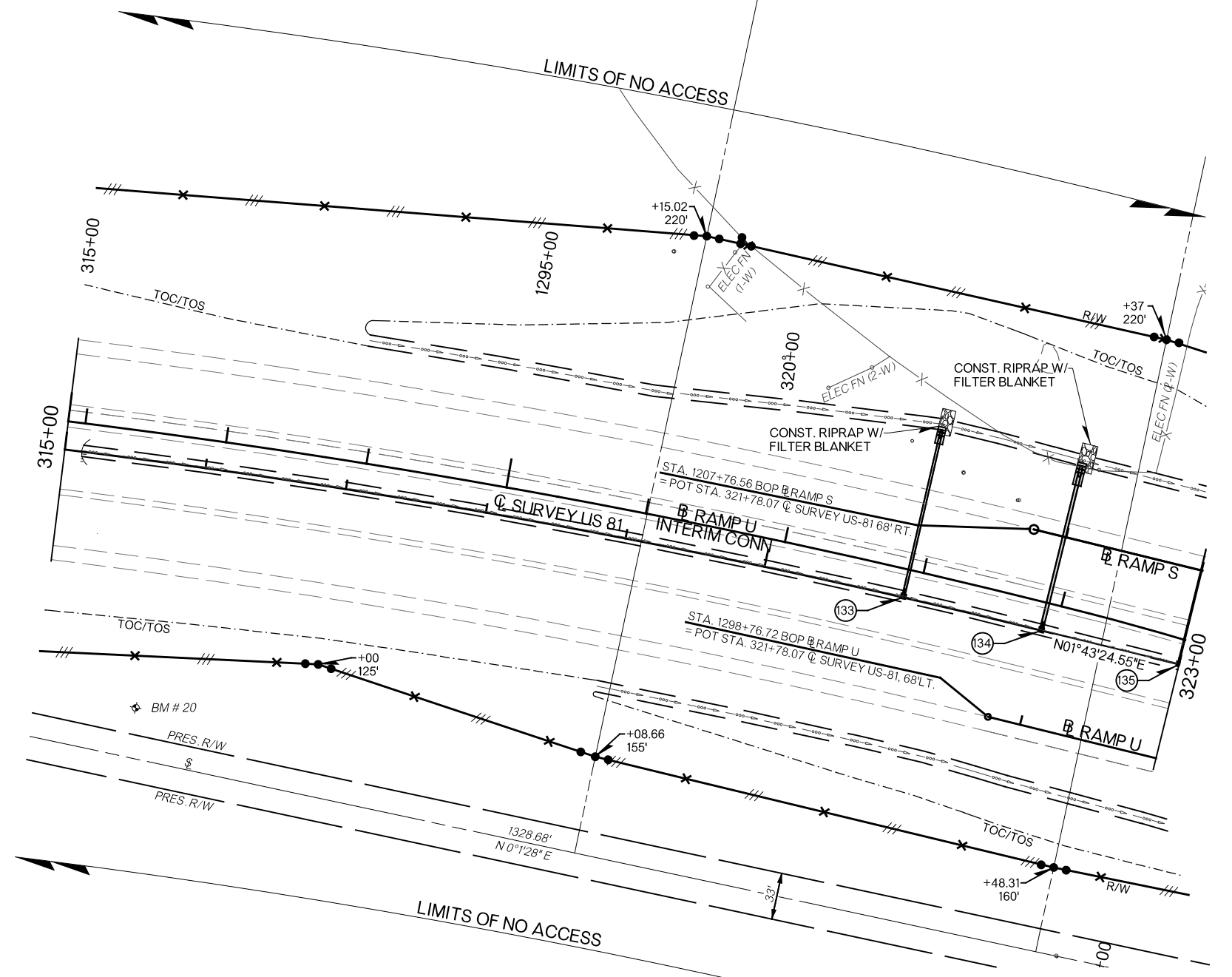
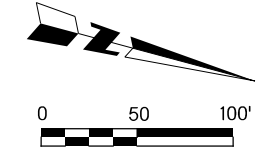
PLAN
US 81 REALIGNMENT
306+00 TO 315+00

US 81 REALIGNMENT
GRADY COUNTY



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROFILE
US 81 REALIGNMENT
306+00 TO 315+00



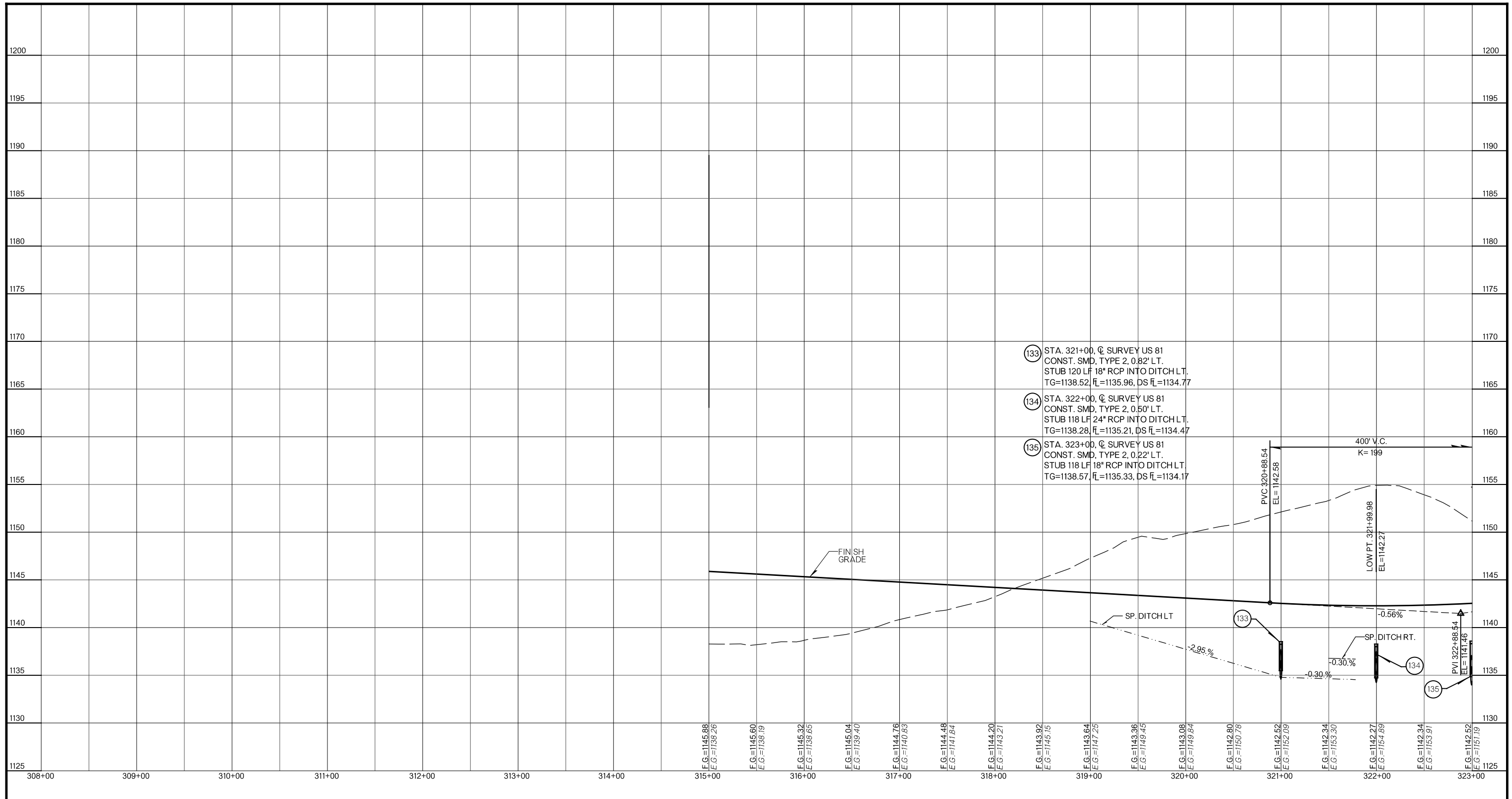
NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

BM # 20 - SET 3/4" I.P. (30" LONG) -
- 1' UNDERGROUND
Q SURVEY US 81
STA. 315+73.98, 173.94' RT.
ELEV. = 1135.8839

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PLAN
US 81 REALIGNMENT
315+00 TO 323+00
State Job No 24428(04) Sheet No R125



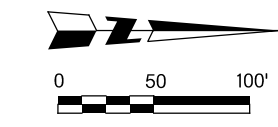
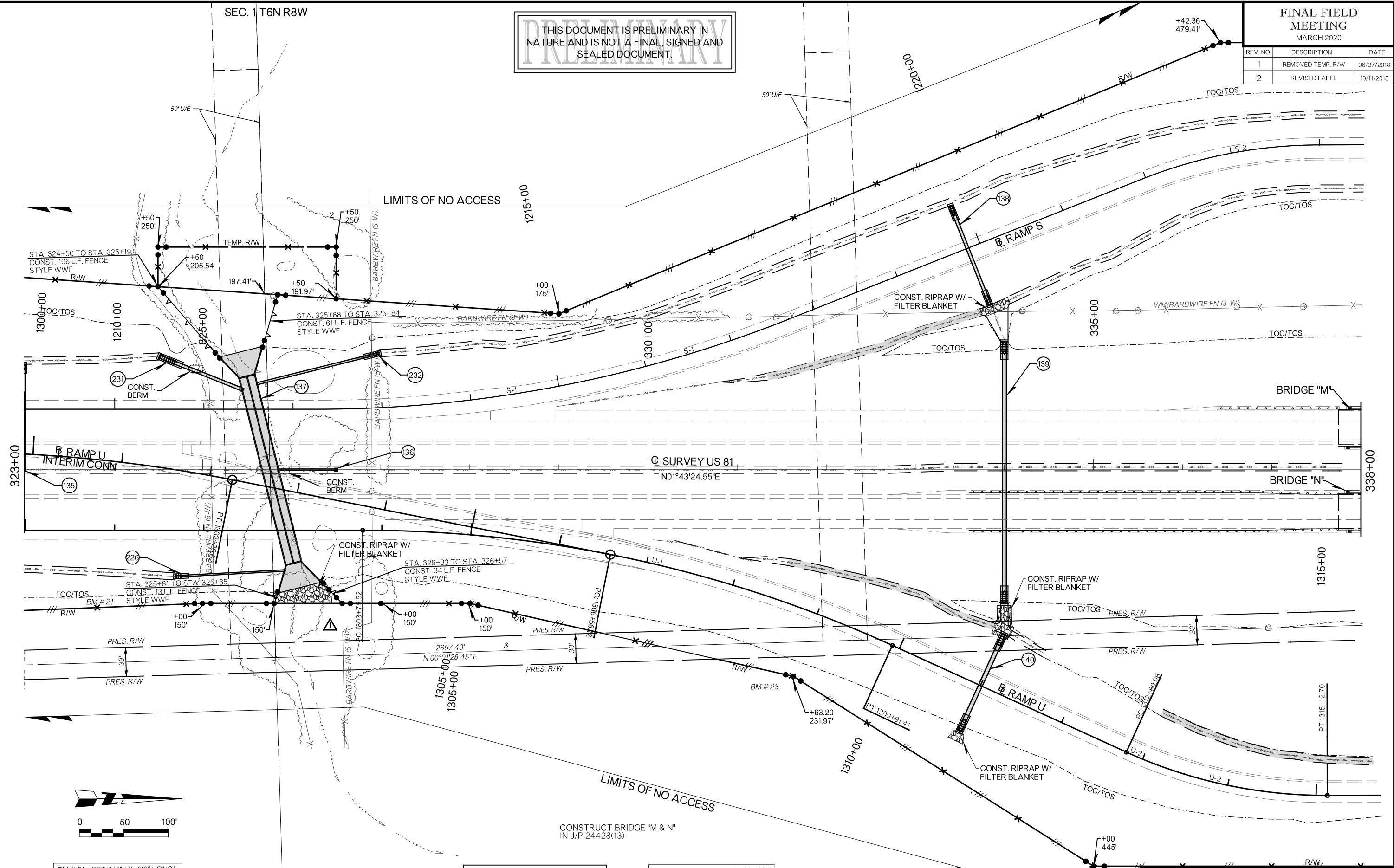
- 133 STA. 321+00, \odot SURVEY US 81
CONST. SMD, TYPE 2, 0.82' LT.
STUB 120 LF 18" RCP INTO DITCH LT.
TG=1138.52, \bar{r}_L =1135.96, DS \bar{r}_L =1134.77
- 134 STA. 322+00, \odot SURVEY US 81
CONST. SMD, TYPE 2, 0.50' LT.
STUB 118 LF 24" RCP INTO DITCH LT.
TG=1138.28, \bar{r}_L =1135.21, DS \bar{r}_L =1134.47
- 135 STA. 323+00, \odot SURVEY US 81
CONST. SMD, TYPE 2, 0.22' LT.
STUB 118 LF 18" RCP INTO DITCH LT.
TG=1138.57, \bar{r}_L =1135.33, DS \bar{r}_L =1134.17

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROFILE
US 81 REALIGNMENT
308+00 TO 323+00

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

| FINAL FIELD MEETING | | |
|---------------------|-------------------|------------|
| MARCH 2020 | | |
| REV. NO. | DESCRIPTION | DATE |
| 1 | REMOVED TEMP. R/W | 06/27/2018 |
| 2 | REVISED LABEL | 10/11/2018 |



BM # 21 - SET 3/4" I.P. (30" LONG) -
- 1' UNDERGROUND
@ SURVEY US 81
STA. 323+71.24, 162.63' RT.
ELEV. = 1143.2519

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE @ SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

BM # 23 - SET 3/4" I.P. (30" LONG)
@ SURVEY US 81
STA. 331+35.67, 232.88' RT.
ELEV. = 1132+09.86

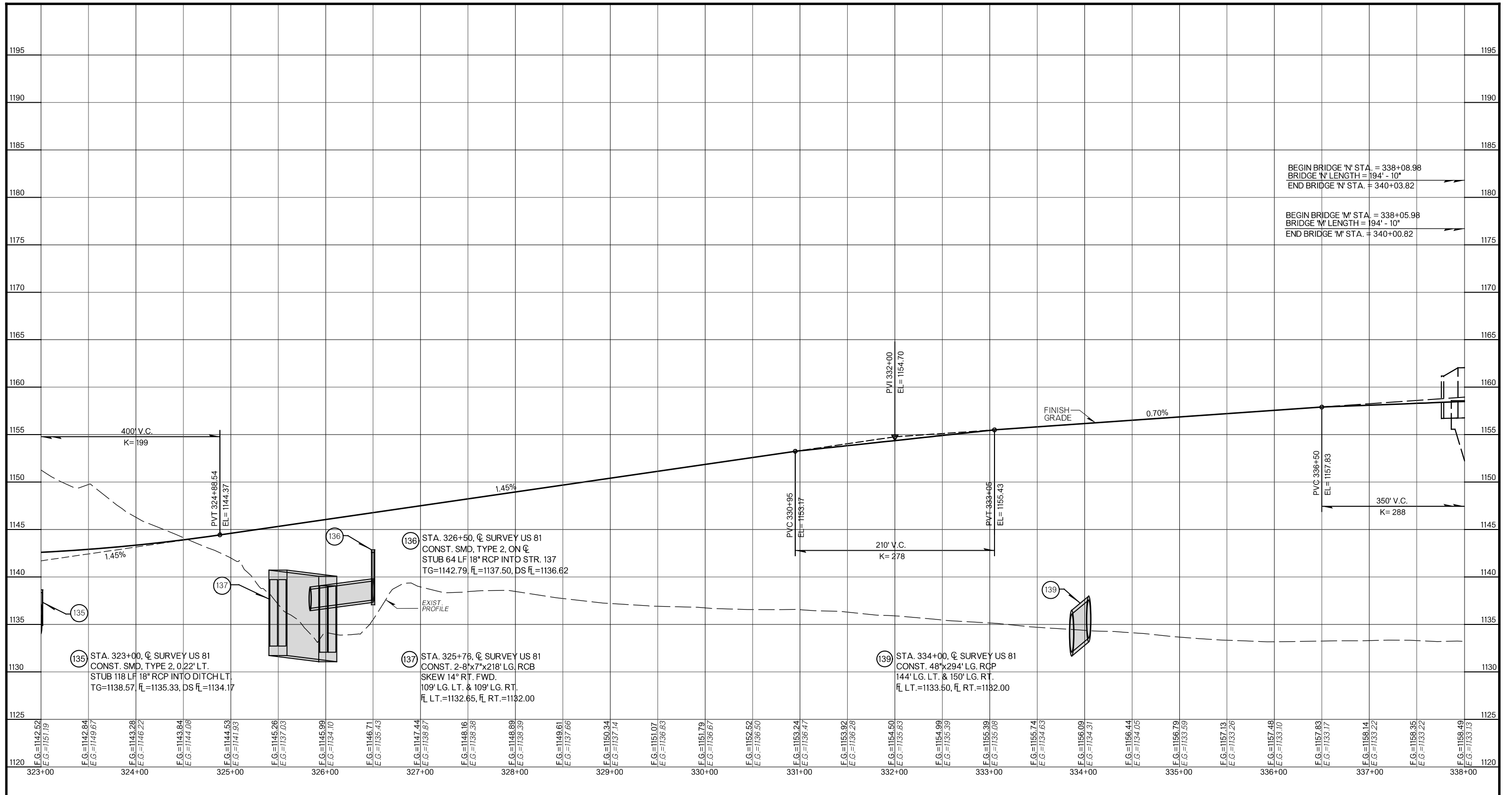
CONSTRUCT BRIDGE "M & N" IN J/P 24428(13)

PLAN
US 81 REALIGNMENT
323+00 TO 338+00

SEC. 6 T6N R7W

SEC. 1 T6N R8W

US 81 REALIGNMENT
GRADY COUNTY



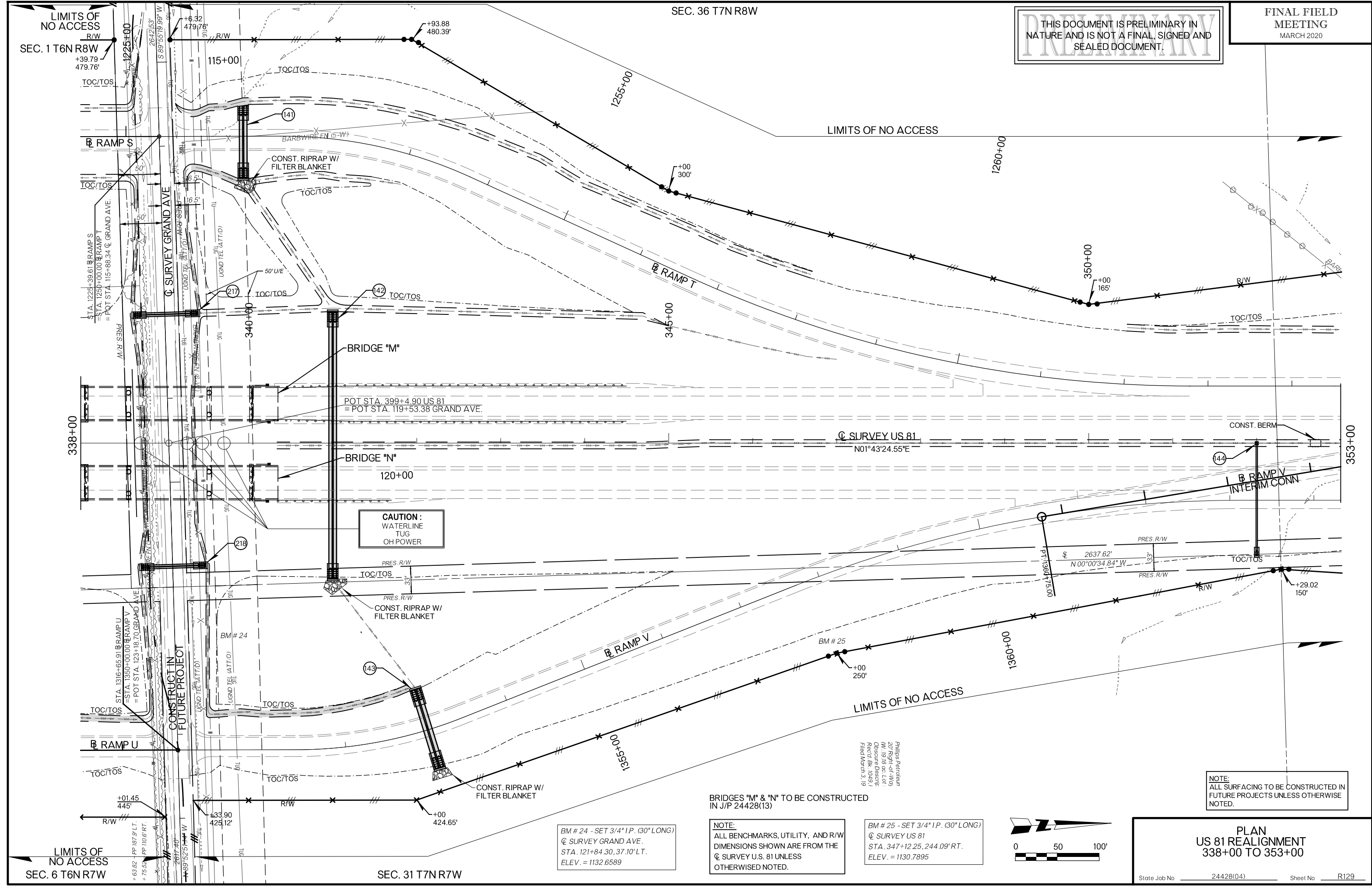
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL SIGNED AND SEALED DOCUMENT.

PROFILE
US 81 REALIGNMENT
323+00 TO 338+00

State Job No. 24428(04) Sheet No. R128

US 81 REALIGNMENT
GRADY COUNTY

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



CAUTION:
WATERLINE
TUG
OH POWER

BRIDGES "M" & "N" TO BE CONSTRUCTED IN J/P 24428(13)

BM # 24 - SET 3/4" I.P. (30" LONG)
Q SURVEY GRAND AVE.
STA. 121+84.30, 37.10' LT.
ELEV. = 1132.6589

NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

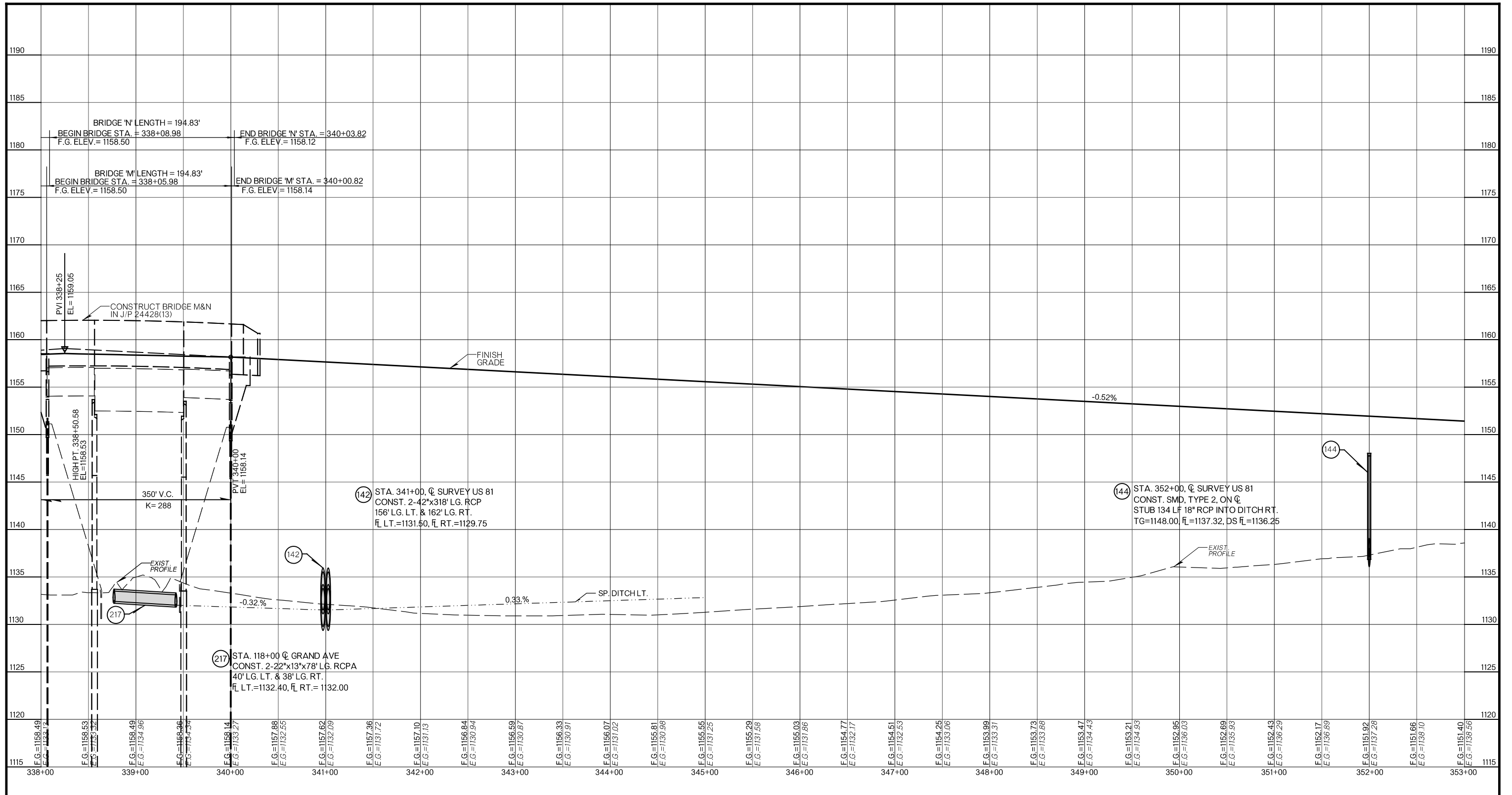
BM # 25 - SET 3/4" I.P. (30" LONG)
Q SURVEY US 81
STA. 347+12.25, 244.09' RT.
ELEV. = 1130.7895

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.



PLAN
US 81 REALIGNMENT
338+00 TO 353+00

US 81 REALIGNMENT GRADY COUNTY

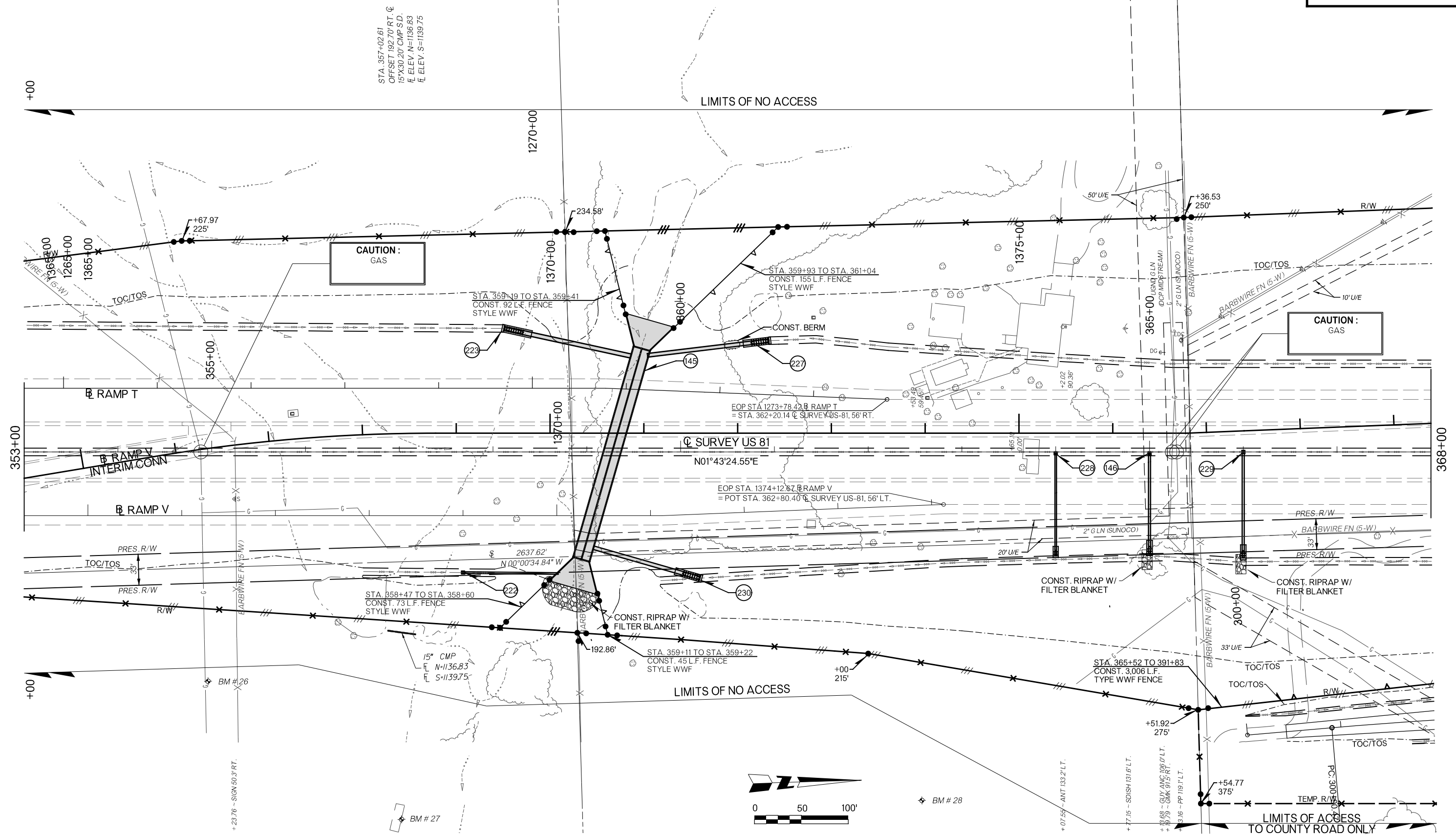


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROFILE
US 81 REALIGNMENT
338+00 TO 353+00

State Job No. 24428(04) Sheet No. R130

US 81 REALIGNMENT
GRADY COUNTY



BM # 26 - SET 3/4" I.P. (30" LONG)
 @ SURVEY US 81
 STA. 354+96.15, 244 55' RT.
 ELEV. = 1140.1674

NOTE:
 ALL SURFACING TO BE CONSTRUCTED IN
 FUTURE PROJECTS UNLESS OTHERWISE
 NOTED.

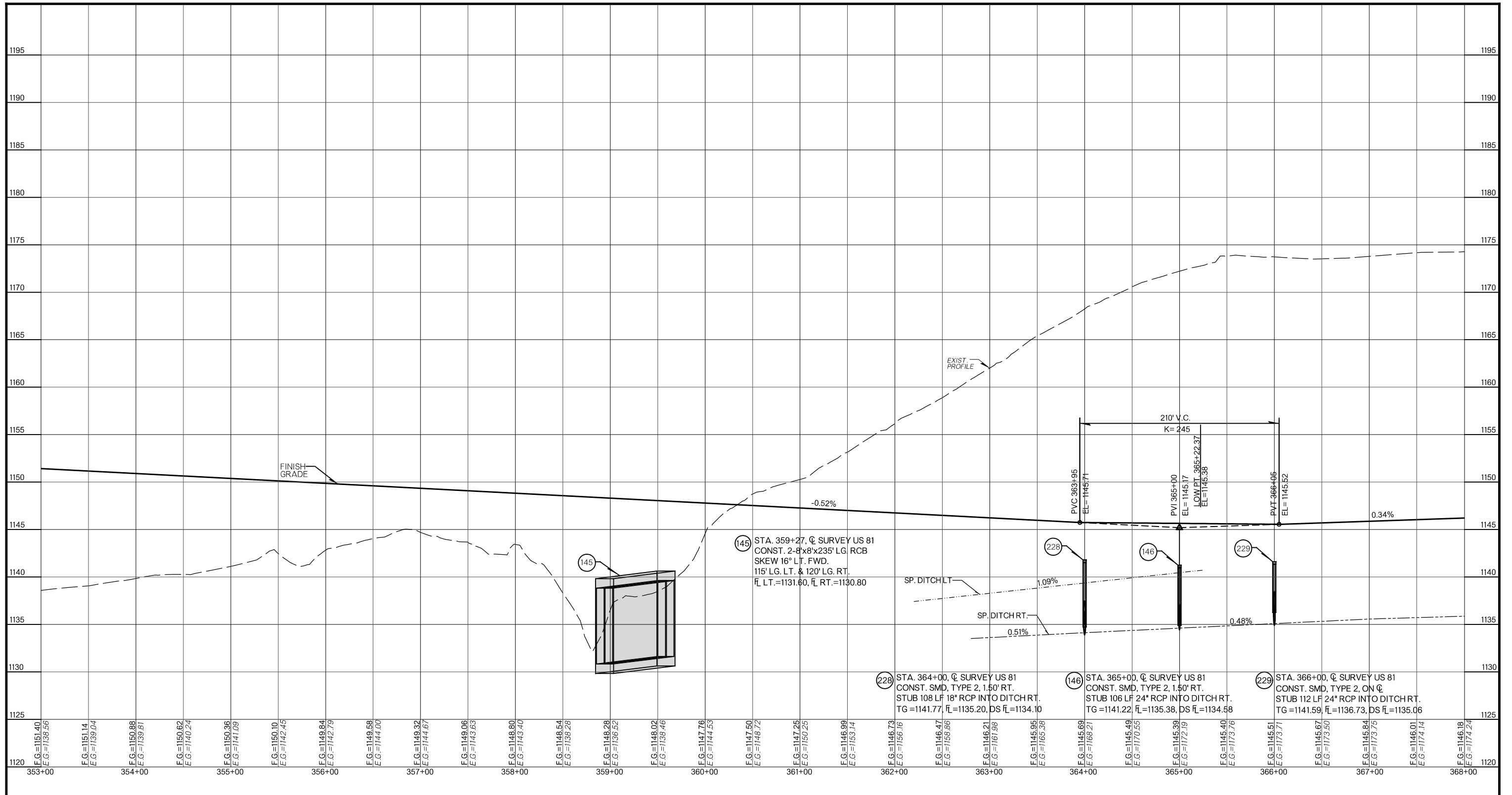
BM # 27 - CHIS. "X" SET ON N.E. CORNER
 OF HIGHEST CONC. SLAB, SOUTH OF
 LINE CREEK
 @ SURVEY US 81
 STA. 357+02.39, 391.80' RT.
 ELEV. = 1136.5030

NOTE:
 ALL BENCHMARKS, UTILITY, AND R/W
 DIMENSIONS SHOWN ARE FROM THE
 @ SURVEY U.S. 81 UNLESS
 OTHERWISE NOTED.

BM # 28 - SET 3/4" I.P. (30" LONG)
 @ SURVEY US 81
 STA. 362+57.30, 371.62' RT.
 ELEV. = 1147.9903

THIS DOCUMENT IS PRELIMINARY IN
 NATURE AND IS NOT A FINAL SIGNED AND
 SEALED DOCUMENT.

PLAN
 US 81 REALIGNMENT
 353+00 TO 368+00

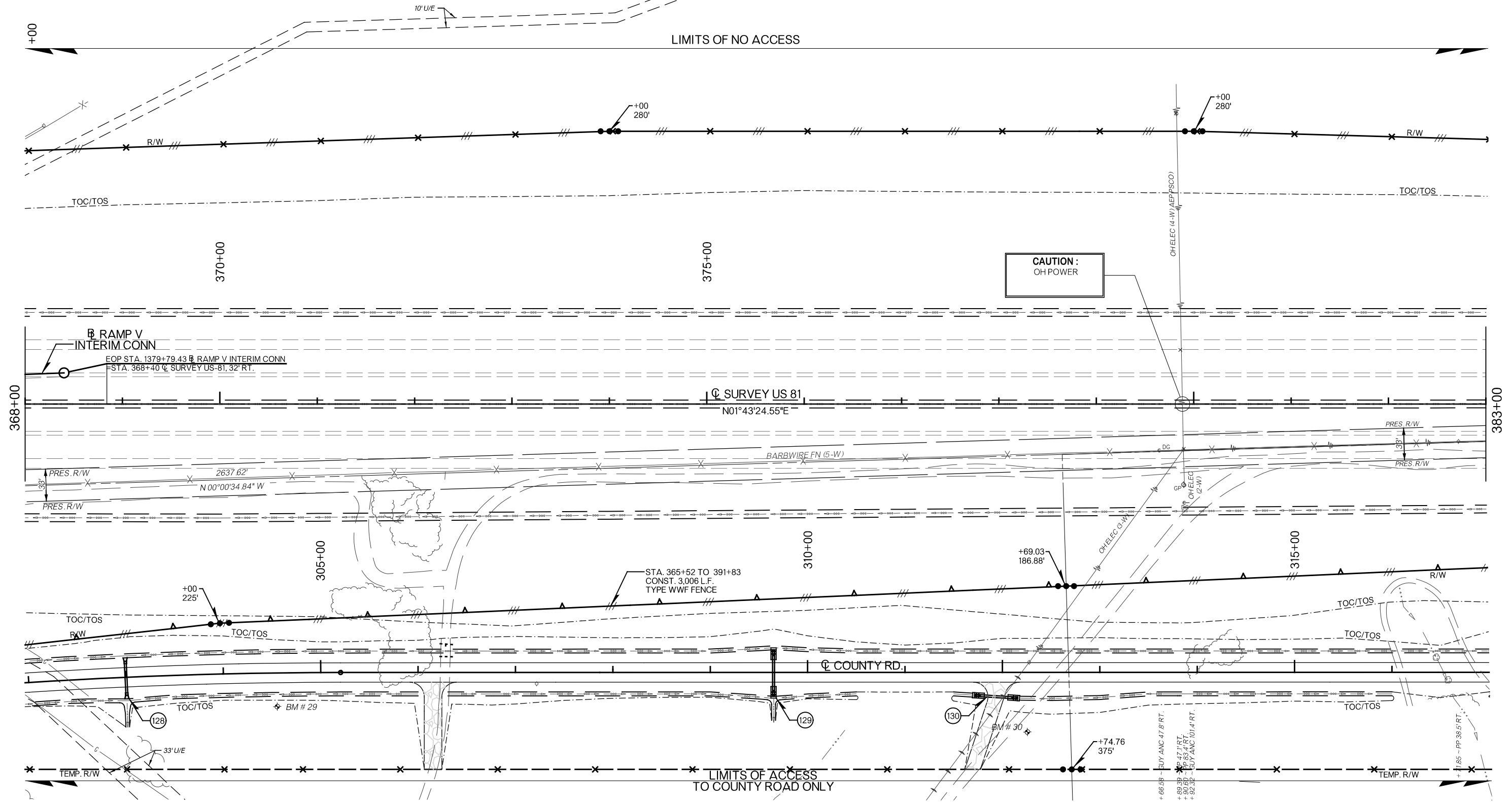


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROFILE US 81 REALIGNMENT 353+00 TO 368+00

State Job No. 24428(04) Sheet No. R132

US 81 REALIGNMENT
GRADY COUNTY



BM # 29 - SET 3/4" I.P. (30" LONG)
 @ SURVEY US 81
 STA. 370+58.95, 310.52' RT.
 ELEV. = 1165.0940

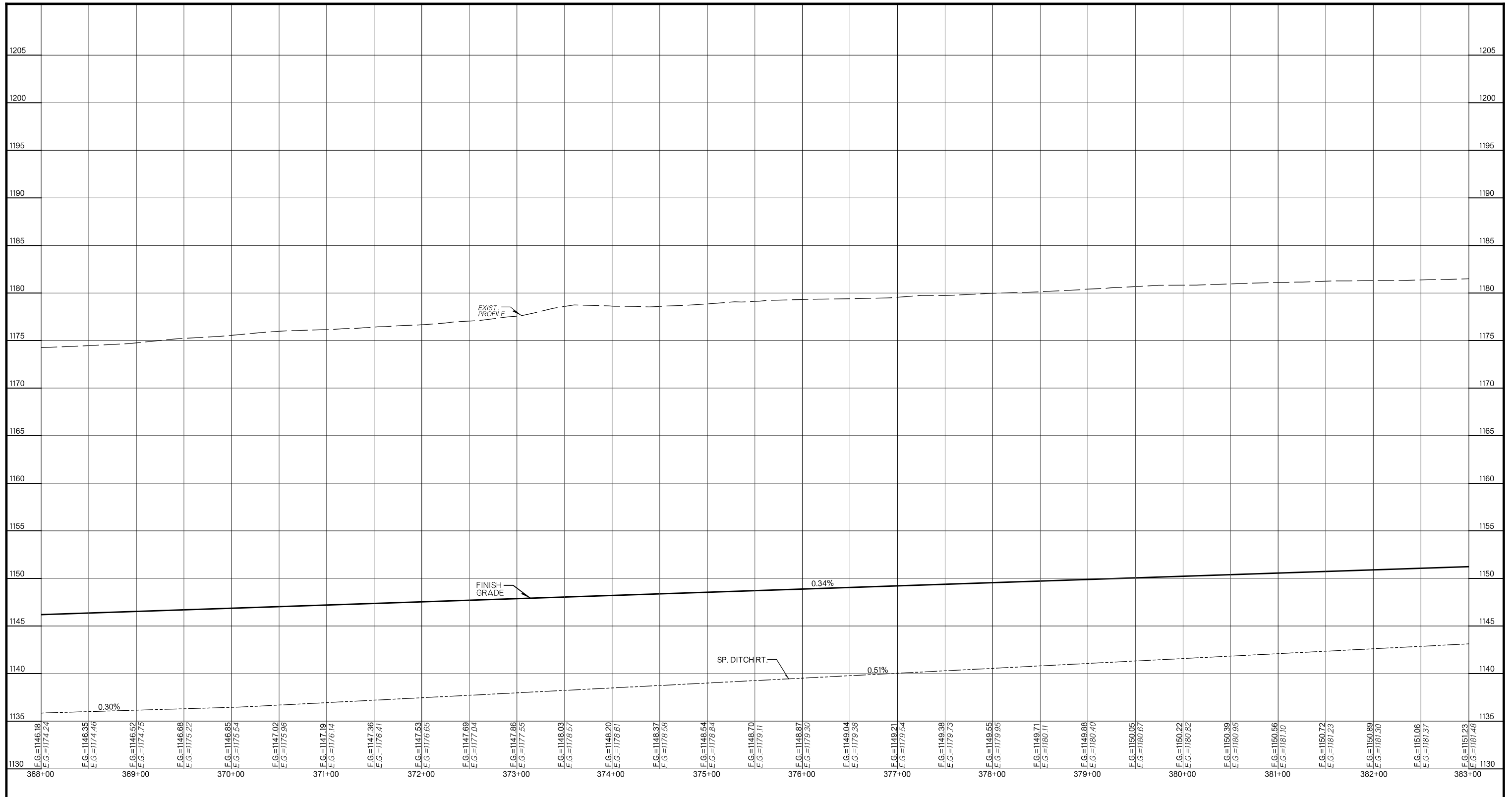
NOTE:
 ALL SURFACING TO BE CONSTRUCTED IN
 FUTURE PROJECTS UNLESS OTHERWISE
 NOTED.

NOTE:
 ALL BENCHMARKS, UTILITY, AND R/W
 DIMENSIONS SHOWN ARE FROM THE
 @ SURVEY U.S. 81 UNLESS
 OTHERWISE NOTED.

BM # 30 - SET 3/4" I.P. (30" LONG)
 @ SURVEY US 81
 STA. 378+29.20, 336.61 RT.
 ELEV. = 1174.6291

THIS DOCUMENT IS PRELIMINARY IN
 NATURE AND IS NOT A FINAL, SIGNED AND
 SEALED DOCUMENT.

PLAN
 US 81 REALIGNMENT
 368+00 TO 383+00

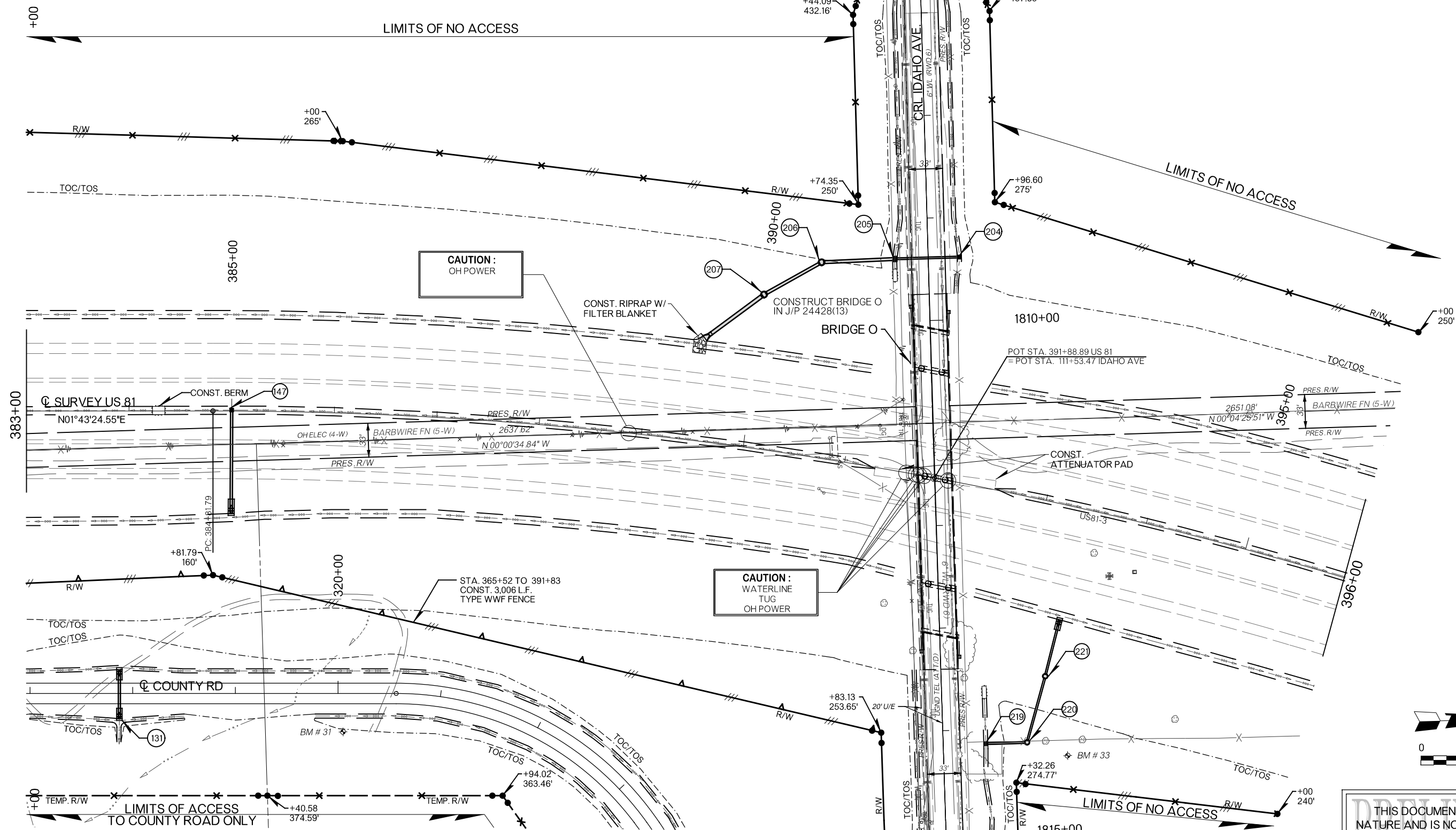


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROFILE
US 81 REALIGNMENT
368+00 TO 383+00

State Job No. 24428(04) Sheet No. R134

US 81 REALIGNMENT
GRADY COUNTY



CAUTION:
OH POWER

CAUTION:
WATERLINE
TUG
OH POWER

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

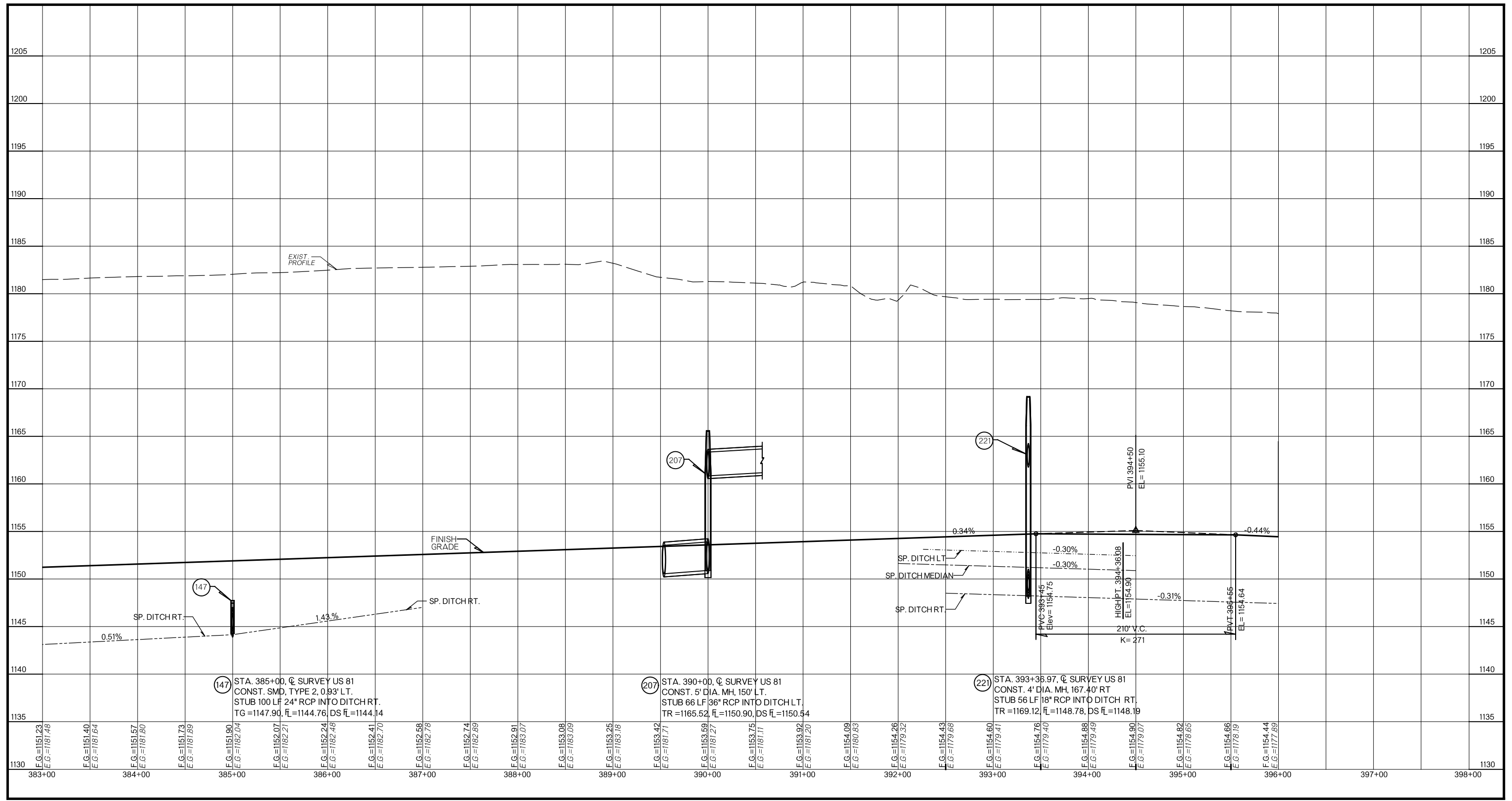
BM # 31 - SET 3/4" I.P. (30" LONG)
@ SURVEY US 81
STA. 386+19.74, 310.53' RT.
ELEV. = 1172.3735

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

SEC. 30 T7N R7W
BM # 33 - SET 3/4" I.P. (30" LONG)
@ SURVEY IDAHO AVE.
STA. 114+28.25, 120.83' LT.
ELEV. = 1177.0519

NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE @ SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

US 81 REALIGNMENT
GRADY COUNTY



147 STA. 385+00, Q SURVEY US 81
CONST. SMD, TYPE 2, 0.93' LT.
STUB 100 LF 24" RCP INTO DITCH RT.
TG = 1147.90, f_L = 1144.76, DS f_L = 1144.14

207 STA. 390+00, Q SURVEY US 81
CONST. 5' DIA. MH, 150' LT.
STUB 66 LF 36" RCP INTO DITCH LT.
TR = 1165.52, f_L = 1150.90, DS f_L = 1150.54

221 STA. 393+36.97, Q SURVEY US 81
CONST. 4' DIA. MH, 167.40' RT
STUB 56 LF 18" RCP INTO DITCH RT.
TR = 1169.12, f_L = 1148.78, DS f_L = 1148.19

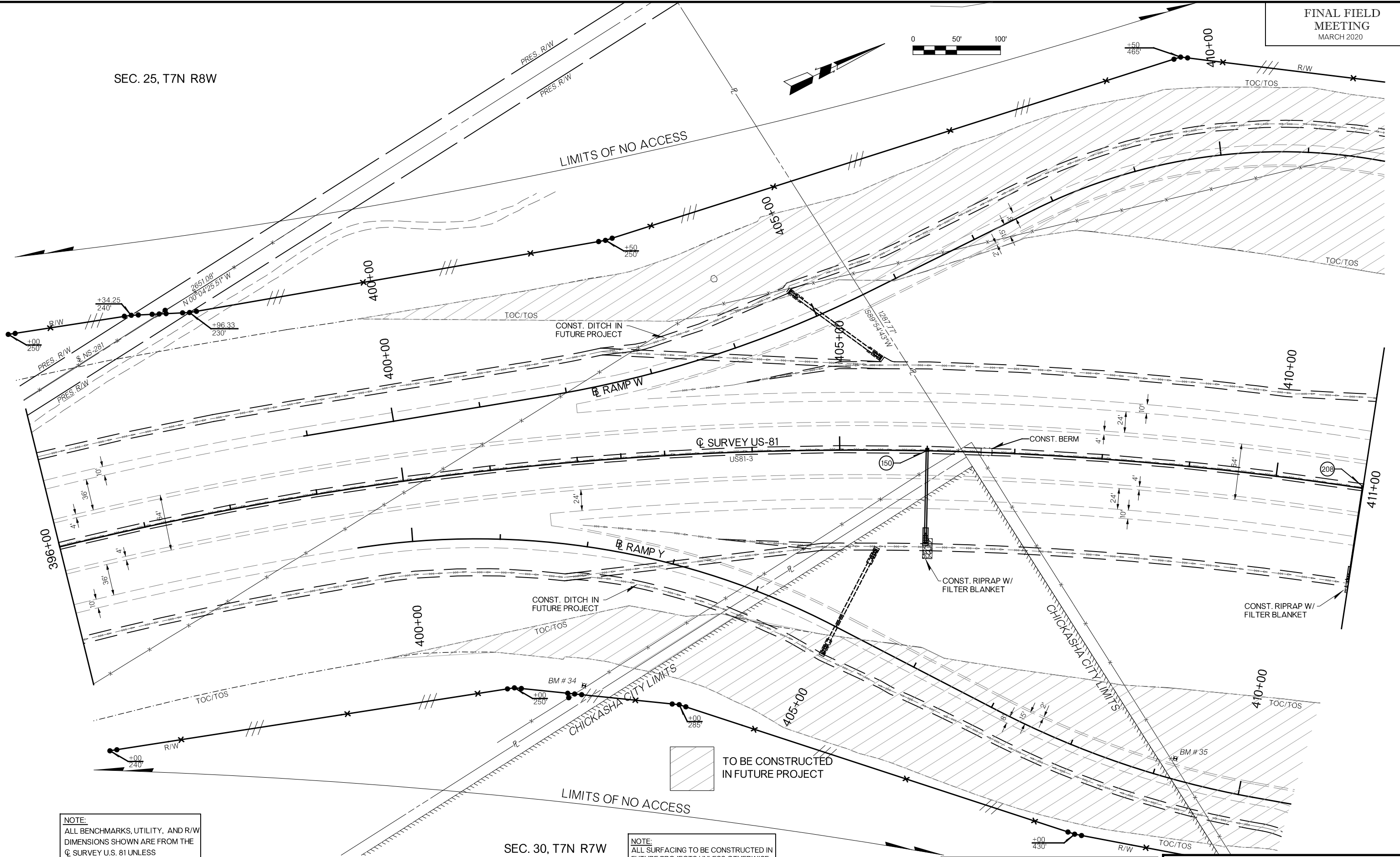
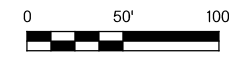
THIS DOCUMENT IS PRELIMINARY
IN NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.
PRELIMINARY

PROFILE
US 81 REALIGNMENT
383+00 TO 396+00
State Job No. 24428(04) Sheet No. R136

US 81 REALIGNMENT
GRADY COUNTY

SEC. 25, T7N R8W

SEC. 30, T7N R7W



NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE \odot SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

BM #32 1873.77' RT. OF \odot SURVEY US 81
CHIS. *X* SET, NW COR. PAD OF WATER VAULT
STA. 399+38.08 ELEV: 1176.66

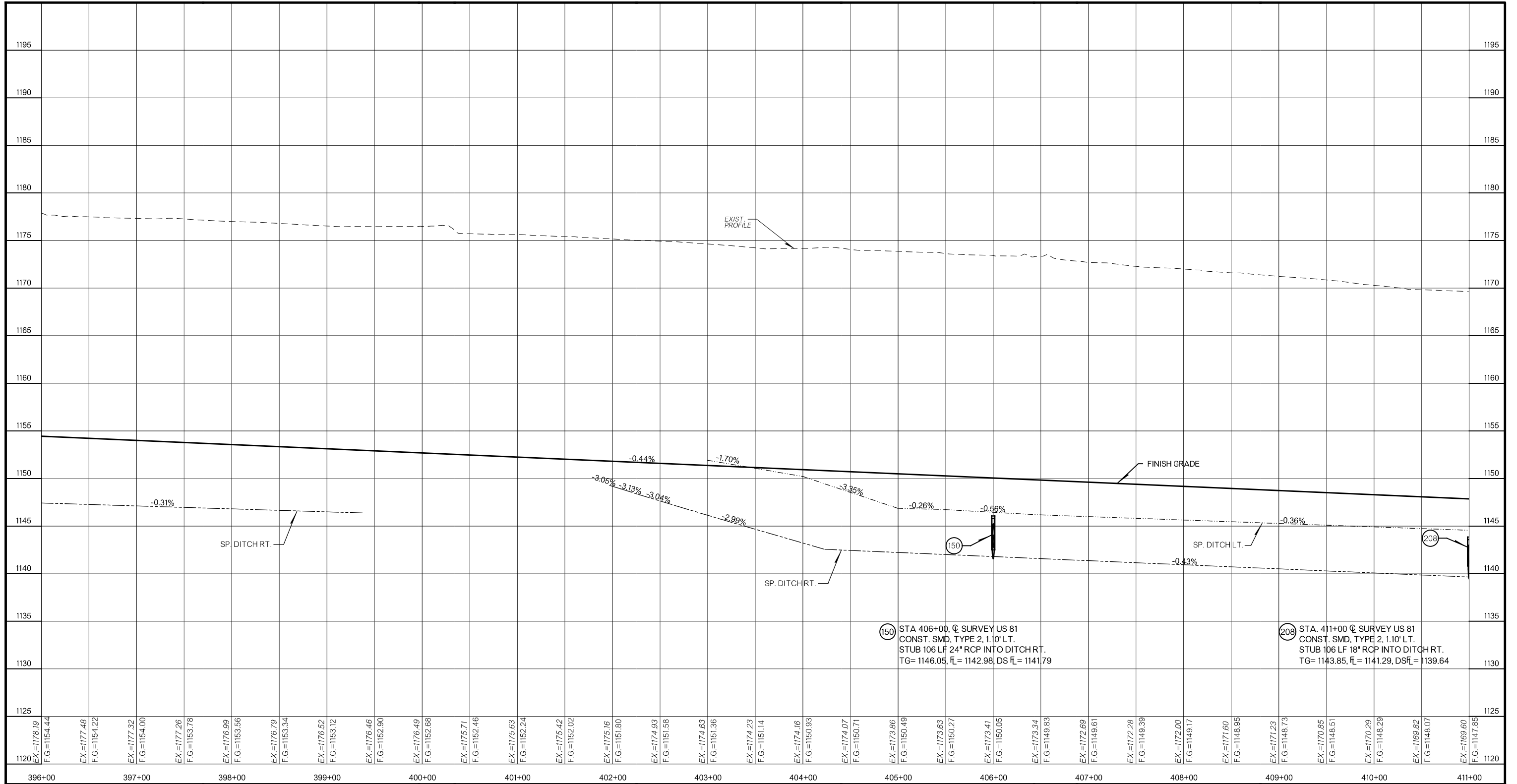
BM #34 255.39' RT. \odot SURVEY US 81
SET 3/4" I.P. (30" LONG)
STA. 401+84.95 ELEV: 1174.28

BM #232 2379.36' RT. OF \odot SURVEY US 81
80D SPIKE IN POWER POLE
STA. 406+08.72 ELEV: 1160.59

BM #35 337.74' RT. OF \odot SURVEY US 81
SET 3/4" I.P. (30" LONG)
STA. 409+18.77 ELEV: 1169.99

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PLAN
US-81 REALIGNMENT
396+00 TO 411+00

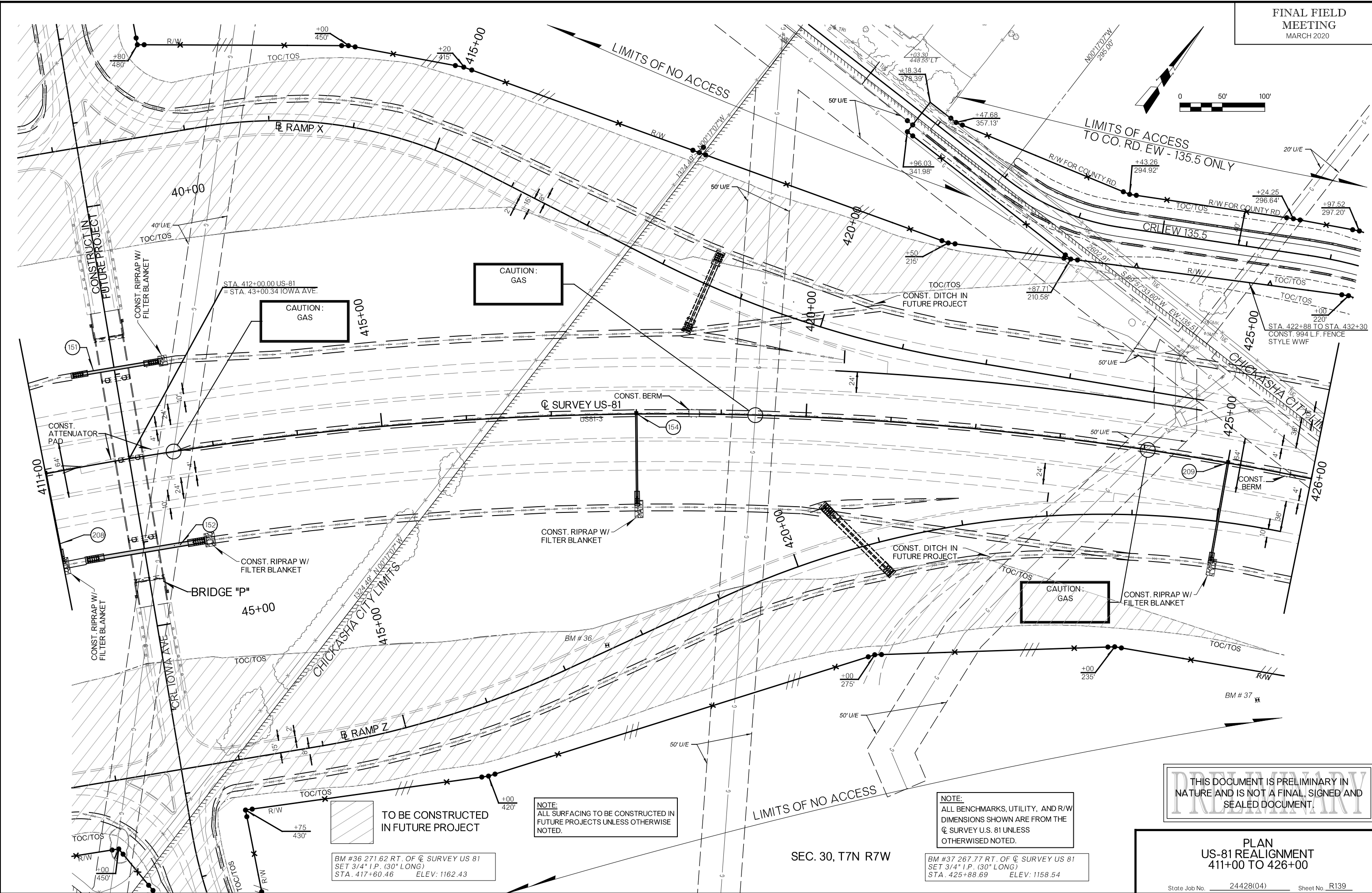
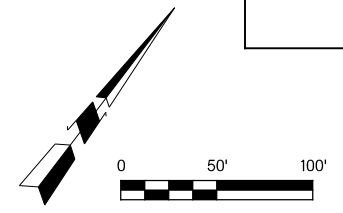


150 STA. 406+00, Q SURVEY US 81
CONST. SMD, TYPE 2, 1.10' LT.
STUB 106 LF 24" RCP INTO DITCH RT.
TG= 1146.05, \bar{r}_L = 1142.98, DS \bar{r}_L = 1141.79

208 STA. 411+00 Q SURVEY US 81
CONST. SMD, TYPE 2, 1.10' LT.
STUB 106 LF 18" RCP INTO DITCH RT.
TG= 1143.85, \bar{r}_L = 1141.29, DS \bar{r}_L = 1139.64

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROFILE
US-81 REALIGNMENT
396+00 TO 411+00



CAUTION:
GAS

CAUTION:
GAS

CAUTION:
GAS

TO BE CONSTRUCTED
IN FUTURE PROJECT

BM #36 271.62 RT. OF @ SURVEY US 81
SET 3/4" I.P. (30" LONG)
STA. 417+60.46 ELEV: 1162.43

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, AND R/W
DIMENSIONS SHOWN ARE FROM THE
@ SURVEY U.S. 81 UNLESS
OTHERWISE NOTED.

THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

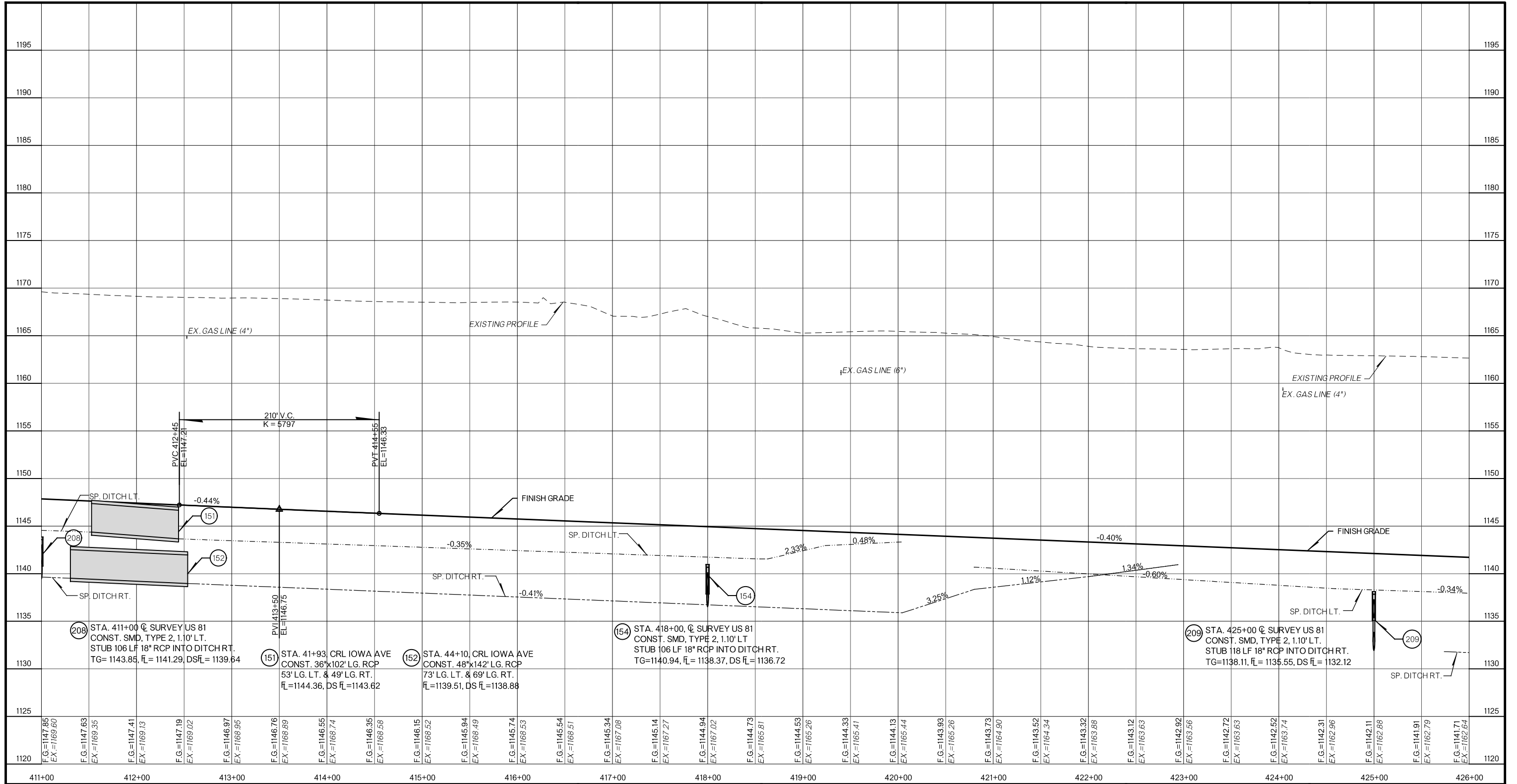
PLAN
US-81 REALIGNMENT
411+00 TO 426+00

State Job No. 24428(04) Sheet No. R139

SEC. 30, T7N R7W

BM #37 267.77 RT. OF @ SURVEY US 81
SET 3/4" I.P. (30" LONG)
STA. 425+88.69 ELEV: 1158.54

US 81 REALIGNMENT
GRADY COUNTY

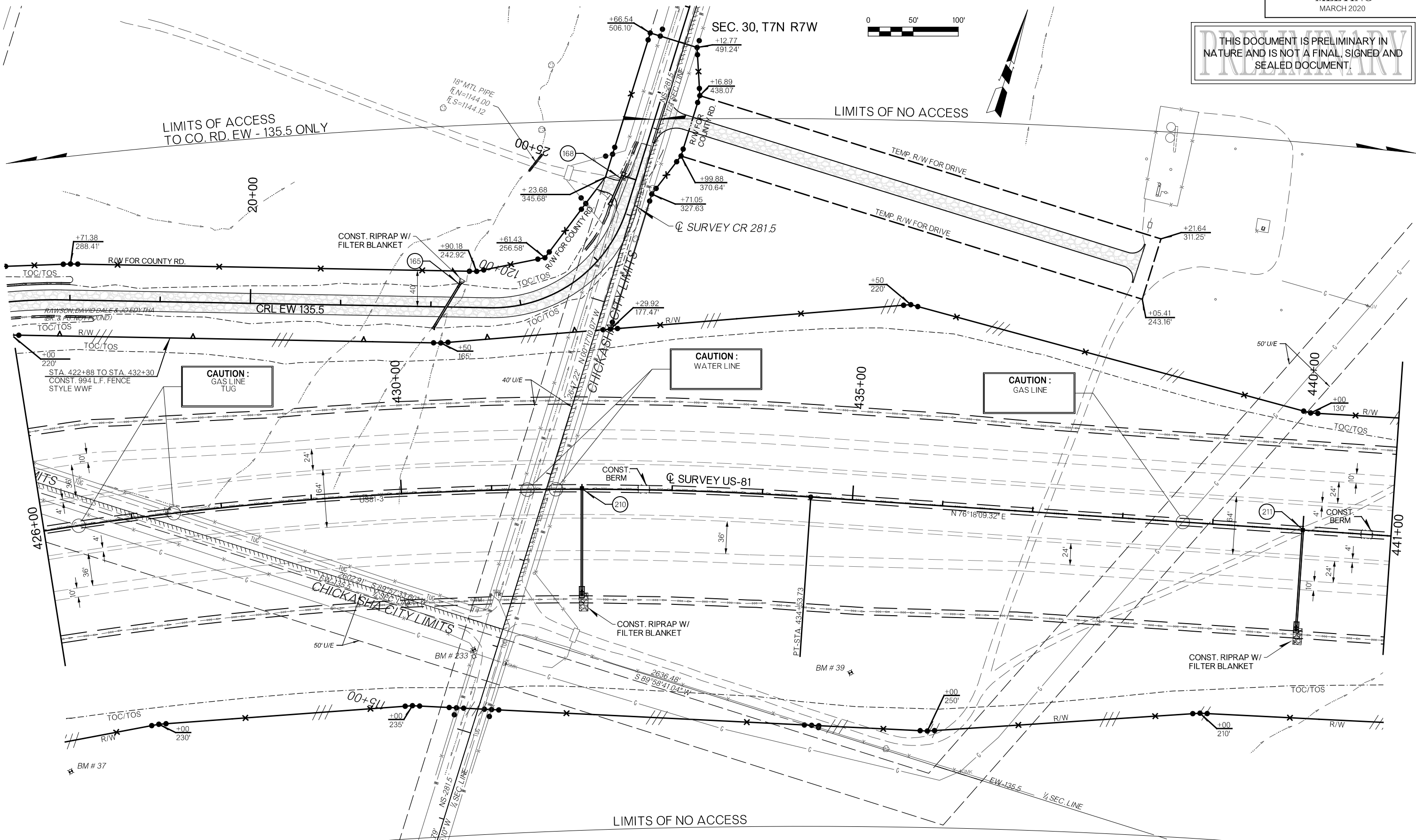


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROFILE US-81 REALIGNMENT 411+00 TO 426+00

GRADY COUNTY US-81 REALIGNMENT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



BM # 37 - SET 3/4" I.P. (30" LONG)
Q SURVEY US 81
STA. 425+88.69, 267.77' RT.
ELEV. = 1158.5366

BM # 233 - 60D SPIKE IN FENCE CORNER
POST, S.W. QUADRANT OF INTERSECTION
Q SURVEY NS 281.5
STA. 115+89.10, 23.91' LT.
ELEV. = 1160.7790

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

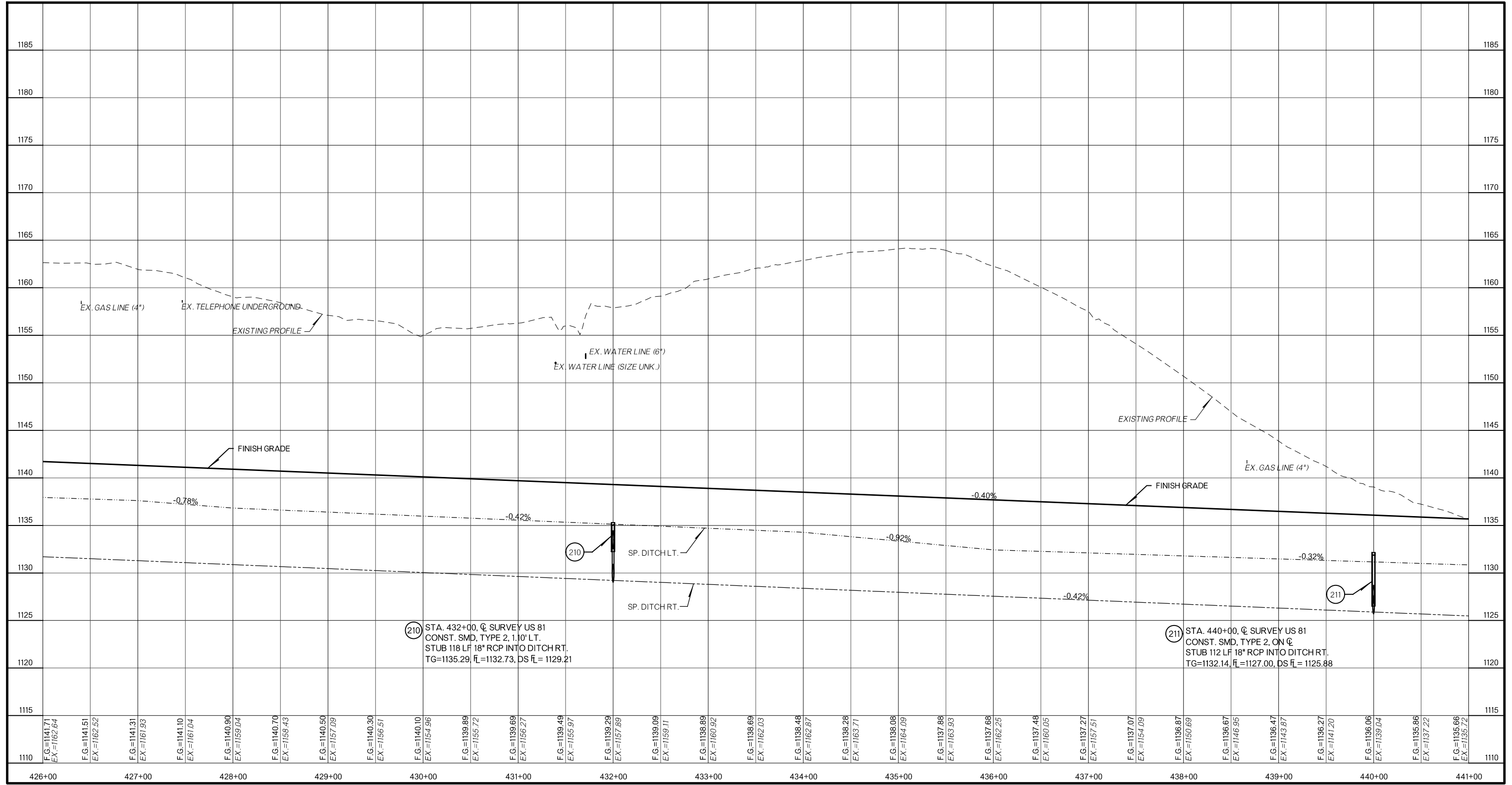
BM # 39 - SET 3/4" I.P. (30" LONG)
Q SURVEY US 81
STA. 435+13.73, 191.30' RT.
ELEV. = 1162.2182

NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

**PLAN
US-81 REALIGNMENT
426+00 TO 441+00**

State Job No. 24428(04) Sheet No. R141

GRADY COUNTY
US 81 REALIGNMENT



(210) STA. 432+00, Q SURVEY US 81
CONST. SMD, TYPE 2, 1.10' LT.
STUB 118 LF 18" RCP INTO DITCH RT.
TG=1135.29, FL=1132.73, DS FL= 1129.21

(211) STA. 440+00, Q SURVEY US 81
CONST. SMD, TYPE 2, ON Q
STUB 112 LF 18" RCP INTO DITCH RT.
TG=1132.14, FL=1127.00, DS FL= 1125.88

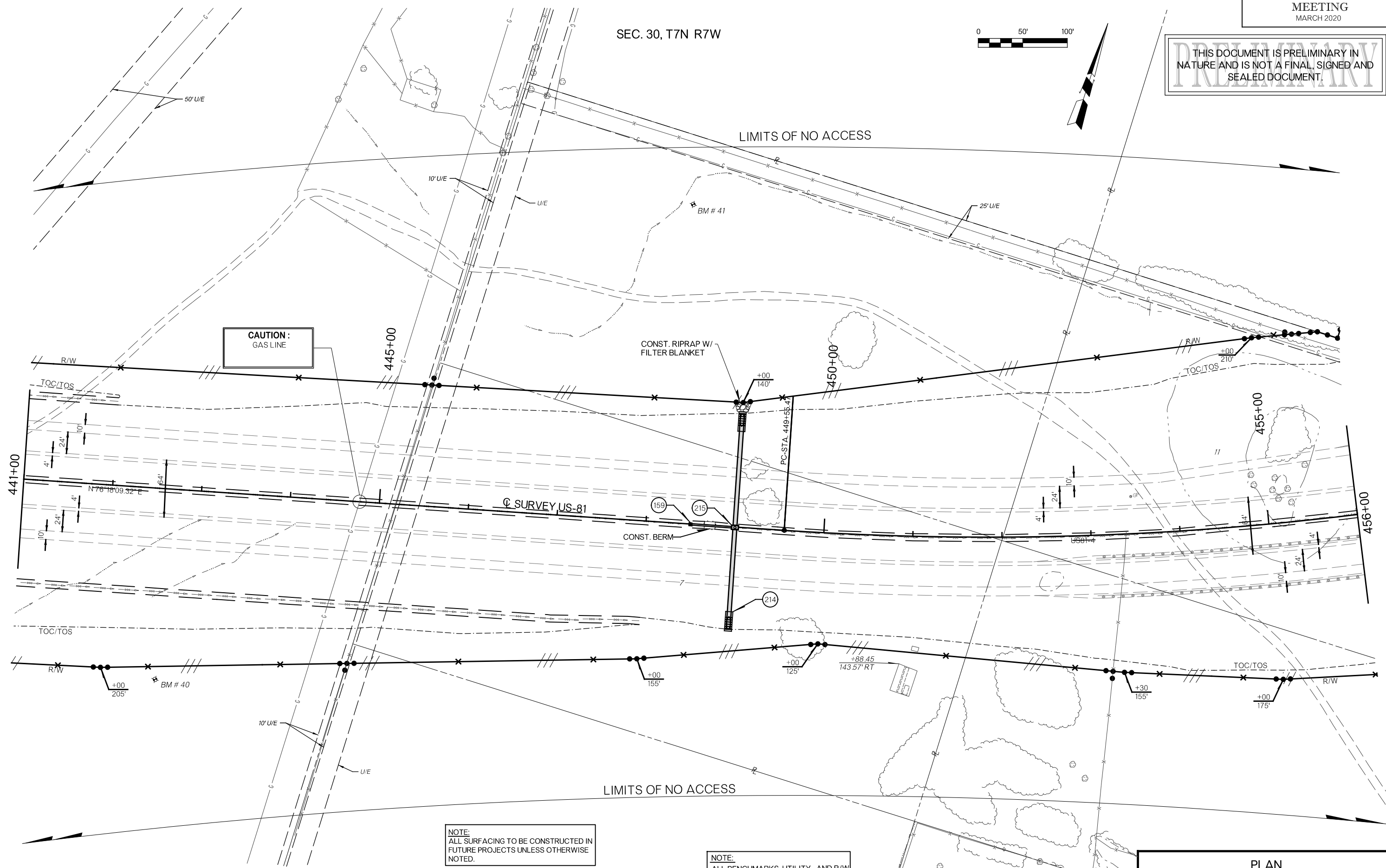
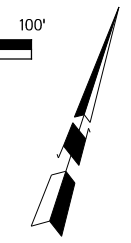
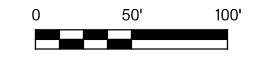
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROFILE US-81 REALIGNMENT 426+00 TO 441+00

US-81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEC. 30, T7N R7W



BM #40 214.40 RT. OF \odot SURVEY US-81
SET 3/4" I.P. (30" LONG)
STA. 442+62.04 ELEV: 1139.76

BM #234 480.86 RT. OF \odot SURVEY US-81
80D SPIKE IN S. BRACE OF FENCE COR. POST
STA. 444+05.54 ELEV: 1142.45

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

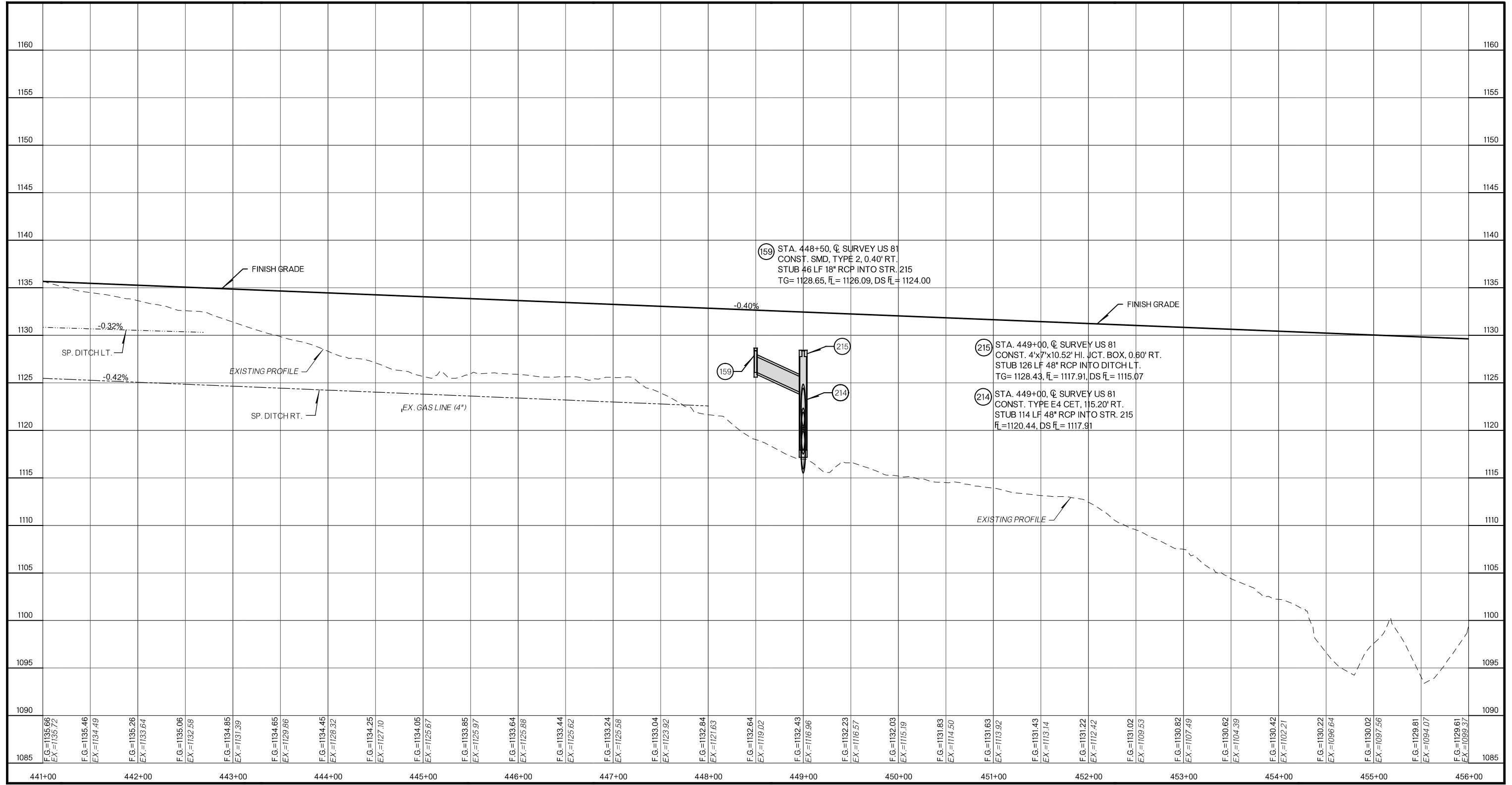
BM #41 358.03 LT. OF \odot SURVEY US-81
SET 3/4" I.P. (30" LONG)
STA. 448+29.10 ELEV: 1108.41

NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE \odot SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

BM #235 732.35 RT. OF \odot SURVEY US-81
80D SPIKE IN POWER POLE
STA. 455+62.40 ELEV: 1105.83

PLAN
US-81 REALIGNMENT
441+00 TO 456+00

US 81 REALIGNMENT GRADY COUNTY



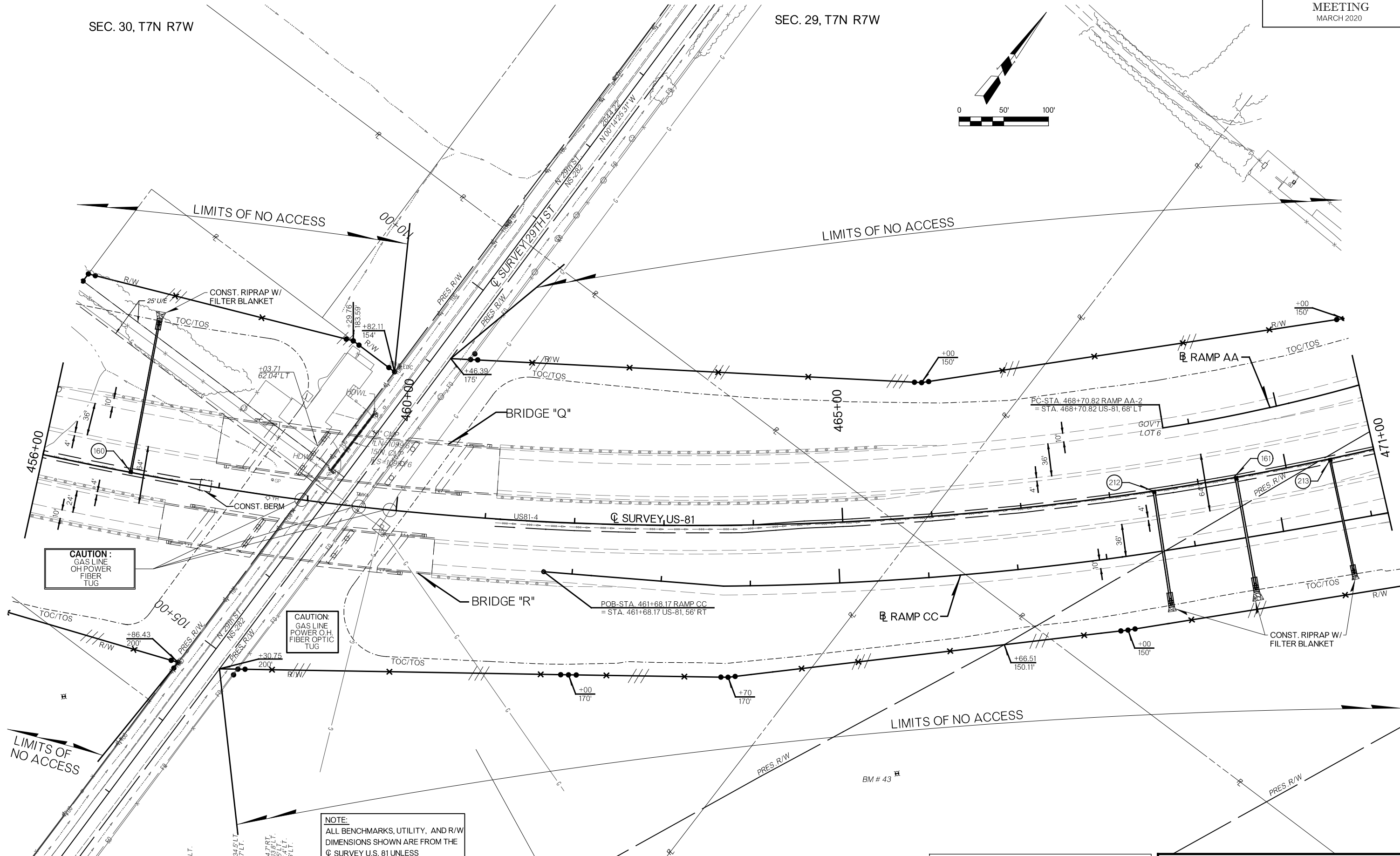
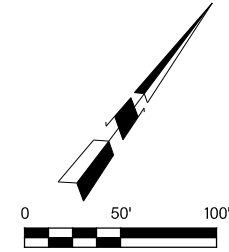
| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|------|
| 1085 | F.G.=1135.66 EX.=1136.72 | F.G.=1135.46 EX.=1134.49 | F.G.=1135.26 EX.=1133.64 | F.G.=1135.06 EX.=1132.58 | F.G.=1134.85 EX.=1131.39 | F.G.=1134.65 EX.=1129.86 | F.G.=1134.45 EX.=1128.32 | F.G.=1134.25 EX.=1127.10 | F.G.=1134.05 EX.=1125.67 | F.G.=1133.85 EX.=1125.97 | F.G.=1133.64 EX.=1125.88 | F.G.=1133.44 EX.=1125.62 | F.G.=1133.24 EX.=1125.58 | F.G.=1133.04 EX.=1123.92 | F.G.=1132.84 EX.=1121.63 | F.G.=1132.64 EX.=1119.02 | F.G.=1132.43 EX.=1116.96 | F.G.=1132.23 EX.=1116.57 | F.G.=1132.03 EX.=1115.19 | F.G.=1131.83 EX.=1114.50 | F.G.=1131.63 EX.=1113.92 | F.G.=1131.43 EX.=1113.14 | F.G.=1131.22 EX.=1112.42 | F.G.=1131.02 EX.=1109.53 | F.G.=1130.82 EX.=1107.49 | F.G.=1130.62 EX.=1104.39 | F.G.=1130.42 EX.=1102.21 | F.G.=1130.22 EX.=1096.64 | F.G.=1130.02 EX.=1097.56 | F.G.=1129.81 EX.=1094.07 | F.G.=1129.61 EX.=1099.37 | 1085 |
| 441+00 | 442+00 | 443+00 | 444+00 | 445+00 | 446+00 | 447+00 | 448+00 | 449+00 | 450+00 | 451+00 | 452+00 | 453+00 | 454+00 | 455+00 | 456+00 | | | | | | | | | | | | | | | | | |

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROFILE
US-81 REALIGNMENT
441+00 TO 456+00

SEC. 30, T7N R7W

SEC. 29, T7N R7W



CAUTION:
GAS LINE
OH POWER
FIBER
TUG

CAUTION:
GAS LINE
POWER O.H.
FIBER OPTIC
TUG

NOTE:
ALL BENCHMARKS, UTILITY, AND R/W
DIMENSIONS SHOWN ARE FROM THE
Q SURVEY U.S. 81 UNLESS
OTHERWISE NOTED.

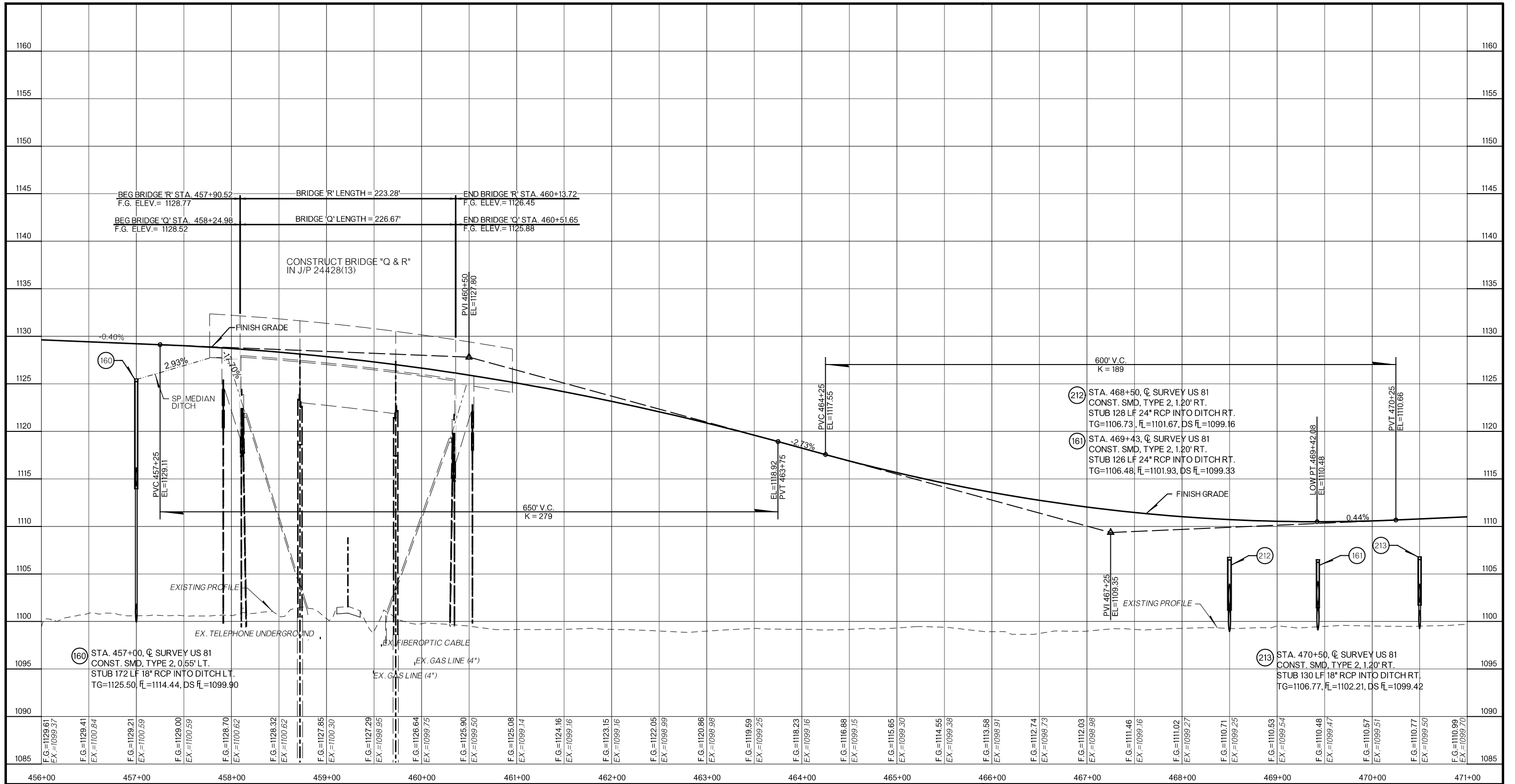
NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

BM #43 284.38 RT. OF Q SURVEY US-81
SET 3/4" I.P. (30" LONG)
STA. 465+43.47 ELEV: 1098.98

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

PLAN
US-81 REALIGNMENT
456+00 TO 471+00

US 81 REALIGNMENT
GRADY COUNTY



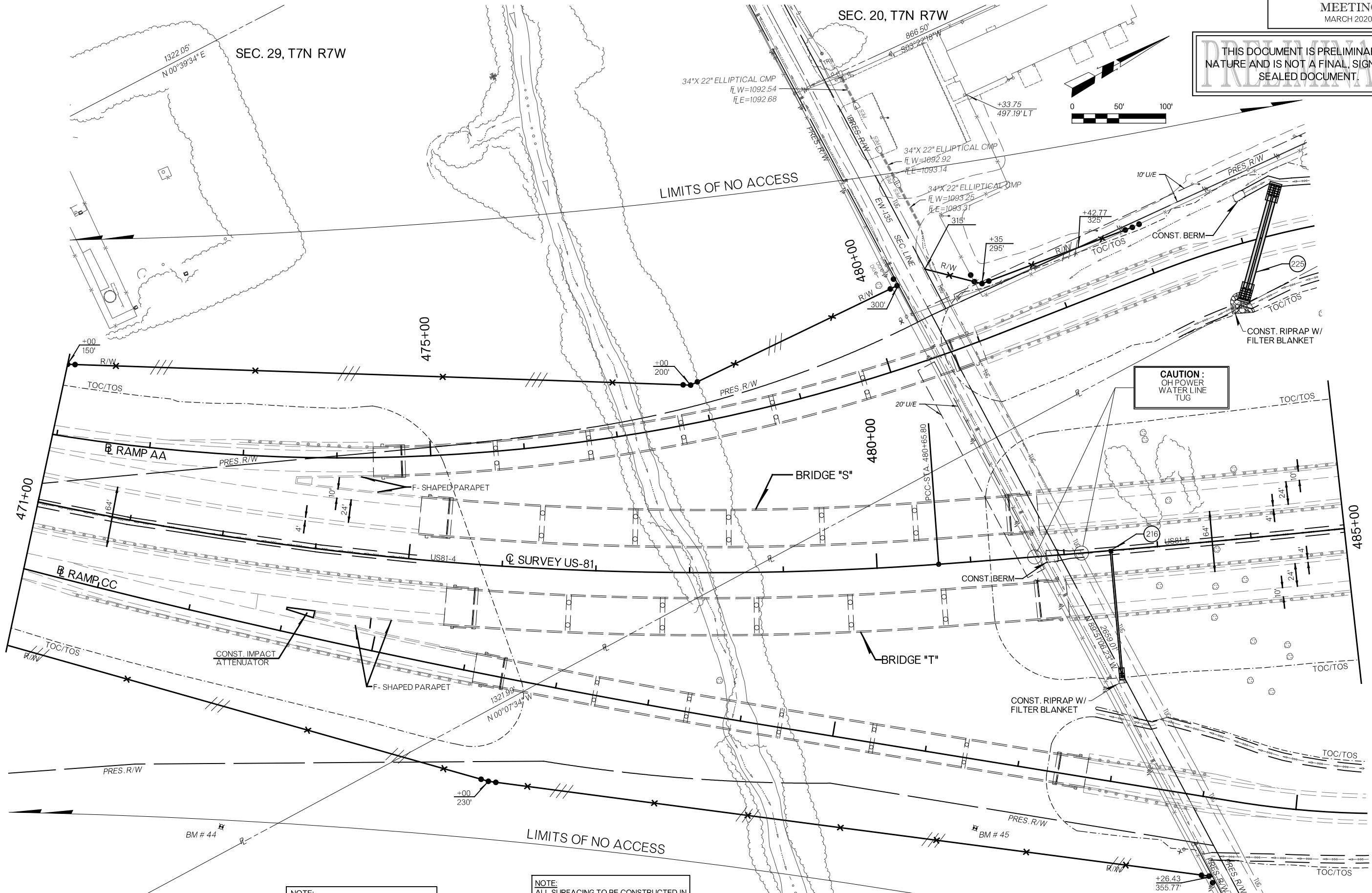
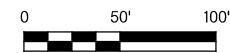
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROFILE
US-81 REALIGNMENT
456+00 TO 471+00

US-81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PRELIMINARY



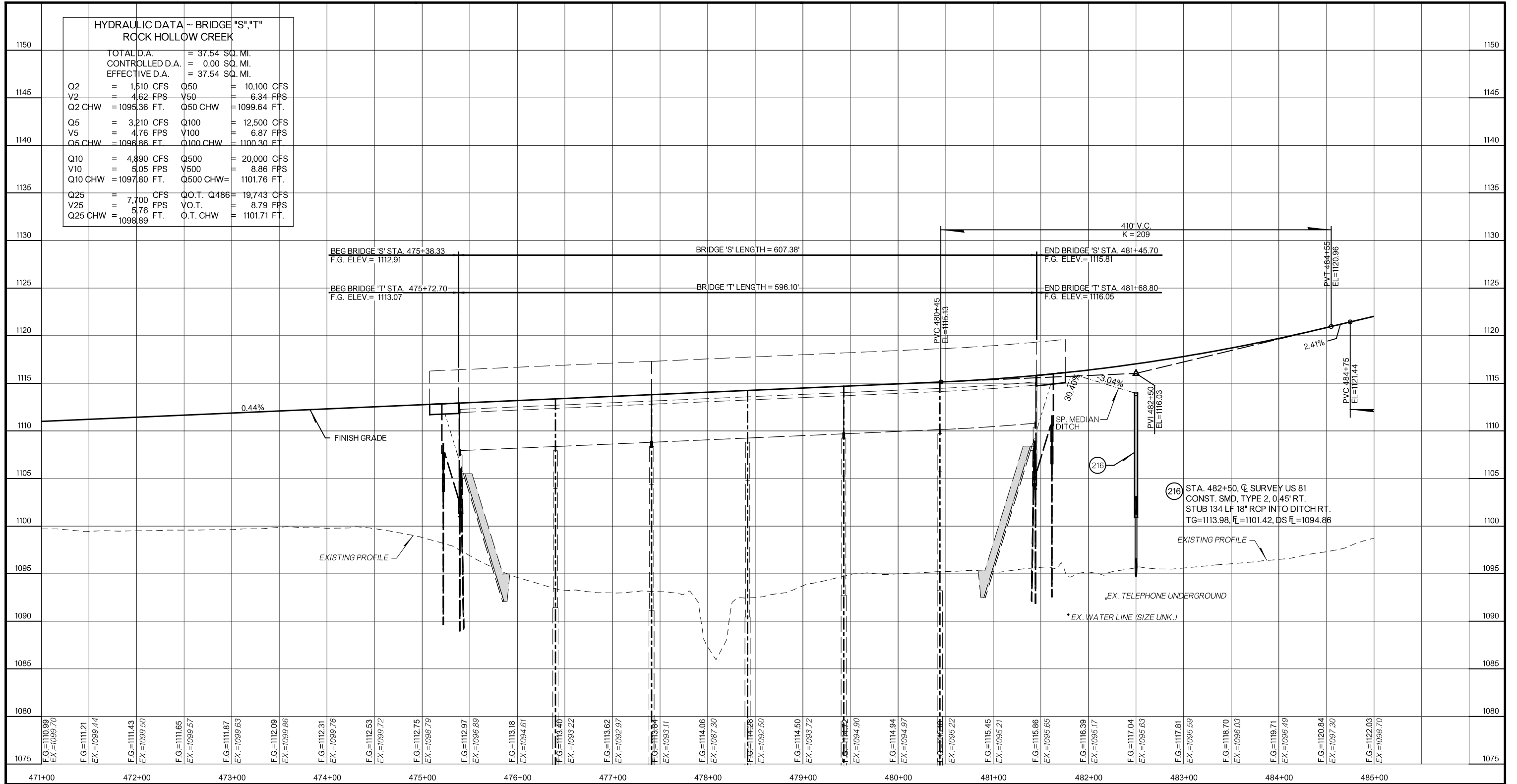
BM #44 307.35 RT. OF \odot SURVEY US-81
SET 3/4" I.P. (30" LONG)
STA. 473+41.96 ELEV: 1098.80

NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE \odot SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

BM #45 283.55 RT. OF \odot SURVEY US-81
SET 3/4" I.P. (30" LONG) - 1' UNDERGROUND
STA. 480+83.78 ELEV: 1093.72

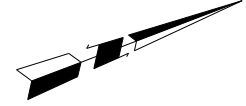
PLAN
US-81 REALIGNMENT
471+00 TO 485+00



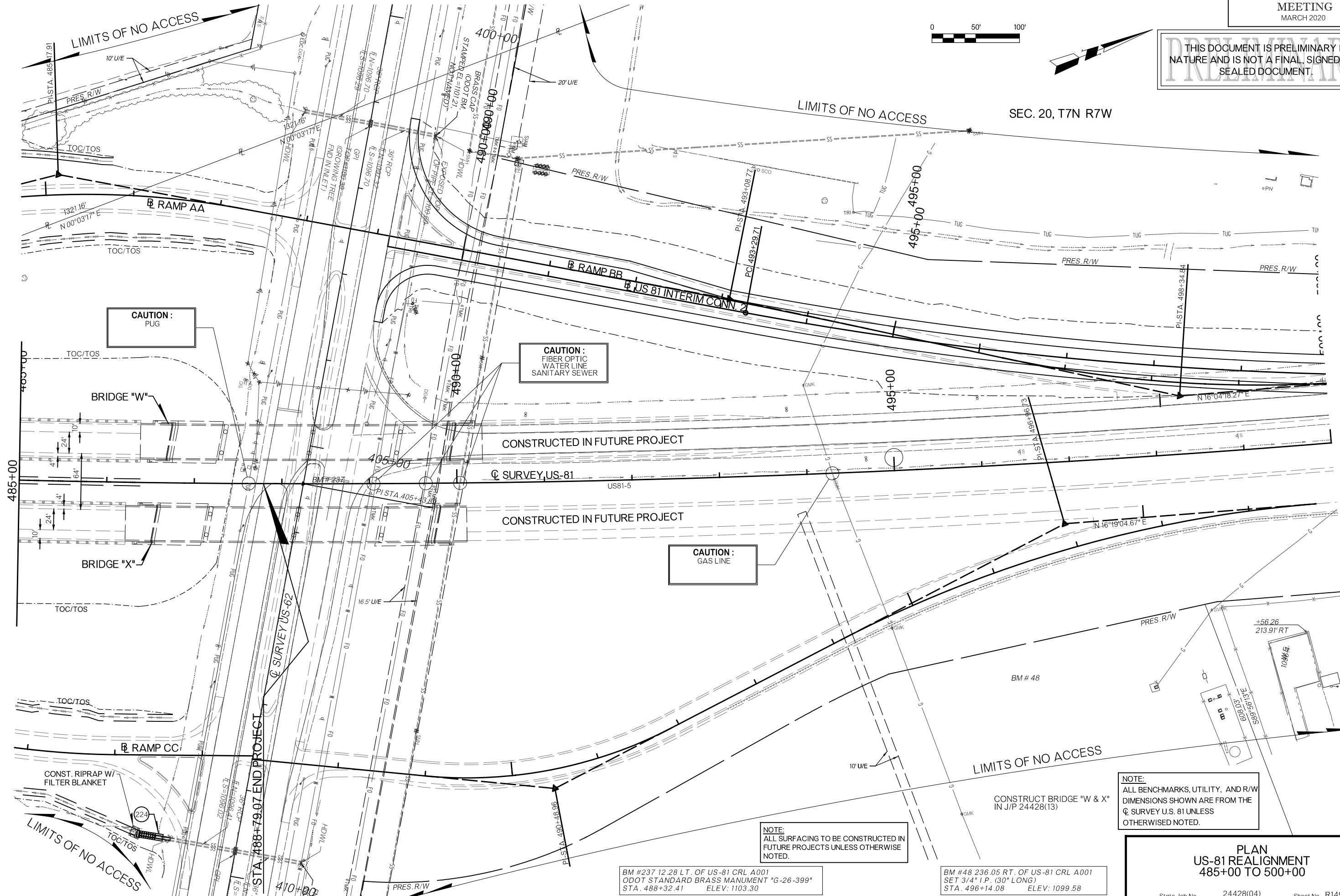
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROFILE
US-81 REALIGNMENT
471+00 TO 485+00

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SEC. 20, T7N R7W



CAUTION:
PUG

CAUTION:
FIBER OPTIC
WATER LINE
SANITARY SEWER

CAUTION:
GAS LINE

NOTE:
ALL BENCHMARKS, UTILITY, AND R/W
DIMENSIONS SHOWN ARE FROM THE
C SURVEY U.S. 81 UNLESS
OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

CONSTRUCT BRIDGE "W & X"
IN J/P 24428(13)

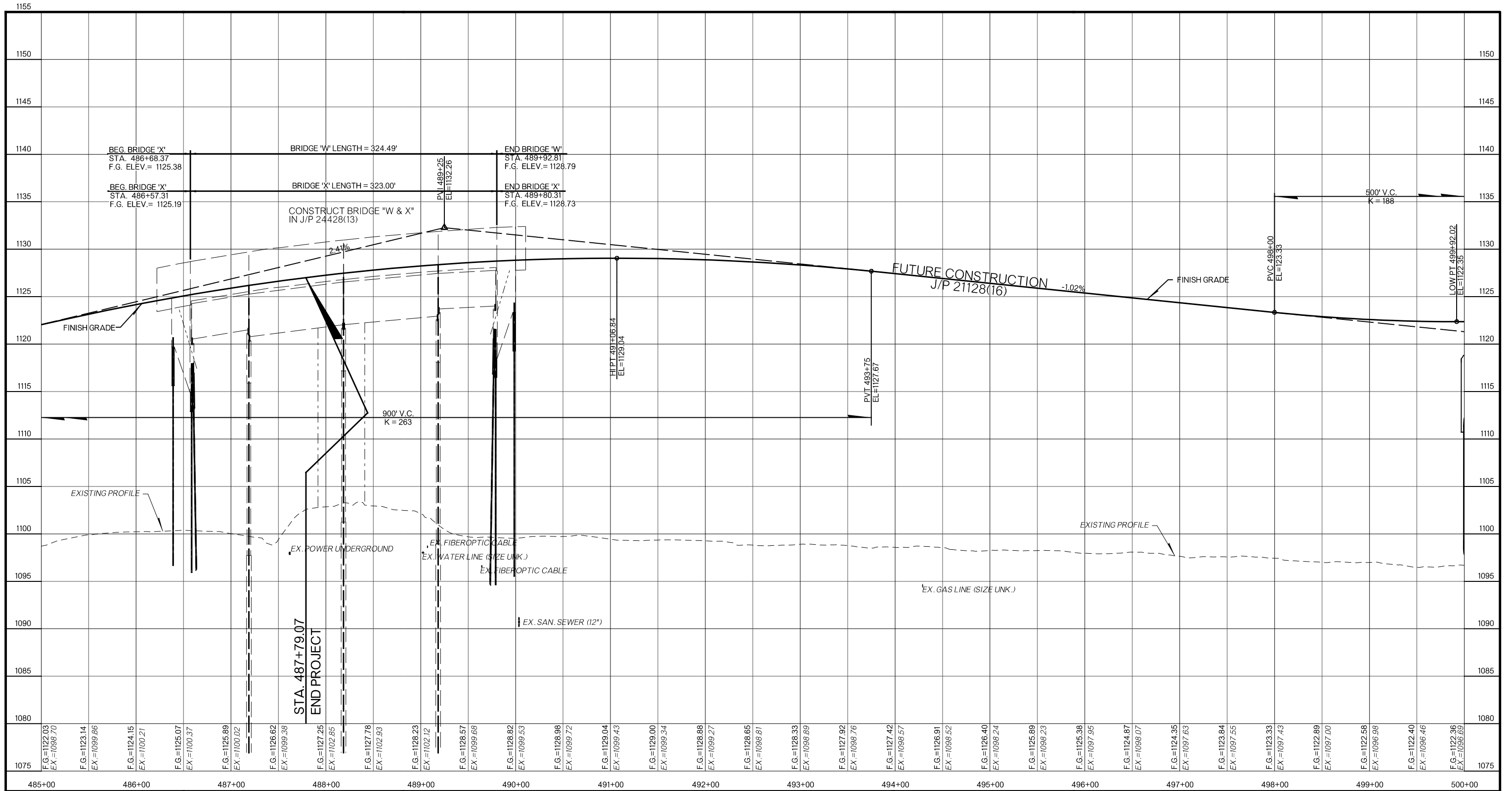
BM #237 12.28 LT. OF US-81 CRL A001
ODOT STANDARD BRASS MANUMENT *G-26-399*
STA. 488+32.41 ELEV: 1103.30

BM #48 236.05 RT. OF US-81 CRL A001
SET 3/4" I.P. (30" LONG)
STA. 496+14.08 ELEV: 1099.58

PLAN
US-81 REALIGNMENT
485+00 TO 500+00

State Job No. 24428(04) Sheet No. R149

US 81 REALIGNMENT GRADY COUNTY

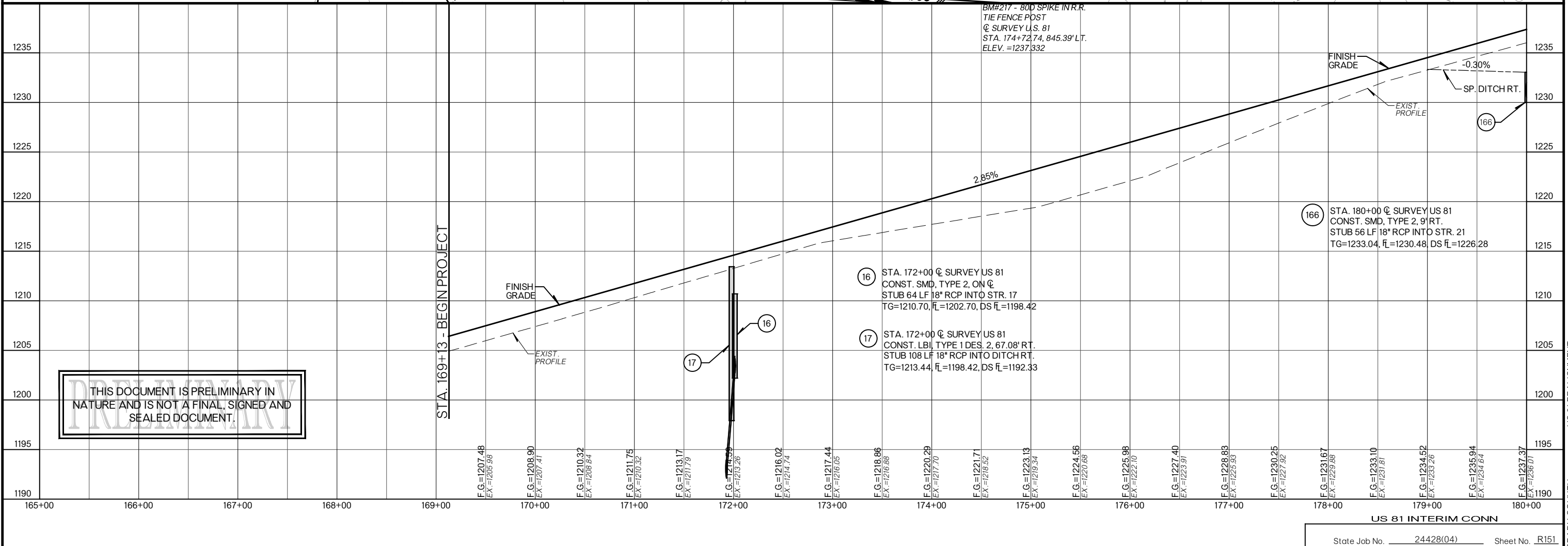
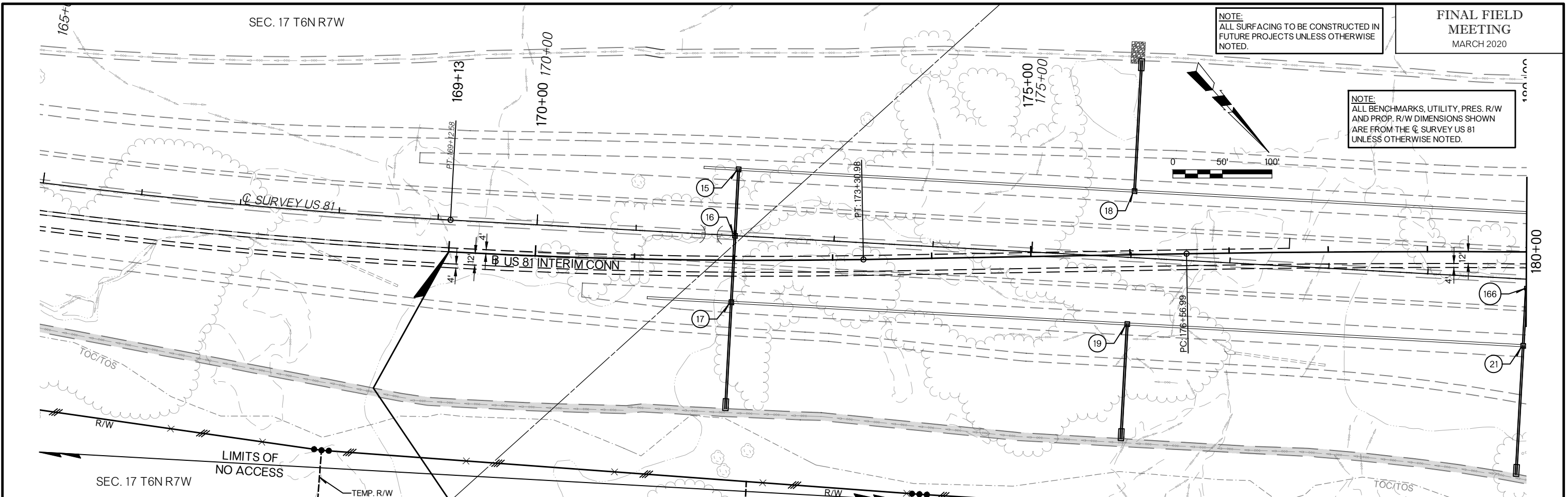
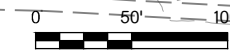


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROFILE
US-81 REALIGNMENT
485+00 TO 500+00

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY US 81
UNLESS OTHERWISE NOTED.



BM#217 - 800 SPIKE IN R.R.
TIE FENCE POST
Q SURVEY U.S. 81
STA. 174+72.74, 845.39' LT.
ELEV. = 1237.332

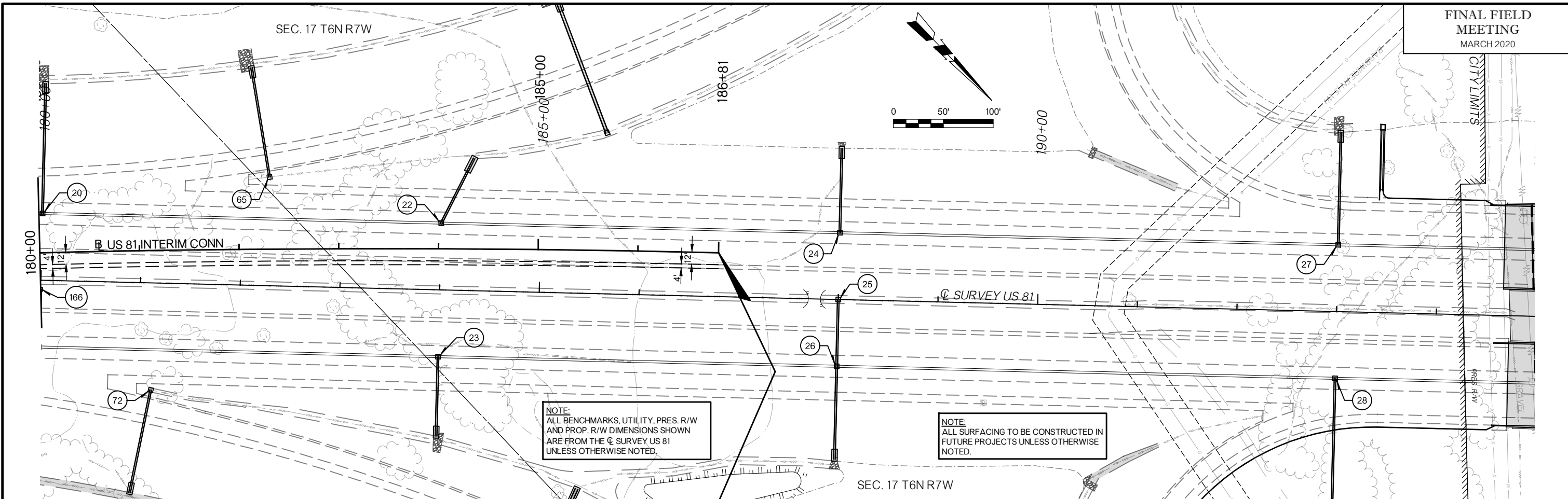
FINISH GRADE
EXIST. PROFILE
SP. DITCH RT.

166 STA. 180+00 Q SURVEY US 81
CONST. SMD, TYPE 2, 9' RT.
STUB 56 LF 18" RCP INTO STR. 21
TG=1233.04, fL=1230.48, DS fL=1226.28

16 STA. 172+00 Q SURVEY US 81
CONST. SMD, TYPE 2, ON Q
STUB 64 LF 18" RCP INTO STR. 17
TG=1210.70, fL=1202.70, DS fL=1198.42

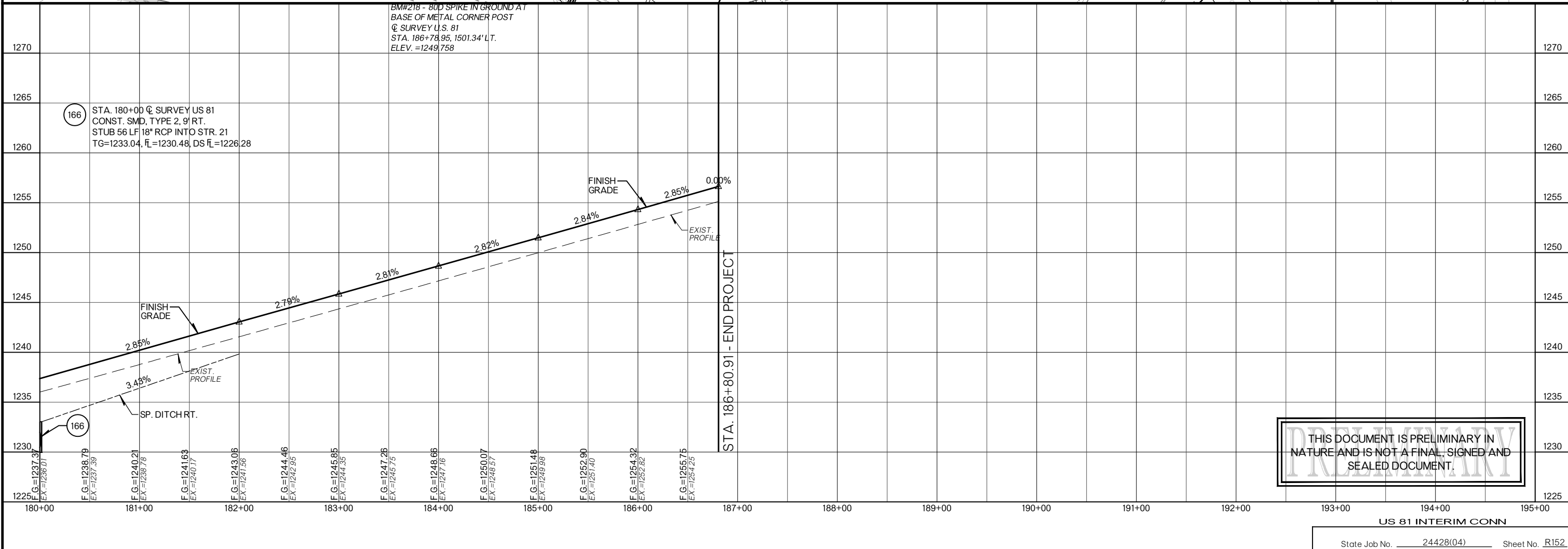
17 STA. 172+00 Q SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' RT.
STUB 108 LF 18" RCP INTO DITCH RT.
TG=1213.44, fL=1198.42, DS fL=1192.33

THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.



NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE ϕ SURVEY US 81
UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEC. 18 T6N R7W

EXISTING LIMITS OF NO ACCESS

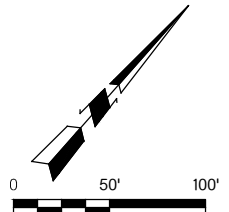
240+00

245+00

250+00

252+00

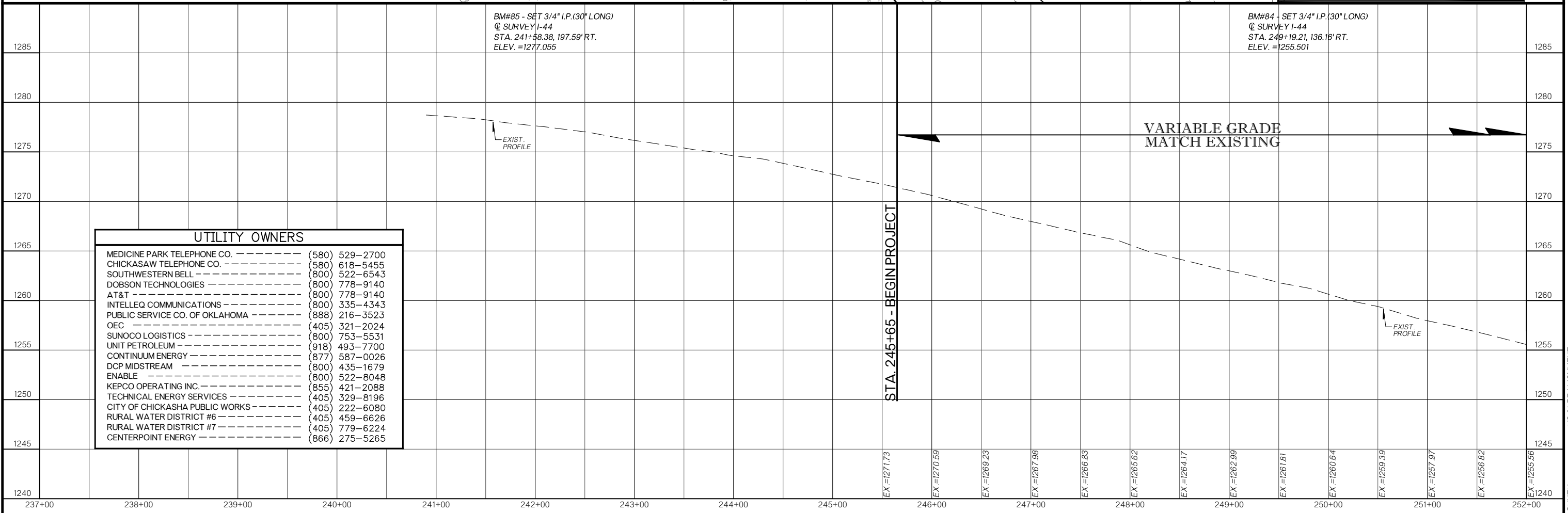
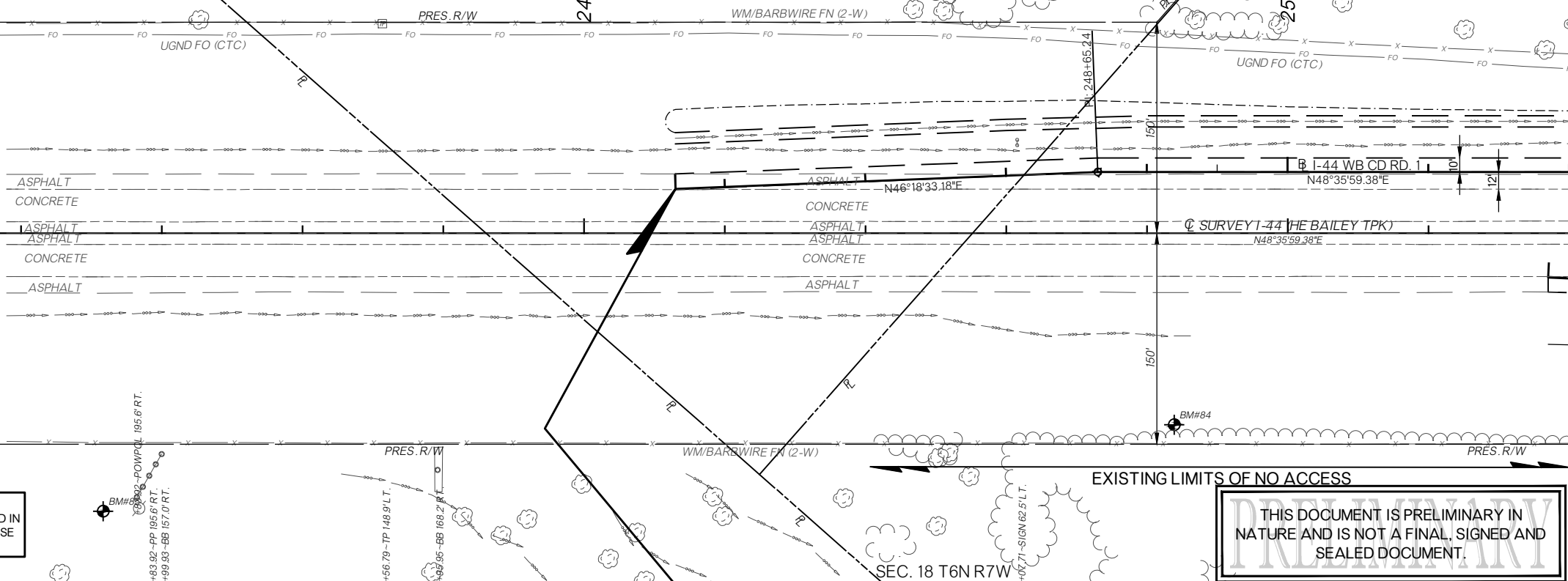
NOTE:
PROPOSED GRADING SHALL NOT IMPACT
EXISTING SHOULDER IN THIS PROJECT
UNLESS OTHERWISE NOTED.



NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND R/W DIMENSIONS SHOWN ARE FROM
THE C SURVEY I-44 UNLESS OTHERWISE
NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.



| UTILITY OWNERS | | |
|--------------------------------|-------|----------------|
| MEDICINE PARK TELEPHONE CO. | ----- | (580) 529-2700 |
| CHICKASAW TELEPHONE CO. | ----- | (580) 618-5455 |
| SOUTHWESTERN BELL | ----- | (800) 522-6543 |
| DOBSON TECHNOLOGIES | ----- | (800) 778-9140 |
| AT&T | ----- | (800) 778-9140 |
| INTELLEQ COMMUNICATIONS | ----- | (800) 335-4343 |
| PUBLIC SERVICE CO. OF OKLAHOMA | ----- | (888) 216-3523 |
| OEC | ----- | (405) 321-2024 |
| SUNOCO LOGISTICS | ----- | (800) 753-5531 |
| UNIT PETROLEUM | ----- | (918) 493-7700 |
| CONTINUUM ENERGY | ----- | (877) 587-0026 |
| DCP MIDSTREAM | ----- | (800) 435-1679 |
| ENABLE | ----- | (800) 522-8048 |
| KEPCO OPERATING INC. | ----- | (855) 421-2088 |
| TECHNICAL ENERGY SERVICES | ----- | (405) 329-8196 |
| CITY OF CHICKASHA PUBLIC WORKS | ----- | (405) 222-6080 |
| RURAL WATER DISTRICT #6 | ----- | (405) 459-6626 |
| RURAL WATER DISTRICT #7 | ----- | (405) 779-6224 |
| CENTERPOINT ENERGY | ----- | (866) 275-5265 |

BM#85 - SET 3/4" I.P. (30" LONG)
C SURVEY I-44
STA. 241+58.38, 197.59' RT.
ELEV. =1277.055

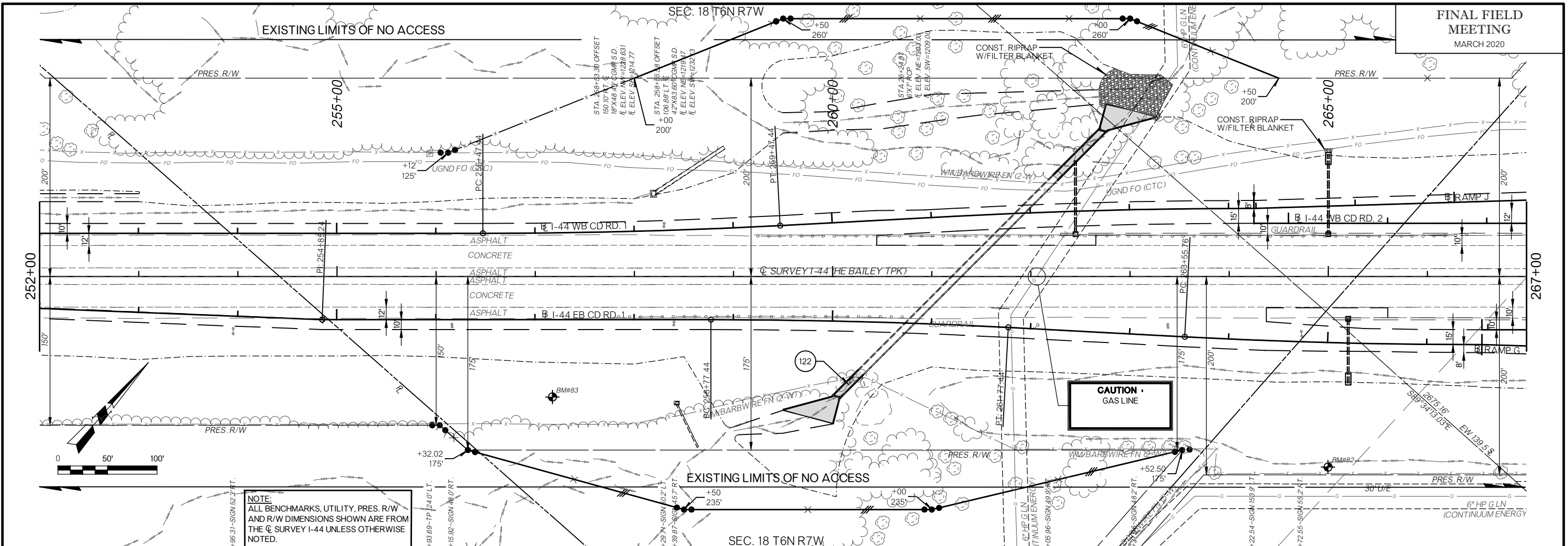
BM#84 - SET 3/4" I.P. (30" LONG)
C SURVEY I-44
STA. 249+19.21, 136.16' RT.
ELEV. =1255.501

STA. 245+65 - BEGIN PROJECT

VARIABLE GRADE
MATCH EXISTING

EXIST. PROFILE

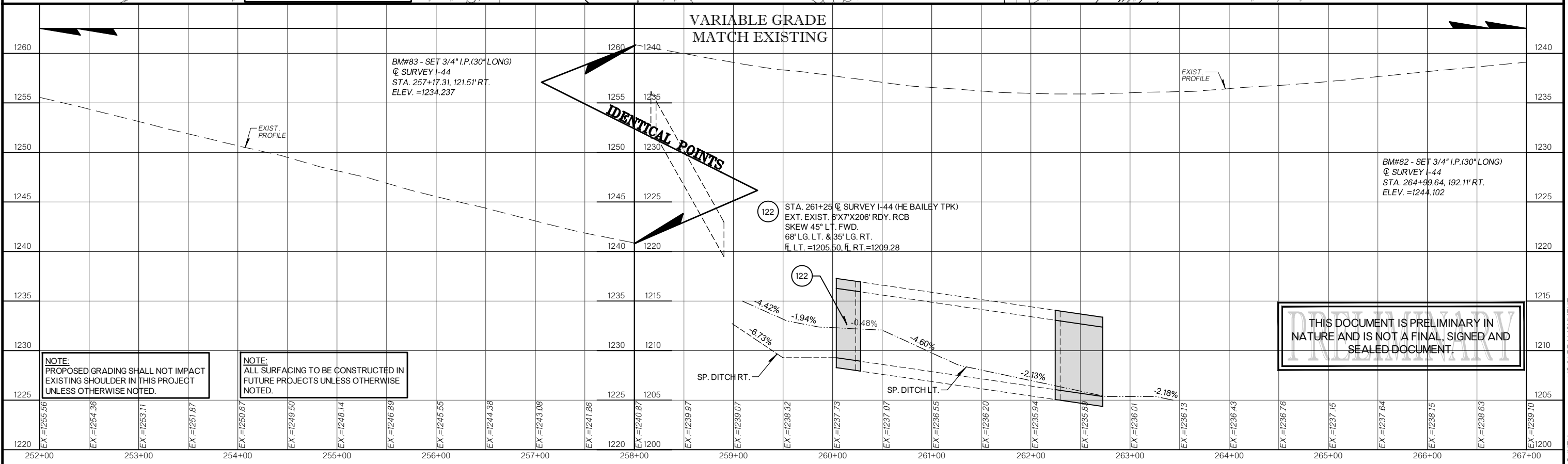
- EX.=1271.73
- EX.=1270.59
- EX.=1269.23
- EX.=1267.98
- EX.=1266.83
- EX.=1265.62
- EX.=1264.17
- EX.=1262.99
- EX.=1261.81
- EX.=1260.64
- EX.=1259.39
- EX.=1257.97
- EX.=1256.82
- EX.=1255.56



NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND R/W DIMENSIONS SHOWN ARE FROM
THE Q SURVEY I-44 UNLESS OTHERWISE
NOTED.

CAUTION
GAS LINE

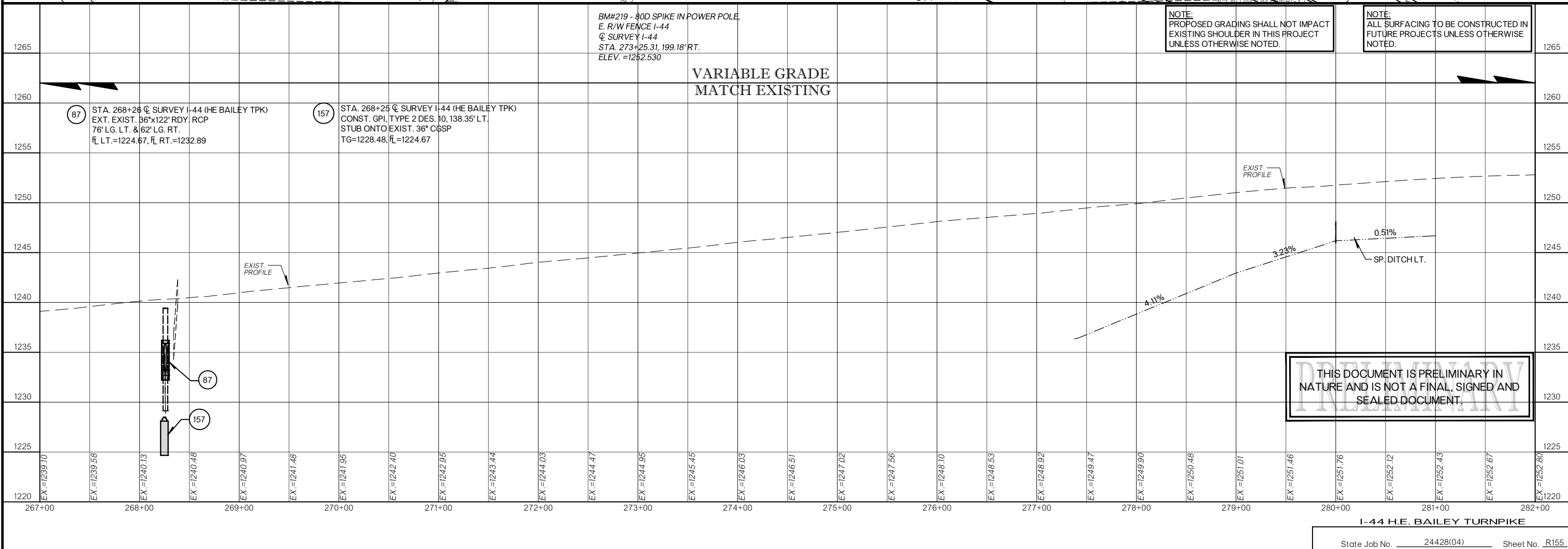
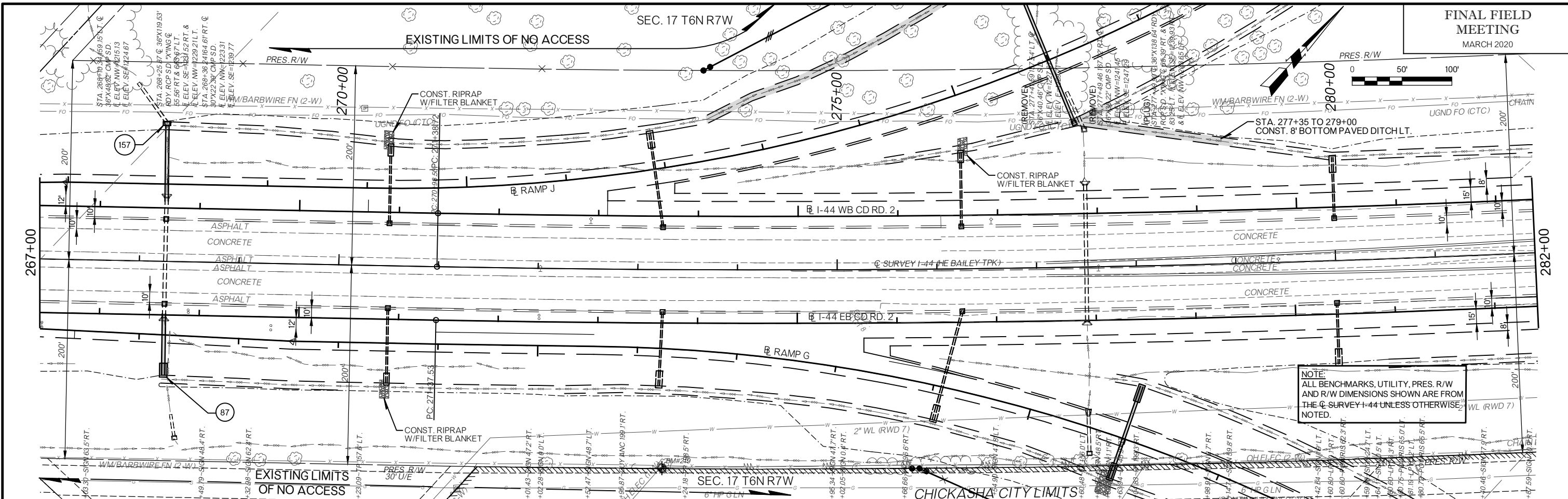
VARIABLE GRADE
MATCH EXISTING



NOTE:
PROPOSED GRADING SHALL NOT IMPACT
EXISTING SHOULDER IN THIS PROJECT
UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

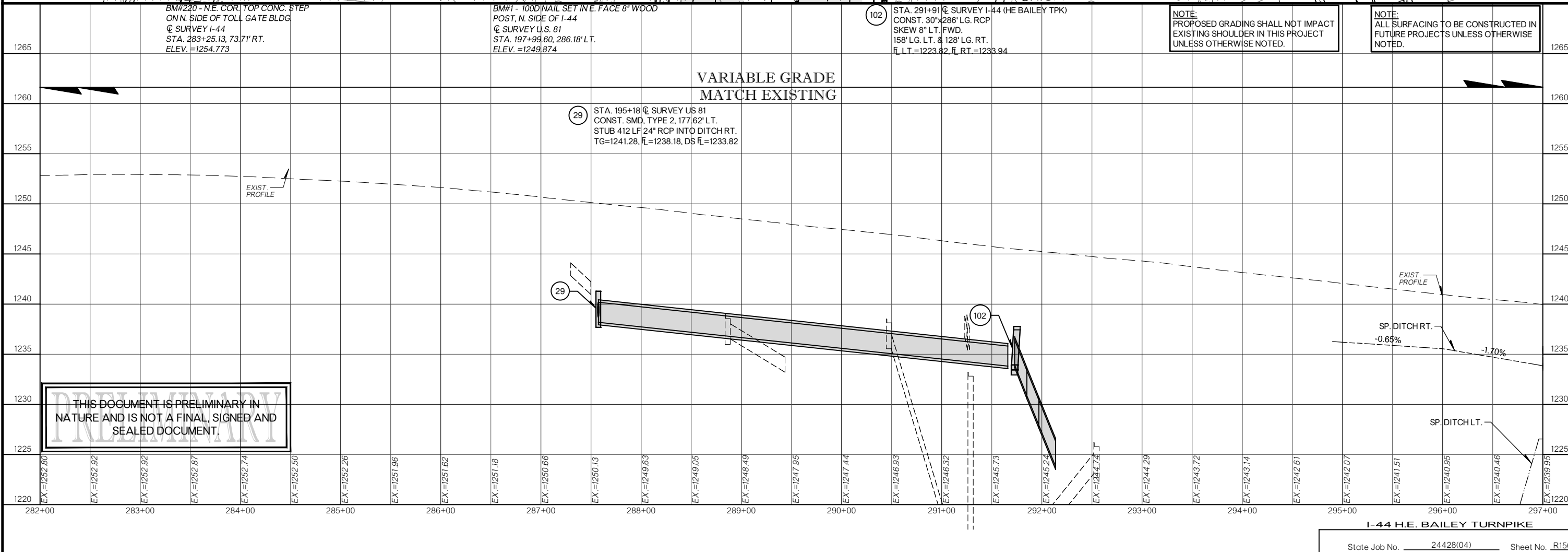
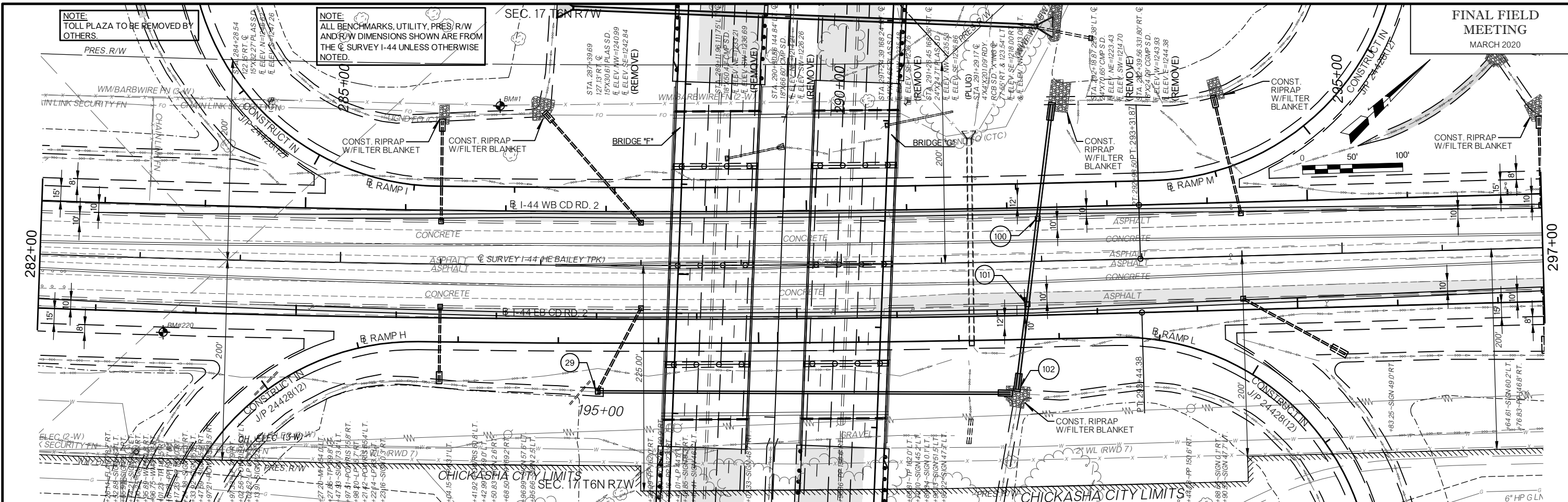
THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

NOTE:
TOLL PLAZA TO BE REMOVED BY OTHERS.

NOTE:
ALL BENCHMARKS, UTILITY, PRES./R/W AND R/W DIMENSIONS SHOWN ARE FROM THE C. SURVEY I-44 UNLESS OTHERWISE NOTED.



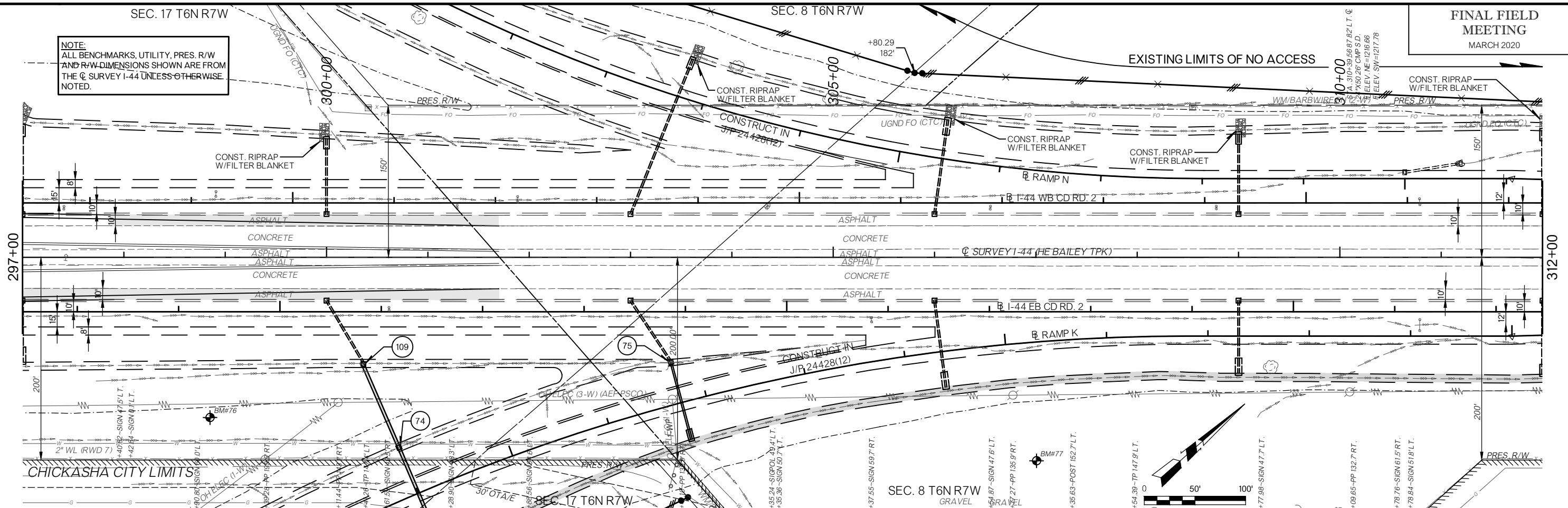
VARIABLE GRADE MATCH EXISTING

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

NOTE:
PROPOSED GRADING SHALL NOT IMPACT EXISTING SHOULDER IN THIS PROJECT UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND R/W-DIMENSIONS SHOWN ARE FROM
THE Q SURVEY I-44 UNLESS OTHERWISE
NOTED.



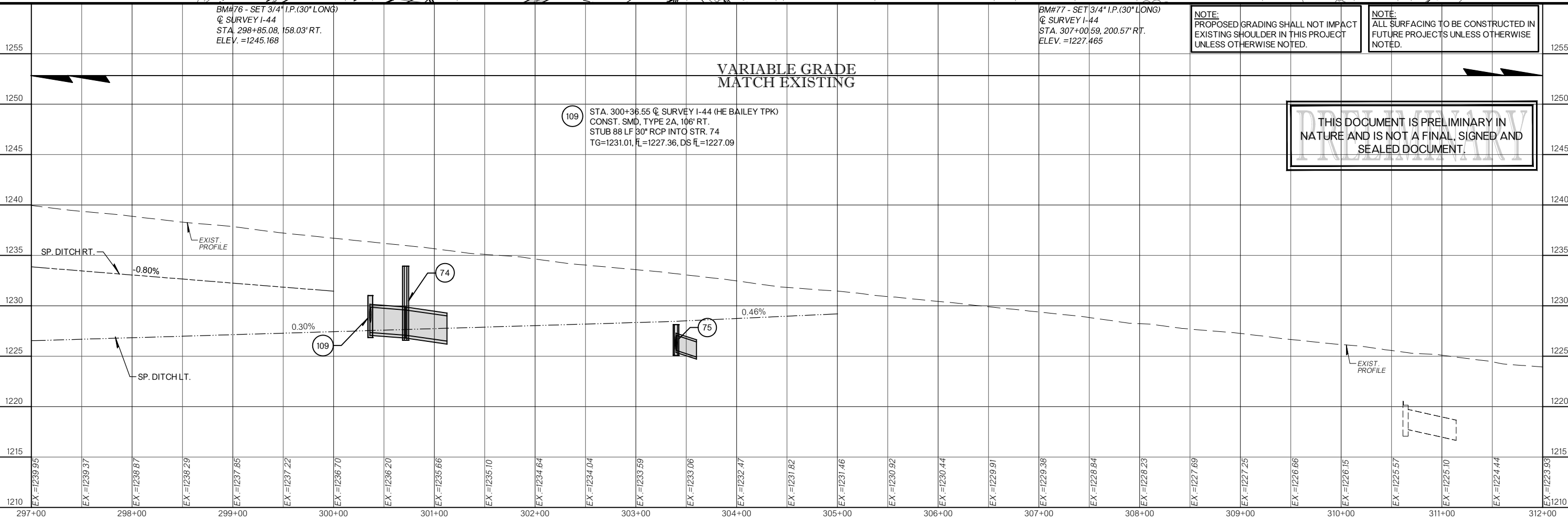
NOTE:
PROPOSED GRADING SHALL NOT IMPACT
EXISTING SHOULDER IN THIS PROJECT
UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

VARIABLE GRADE
MATCH EXISTING

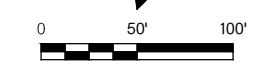
THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

109 STA. 300+36.55 Q SURVEY I-44 (HE BAILEY TPK)
CONST. SMD, TYPE 2A, 106' RT.
STUB 88 LF 30" RCP INTO STR. 74
TG=1231.01, f_L =1227.36, $DS f_L$ =1227.09

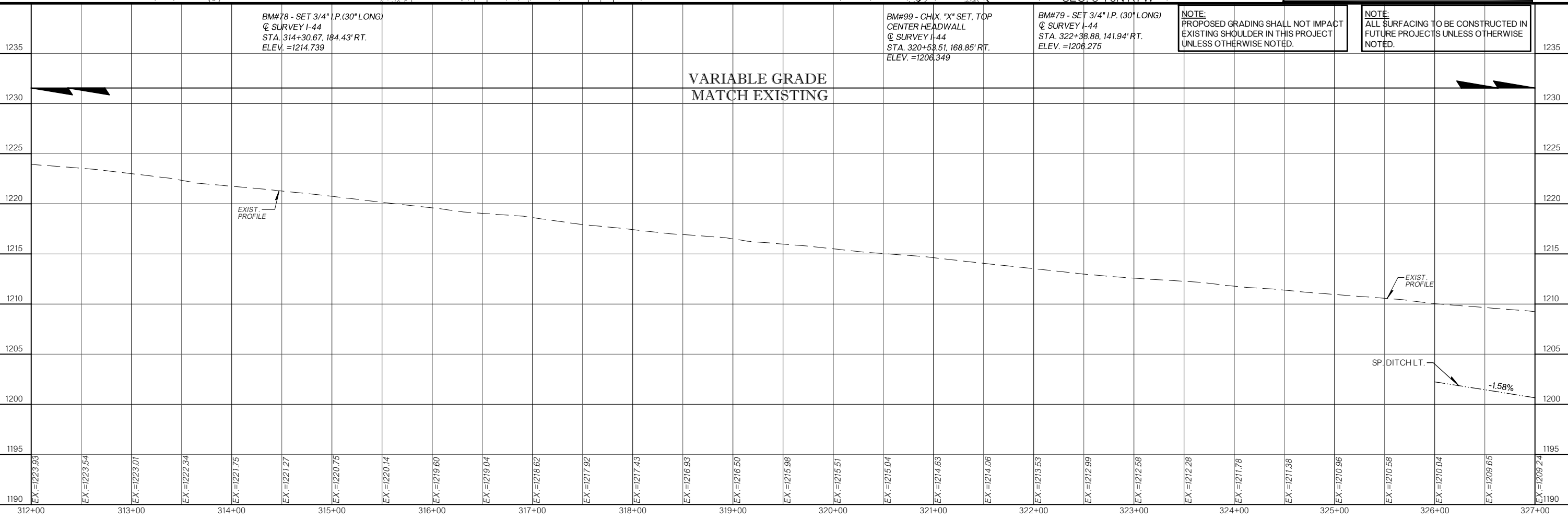
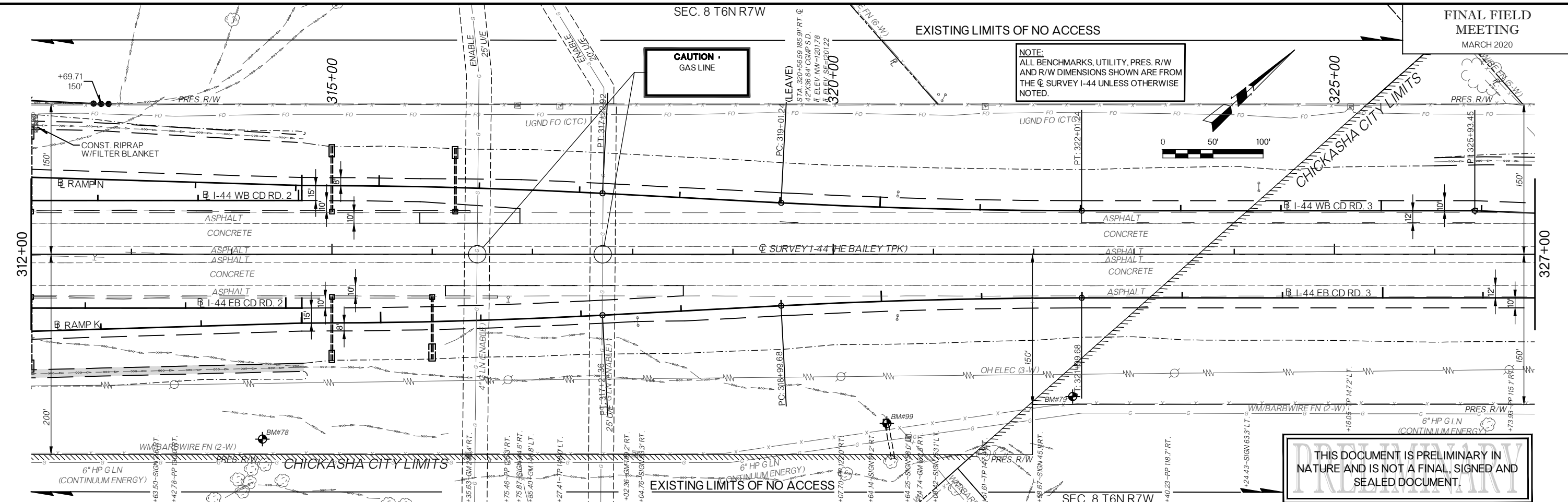


CAUTION
GAS LINE

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND R/W DIMENSIONS SHOWN ARE FROM
THE Q. SURVEY I-44 UNLESS OTHERWISE
NOTED.



THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.



BM#78 - SET 3/4" I.P. (30" LONG)
Q. SURVEY I-44
STA. 314+30.67, 184.43' RT.
ELEV. =1214.739

BM#99 - CHIX. "X" SET, TOP
CENTER HEADWALL
Q. SURVEY I-44
STA. 320+58.51, 168.85' RT.
ELEV. =1206.349

BM#79 - SET 3/4" I.P. (30" LONG)
Q. SURVEY I-44
STA. 322+38.88, 141.94' RT.
ELEV. =1206.275

NOTE:
PROPOSED GRADING SHALL NOT IMPACT
EXISTING SHOULDER IN THIS PROJECT
UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

VARIABLE GRADE
MATCH EXISTING

EXIST. PROFILE

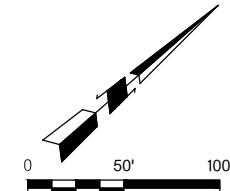
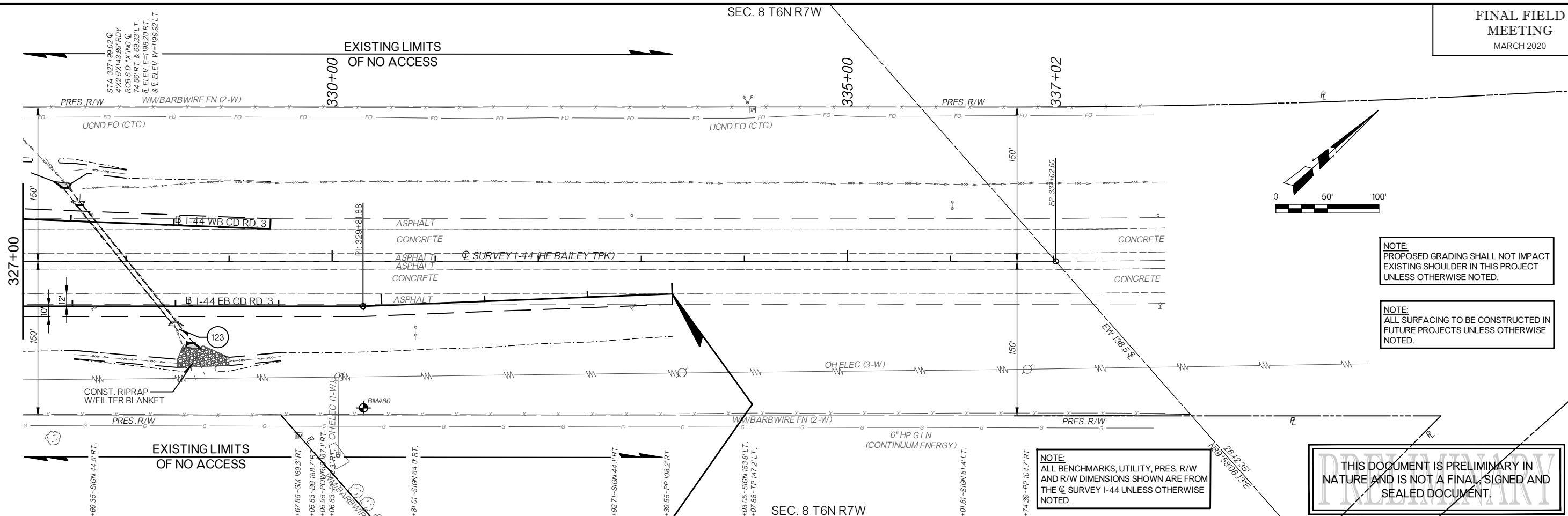
EXIST. PROFILE

SP. DITCH LT.

-1.58%

SEC. 8 T6N R7W

SEC. 8 T6N R7W



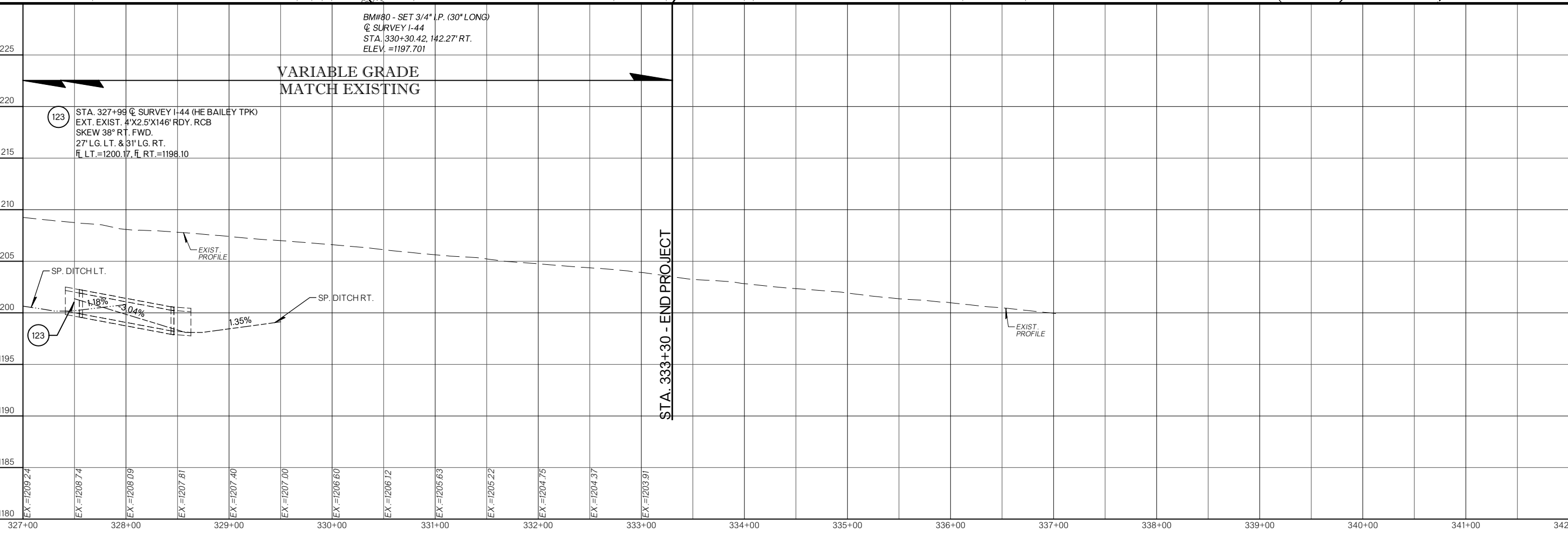
NOTE:
PROPOSED GRADING SHALL NOT IMPACT EXISTING SHOULDER IN THIS PROJECT UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

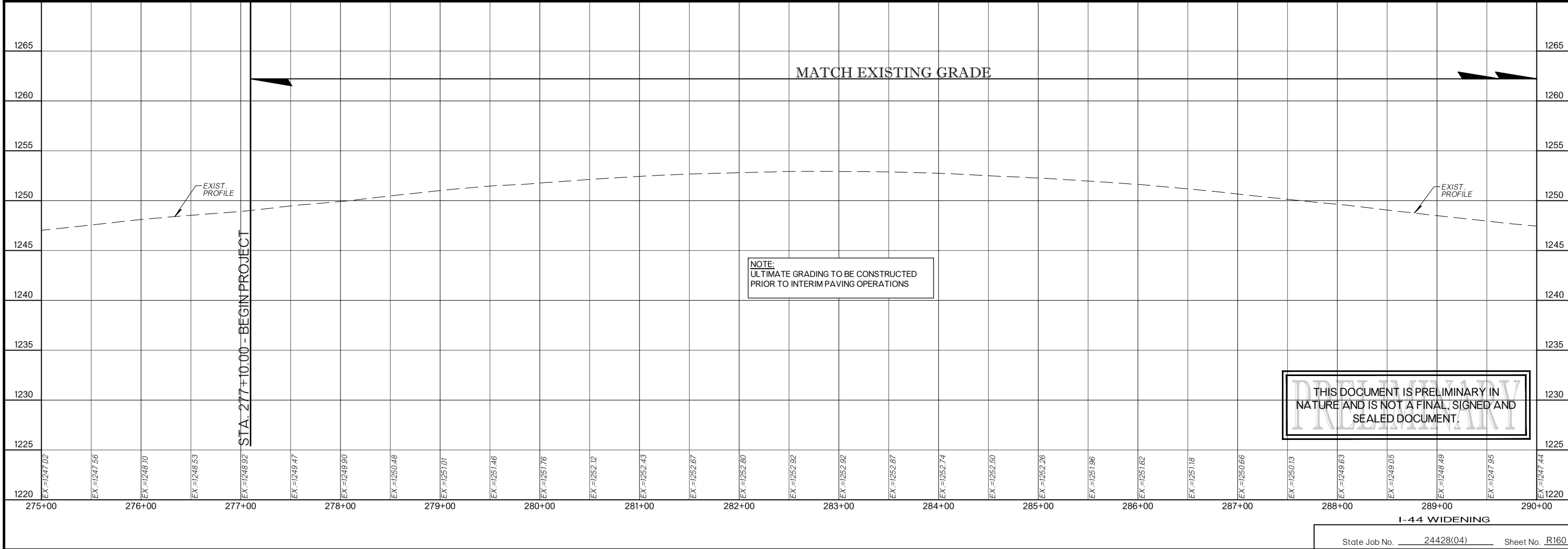
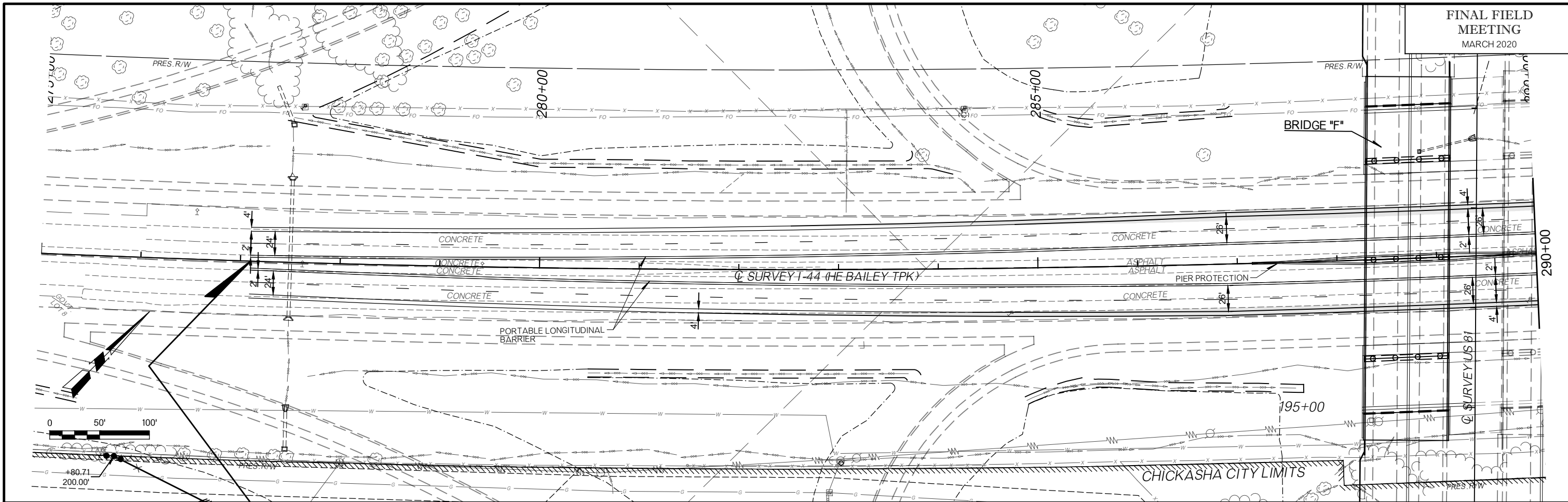
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY I-44 UNLESS OTHERWISE NOTED.

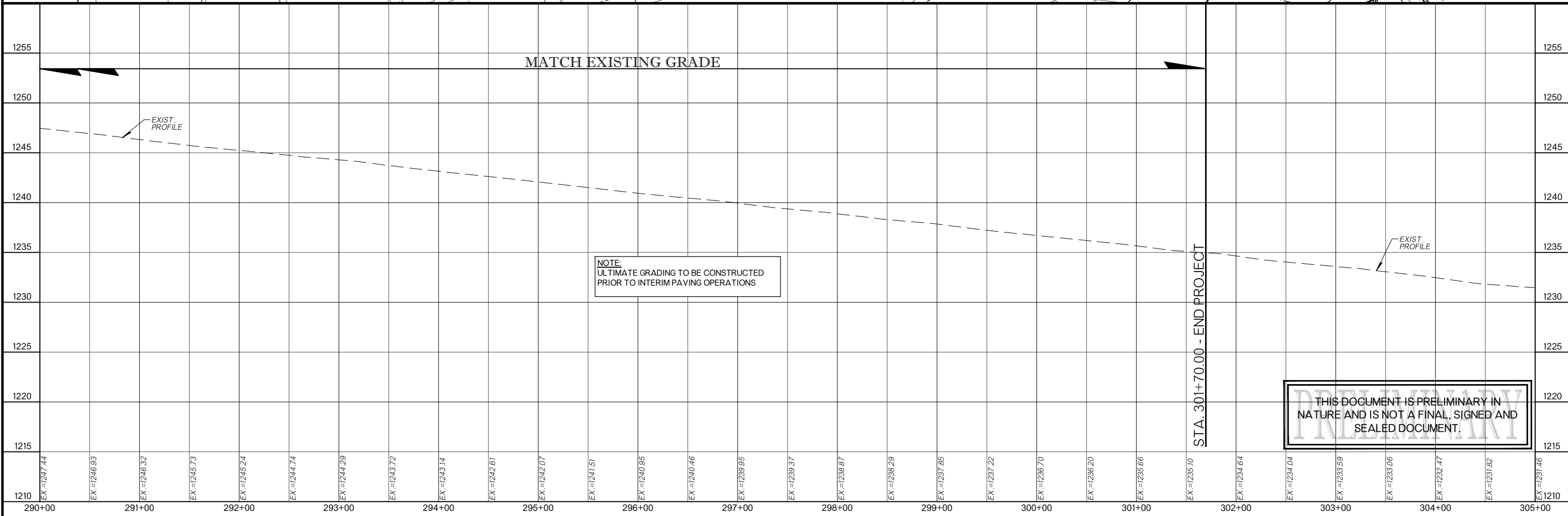
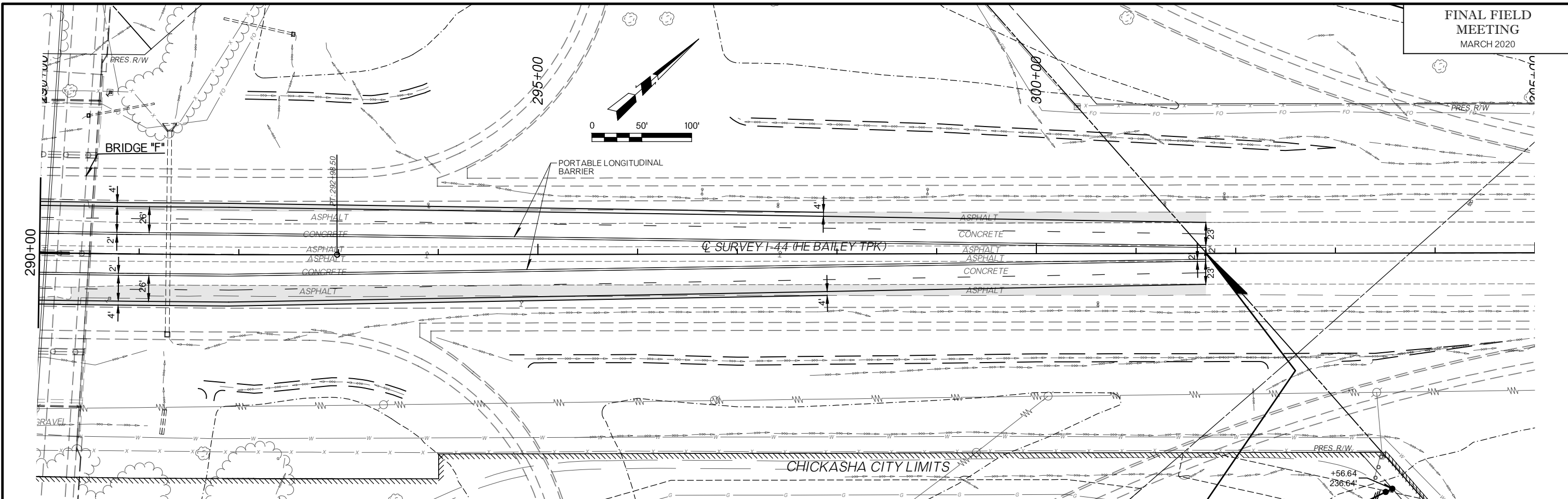
VARIABLE GRADE MATCH EXISTING



STA. 333+30 - END PROJECT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

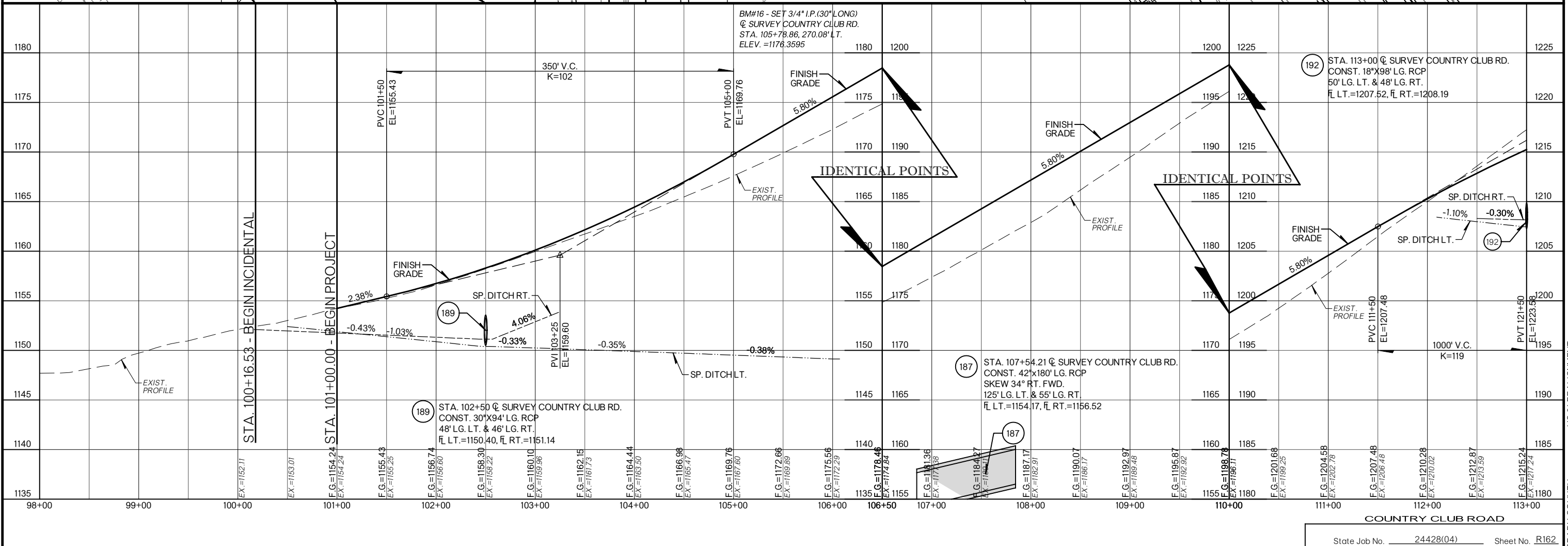
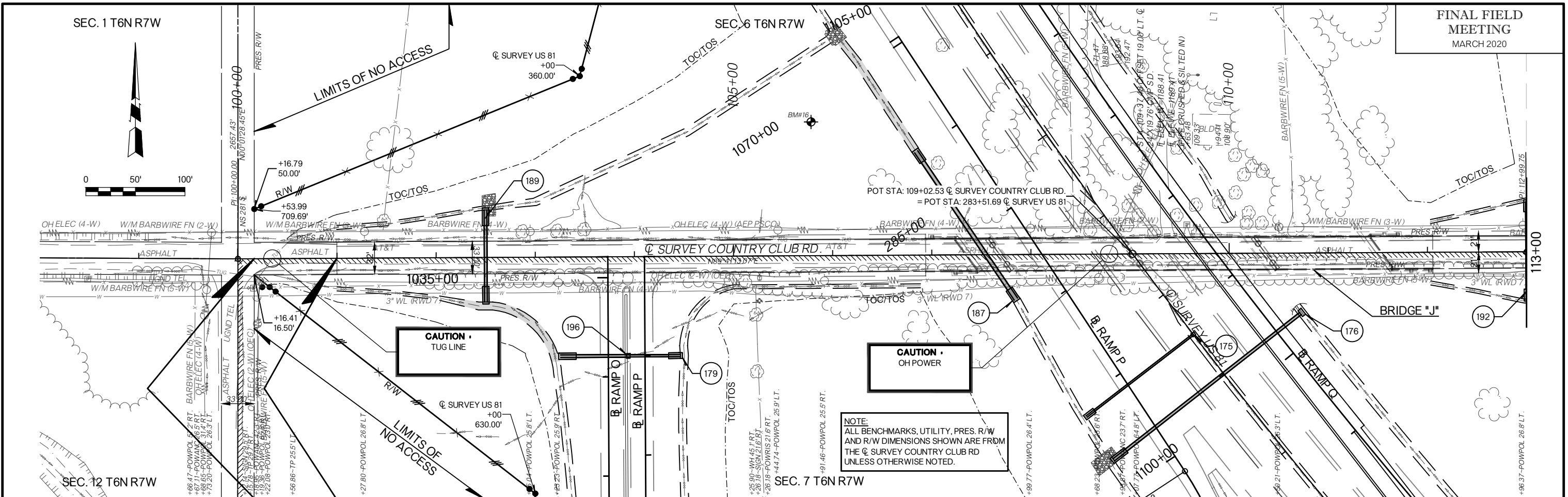
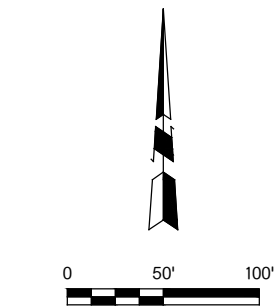


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

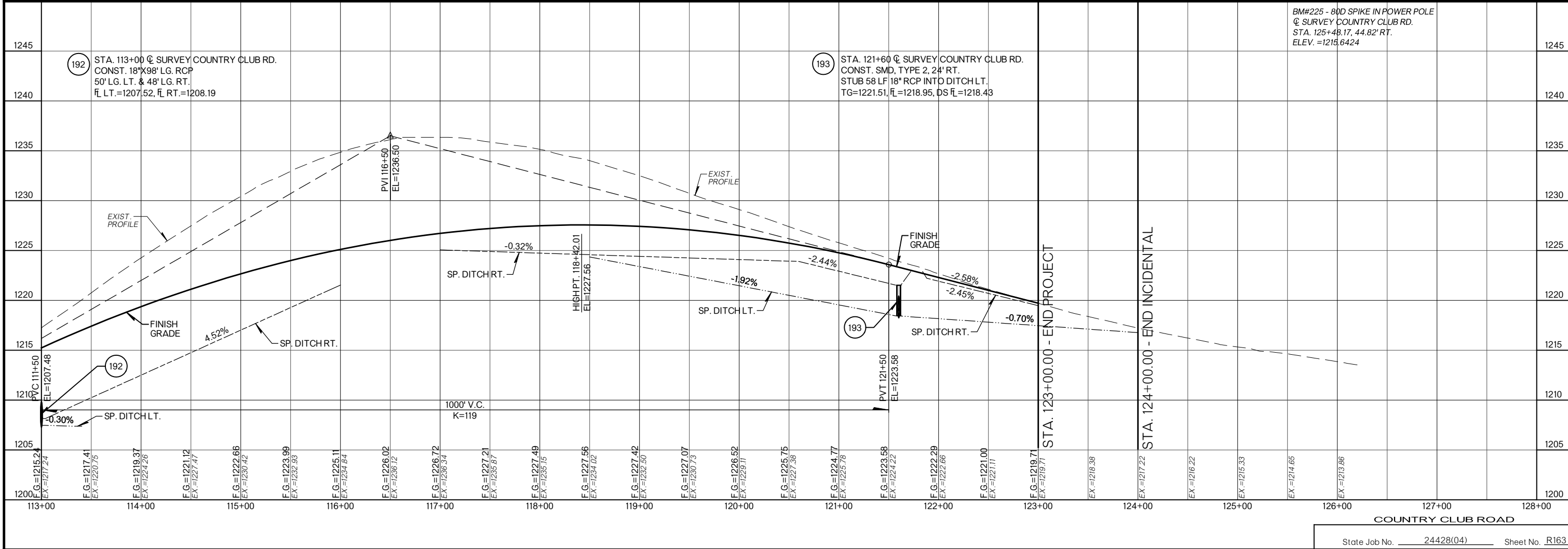
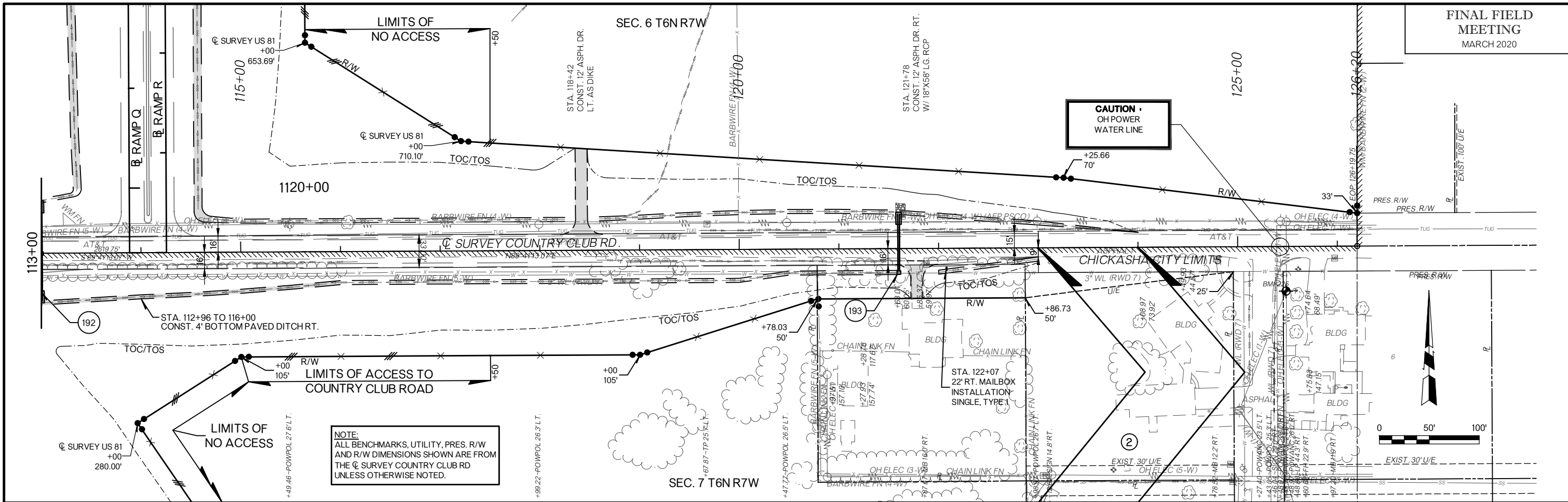
SEC. 1 T6N R7W

SEC. 6 T6N R7W

SEC. 7 T6N R7W



GRADY COUNTY US 81 REALIGNMENT



BM#225 - 80D SPIKE IN POWER POLE
@ SURVEY COUNTRY CLUB RD.
STA. 125+48.17, 44.82' RT.
ELEV. =1215.6424

192 STA. 113+00 @ SURVEY COUNTRY CLUB RD.
CONST. 18"X98' LG. RCP
50' LG. LT. & 48' LG. RT.
FL LT.=1207.52, FL RT.=1208.19

193 STA. 121+60 @ SURVEY COUNTRY CLUB RD.
CONST. SMD, TYPE 2, 24' RT.
STUB 58 LF 18" RCP INTO DITCH LT.
TG=1221.51, FL=1218.95, DS FL=1218.43

STA. 123+00.00 - END PROJECT

STA. 124+00.00 - END INCIDENTAL

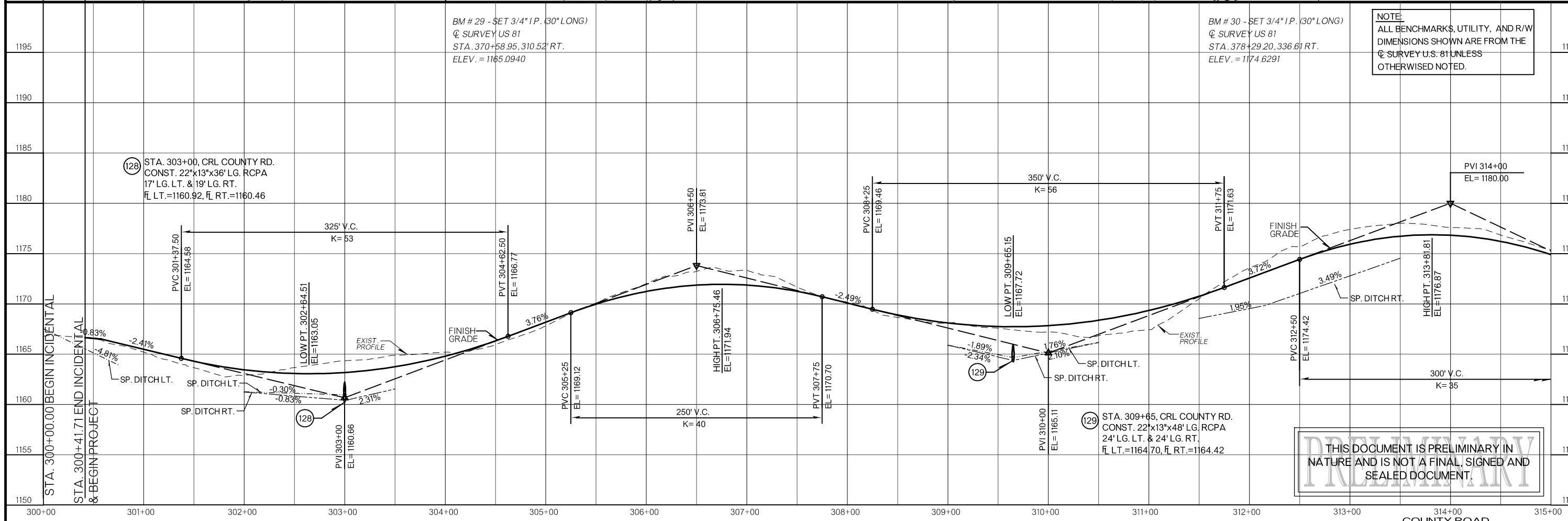
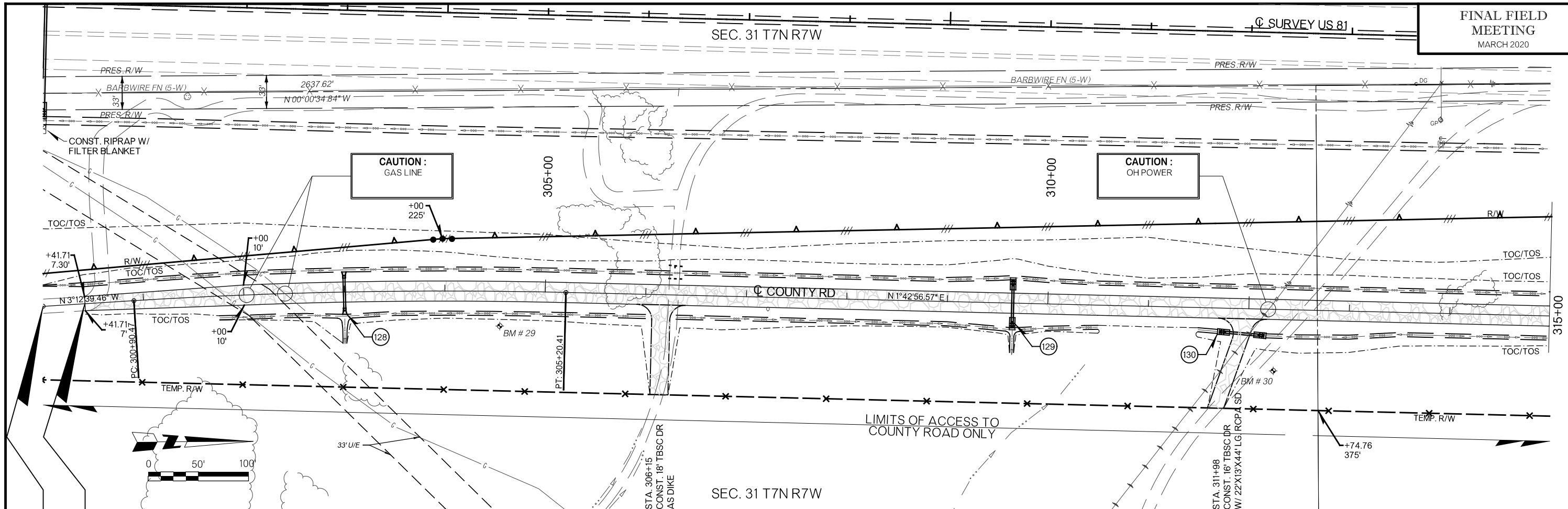
COUNTRY CLUB ROAD

State Job No. 24428(04) Sheet No. R163

US 81 REALIGNMENT
GRADY COUNTY

SEC. 31 T7N R7W

CL SURVEY US 81

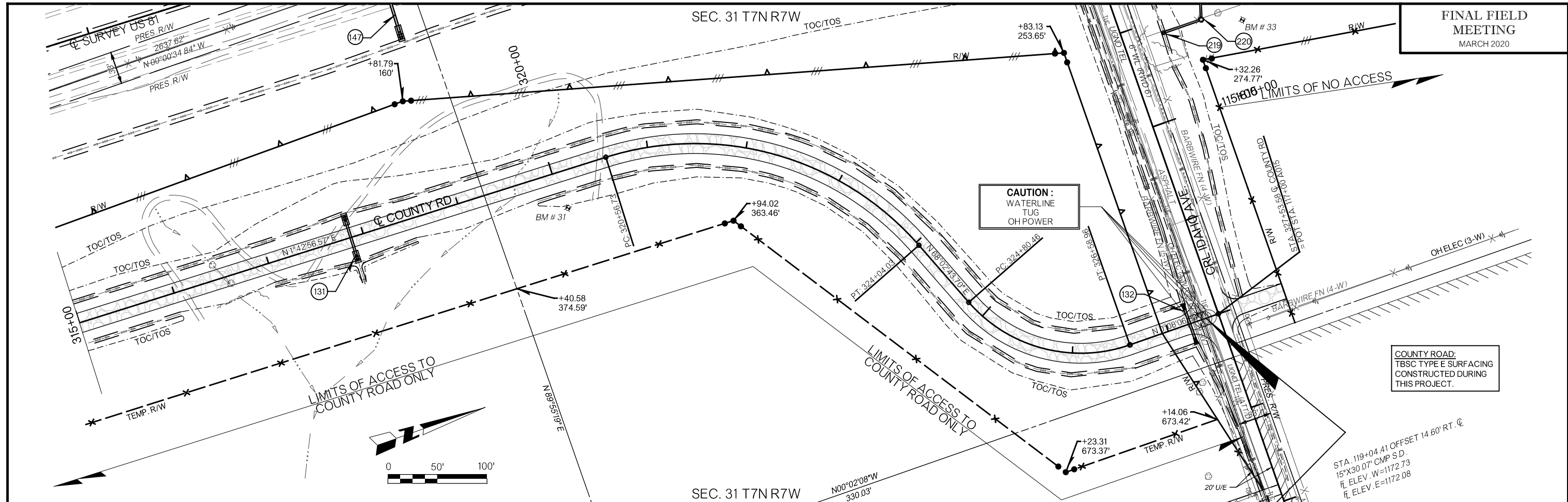


NOTE:
ALL BENCHMARKS, UTILITY, AND R/W
DIMENSIONS SHOWN ARE FROM THE
CL SURVEY U.S. 81 UNLESS
OTHERWISE NOTED.

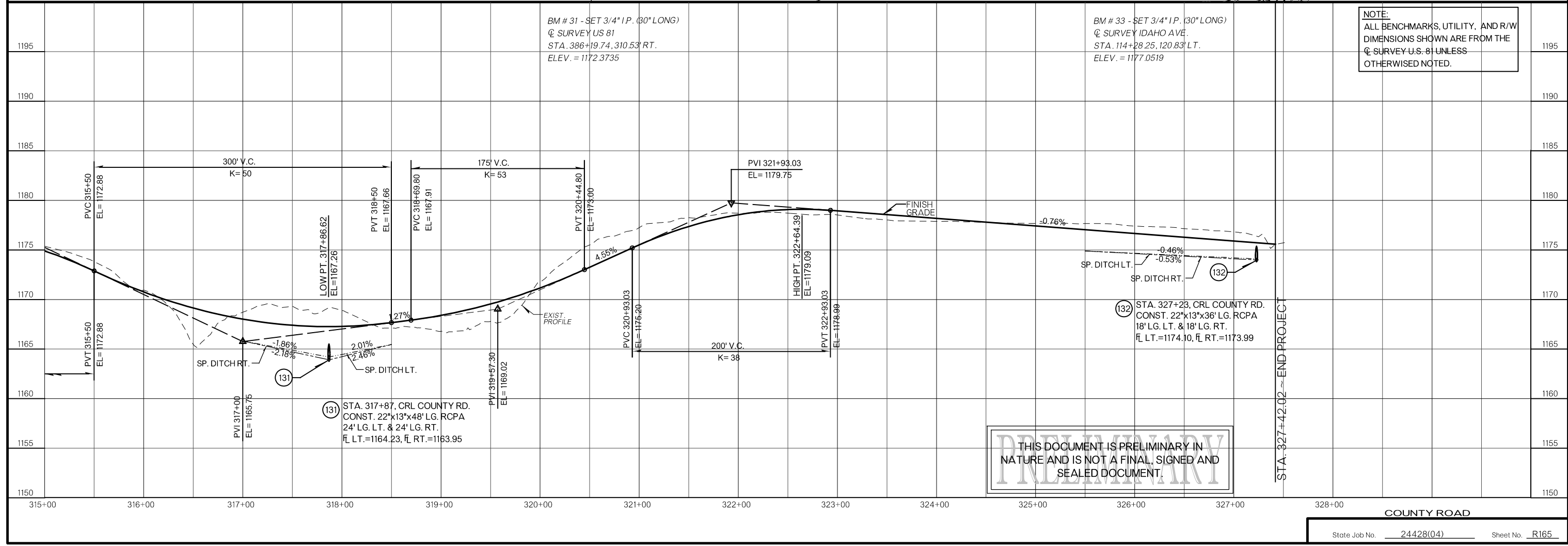
THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

US 81 REALIGNMENT
GRADY COUNTY

SEC. 31 T7N R7W



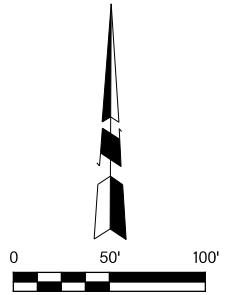
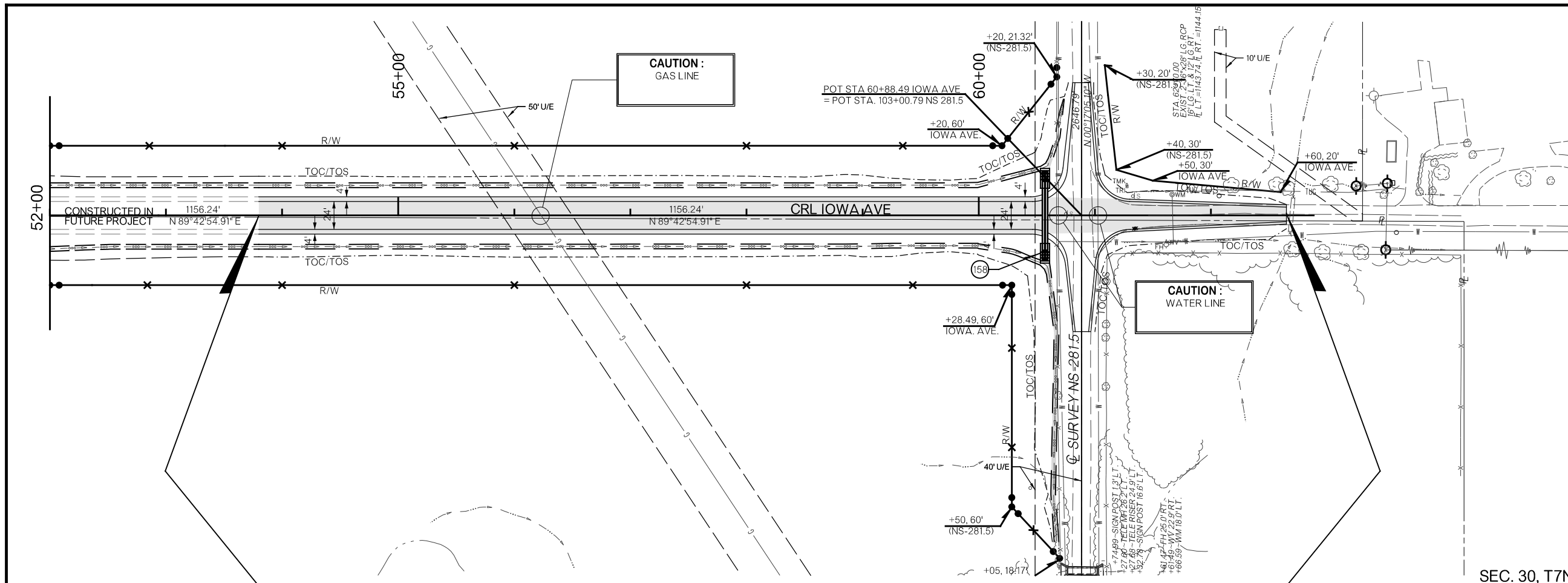
SEC. 31 T7N R7W



NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

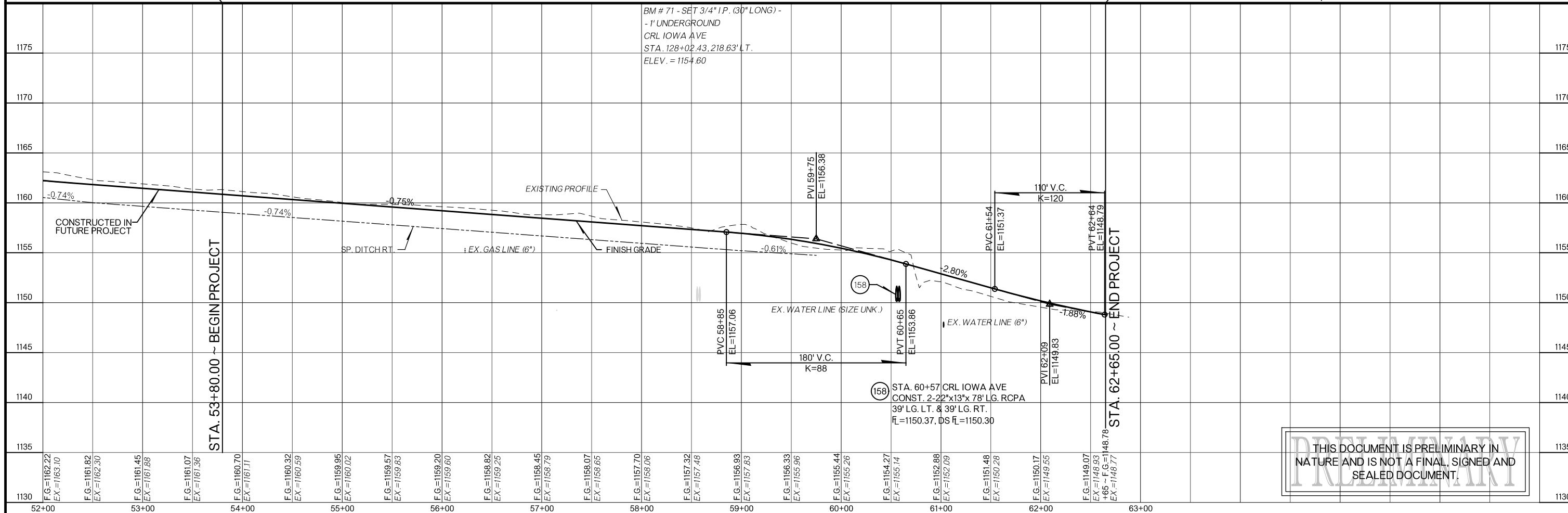
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

STA. 327+42.02 ~ END PROJECT



NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE C SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

SEC. 30, T7N R7W



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

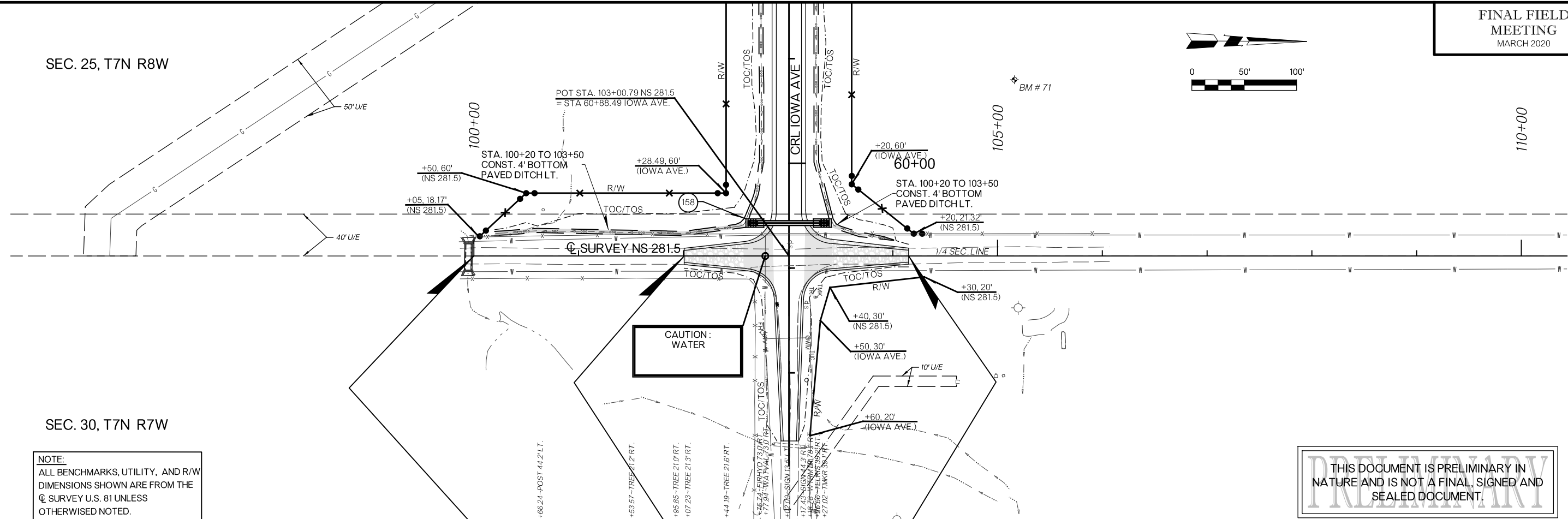
| | | | | | | | | | | | | | | | | | | | | | | |
|-------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|-----------------------------|--|
| 1130 | F.G.=1162.22 EX.=1163.10 | F.G.=1161.82 EX.=1162.30 | F.G.=1161.45 EX.=1161.88 | F.G.=1161.07 EX.=1161.36 | F.G.=1160.70 EX.=1161.11 | F.G.=1160.32 EX.=1160.59 | F.G.=1159.95 EX.=1160.02 | F.G.=1159.57 EX.=1159.83 | F.G.=1159.20 EX.=1159.60 | F.G.=1158.82 EX.=1159.25 | F.G.=1158.45 EX.=1158.79 | F.G.=1158.07 EX.=1158.65 | F.G.=1157.70 EX.=1158.06 | F.G.=1157.32 EX.=1157.48 | F.G.=1156.93 EX.=1157.83 | F.G.=1156.33 EX.=1155.96 | F.G.=1155.44 EX.=1155.26 | F.G.=1154.27 EX.=1155.14 | F.G.=1152.88 EX.=1152.09 | F.G.=1151.48 EX.=1150.28 | F.G.=1150.17 EX.=1149.55 | F.G.=1149.07 EX.=1148.93 +65 ~ F.G.=1148.78 EX.=1148.77 |
| 52+00 | 53+00 | 54+00 | 55+00 | 56+00 | 57+00 | 58+00 | 59+00 | 60+00 | 61+00 | 62+00 | 63+00 | | | | | | | | | | | |

US-81 REALIGNMENT
GRADY COUNTY



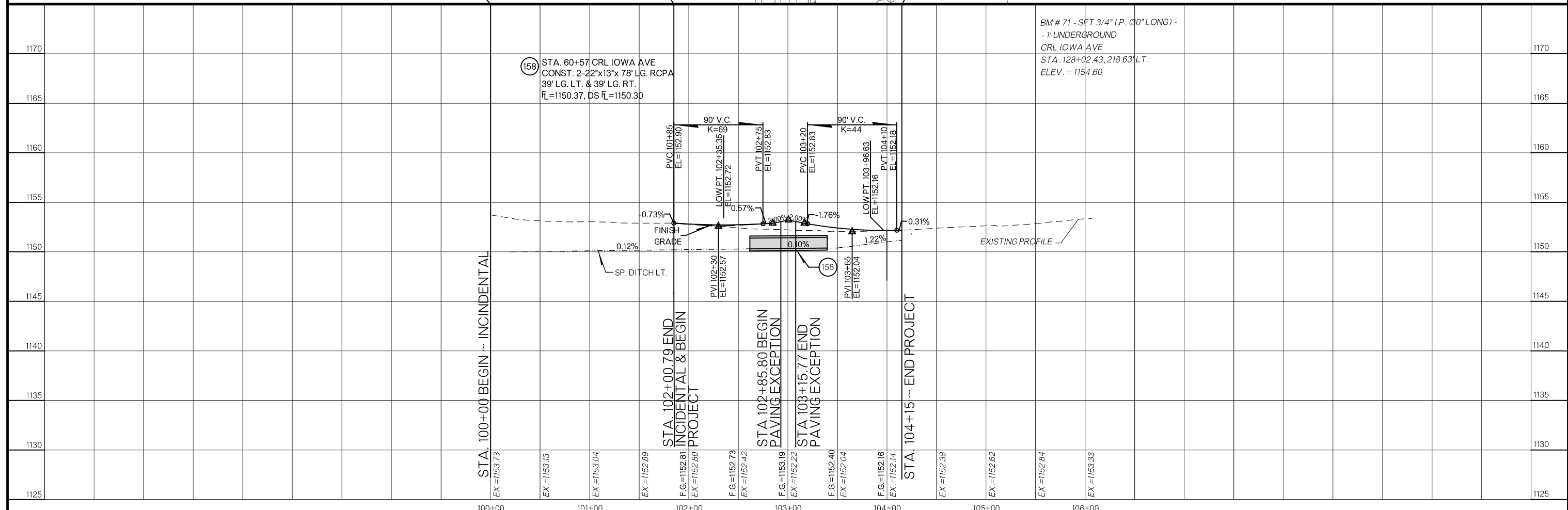
SEC. 25, T7N R8W

SEC. 30, T7N R7W



NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE \odot SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

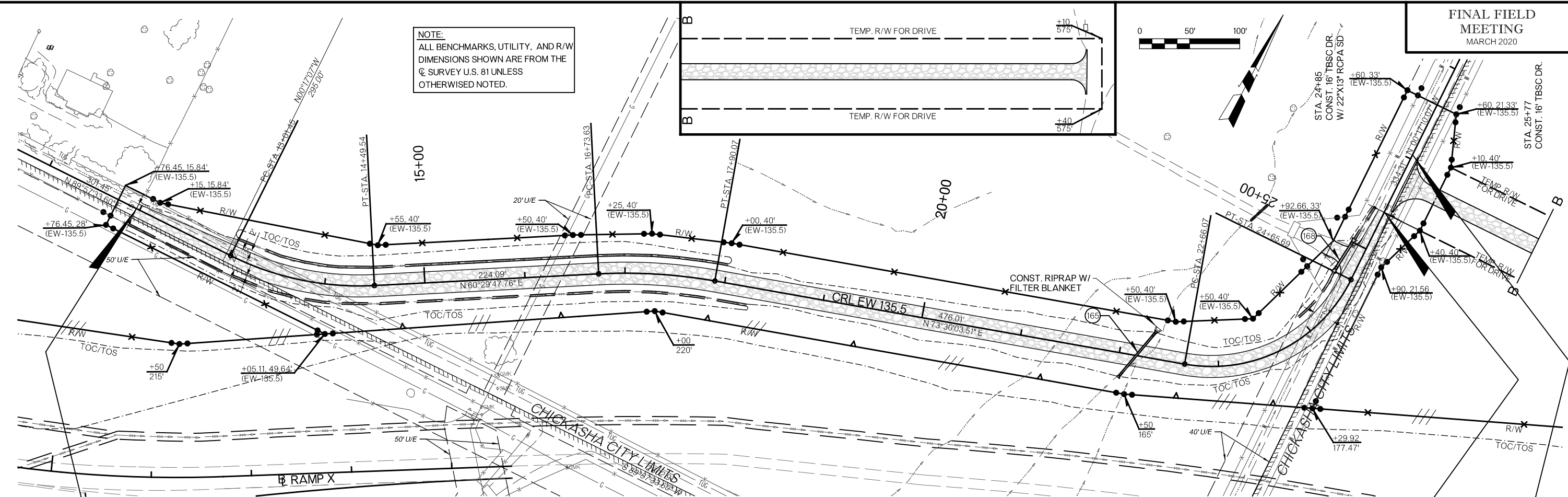
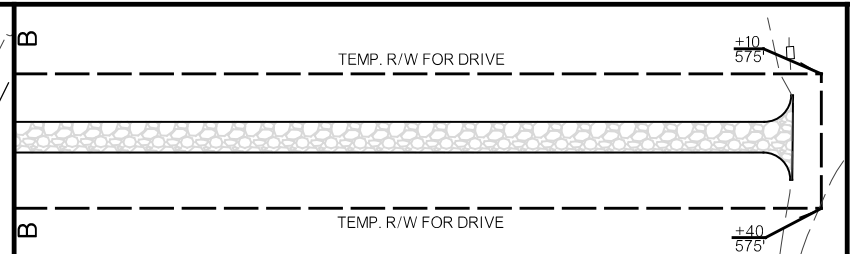
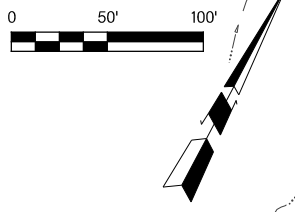


BM # 71 - SET 3/4" I.P. (30" LONG) -
- 1' UNDERGROUND
CRL IOWA AVE
STA. 128+02.43, 218.63' LT.
ELEV. = 1154.60

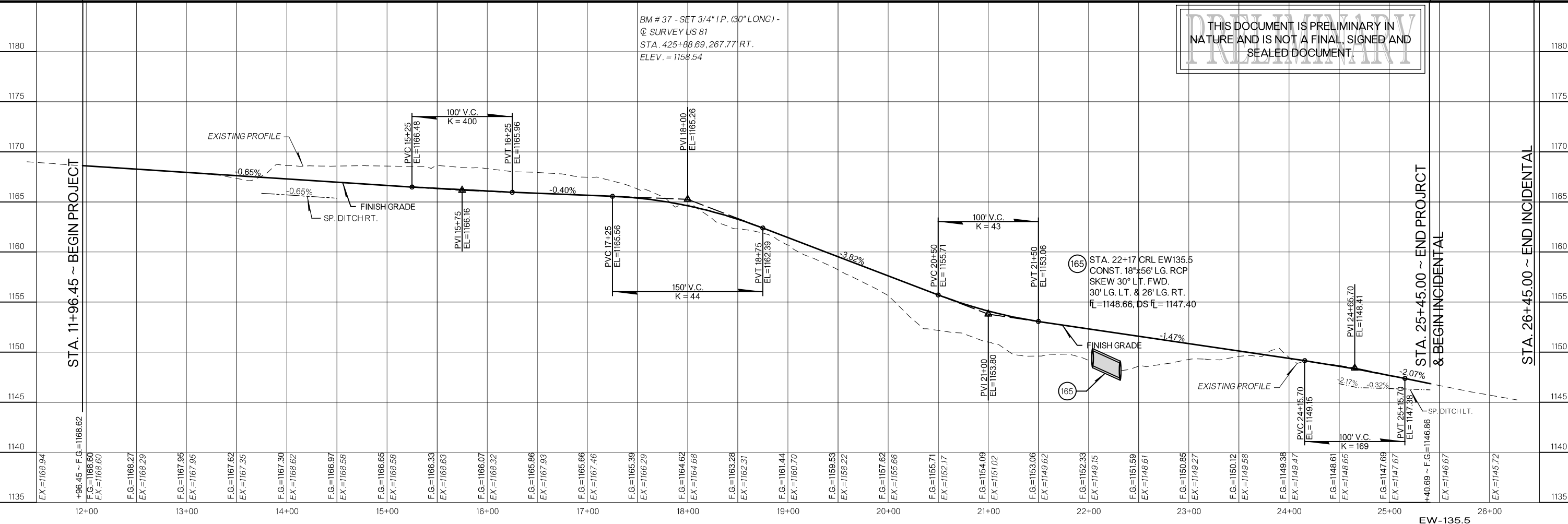
158 STA. 60+57 CRL IOWA AVE
CONST. 2-22"x13"x 78' LG. RCPA
39' LG. LT. & 39' LG. RT.
f_L = 1150.37, DS f_L = 1150.30

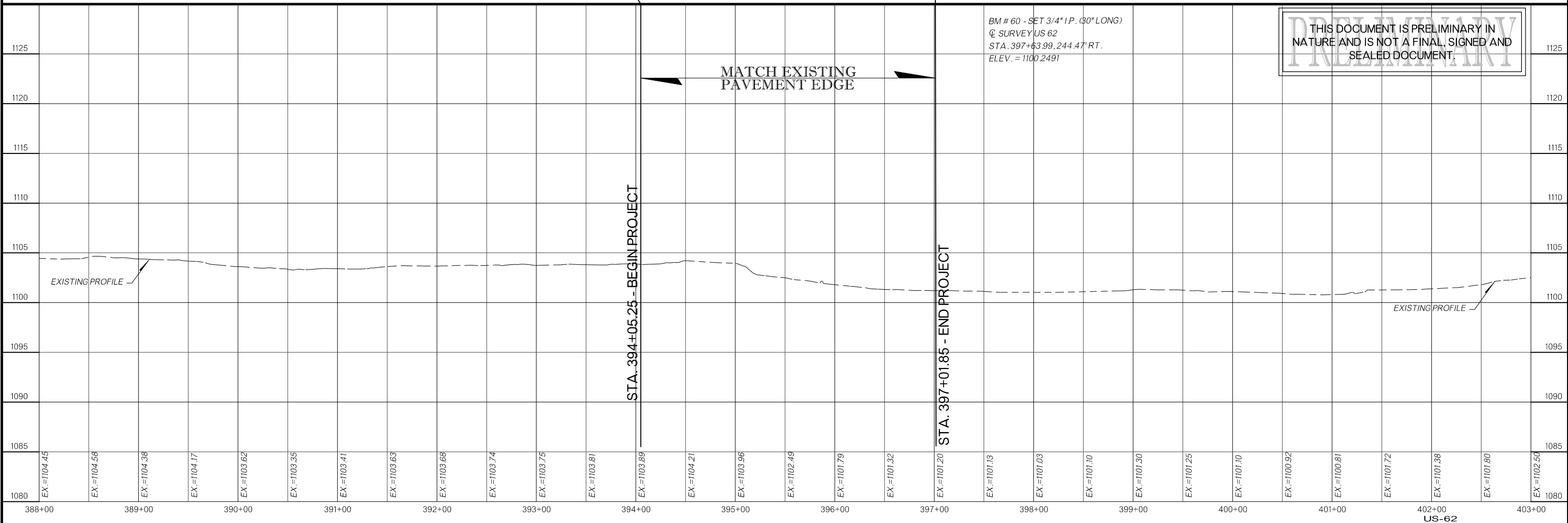
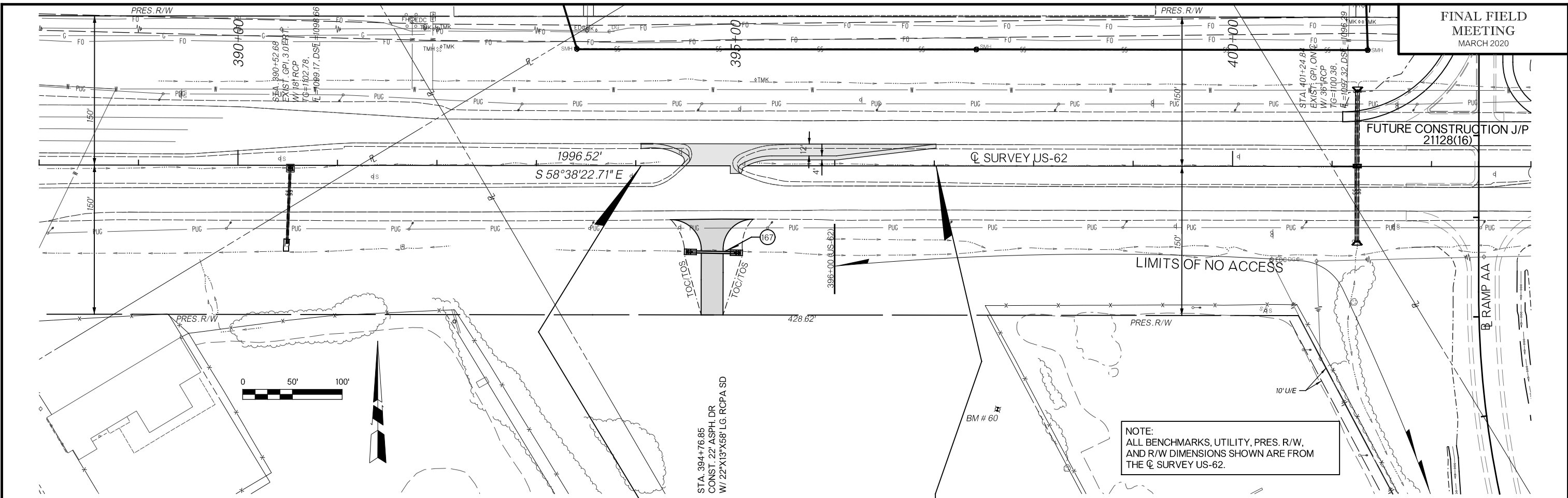
NS 281.5

NOTE:
ALL BENCHMARK, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE
C SURVEY U.S. 81 UNLESS OTHERWISE NOTED.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.





THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEC. 20 T6N R7W

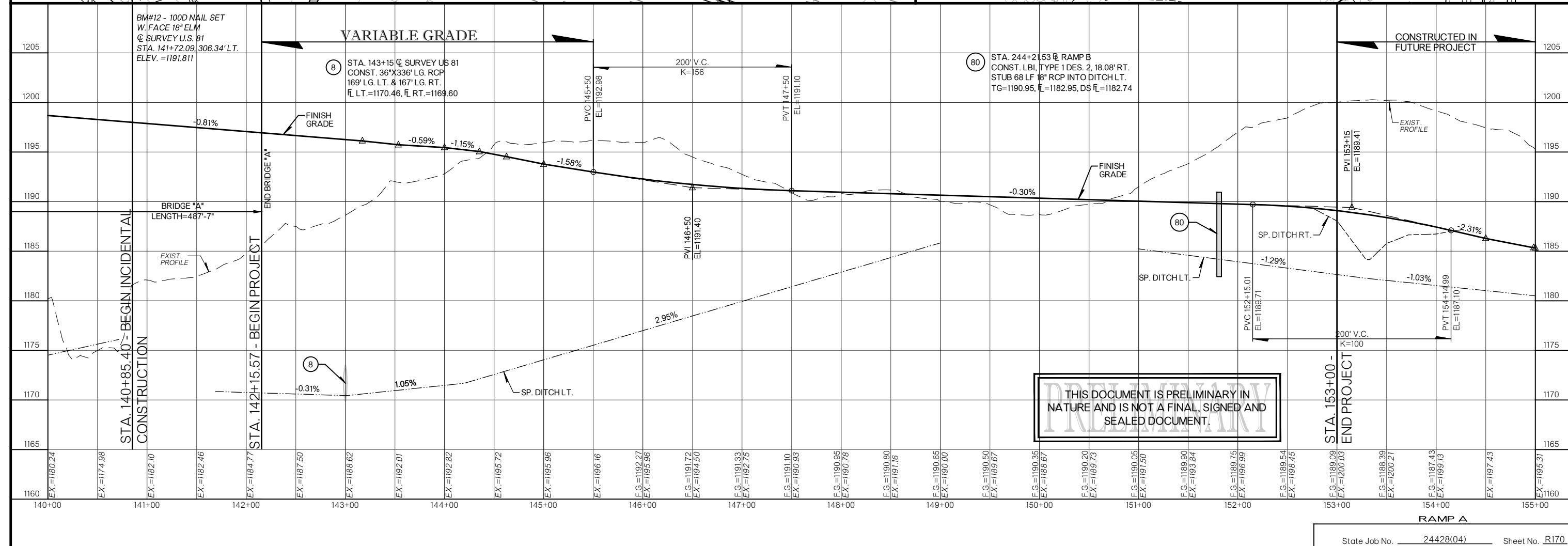
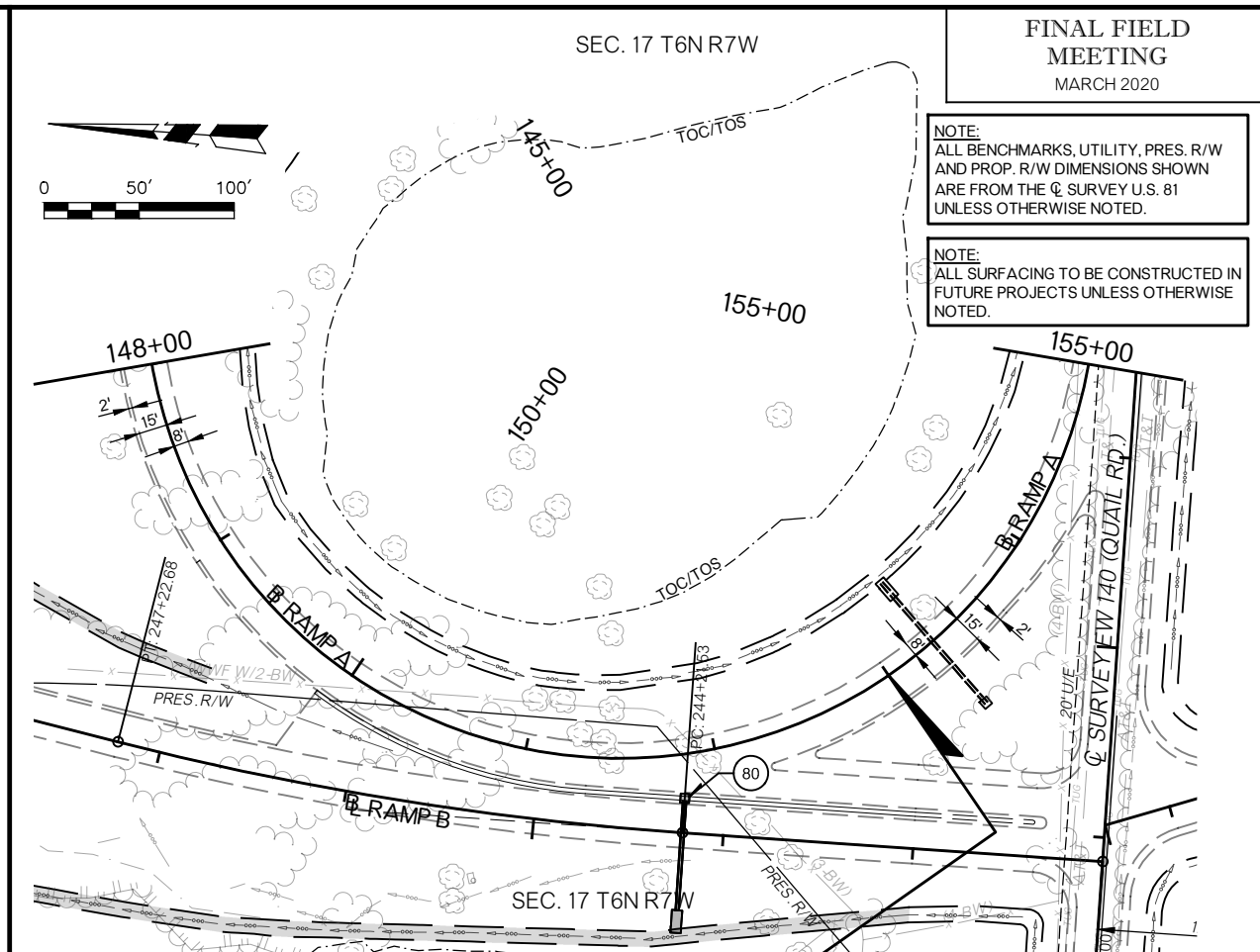
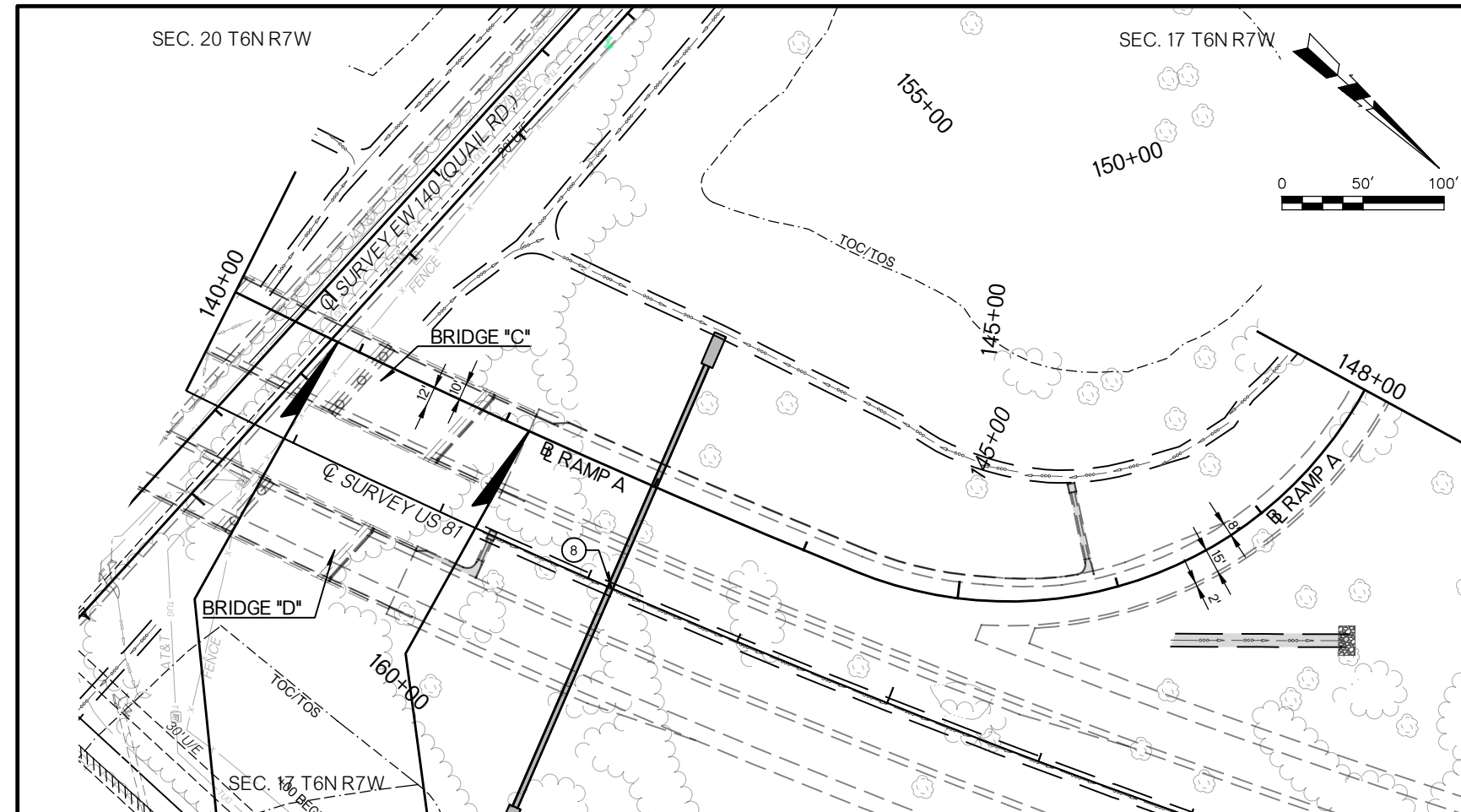
SEC. 17 T6N R7W

SEC. 17 T6N R7W

FINAL FIELD MEETING
MARCH 2020

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

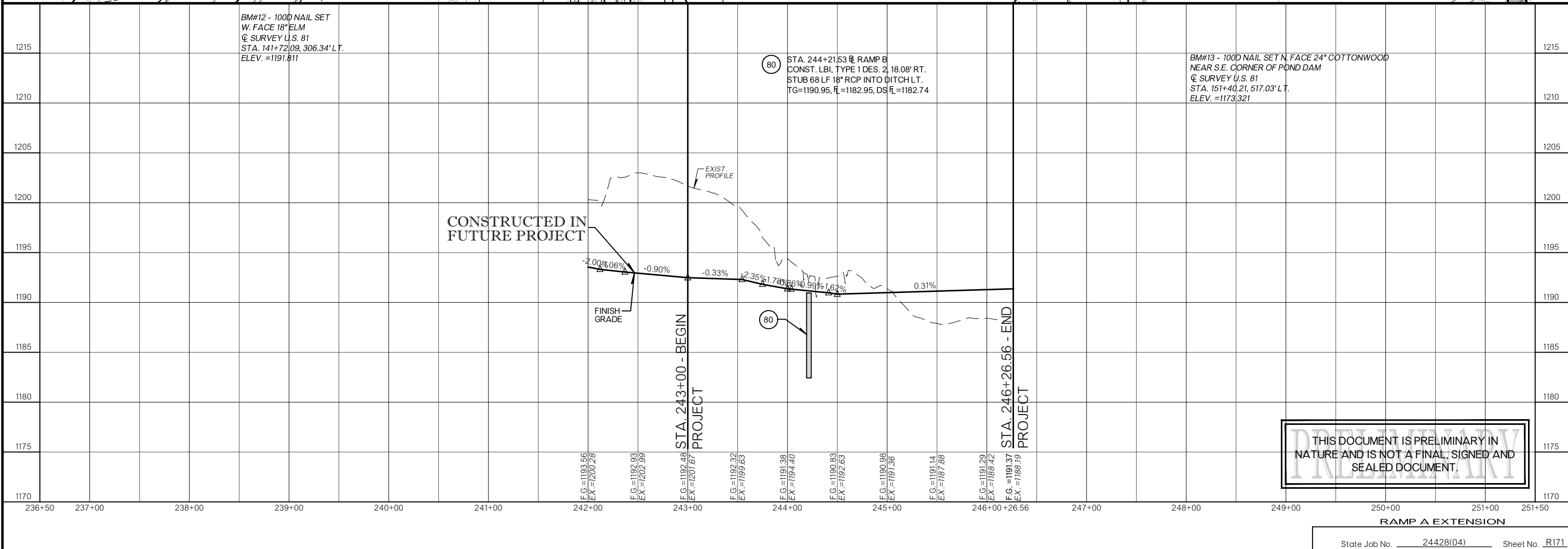
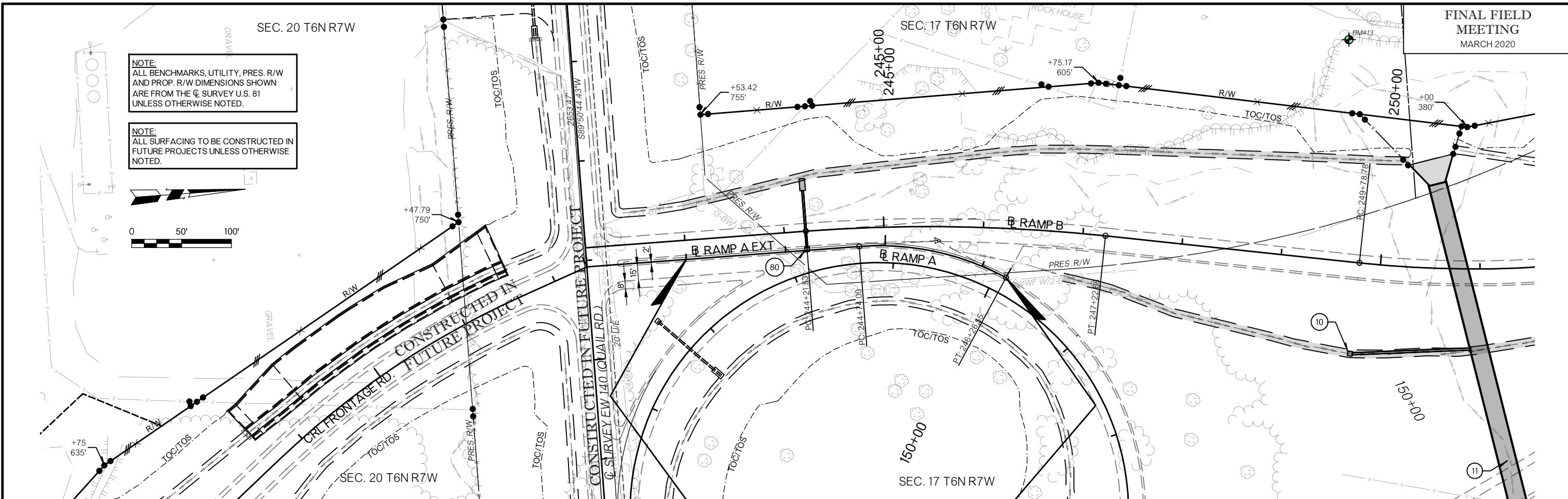
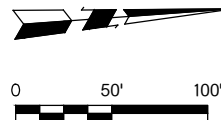
NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY U.S. 81
UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

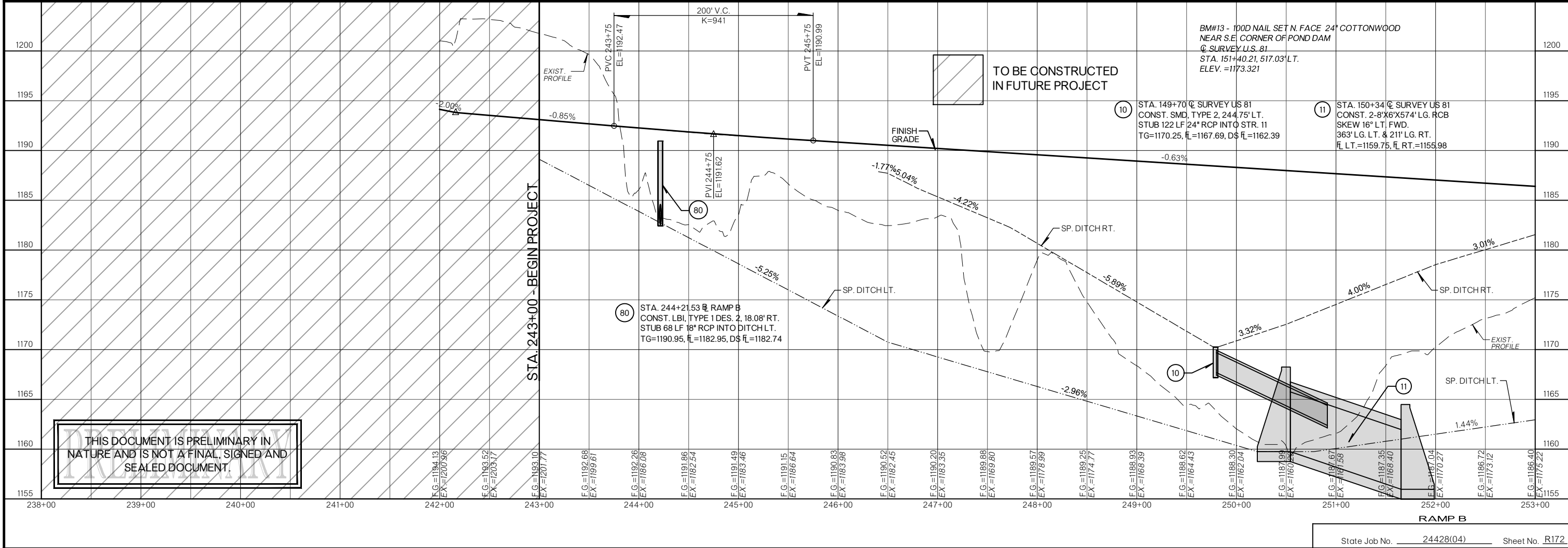
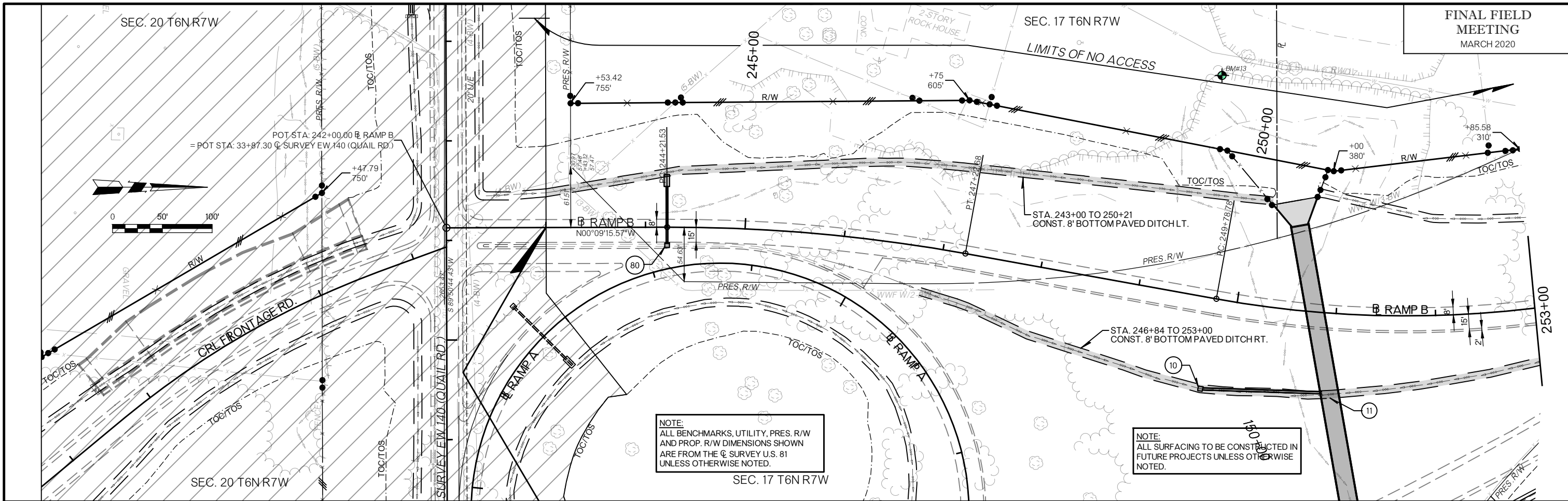


BM#12 - 100D NAIL SET
W. FACE 18" ELM
Q SURVEY U.S. 81
STA. 141+72.09, 306.34' LT.
ELEV. =1191.811

80 STA. 244+21.53 RAMP B
CONST. LBI, TYPE 1 DES. 2, 18.08' RT.
STUB 68 LF 18" RCP INTO DITCH LT.
TG=1190.95, FL=1182.95, DS FL=1182.74

BM#13 - 100D NAIL SET N. FACE 24" COTTONWOOD
NEAR S.E. CORNER OF POND DAM
Q SURVEY U.S. 81
STA. 151+40.21, 517.03' LT.
ELEV. =1173.321

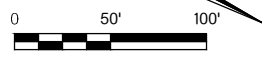
THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.



SEC. 17 T6N R7W

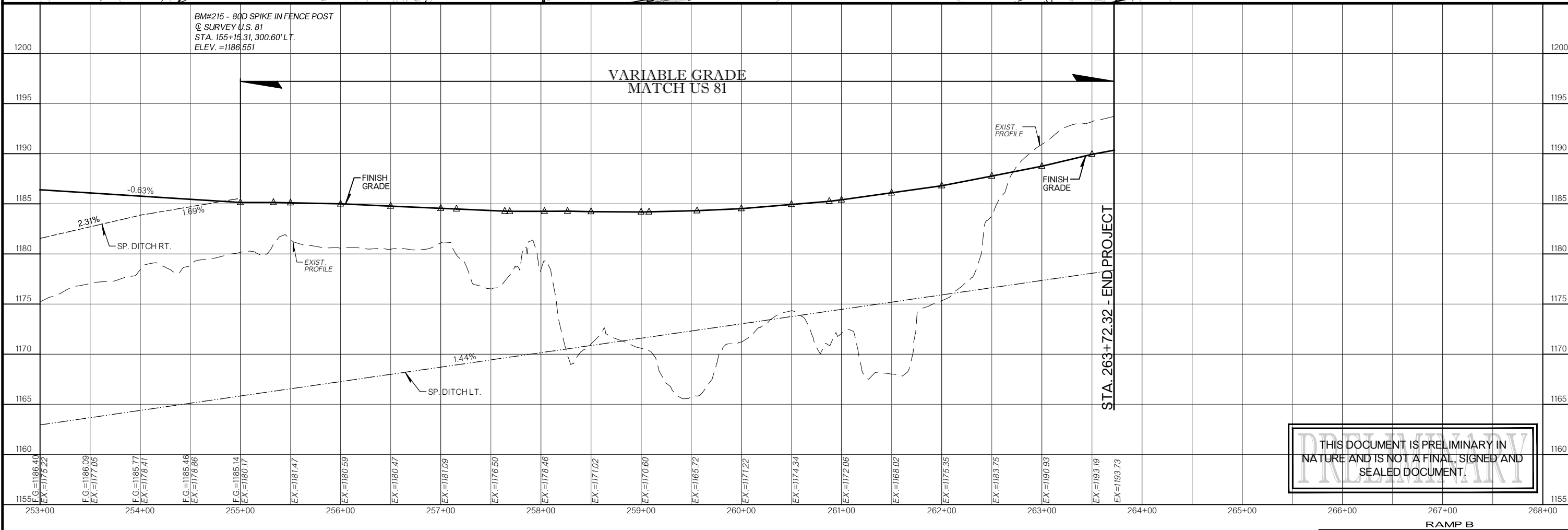
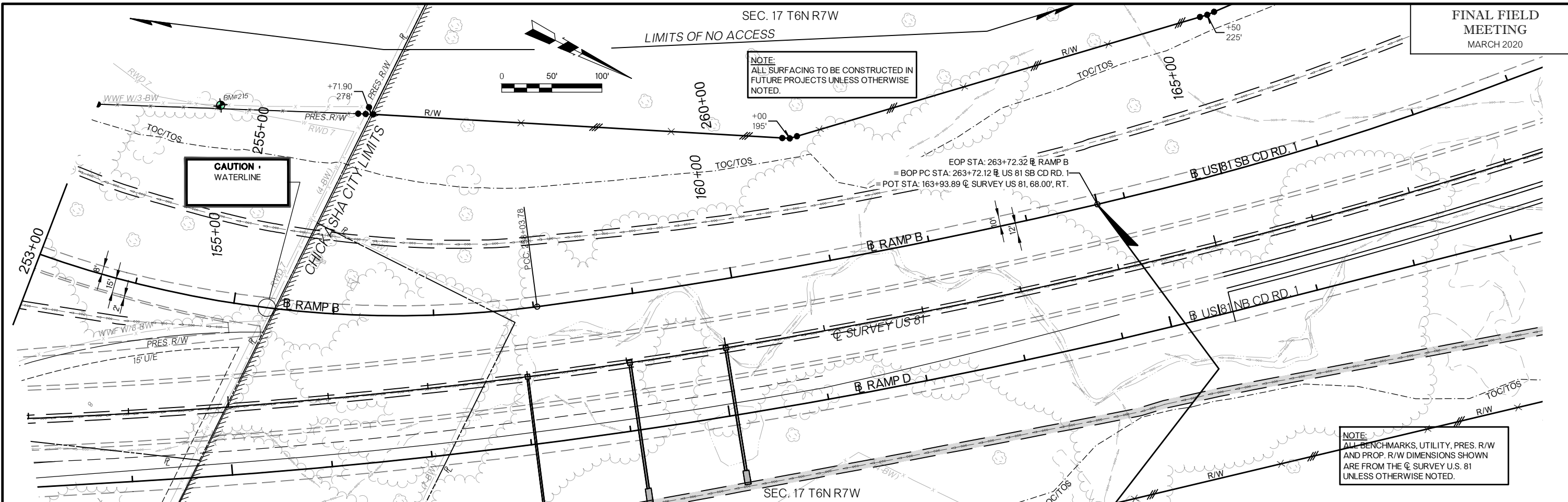
LIMITS OF NO ACCESS

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.



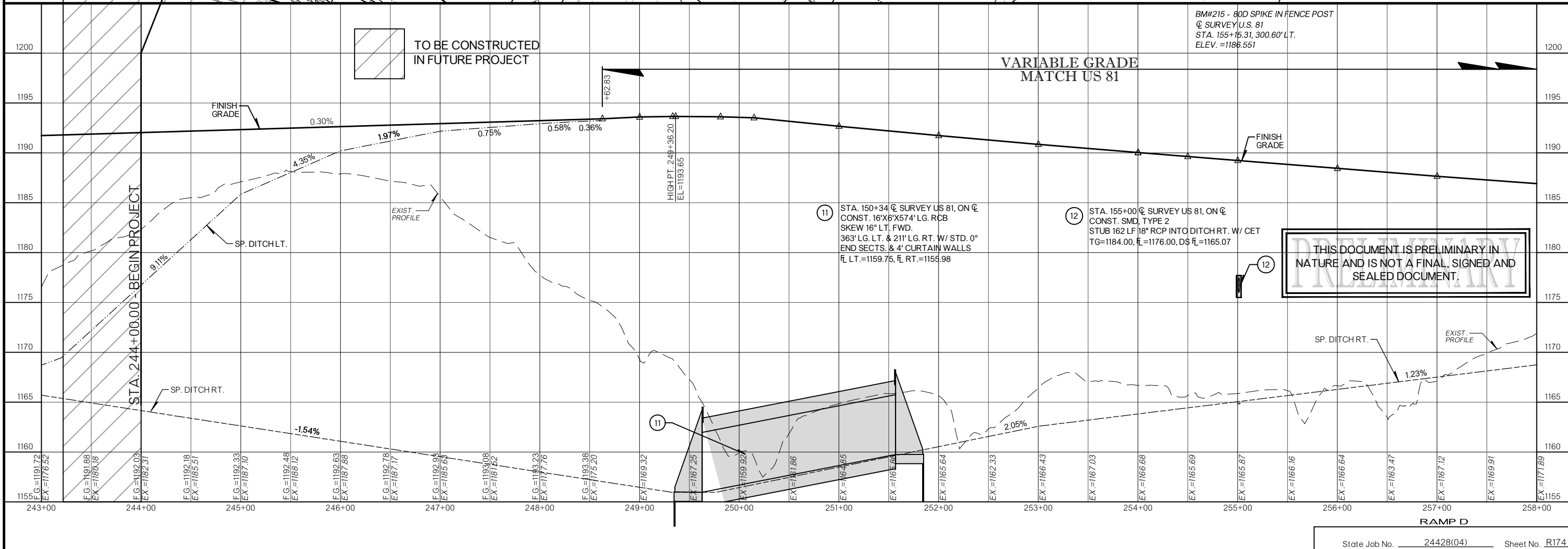
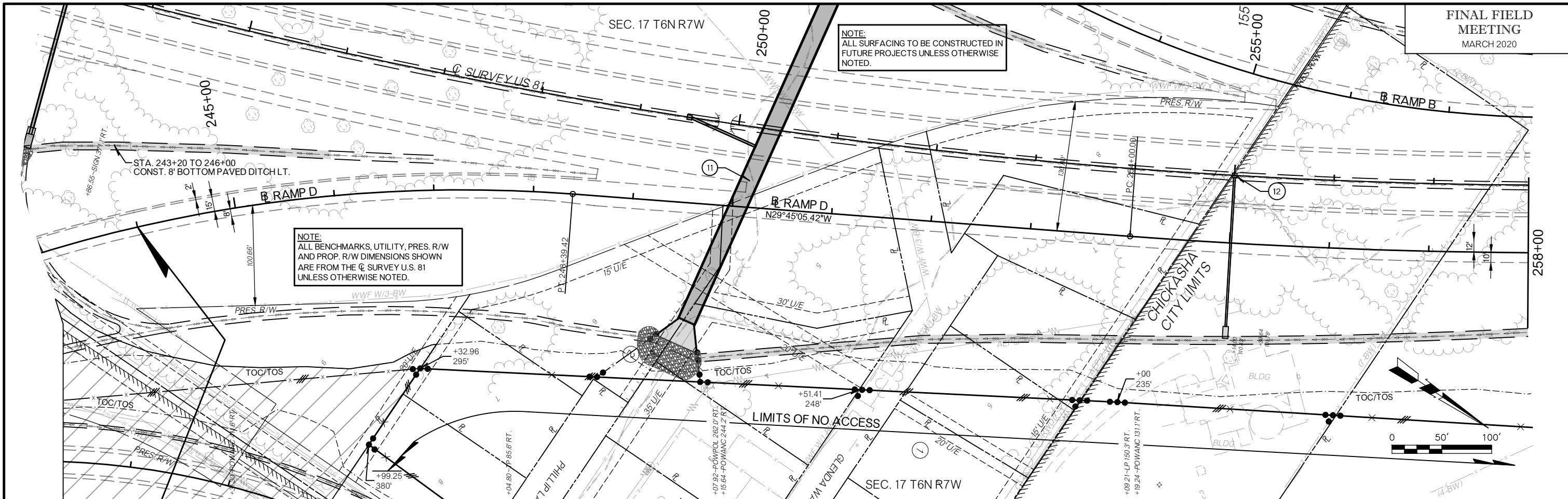
CAUTION
WATERLINE

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE C SURVEY U.S. 81 UNLESS OTHERWISE NOTED.



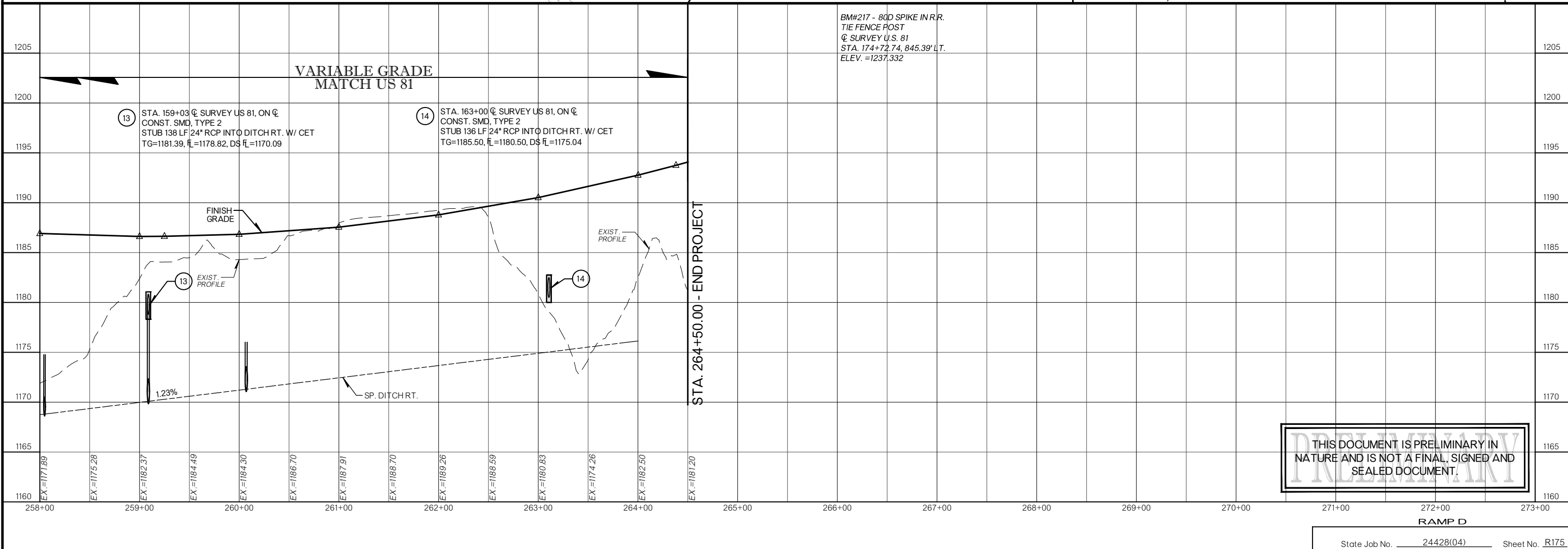
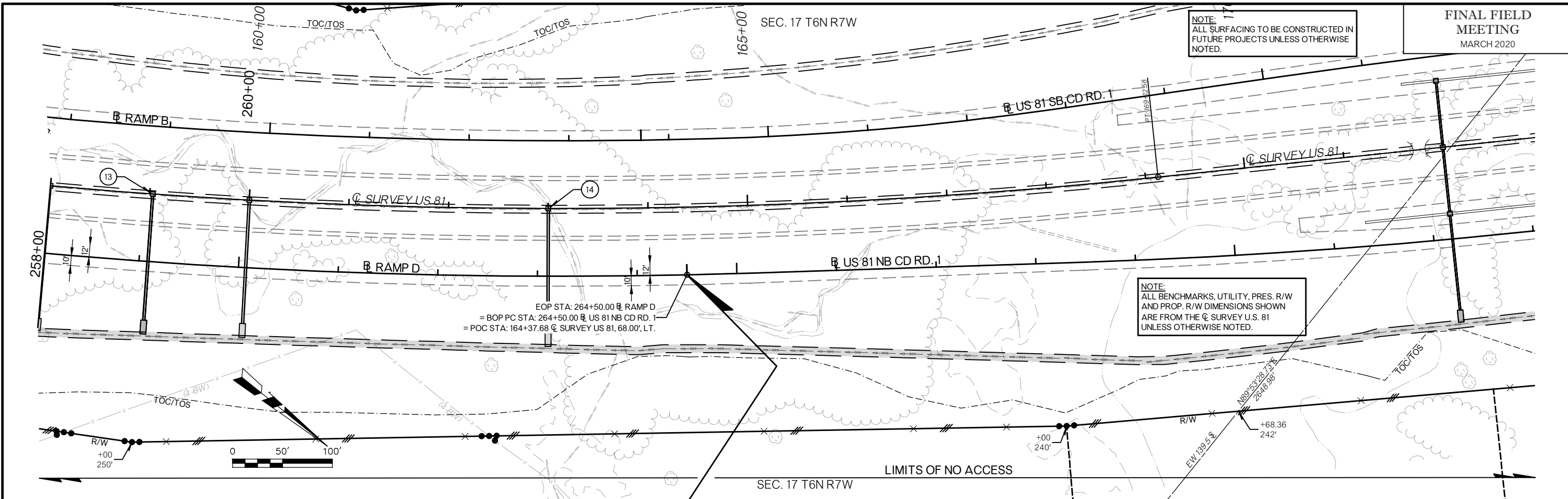
PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADY COUNTY
US 81 REALIGNMENT



NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.



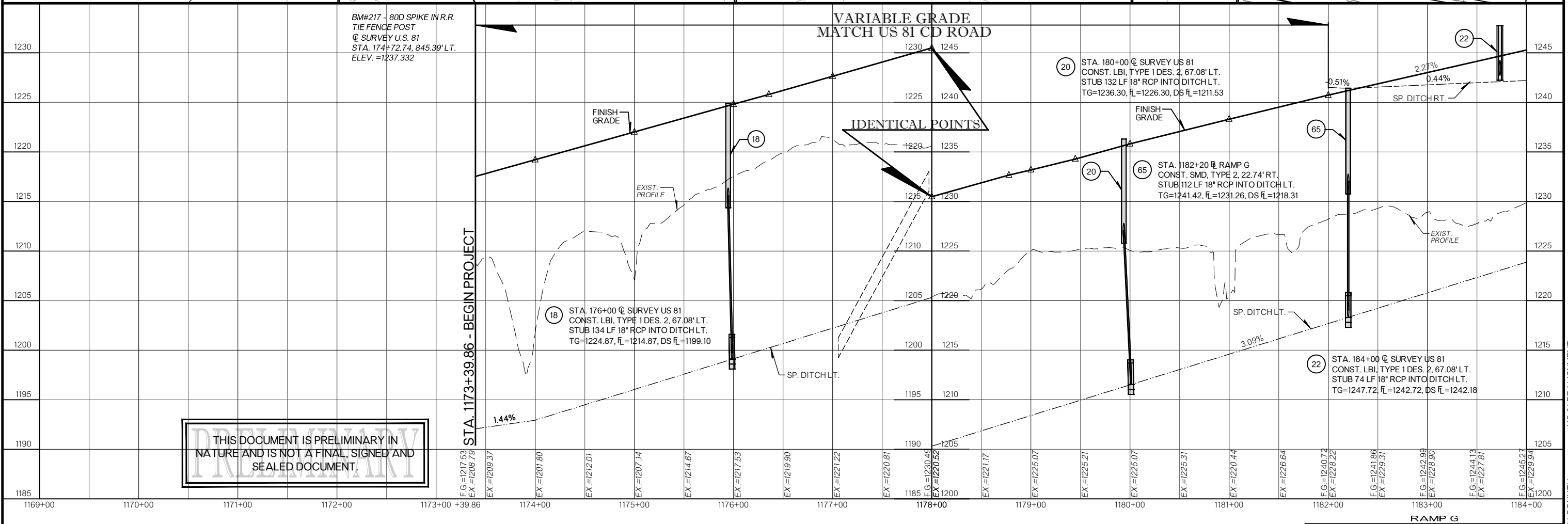
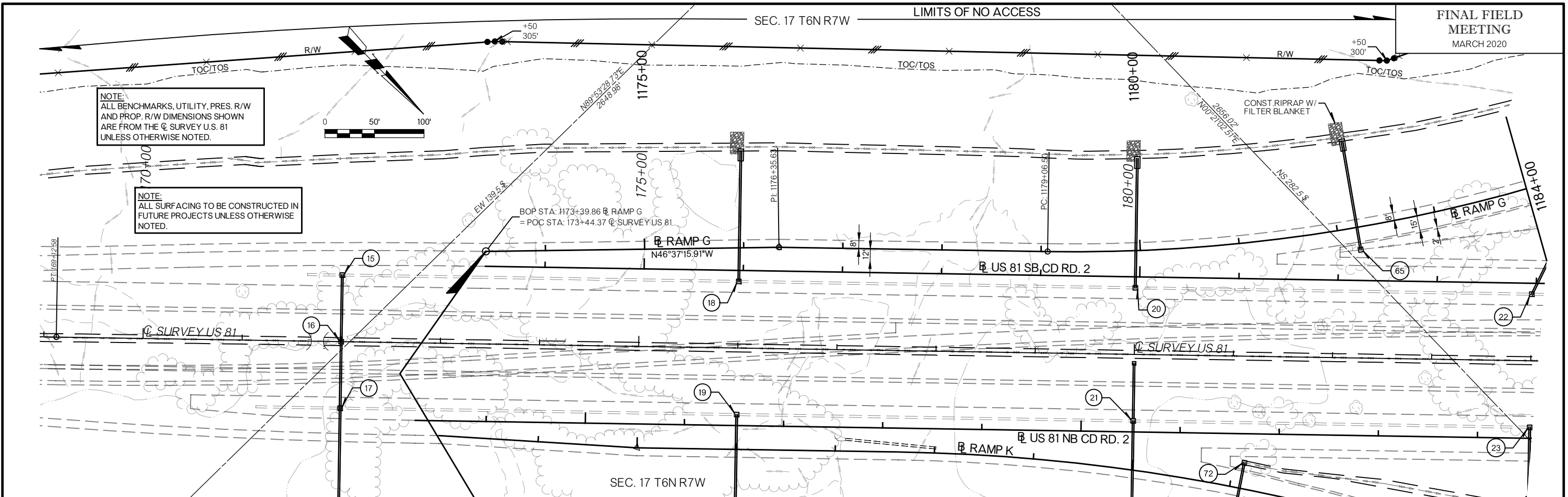
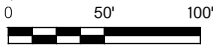
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

RAMP D

GRADY COUNTY US 81 REALIGNMENT

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY U.S. 81
UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

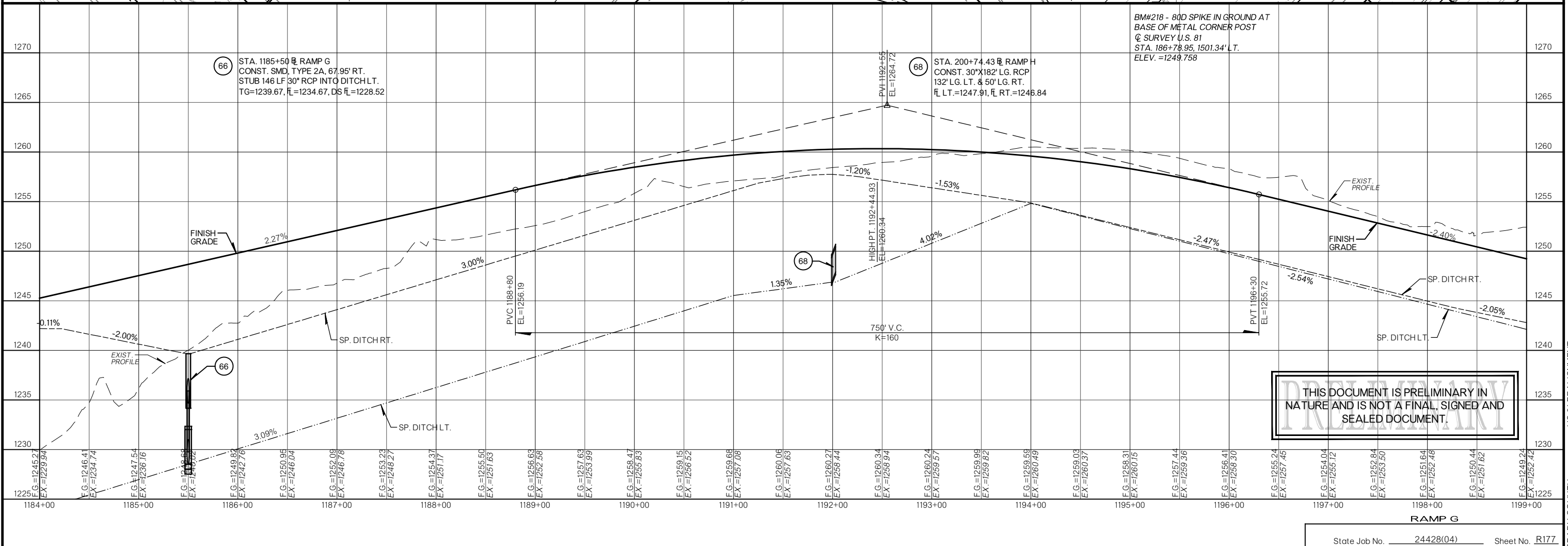
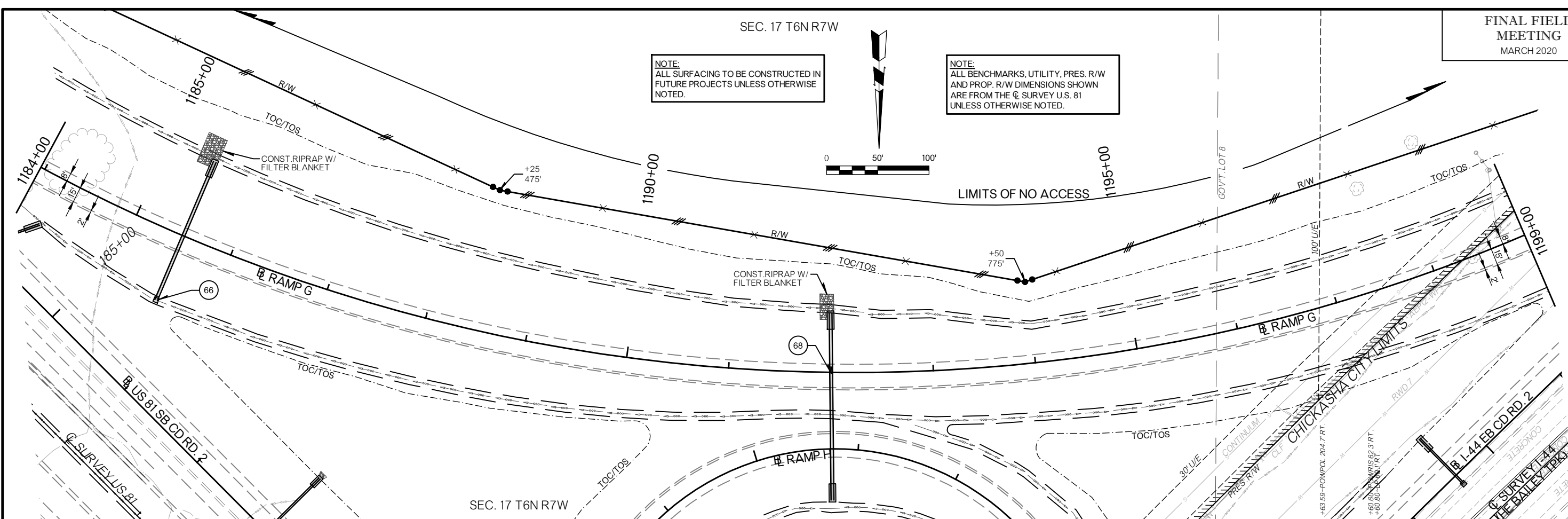
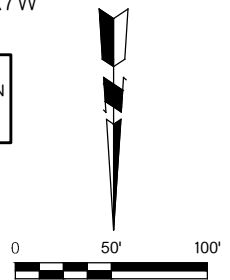


THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

SEC. 17 T6N R7W

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE \odot SURVEY U.S. 81 UNLESS OTHERWISE NOTED.



66 STA. 1185+50 RAMP G
CONST. SMD, TYPE 2A, 67.95' RT.
STUB 146 LF 30" RCP INTO DITCH LT.
TG=1239.67, FL=1234.67, DS FL=1228.52

68 STA. 200+74.43 RAMP H
CONST. 30"x182' LG. RCP
132' LG. LT. & 50' LG. RT.
FL LT.=1247.91, FL RT.=1246.84

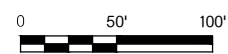
BM#218 - 80D SPIKE IN GROUND AT
BASE OF METAL CORNER POST
 \odot SURVEY U.S. 81
STA. 186+78.95, 1501.34' LT.
ELEV. =1249.758

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

RAMP G

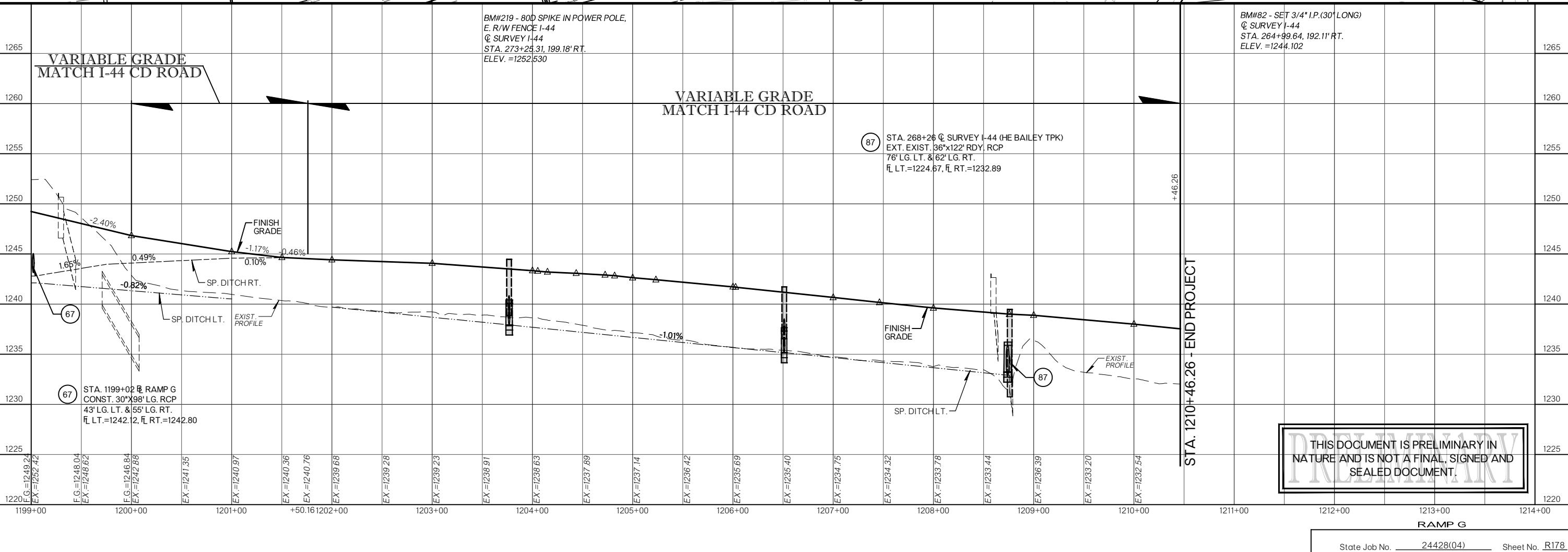
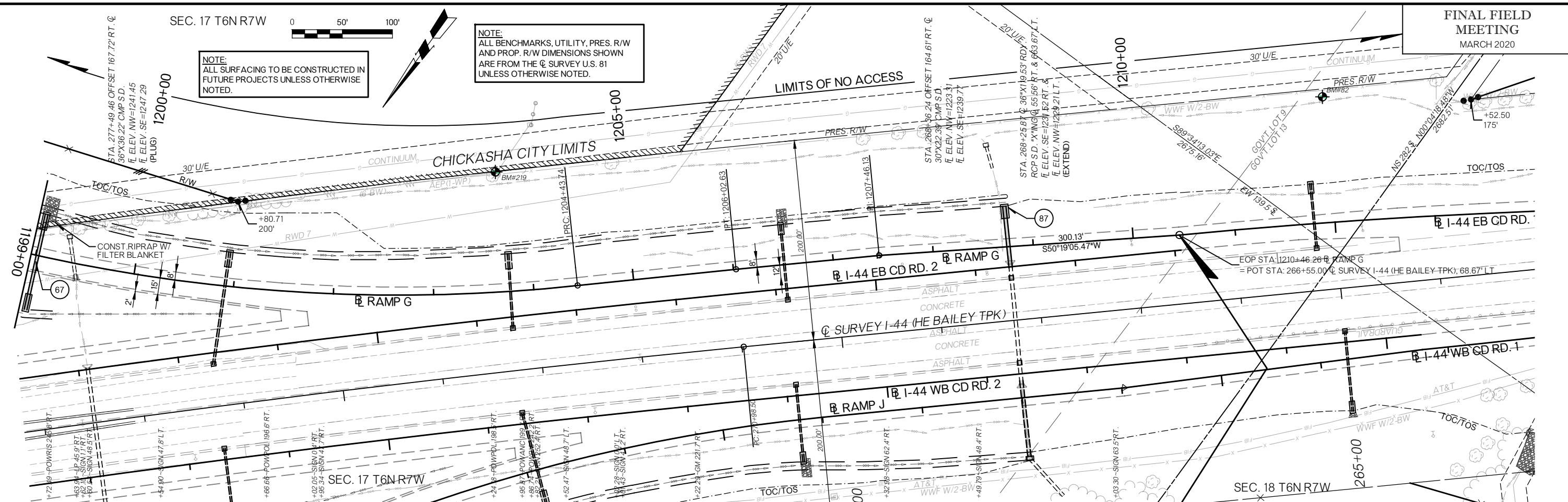
GRADY COUNTY US 81 REALIGNMENT

SEC. 17 T6N R7W



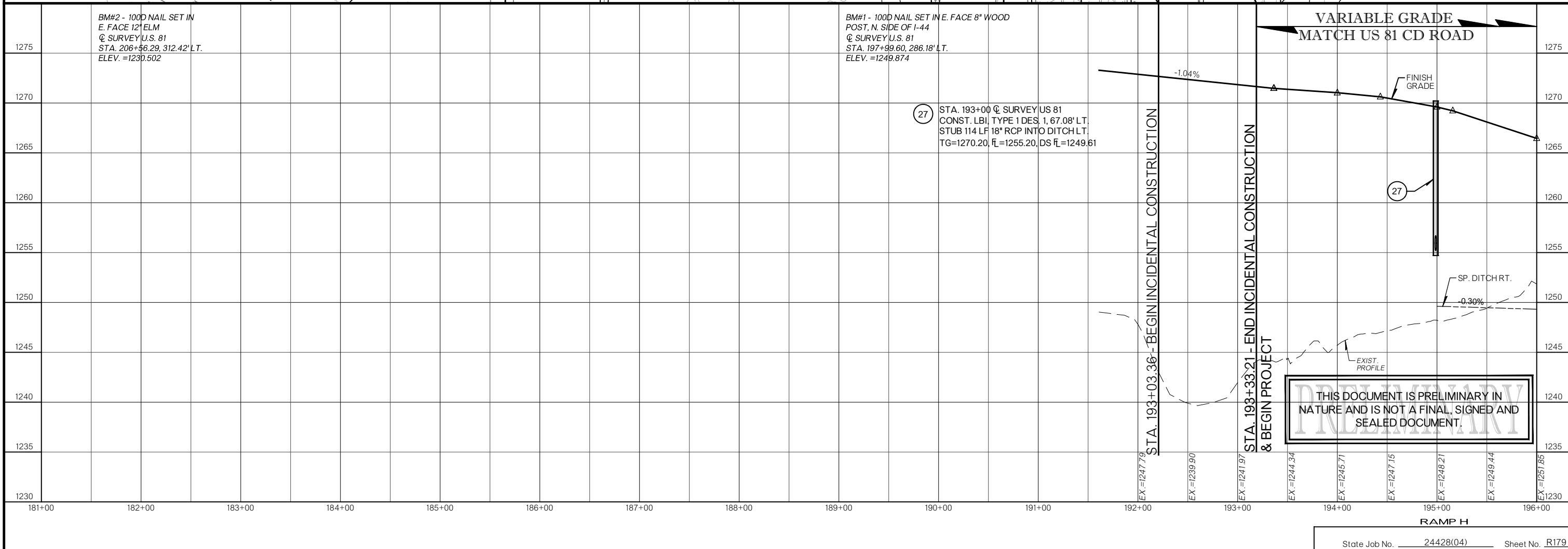
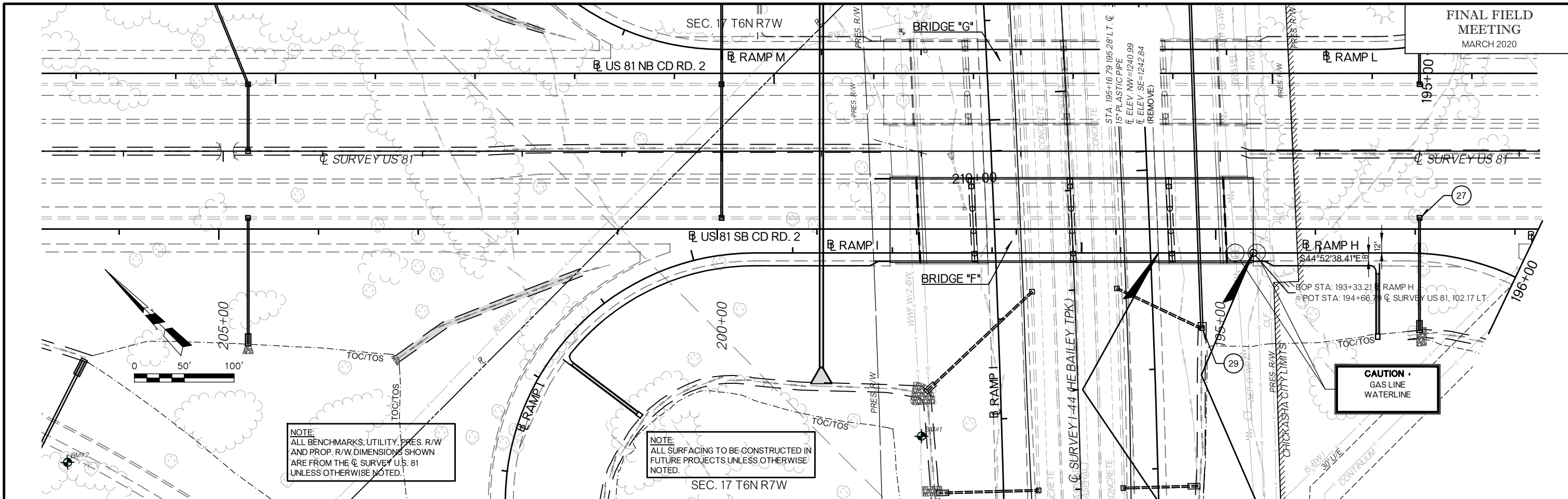
NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

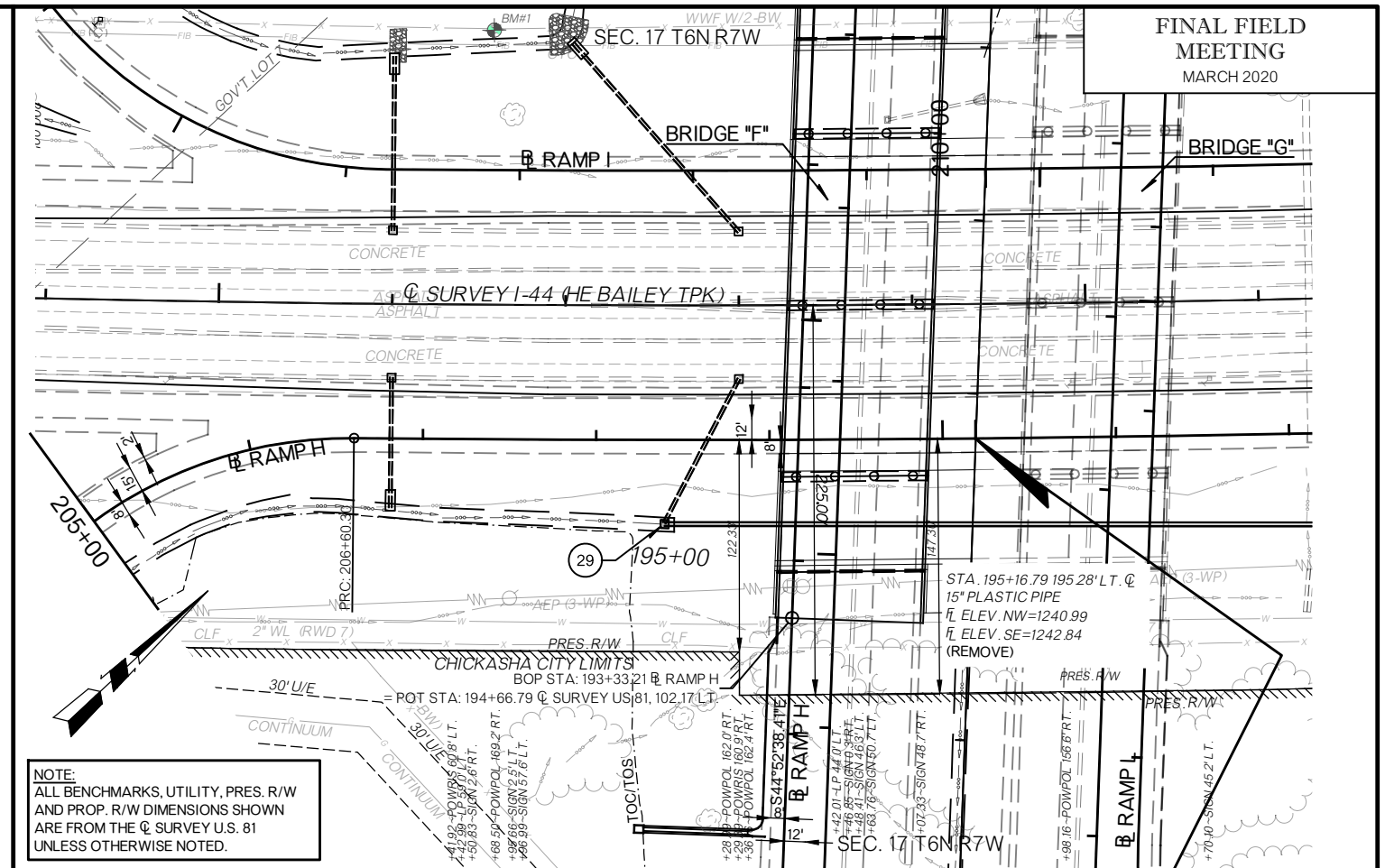
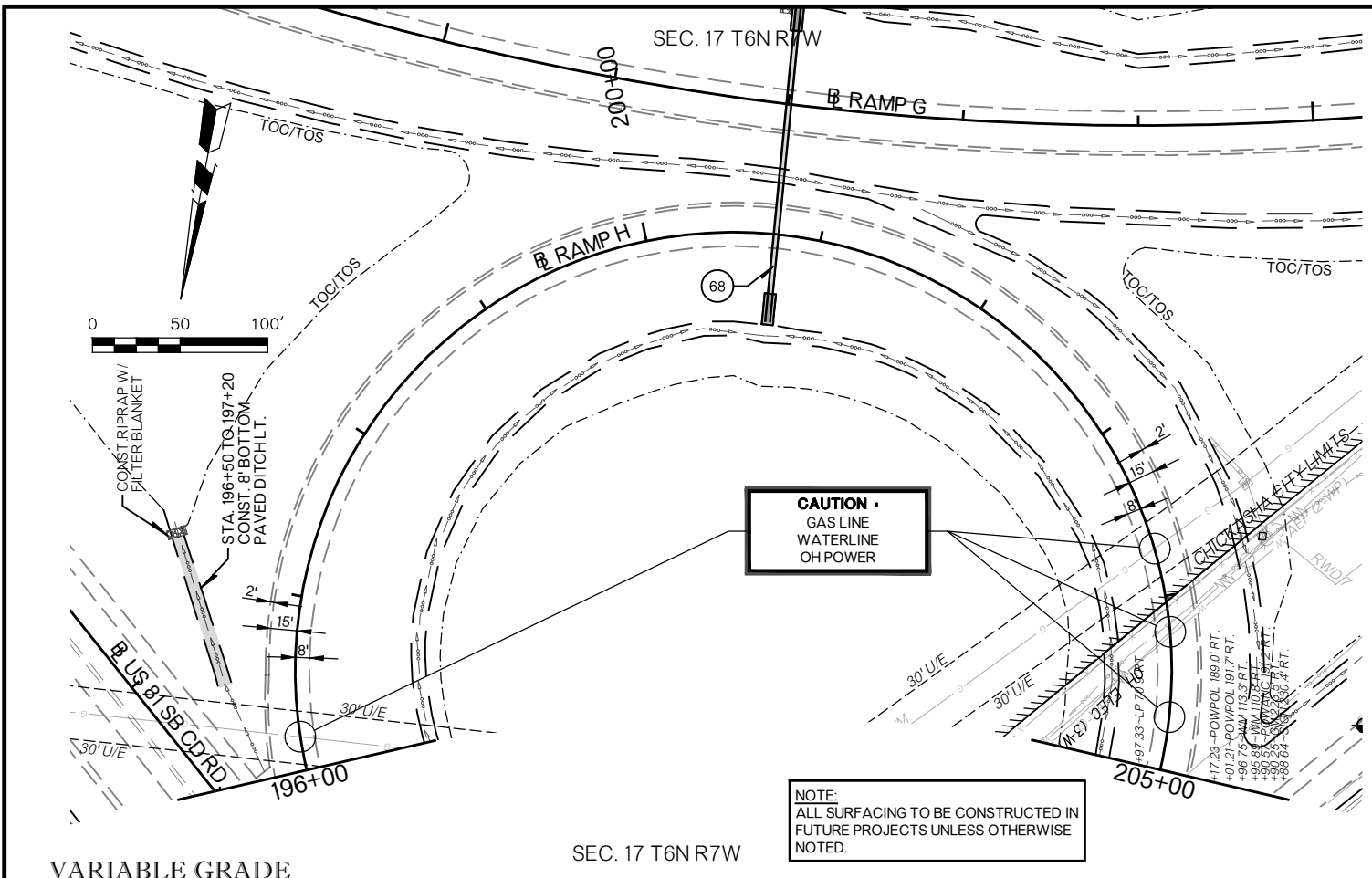
NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.



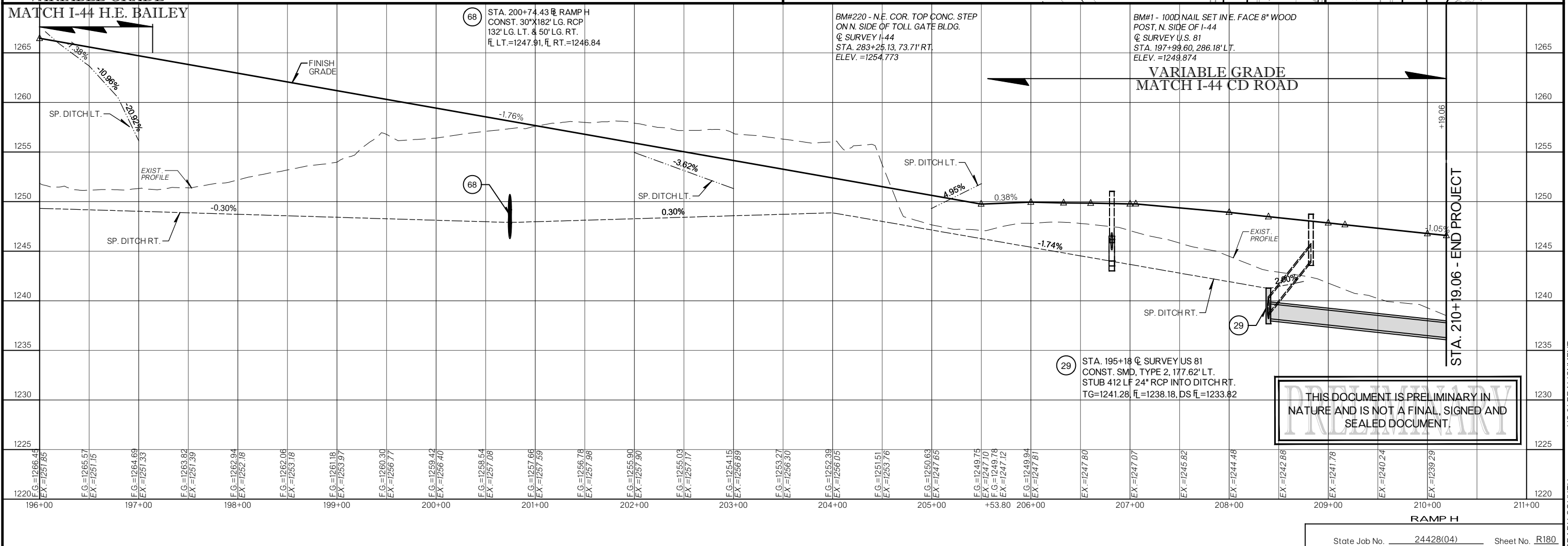
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADY COUNTY US 81 REALIGNMENT



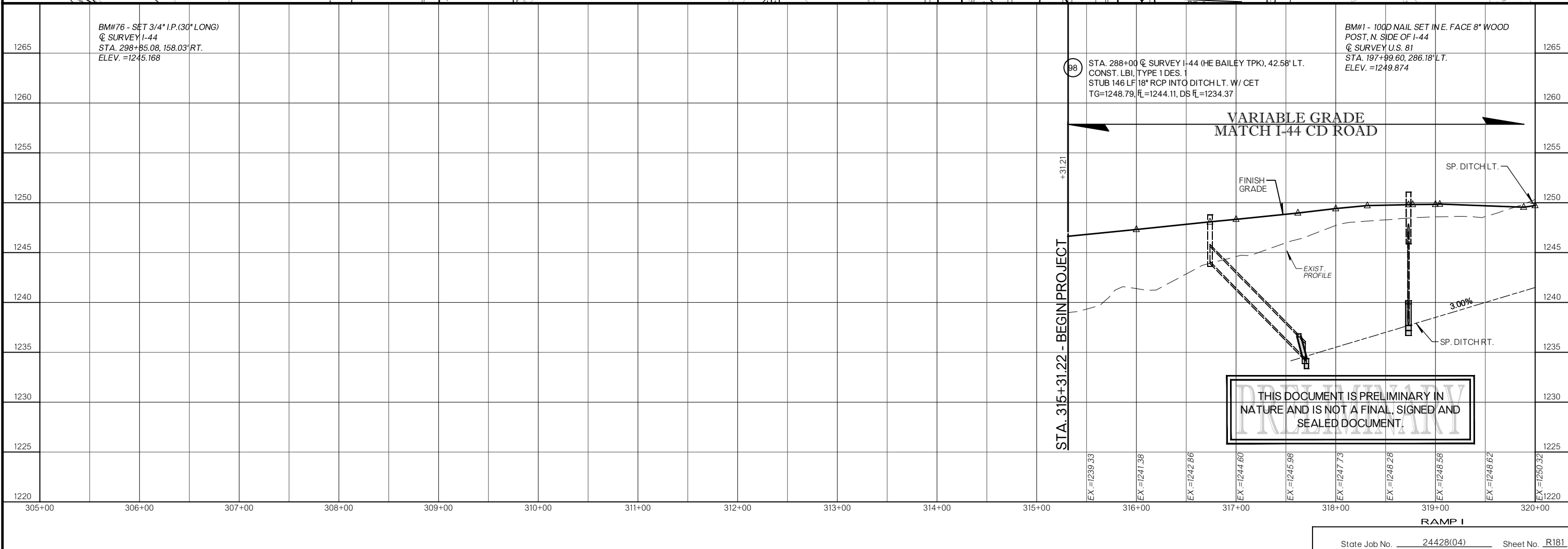
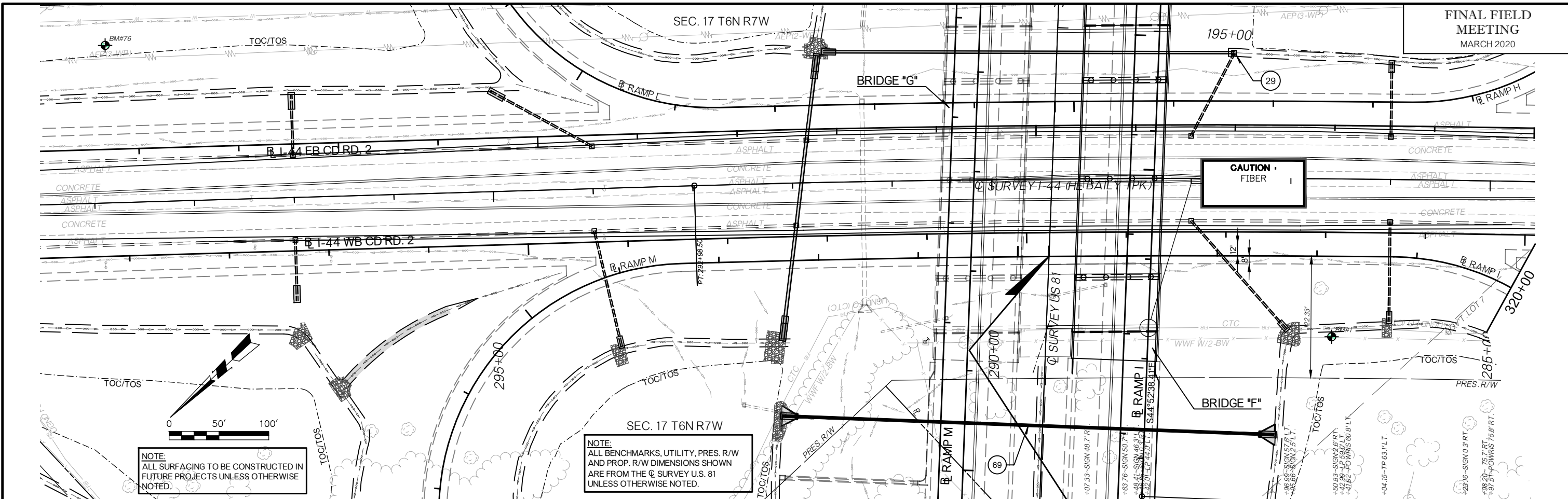


FINAL FIELD MEETING
MARCH 2020

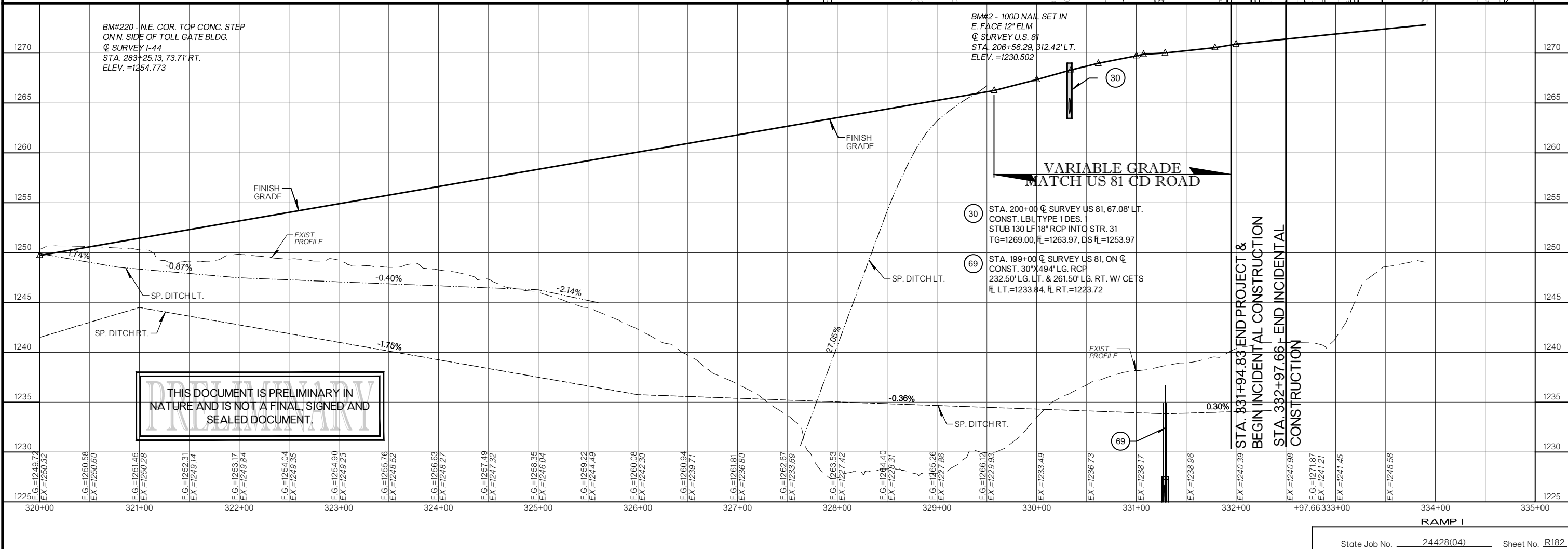
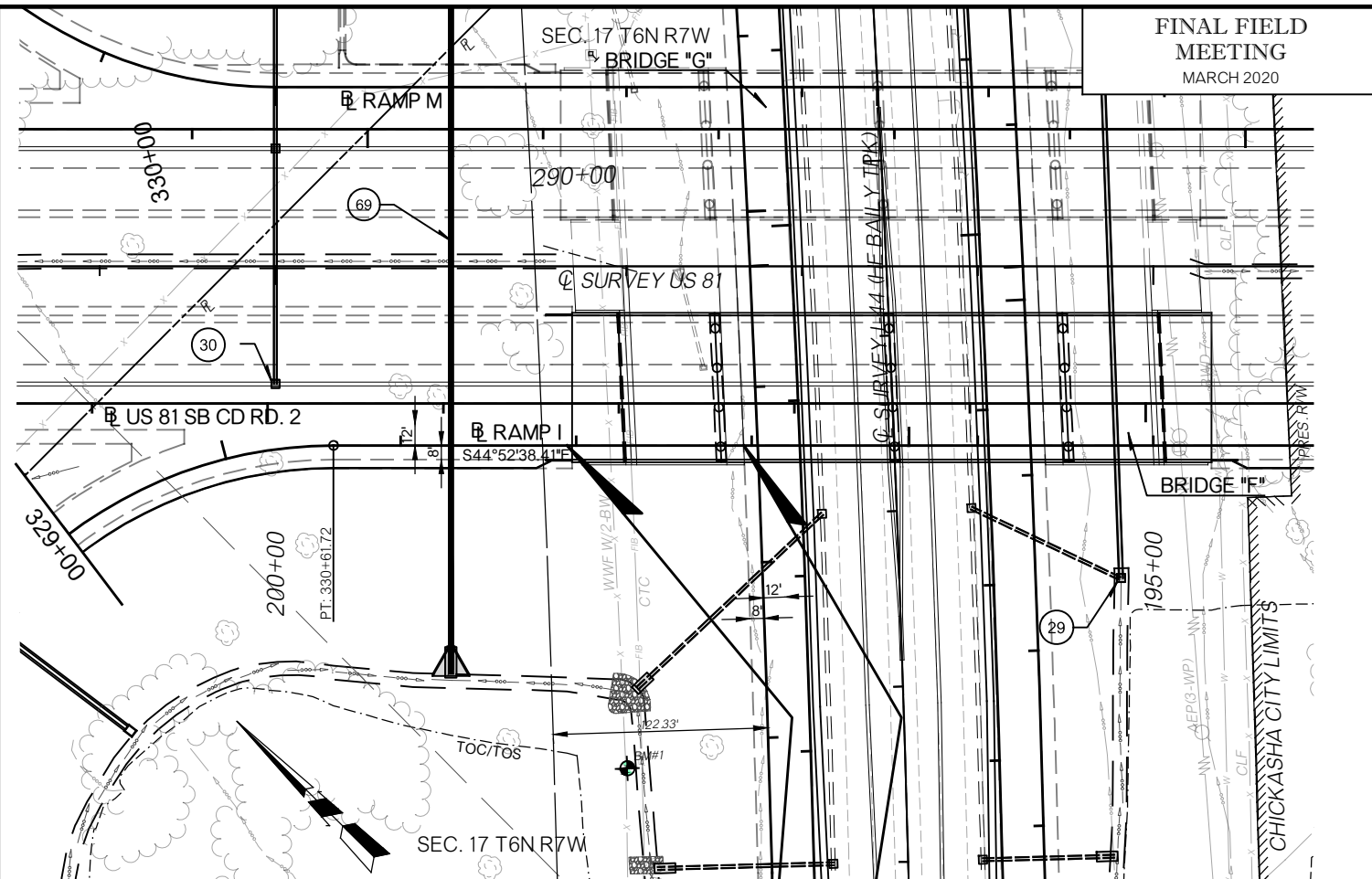
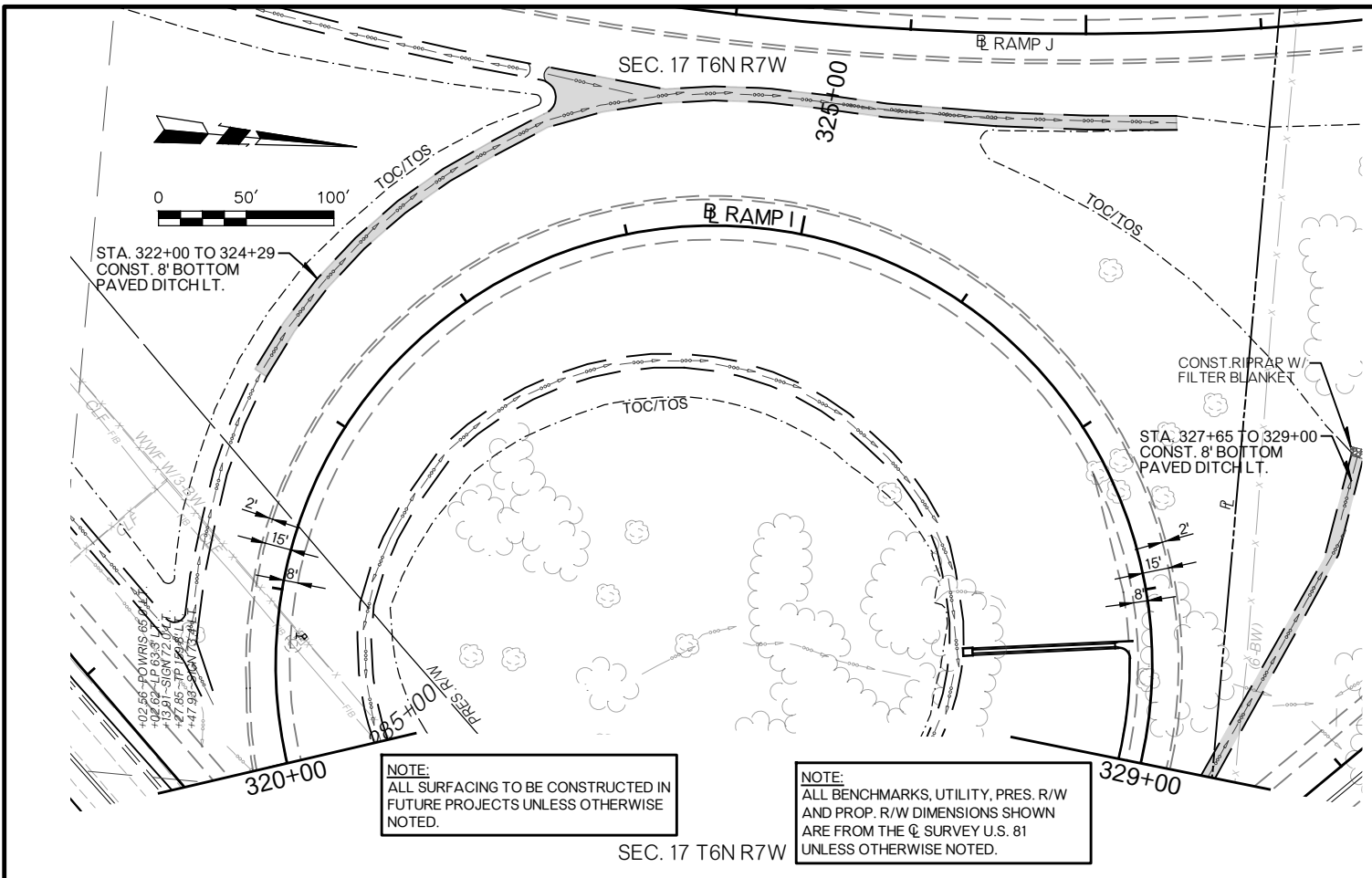


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

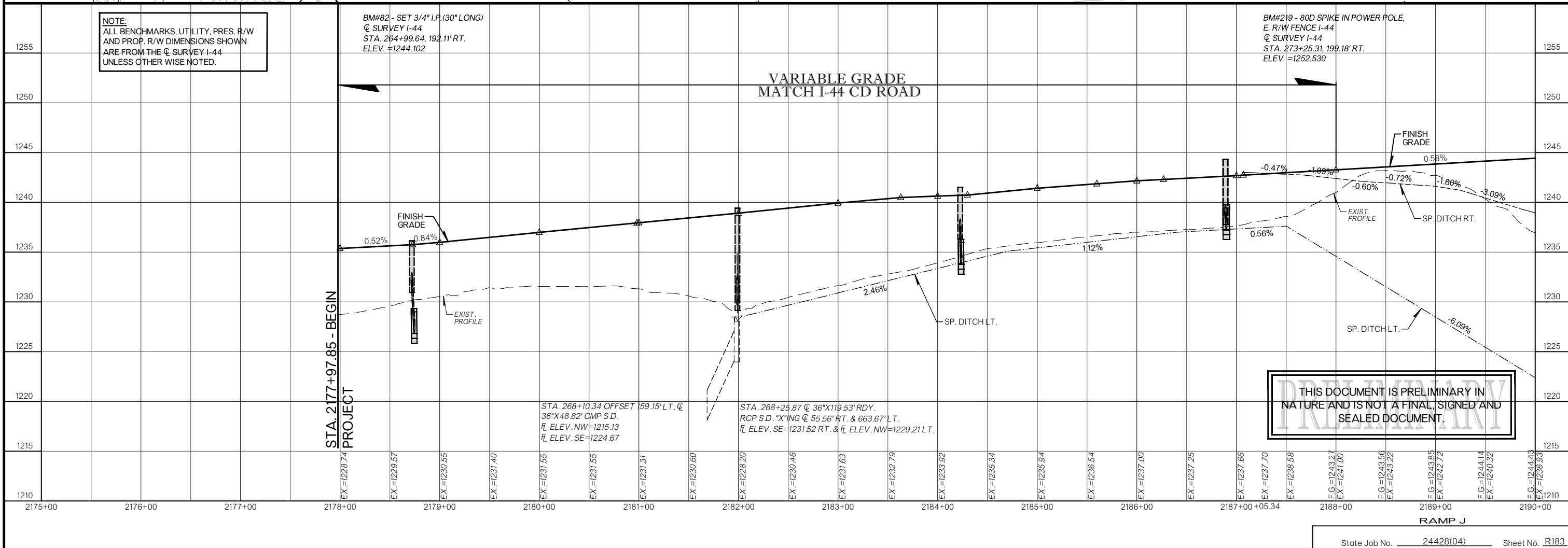
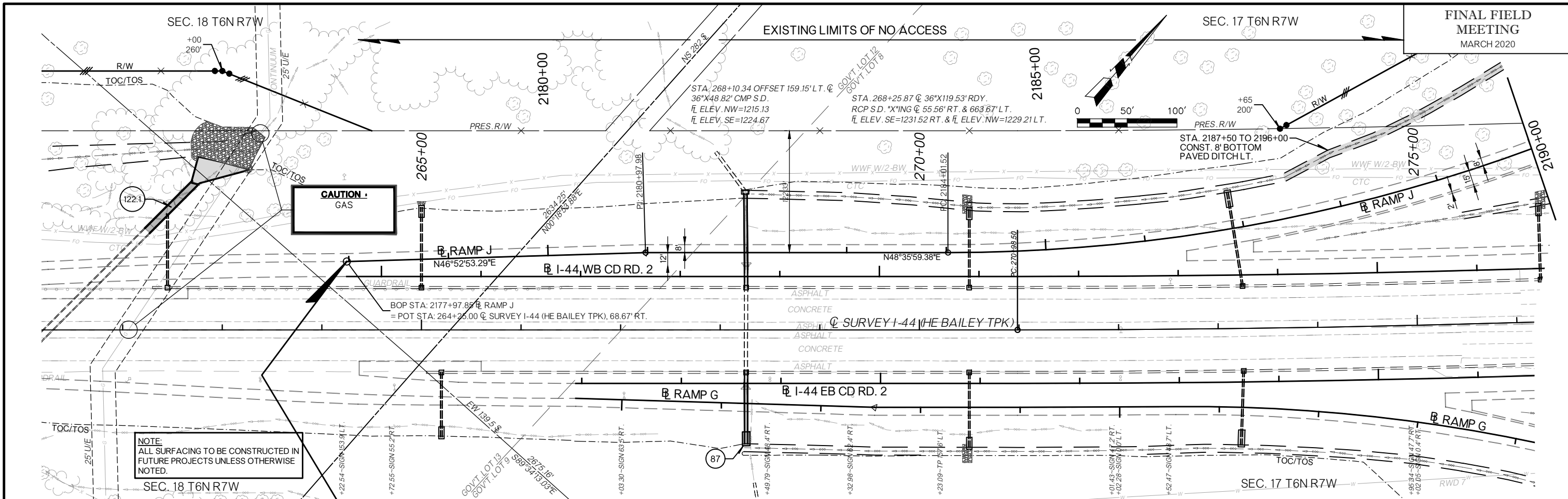
GRADY COUNTY US 81 REALIGNMENT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



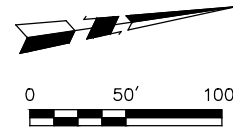
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADY COUNTY US 81 REALIGNMENT

SEC. 17 T6N R7W

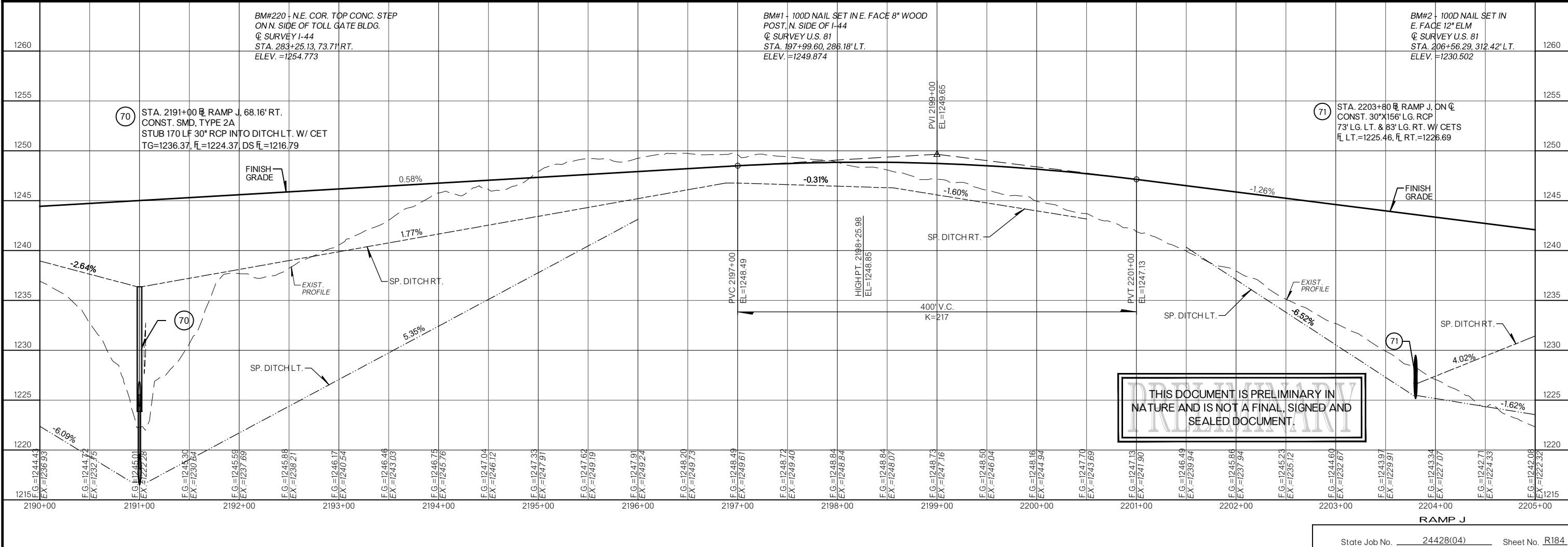
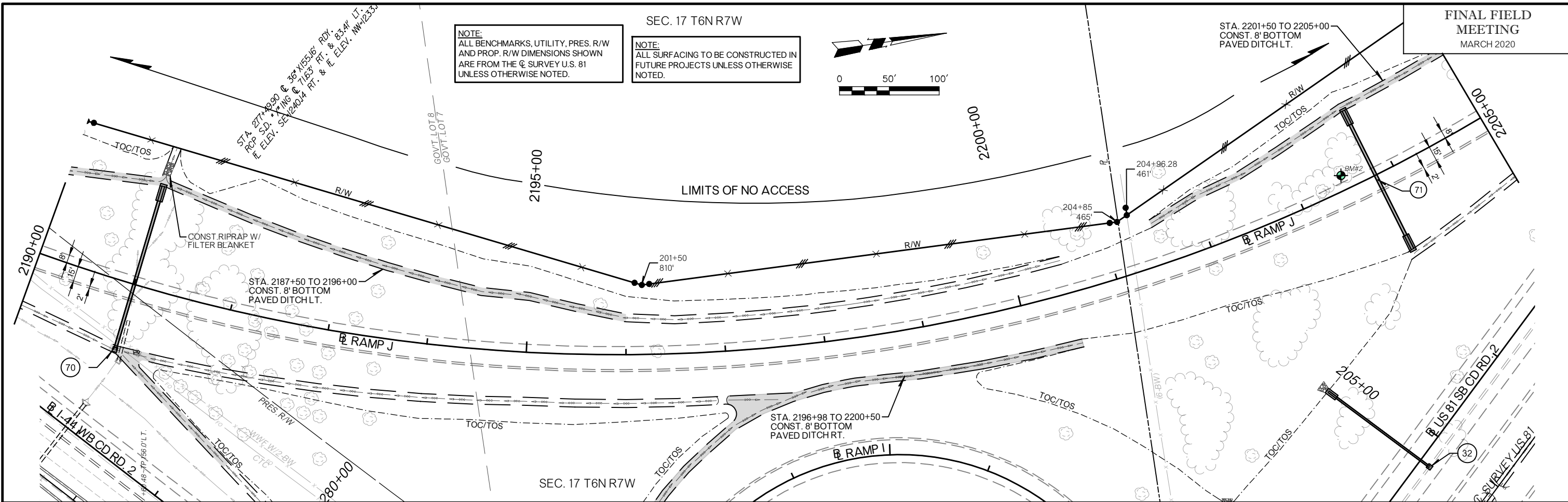
NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY U.S. 81
UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.



STA. 2201+50 TO 2205+00
CONST. 8" BOTTOM
PAVED DITCH LT.

FINAL FIELD
MEETING
MARCH 2020



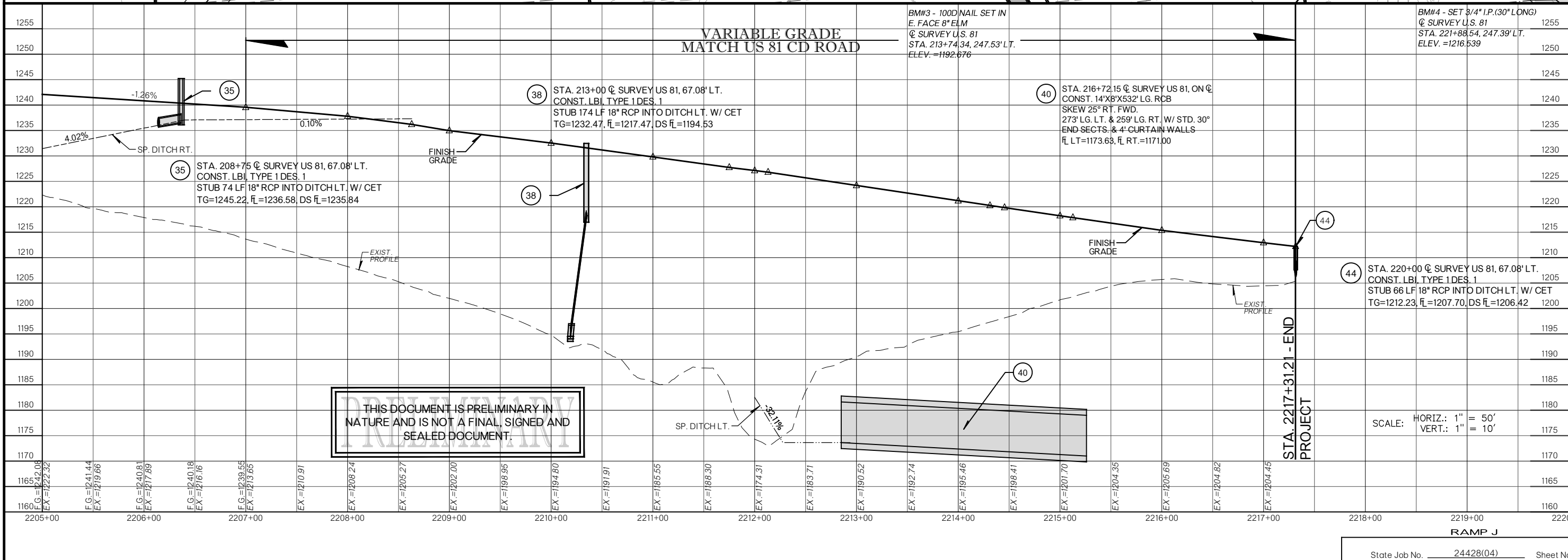
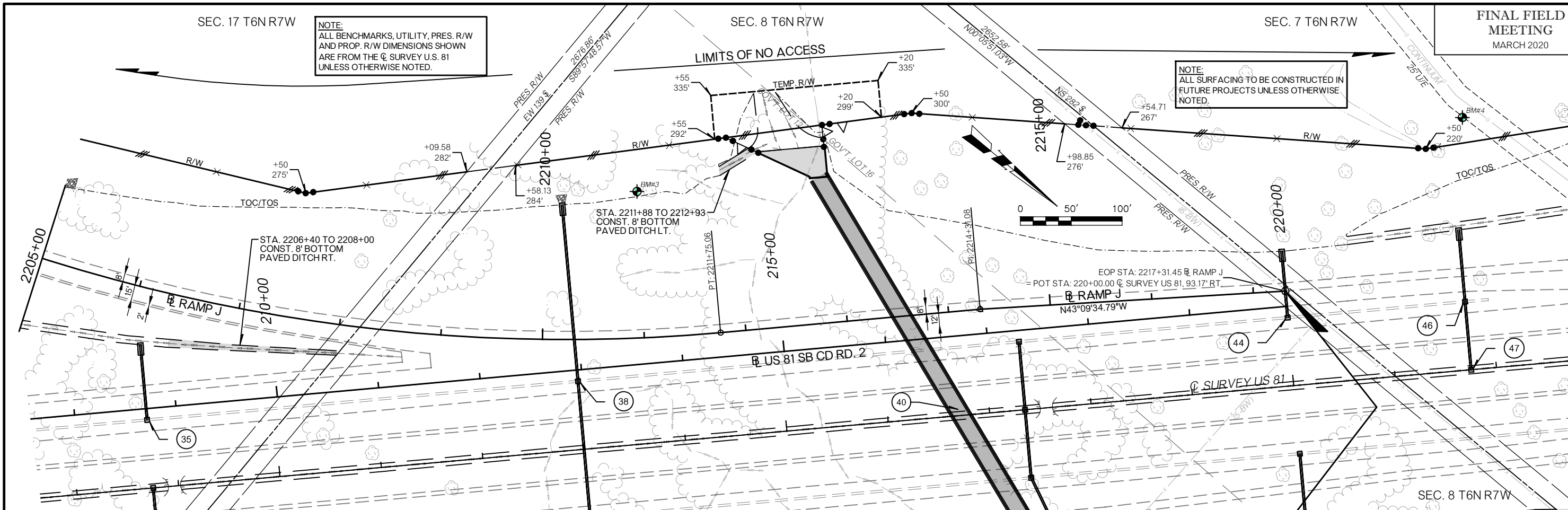
SEC. 17 T6N R7W

SEC. 8 T6N R7W

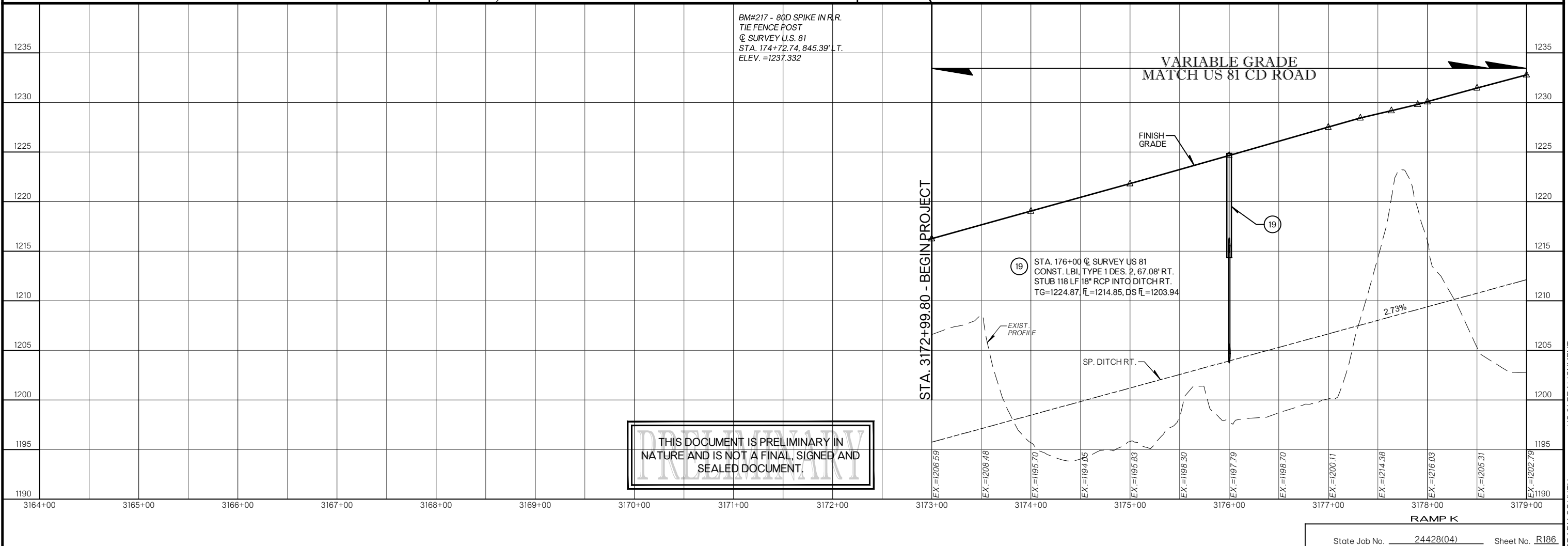
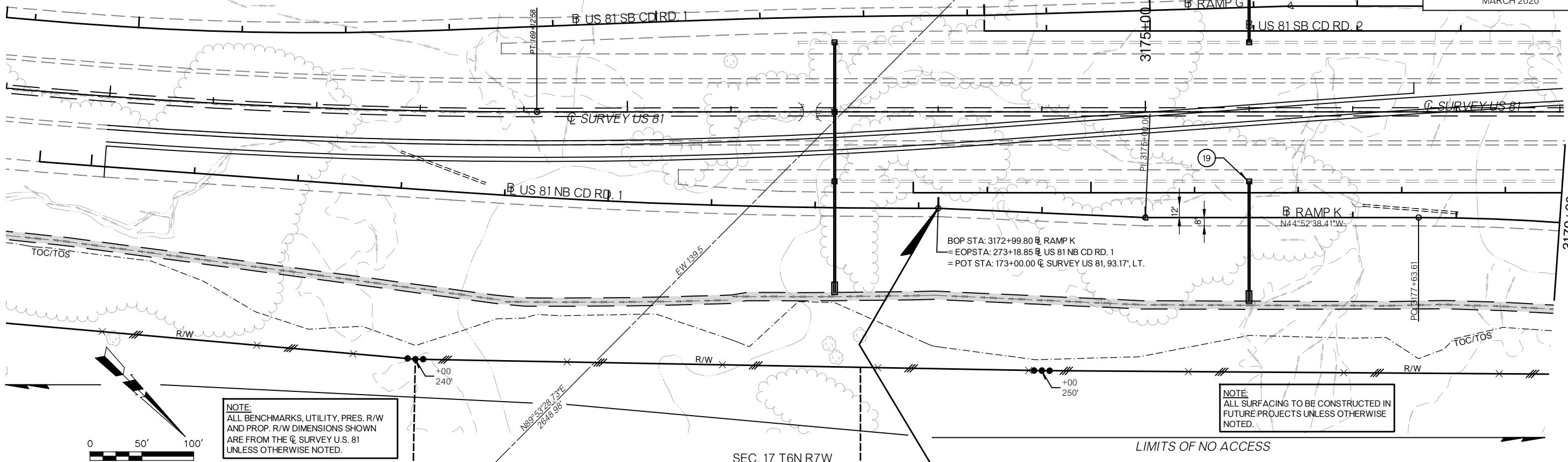
SEC. 7 T6N R7W

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY U.S. 81
UNLESS OTHERWISE NOTED.

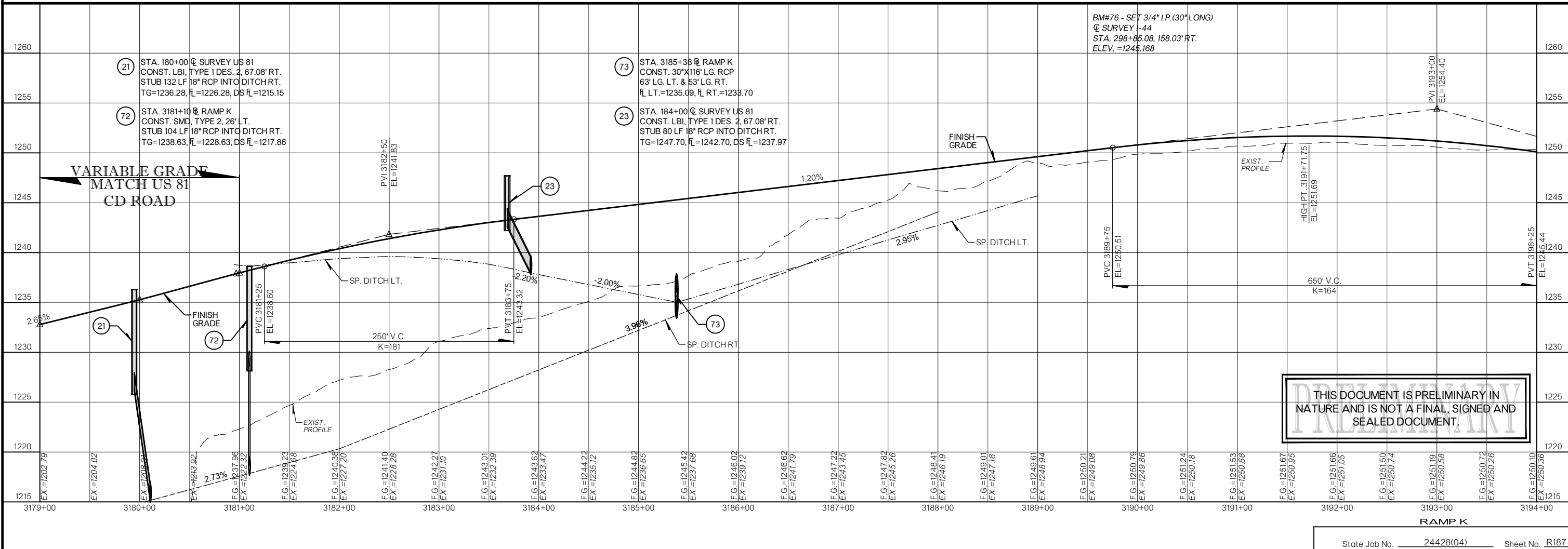
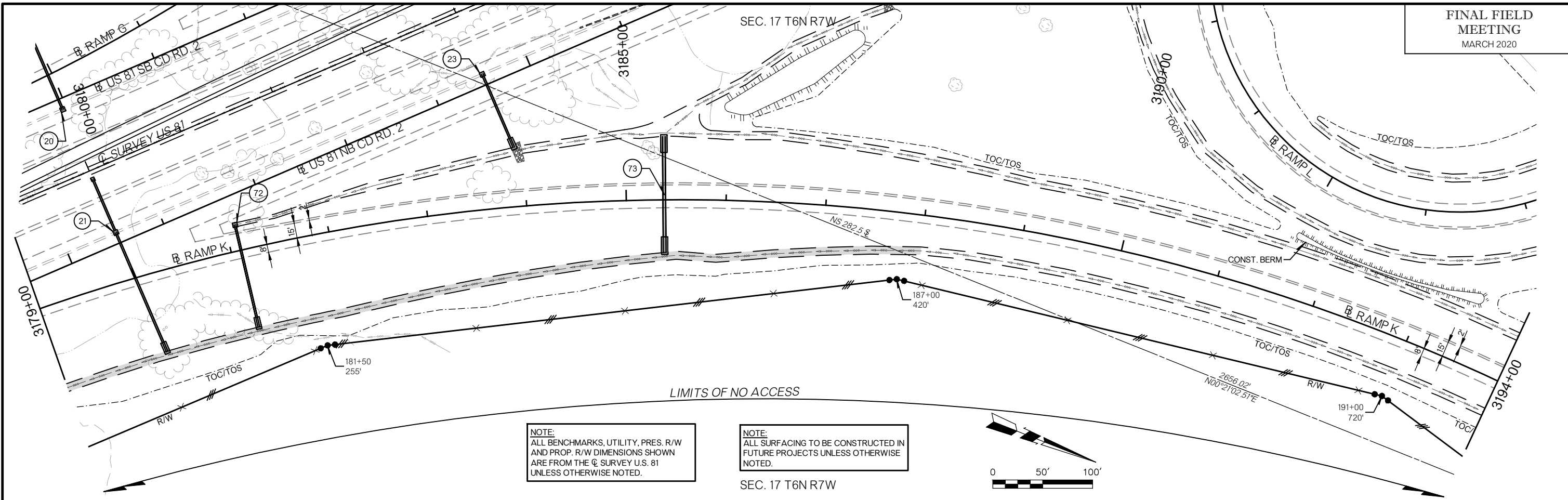
NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.



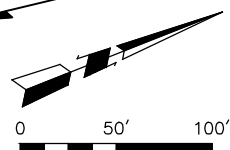
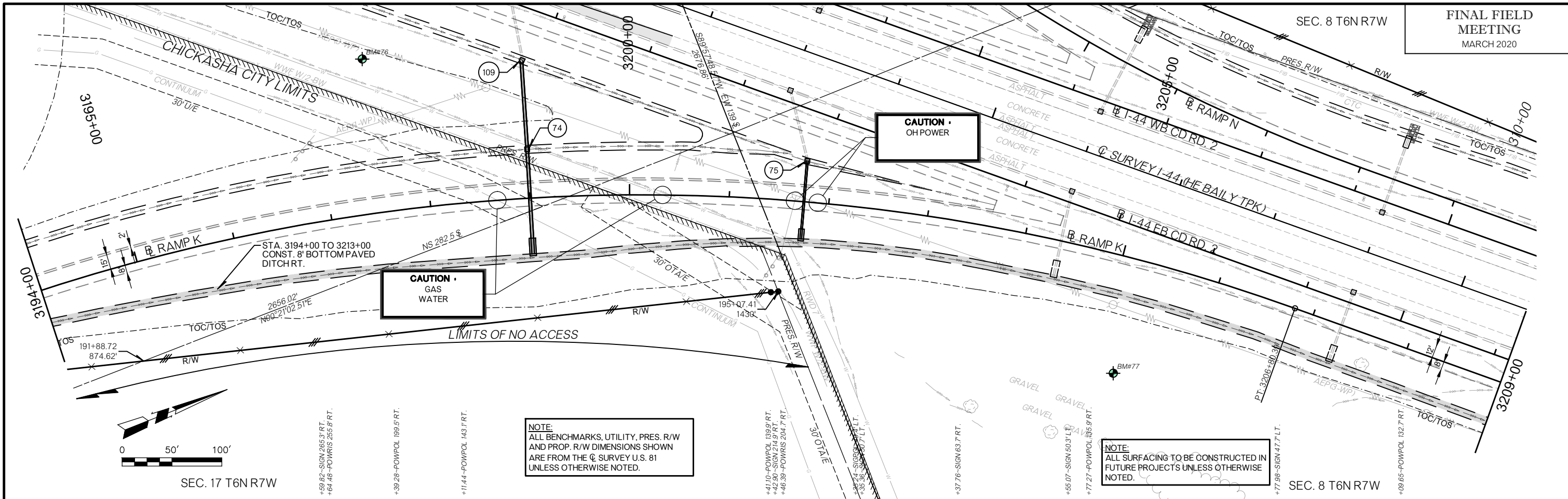
SEC. 17 T6N R7W



GRADY COUNTY
US 81 REALIGNMENT

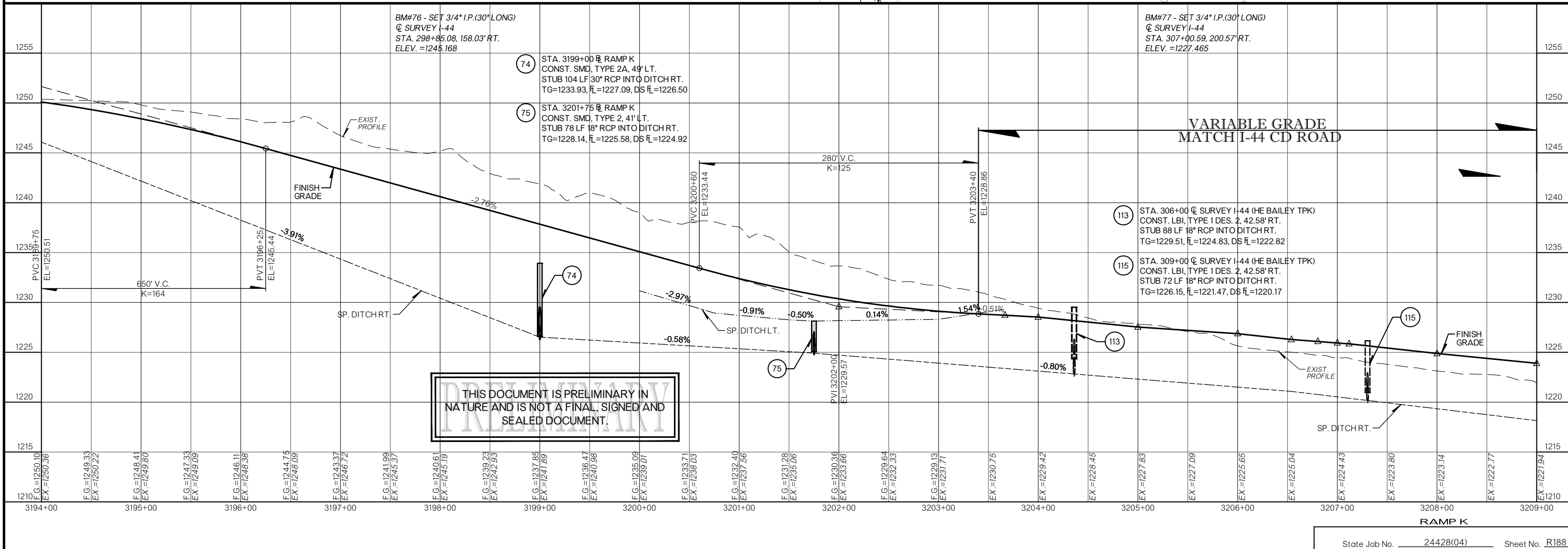


GRADY COUNTY US 81 REALIGNMENT

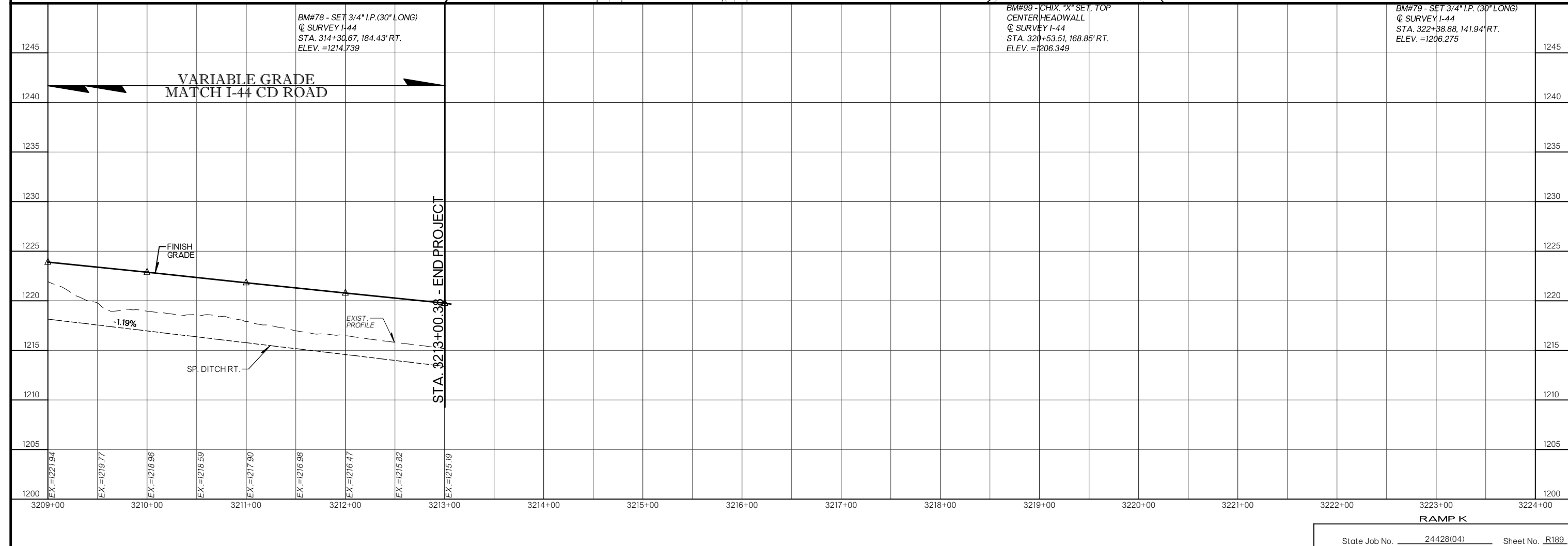
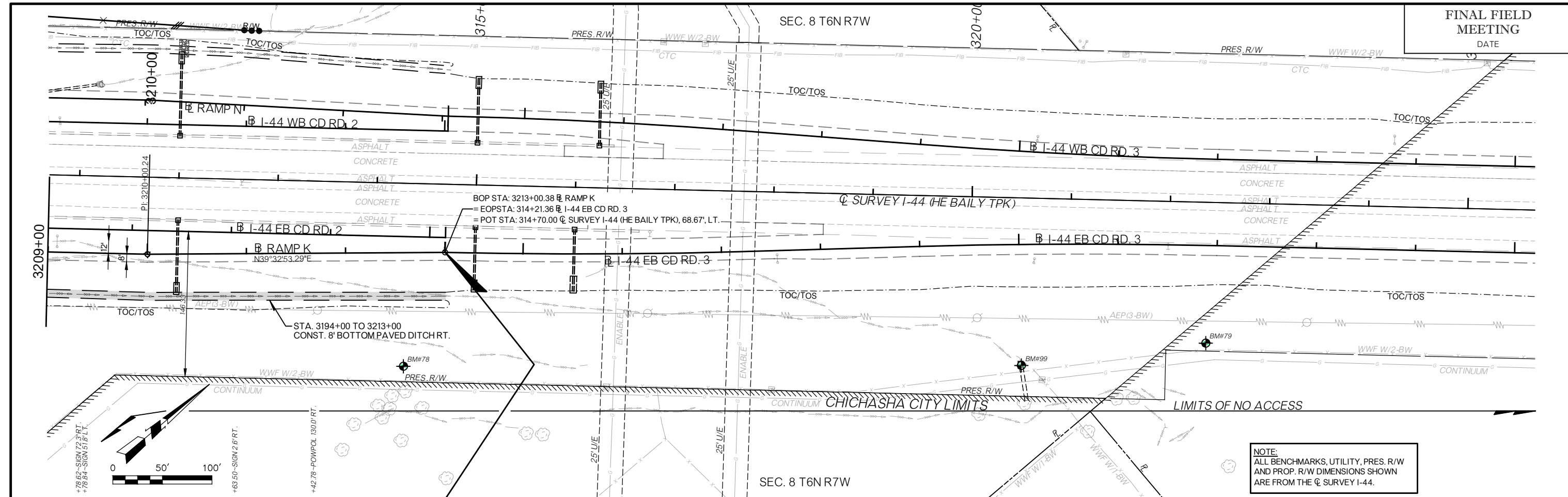


NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



BM#78 - SET 3/4" I.P. (30" LONG)
 Q SURVEY I-44
 STA. 314+30.67, 184.43' RT.
 ELEV. = 1214.739

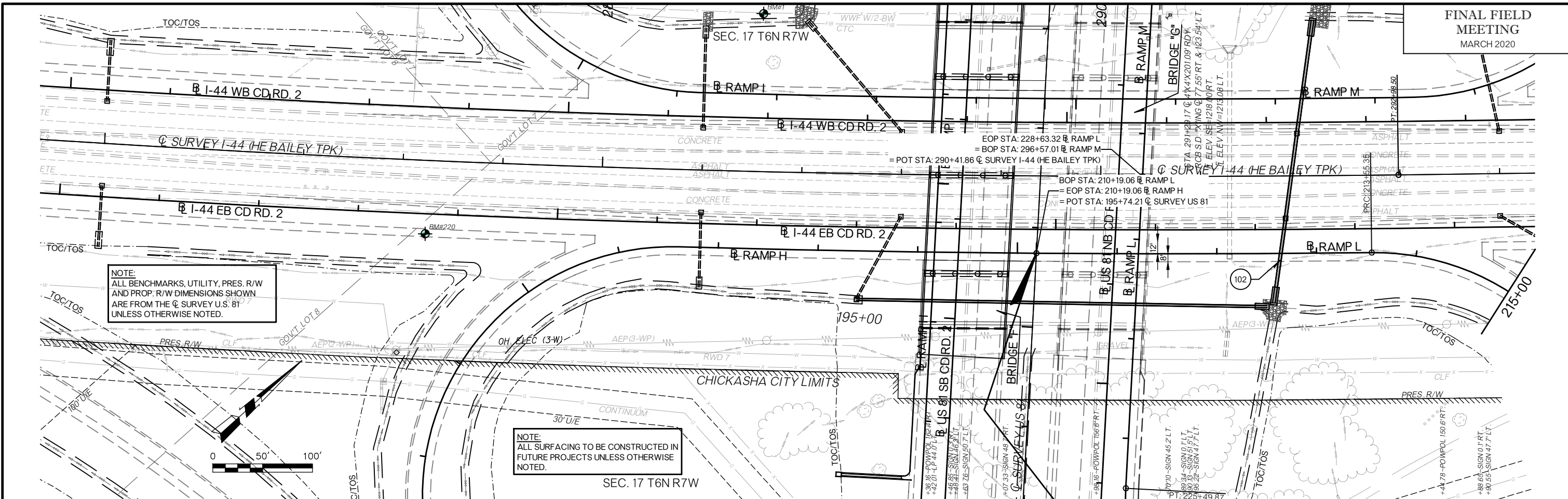
BM#99 - CHIX. 'X' SET, TOP
 CENTER HEADWALL
 Q SURVEY I-44
 STA. 320+53.51, 168.85' RT.
 ELEV. = 1206.349

BM#79 - SET 3/4" I.P. (30" LONG)
 Q SURVEY I-44
 STA. 322+38.88, 141.94' RT.
 ELEV. = 1206.275

VARIABLE GRADE
 MATCH I-44 CD ROAD

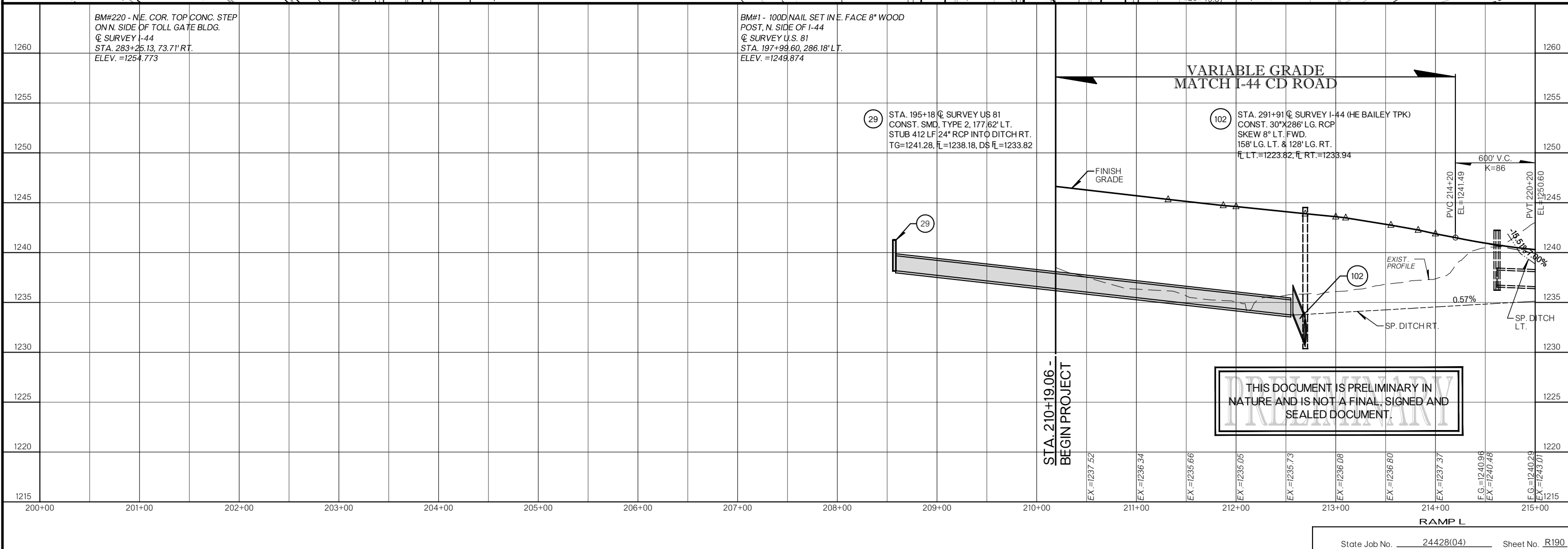
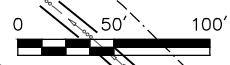
STA. 3213+00.38 - END PROJECT

RAMP K



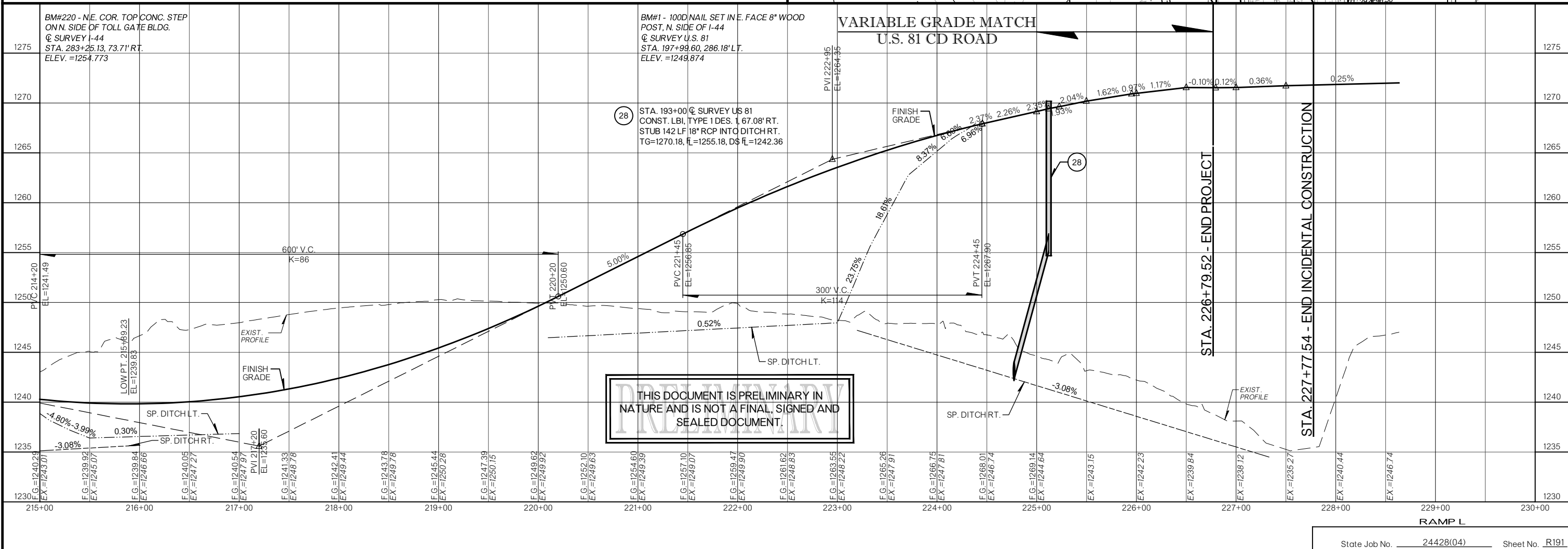
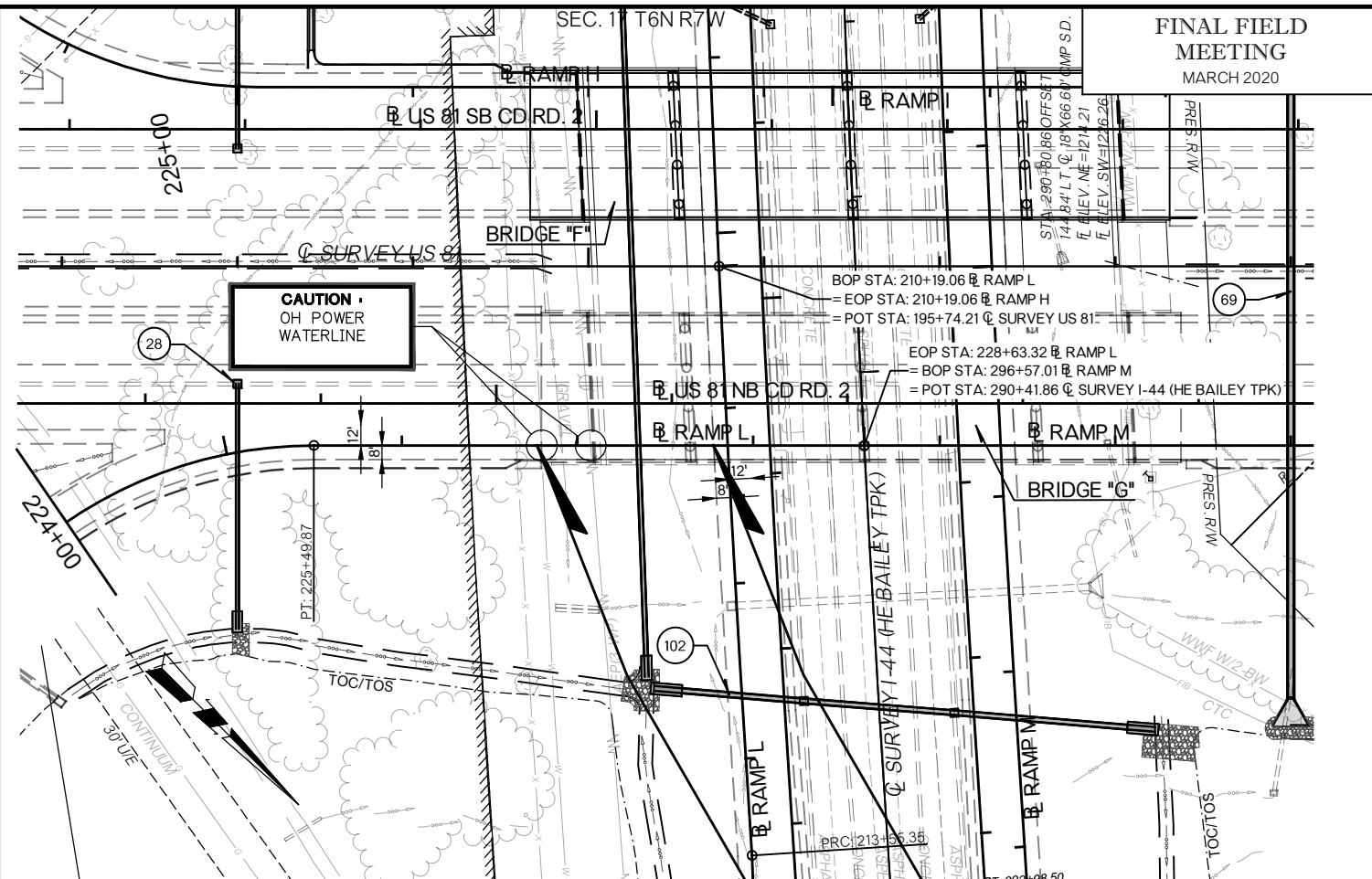
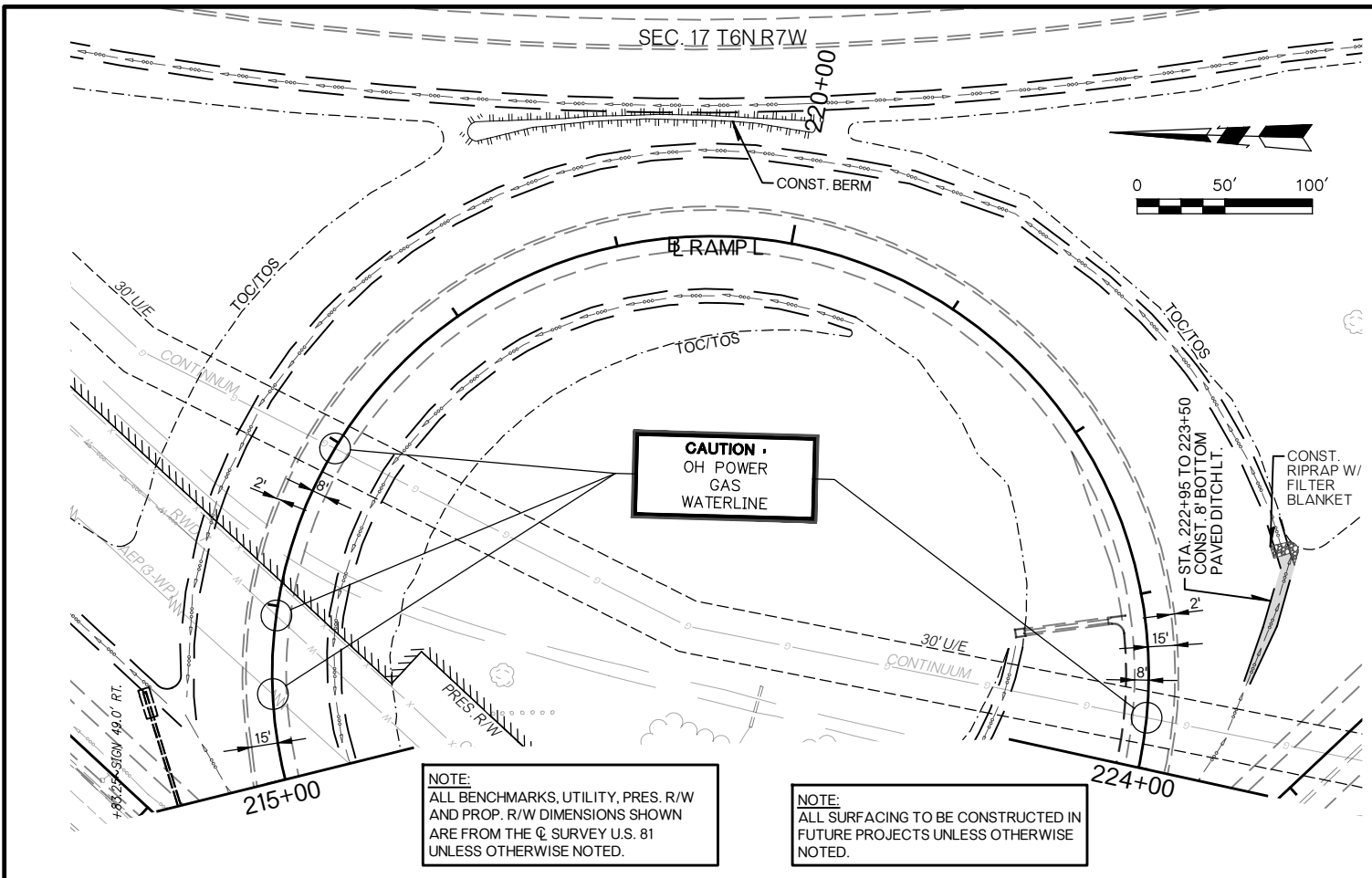
NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY U.S. 81
UNLESS OTHERWISE NOTED.

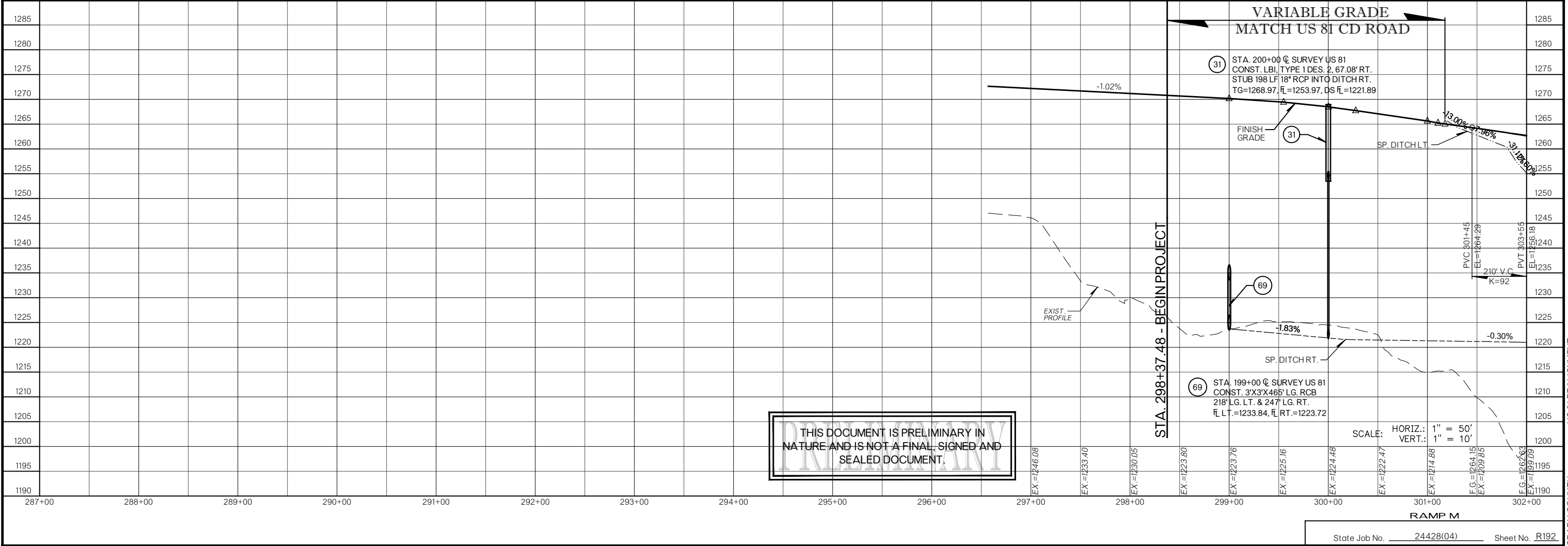
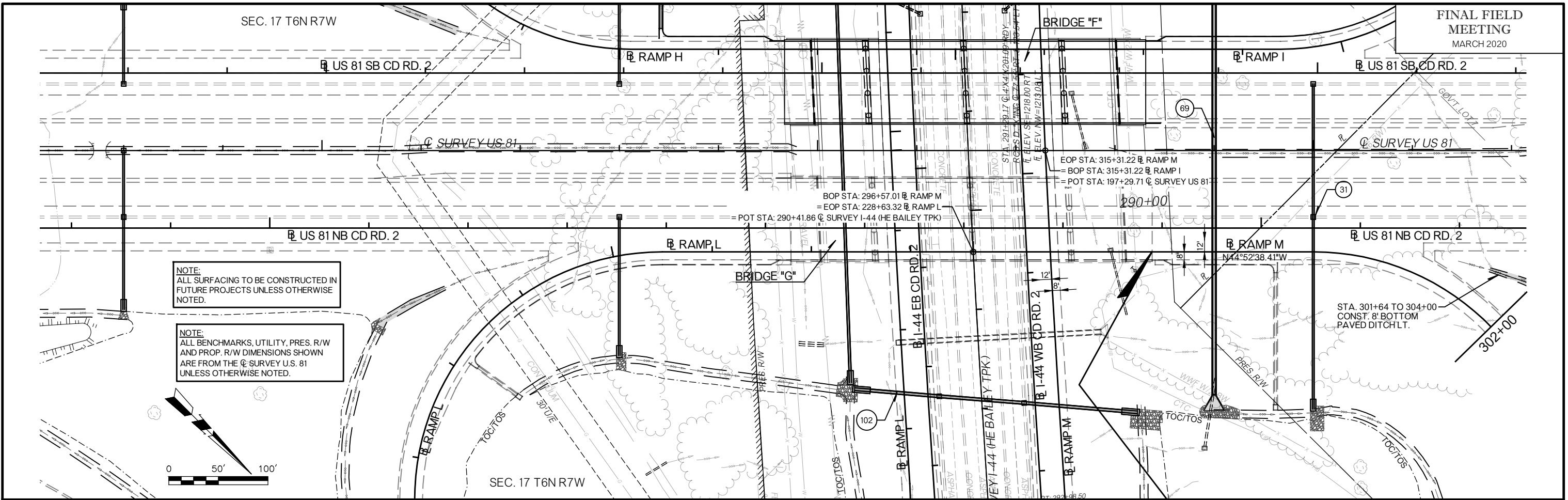
NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.



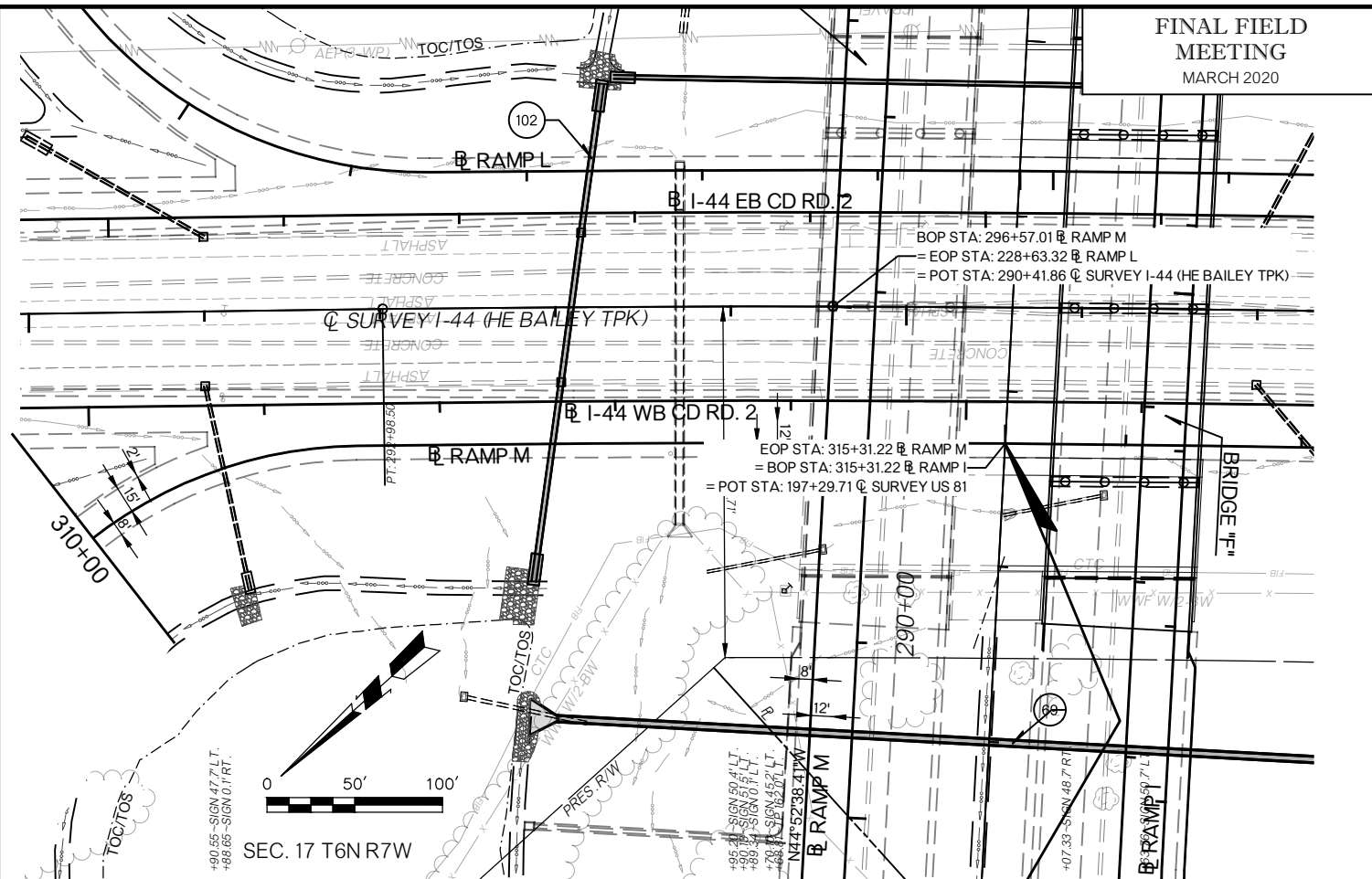
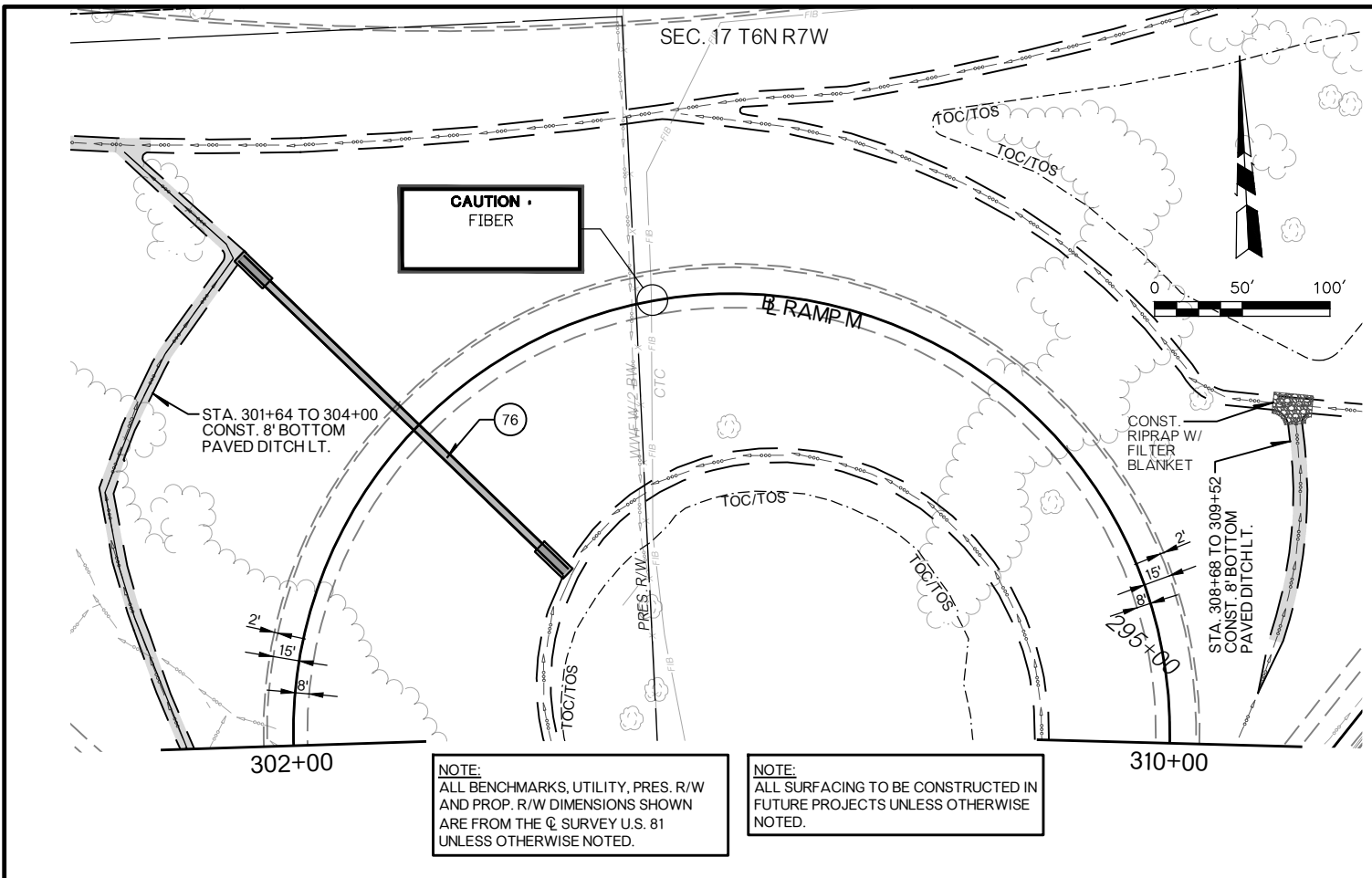
THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

STA. 210+19.06 -
BEGIN PROJECT



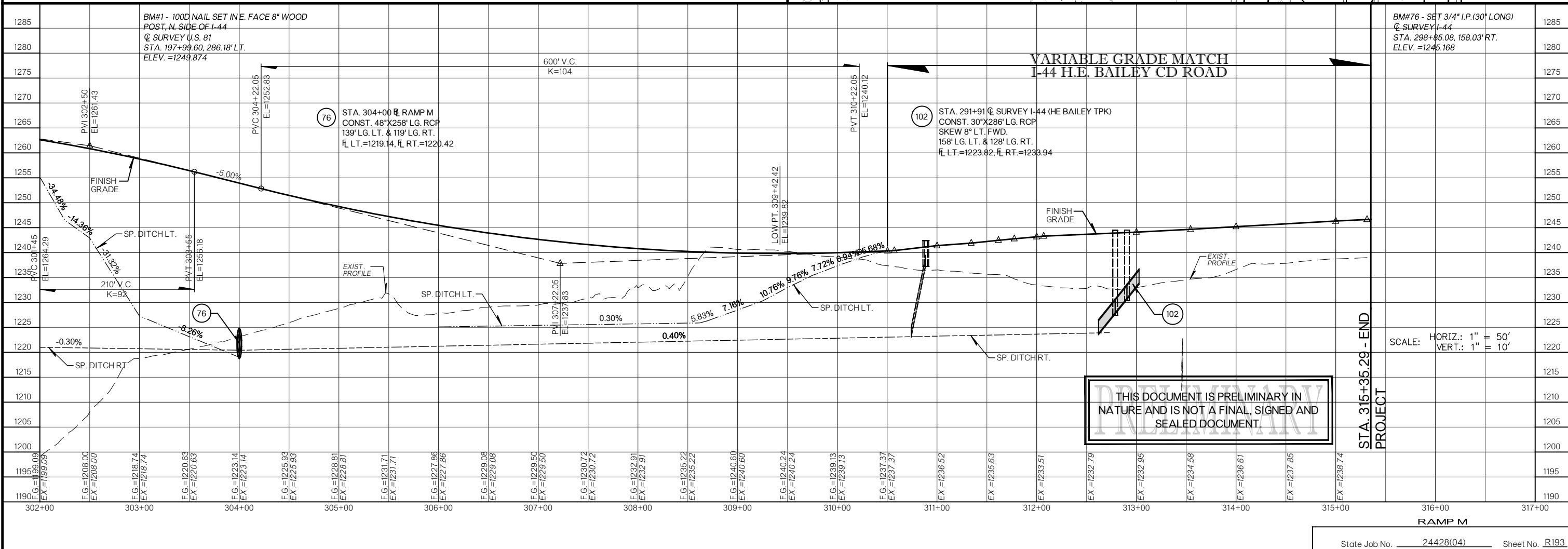


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY U.S. 81
UNLESS OTHERWISE NOTED.

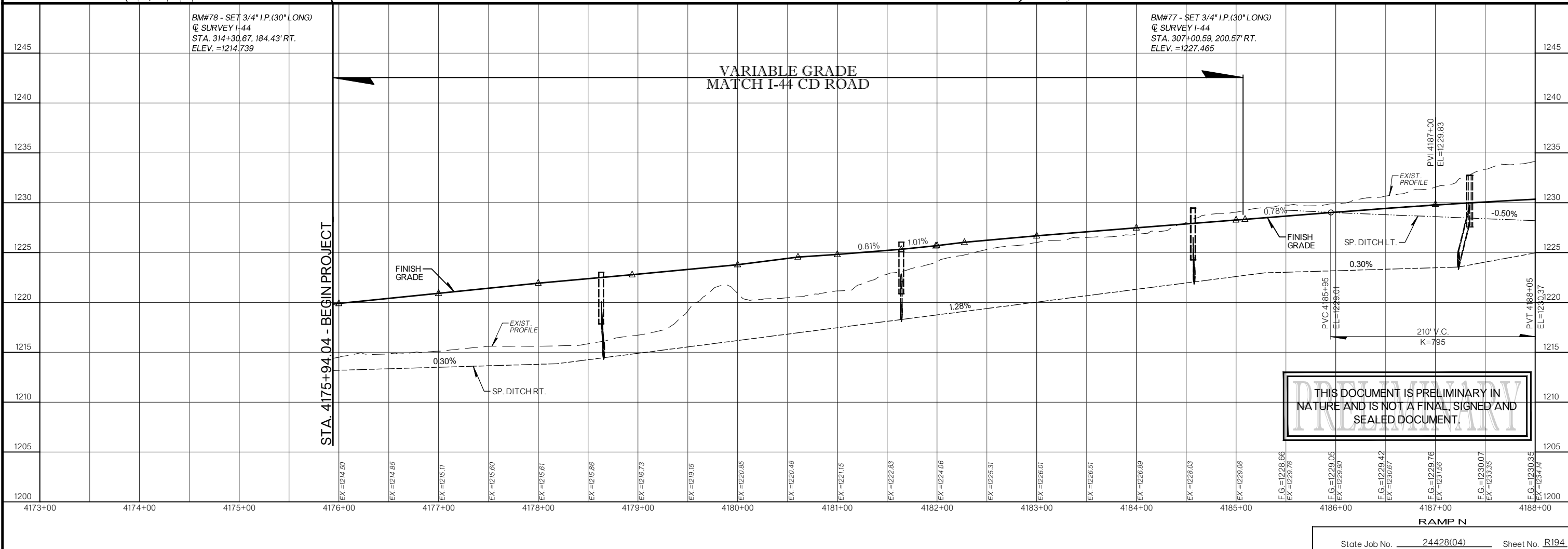
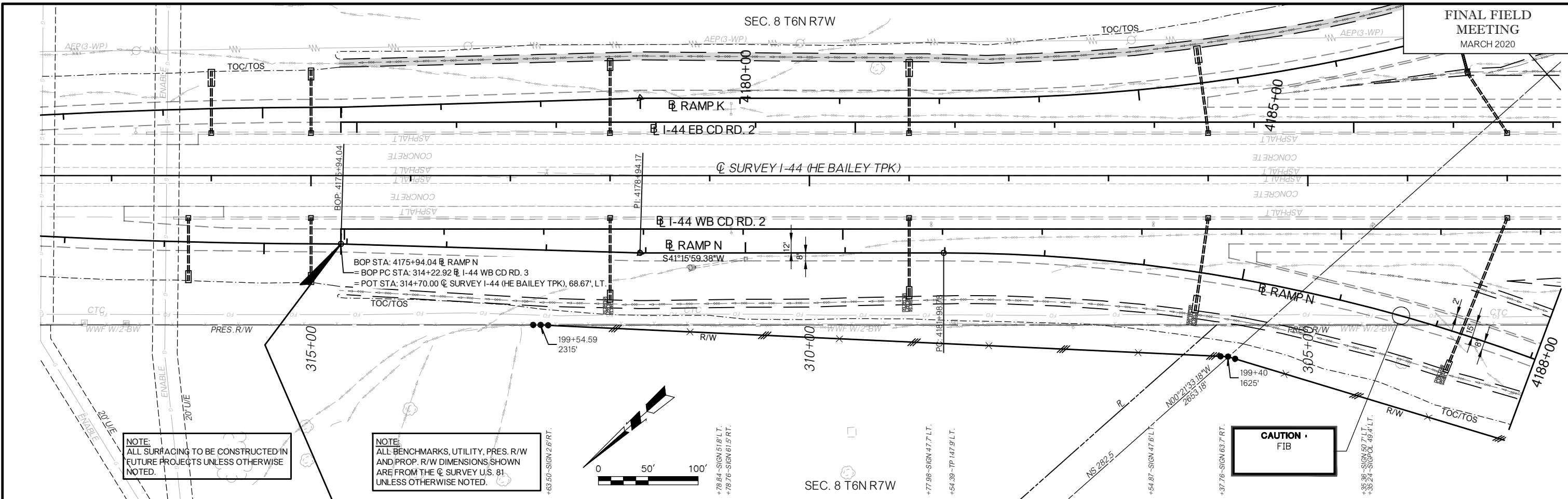
NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.



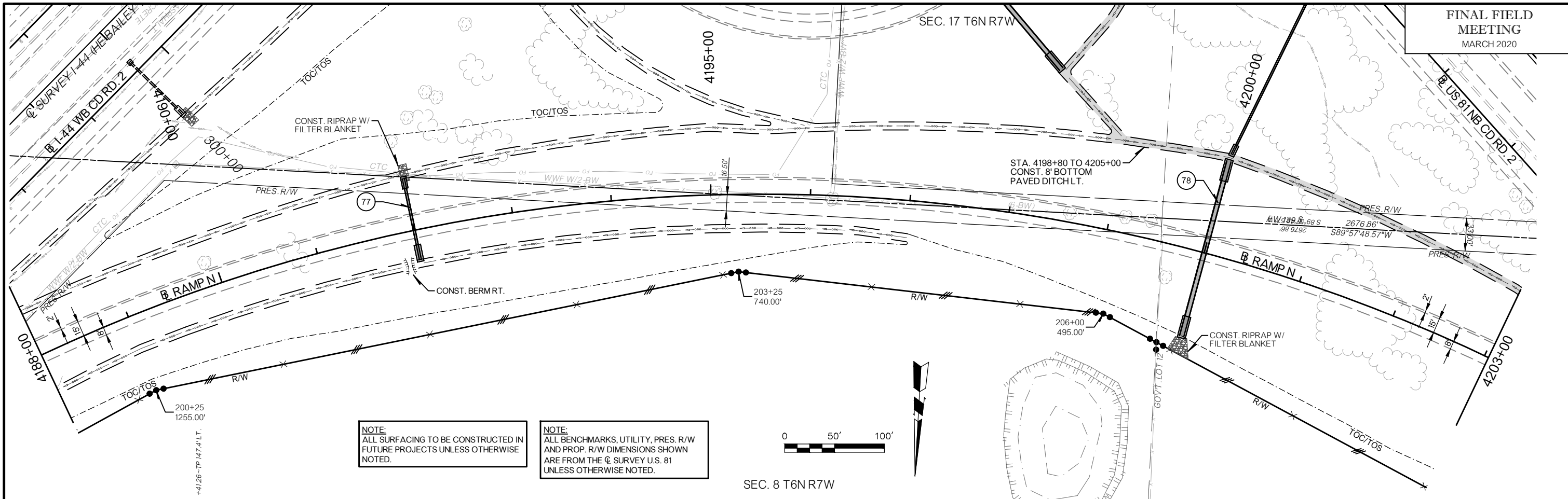
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

STA. 315+35.29 - END PROJECT

SCALE: HORIZ.: 1" = 50'
VERT.: 1" = 10'

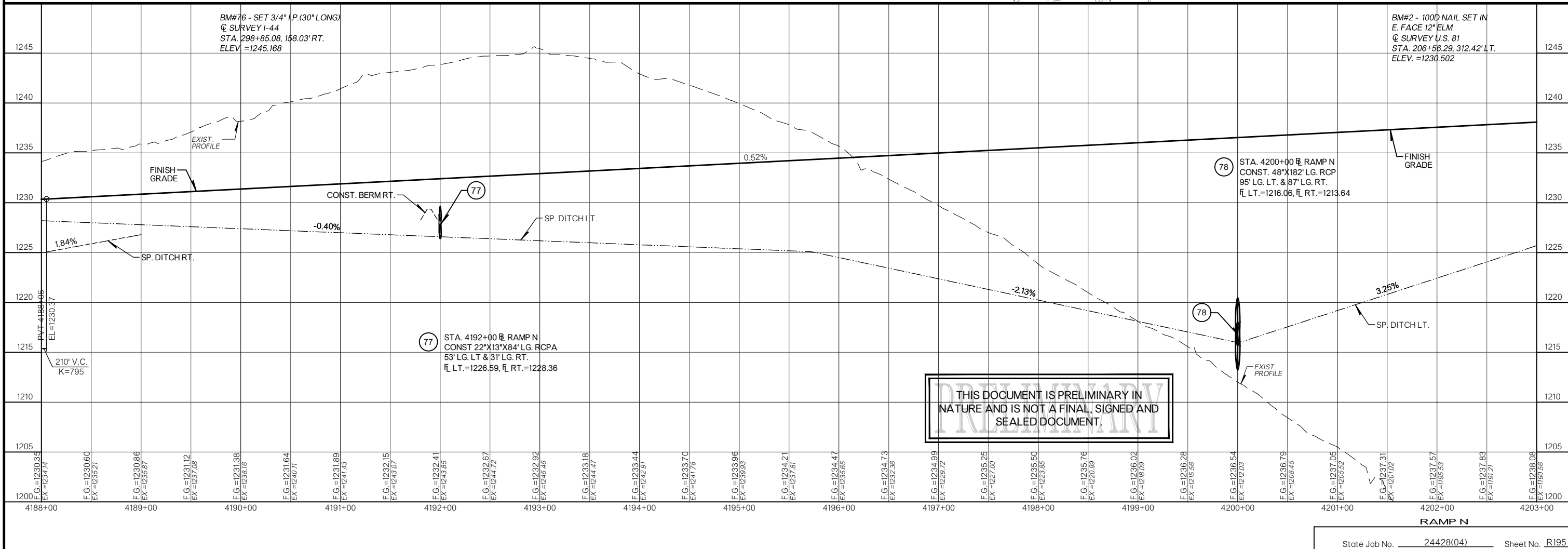


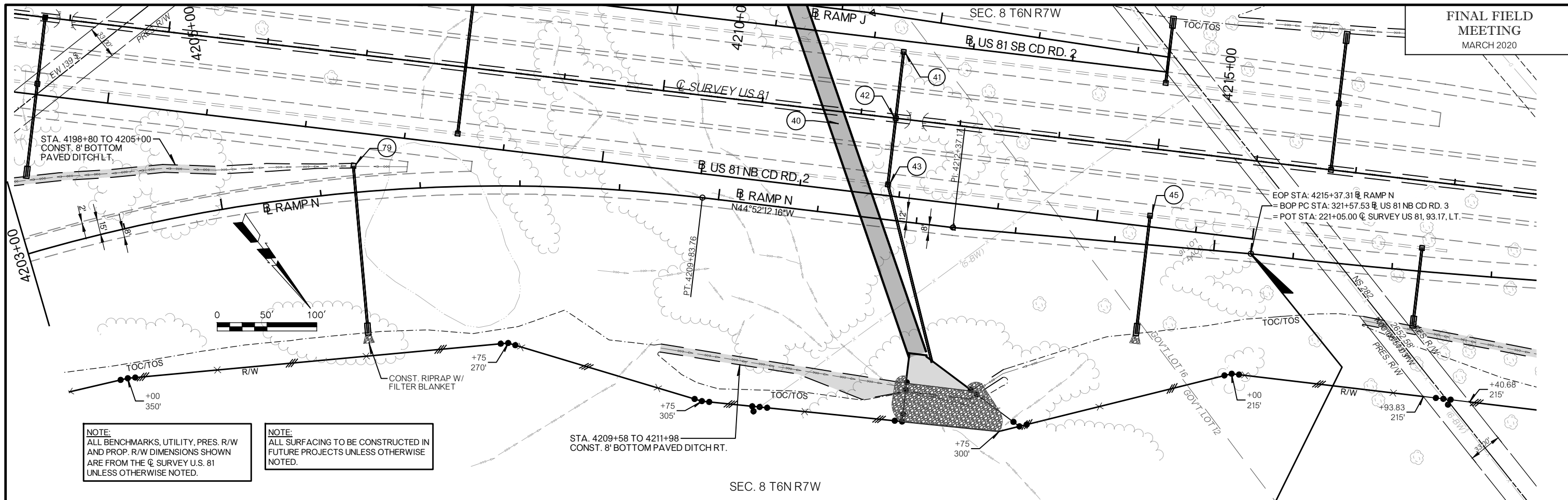
GRADY COUNTY US 81 REALIGNMENT



NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

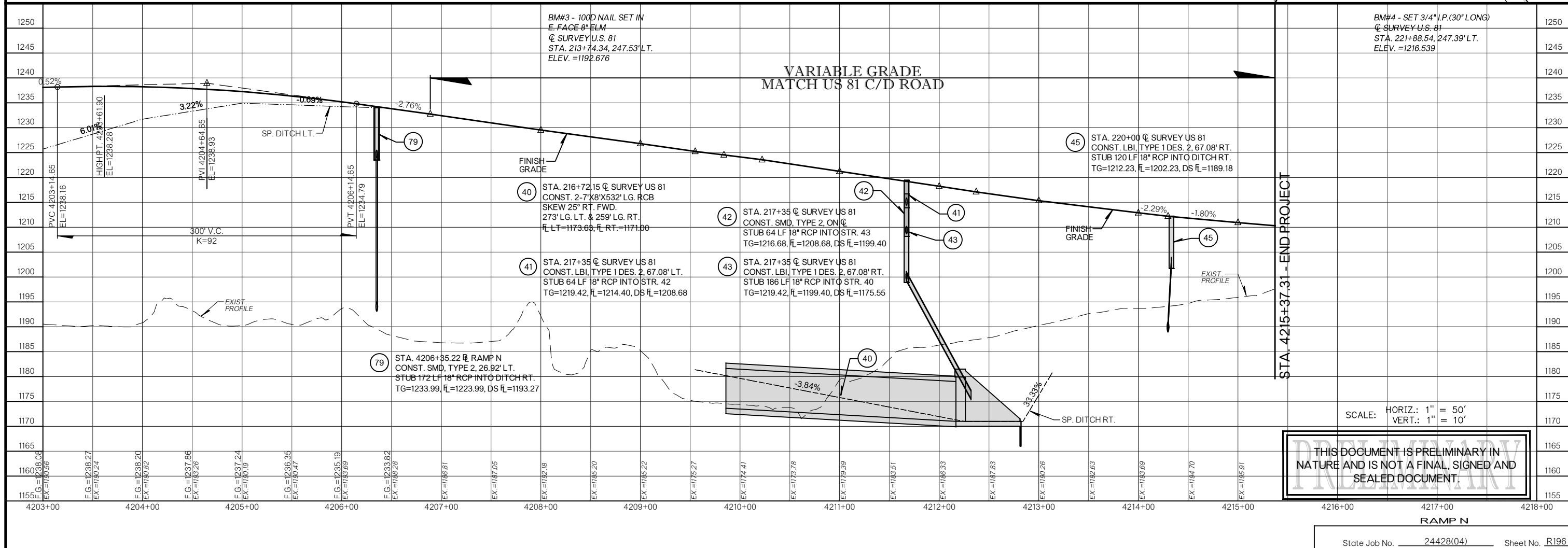




NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

SEC. 8 T6N R7W



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

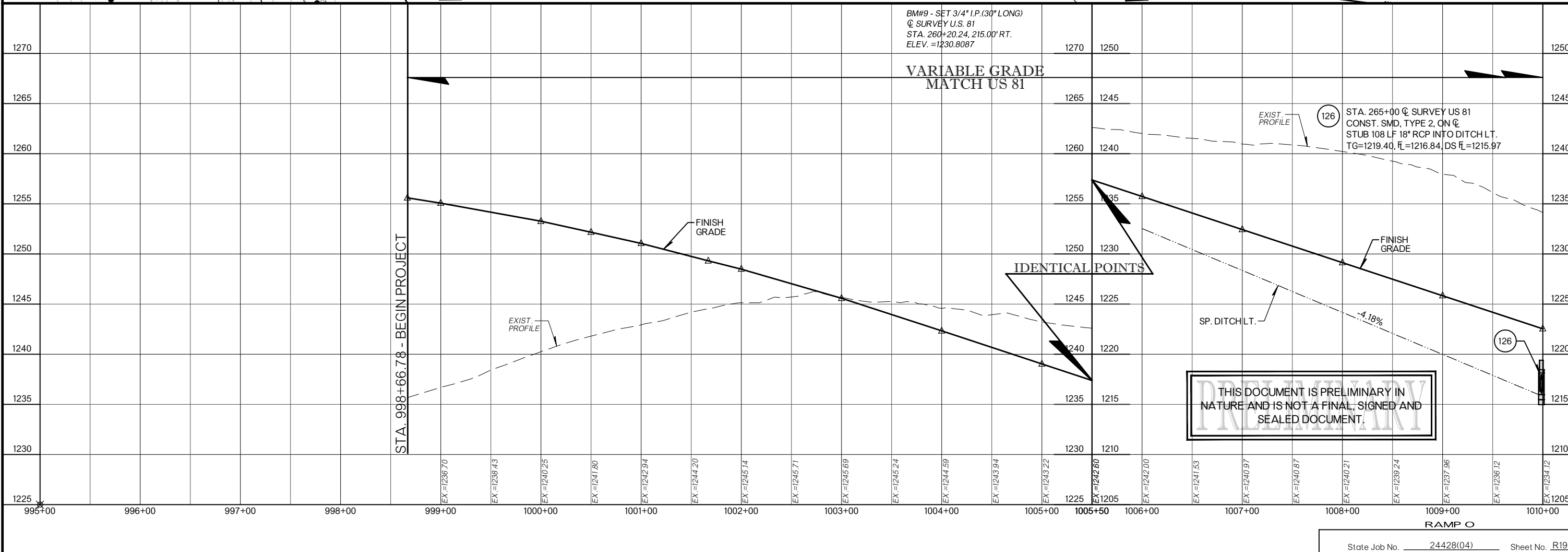
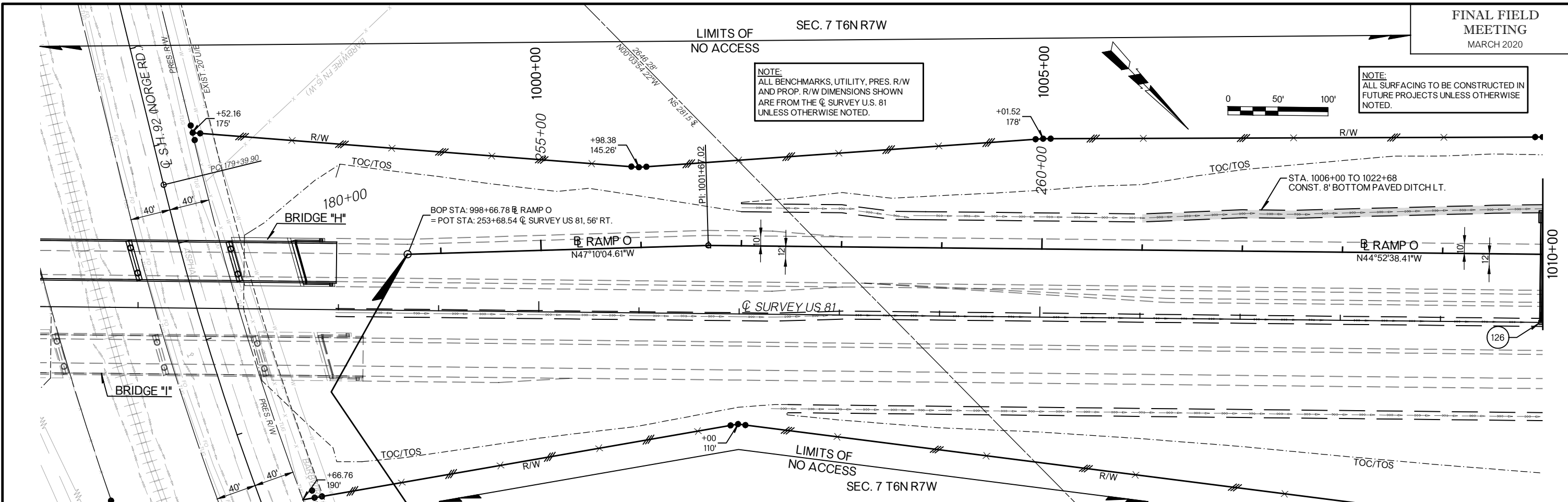
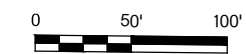
SCALE: HORIZ.: 1" = 50'
VERT.: 1" = 10'

GRADY COUNTY US 81 REALIGNMENT

SEC. 7 T6N R7W
LIMITS OF NO ACCESS

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY U.S. 81
UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

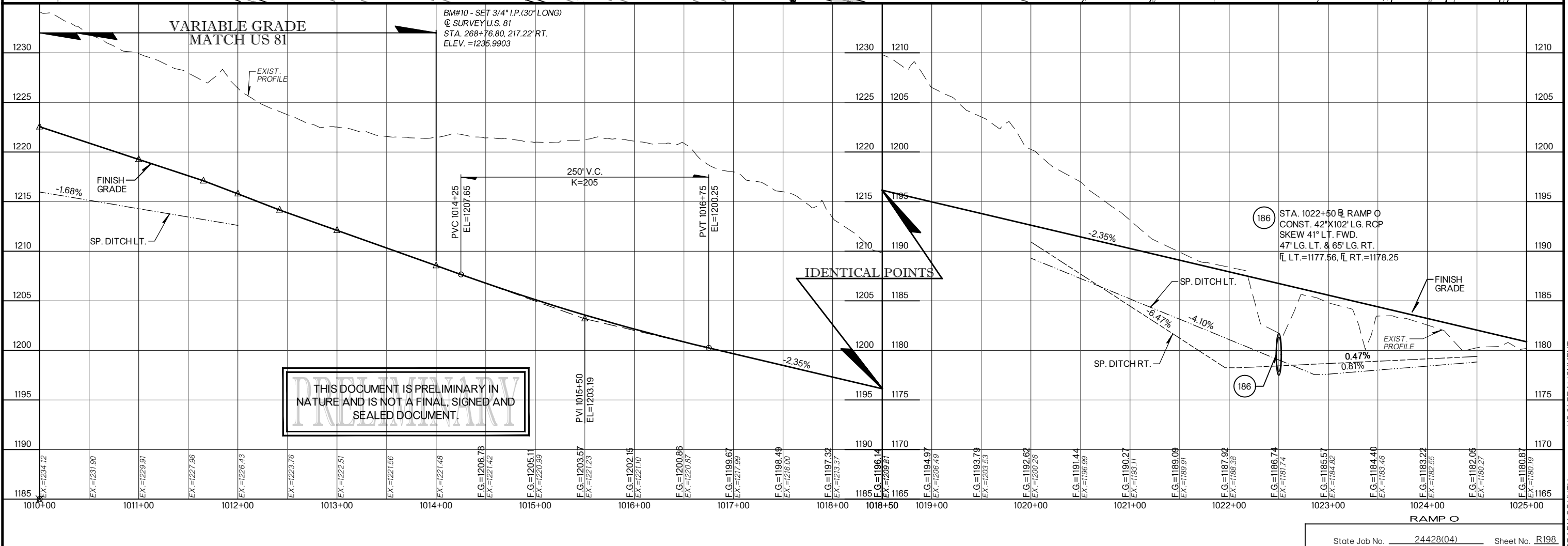
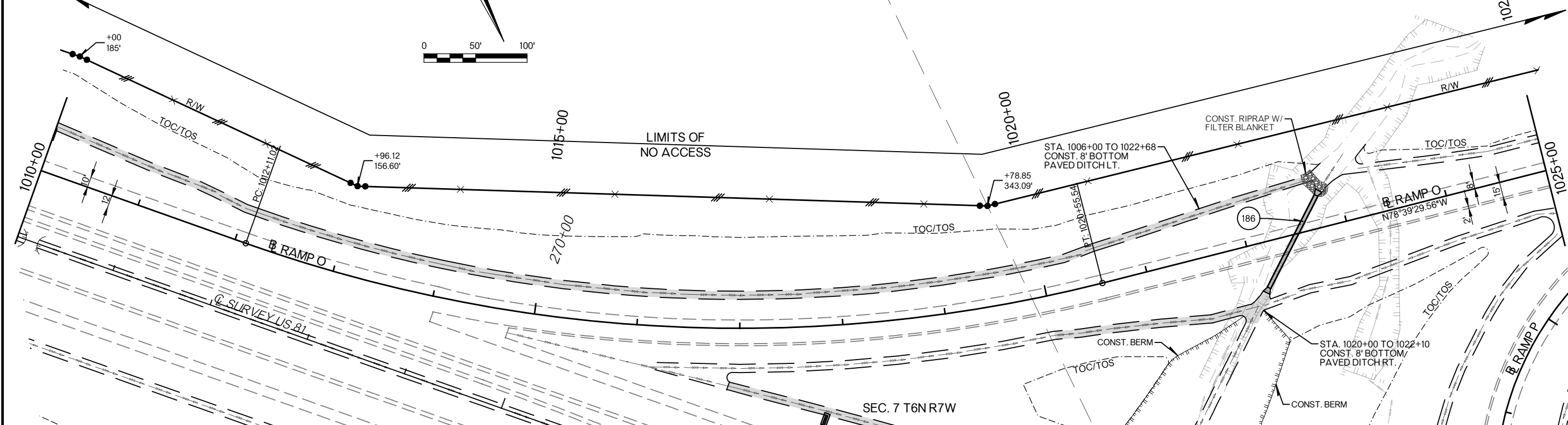
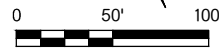


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADY COUNTY US 81 REALIGNMENT

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

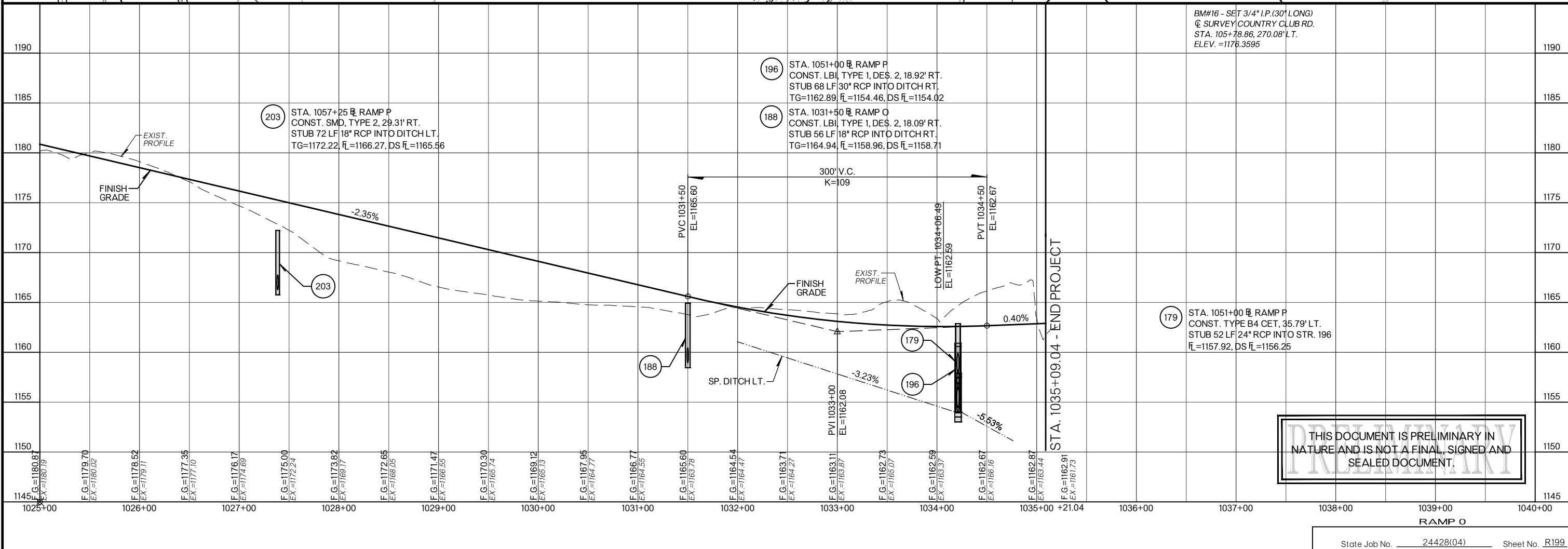
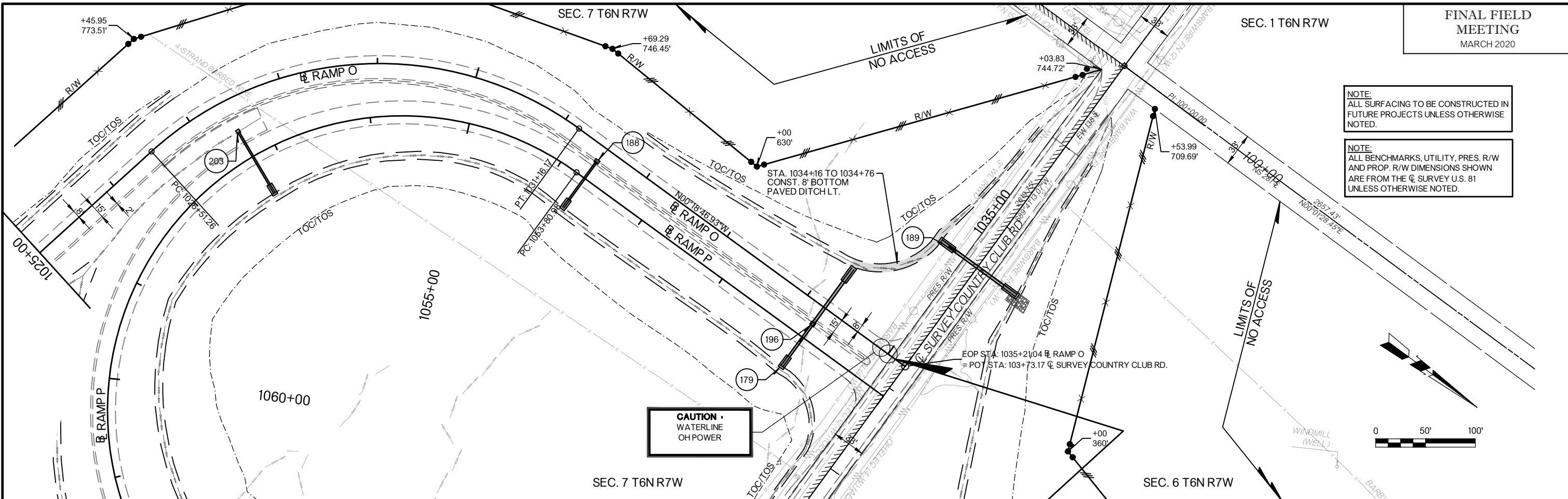
NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY U.S. 81
UNLESS OTHERWISE NOTED.



THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.



BM#16 - SET 3/4" I.P. (30" LONG)
Q SURVEY COUNTRY CLUB RD.
STA. 105+78.86, 270.08' LT.
ELEV. =1176.3595

203 STA. 1057+25 RAMP P
CONST. SMD, TYPE 2, 29.31' RT.
STUB 72 LF 18" RCP INTO DITCH LT.
TG=1172.22, f_l=1166.27, DS f_l=1165.56

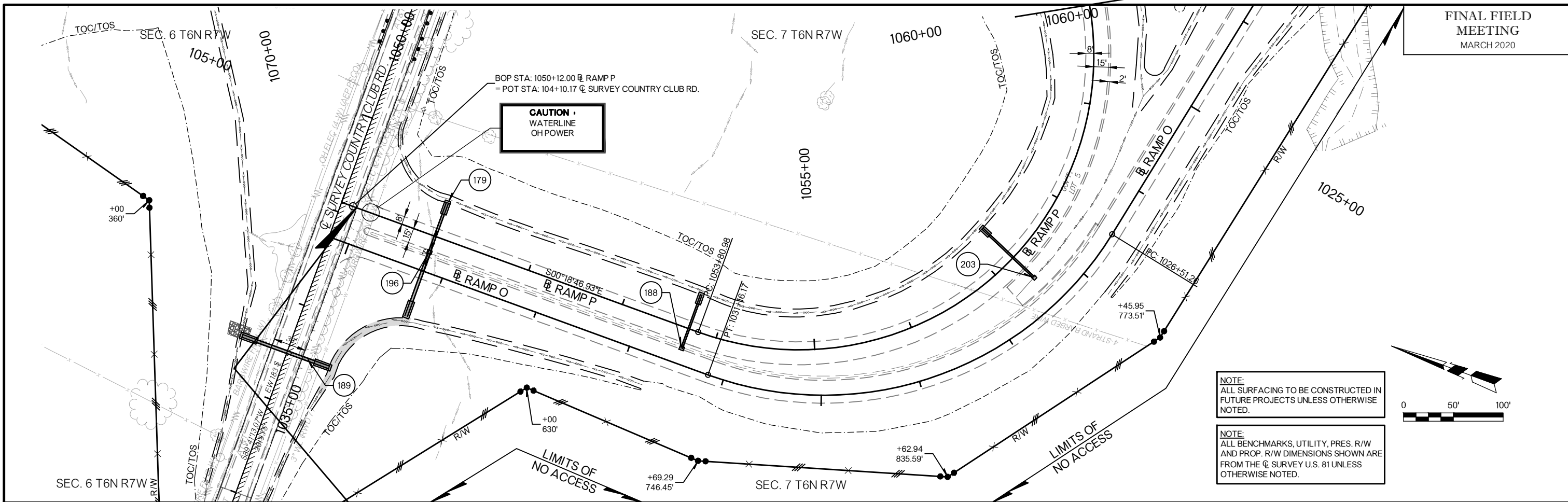
196 STA. 1051+00 RAMP P
CONST. LBI, TYPE 1, DES. 2, 18.92' RT.
STUB 68 LF 30" RCP INTO DITCH RT.
TG=1162.89, f_l=1154.46, DS f_l=1154.02

188 STA. 1031+50 RAMP O
CONST. LBI, TYPE 1, DES. 2, 18.09' RT.
STUB 56 LF 18" RCP INTO DITCH RT.
TG=1164.94, f_l=1158.96, DS f_l=1158.71

179 STA. 1051+00 RAMP P
CONST. TYPE B4 CET, 35.79' LT.
STUB 52 LF 24" RCP INTO STR. 196
f_l=1157.92, DS f_l=1156.25

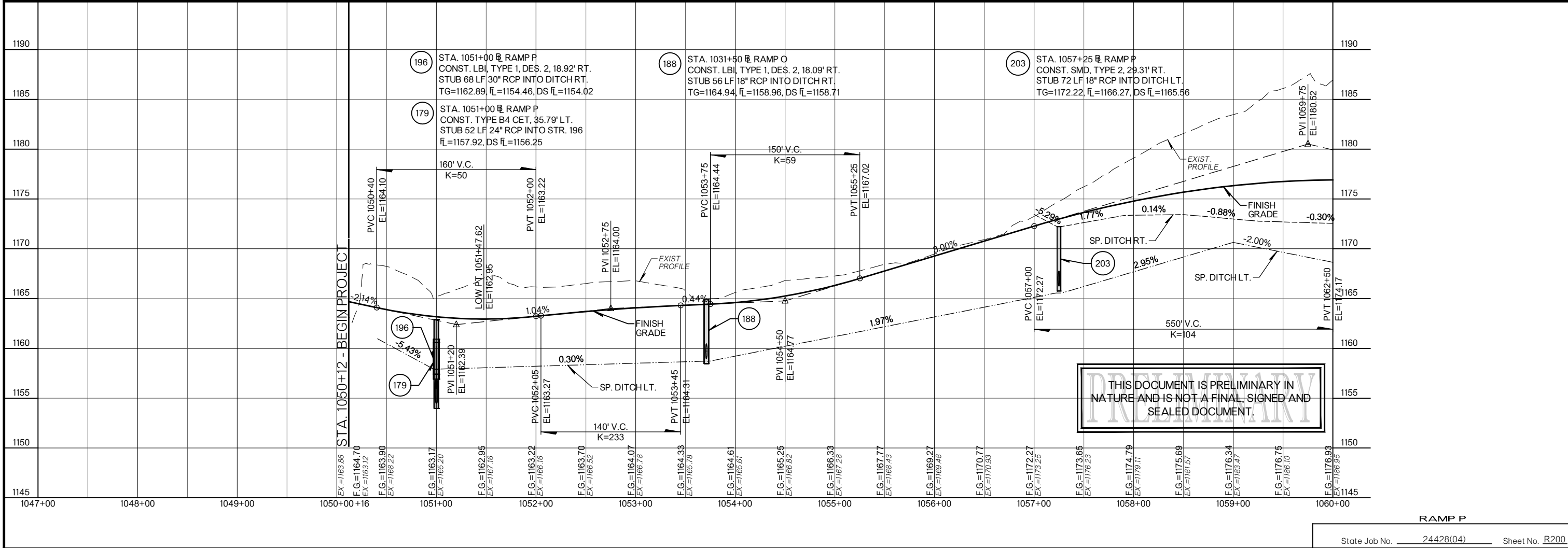
PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADY COUNTY US 81 REALIGNMENT



NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.



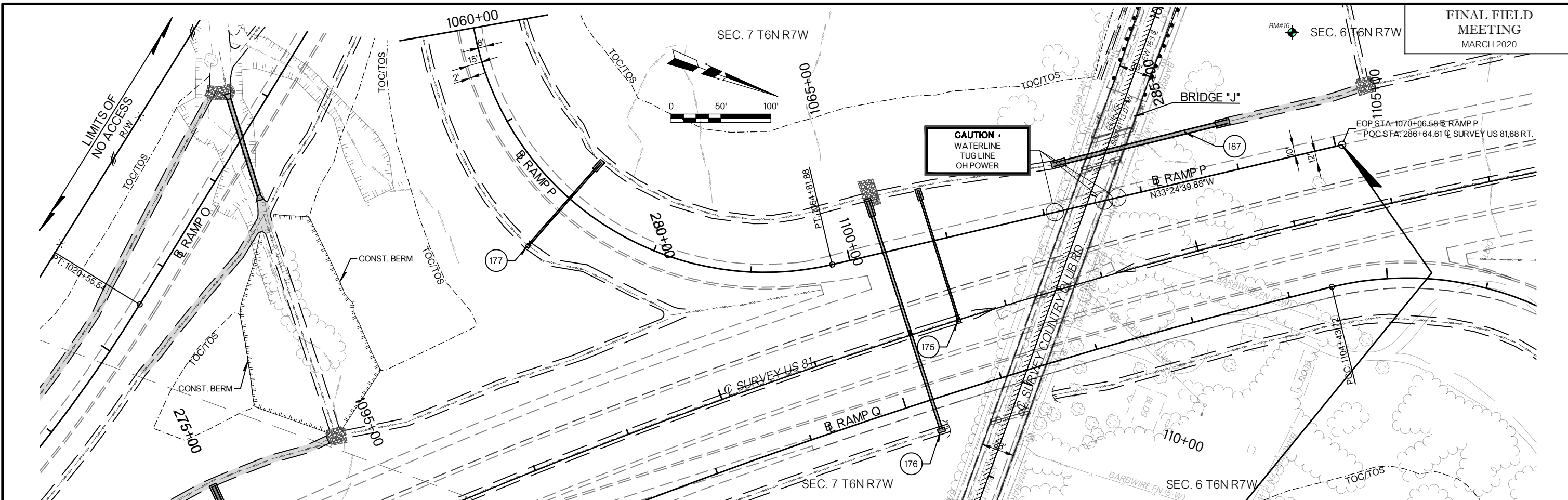
196 STA. 1051+00 RAMP P
CONST. LBL TYPE 1, DES. 2, 18.92' RT.
STUB 68 LF 30" RCP INTO DITCH RT.
TG=1162.89, \bar{r}_L =1154.46, DS \bar{r}_L =1154.02

179 STA. 1051+00 RAMP P
CONST. TYPE B4 CET, 35.79' LT.
STUB 52 LF 24" RCP INTO STR. 196
 \bar{r}_L =1157.92, DS \bar{r}_L =1156.25

188 STA. 1031+50 RAMP O
CONST. LBL TYPE 1, DES. 2, 18.09' RT.
STUB 56 LF 18" RCP INTO DITCH RT.
TG=1164.94, \bar{r}_L =1158.96, DS \bar{r}_L =1158.71

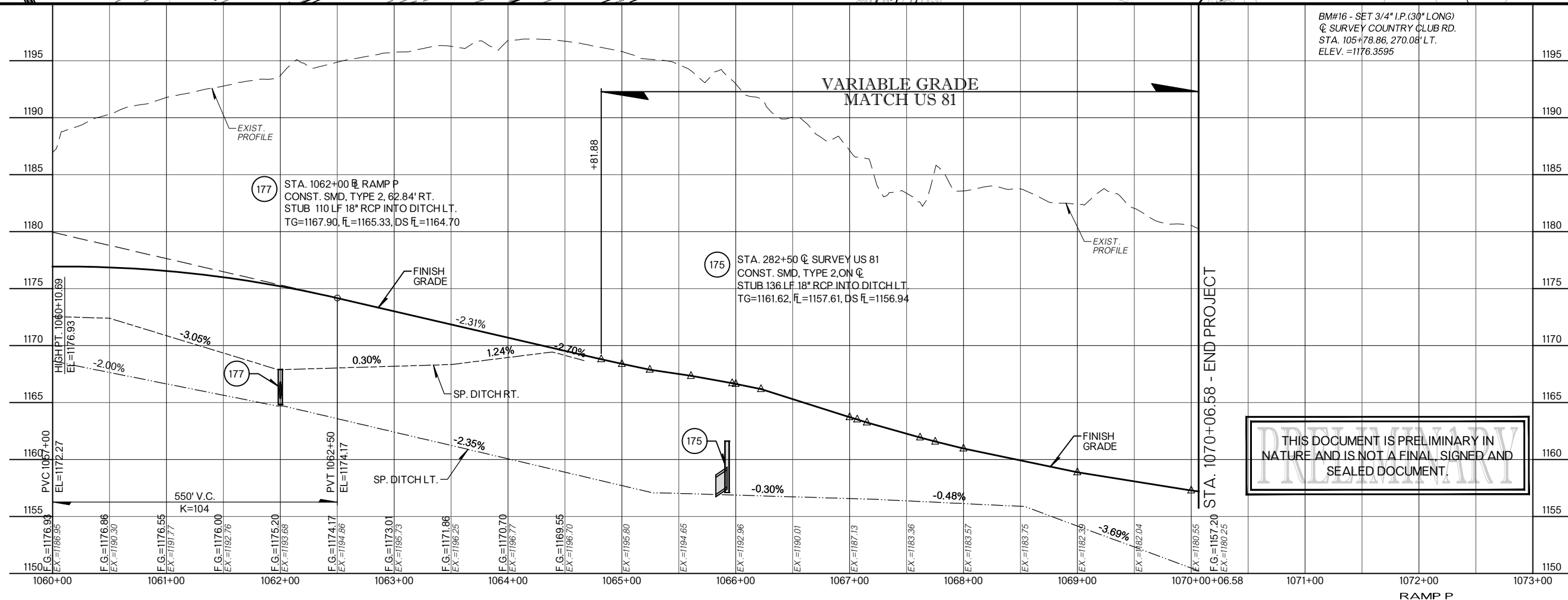
203 STA. 1057+25 RAMP P
CONST. SMD, TYPE 2, 29.31' RT.
STUB 72 LF 18" RCP INTO DITCH LT.
TG=1172.22, \bar{r}_L =1166.27, DS \bar{r}_L =1165.56

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



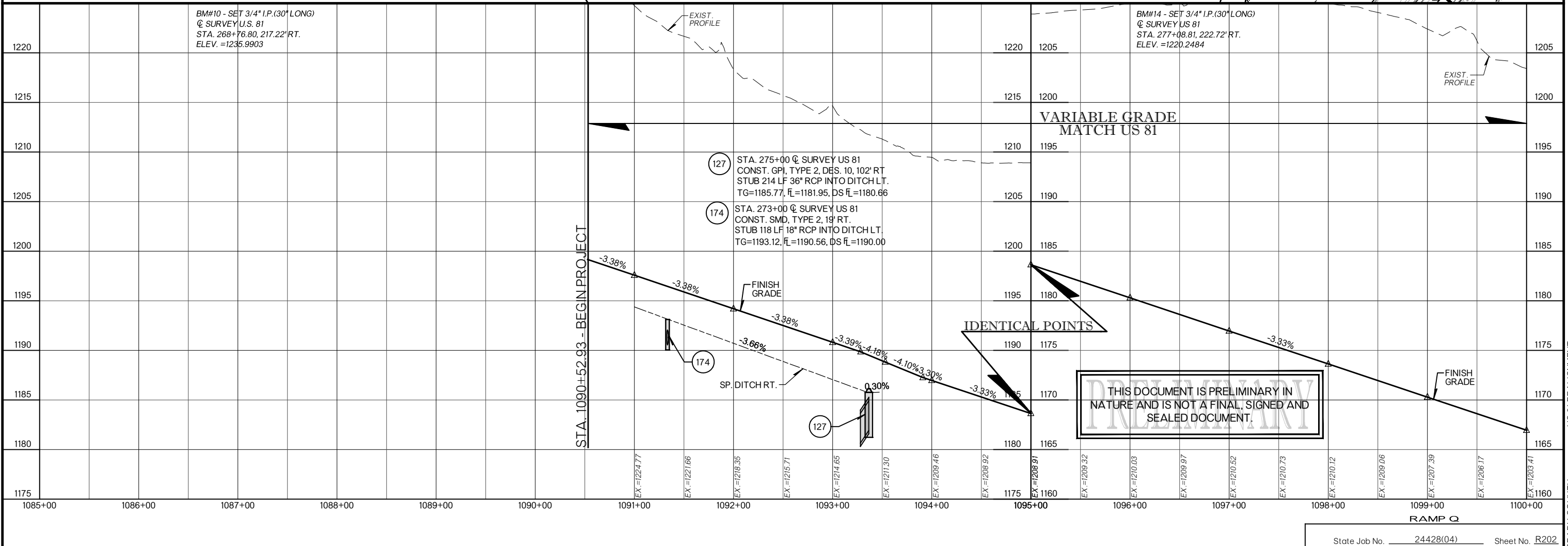
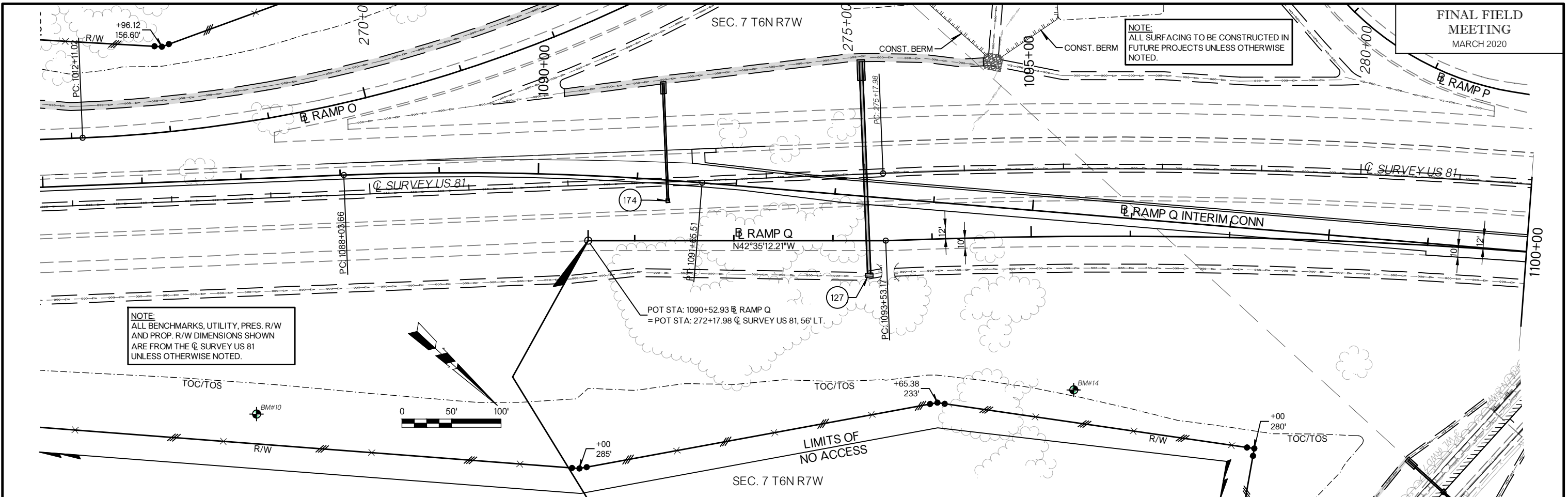
NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE C SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

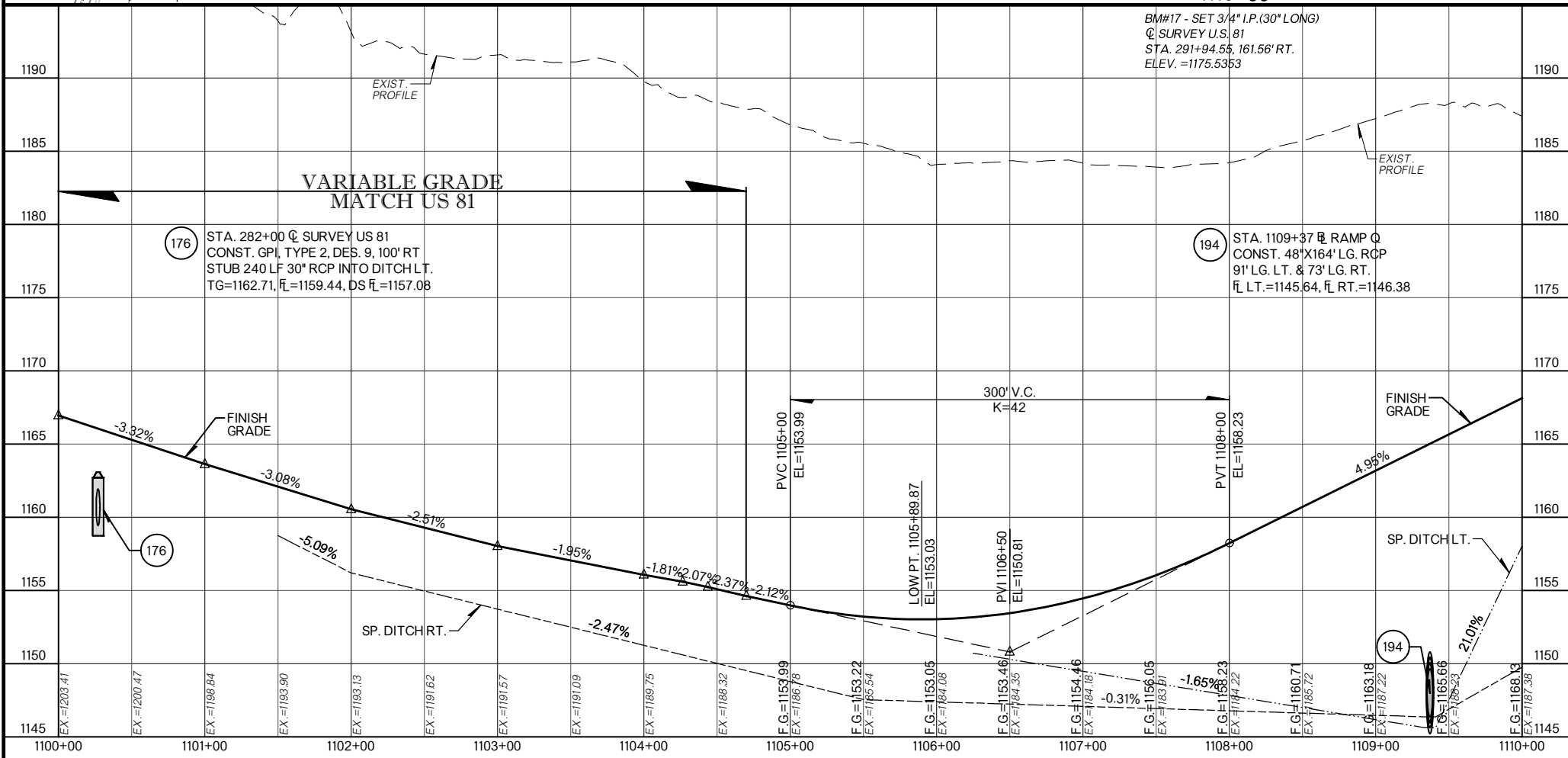
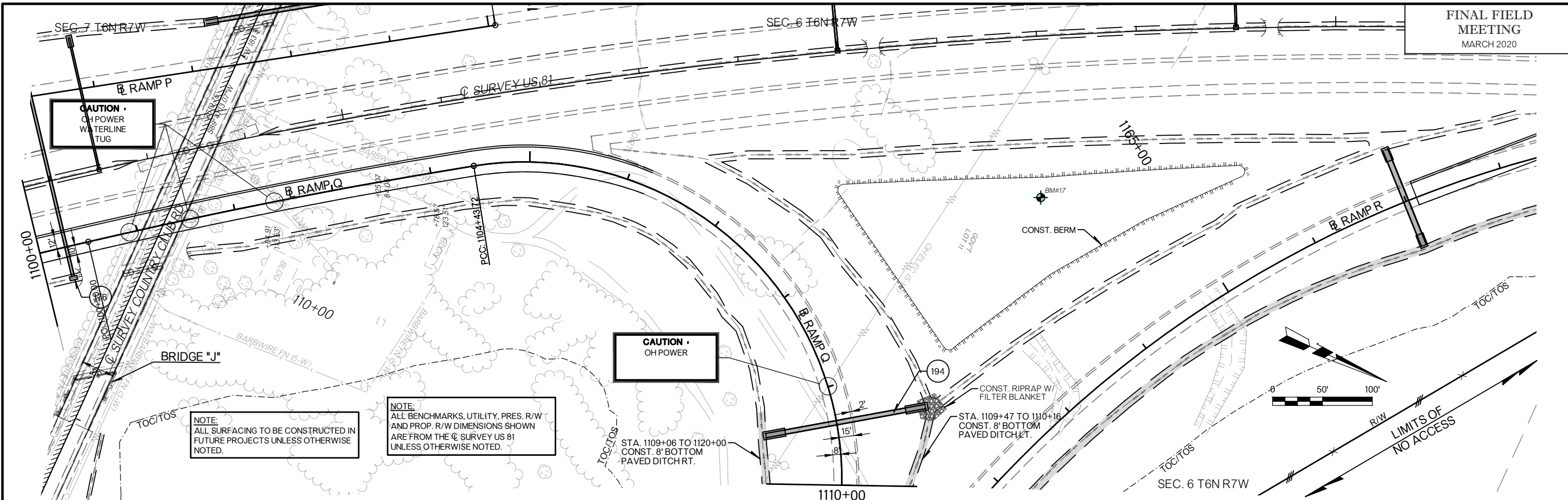


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

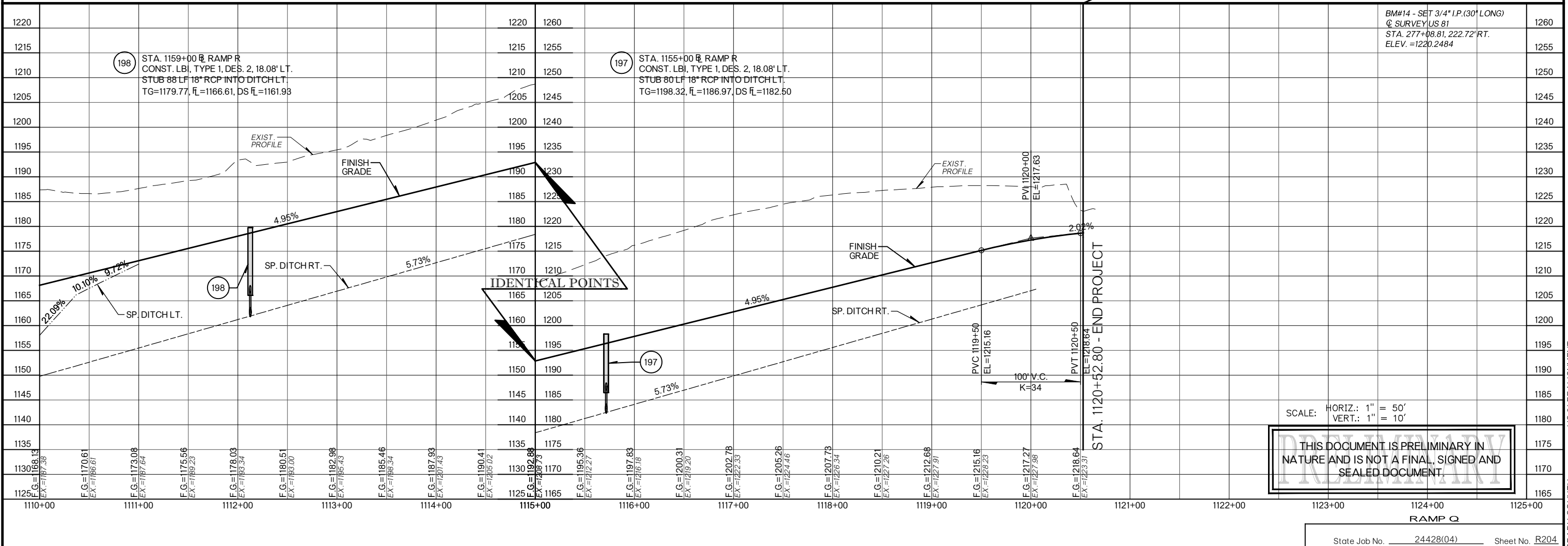
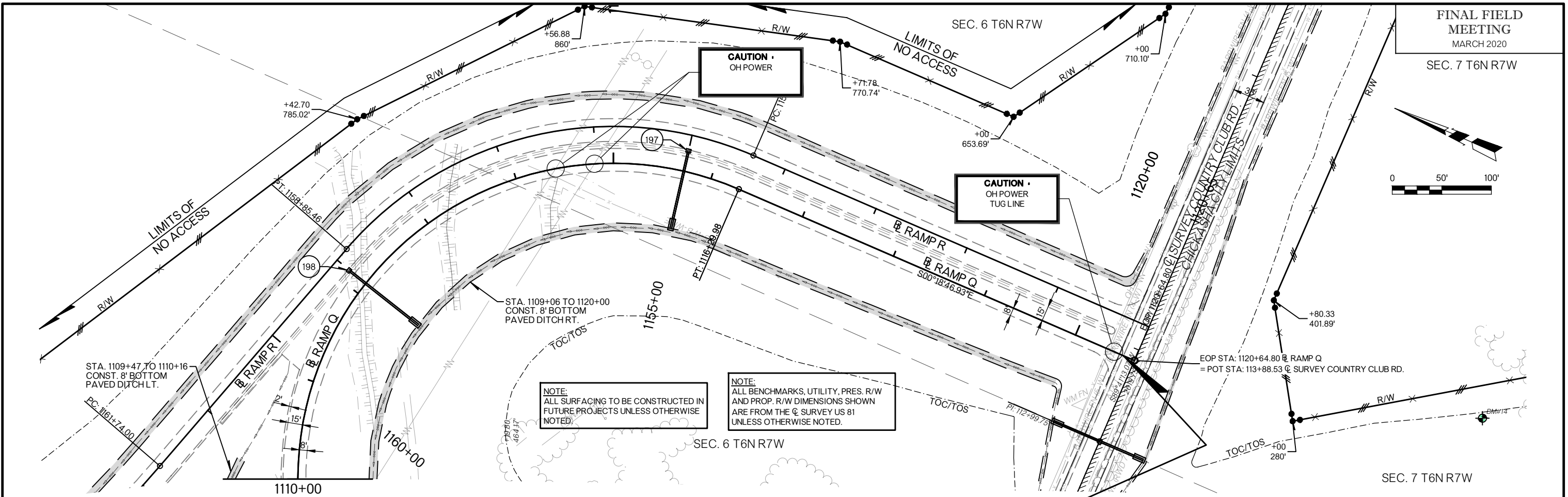
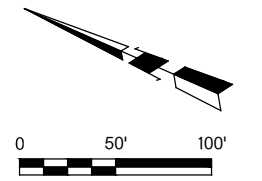


GRADY COUNTY
US 81 REALIGNMENT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADY COUNTY US 81 REALIGNMENT



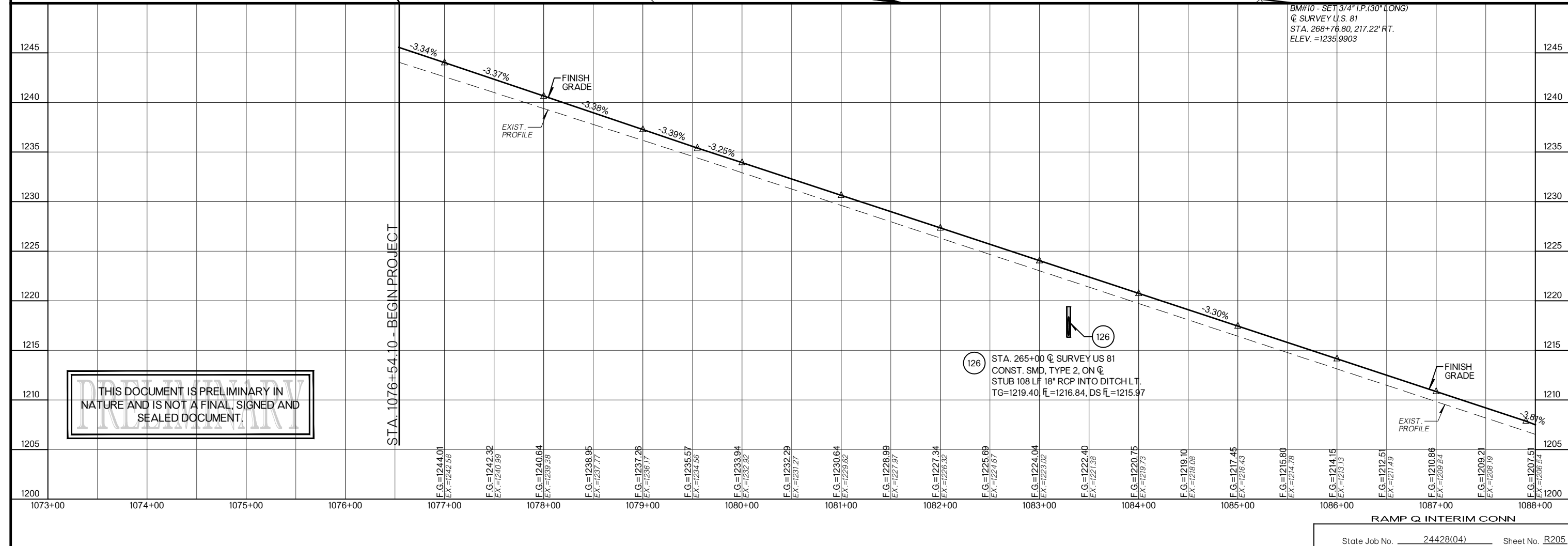
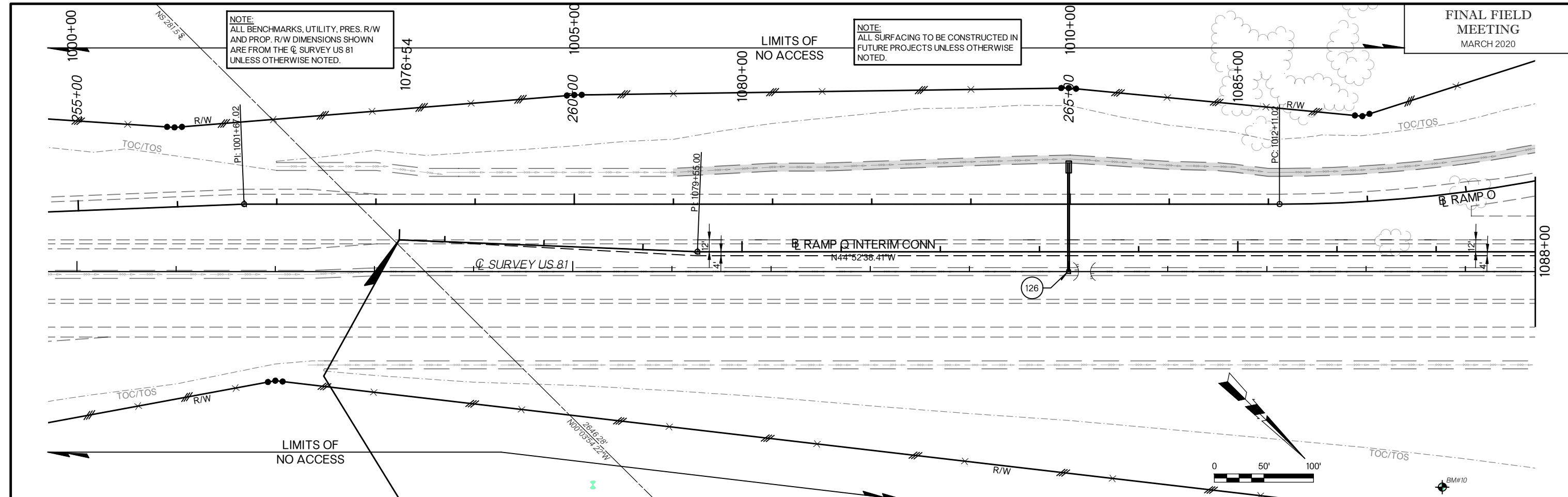
SCALE: HORIZ.: 1" = 50'
VERT.: 1" = 10'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADY COUNTY US 81 REALIGNMENT

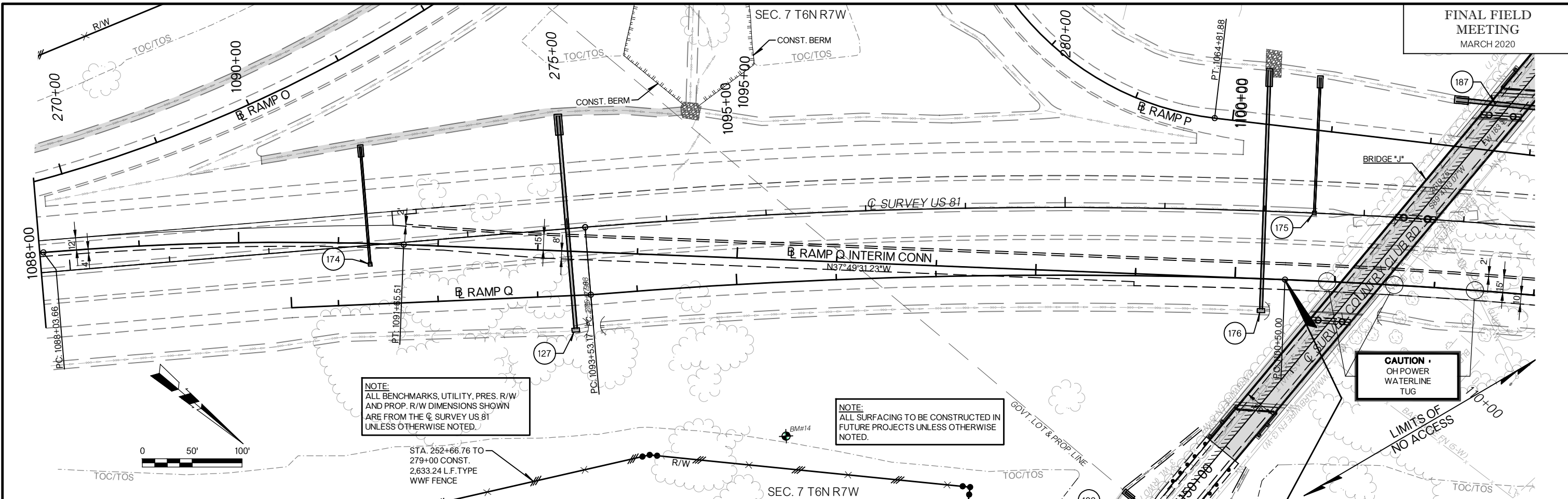
NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE \odot SURVEY US 81
UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

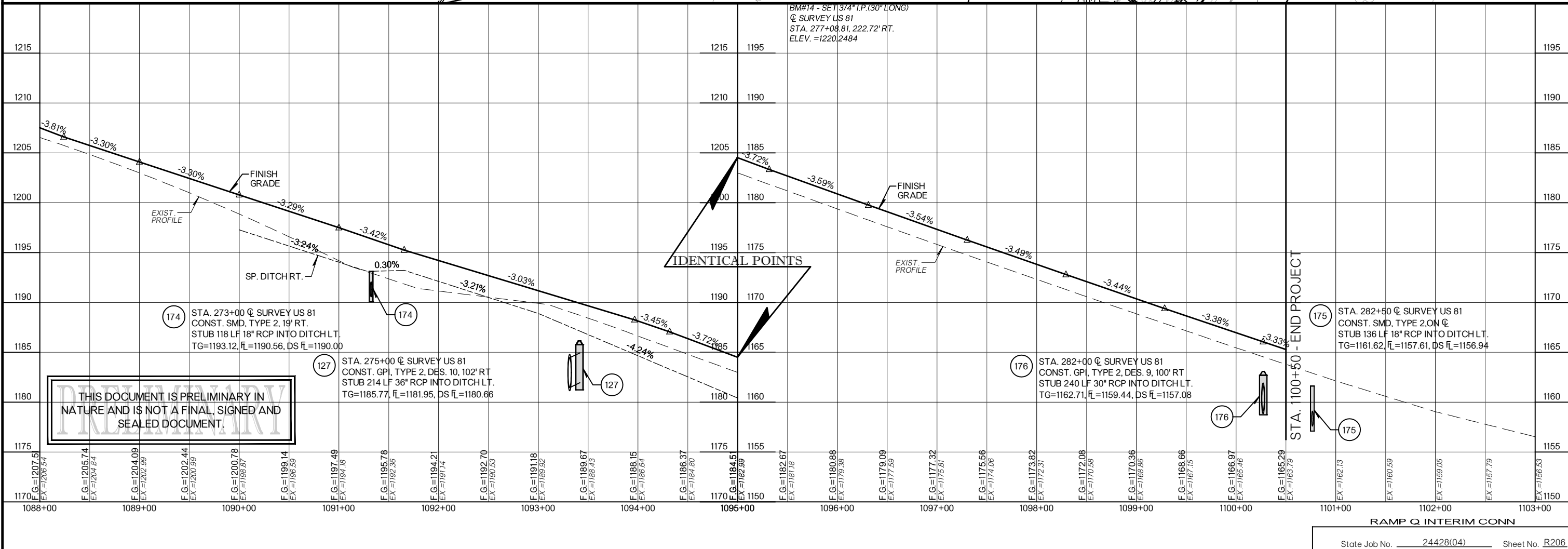
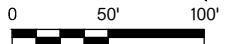
STA. 1076+54.10 - BEGIN PROJECT



NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY US 81
UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

CAUTION
OH POWER
WATERLINE
TUG



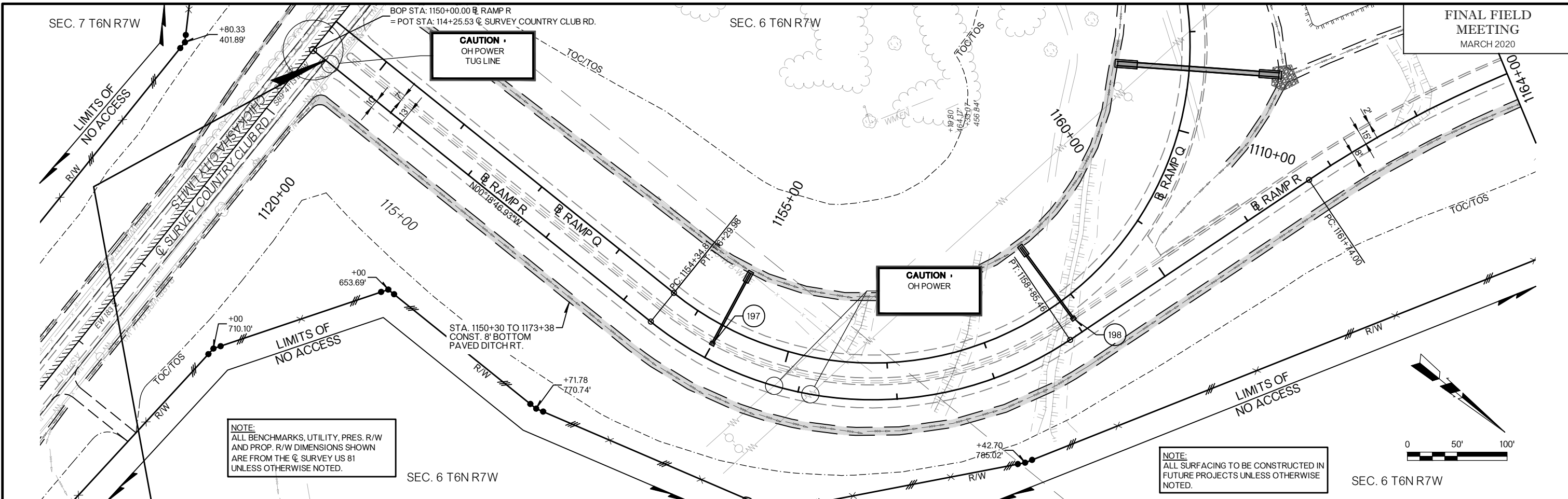
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

174 STA. 273+00 Q SURVEY US 81
CONST. SMD, TYPE 2, 19' RT.
STUB 118 LF 18" RCP INTO DITCH LT.
TG=1193.12, FL=1190.56, DS FL=1190.00

127 STA. 275+00 Q SURVEY US 81
CONST. GPI, TYPE 2, DES. 10, 102' RT.
STUB 214 LF 36" RCP INTO DITCH LT.
TG=1185.77, FL=1181.95, DS FL=1180.66

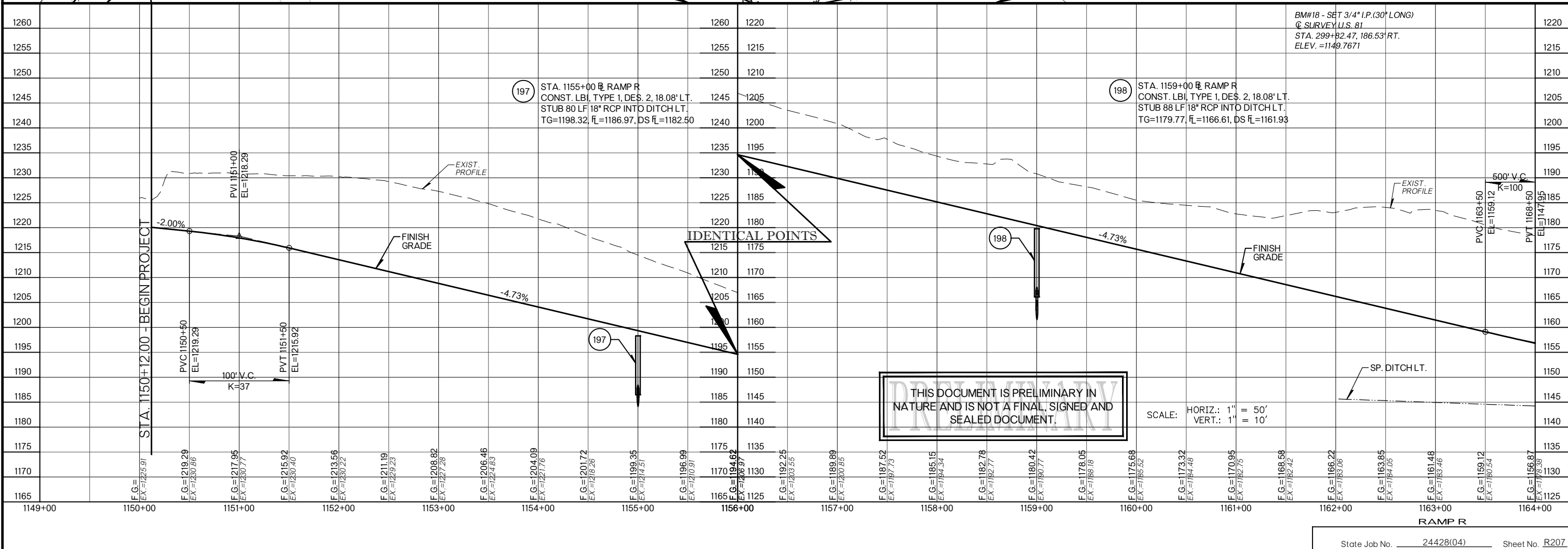
176 STA. 282+00 Q SURVEY US 81
CONST. GPI, TYPE 2, DES. 9, 100' RT.
STUB 240 LF 30" RCP INTO DITCH LT.
TG=1162.71, FL=1159.44, DS FL=1157.08

175 STA. 282+50 Q SURVEY US 81
CONST. SMD, TYPE 2, ON Q
STUB 136 LF 18" RCP INTO DITCH LT.
TG=1161.62, FL=1157.61, DS FL=1156.94



NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY US 81
UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

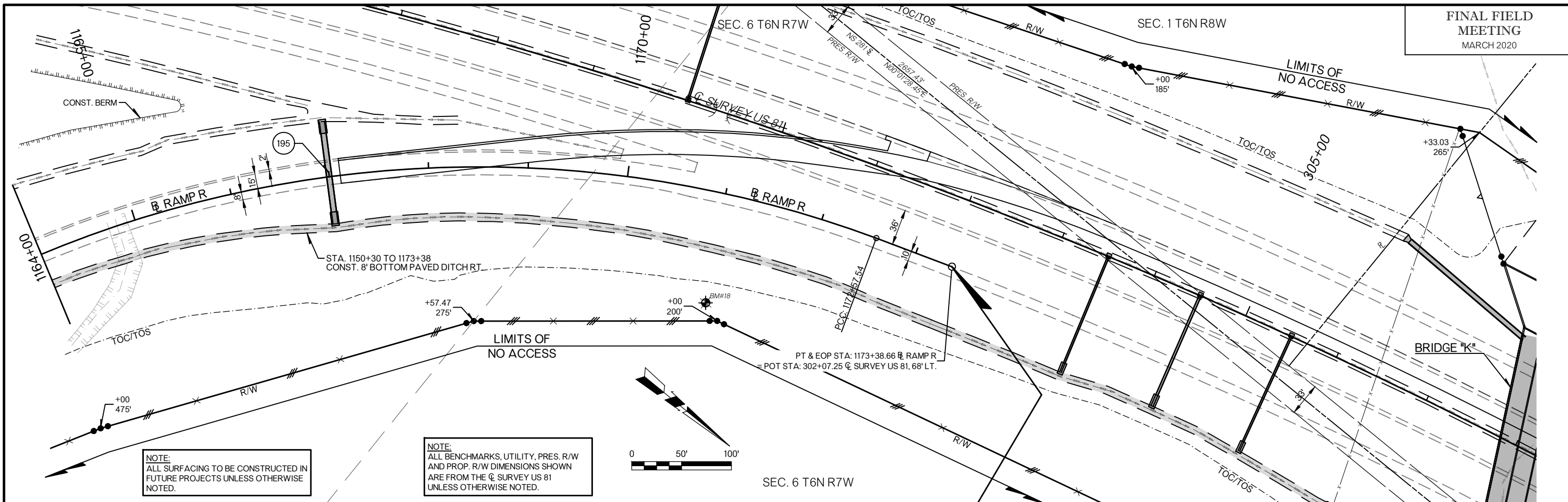


197 STA. 1155+00 RAMP R
CONST. LBI, TYPE 1, DES. 2, 18.08' LT.
STUB 80 LF 18" RCP INTO DITCH LT.
TG=1198.32, FL=1186.97, DS FL=1182.50

198 STA. 1159+00 RAMP R
CONST. LBI, TYPE 1, DES. 2, 18.08' LT.
STUB 88 LF 18" RCP INTO DITCH LT.
TG=1179.77, FL=1166.61, DS FL=1161.93

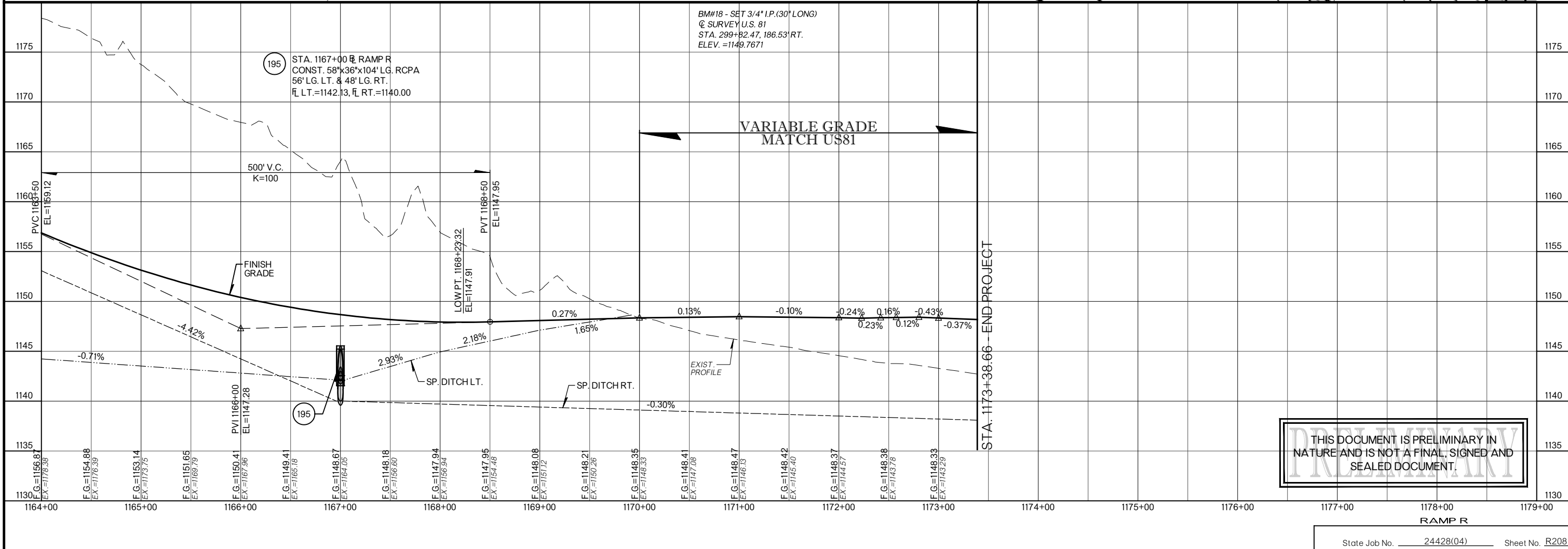
THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

SCALE: HORIZ.: 1" = 50'
VERT.: 1" = 10'



NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY US 81 UNLESS OTHERWISE NOTED.

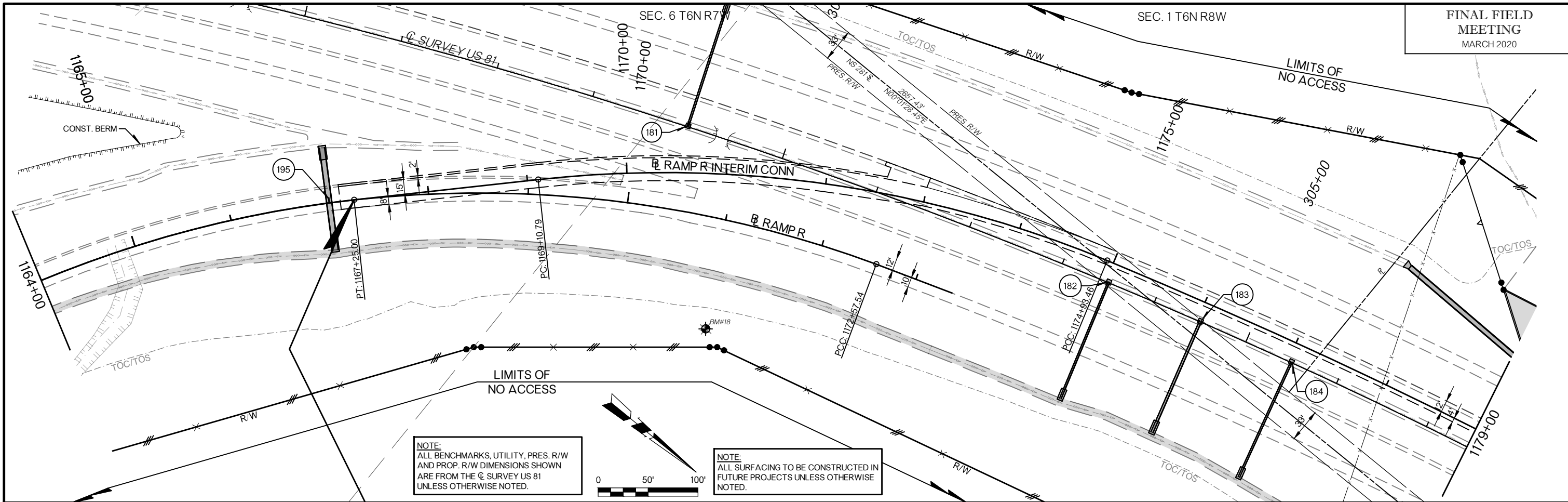


BM#18 - SET 3/4" I.P. (30' LONG)
Q SURVEY U.S. 81
STA. 299+82.47, 186.53' RT.
ELEV. = 1149.7671

195 STA. 1167+00 RAMP R
CONST. 58"x36"x104' LG. RCPA
56' LG. LT. & 48' LG. RT.
FL LT.=1142.13, FL RT.=1140.00

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADY COUNTY US 81 REALIGNMENT

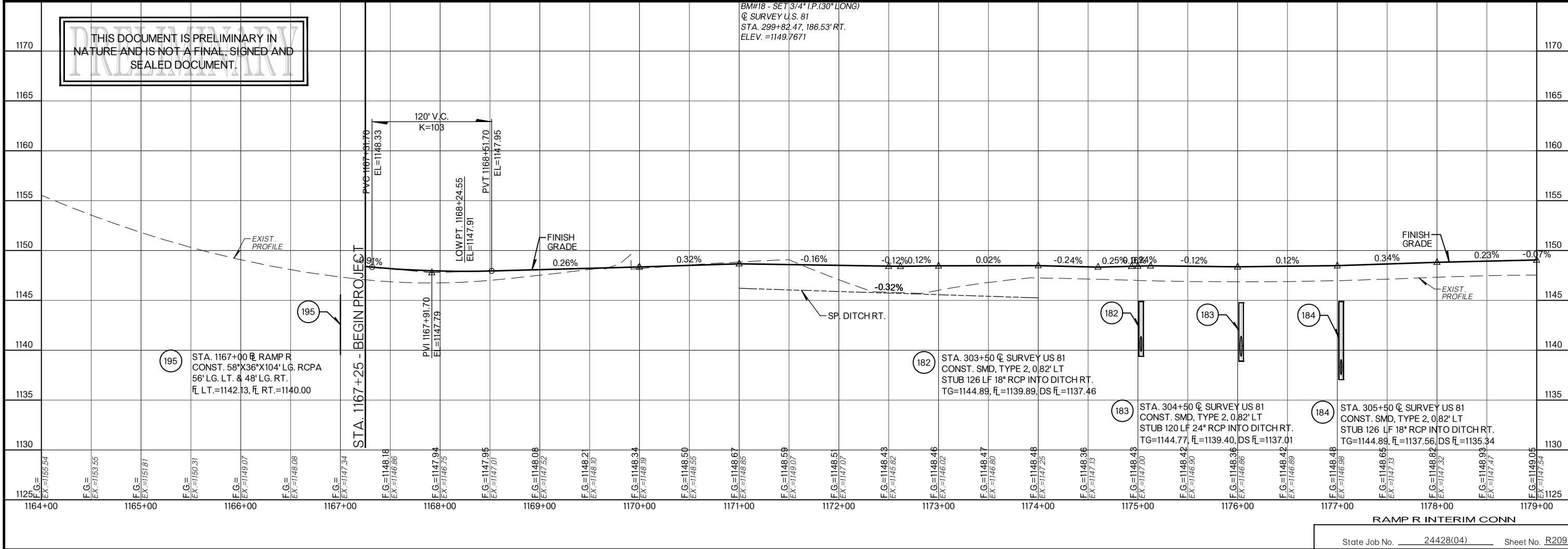


NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W
AND PROP. R/W DIMENSIONS SHOWN
ARE FROM THE Q SURVEY US 81
UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN
FUTURE PROJECTS UNLESS OTHERWISE
NOTED.

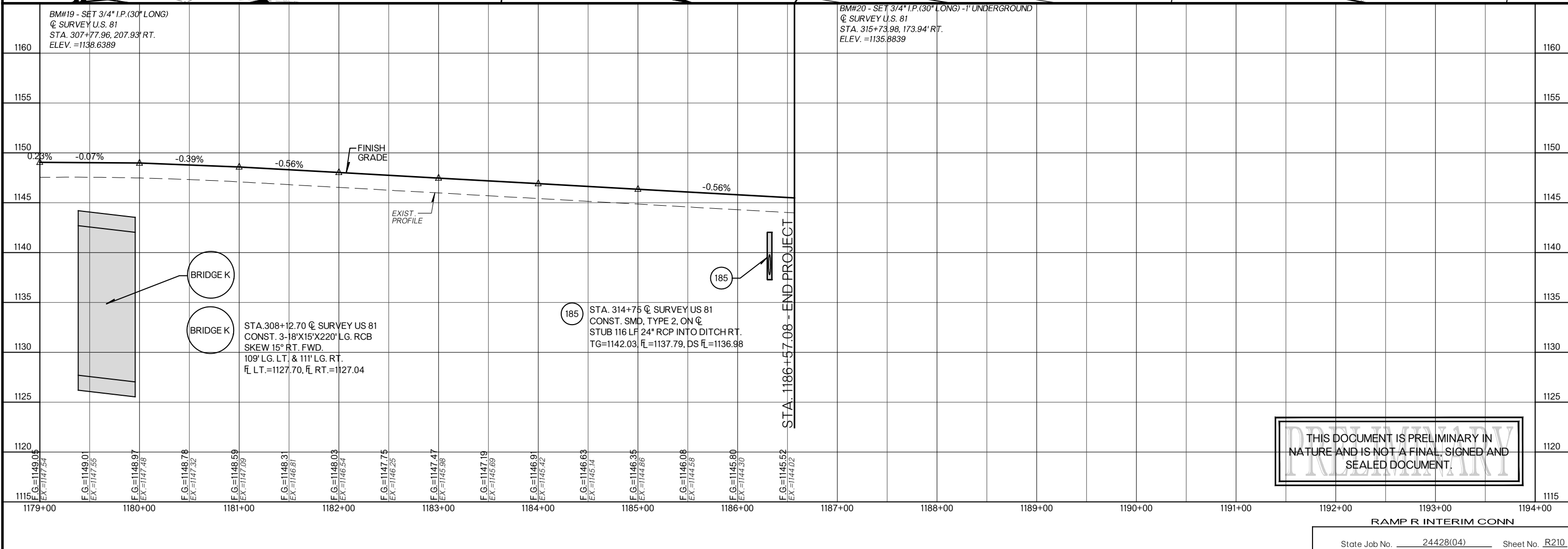
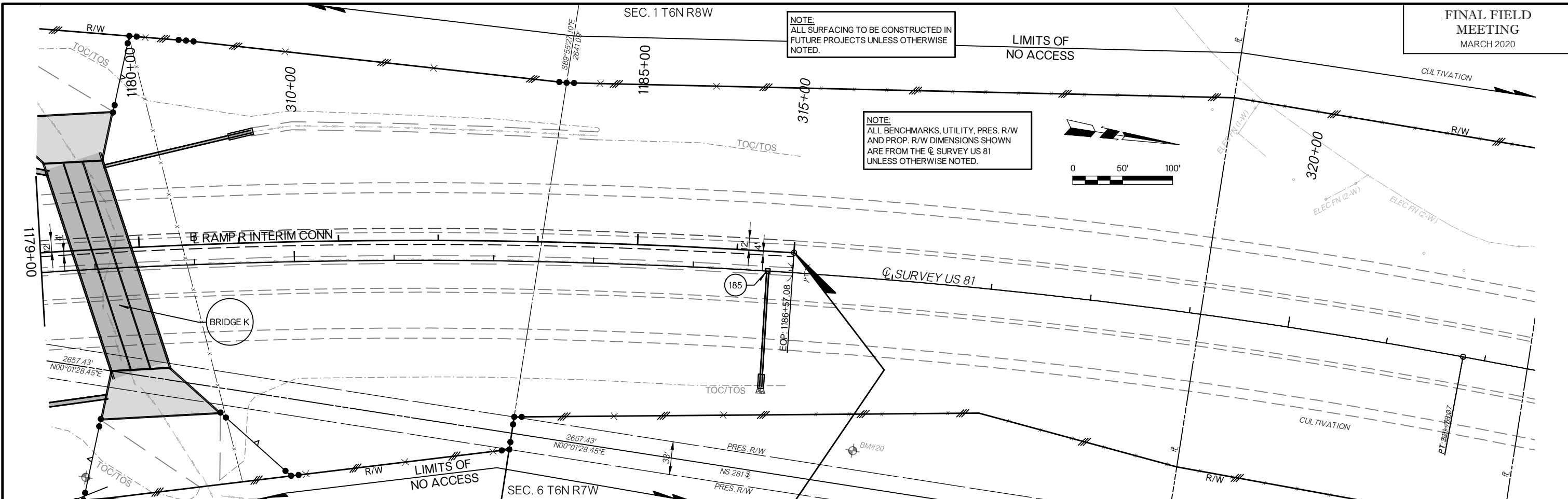
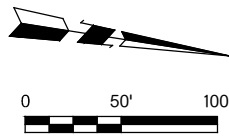
THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

BM#18 - SET 3/4" I.P. (30" LONG)
Q SURVEY U.S. 81
STA. 299+82.47, 186.53' RT.
ELEV. =1149.7671



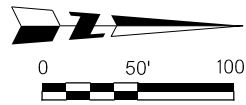
NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W AND PROP. R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY US 81 UNLESS OTHERWISE NOTED.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

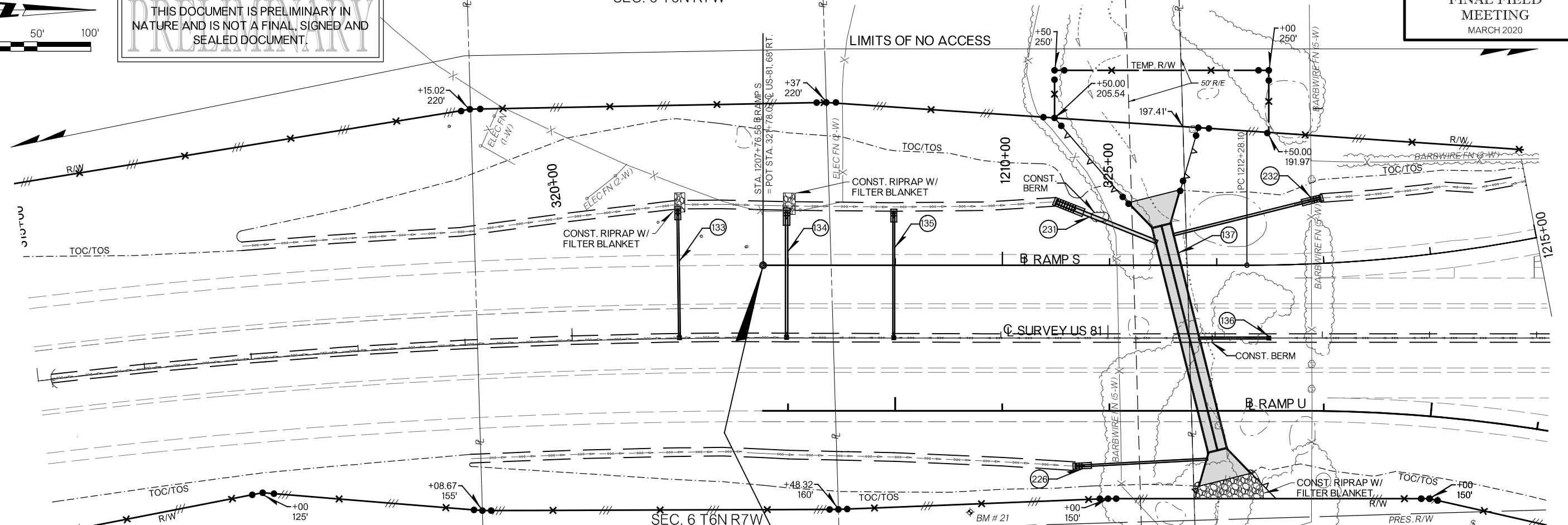
US 81 REALIGNMENT
GRADY COUNTY



PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

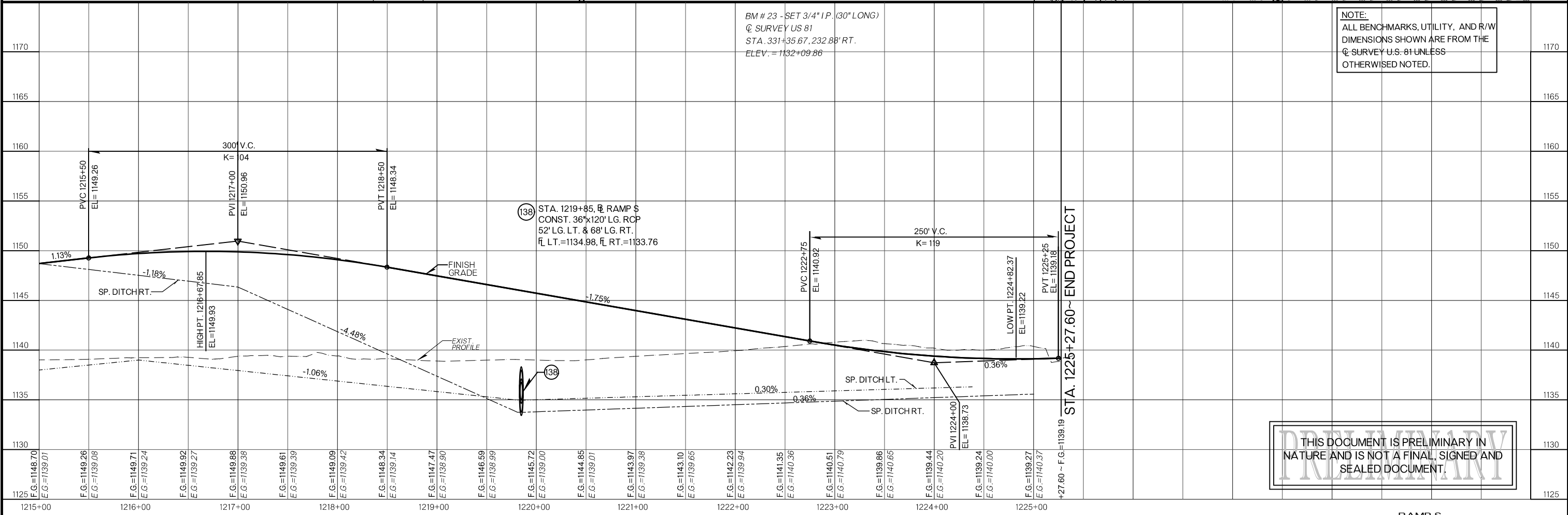
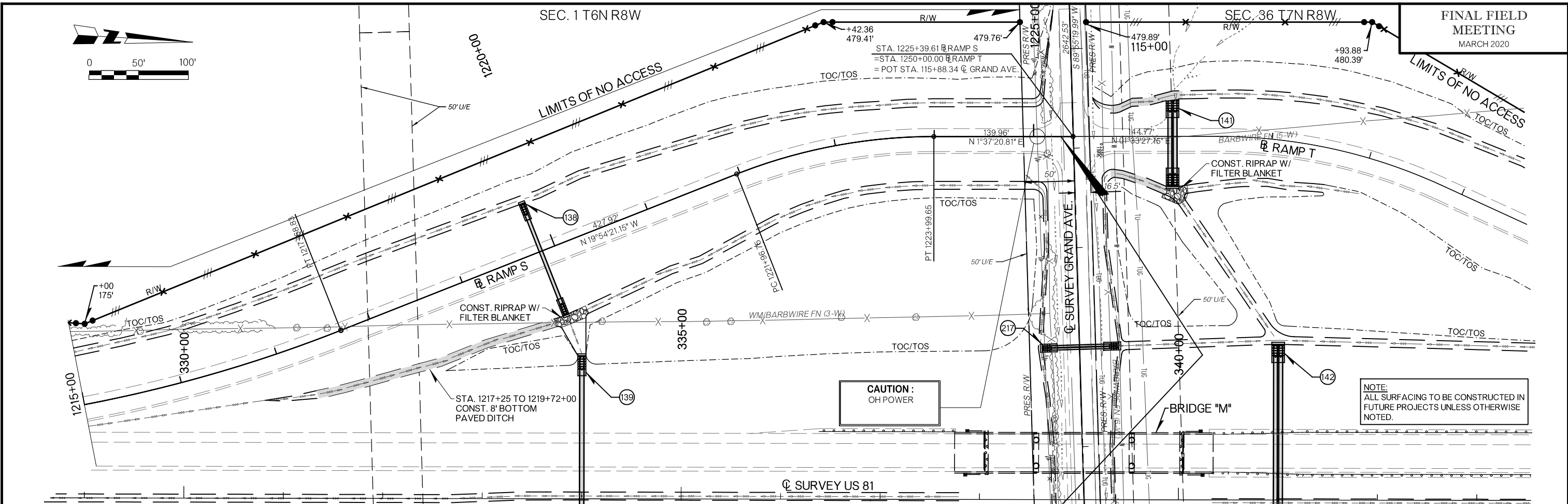
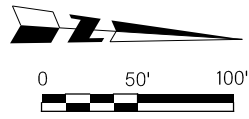
SEC. 6 T6N R7W

FINAL FIELD MEETING
MARCH 2020



SEC. 6 T6N R7W

| | | | | | | | | | | | | | | | | | |
|---------|--|---|--|---|---|---|---------|---|---------|---------|---------|---------|---------|---------|---------|-------------------|--------------|
| 1170 | <p>NOTE: ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.</p> | <p>NOTE: ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.</p> | <p>BM # 21 - SET 3/4" I.P. (30" LONG) - 1' UNDERGROUND Q SURVEY US 81 STA. 323+71.24, 162.63 RT. ELEV. = 1143.2519</p> | | 1170 | | | | | | | | | | | | |
| 1165 | | | | <p>VARIABLE GRADE MATCH US 81</p> | 1165 | | | | | | | | | | | | |
| 1160 | | | | | 1160 | | | | | | | | | | | | |
| 1155 | | | | <p>(134) STA. 322+00, Q SURVEY US 81 CONST. SMD, TYPE 2, 0.50' LT. STUB 118 LF 24" RCP INTO DITCH LT. TG=1138.28, FL=1135.21, DS FL=1134.47</p> | <p>(231) STA. 1210+50, RAMP S CONST. TYPE D6 CET, 59.16' LT. STUB 102 LF 36" RCP INTO STR. 137 FL=1133.72, DS FL=1133.42</p> | <p>(232) STA. 1213+00, RAMP S CONST. TYPE B6 CET, 61' LT. STUB 142 LF 24" RCP INTO STR. 137 FL=1136.00, DS FL=1135.29</p> | 1155 | | | | | | | | | | |
| 1150 | | | | <p>(135) STA. 323+00, Q SURVEY US 81 CONST. SMD, TYPE 2, 0.22' LT. STUB 118 LF 18" RCP INTO DITCH LT. TG=1138.57, FL=1135.33, DS FL=1134.17</p> | <p>(137) STA. 325+76, Q SURVEY US 81 CONST. 2-8"x7"x218' LG. RCB SKEW 14° RT. FWD. 109' LG. LT. & 109' LG. RT. FL LT.=1132.65, FL RT.=1132.00</p> | 1150 | | | | | | | | | | | |
| 1145 | | | | | 1145 | | | | | | | | | | | | |
| 1140 | | | | <p>STA. 1207+76.56 ~ BEGIN PROJECT</p> <p>FINISH GRADE</p> <p>EXIST. PROFILE</p> <p>SP. DITCH LT.</p> <p>16.36%</p> <p>3.90%</p> <p>16.65%</p> <p>1.00%</p> | 1140 | | | | | | | | | | | | |
| 1135 | | | | | 1135 | | | | | | | | | | | | |
| 1130 | | | | | 1130 | | | | | | | | | | | | |
| 1125 | | | | | 1125 | | | | | | | | | | | | |
| 1200+00 | 1201+00 | 1202+00 | 1203+00 | 1204+00 | 1205+00 | 1206+00 | 1207+00 | 1208+00 | 1209+00 | 1210+00 | 1211+00 | 1212+00 | 1213+00 | 1214+00 | 1215+00 | US 81 REALIGNMENT | |
| | | | | | | | | <p>STA. 1207+76.56 - F.G.=1143.16</p> <p>E.G.=1156.19</p> <p>E.G.=1155.33</p> <p>E.G.=1151.55</p> <p>E.G.=1149.83</p> <p>E.G.=1146.27</p> <p>E.G.=1144.18</p> <p>E.G.=1142.57</p> <p>E.G.=1140.36</p> <p>E.G.=1138.78</p> <p>E.G.=1136.75</p> <p>E.G.=1139.50</p> <p>E.G.=1139.37</p> <p>E.G.=1139.36</p> <p>E.G.=1139.20</p> <p>F.G.=1148.70</p> <p>E.G.=1139.01</p> | | | | | | | | | GRADY COUNTY |



NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE \odot SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADY COUNTY
US 81 REALIGNMENT

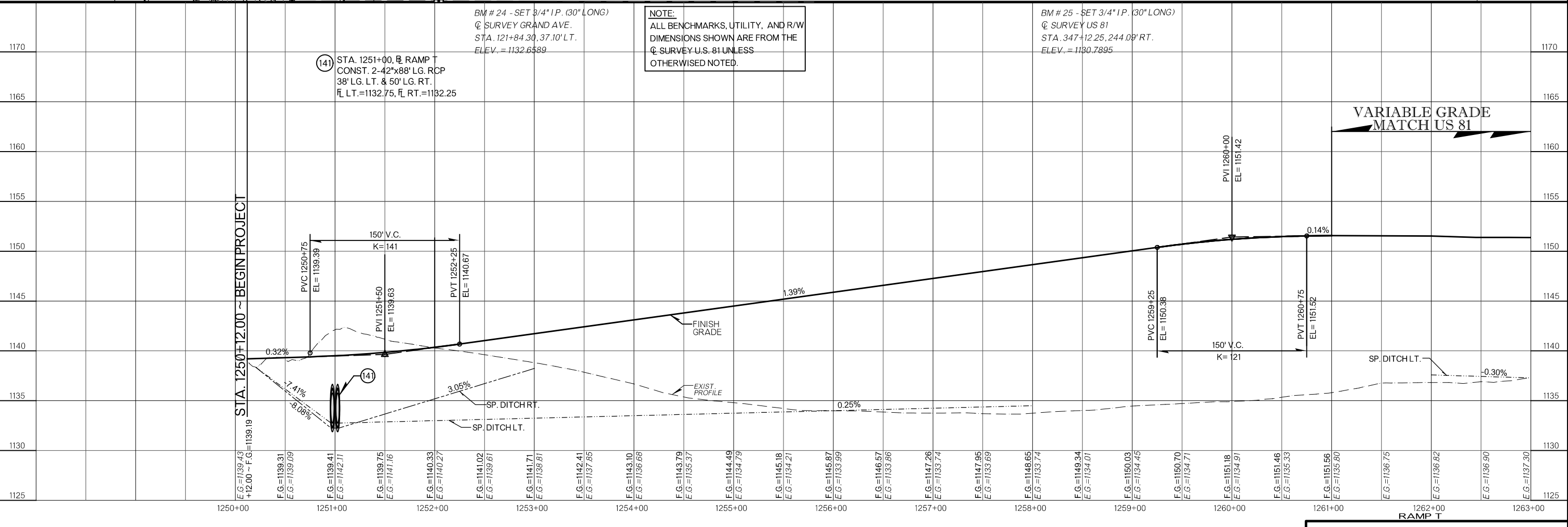
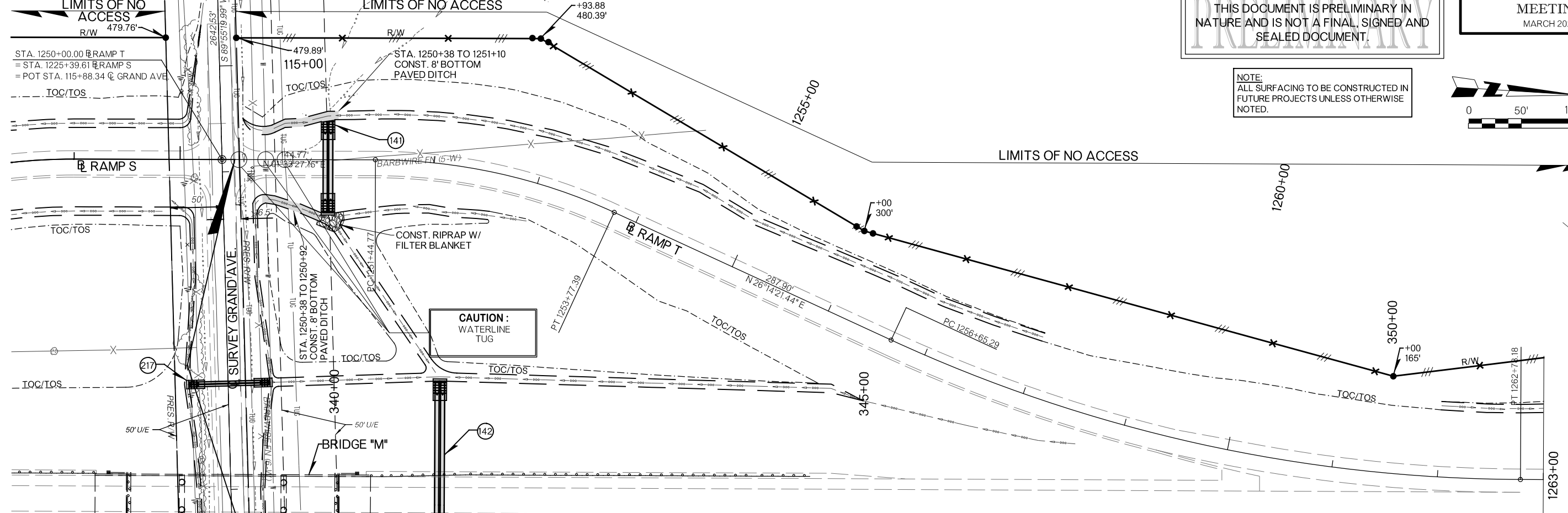
SEC. 1 T6N R8W

SEC. 36 T7N R8W

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.



NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE C. SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

(141) STA. 1251+00, RAMP T
CONST. 2-42"x88" LG. RCP
38' LG. LT. & 50' LG. RT.
FL LT.=1132.75, FL RT.=1132.25

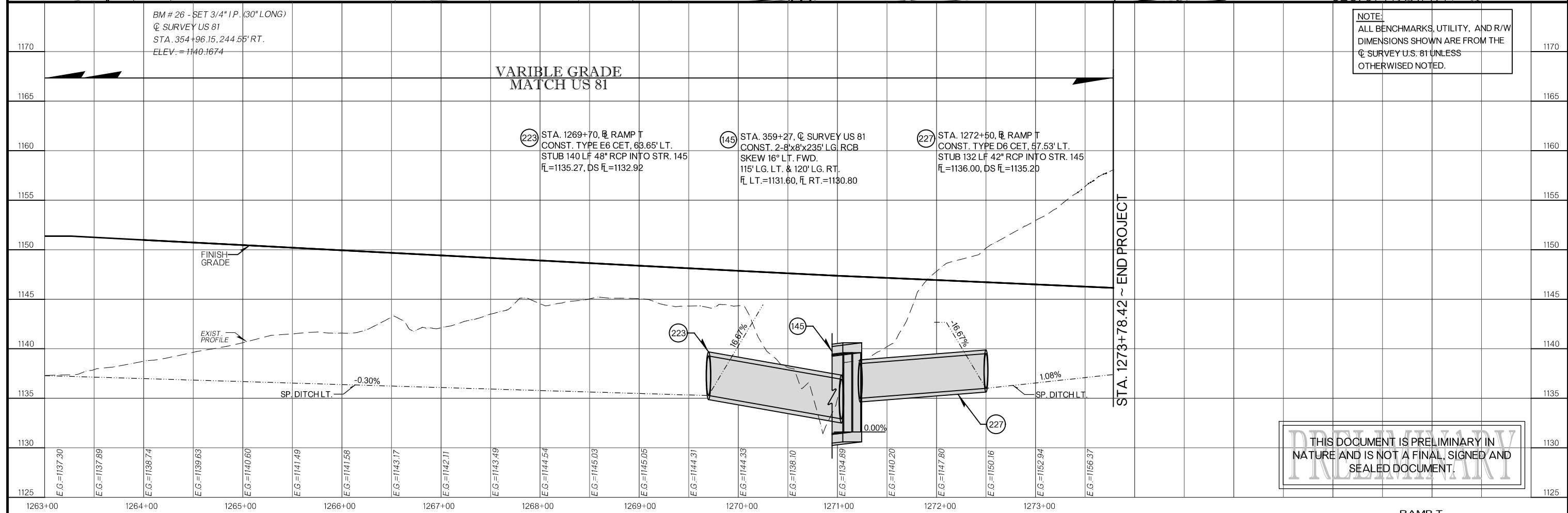
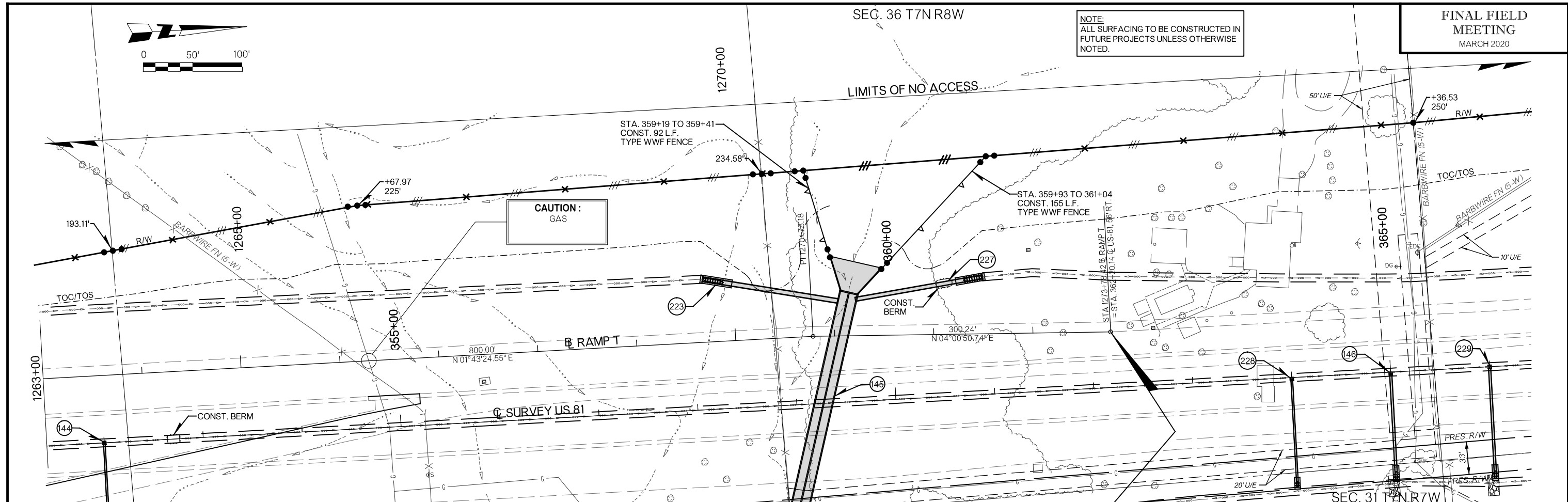
BM # 24 - SET 3/4" I.P. (30" LONG)
C. SURVEY GRAND AVE.
STA. 121+84.30, 37.10' LT.
ELEV. = 1132.6589

BM # 25 - SET 3/4" I.P. (30" LONG)
C. SURVEY US 81
STA. 347+12.25, 244.09' RT.
ELEV. = 1130.7895

STA. 1250+12.00 ~ BEGIN PROJECT

US 81 REALIGNMENT
GRADY COUNTY

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

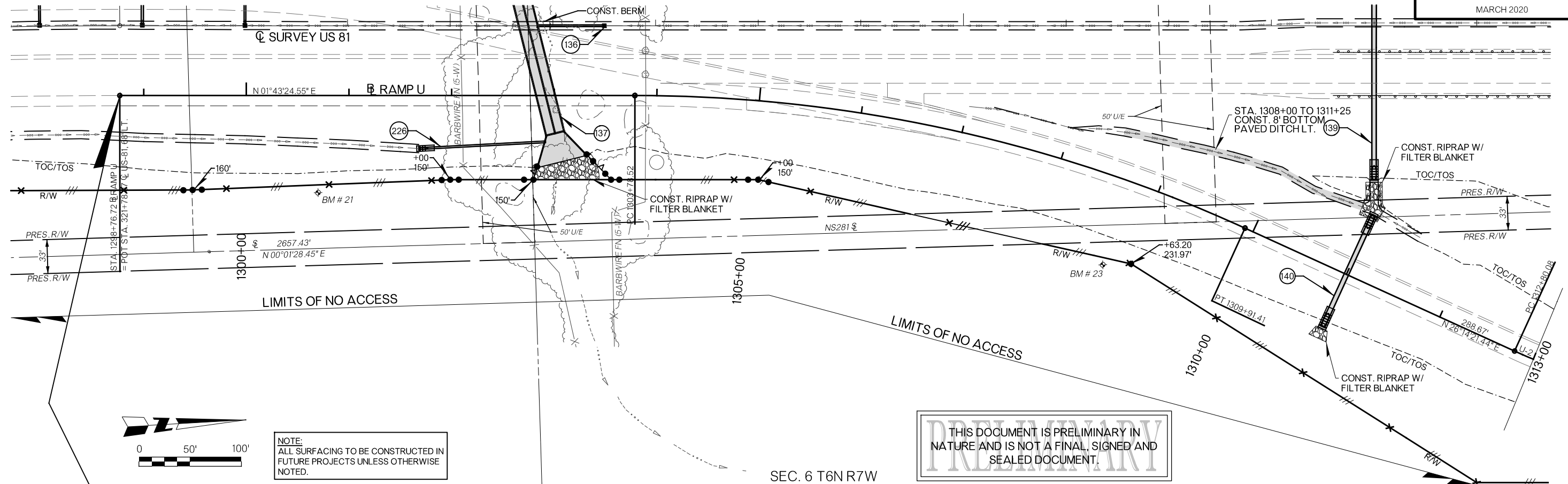


NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

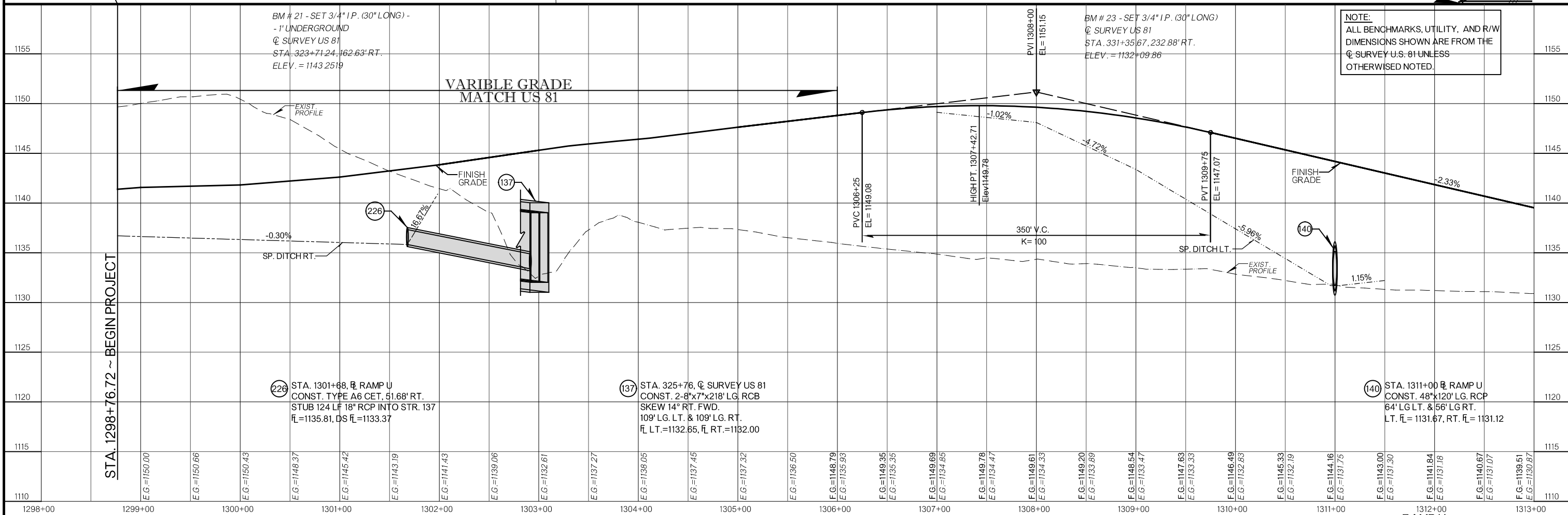
RAMP T

GRADY COUNTY US 81 REALIGNMENT



SEC. 6 T6N R7W

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

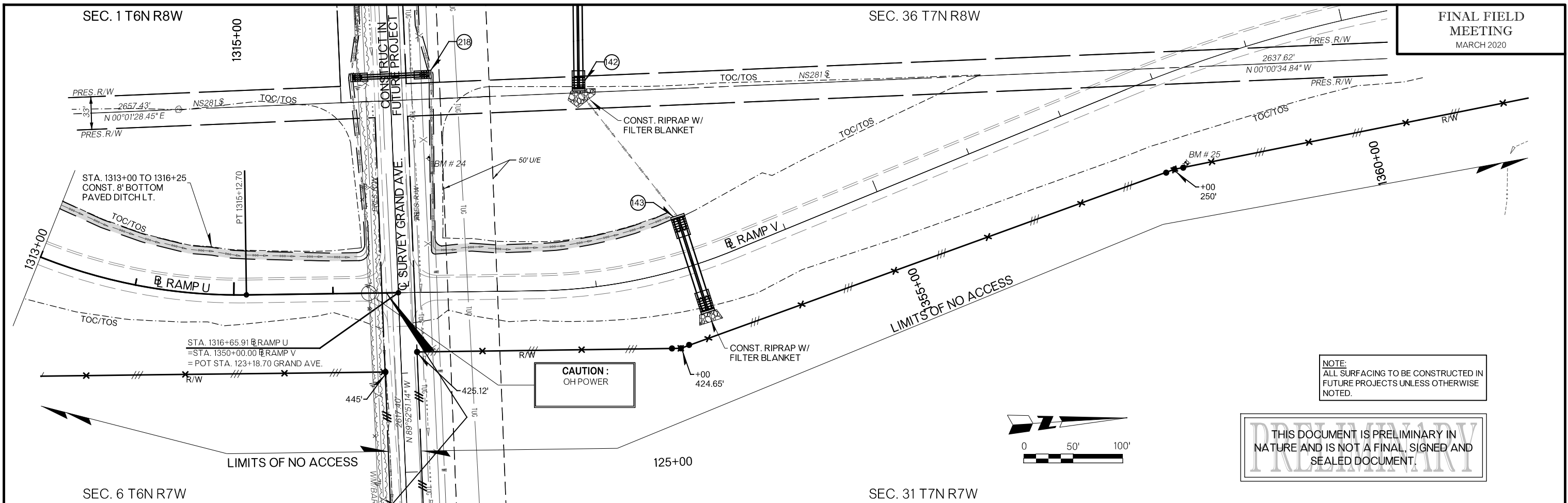


STA. 1298+76.72 ~ BEGIN PROJECT

(226) STA. 1301+68, RAMP U
CONST. TYPE A6 CET, 51.68' RT.
STUB 124 LF 18" RCP INTO STR. 137
FL = 1135.81, DS FL = 1133.37

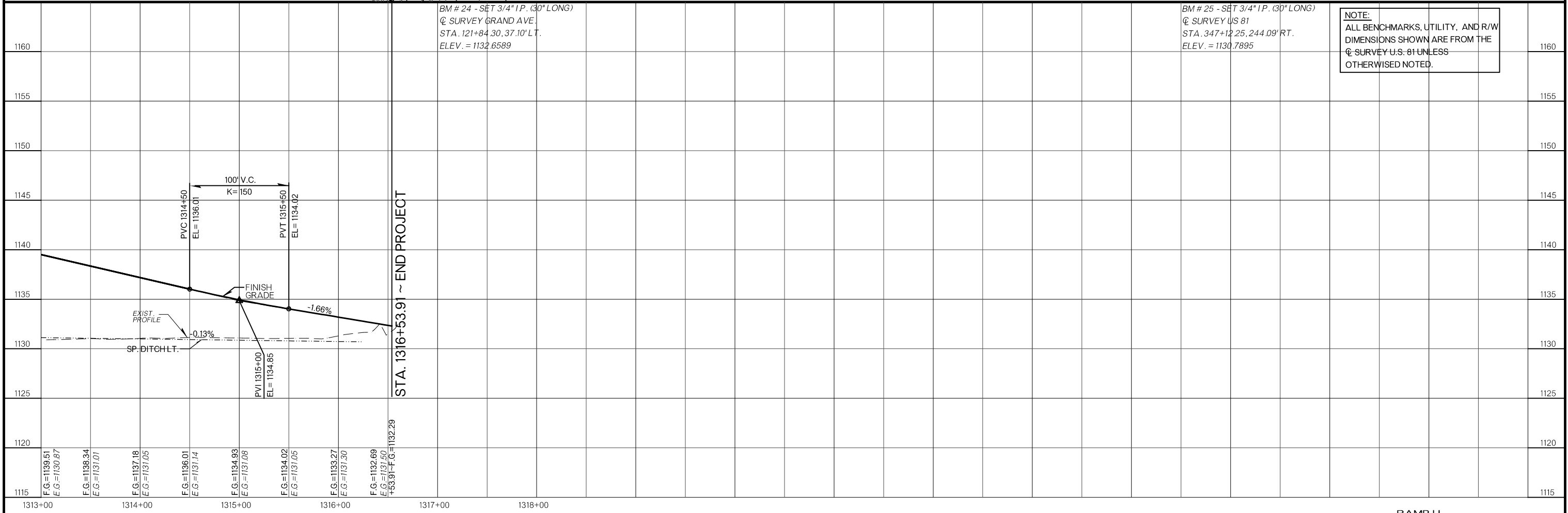
(137) STA. 325+76, Q SURVEY US 81
CONST. 2-8"x7"x218' LG. RCB
SKEW 14° RT. FWD.
109' LG. LT. & 109' LG. RT.
FL LT. = 1132.65, FL RT. = 1132.00

(140) STA. 1311+00 RAMP U
CONST. 48"x120' LG. RCP
64' LG LT. & 56' LG RT.
LT. FL = 1131.67, RT. FL = 1131.12



NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



BM # 24 - SET 3/4" I.P. (30" LONG)
@ SURVEY GRAND AVE.
STA. 121+84.30, 37.10' LT.
ELEV. = 1132.6589

BM # 25 - SET 3/4" I.P. (30" LONG)
@ SURVEY US 81
STA. 347+12.25, 244.09' RT.
ELEV. = 1130.7895

NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE @ SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

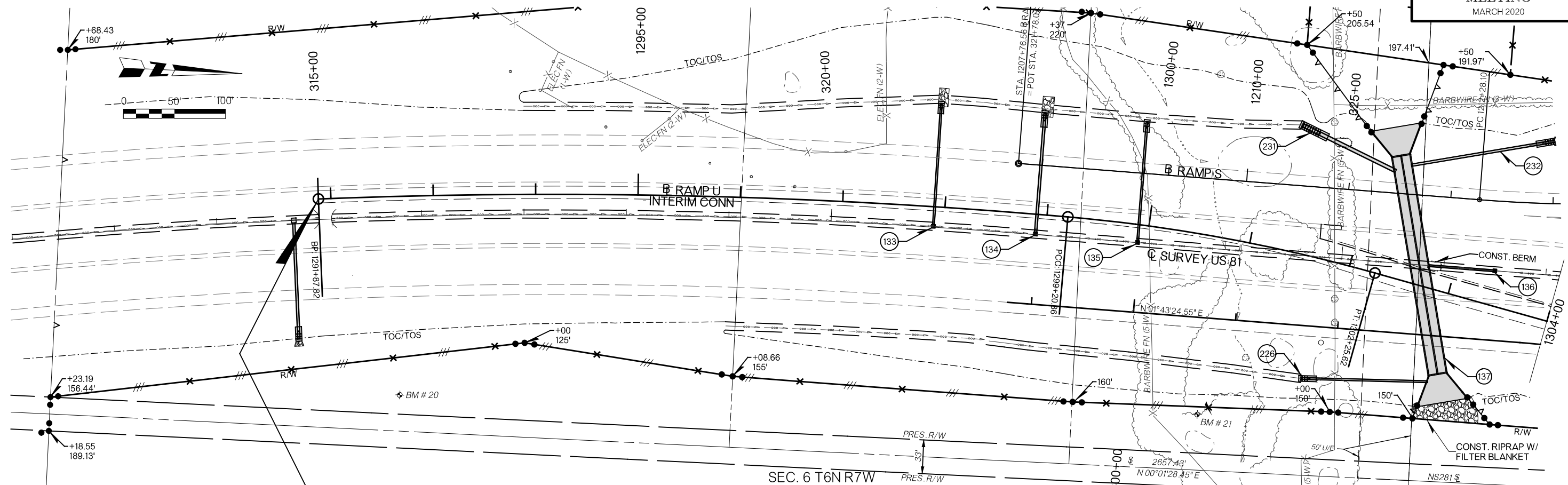
STA. 1316+53.91 ~ END PROJECT

+53.91 FT G.=1132.29

RAMP U

SEC. 1 T6N R8W

SEC. 6 T6N R7W



NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

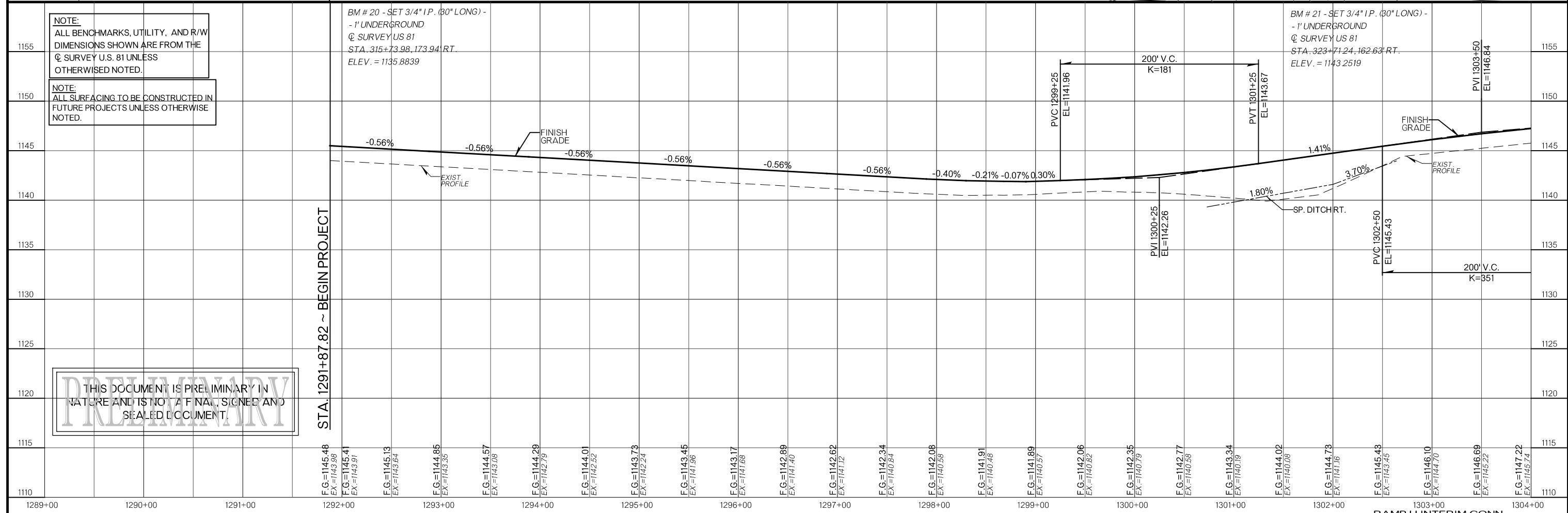
NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

BM # 20 - SET 3/4" I.P. (30" LONG) -
- 1' UNDERGROUND
Q SURVEY US 81
STA. 315+73.98, 173.94 RT.
ELEV. = 1135.8839

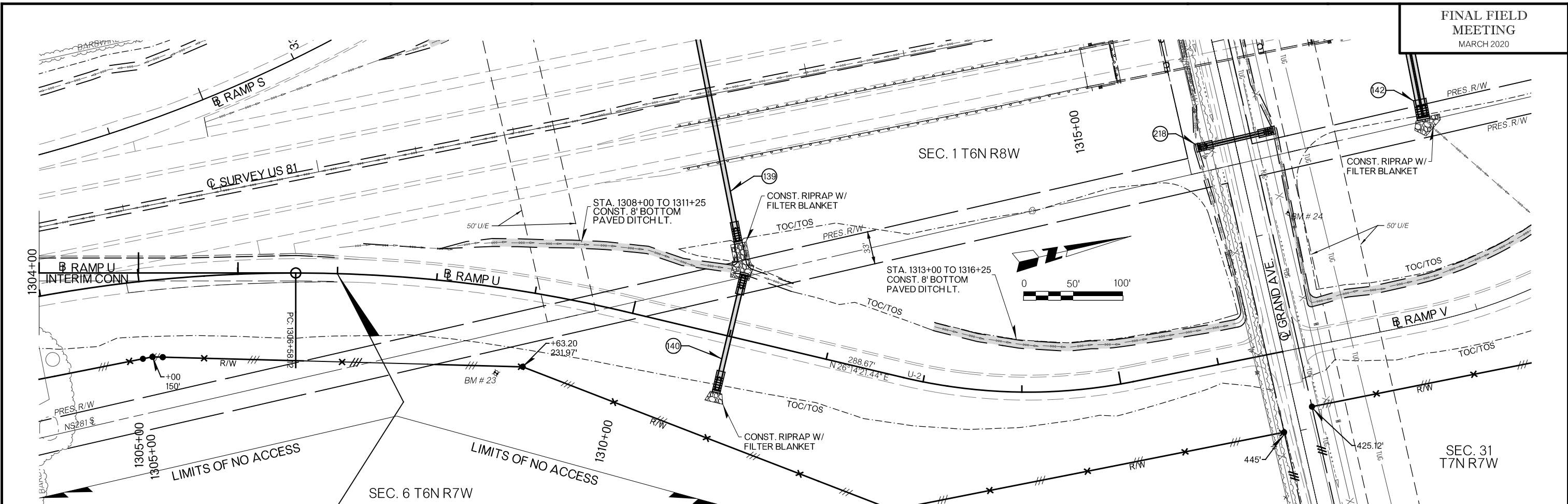
BM # 21 - SET 3/4" I.P. (30" LONG) -
- 1' UNDERGROUND
Q SURVEY US 81
STA. 323+71.24, 162.63 RT.
ELEV. = 1143.2519

STA. 1291+87.82 ~ BEGIN PROJECT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

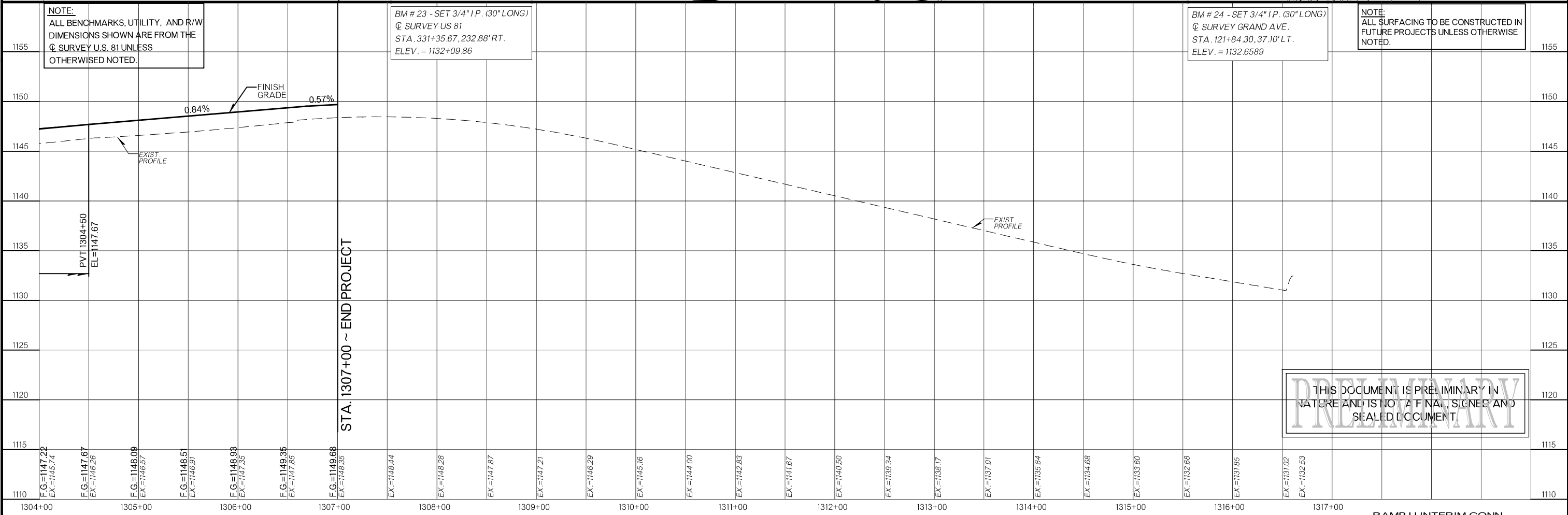


NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

BM # 23 - SET 3/4" I.P. (30" LONG)
Q SURVEY US 81
STA. 331+35.67, 232.88' RT.
ELEV. = 1132+09.86

BM # 24 - SET 3/4" I.P. (30" LONG)
Q SURVEY GRAND AVE.
STA. 121+84.30, 37.10' LT.
ELEV. = 1132.6589

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

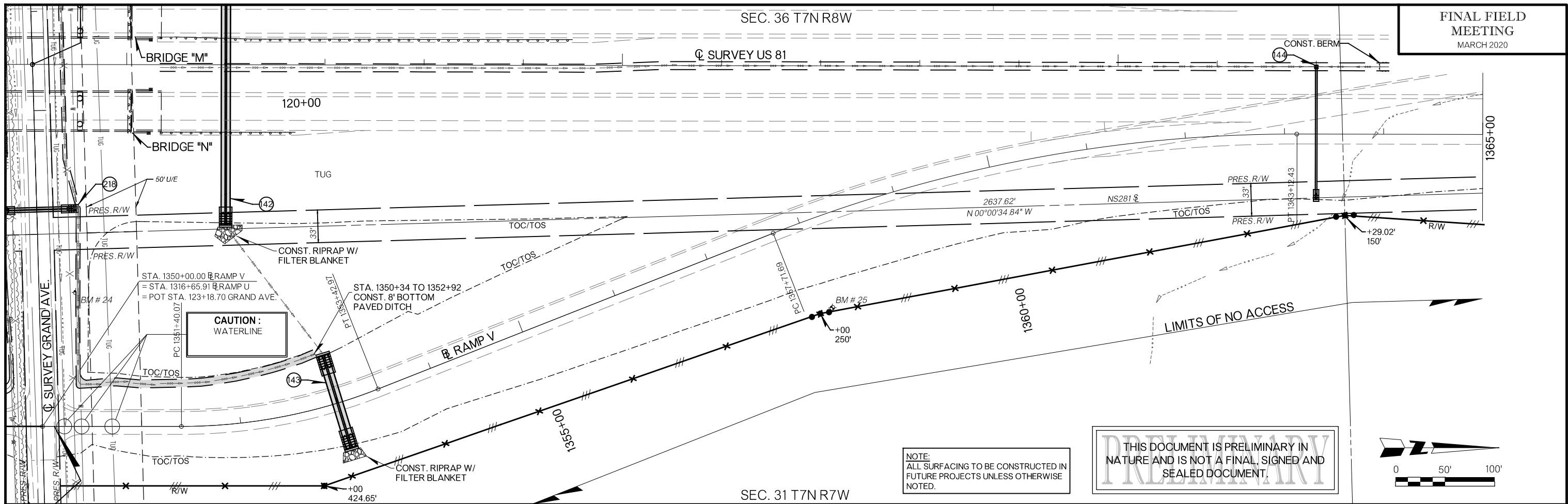


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

RAMP U INTERIM CONN

US 81 REALIGNMENT
GRADY COUNTY

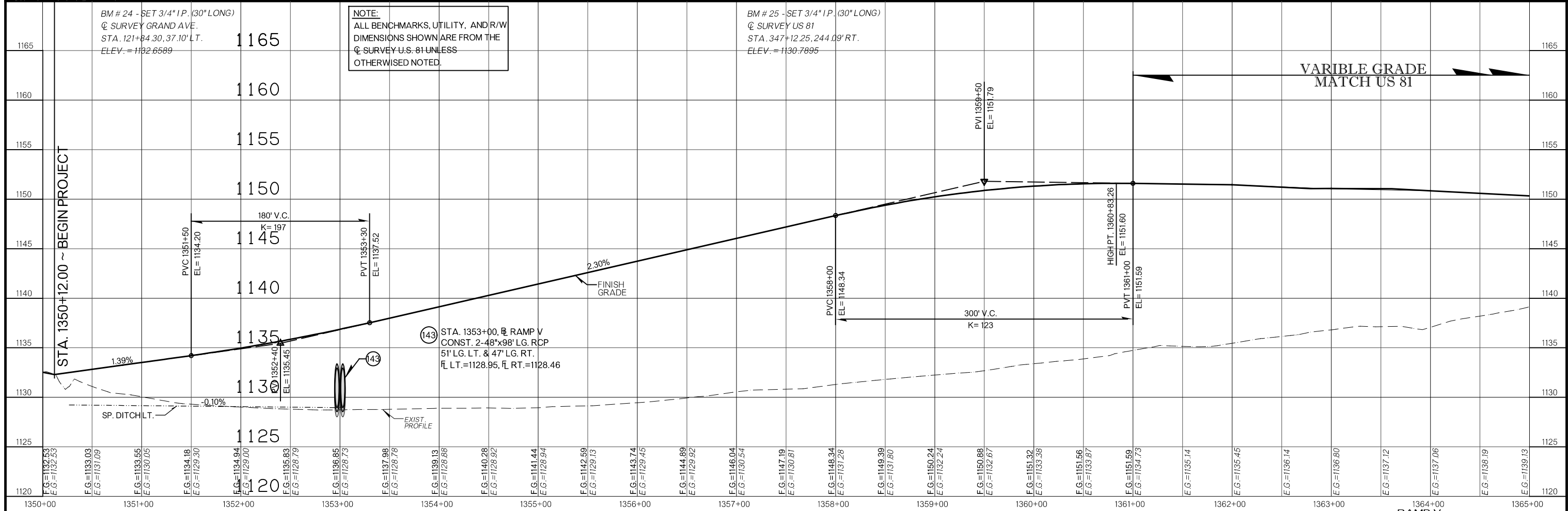
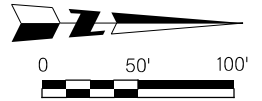
☉ SURVEY US 81



CAUTION:
WATERLINE

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



BM # 24 - SET 3/4" I.P. (30" LONG)
☉ SURVEY GRAND AVE.
STA. 121+84.30, 37.10' LT.
ELEV. = 1132.6589

NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE ☉ SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

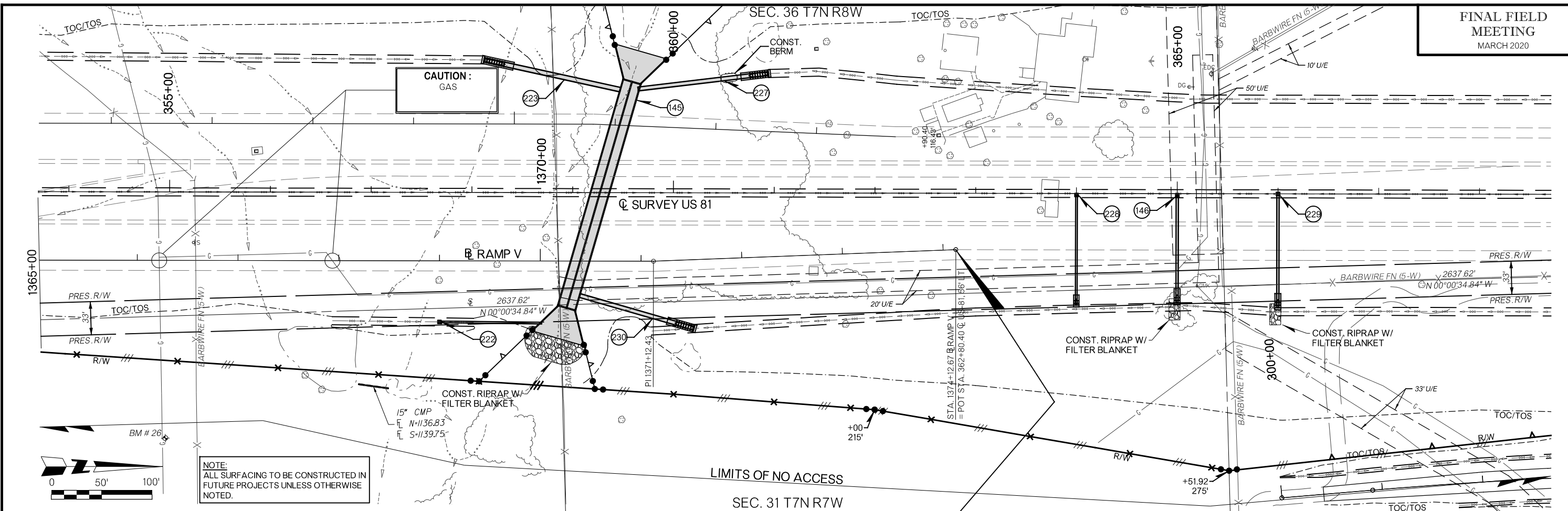
BM # 25 - SET 3/4" I.P. (30" LONG)
☉ SURVEY US 81
STA. 347+12.25, 244.09' RT.
ELEV. = 1130.7895

VARIBLE GRADE MATCH US 81

143 STA. 1353+00, RAMP V
CONST. 2-48"x98' LG. RCP
51' LG. LT. & 47' LG. RT.
FL LT.=1128.95, FL RT.=1128.46

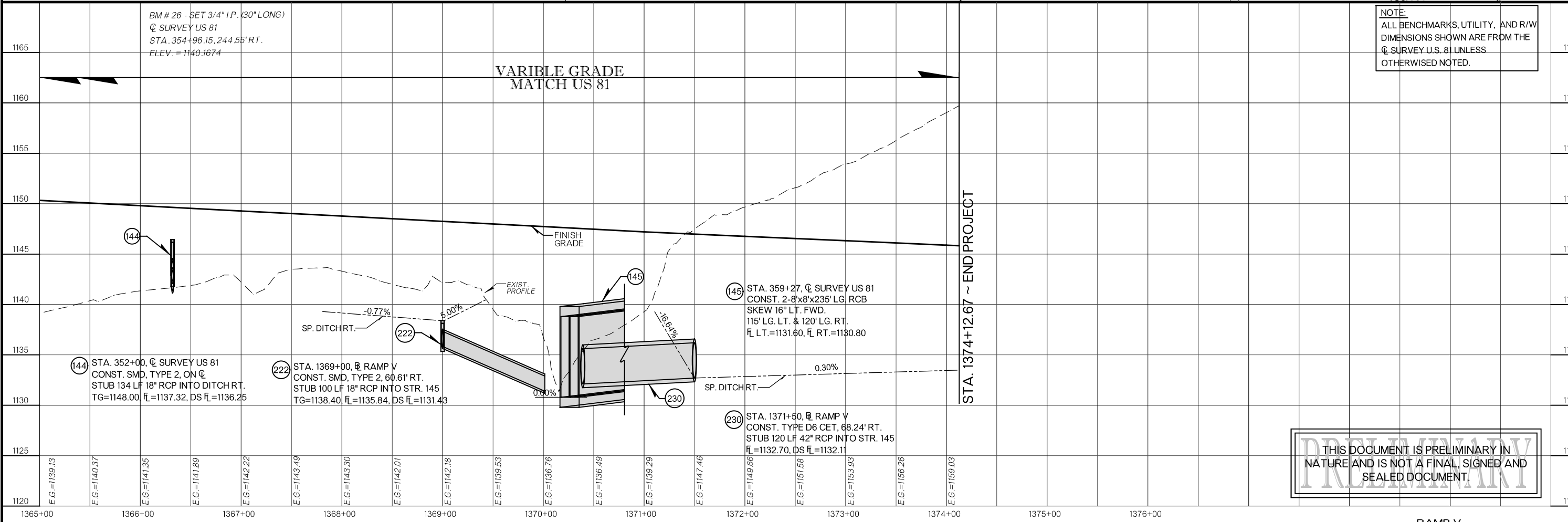
STA. 1350+12.00 ~ BEGIN PROJECT

| | |
|---------|------------------------------|
| 1350+00 | F.G.=1132.53 E.G.=1132.53 |
| 1351+00 | F.G.=1133.03 E.G.=1131.09 |
| 1352+00 | F.G.=1133.55 E.G.=1130.05 |
| 1353+00 | F.G.=1134.18 E.G.=1129.30 |
| 1354+00 | F.G.=1134.94 E.G.=1129.00 |
| 1355+00 | F.G.=1135.83 E.G.=1128.79 |
| 1356+00 | F.G.=1136.65 E.G.=1128.73 |
| 1357+00 | F.G.=1137.98 E.G.=1128.78 |
| 1358+00 | F.G.=1139.13 E.G.=1128.88 |
| 1359+00 | F.G.=1140.28 E.G.=1128.92 |
| 1360+00 | F.G.=1141.44 E.G.=1128.94 |
| 1361+00 | F.G.=1142.59 E.G.=1129.13 |
| 1362+00 | F.G.=1143.74 E.G.=1129.45 |
| 1363+00 | F.G.=1144.89 E.G.=1129.92 |
| 1364+00 | F.G.=1146.04 E.G.=1130.54 |
| 1365+00 | F.G.=1147.19 E.G.=1130.81 |
| 1366+00 | F.G.=1148.34 E.G.=1131.28 |
| 1367+00 | F.G.=1149.39 E.G.=1131.80 |
| 1368+00 | F.G.=1150.24 E.G.=1132.24 |
| 1369+00 | F.G.=1150.88 E.G.=1132.67 |
| 1370+00 | F.G.=1151.32 E.G.=1133.38 |
| 1371+00 | F.G.=1151.56 E.G.=1133.87 |
| 1372+00 | F.G.=1151.59 E.G.=1134.73 |
| 1373+00 | F.G.=1151.14 |
| 1374+00 | F.G.=1150.45 |
| 1375+00 | F.G.=1150.14 |
| 1376+00 | F.G.=1150.80 |
| 1377+00 | F.G.=1151.12 |
| 1378+00 | F.G.=1151.06 |
| 1379+00 | F.G.=1151.19 |
| 1380+00 | F.G.=1151.13 |



NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.



VARIBLE GRADE MATCH US 81

STA. 1374+12.67 ~ END PROJECT

144 STA. 352+00, Q SURVEY US 81
CONST. SMD, TYPE 2, ON Q
STUB 134 LF 18" RCP INTO DITCH RT.
TG=1148.00, FL=1137.32, DS FL=1136.25

222 STA. 1369+00, RAMP V
CONST. SMD, TYPE 2, 60.61' RT.
STUB 100 LF 18" RCP INTO STR. 145
TG=1138.40, FL=1135.84, DS FL=1131.43

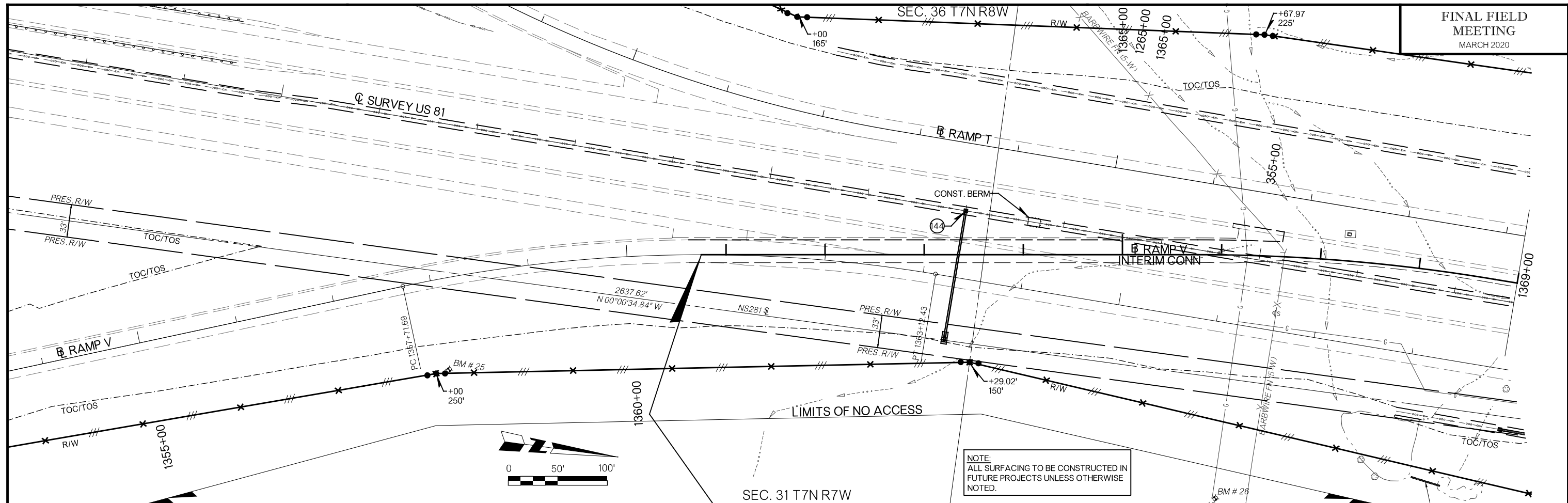
145 STA. 359+27, Q SURVEY US 81
CONST. 2-8"x8"x235' LG RCB
SKEW 16° LT. FWD.
115' LG. LT. & 120' LG. RT.
FL LT.=1131.60, FL RT.=1130.80

230 STA. 1371+50, RAMP V
CONST. TYPE D6 CET, 68.24' RT.
STUB 120 LF 42" RCP INTO STR. 145
FL=1132.70, DS FL=1132.11

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

RAMP V

US 81 REALIGNMENT
GRADY COUNTY

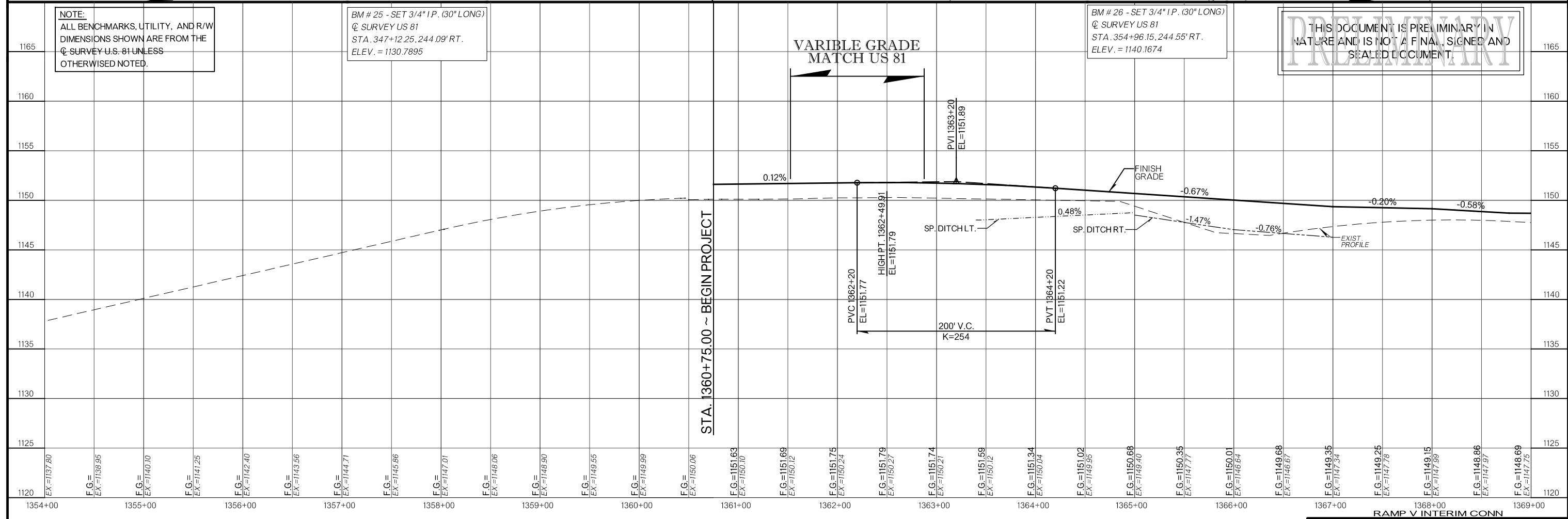


NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE \odot SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

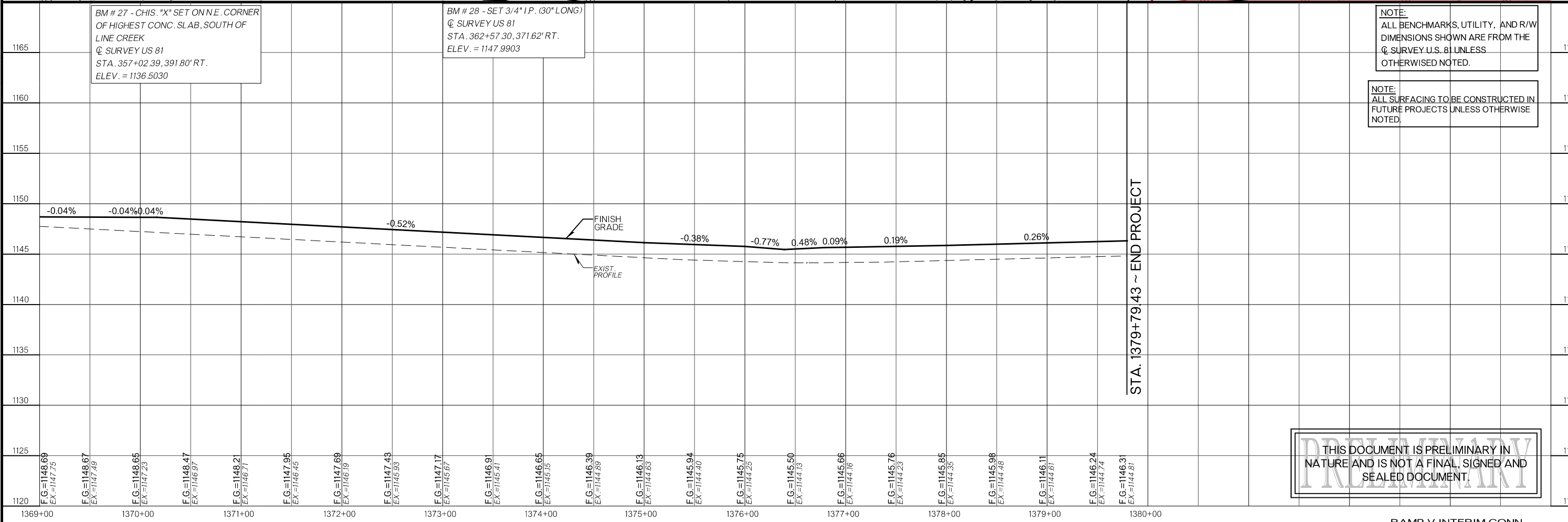
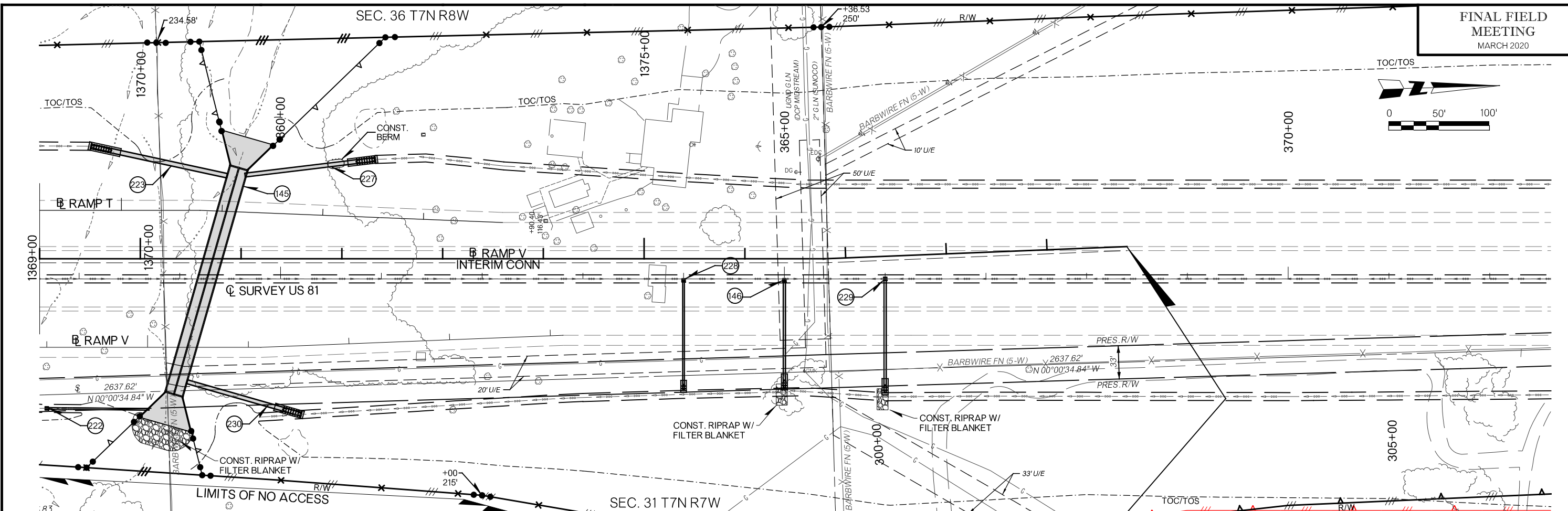
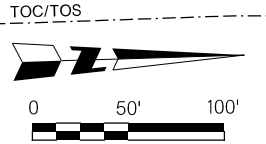
BM # 25 - SET 3/4" I.P. (30" LONG)
 \odot SURVEY US 81
STA. 347+12.25, 244.09' RT.
ELEV. = 1130.7895

BM # 26 - SET 3/4" I.P. (30" LONG)
 \odot SURVEY US 81
STA. 354+96.15, 244.55' RT.
ELEV. = 1140.1674

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY



BM # 27 - CHIS. "X" SET ON N.E. CORNER OF HIGHEST CONC. SLAB, SOUTH OF LINE CREEK
 Q SURVEY US 81
 STA. 357+02.39, 391.80' RT.
 ELEV. = 1136.5030

BM # 28 - SET 3/4" I.P. (30" LONG)
 Q SURVEY US 81
 STA. 362+57.30, 371.62' RT.
 ELEV. = 1147.9903

NOTE:
 ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

NOTE:
 ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

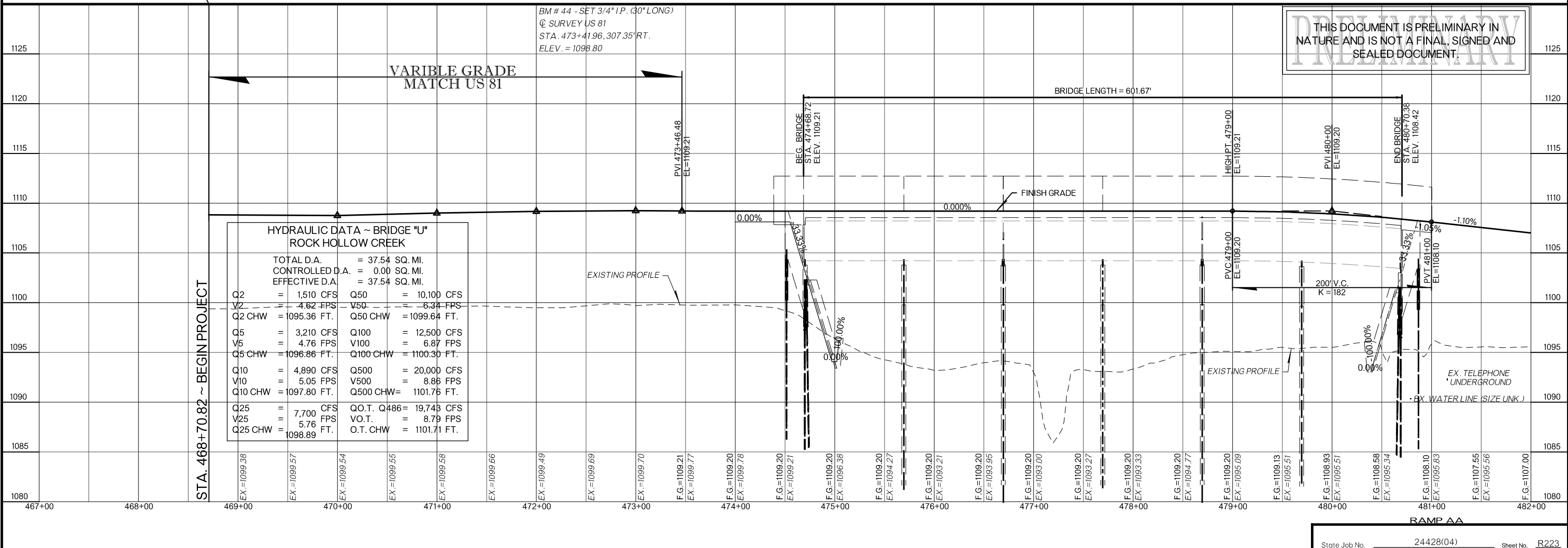
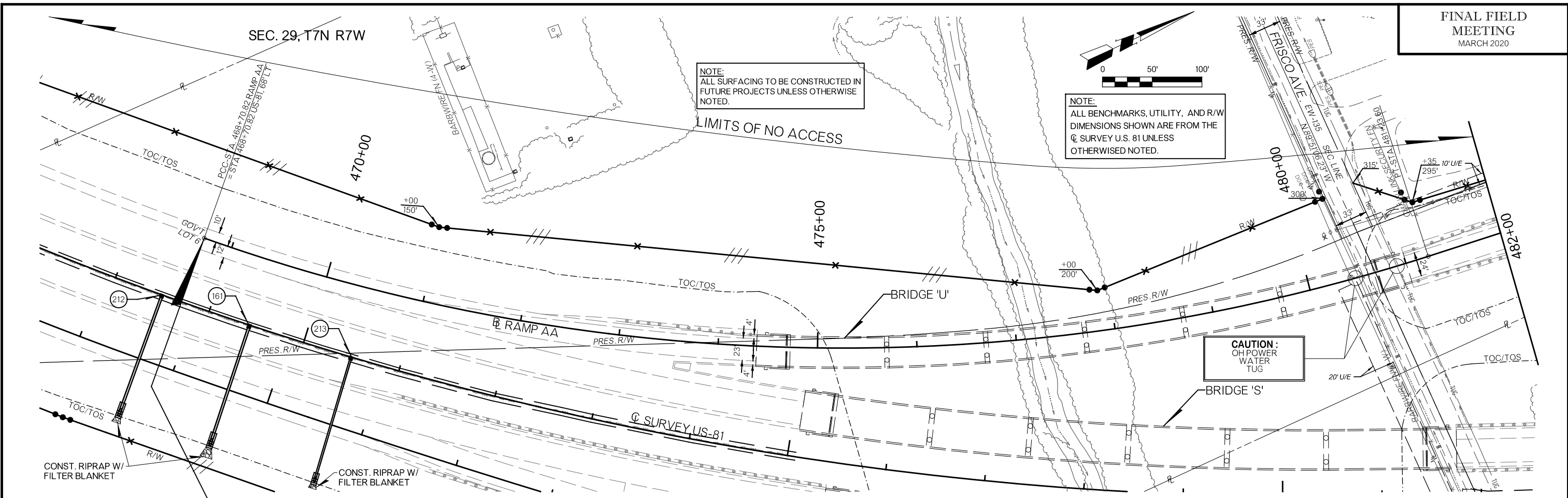
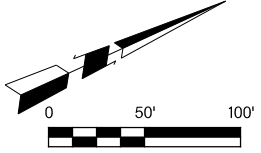
STA. 1379+79.43 ~ END PROJECT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SEC. 29, T7N R7W

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE C SURVEY U.S. 81 UNLESS OTHERWISE NOTED.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

HYDRAULIC DATA ~ BRIDGE "U" ROCK HOLLOW CREEK

| | | | |
|-----------------|-----------------|-------------|---------------|
| TOTAL D.A. | = 37.54 SQ. MI. | | |
| CONTROLLED D.A. | = 0.00 SQ. MI. | | |
| EFFECTIVE D.A. | = 37.54 SQ. MI. | | |
| Q2 | = 1,510 CFS | Q50 | = 10,100 CFS |
| V2 | = 4.62 FPS | V50 | = 6.34 FPS |
| Q2 CHW | = 1095.36 FT. | Q50 CHW | = 1099.64 FT. |
| Q5 | = 3,210 CFS | Q100 | = 12,500 CFS |
| V5 | = 4.76 FPS | V100 | = 6.87 FPS |
| Q5 CHW | = 1096.86 FT. | Q100 CHW | = 1100.30 FT. |
| Q10 | = 4,890 CFS | Q500 | = 20,000 CFS |
| V10 | = 5.05 FPS | V500 | = 8.86 FPS |
| Q10 CHW | = 1097.80 FT. | Q500 CHW | = 1101.76 FT. |
| Q25 | = 7,700 CFS | Q.O.T. Q486 | = 19,743 CFS |
| V25 | = 5.76 FPS | VO.T. | = 8.79 FPS |
| Q25 CHW | = 1098.89 FT. | O.T. CHW | = 1101.71 FT. |

STA. 468+70.82 ~ BEGIN PROJECT

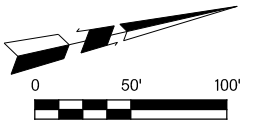
BM # 44 - SET 3/4" I.P. (30" LONG)
C SURVEY US 81
STA. 473+41.96, 307.35' RT.
ELEV. = 1098.80

VARIBLE GRADE MATCH US 81

BRIDGE LENGTH = 601.67'

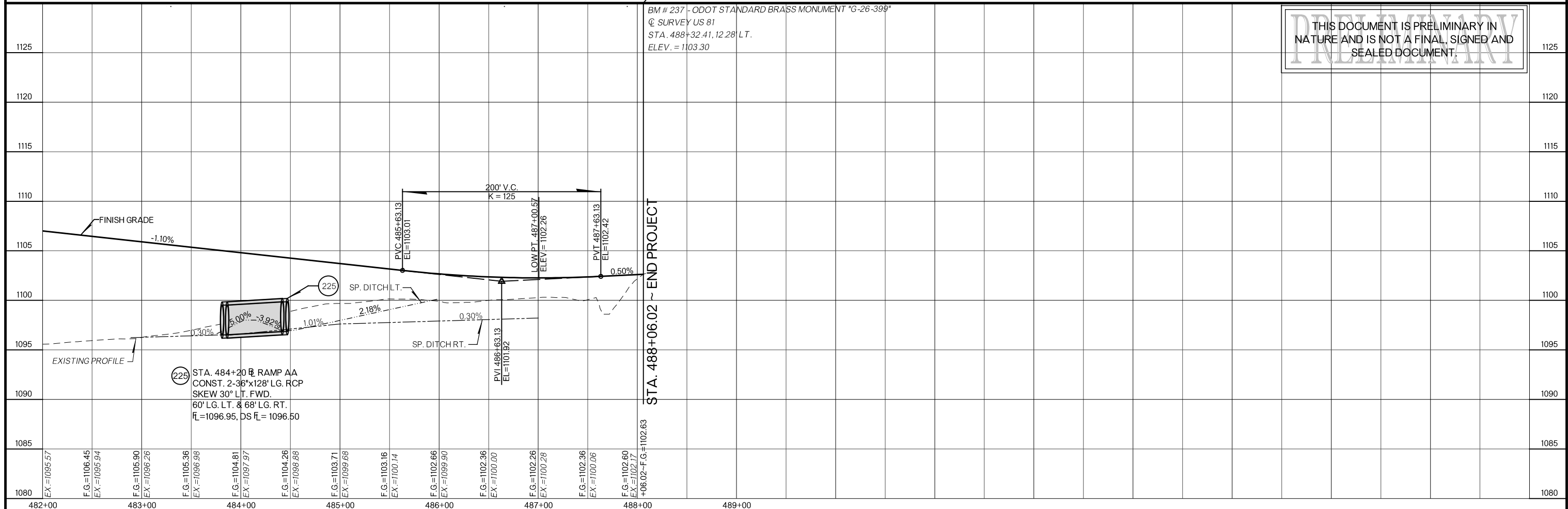
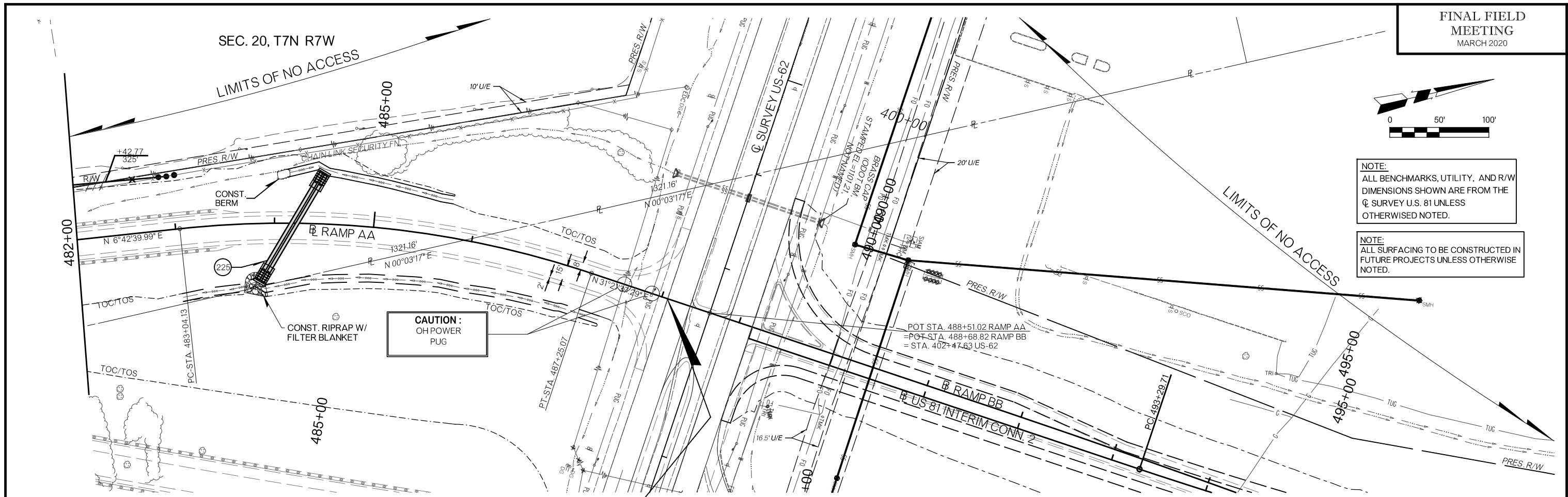
RAMP AA

SEC. 20, T7N R7W
LIMITS OF NO ACCESS



NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE \odot SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

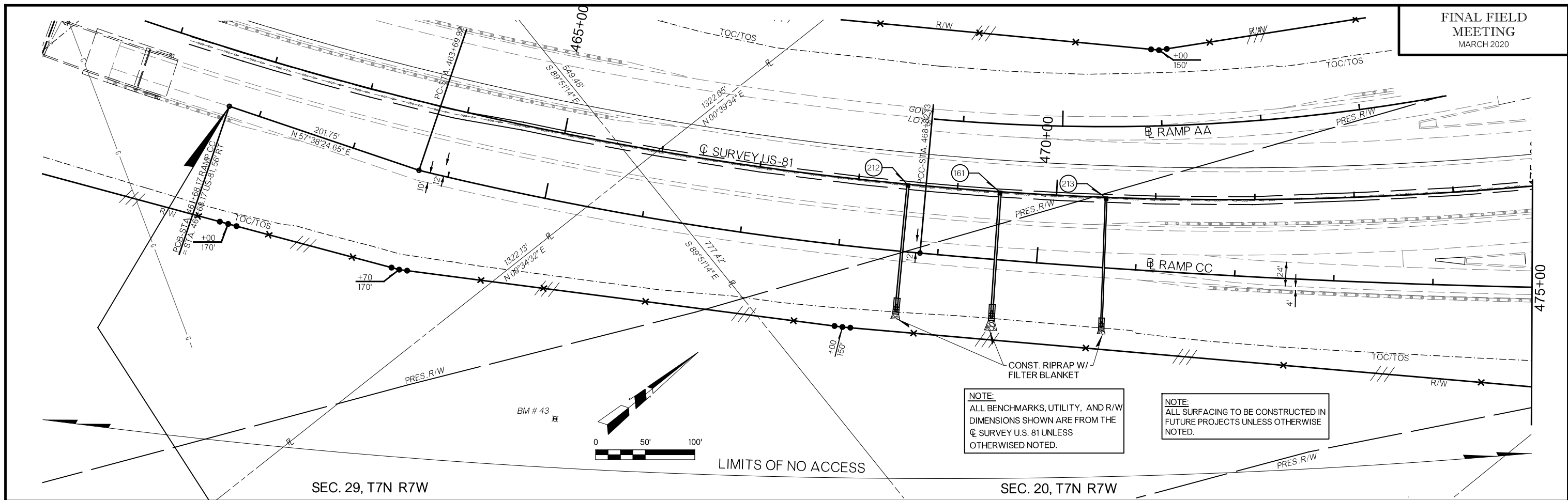
NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

RAMP AA

US-81 REALIGNMENT
GRADY COUNTY

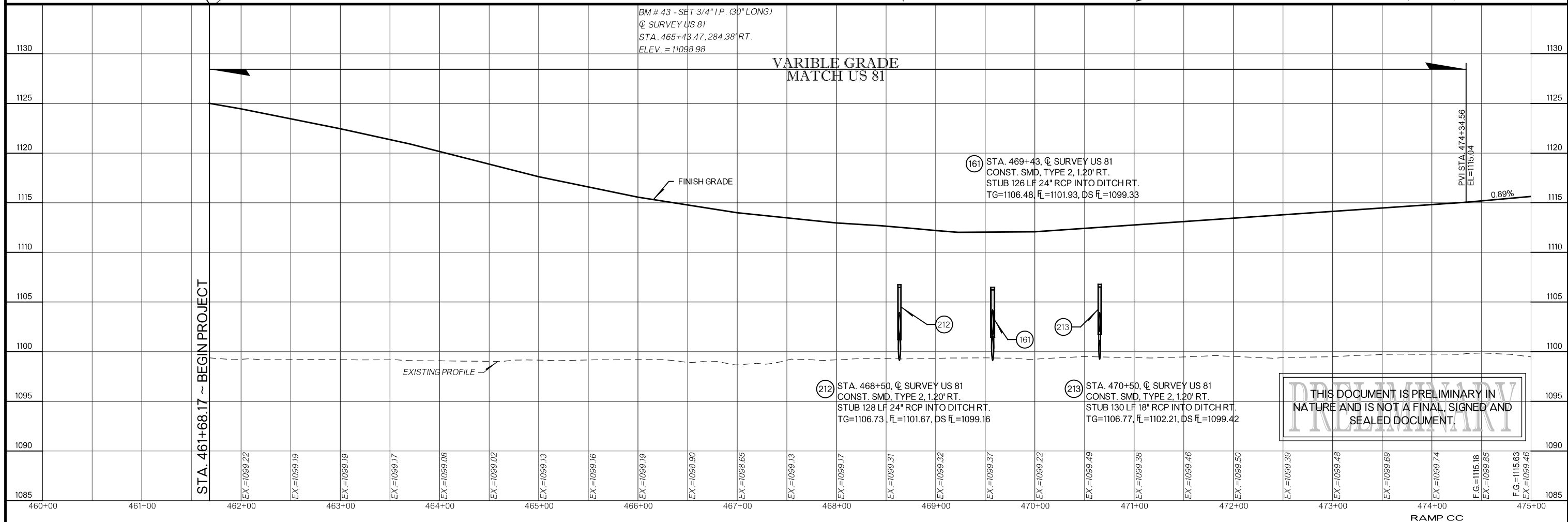


NOTE:
ALL BENCHMARKS, UTILITY, AND R/W DIMENSIONS SHOWN ARE FROM THE \odot SURVEY U.S. 81 UNLESS OTHERWISE NOTED.

NOTE:
ALL SURFACING TO BE CONSTRUCTED IN FUTURE PROJECTS UNLESS OTHERWISE NOTED.

SEC. 29, T7N R7W

SEC. 20, T7N R7W



BM # 43 - SET 3/4" I.P. (30" LONG)
 \odot SURVEY US 81
STA. 465+43.47, 284.38' RT.
ELEV. = 11098.98

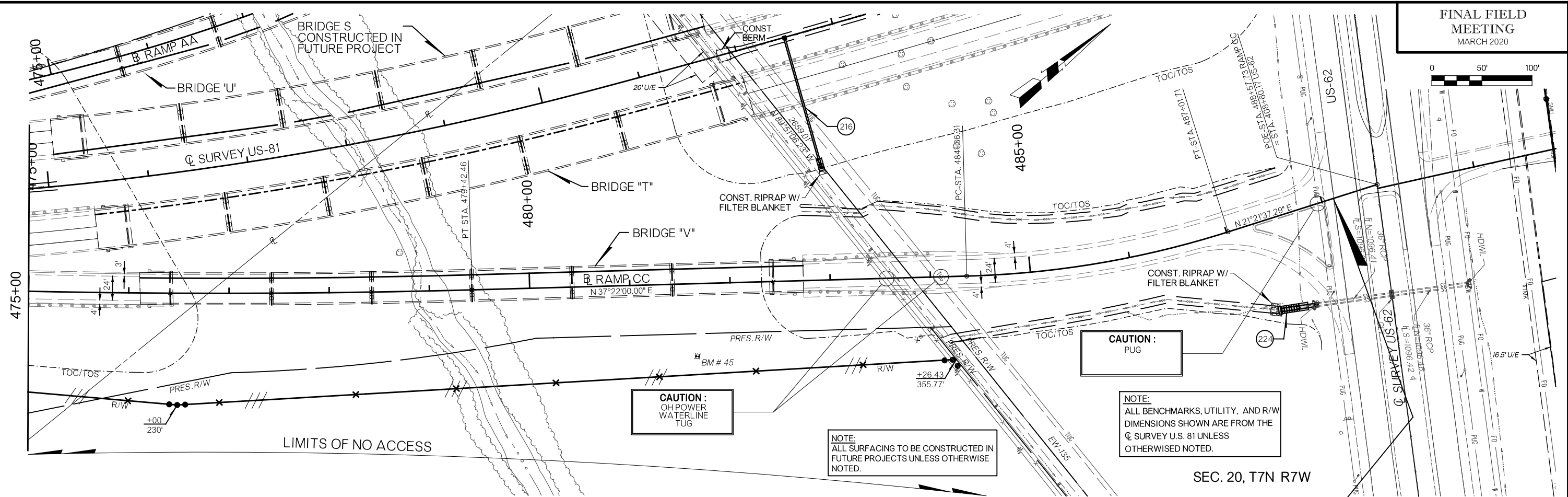
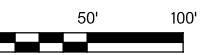
VARIBLE GRADE
MATCH US 81

STA. 461+68.17 ~ BEGIN PROJECT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

RAMP CC

US-81 REALIGNMENT
GRADY COUNTY



SEC. 20, T7N R7W

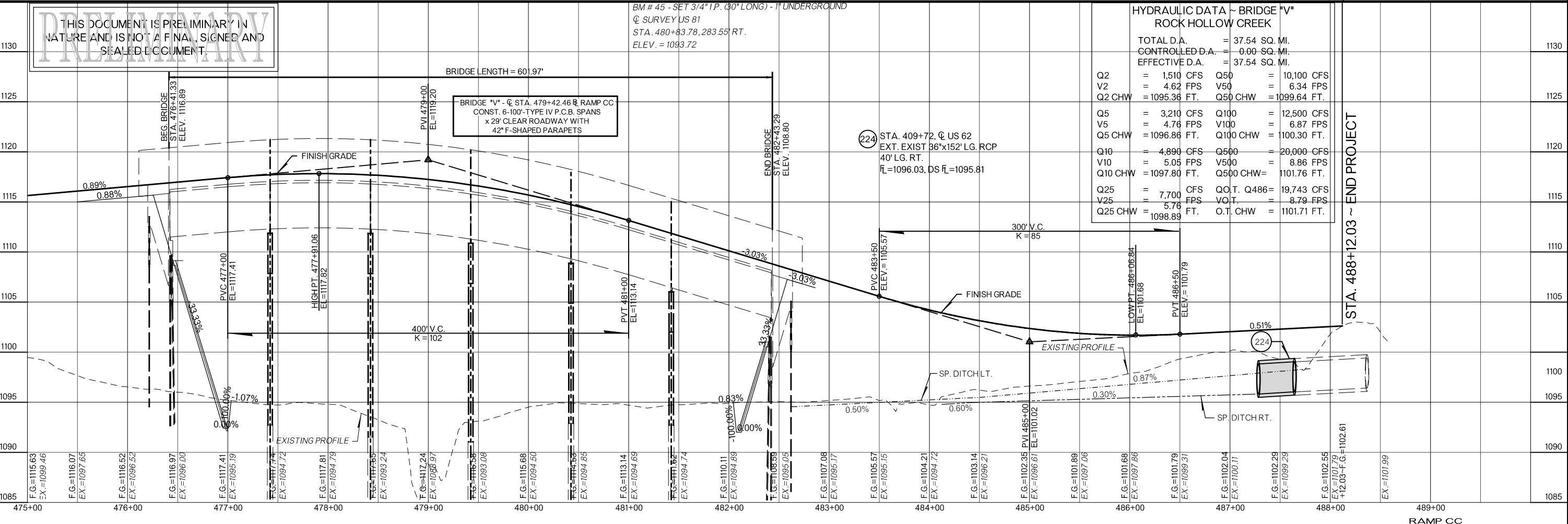
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

BM # 45 - SET 3/4" I.P. (30" LONG) - 1" UNDERGROUND
 Q SURVEY US 81
 STA. 480+83.78, 283.55' RT.
 ELEV. = 1093.72

HYDRAULIC DATA - BRIDGE "V"
 ROCK HOLLOW CREEK

TOTAL D.A. = 37.54 SQ. MI.
 CONTROLLED D.A. = 0.00 SQ. MI.
 EFFECTIVE D.A. = 37.54 SQ. MI.

| | |
|-----------------------|-------------------------|
| Q2 = 1,510 CFS | Q50 = 10,100 CFS |
| V2 = 4.62 FPS | V50 = 6.34 FPS |
| Q2 CHW = 1095.36 FT. | Q50 CHW = 1099.64 FT. |
| Q5 = 3,210 CFS | Q100 = 12,500 CFS |
| V5 = 4.76 FPS | V100 = 6.87 FPS |
| Q5 CHW = 1096.86 FT. | Q100 CHW = 1100.30 FT. |
| Q10 = 4,890 CFS | Q500 = 20,000 CFS |
| V10 = 5.05 FPS | V500 = 8.86 FPS |
| Q10 CHW = 1097.80 FT. | Q500 CHW = 1101.76 FT. |
| Q25 = 7,700 CFS | QO.T. Q486 = 19,743 CFS |
| V25 = 5.76 FPS | VO.T. = 8.79 FPS |
| Q25 CHW = 1098.89 FT. | O.T. CHW = 1101.71 FT. |

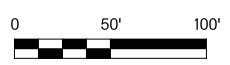


STA. 488+12.03 ~ END PROJECT

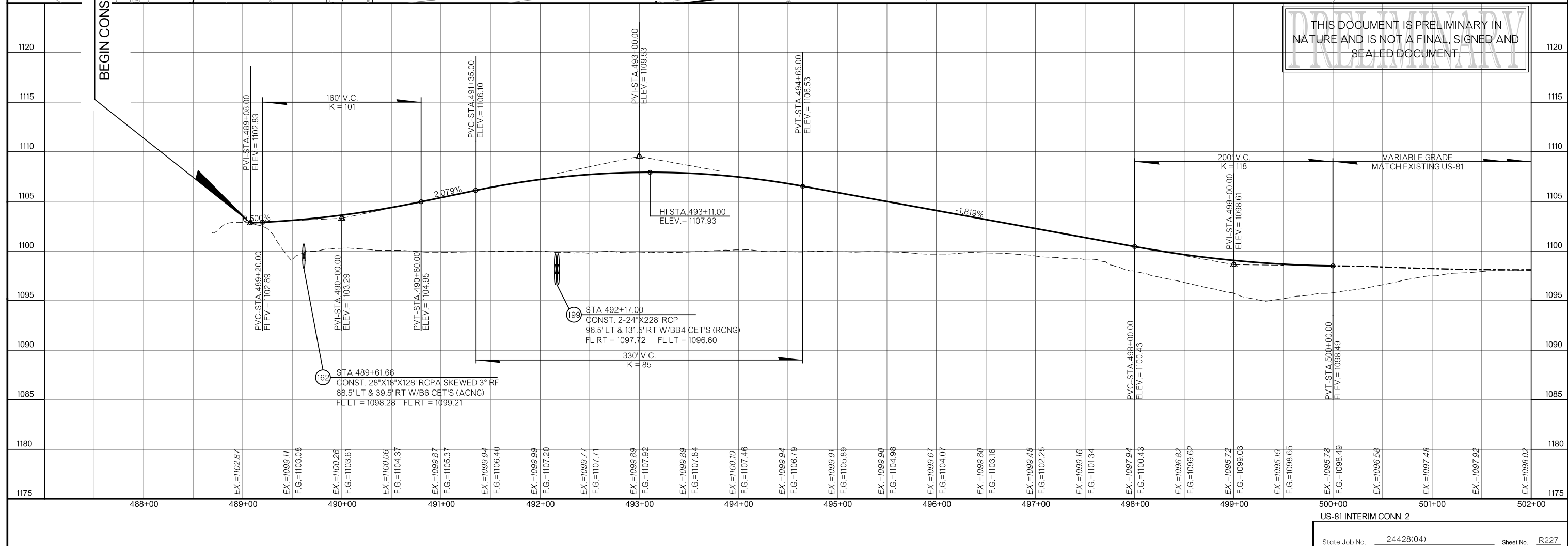
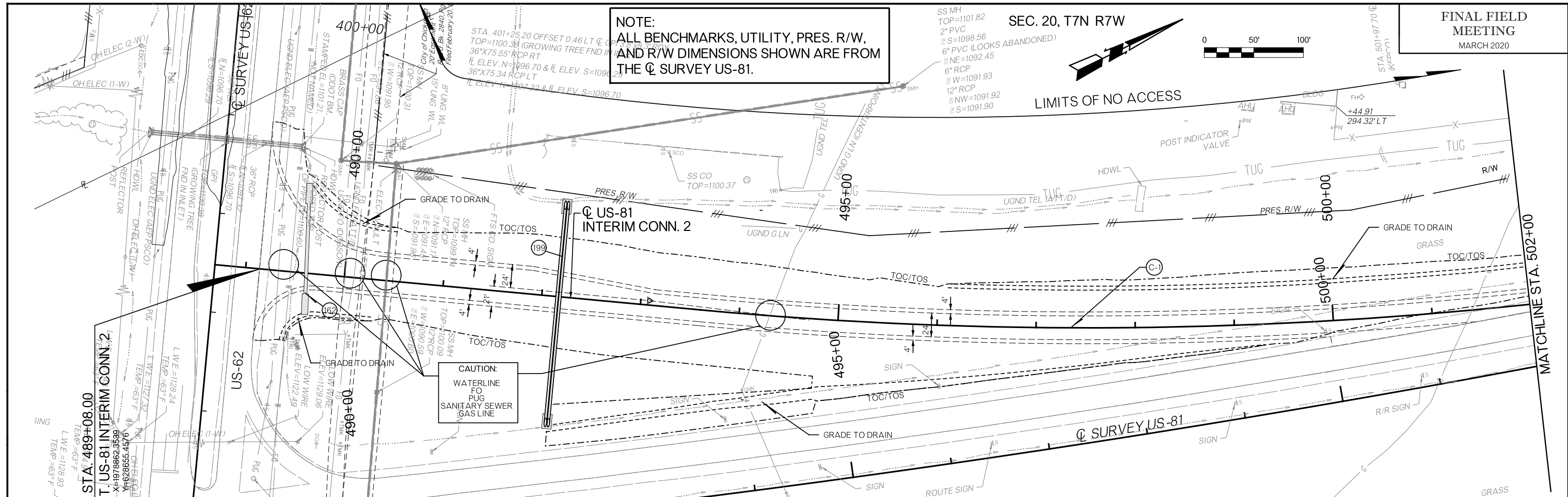
RAMP CC

NOTE: ALL BENCHMARKS, UTILITY, PRES. R/W, AND R/W DIMENSIONS SHOWN ARE FROM THE Q SURVEY US-81.

- SS MH TOP=1101.82
- 2" PVC @ S=1098.56
- 6" PVC (LOOKS ABANDONED) @ NE=1092.45
- 6" RCP @ W=1091.93
- 12" RCP @ NW=1091.92
- @ S=1091.90



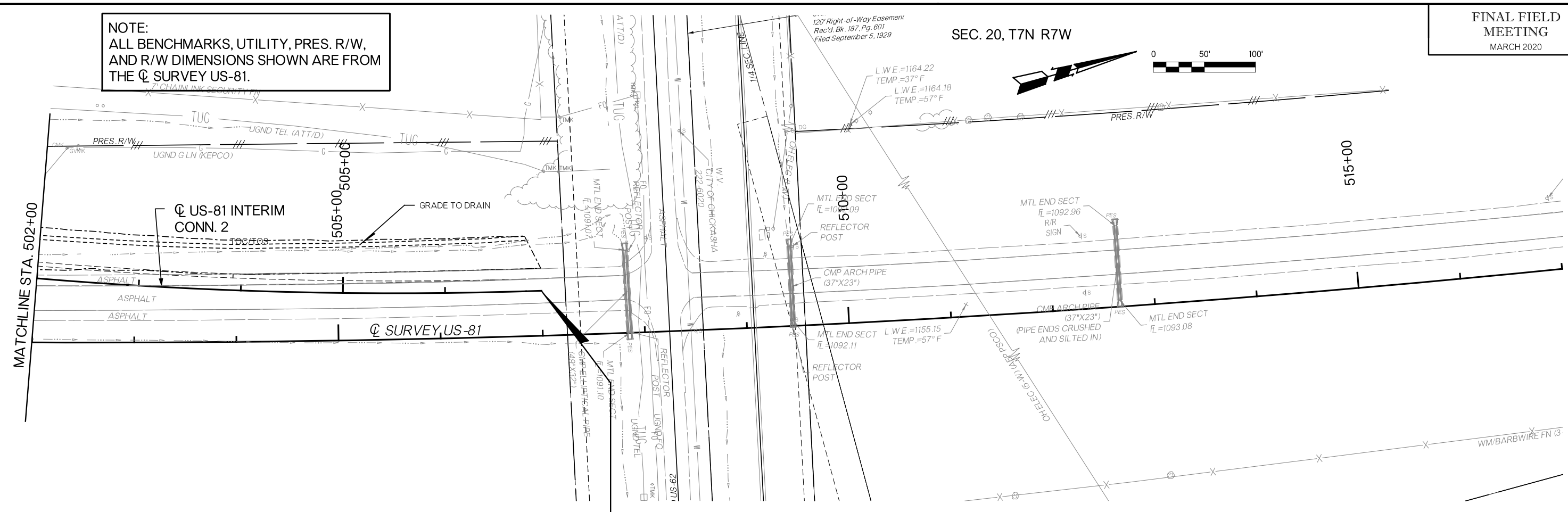
LIMITS OF NO ACCESS



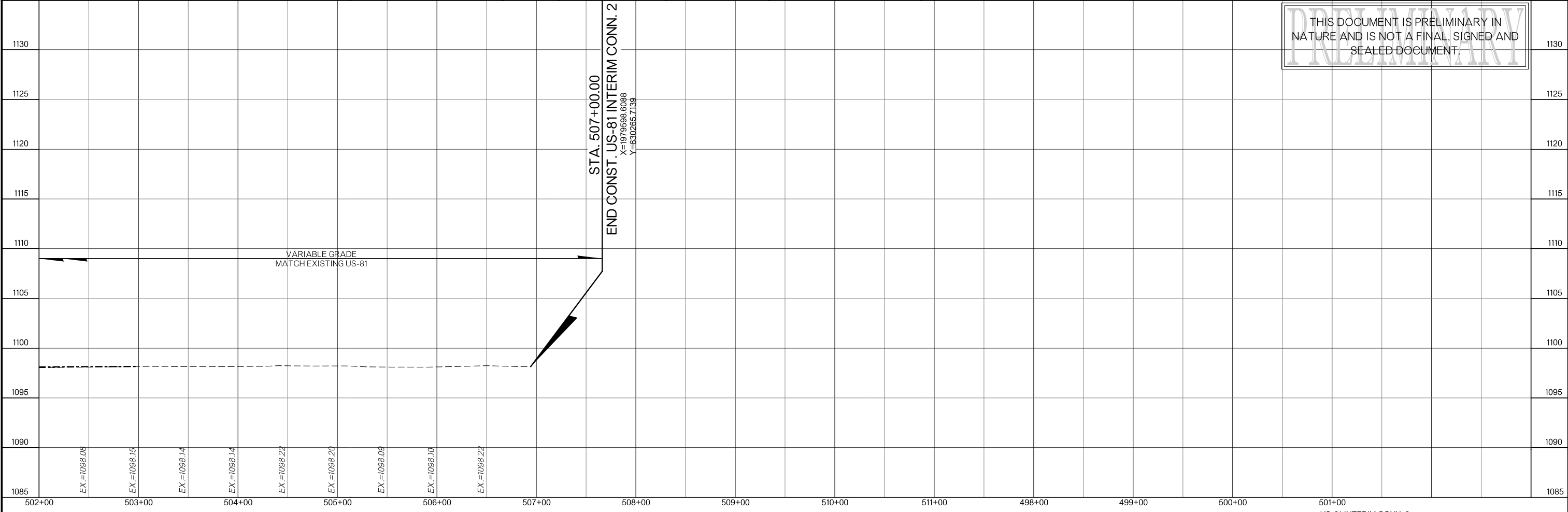
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

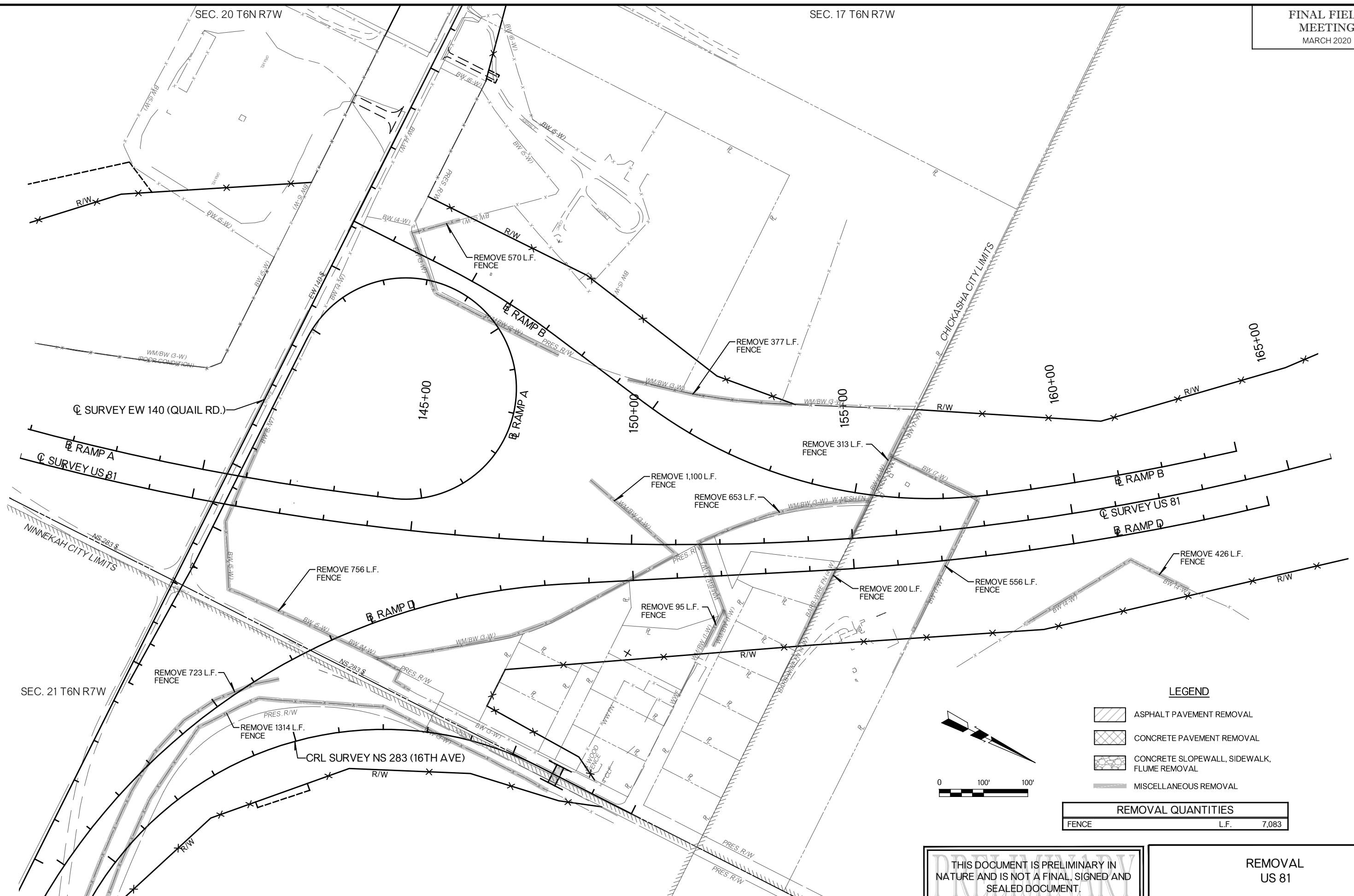
NOTE:
ALL BENCHMARKS, UTILITY, PRES. R/W,
AND R/W DIMENSIONS SHOWN ARE FROM
THE ϕ SURVEY US-81.

SEC. 20, T7N R7W







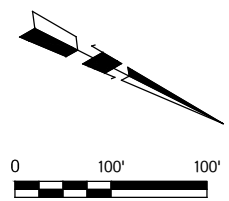
THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.





LEGEND

-  ASPHALT PAVEMENT REMOVAL
-  CONCRETE PAVEMENT REMOVAL
-  CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
-  MISCELLANEOUS REMOVAL



| REMOVAL QUANTITIES | | |
|--------------------|------|-------|
| FENCE | L.F. | 7,083 |

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

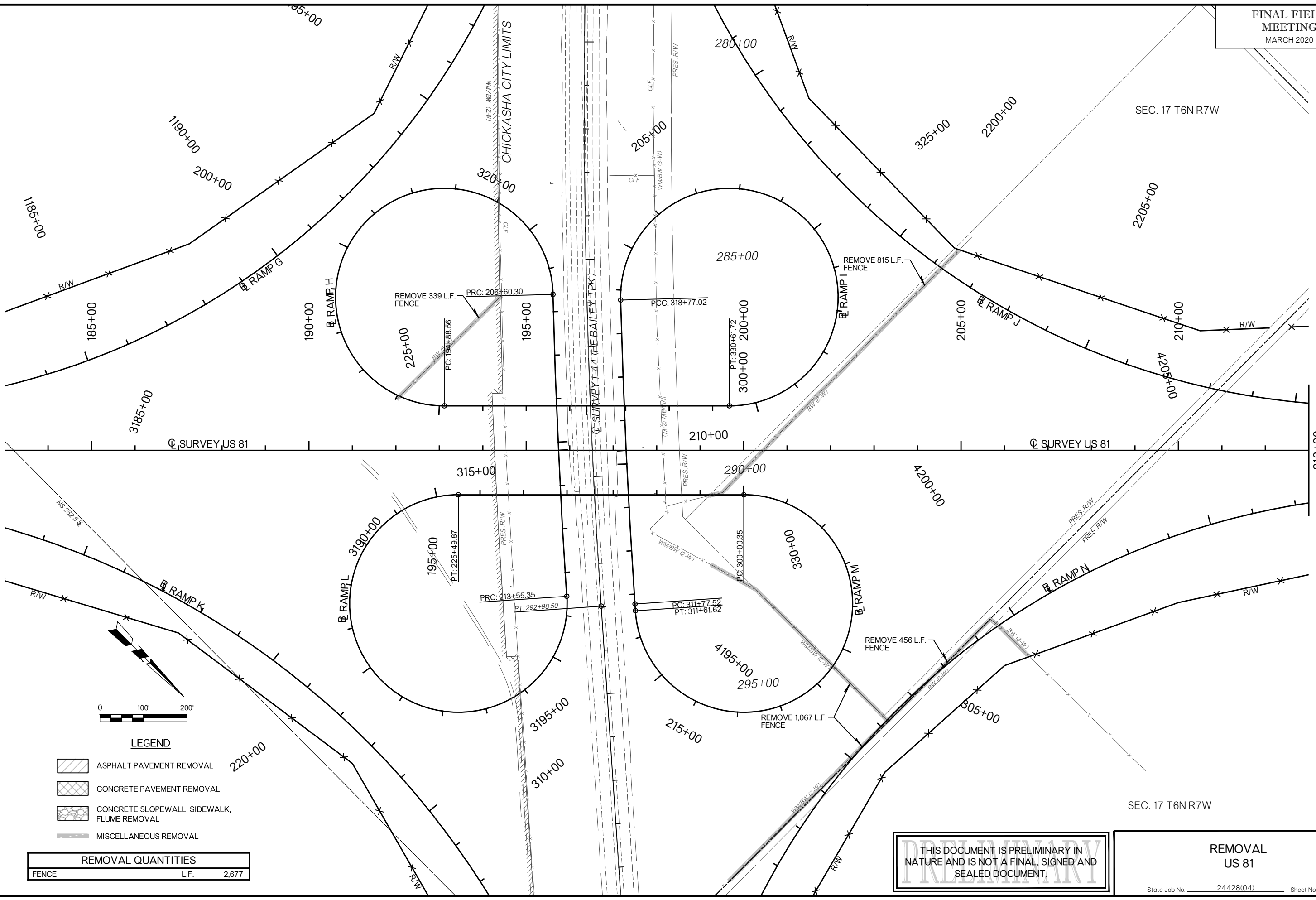
REMOVAL US 81

State Job No. 24428(04) Sheet No. R229

US 81 REALIGNMENT
GRADY COUNTY

SEC. 17 T6N R7W

SEC. 17 T6N R7W



LEGEND

- ASPHALT PAVEMENT REMOVAL
- CONCRETE PAVEMENT REMOVAL
- CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
- MISCELLANEOUS REMOVAL

| REMOVAL QUANTITIES | |
|--------------------|------------|
| FENCE | L.F. 2,677 |

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

REMOVAL US 81

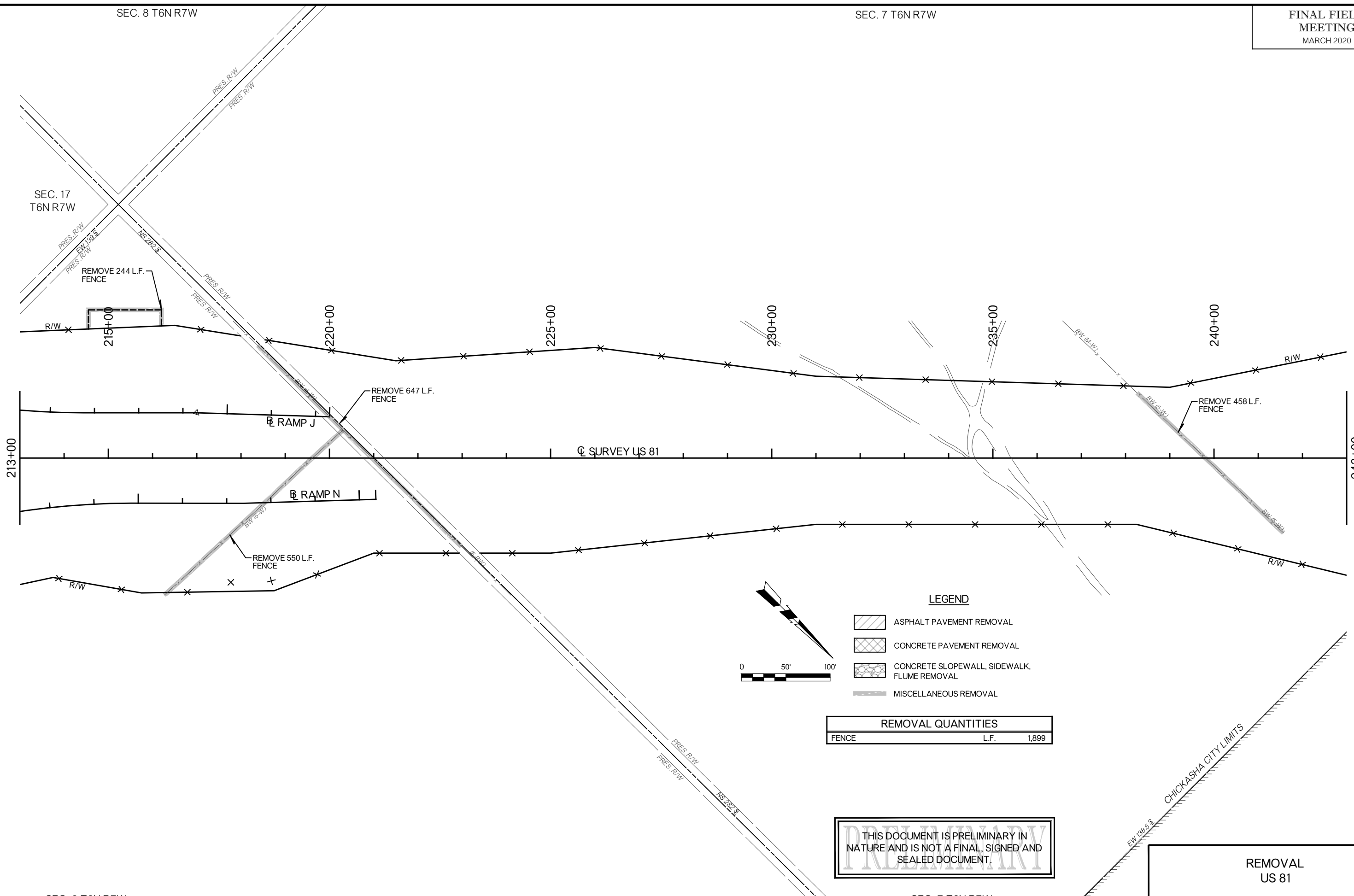
State Job No. 24428(04) Sheet No. R230

GRADY COUNTY US 81 REALIGNMENT





SEC. 8 T6N R7W

SEC. 7 T6N R7W

SEC. 17
T6N R7W



LEGEND

-  ASPHALT PAVEMENT REMOVAL
-  CONCRETE PAVEMENT REMOVAL
-  CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
-  MISCELLANEOUS REMOVAL



| REMOVAL QUANTITIES | | |
|--------------------|------|-------|
| FENCE | L.F. | 1,899 |

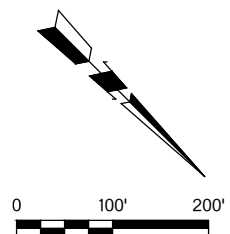
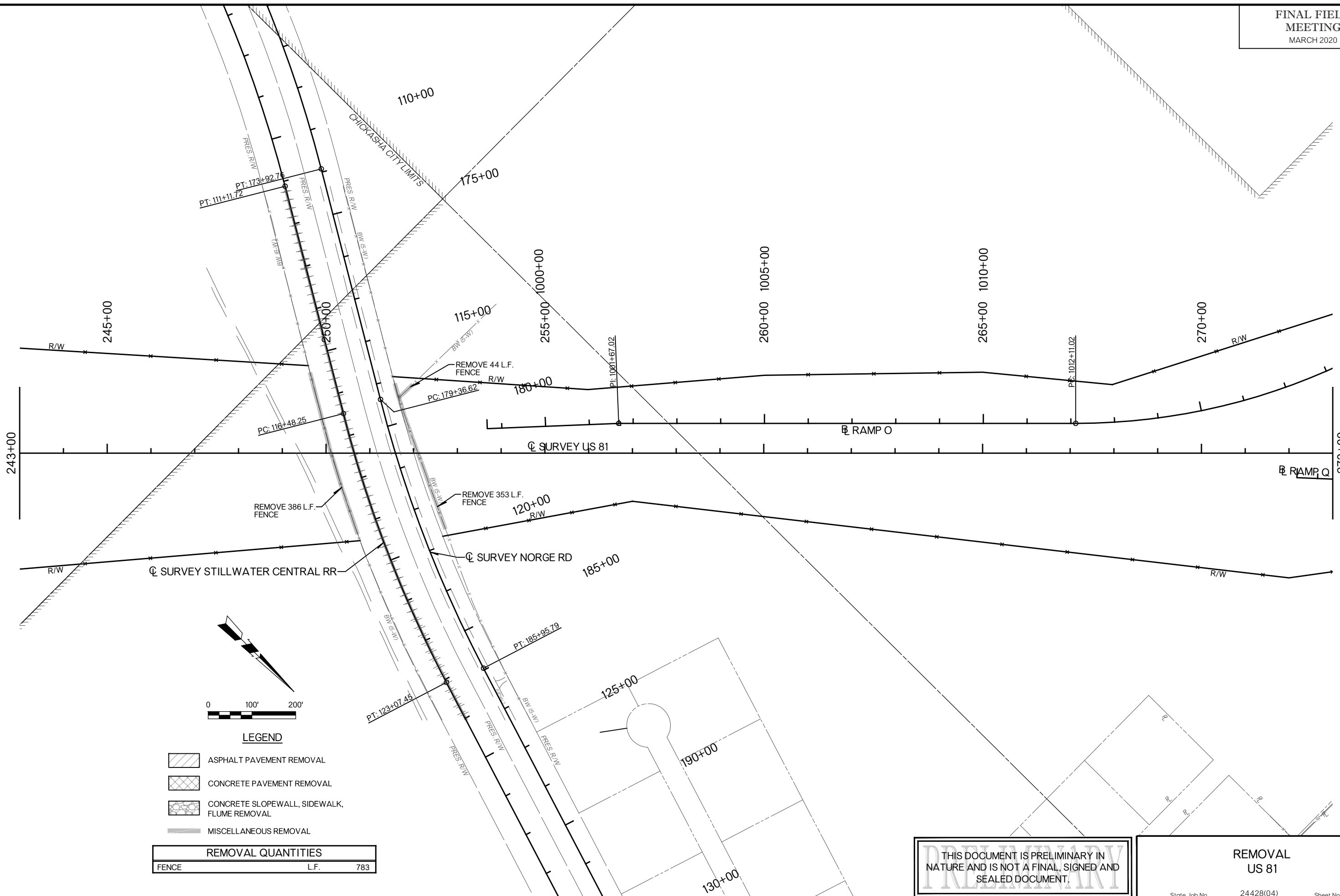
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

REMOVAL US 81





SEC. 8 T6N R7W

SEC. 7 T6N R7W

GRADY COUNTY US 81 REALIGNMENT



LEGEND

-  ASPHALT PAVEMENT REMOVAL
-  CONCRETE PAVEMENT REMOVAL
-  CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
-  MISCELLANEOUS REMOVAL

| REMOVAL QUANTITIES | | |
|--------------------|------|-----|
| FENCE | L.F. | 783 |

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

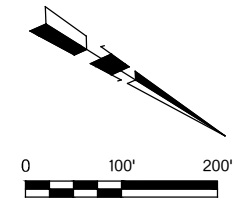
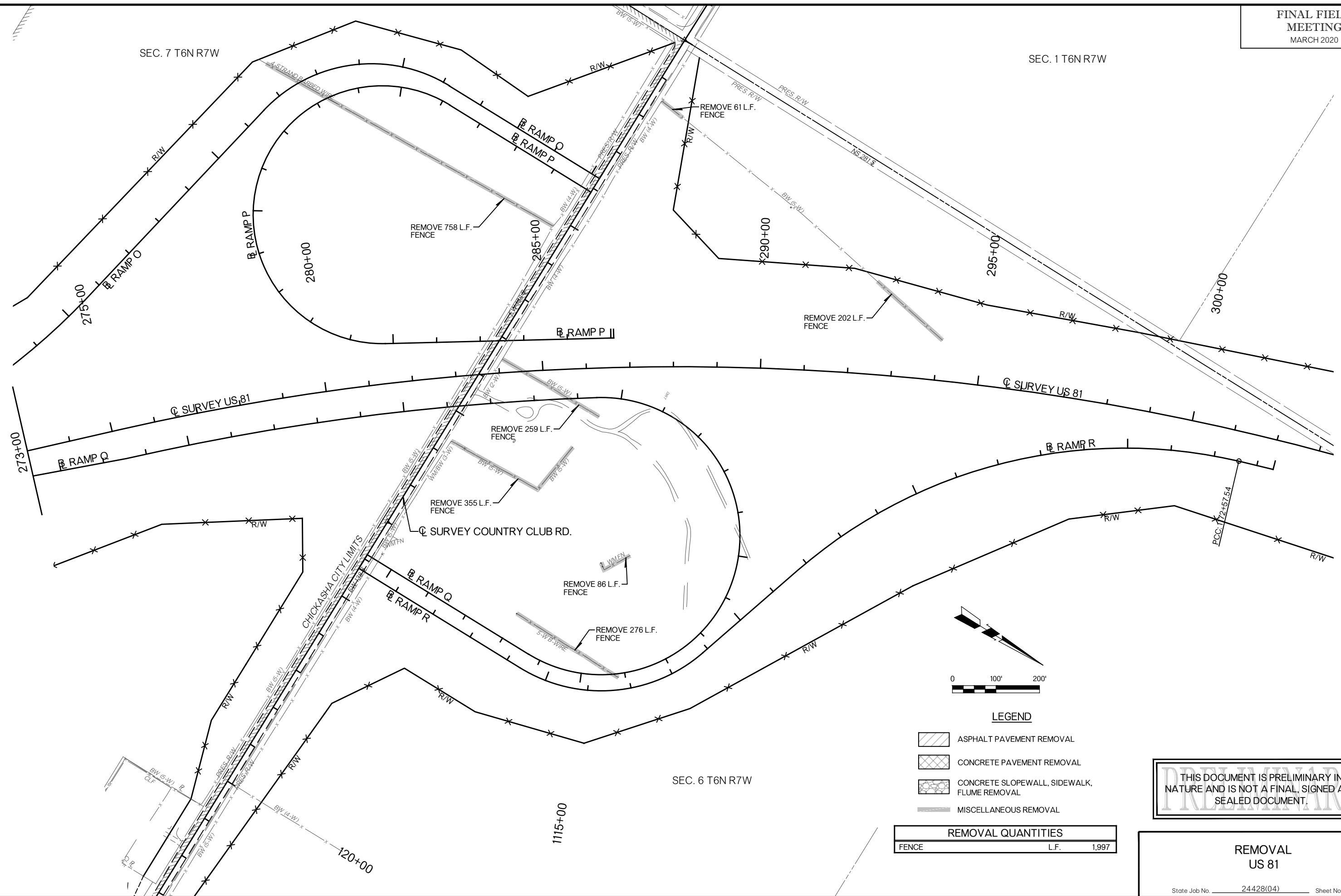
REMOVAL US 81
State Job No. 24428(04) Sheet No. R232

GRADY COUNTY US 81 REALIGNMENT





SEC. 7 T6N R7W

SEC. 1 T6N R7W

SEC. 6 T6N R7W

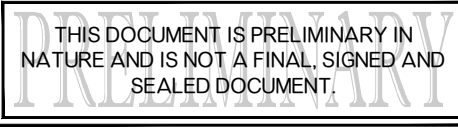


LEGEND

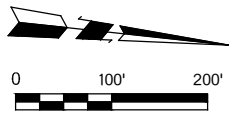
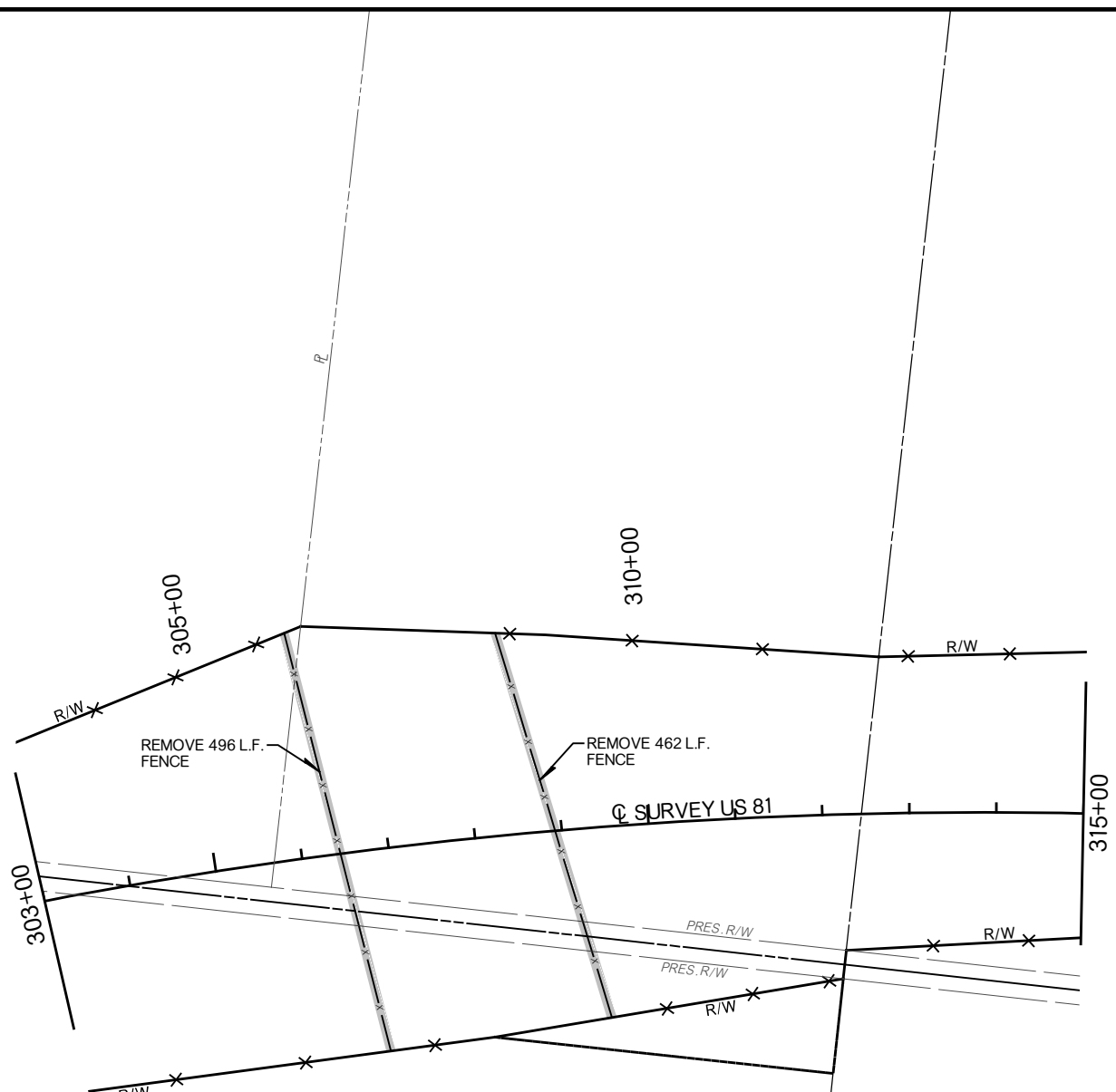
-  ASPHALT PAVEMENT REMOVAL
-  CONCRETE PAVEMENT REMOVAL
-  CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
-  MISCELLANEOUS REMOVAL

| REMOVAL QUANTITIES | | |
|--------------------|------|-------|
| FENCE | L.F. | 1,997 |
| | | |





THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



REMOVAL
US 81



LEGEND

-  ASPHALT PAVEMENT REMOVAL
-  CONCRETE PAVEMENT REMOVAL
-  CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
-  MISCELLANEOUS REMOVAL

| REMOVAL QUANTITIES | | |
|--------------------|------|-----|
| FENCE | L.F. | 958 |





PRELIMINARY
 THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

**REMOVAL
US 81**
 State Job No. 24428(04) Sheet No. R234

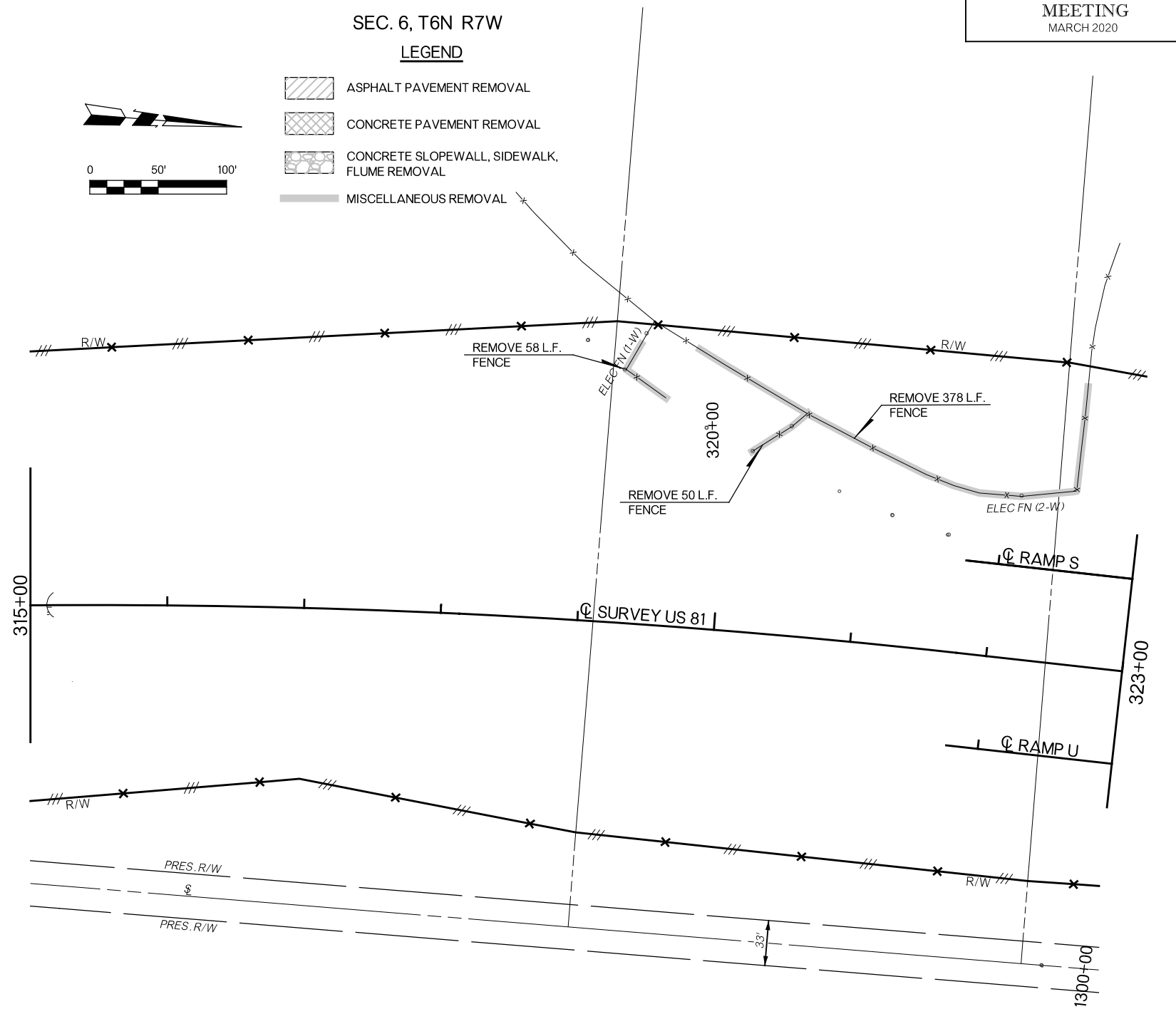
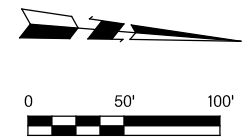
GRADY COUNTY US 81 REALIGNMENT

SEC. 6, T6N R7W

LEGEND

-  ASPHALT PAVEMENT REMOVAL
-  CONCRETE PAVEMENT REMOVAL
-  CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
-  MISCELLANEOUS REMOVAL

| REMOVAL QUANTITIES | | |
|--------------------|------|-----|
| FENCE | L.F. | 486 |



SEC. 6, T6N R7W

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

REMOVAL
US 81

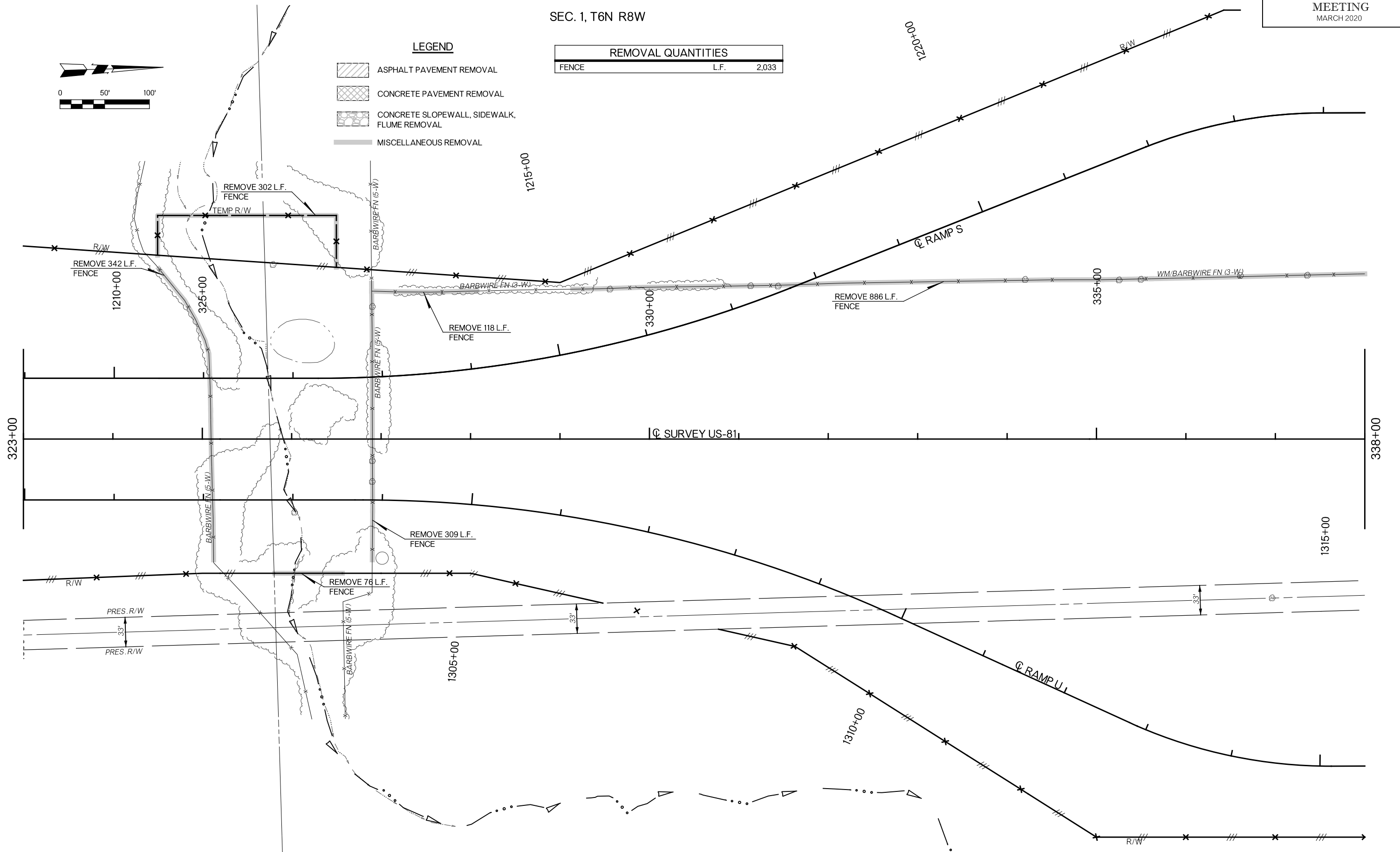
SEC. 1, T6N R8W



LEGEND

- ASPHALT PAVEMENT REMOVAL
- CONCRETE PAVEMENT REMOVAL
- CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
- MISCELLANEOUS REMOVAL

| REMOVAL QUANTITIES | |
|--------------------|------------|
| FENCE | L.F. 2,033 |







SEC. 6, T6N R7W

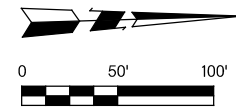
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

REMOVAL
US 81

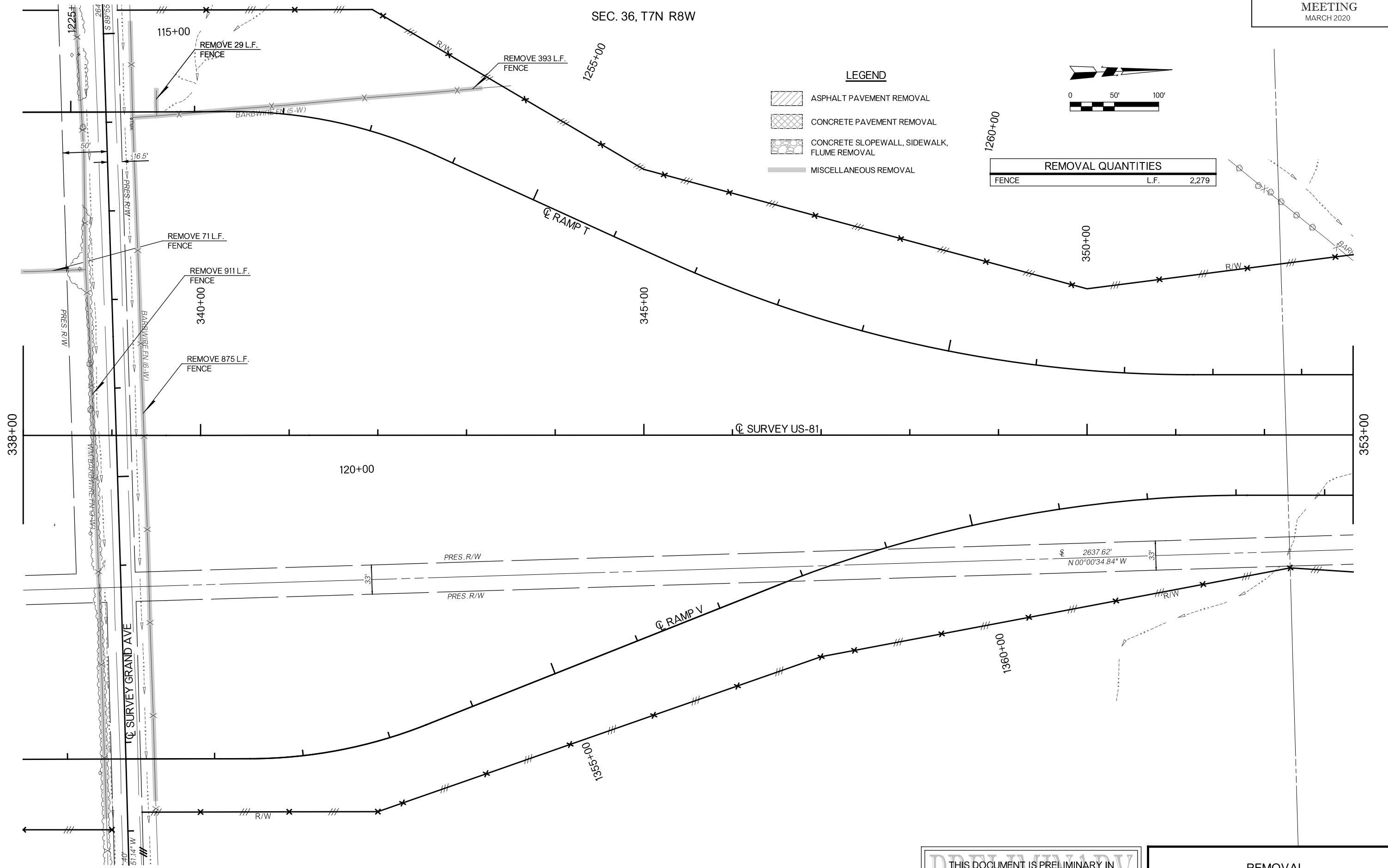
SEC. 36, T7N R8W

LEGEND

-  ASPHALT PAVEMENT REMOVAL
-  CONCRETE PAVEMENT REMOVAL
-  CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
-  MISCELLANEOUS REMOVAL



| REMOVAL QUANTITIES | |
|--------------------|------------|
| FENCE | L.F. 2,279 |



SEC. 31, T7N R7W





THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

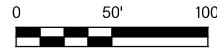
REMOVAL US 81

GRADY COUNTY US 81 REALIGNMENT

| REMOVAL QUANTITIES | | |
|--------------------|------|-------|
| FENCE | L.F. | 2,137 |

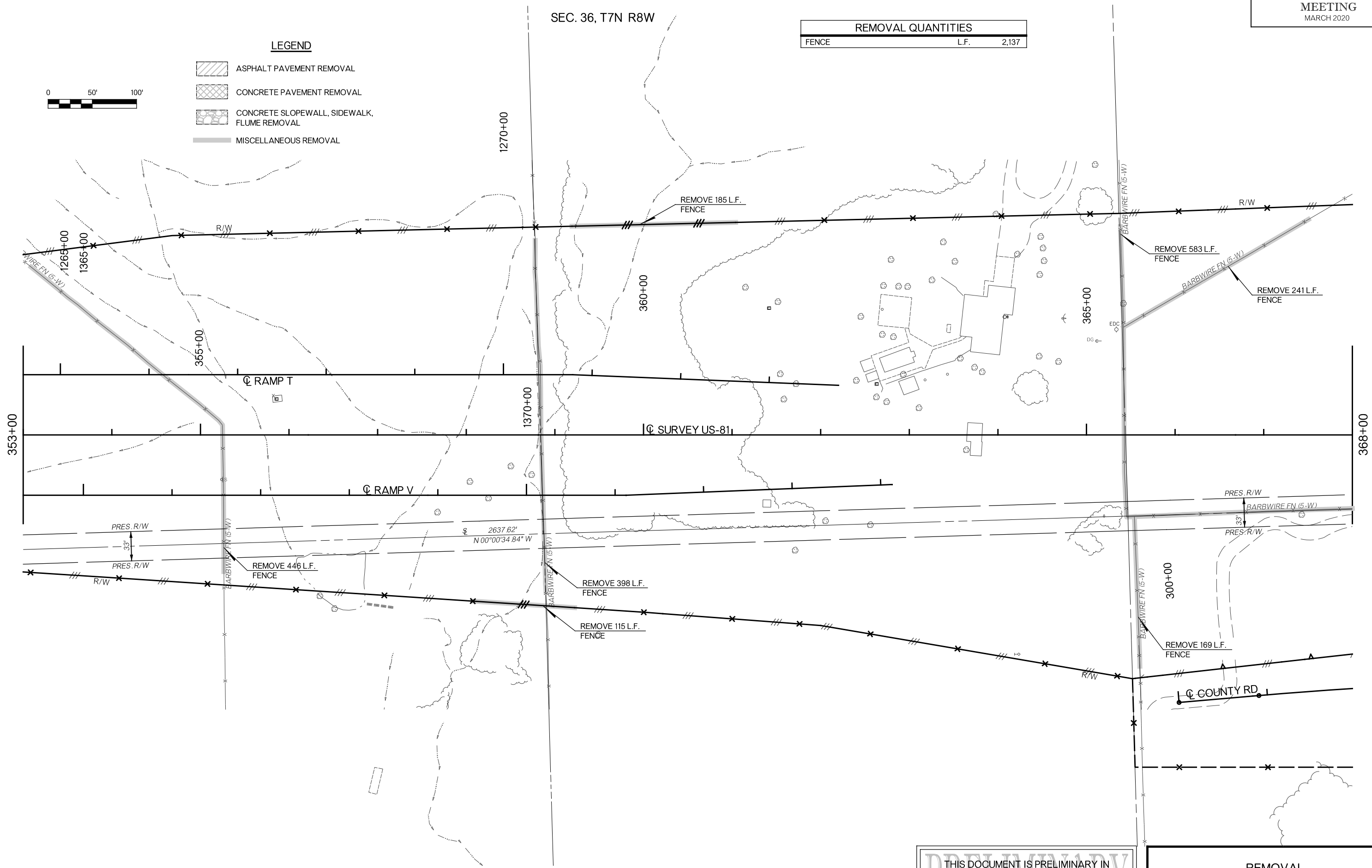
LEGEND

-  ASPHALT PAVEMENT REMOVAL
-  CONCRETE PAVEMENT REMOVAL
-  CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
-  MISCELLANEOUS REMOVAL



SEC. 36, T7N R8W

SEC. 31, T7N R7W

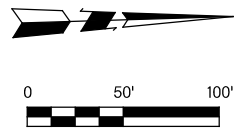


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.





REMOVAL US 81

State Job No. 24428(04) Sheet No. R238

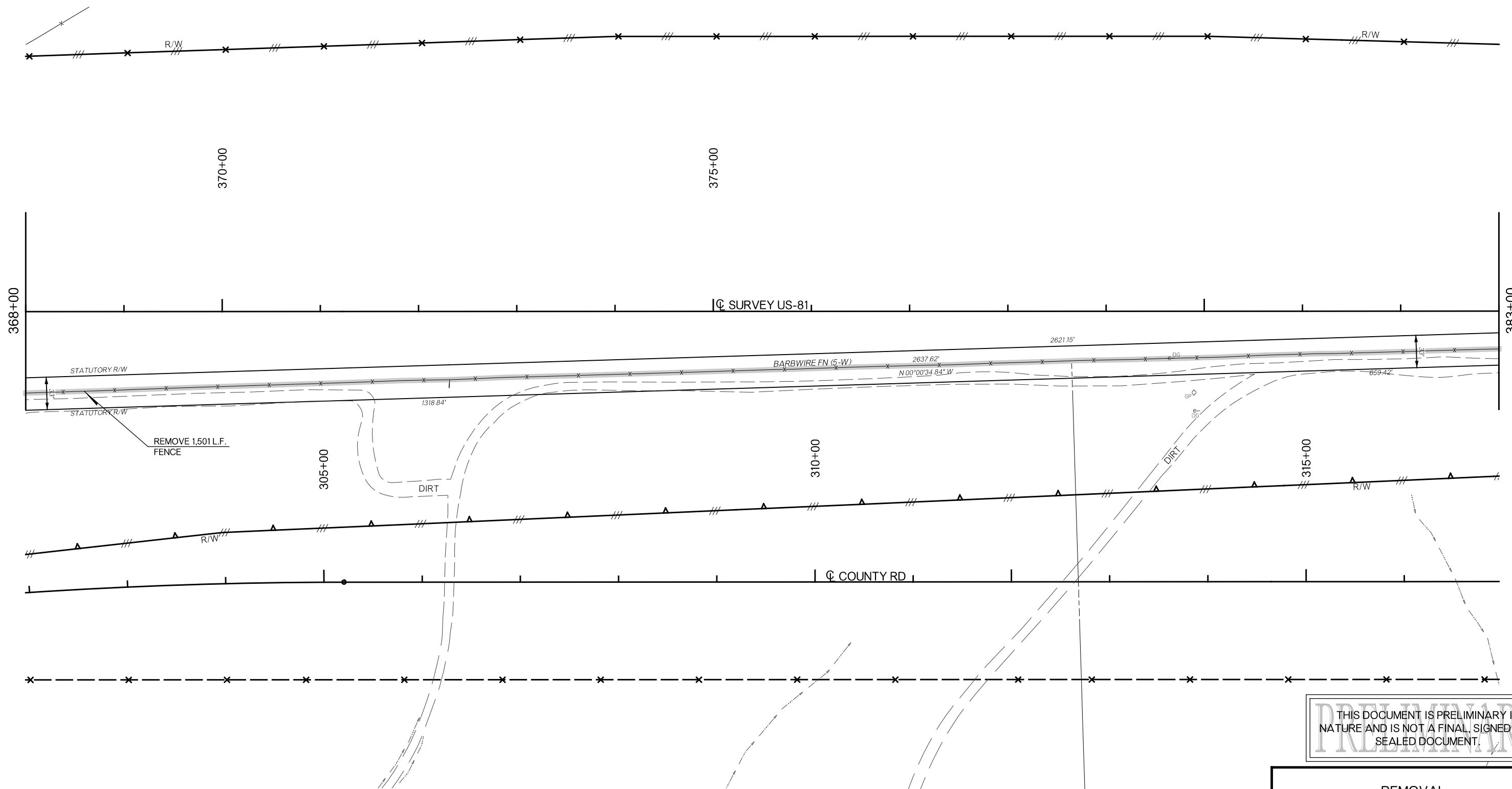
GRADY COUNTY US 81 REALIGNMENT



LEGEND

-  ASPHALT PAVEMENT REMOVAL
-  CONCRETE PAVEMENT REMOVAL
-  CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
-  MISCELLANEOUS REMOVAL

| REMOVAL QUANTITIES | |
|--------------------|------------|
| FENCE | L.F. 1,501 |



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

**REMOVAL
US 81**





State Job No. 24428(04) Sheet No. R239

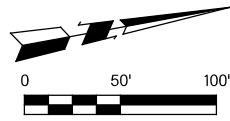
SEC. 36, T7N R8W

SEC. 31, T7N R7W

FINAL FIELD MEETING
MARCH 2020

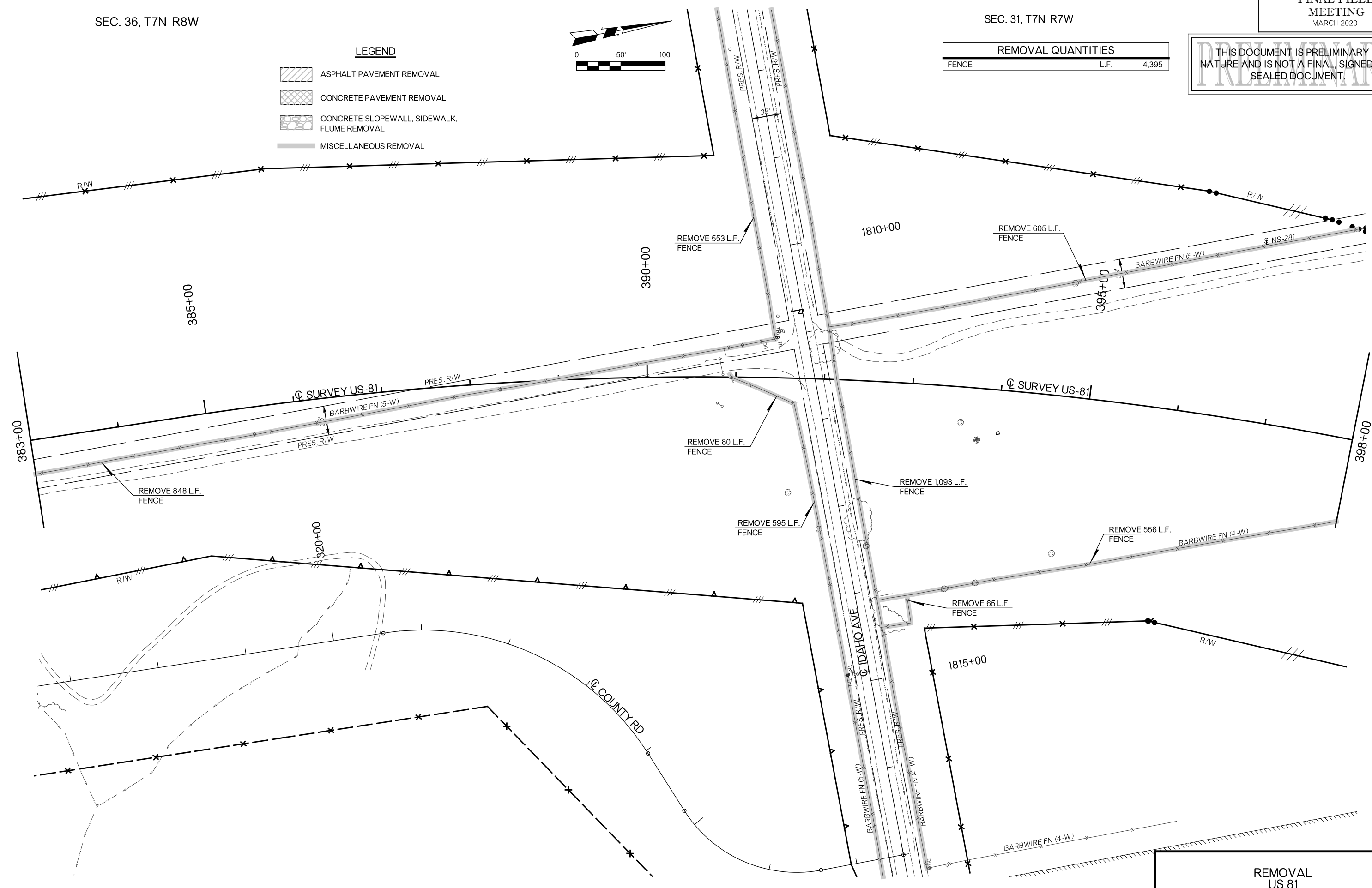
LEGEND

-  ASPHALT PAVEMENT REMOVAL
-  CONCRETE PAVEMENT REMOVAL
-  CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
-  MISCELLANEOUS REMOVAL



| REMOVAL QUANTITIES | |
|--------------------|------------|
| FENCE | L.F. 4,395 |

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SEC. 31, T7N R7W

SEC. 30, T7N R7W





REMOVAL US 81

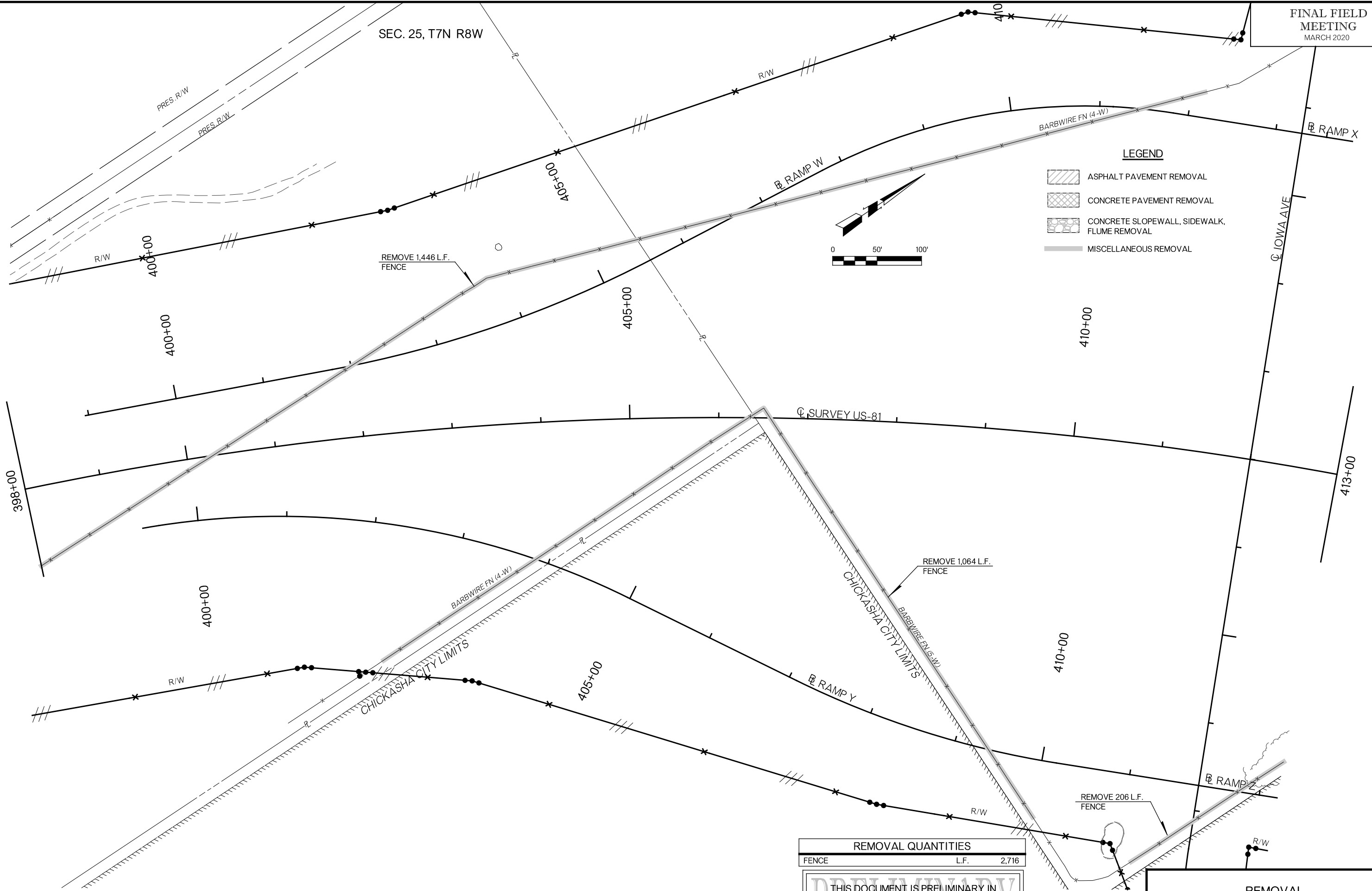
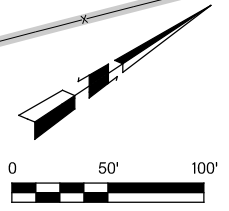
US 81 REALIGNMENT GRADY COUNTY

SEC. 25, T7N R8W

SEC. 30, T7N R7W

LEGEND

-  ASPHALT PAVEMENT REMOVAL
-  CONCRETE PAVEMENT REMOVAL
-  CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
-  MISCELLANEOUS REMOVAL



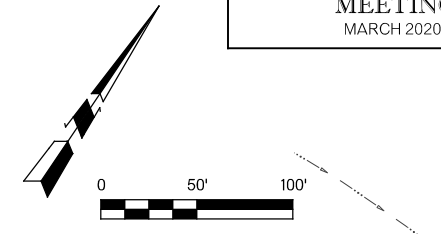
| REMOVAL QUANTITIES | | |
|--------------------|------|-------|
| FENCE | L.F. | 2,716 |

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

REMOVAL US 81





State Job No. 24428(04) Sheet No. R241

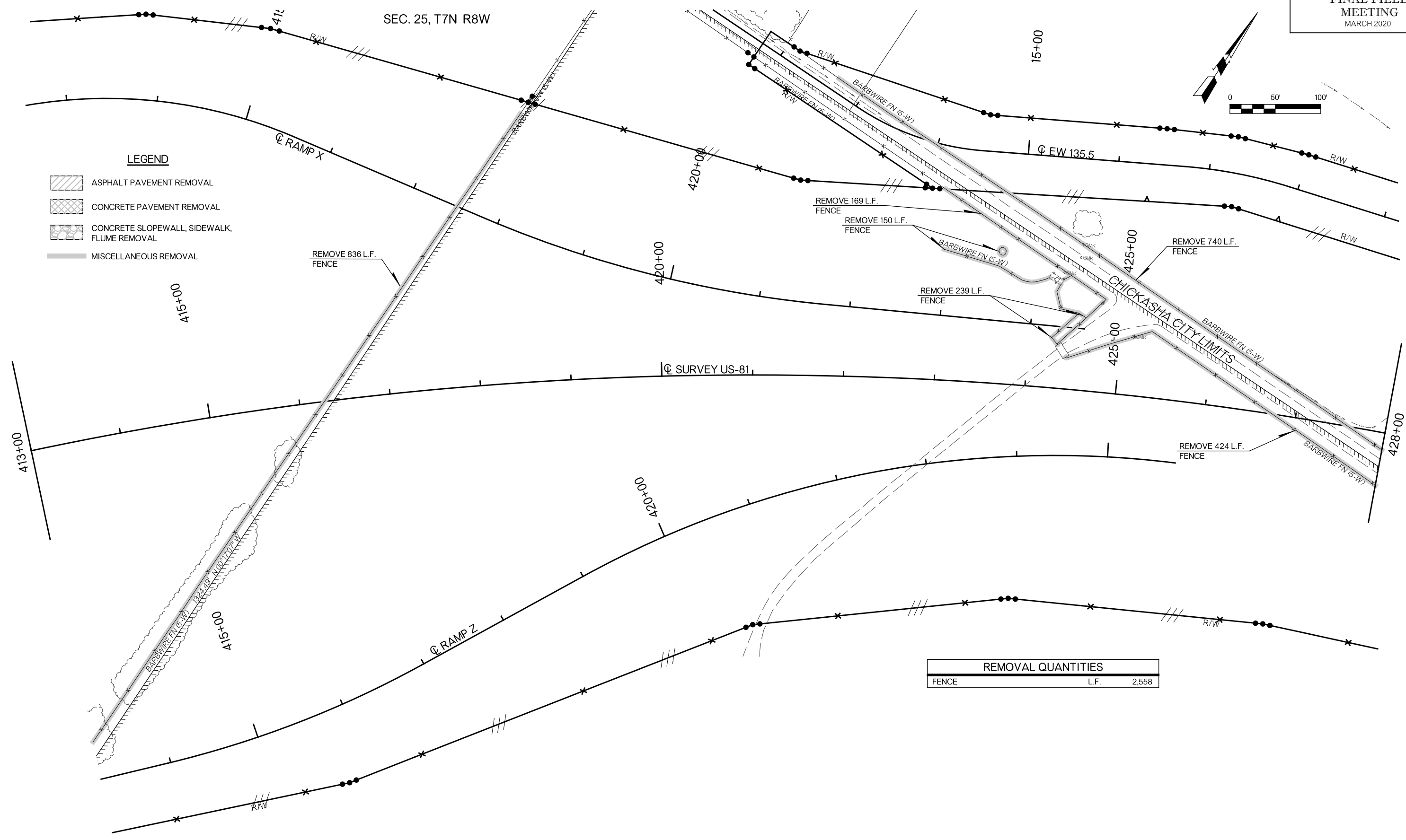
US 81 REALIGNMENT GRADY COUNTY



SEC. 25, T7N R8W

LEGEND

-  ASPHALT PAVEMENT REMOVAL
-  CONCRETE PAVEMENT REMOVAL
-  CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
-  MISCELLANEOUS REMOVAL



| REMOVAL QUANTITIES | | |
|--------------------|------|-------|
| FENCE | L.F. | 2,558 |

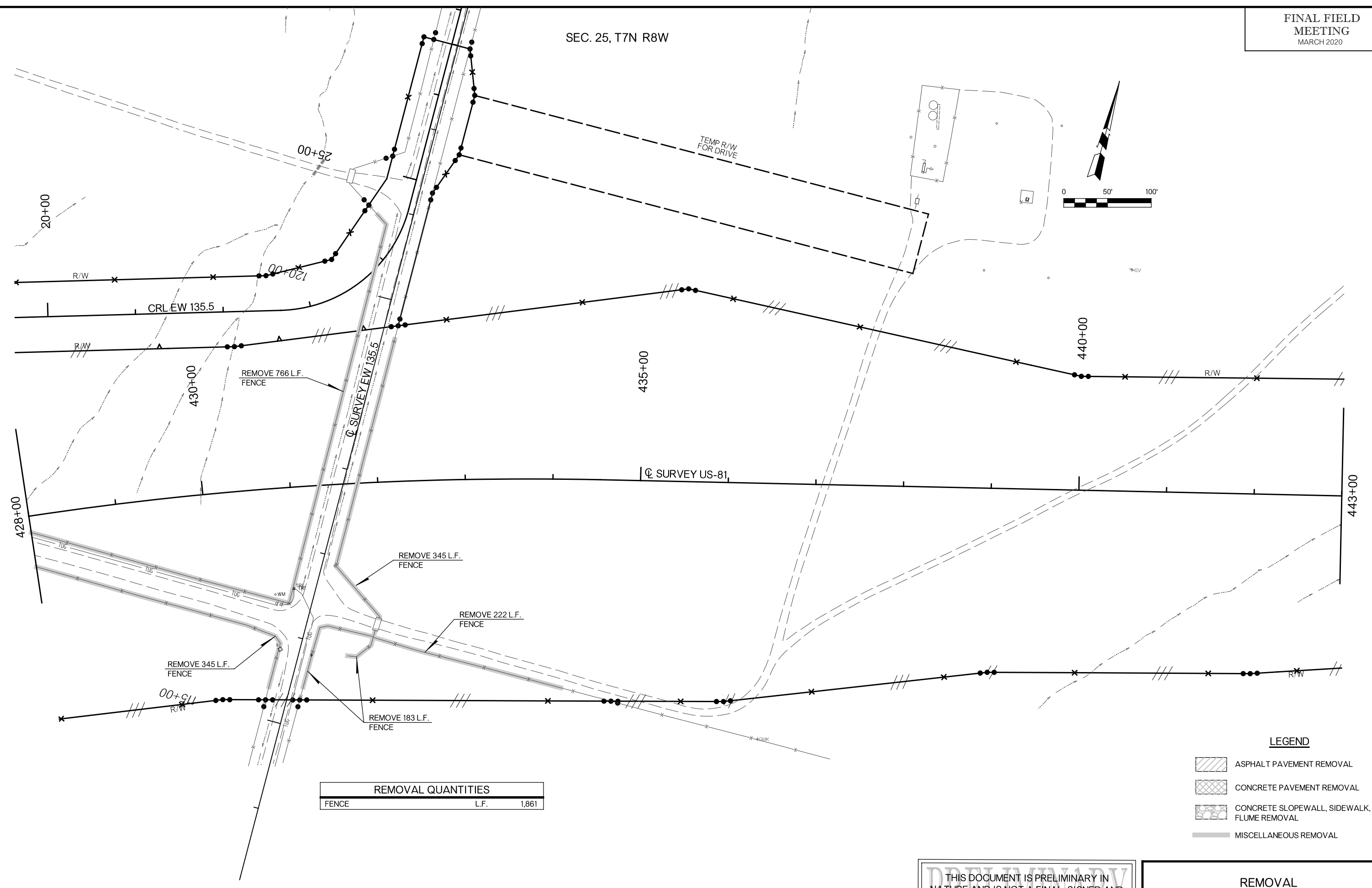
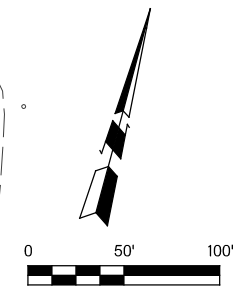
SEC. 30, T7N R7W

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

REMOVAL US 81

US 81 REALIGNMENT GRADY COUNTY

SEC. 25, T7N R8W



| REMOVAL QUANTITIES | |
|--------------------|------------|
| FENCE | L.F. 1,861 |

- LEGEND**
- ASPHALT PAVEMENT REMOVAL
 - CONCRETE PAVEMENT REMOVAL
 - CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
 - MISCELLANEOUS REMOVAL

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

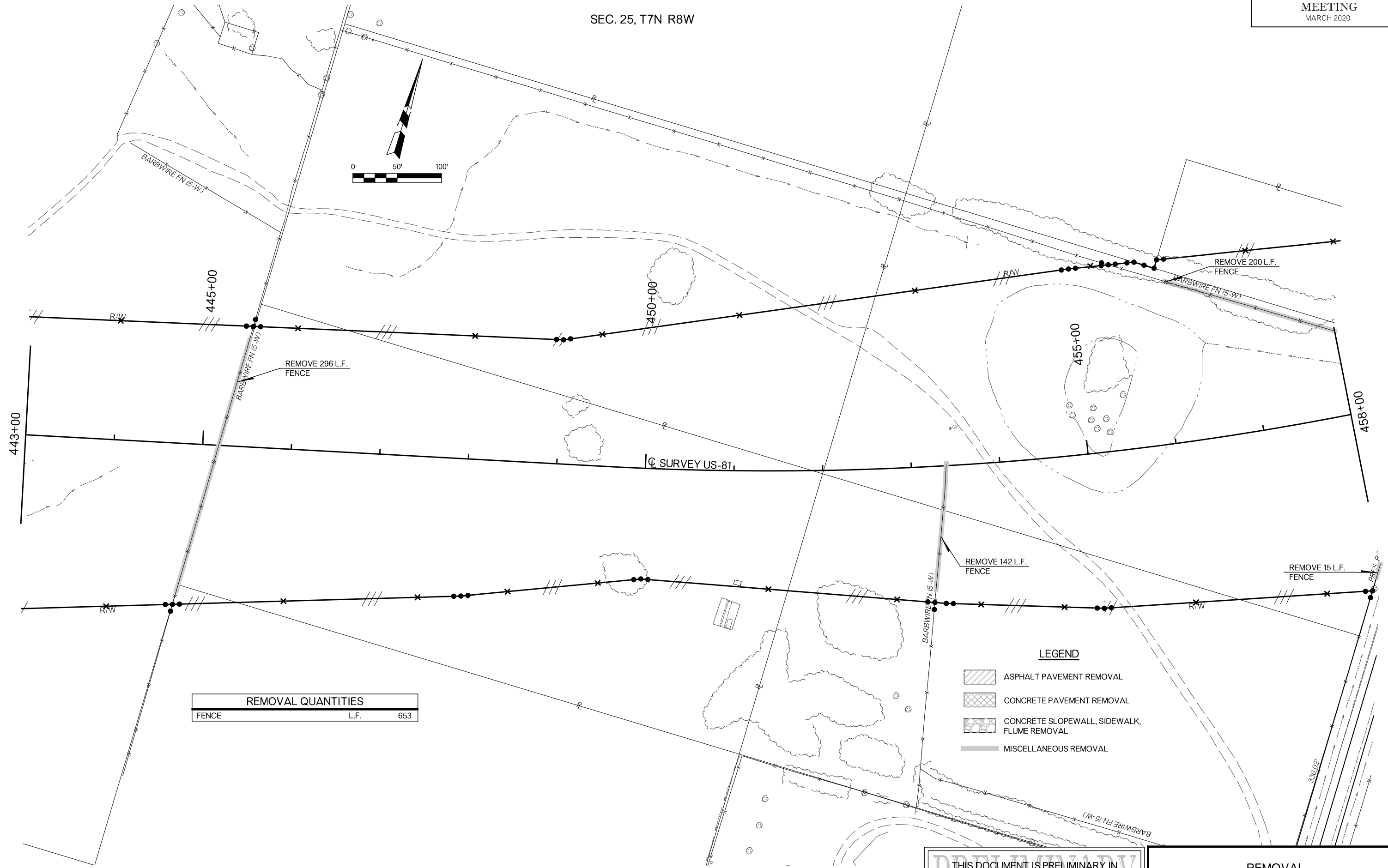
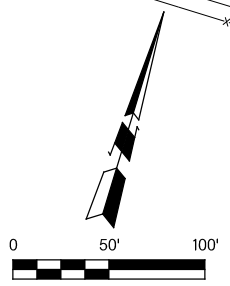
REMOVAL US 81

State Job No. 24428(04) Sheet No. R243

SEC. 30, T7N R7W

US 81 REALIGNMENT
GRADY COUNTY

SEC. 25, T7N R8W



| REMOVAL QUANTITIES | | |
|--------------------|------|-----|
| FENCE | L.F. | 653 |

- LEGEND**
- ASPHALT PAVEMENT REMOVAL
 - CONCRETE PAVEMENT REMOVAL
 - CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
 - MISCELLANEOUS REMOVAL

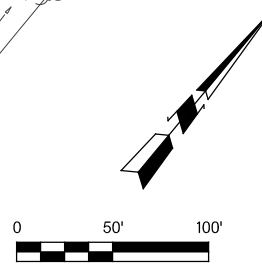
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

REMOVAL US 81

SEC. 30, T7N R7W

US 81 REALIGNMENT GRADY COUNTY

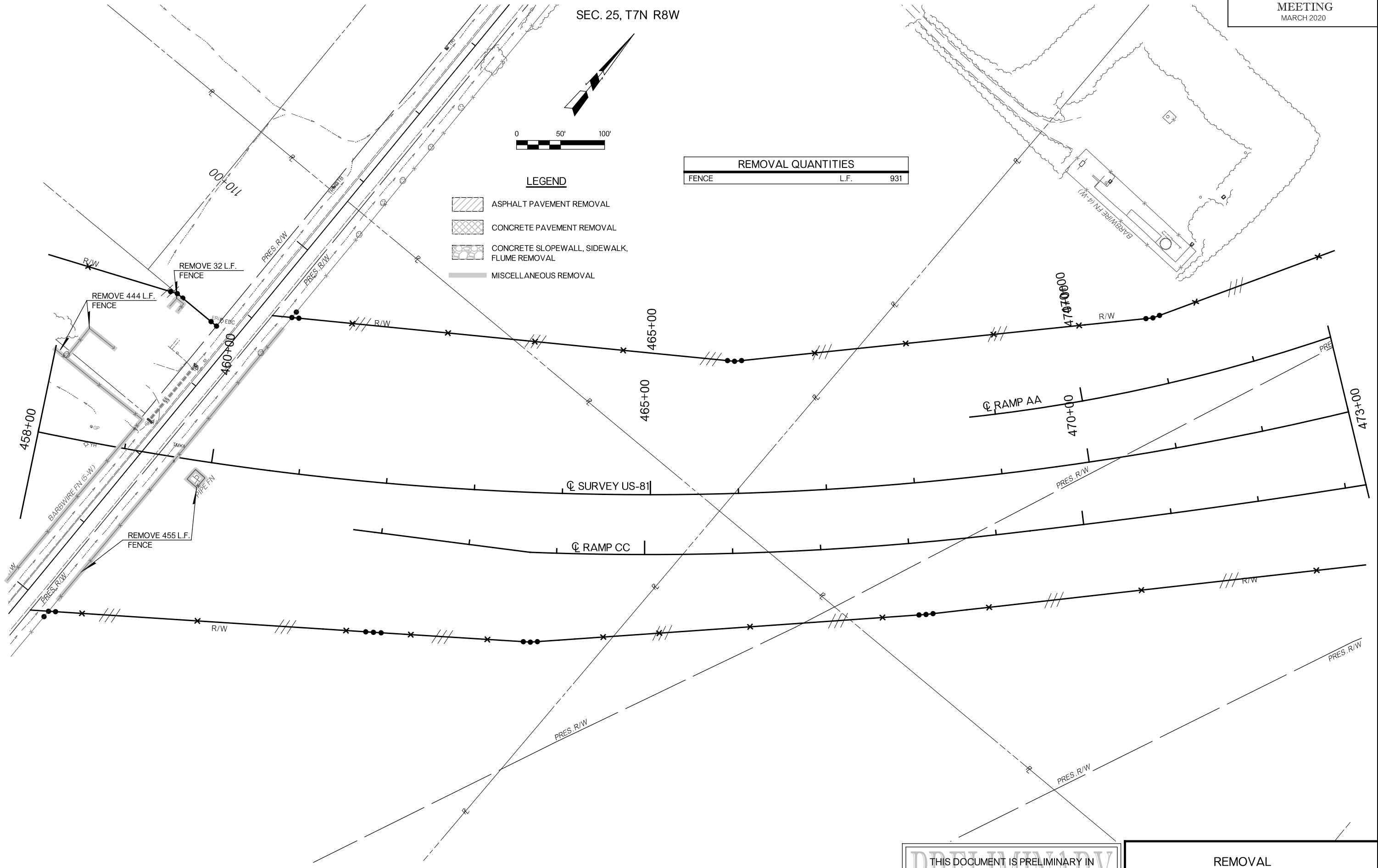
SEC. 25, T7N R8W



| REMOVAL QUANTITIES | |
|--------------------|----------|
| FENCE | L.F. 931 |

LEGEND

- ASPHALT PAVEMENT REMOVAL
- CONCRETE PAVEMENT REMOVAL
- CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
- MISCELLANEOUS REMOVAL



SEC. 30, T7N R7W

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

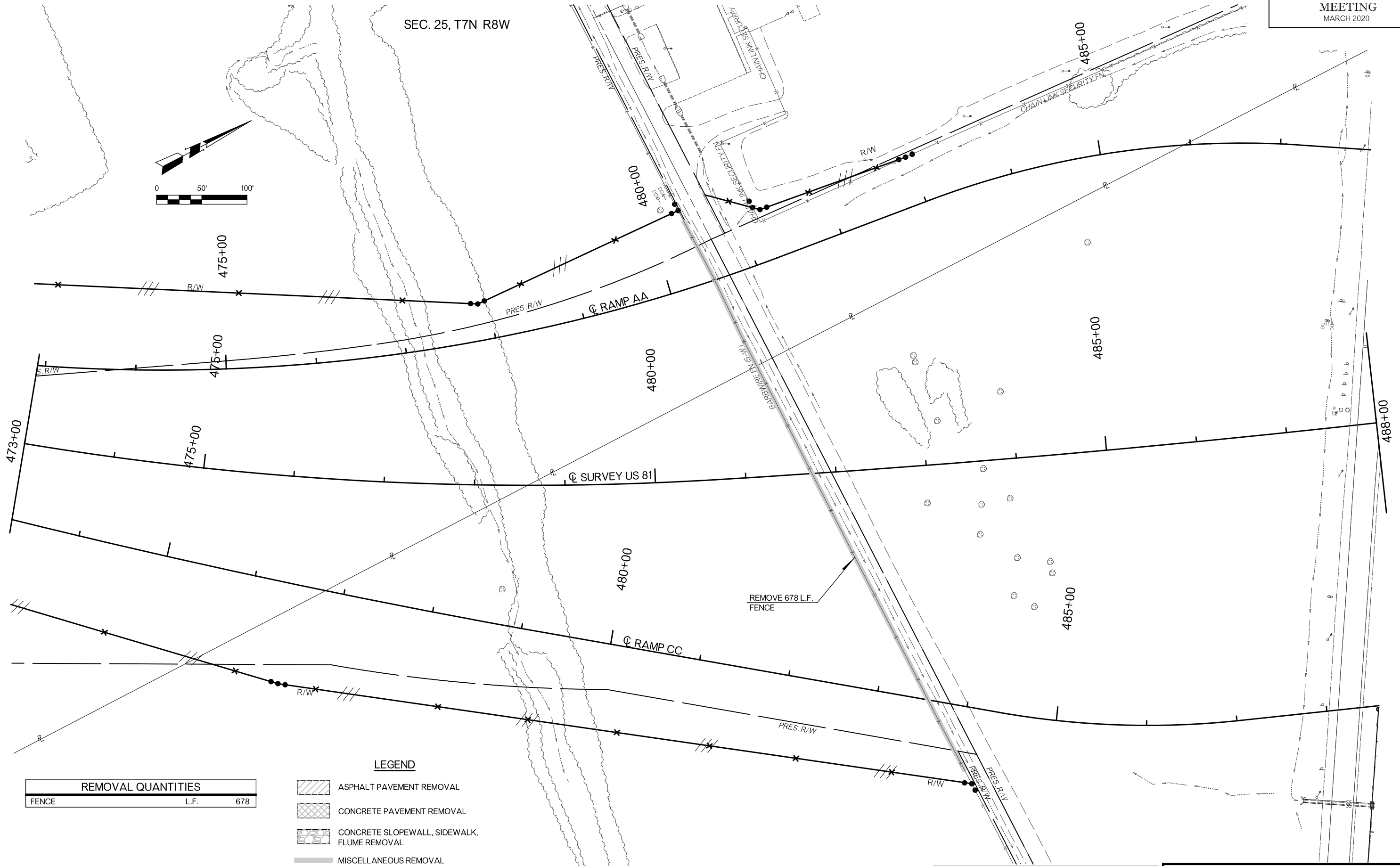
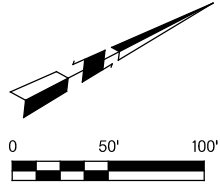
**REMOVAL
US 81**

State Job No. 24428(04) Sheet No. R245

GRADY COUNTY
US 81 REALIGNMENT

SEC. 25, T7N R8W

SEC. 30, T7N R7W



| REMOVAL QUANTITIES | |
|--------------------|----------|
| FENCE | L.F. 678 |

- LEGEND**
- ASPHALT PAVEMENT REMOVAL
 - CONCRETE PAVEMENT REMOVAL
 - CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
 - MISCELLANEOUS REMOVAL

REMOVE 678 L.F. FENCE





THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

REMOVAL US 81
State Job No. 24428(04) Sheet No. R244

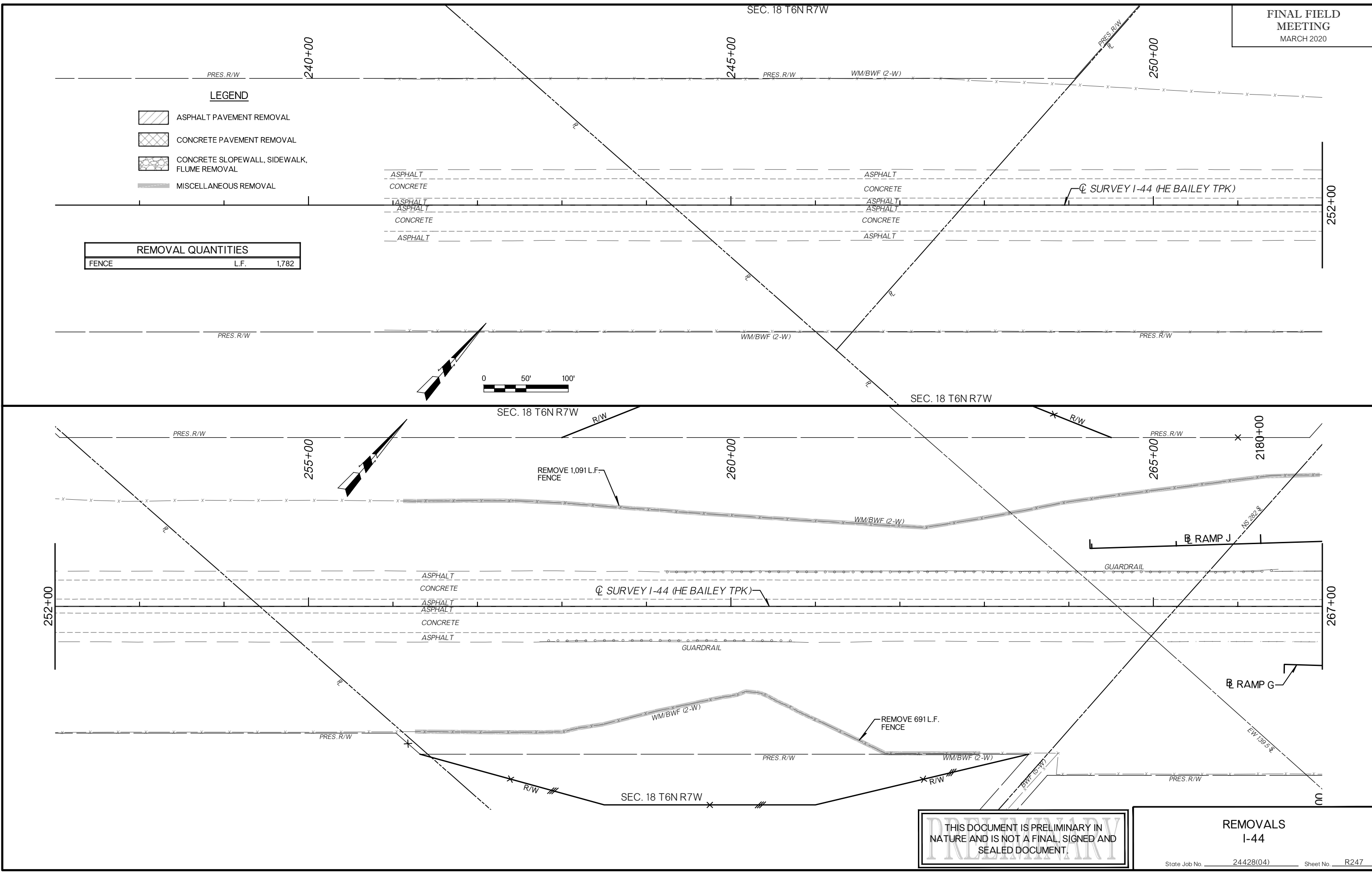
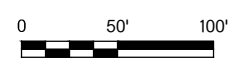
GRADY COUNTY US 81 REALIGNMENT

SEC. 18 T6N R7W

LEGEND

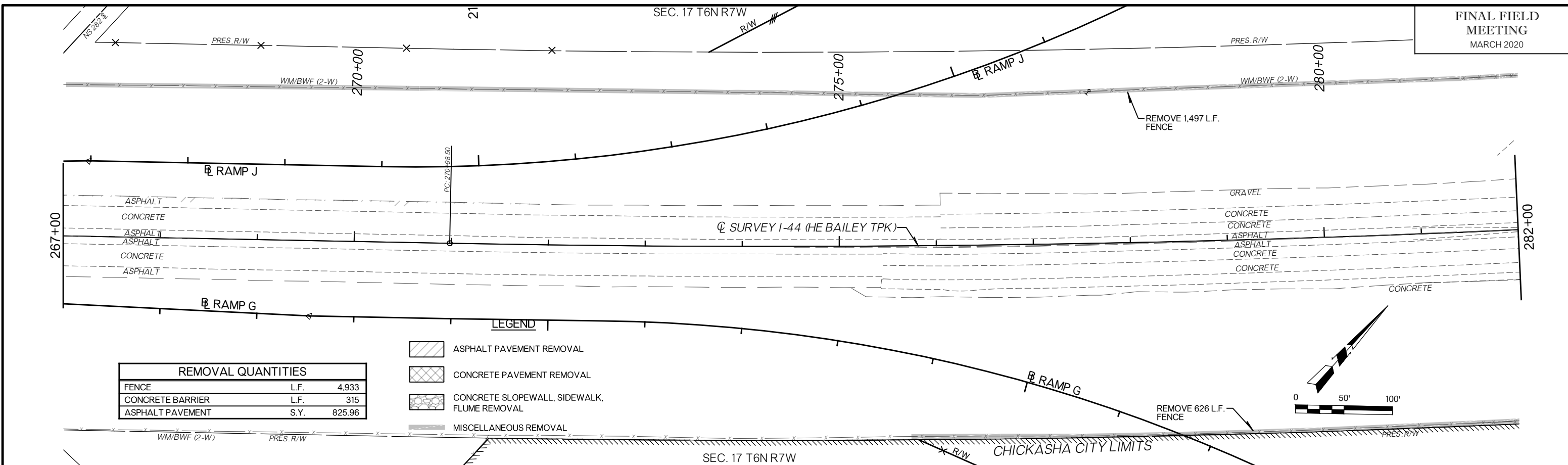
-  ASPHALT PAVEMENT REMOVAL
-  CONCRETE PAVEMENT REMOVAL
-  CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
-  MISCELLANEOUS REMOVAL

| REMOVAL QUANTITIES | | |
|--------------------|------|-------|
| | L.F. | 1,782 |
| FENCE | | |



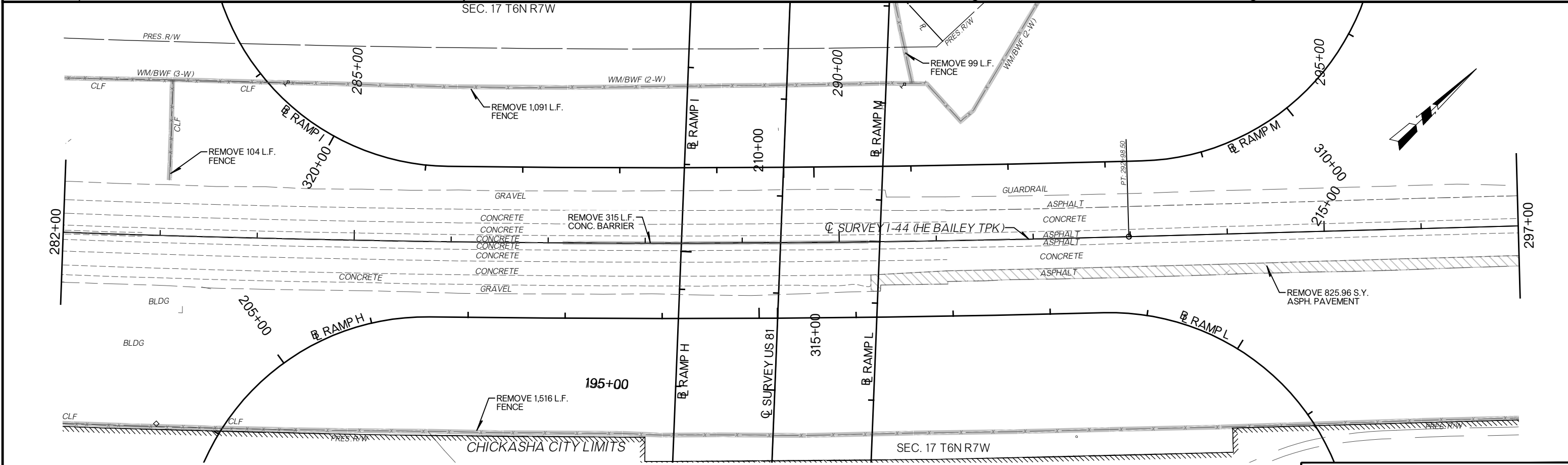
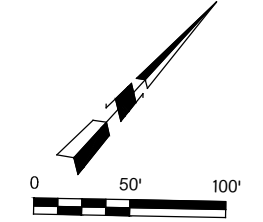
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

REMOVALS I-44
State Job No. 24428(04) Sheet No. R247



| REMOVAL QUANTITIES | | |
|--------------------|------|--------|
| FENCE | L.F. | 4,933 |
| CONCRETE BARRIER | L.F. | 315 |
| ASPHALT PAVEMENT | S.Y. | 825.96 |

- LEGEND**
- ASPHALT PAVEMENT REMOVAL
 - CONCRETE PAVEMENT REMOVAL
 - CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
 - MISCELLANEOUS REMOVAL



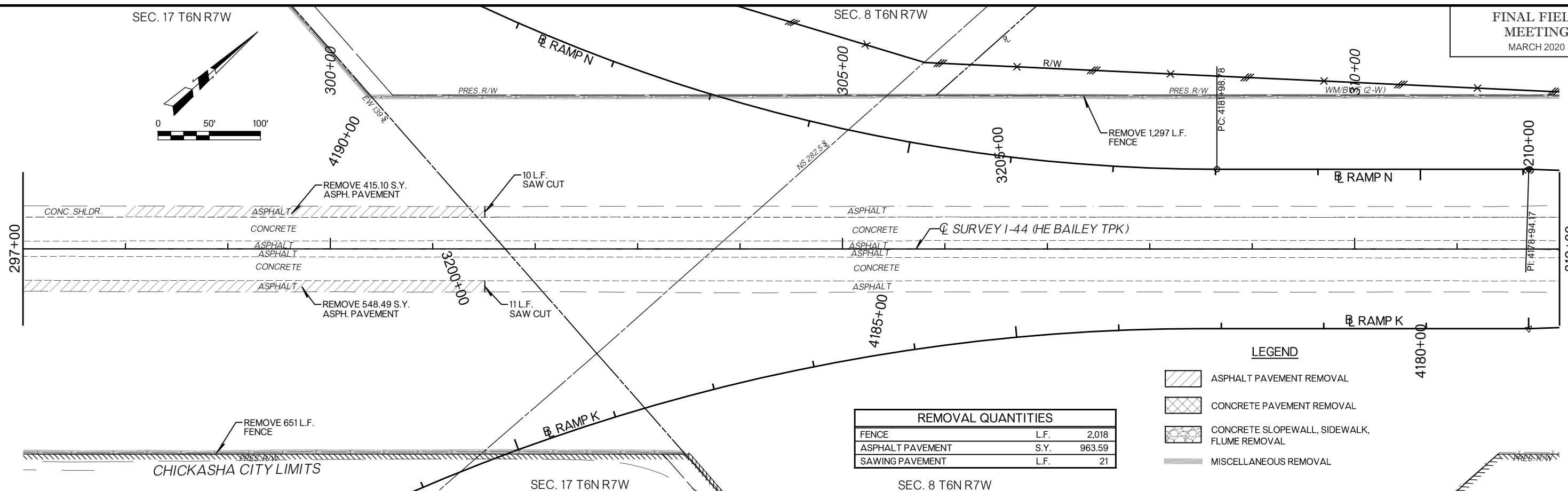
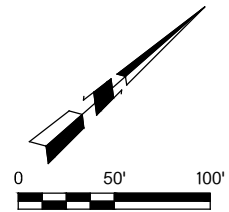
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

REMOVAL I-44
State Job No. 24428(04) Sheet No. R248

GRADY COUNTY US 81 REALIGNMENT

SEC. 17 T6N R7W

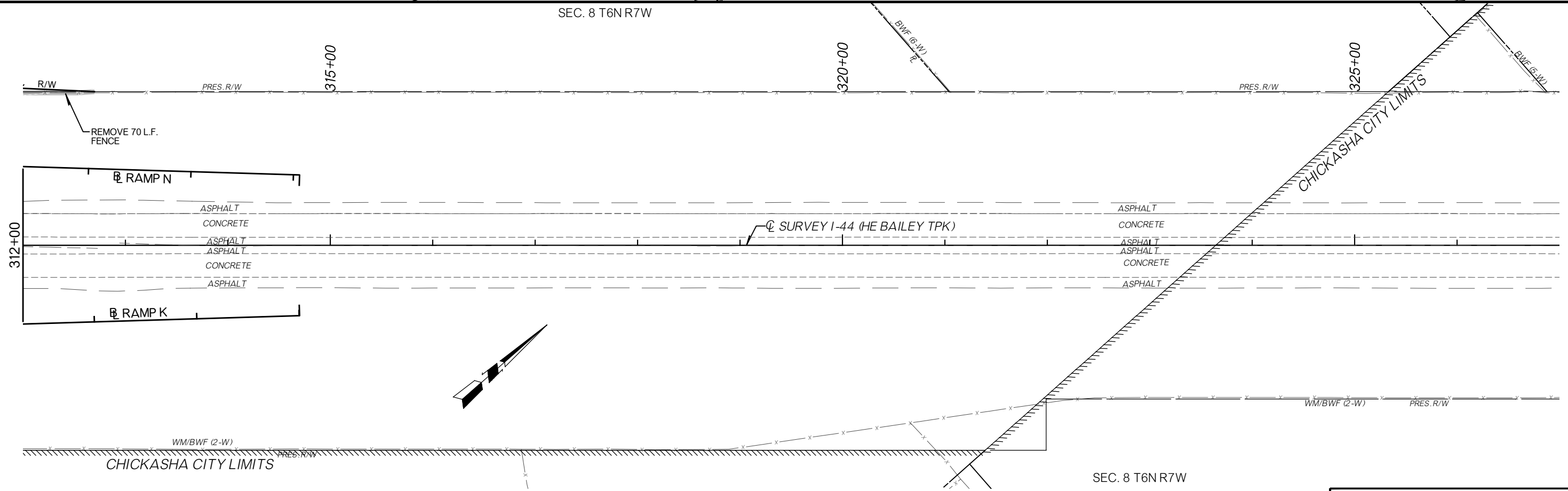
SEC. 8 T6N R7W



LEGEND

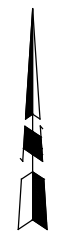
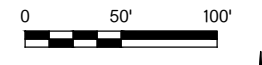
- ASPHALT PAVEMENT REMOVAL
- CONCRETE PAVEMENT REMOVAL
- CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
- MISCELLANEOUS REMOVAL

| REMOVAL QUANTITIES | | |
|--------------------|------|--------|
| FENCE | L.F. | 2,018 |
| ASPHALT PAVEMENT | S.Y. | 963.59 |
| SAWING PAVEMENT | L.F. | 21 |



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

REMOVAL I-44
State Job No. 24428(04) Sheet No. R249

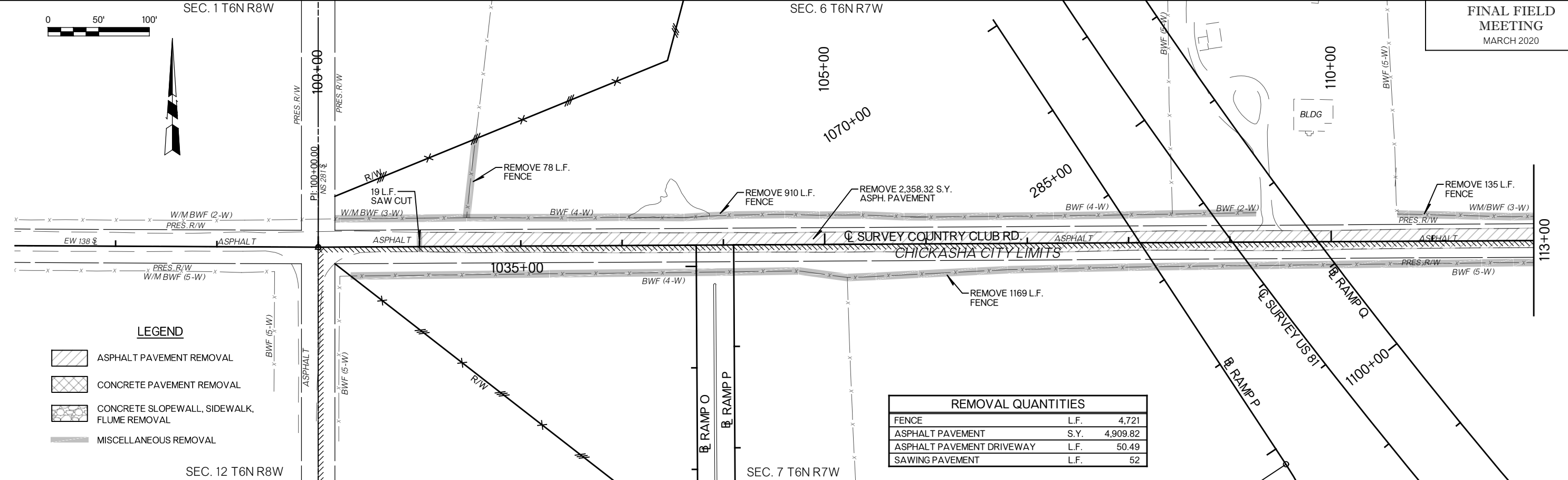


SEC. 1 T6N R8W

SEC. 6 T6N R7W

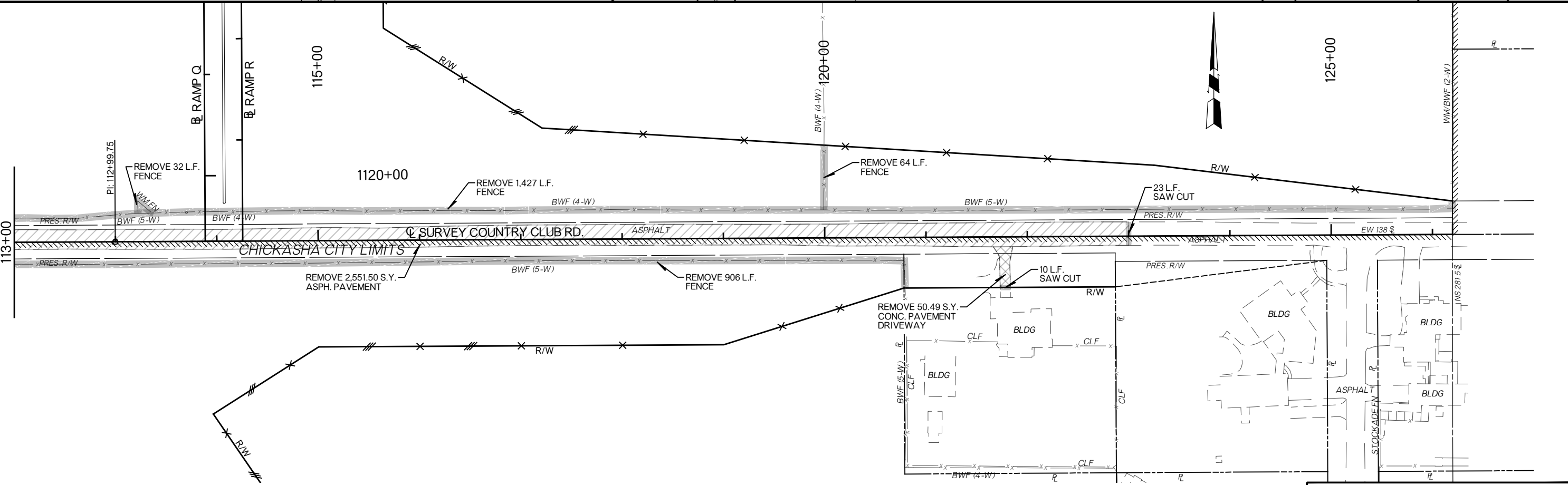
SEC. 12 T6N R8W

SEC. 7 T6N R7W

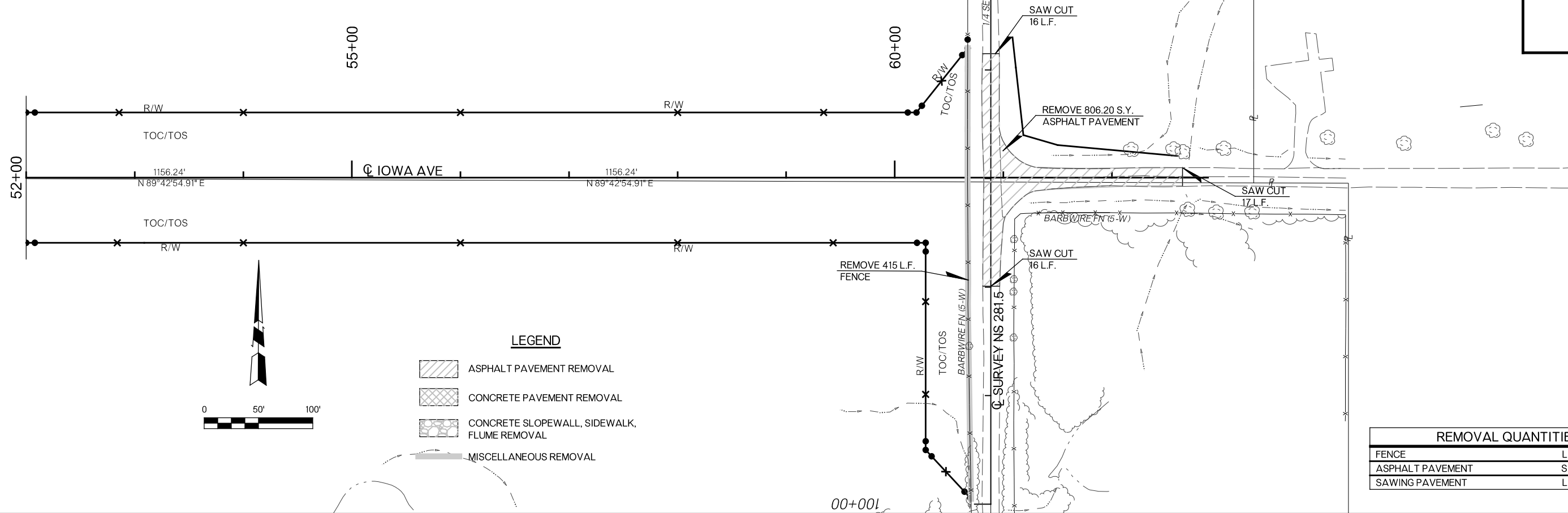


- LEGEND**
- ASPHALT PAVEMENT REMOVAL
 - CONCRETE PAVEMENT REMOVAL
 - CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
 - MISCELLANEOUS REMOVAL





| REMOVAL QUANTITIES | | |
|---------------------------|------|----------|
| FENCE | L.F. | 4,721 |
| ASPHALT PAVEMENT | S.Y. | 4,909.82 |
| ASPHALT PAVEMENT DRIVEWAY | L.F. | 50.49 |
| SAWING PAVEMENT | L.F. | 52 |



REMOVAL COUNTRY CLUB RD.



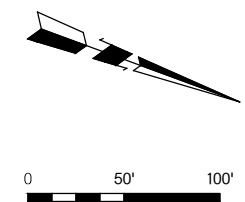
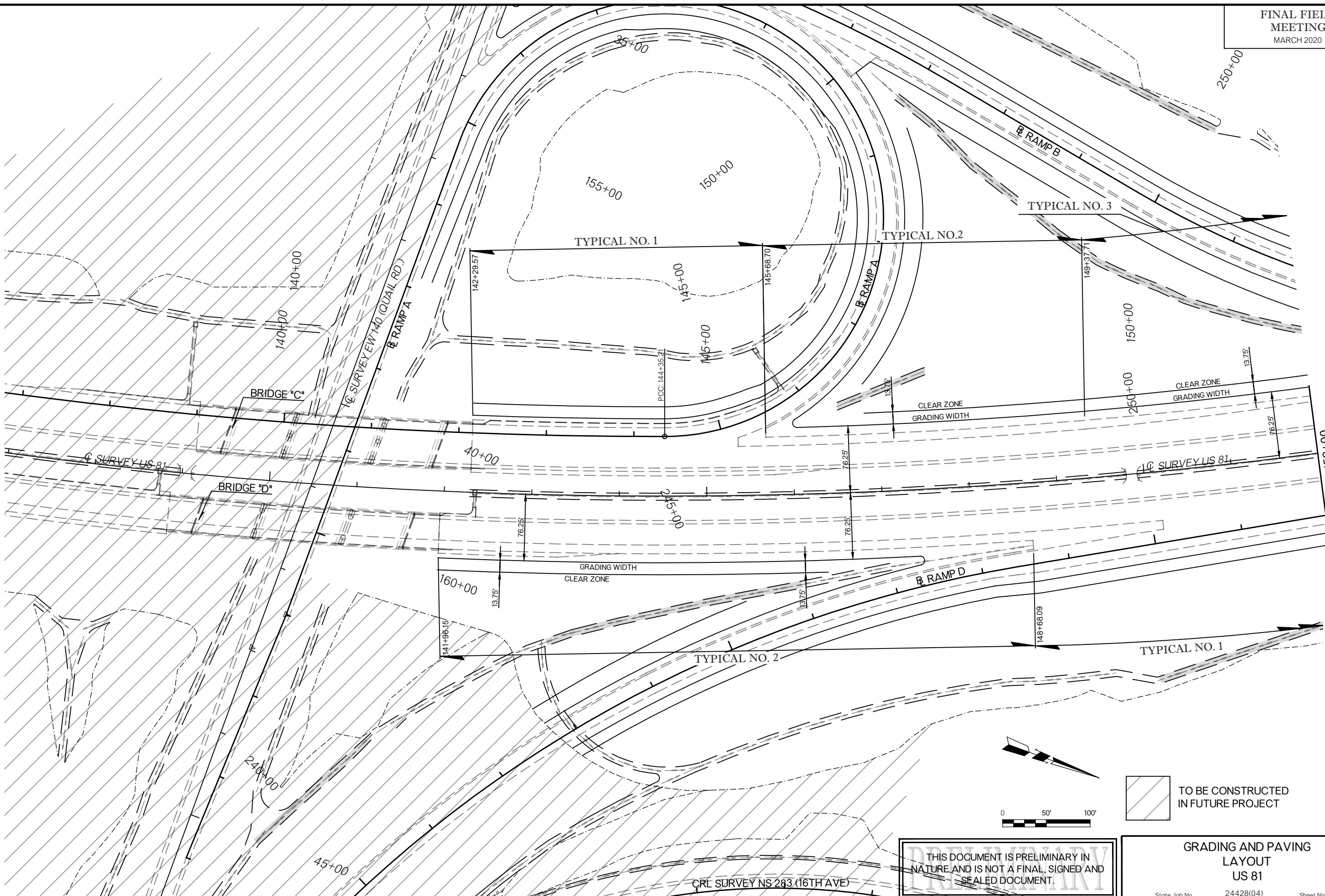
LEGEND

-  ASPHALT PAVEMENT REMOVAL
-  CONCRETE PAVEMENT REMOVAL
-  CONCRETE SLOPEWALL, SIDEWALK, FLUME REMOVAL
-  MISCELLANEOUS REMOVAL

| REMOVAL QUANTITIES | | |
|--------------------|------|--------|
| FENCE | L.F. | 415 |
| ASPHALT PAVEMENT | S.Y. | 806.20 |
| SAWING PAVEMENT | L.F. | 49 |

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

**REMOVAL
IOWA AVE.**



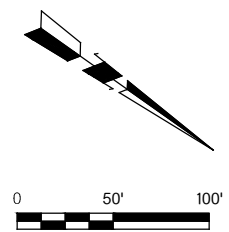
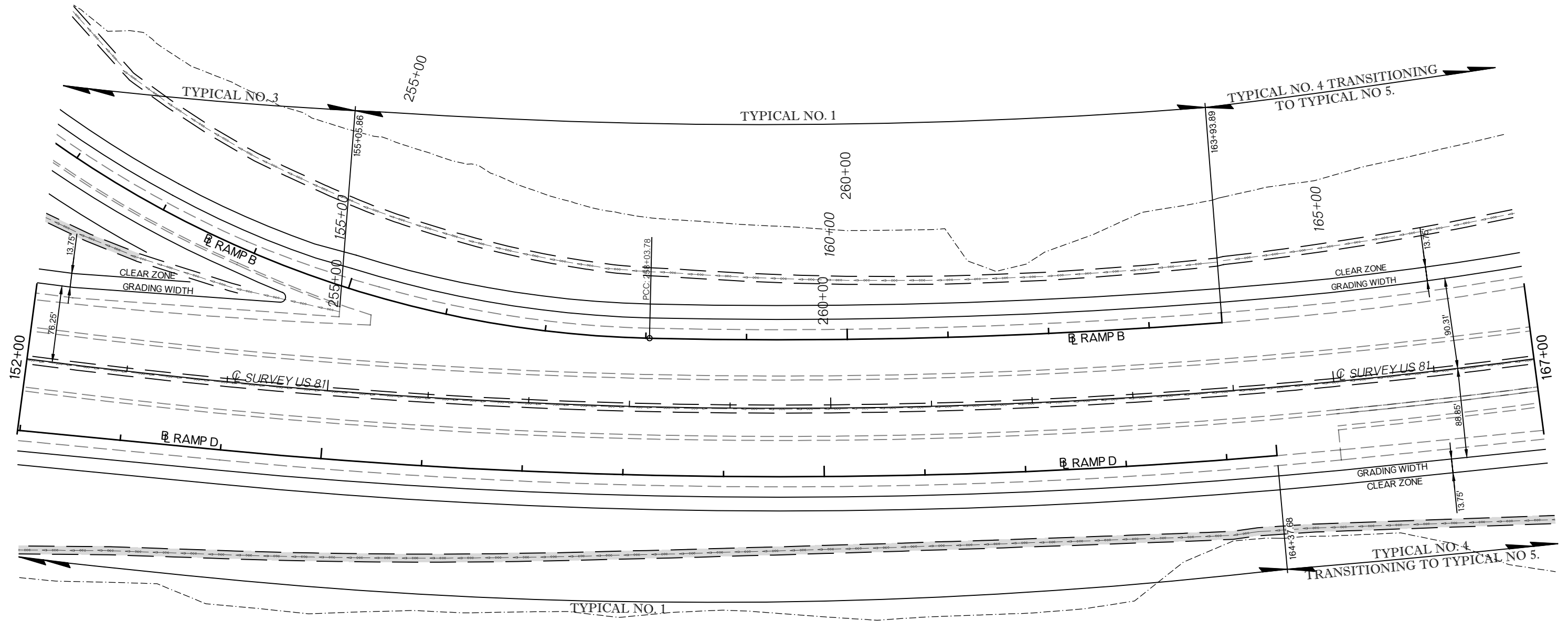
 TO BE CONSTRUCTED IN FUTURE PROJECT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT US 81

State Job No. 24428(04) Sheet No. R252

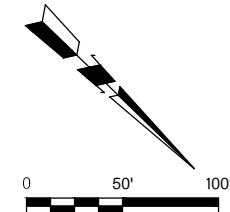
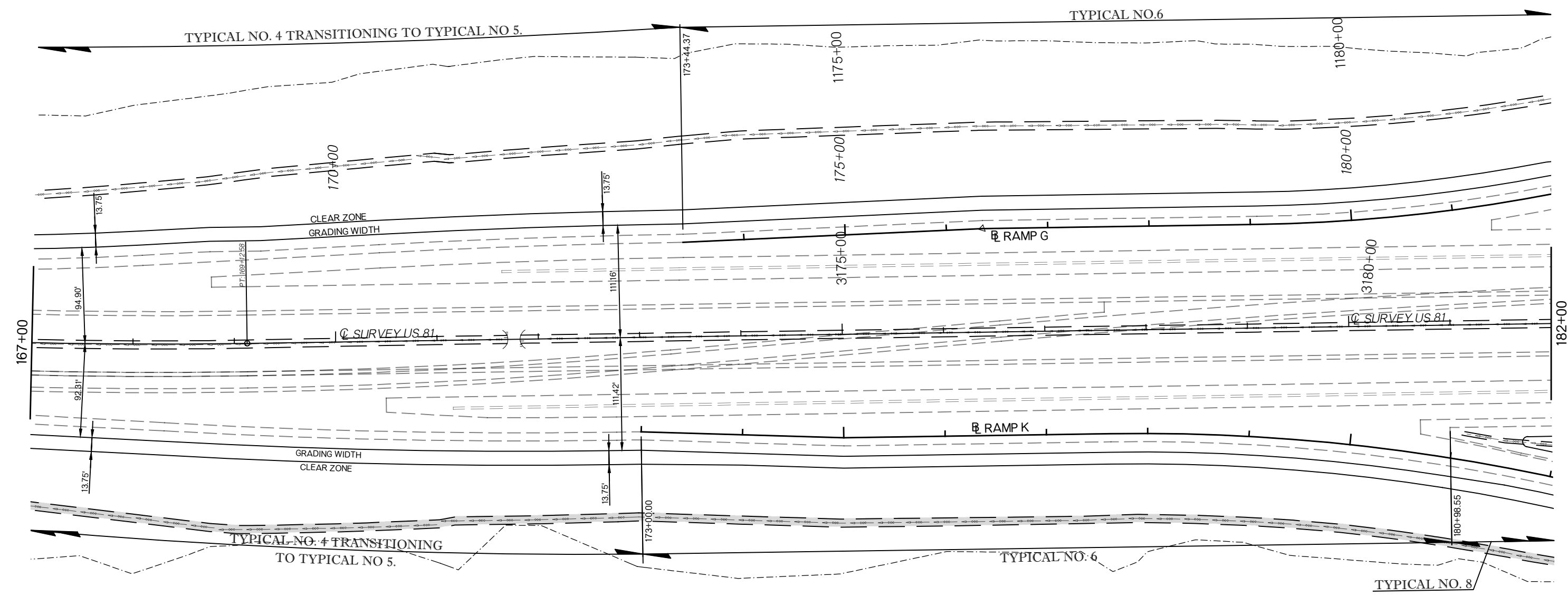
GRADY COUNTY US 81 REALIGNMENT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R253

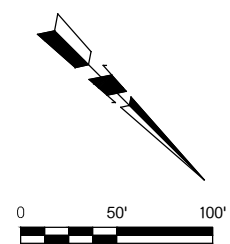
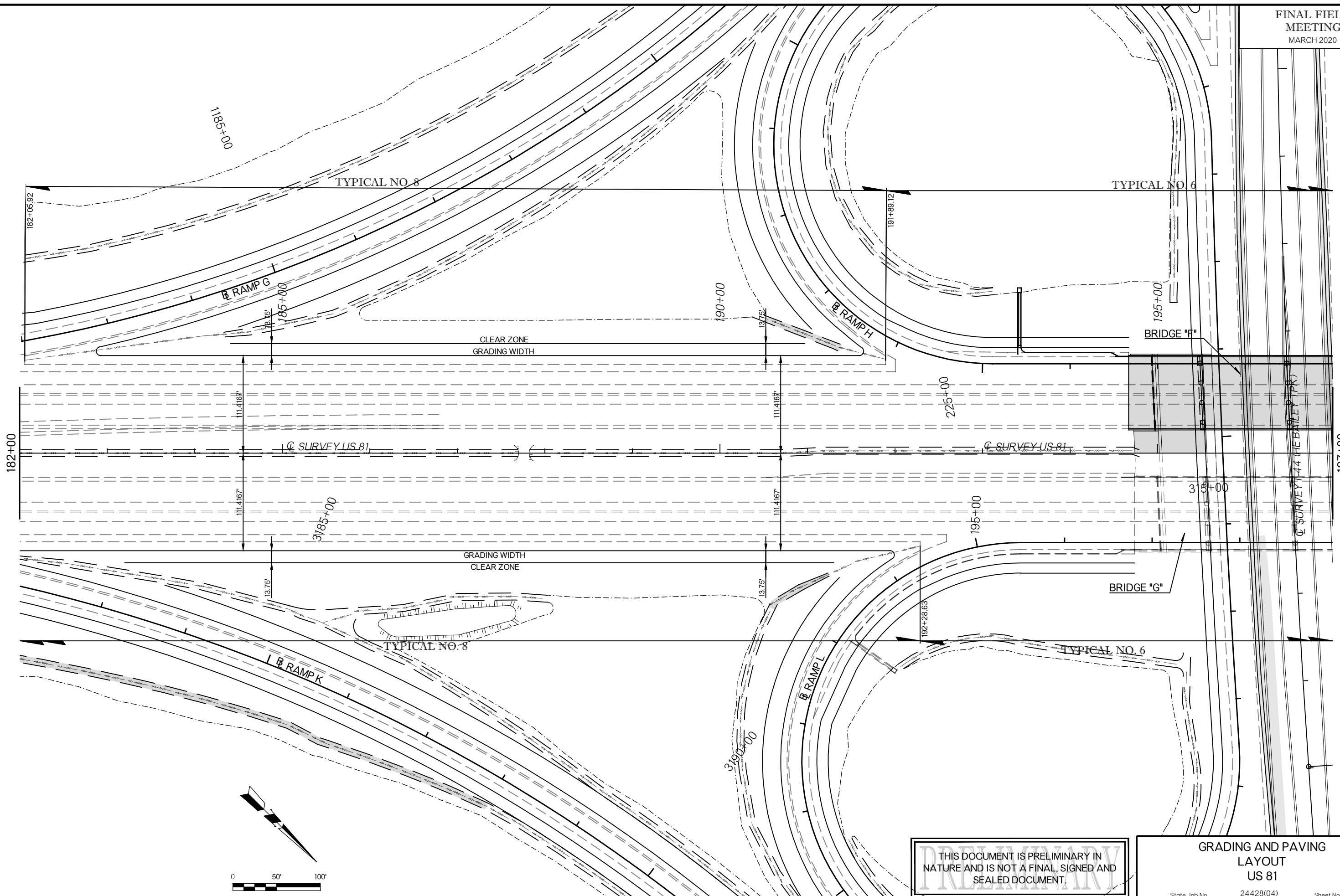
GRADY COUNTY
US 81 REALIGNMENT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

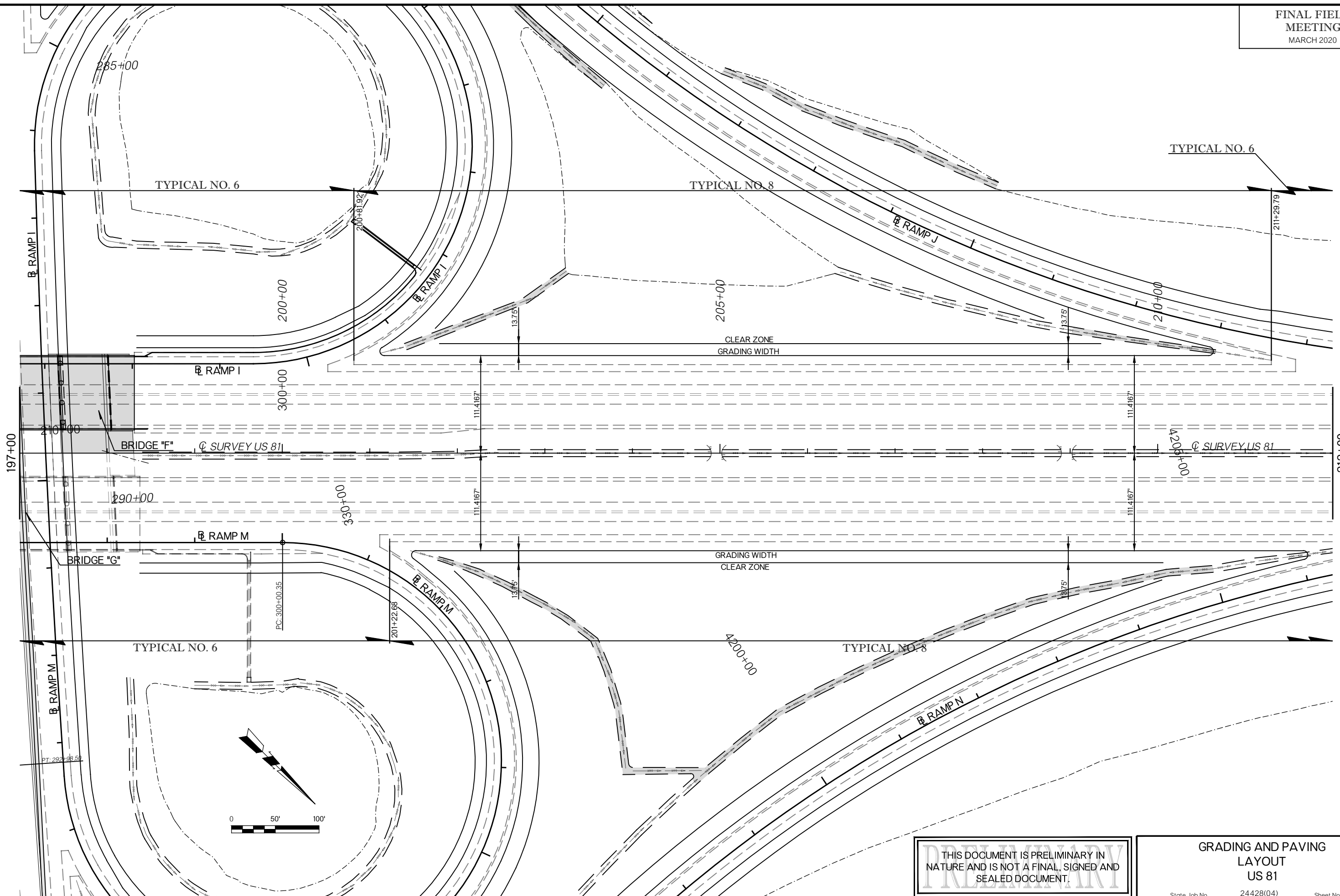
GRADING AND PAVING LAYOUT
US 81
 State Job No. 24428(04) Sheet No. R254

GRADY COUNTY US 81 REALIGNMENT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

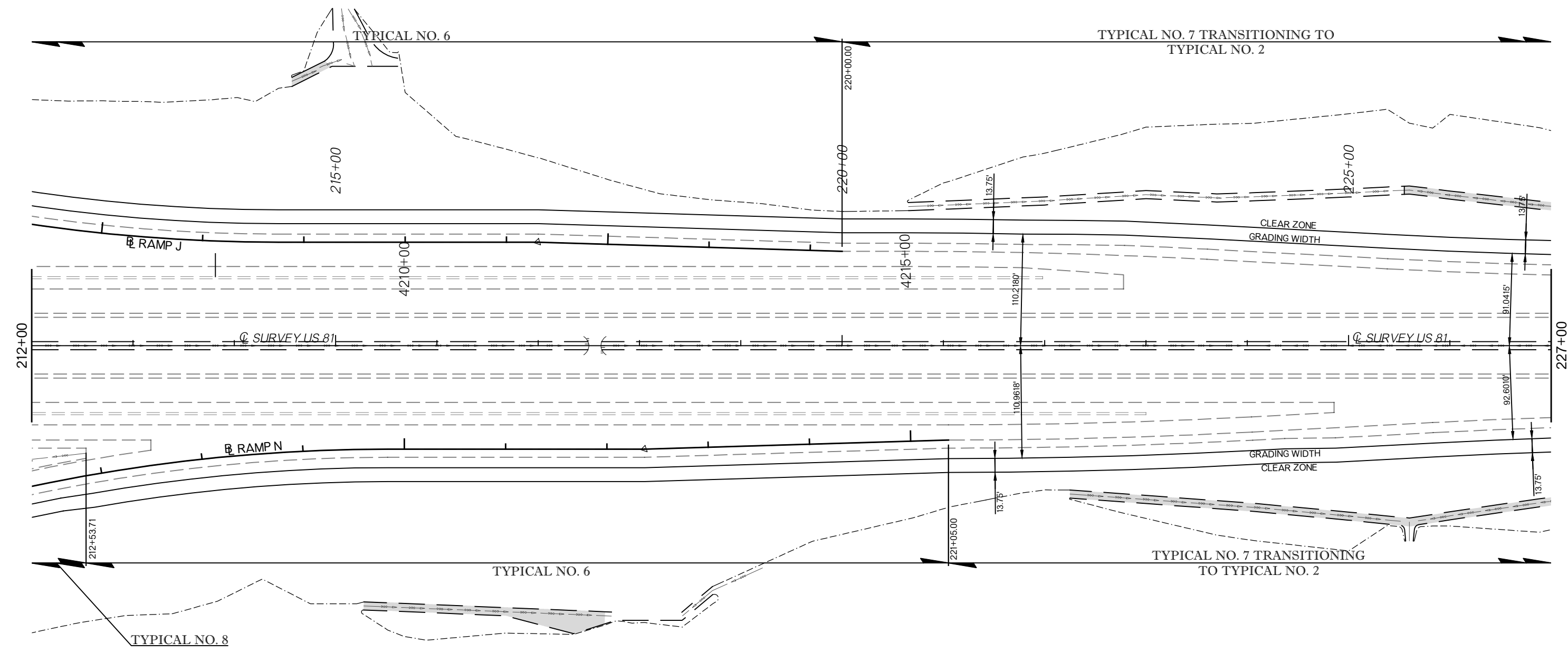
GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R255



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R256

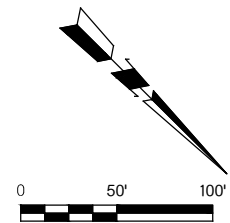
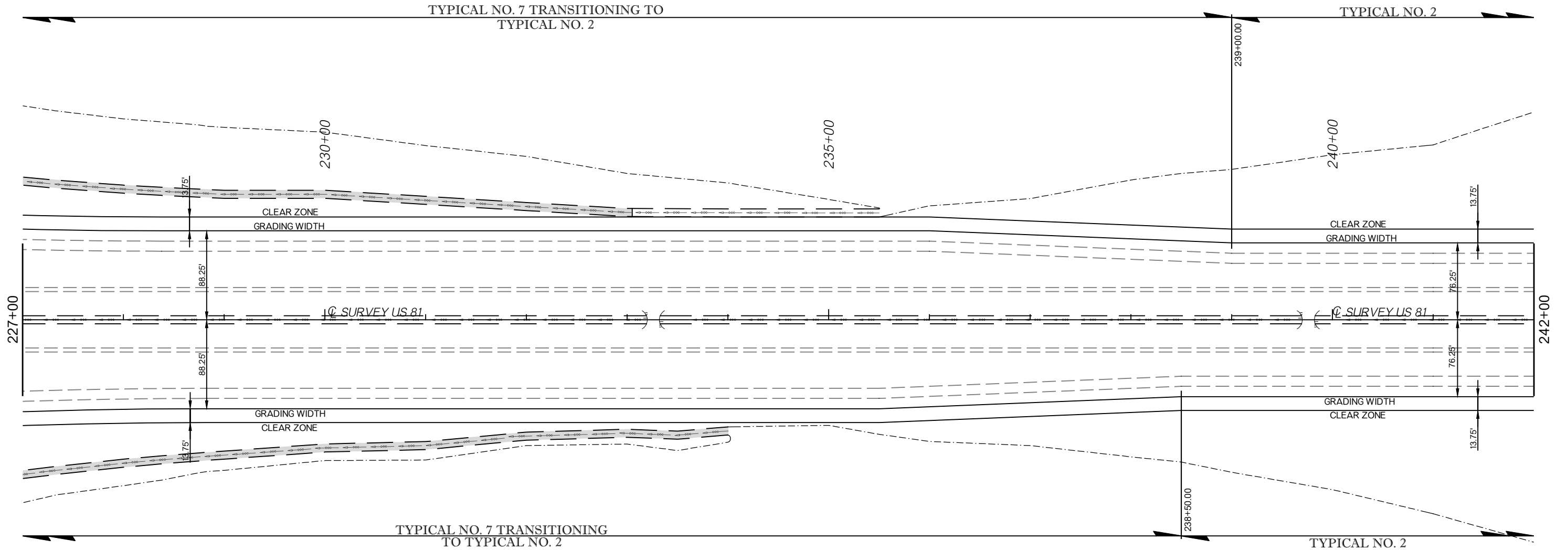
US 81 REALIGNMENT
GRADY COUNTY



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R257

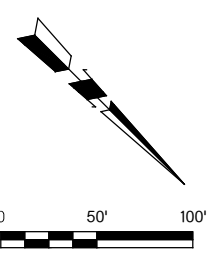
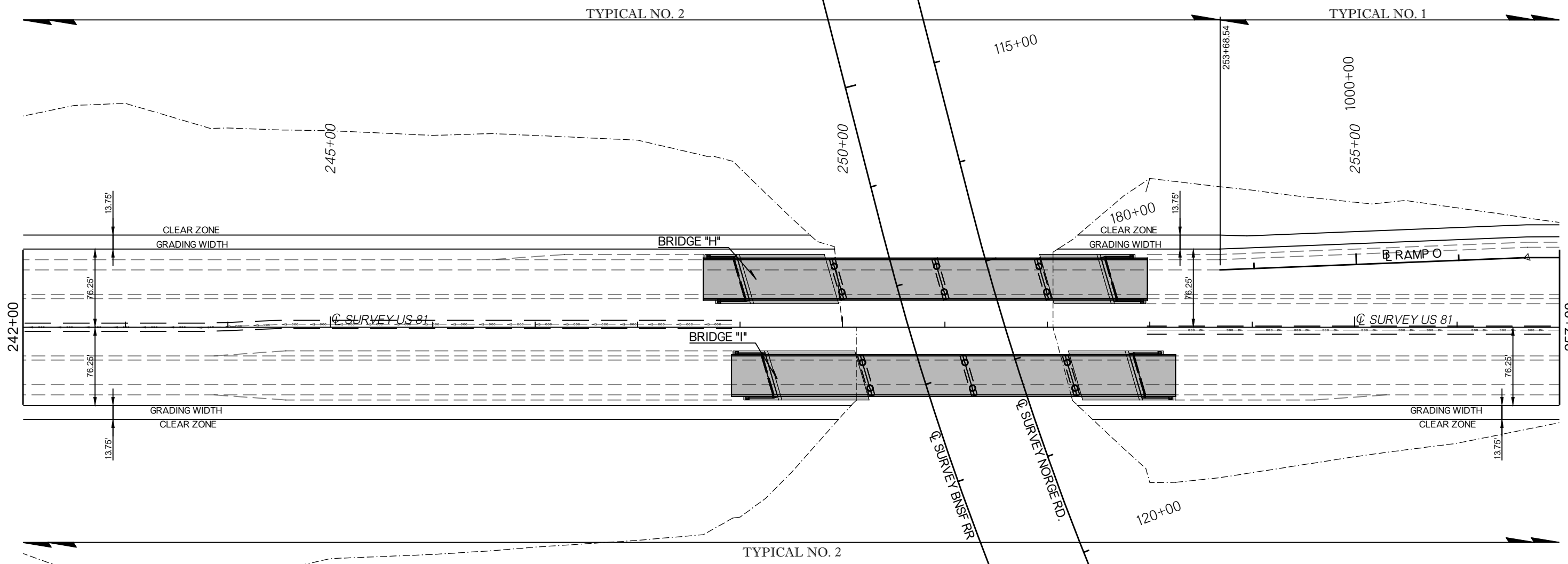
US 81 REALIGNMENT
GRADY COUNTY



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R258

US 81 REALIGNMENT
GRADY COUNTY



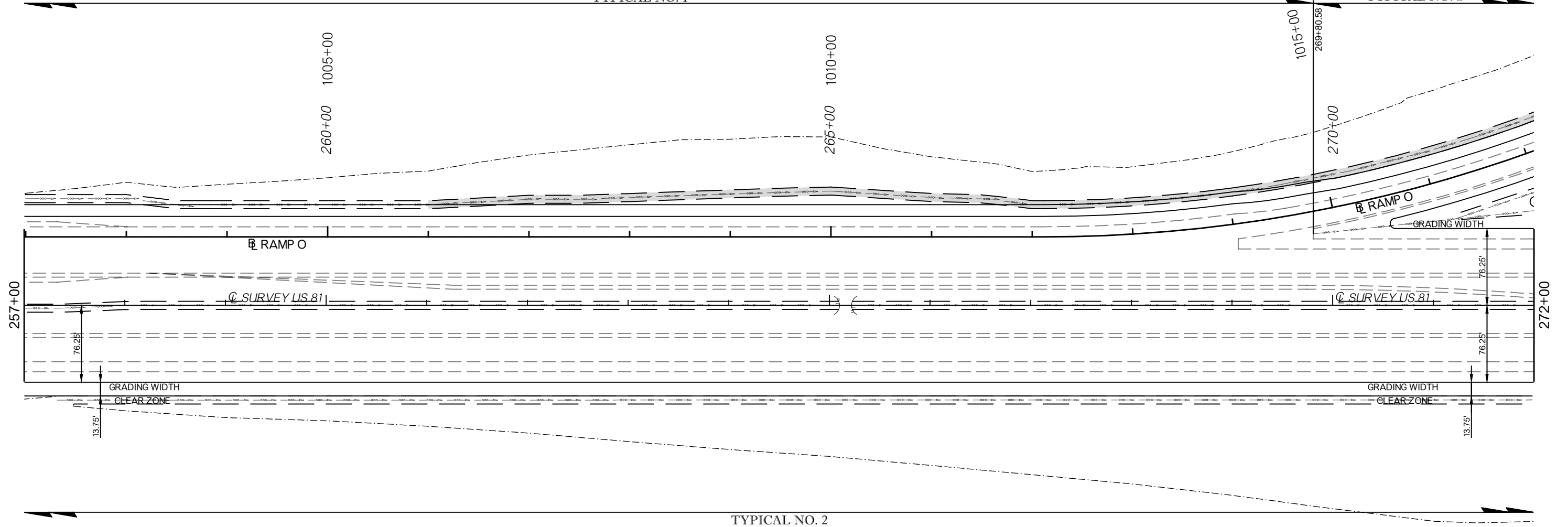
PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R259

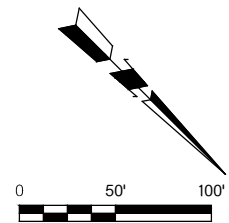
US 81 REALIGNMENT
GRADY COUNTY

TYPICAL NO. 1

TYPICAL NO. 2



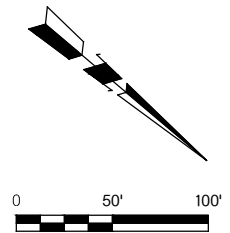
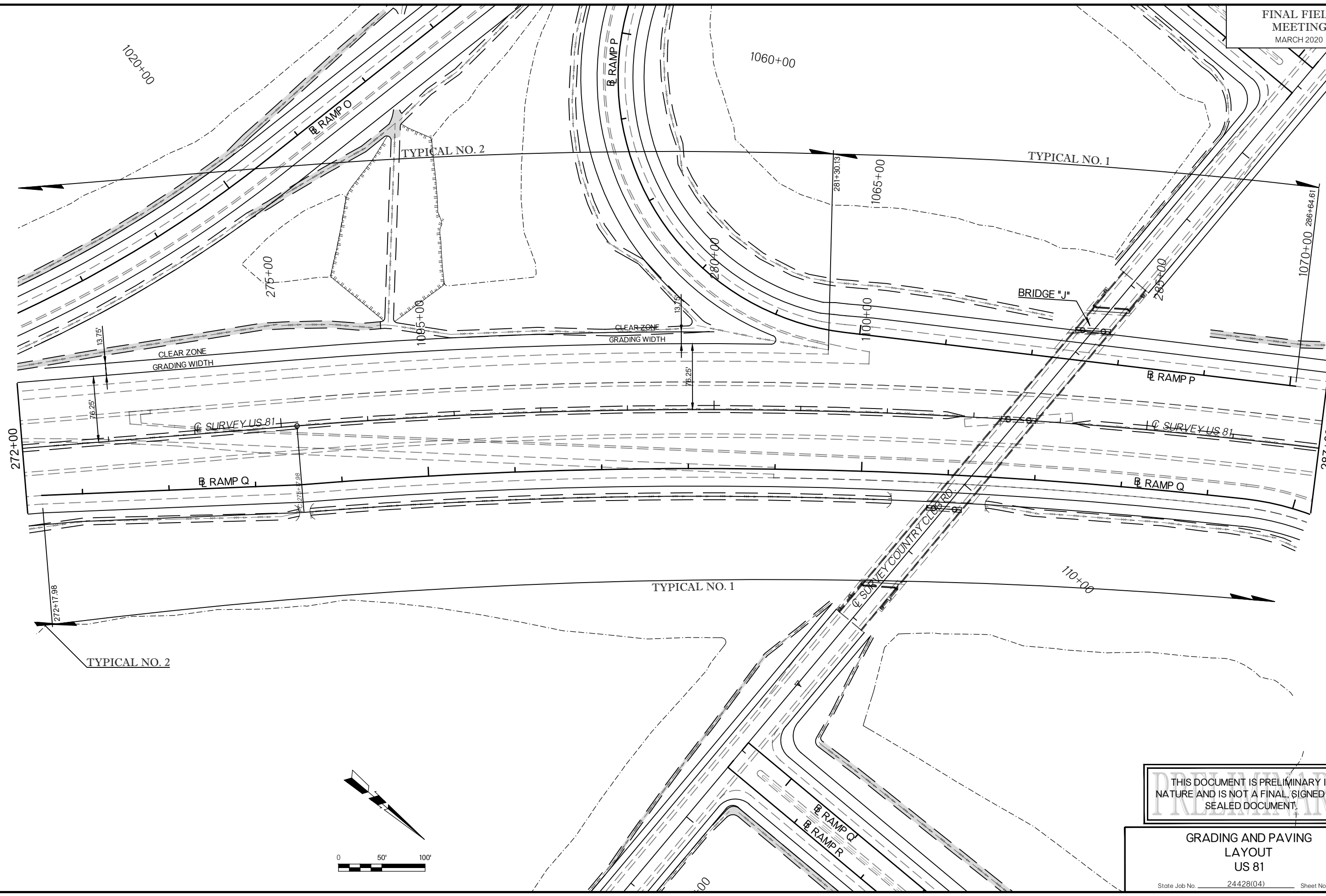
TYPICAL NO. 2



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R260

US 81 REALIGNMENT
GRADY COUNTY

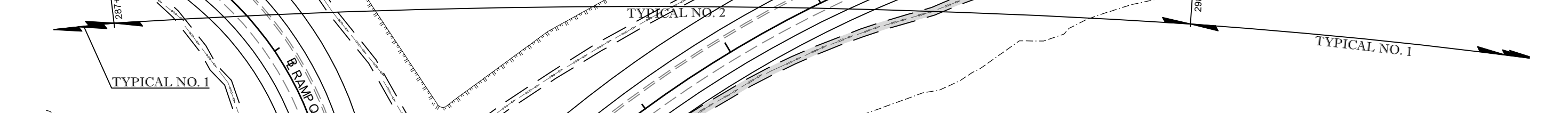
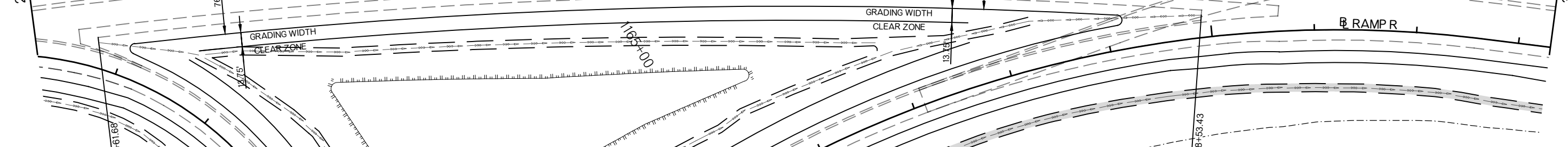
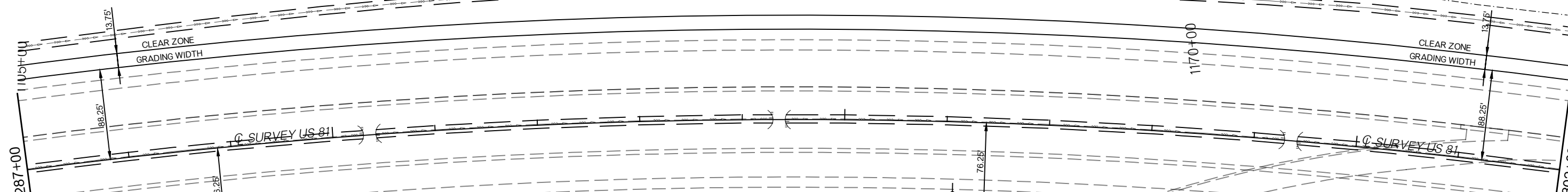


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

**GRADING AND PAVING LAYOUT
US 81**

TYPICAL NO. 9

290+00 295+00 300+00



TYPICAL NO. 2

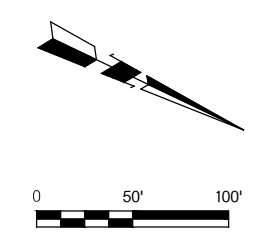
TYPICAL NO. 1

TYPICAL NO. 1

B RAMP

B RAMP

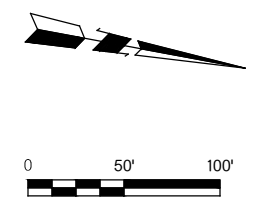
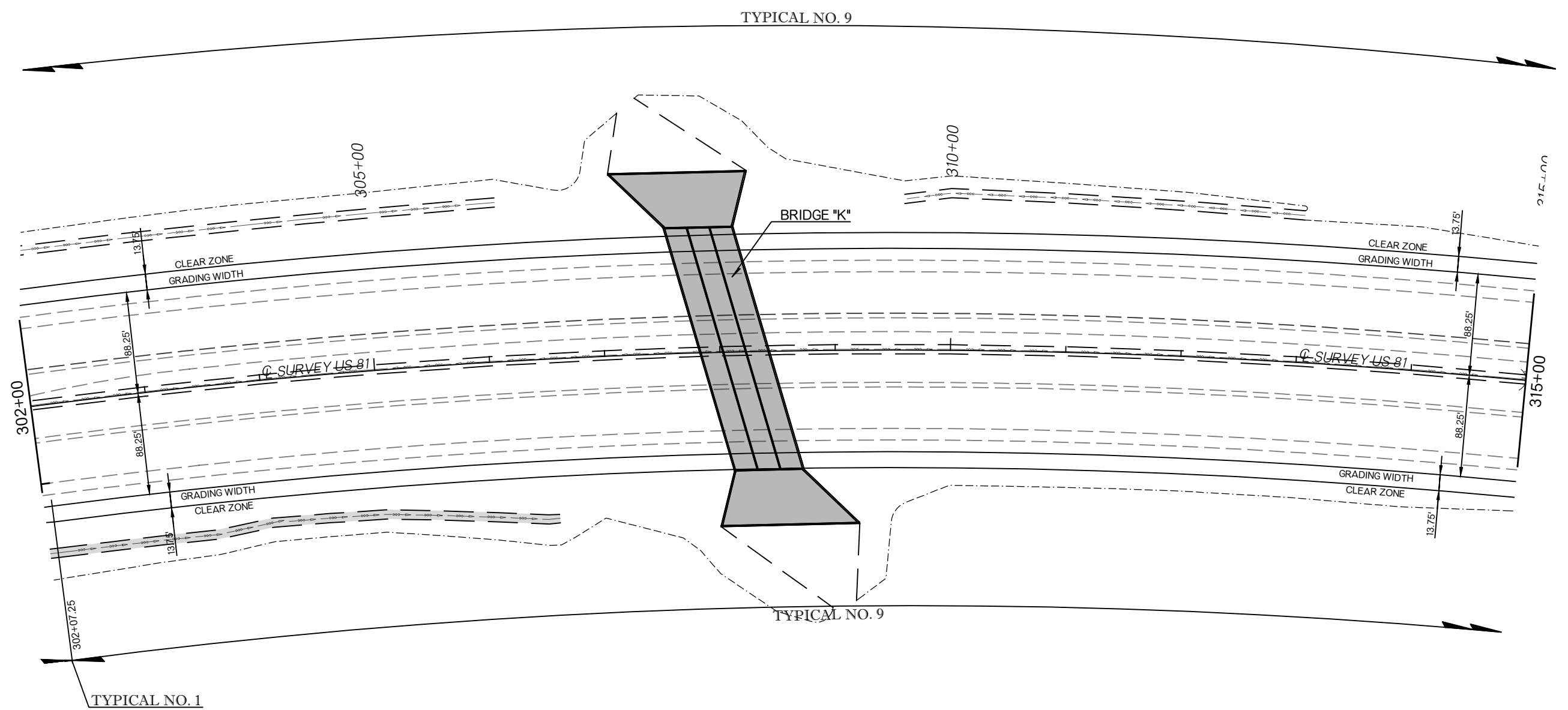
B RAMP



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R262

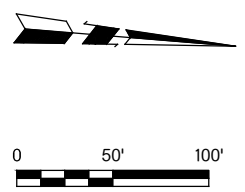
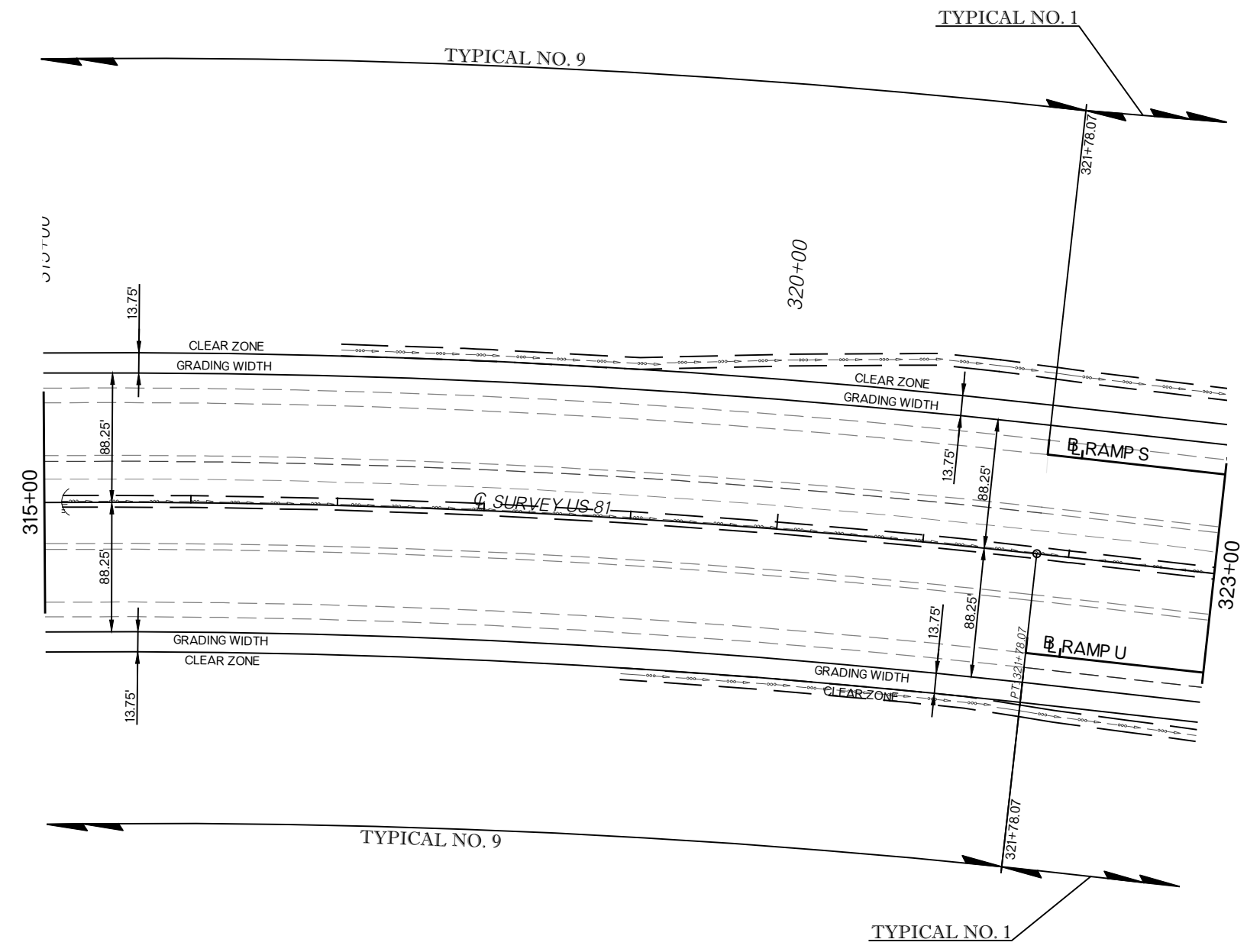
GRADY COUNTY
US 81 REALIGNMENT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R263

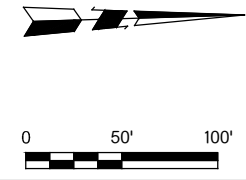
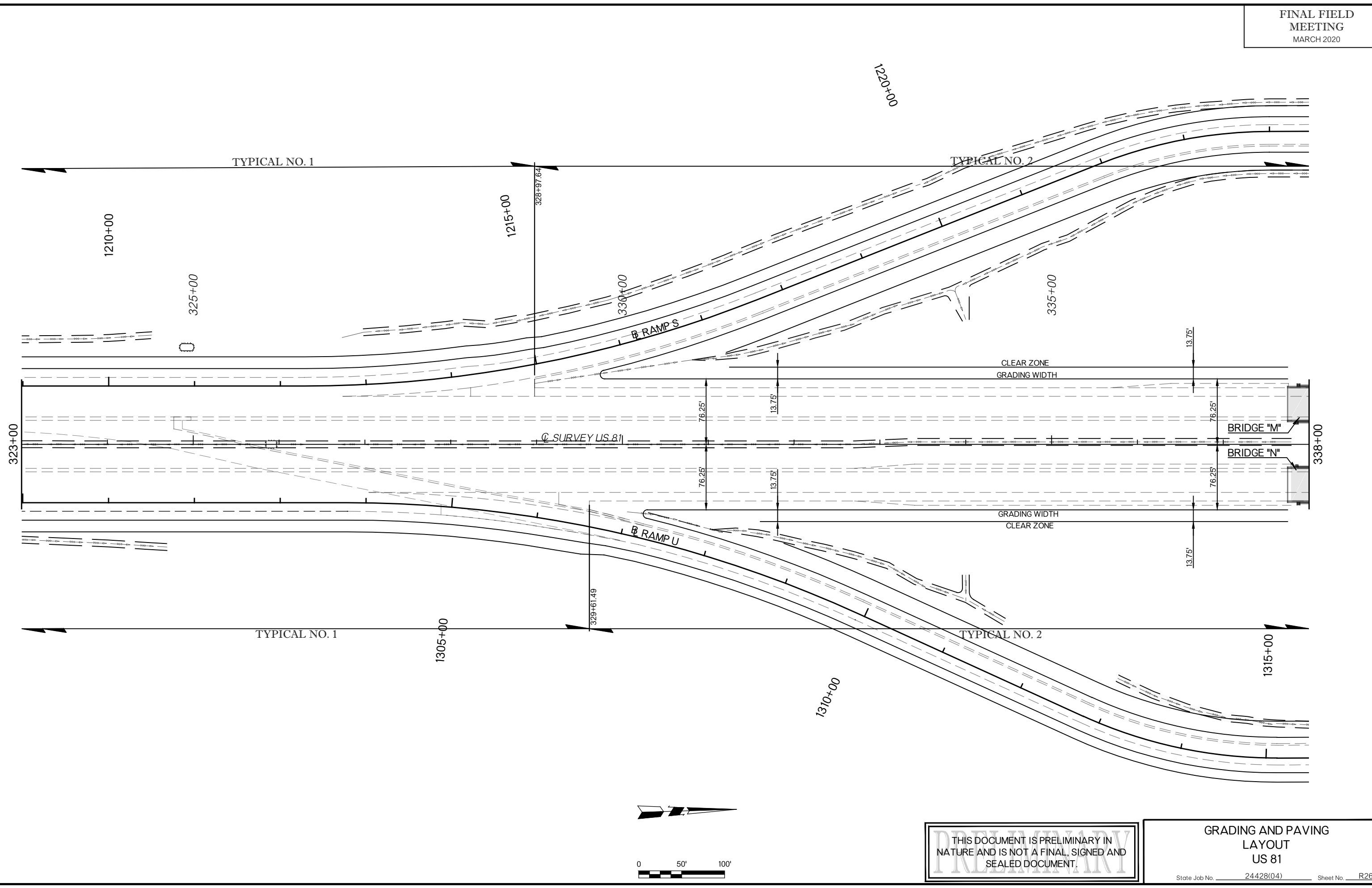
US 81 REALIGNMENT
GRADY COUNTY



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

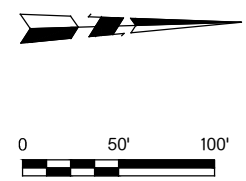
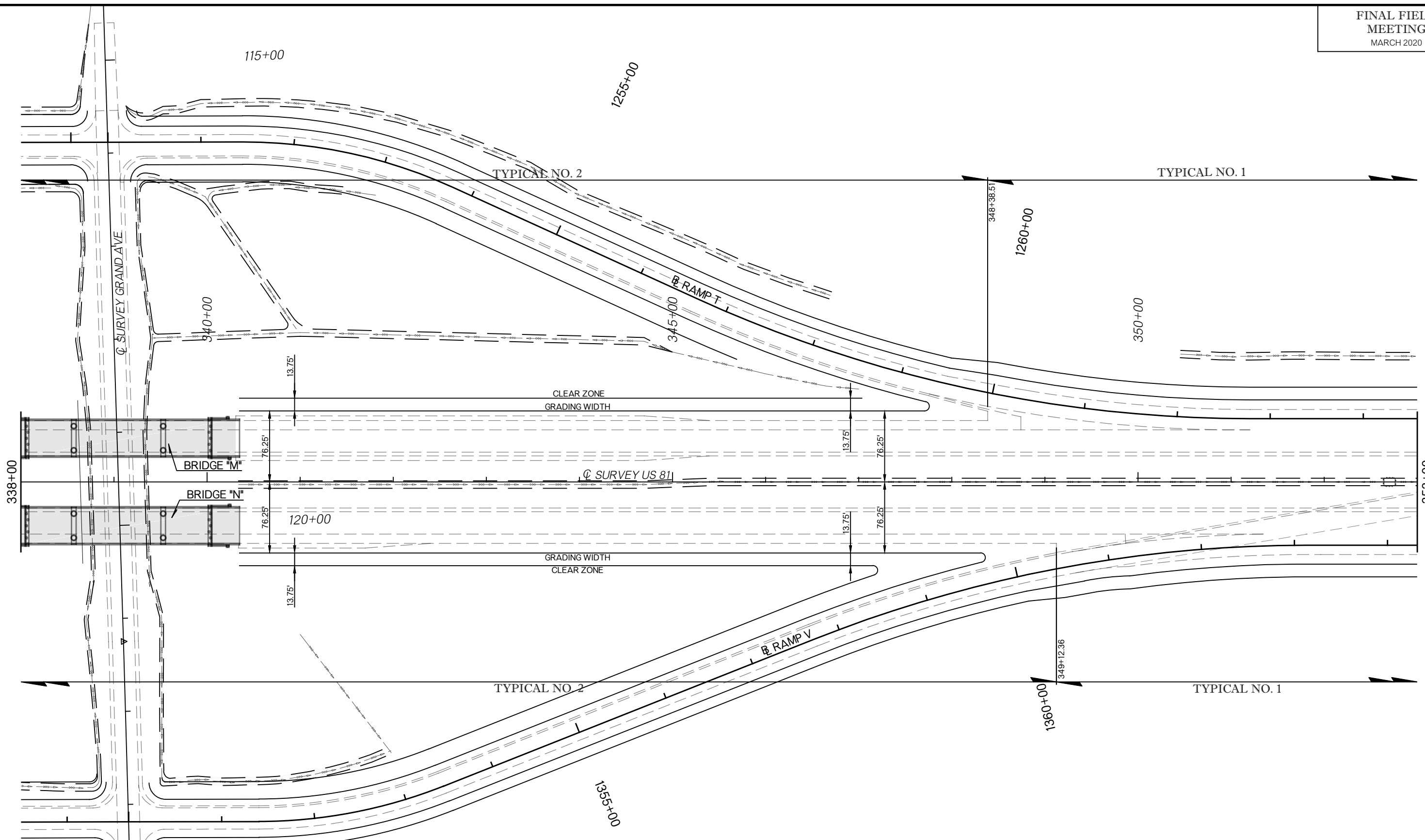
GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R264

US 81 REALIGNMENT
GRADY COUNTY



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

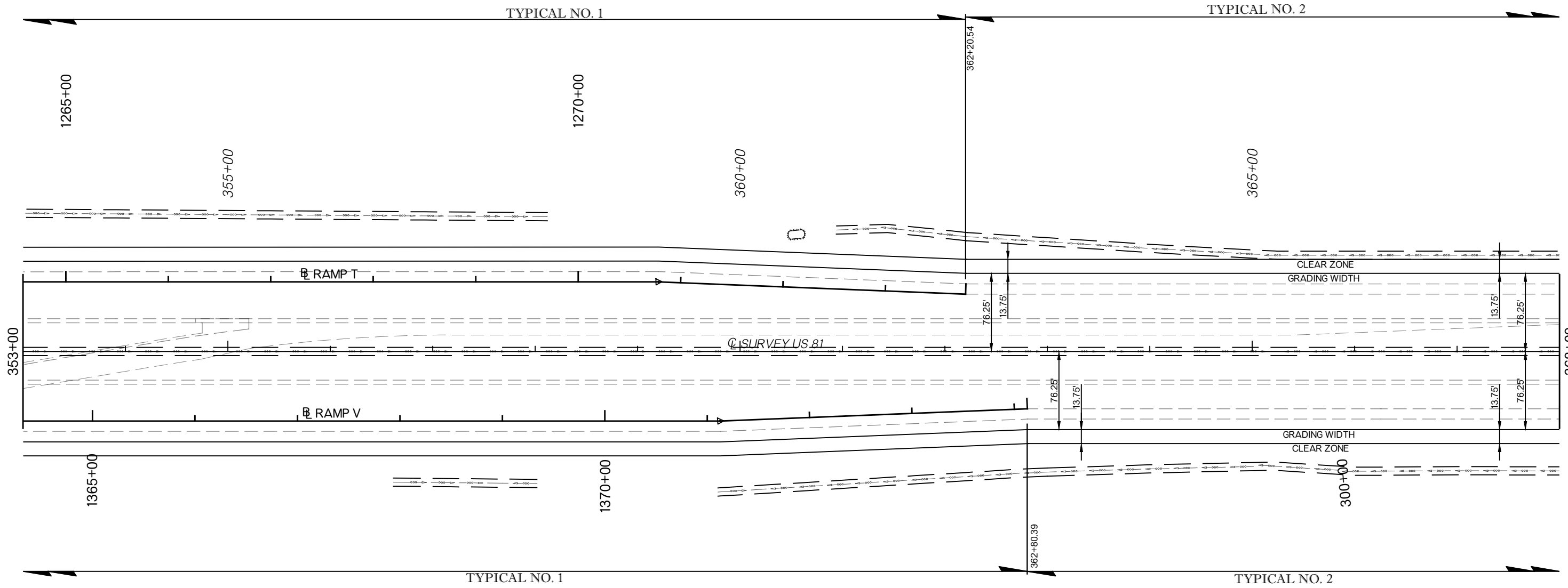
GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R265



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R266

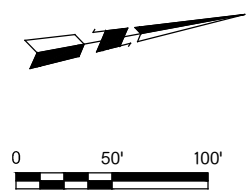
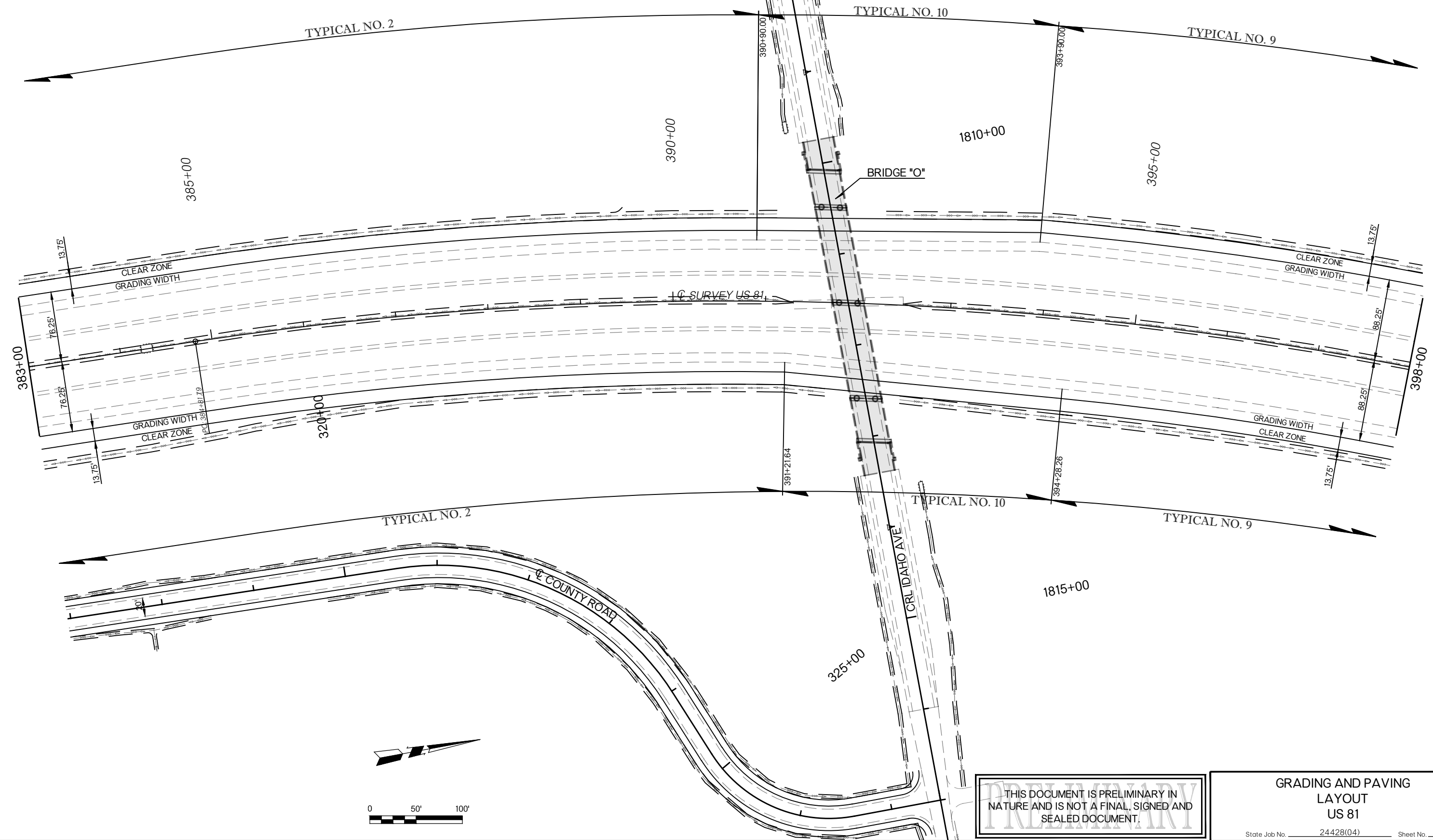
GRADY COUNTY
US 81 REALIGNMENT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R267

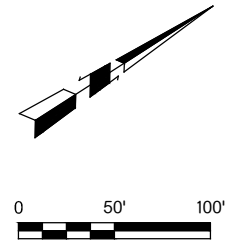
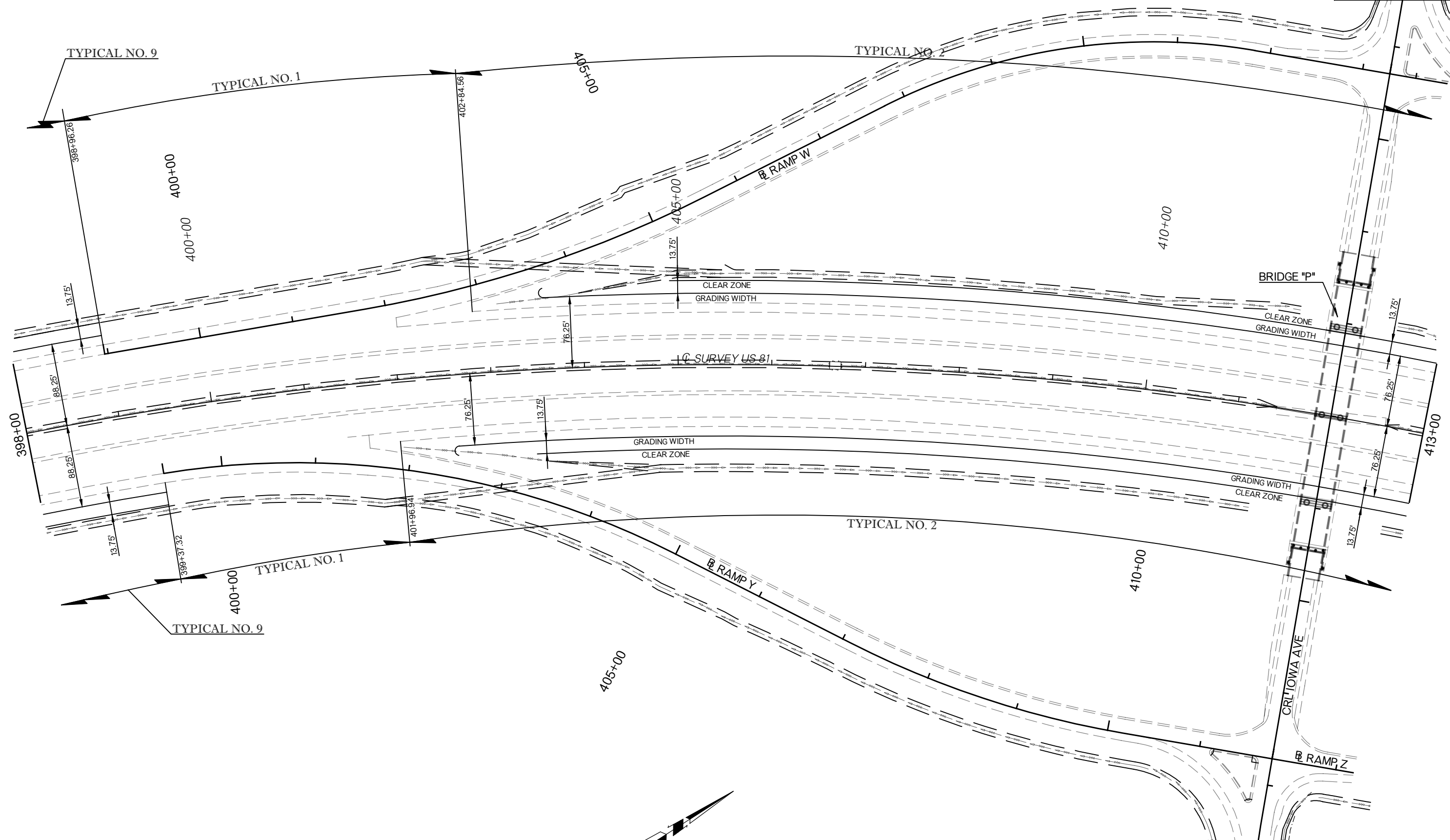
US 81 REALIGNMENT
GRADY COUNTY



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R269

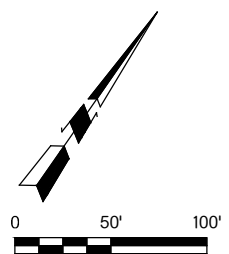
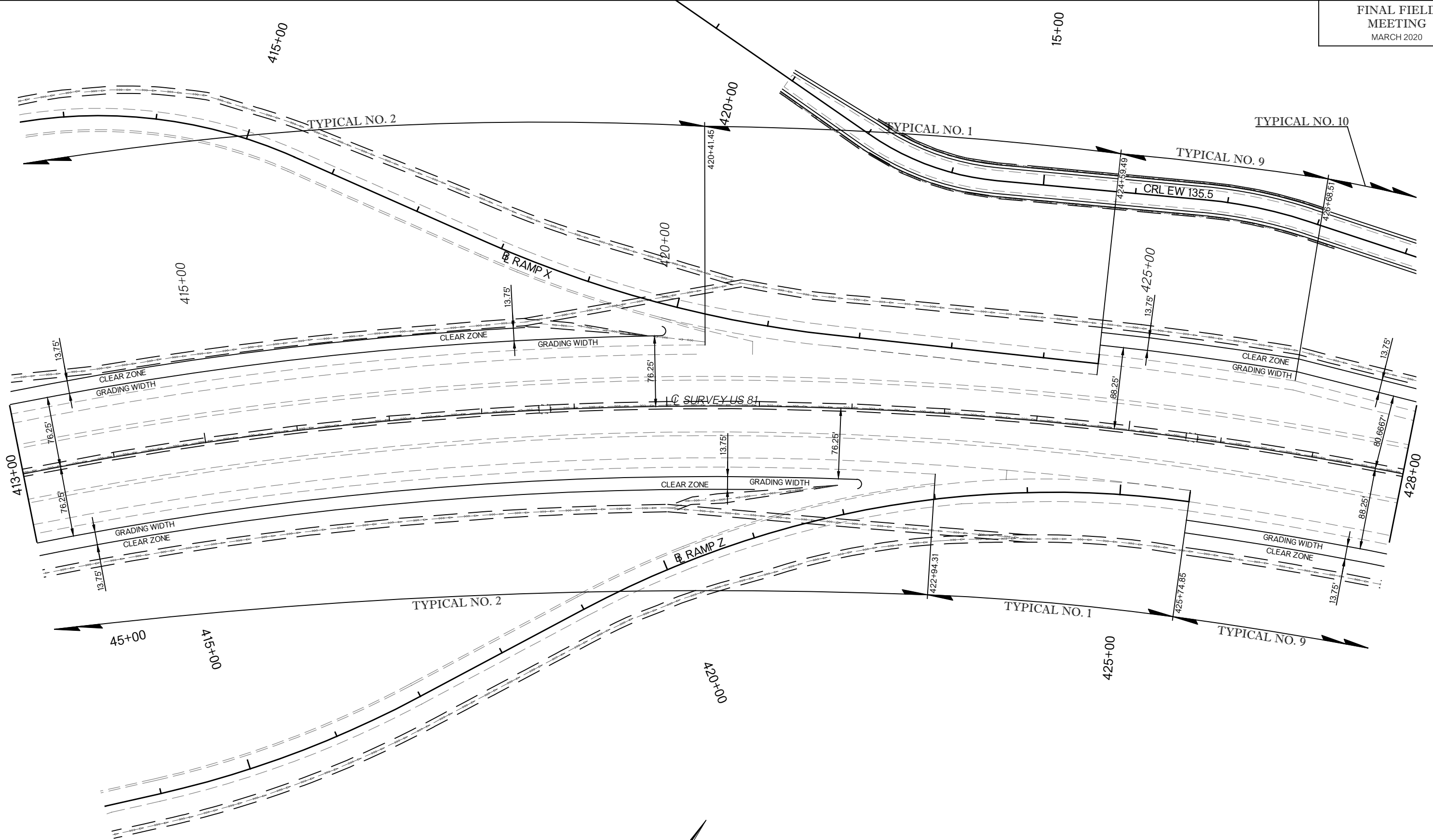
US 81 REALIGNMENT
GRADY COUNTY



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R270

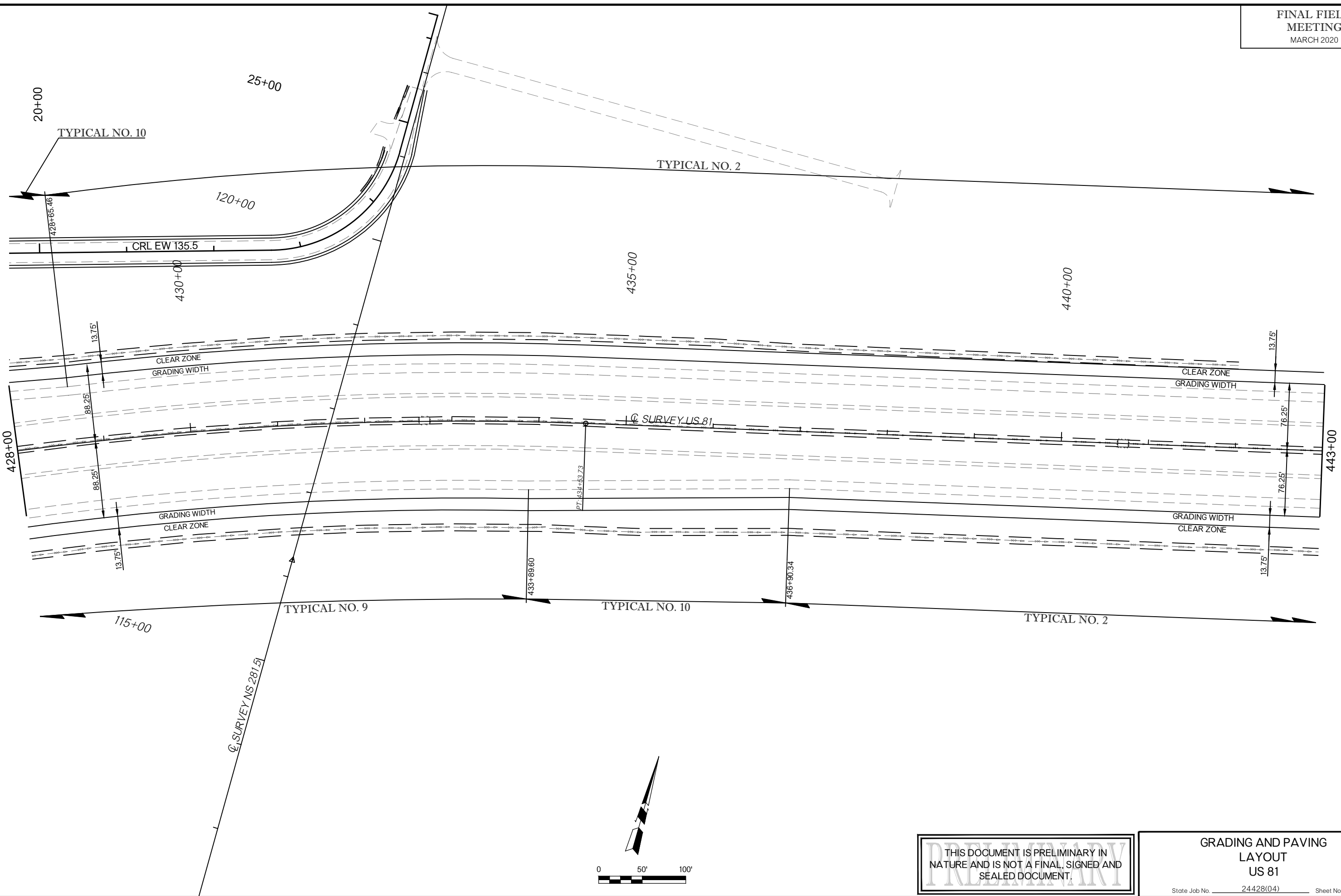
GRADY COUNTY
US 81 REALIGNMENT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R271

GRADY COUNTY
US 81 REALIGNMENT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R272

GRADY COUNTY
US 81 REALIGNMENT

TYPICAL NO. 2

TYPICAL NO. 10

TYPICAL NO. 9

445+00

450+00

455+00

443+00

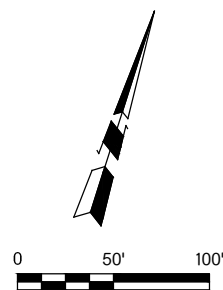
458+00

PG: 449.5517

L & Q SURVEY US 81

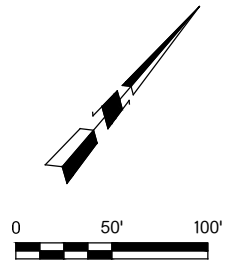
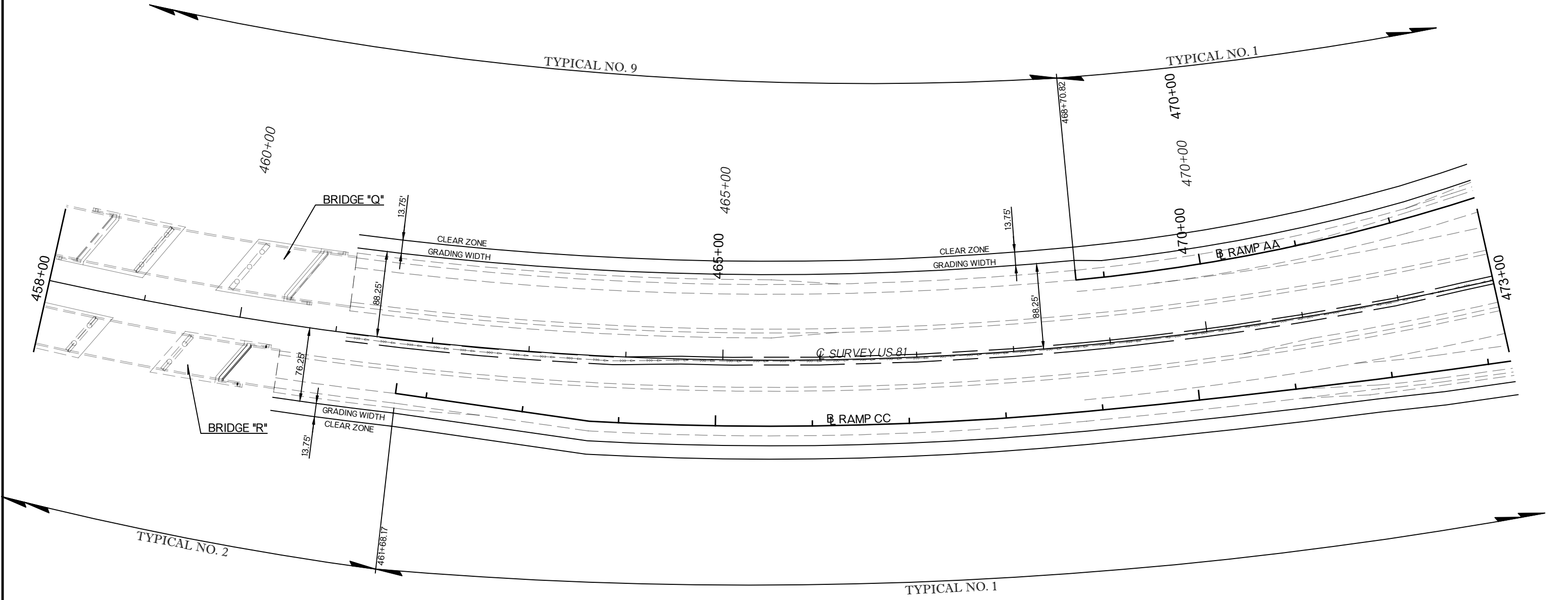
BRIDGE "R"

TYPICAL NO. 2



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

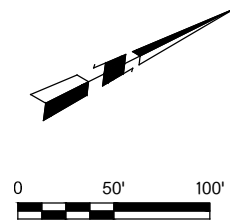
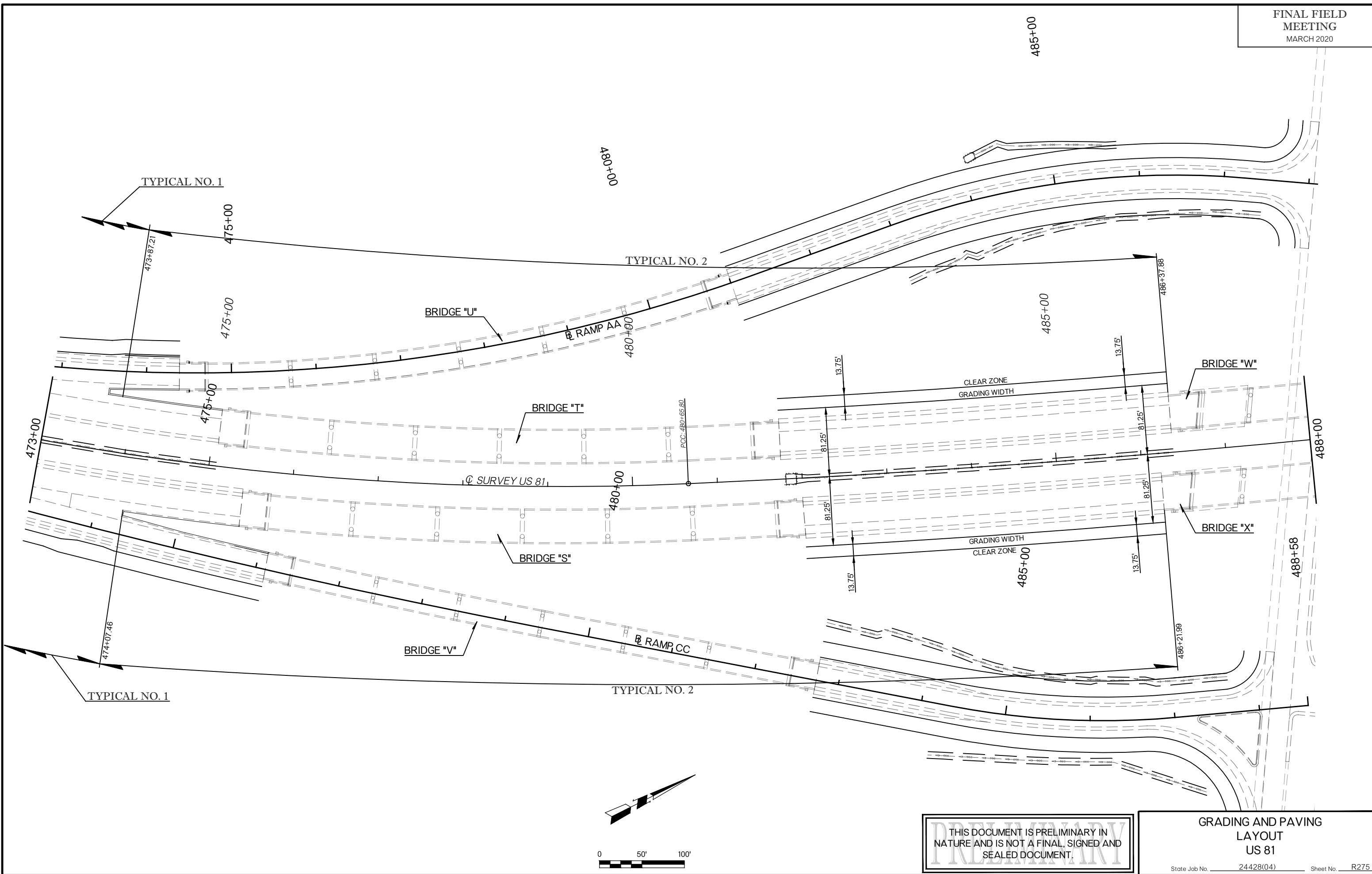
GRADING AND PAVING LAYOUT
US 81



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81
State Job No. 24428(04) Sheet No. R274

GRADY COUNTY
US 81 REALIGNMENT

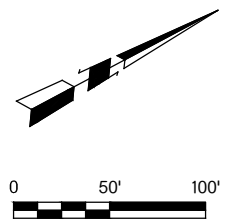


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

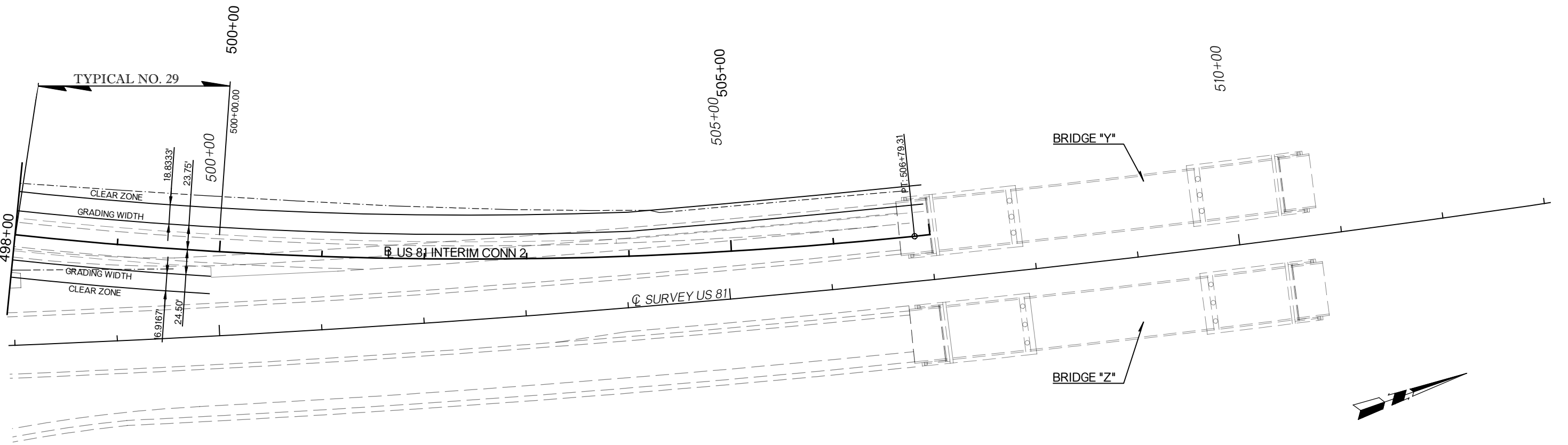
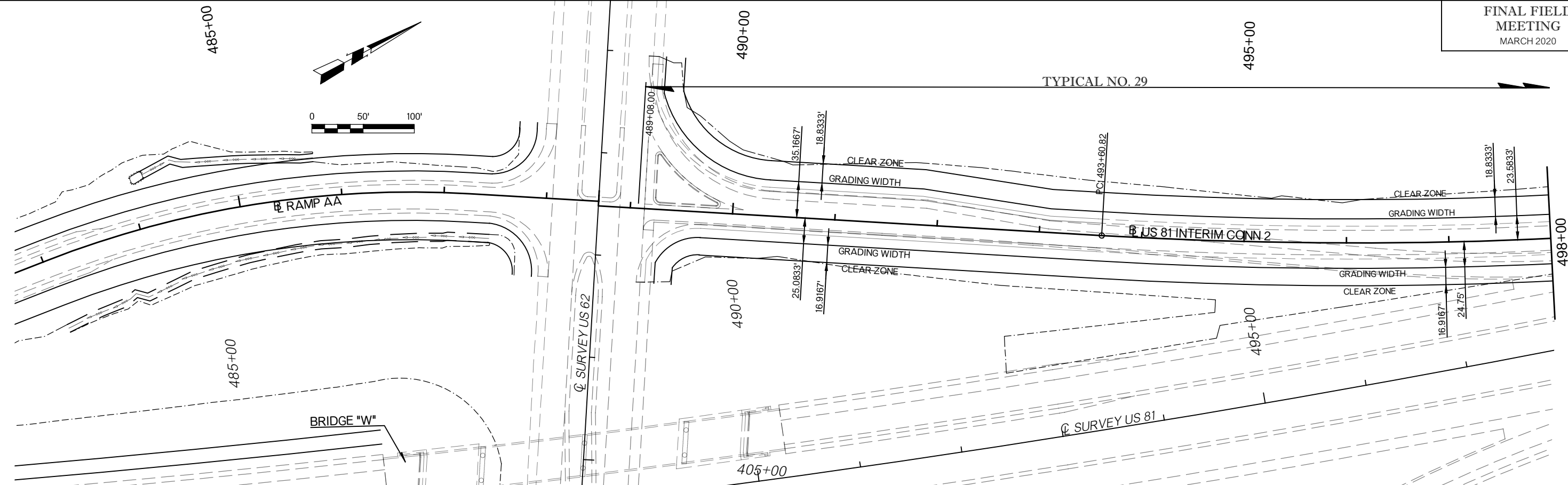
**GRADING AND PAVING LAYOUT
US 81**

State Job No. 24428(04) Sheet No. R275

US 81 REALIGNMENT
GRADY COUNTY



TYPICAL NO. 29



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
US 81 INTERIM CONNECTION 2

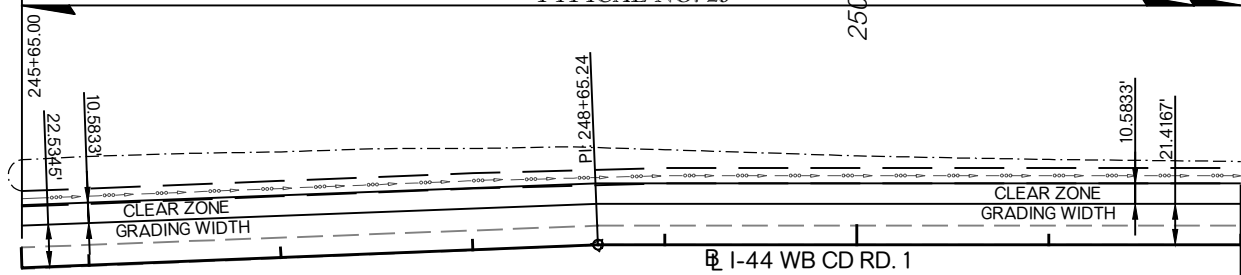
240+00

245+00

250+00

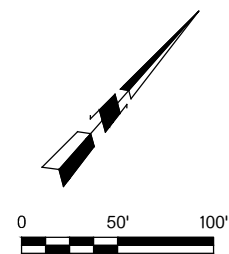
252+00

TYPICAL NO. 23



☉ SURVEY I-44 (HE BAILEY TPK)

TYPICAL NO. 23



255+00

260+00

265+

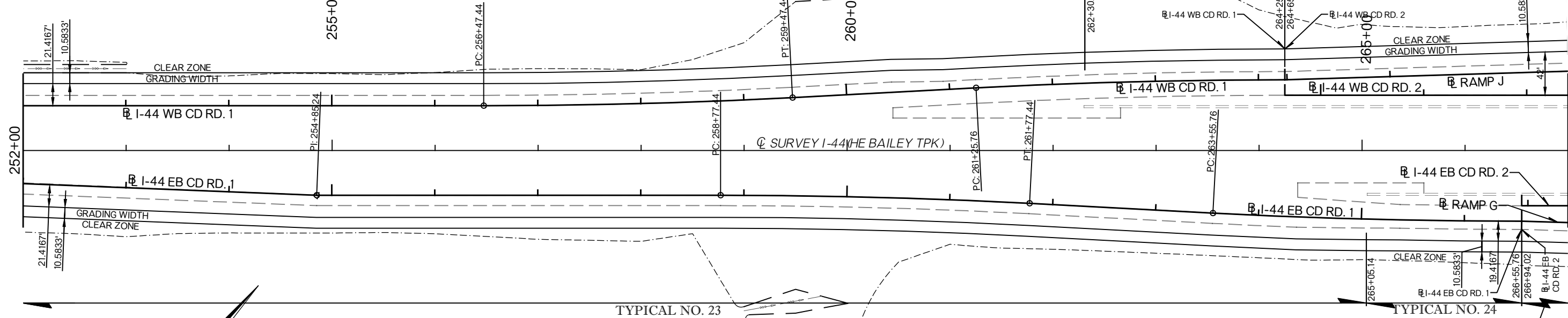
2180+00

267+00

TYPICAL NO. 23

TYPICAL NO. 24

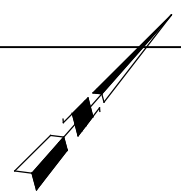
TYPICAL NO. 25



TYPICAL NO. 23

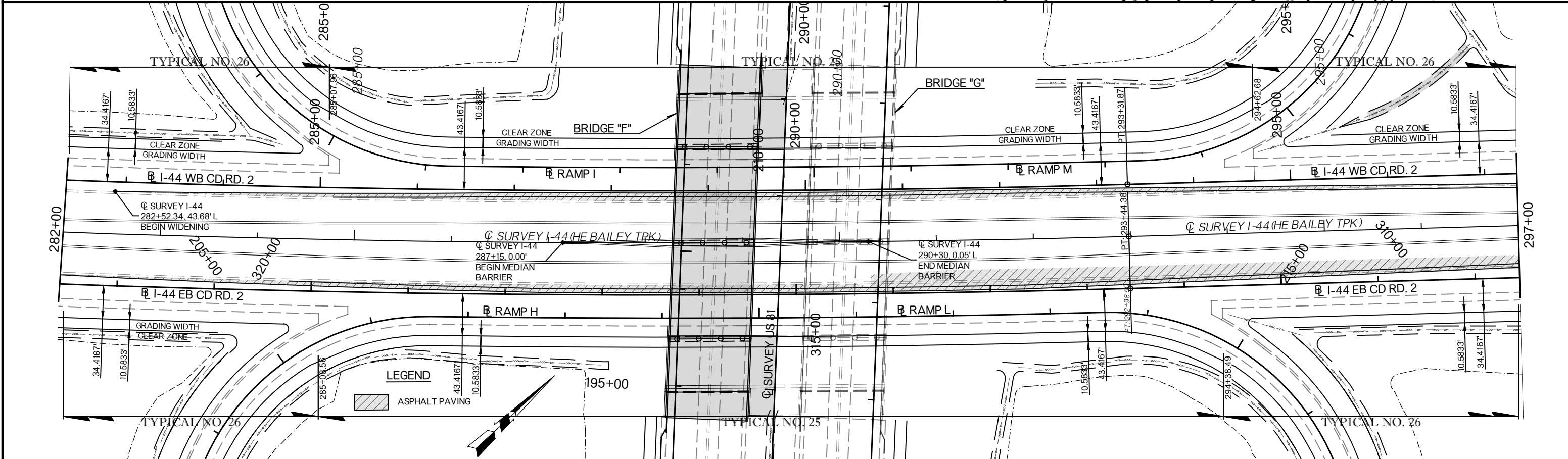
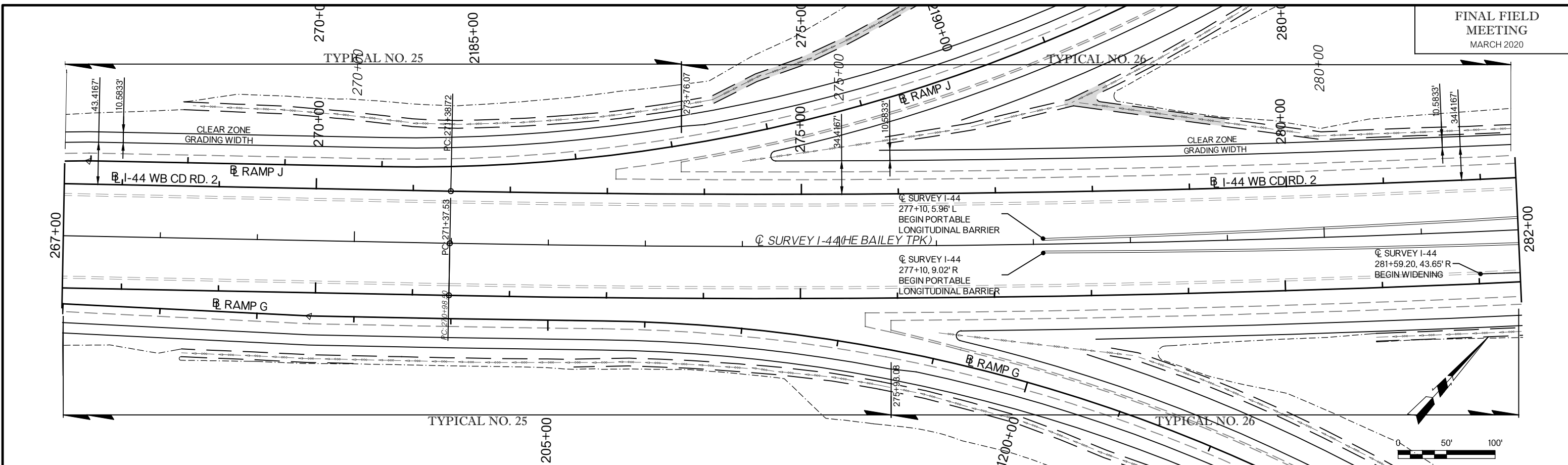
TYPICAL NO. 24

TYPICAL NO. 25

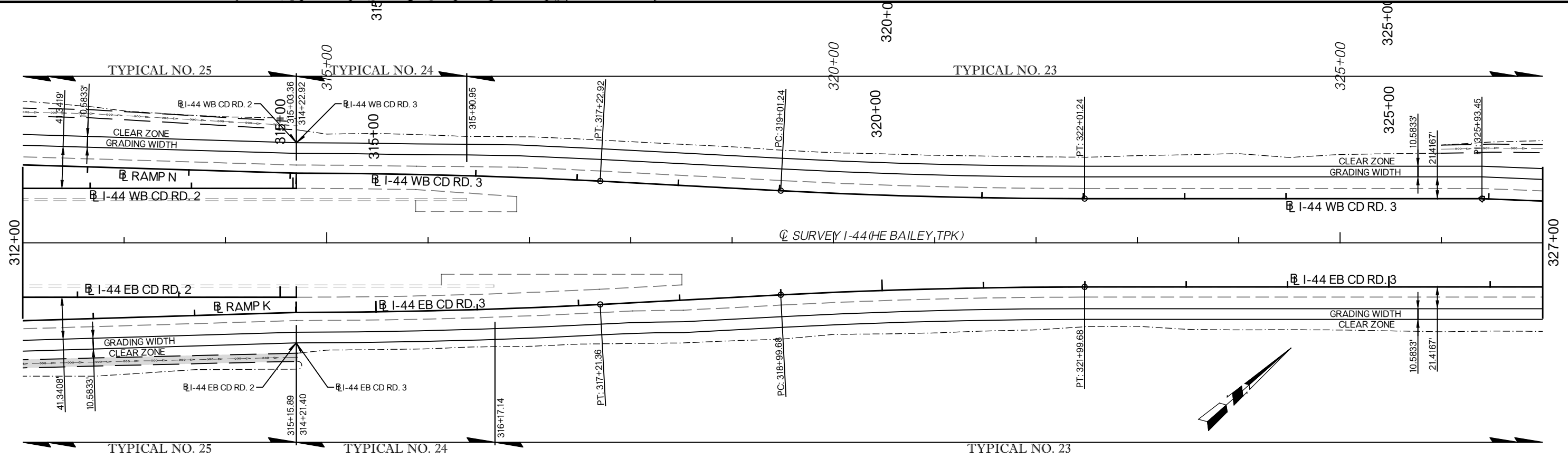
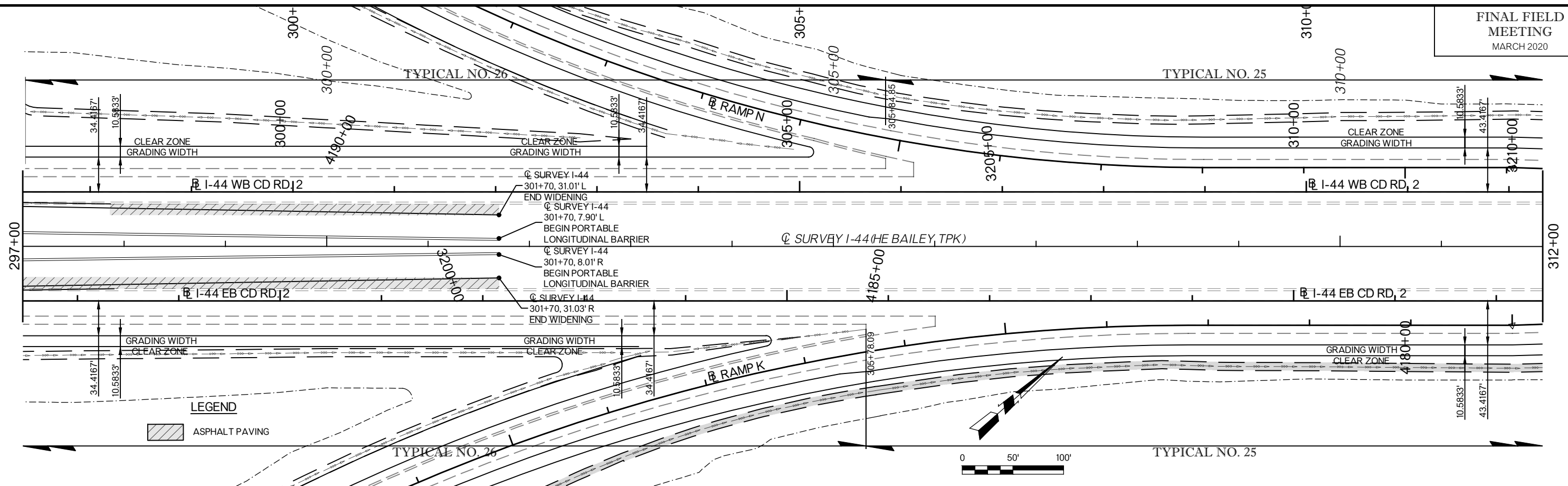


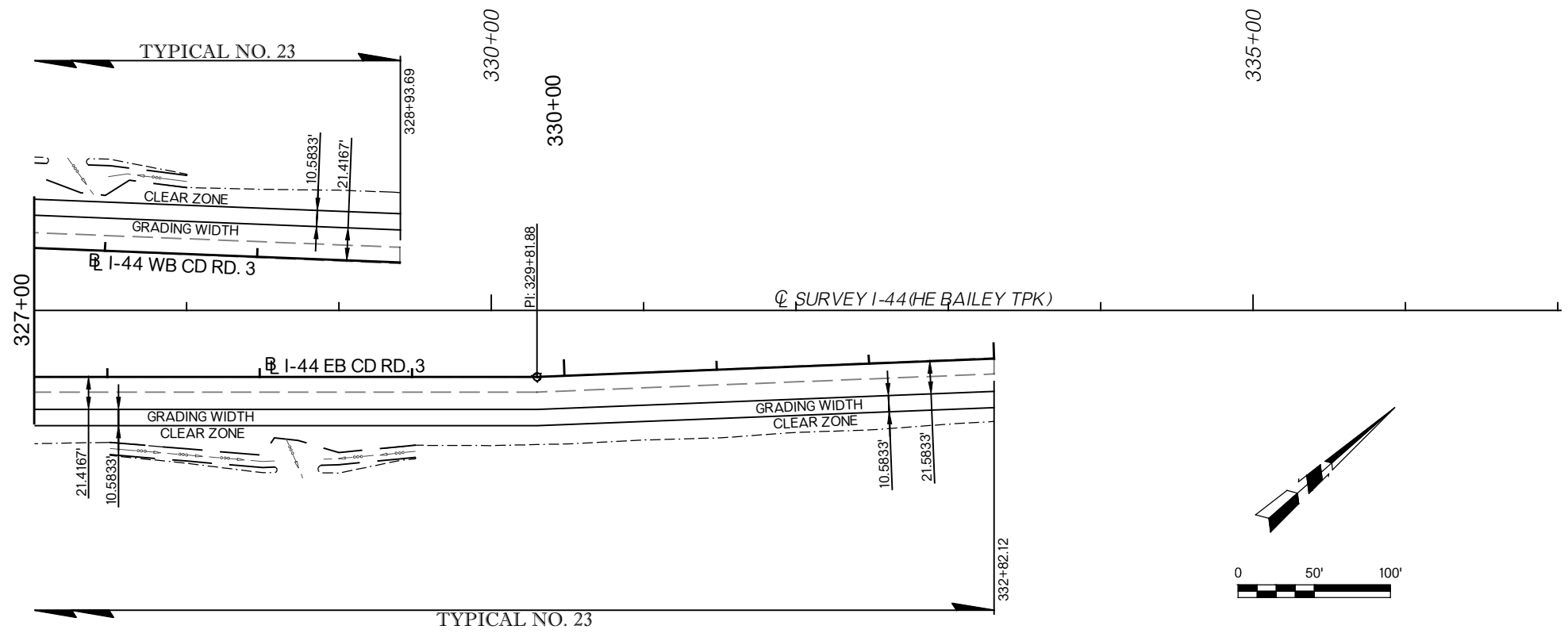
GRADING AND PAVING LAYOUT
I-44

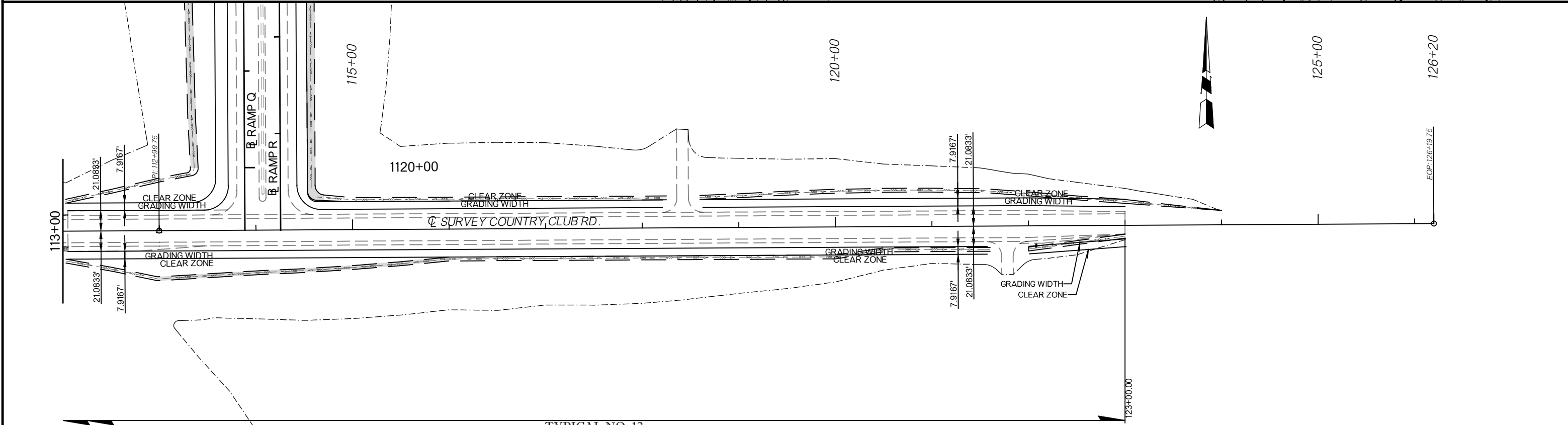
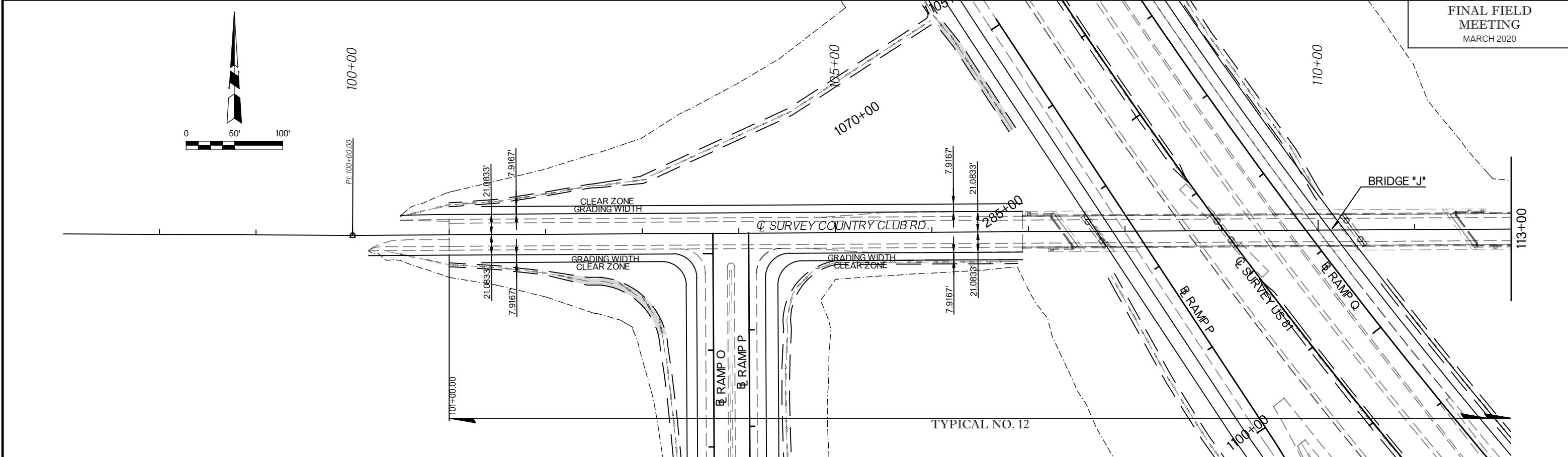
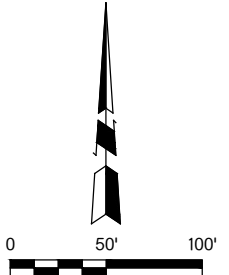
State Job No. 24428(04) Sheet No. R277



GRADING AND PAVING LAYOUT
I-44

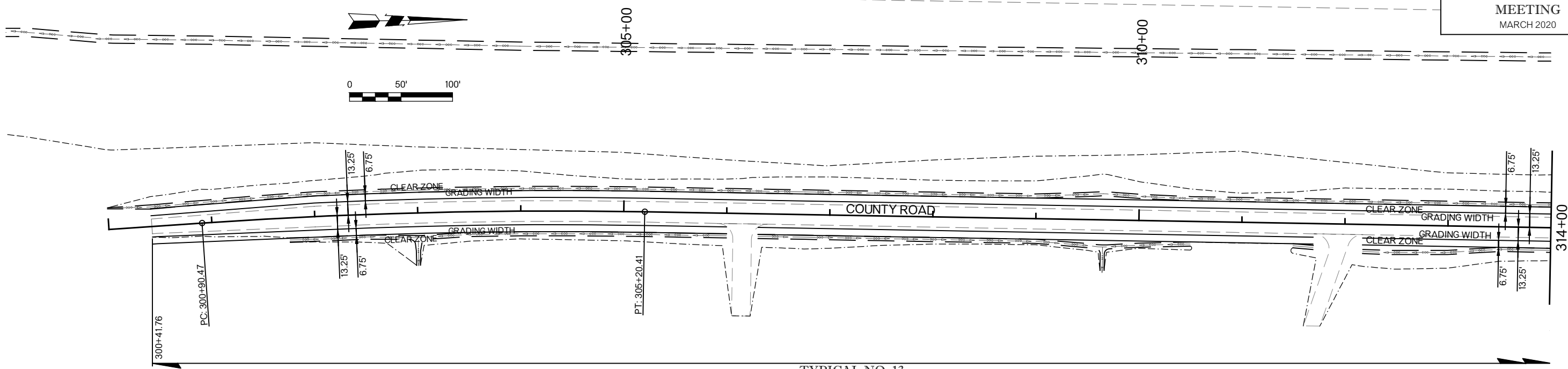
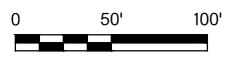




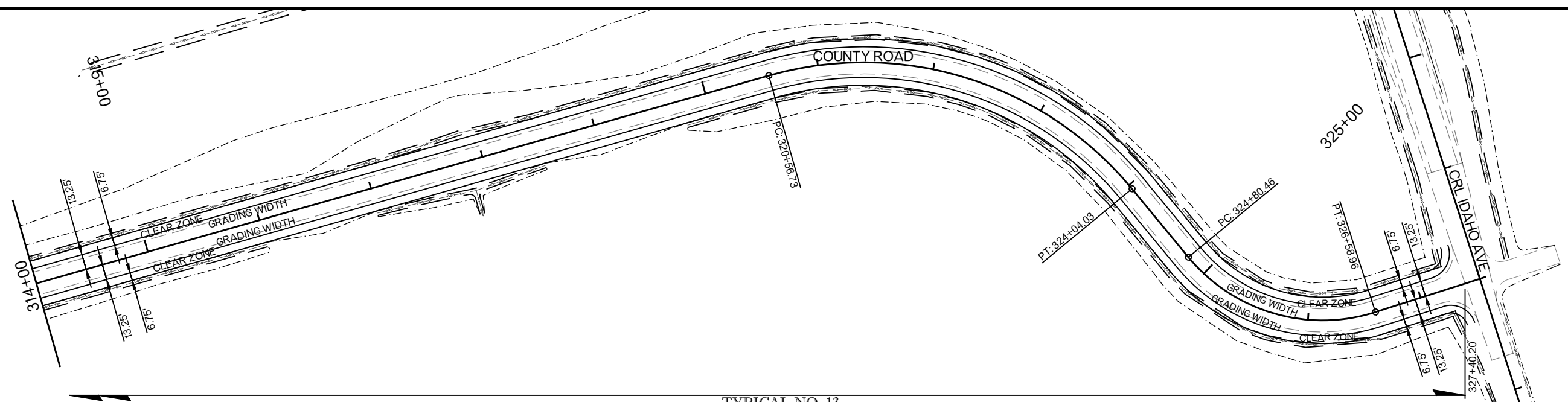


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SURFACING CONSTRUCTION DETAILS
COUNTRY CLUB RD.



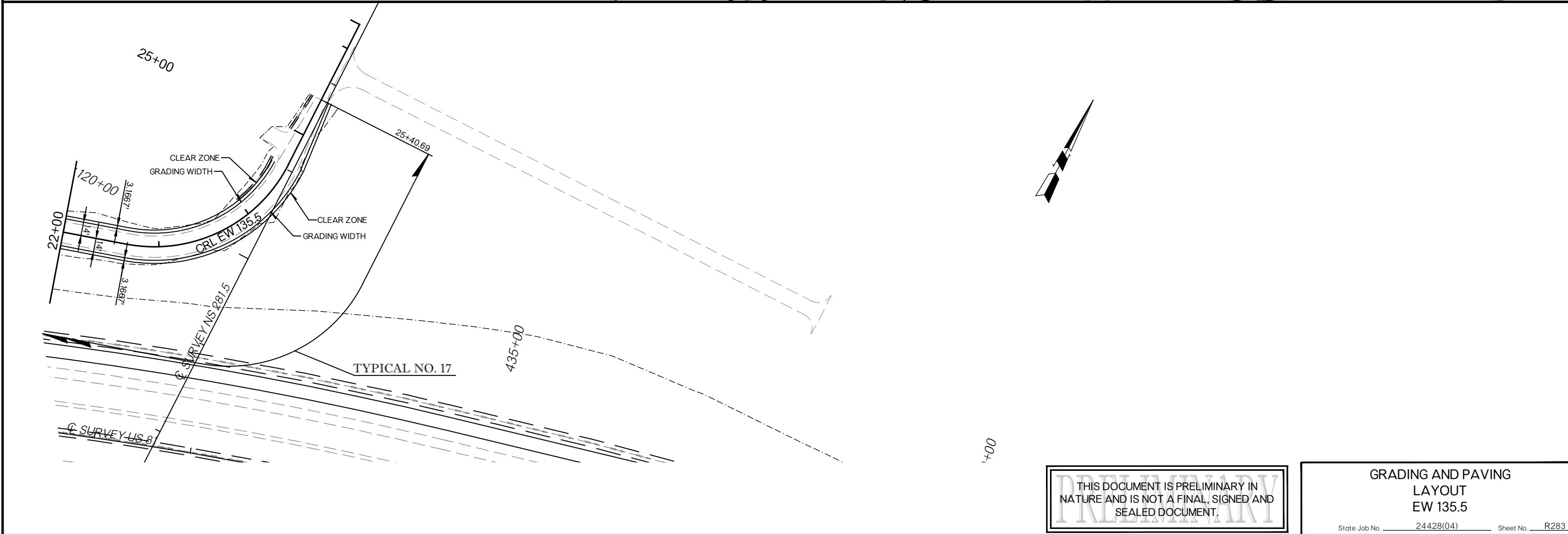
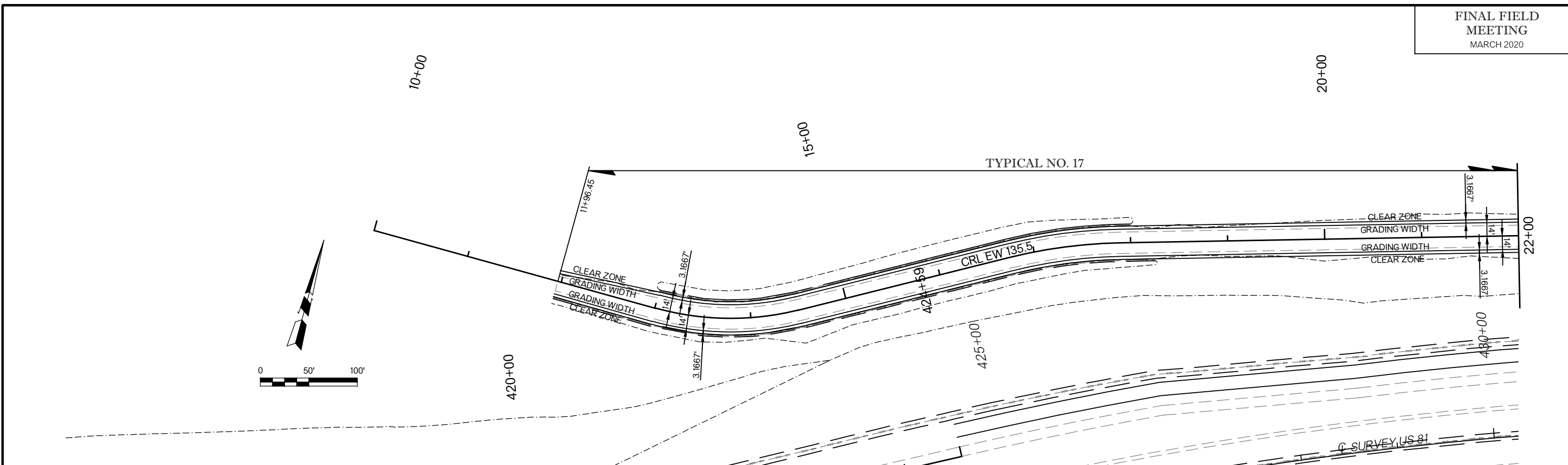
TYPICAL NO. 13



TYPICAL NO. 13

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

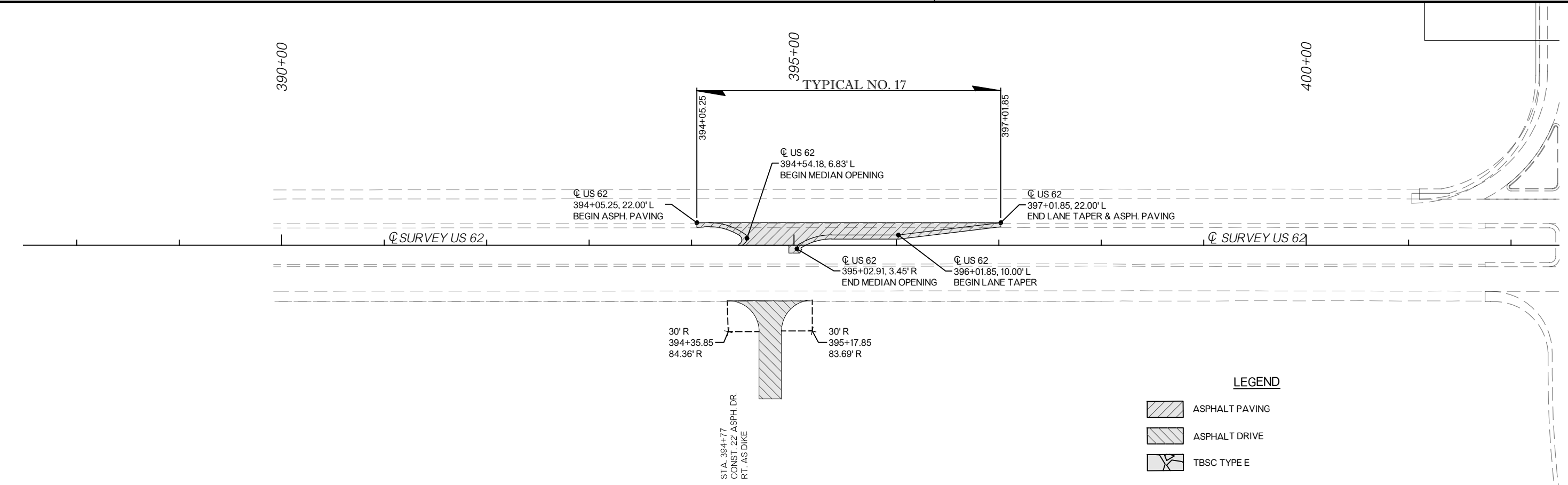
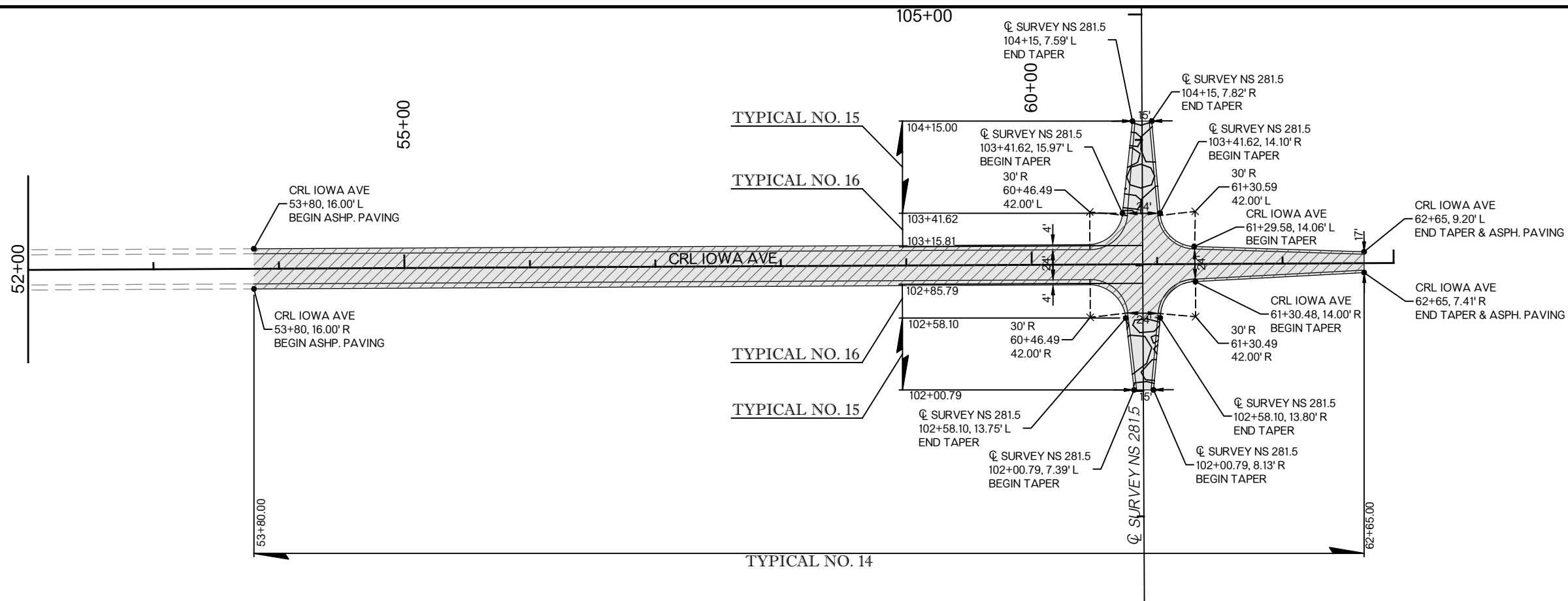
182
GRADING AND PAVING LAYOUT
COUNTY ROAD
State Job No. 24428(04) Sheet No. R282






THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
EW 135.5
State Job No. 24428(04) Sheet No. R283

GRADY COUNTY
US 81 REALIGNMENT

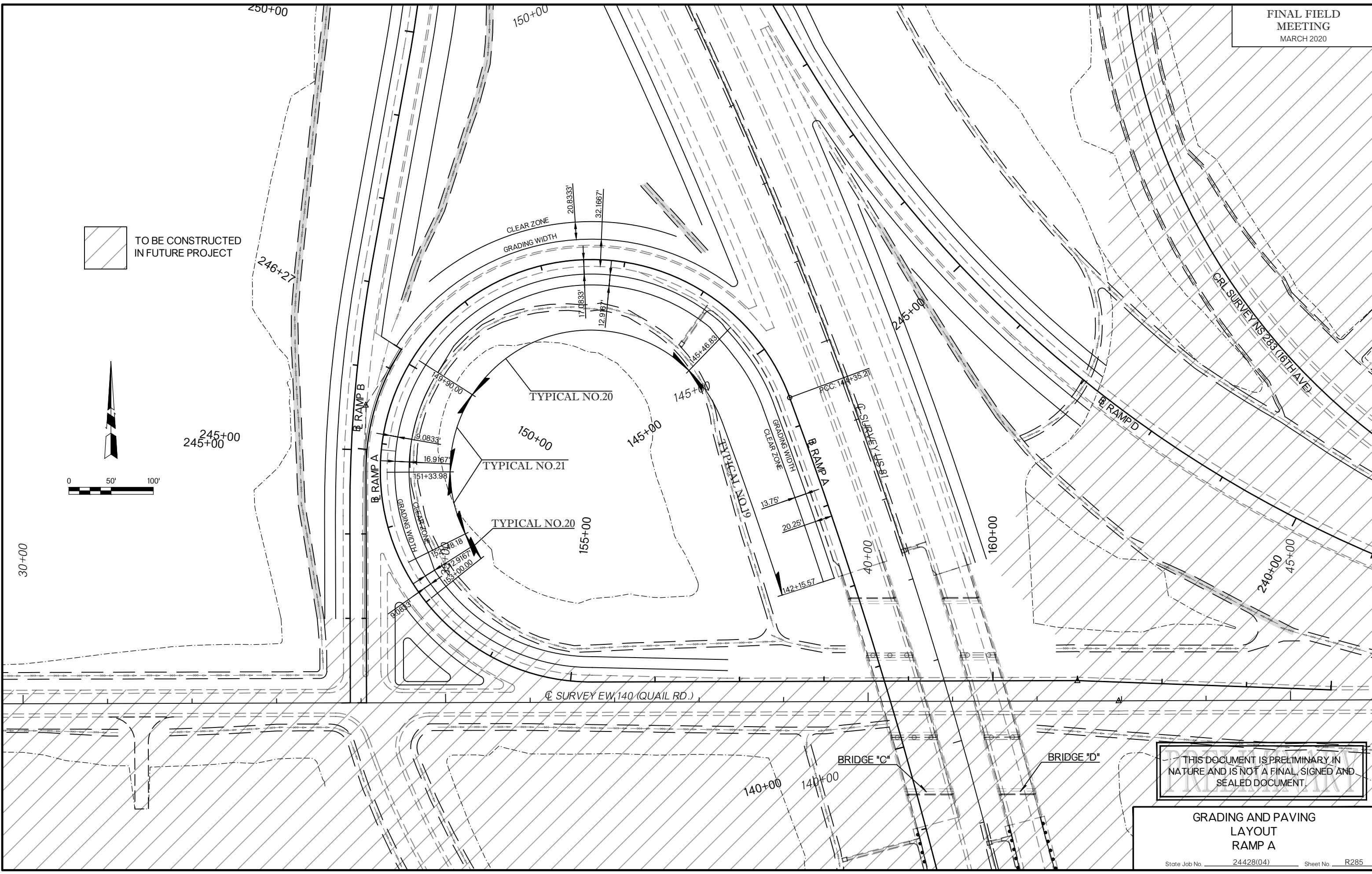
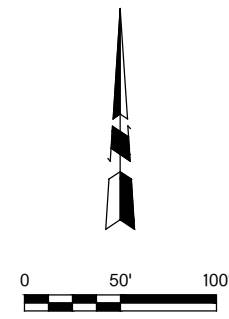


- LEGEND**
-  ASPHALT PAVING
 -  ASPHALT DRIVE
 -  TBS TYPE E

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
IOWA, NS281.5 & US 62

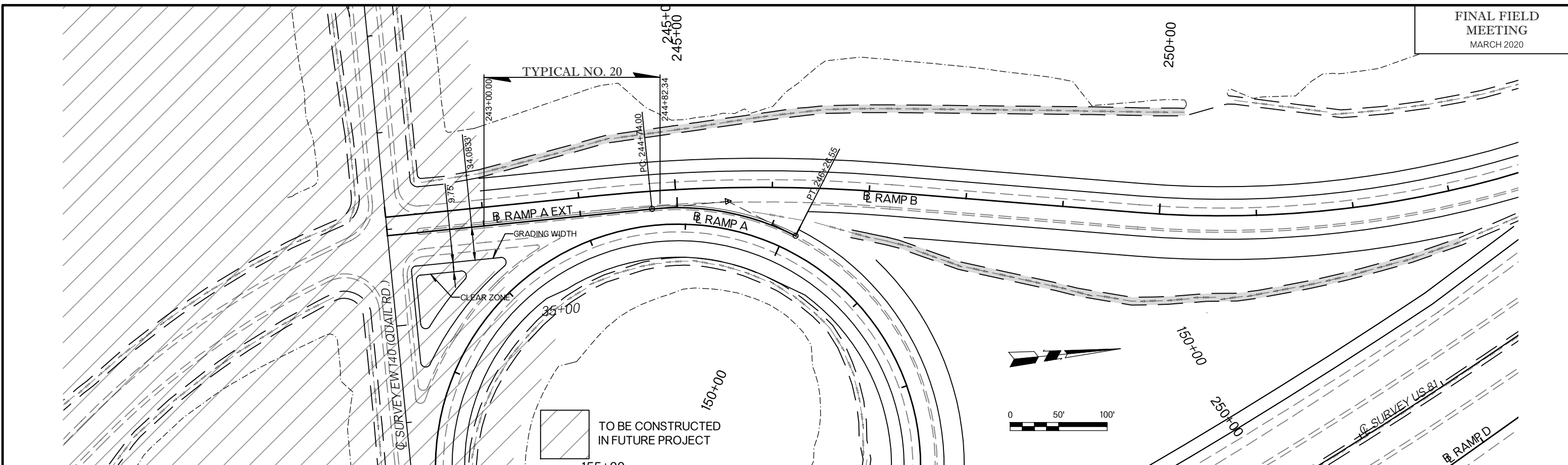
TO BE CONSTRUCTED
IN FUTURE PROJECT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

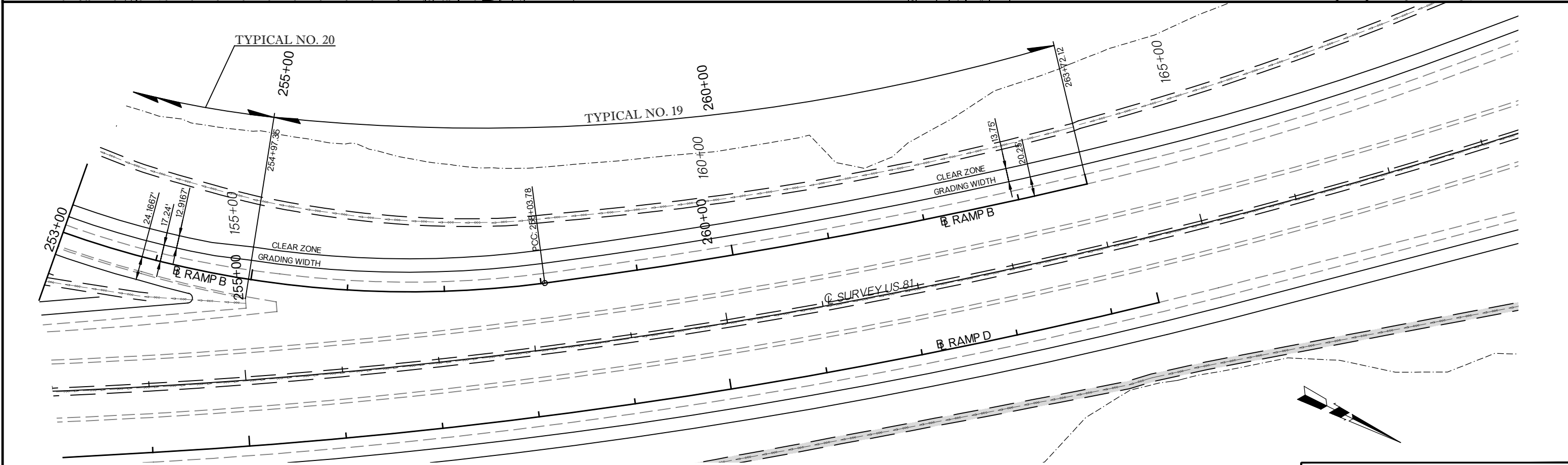
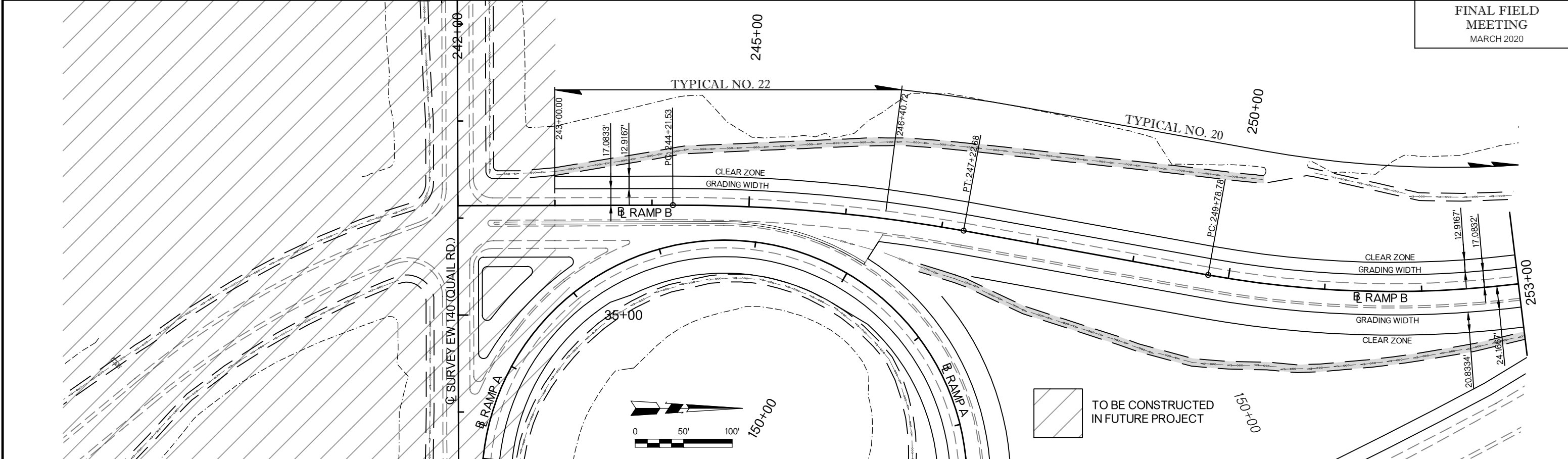
GRADING AND PAVING LAYOUT
RAMP A

US 81 REALIGNMENT
GRADY COUNTY



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
RAMP A EXT
State Job No. 24428(04) Sheet No. R286

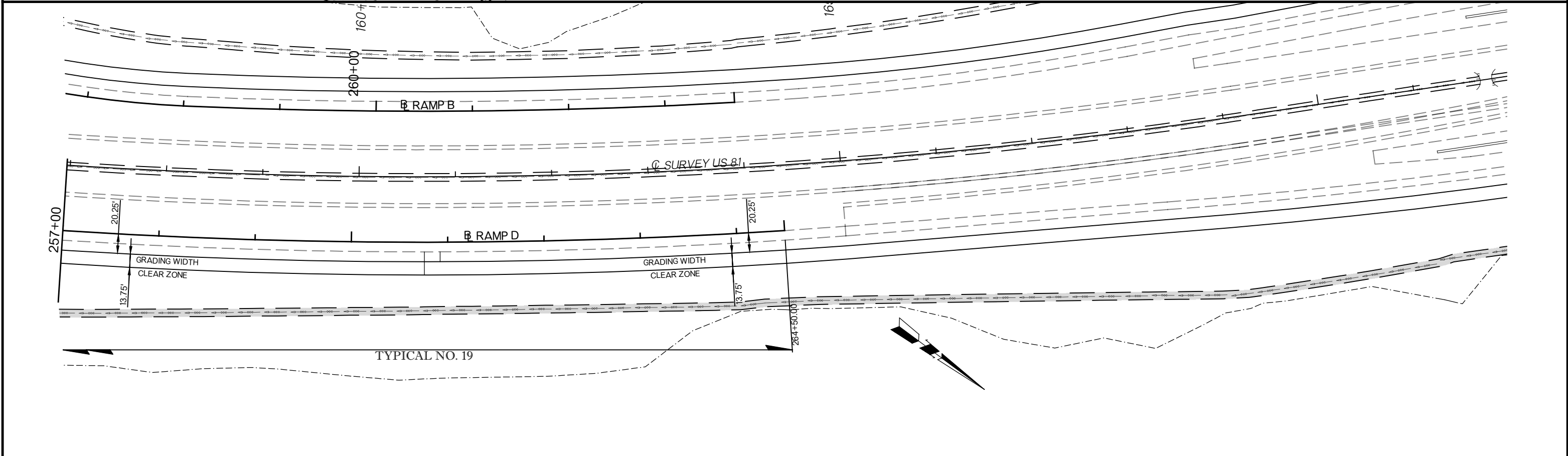
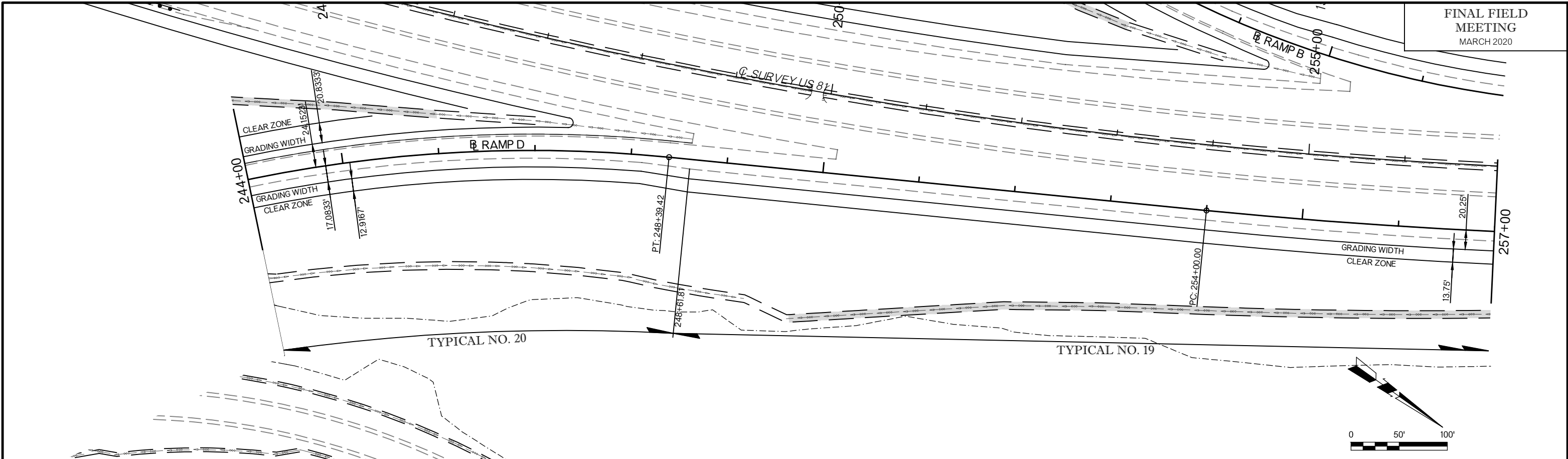


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT RAMP B

State Job No. 24428(04) Sheet No. R287

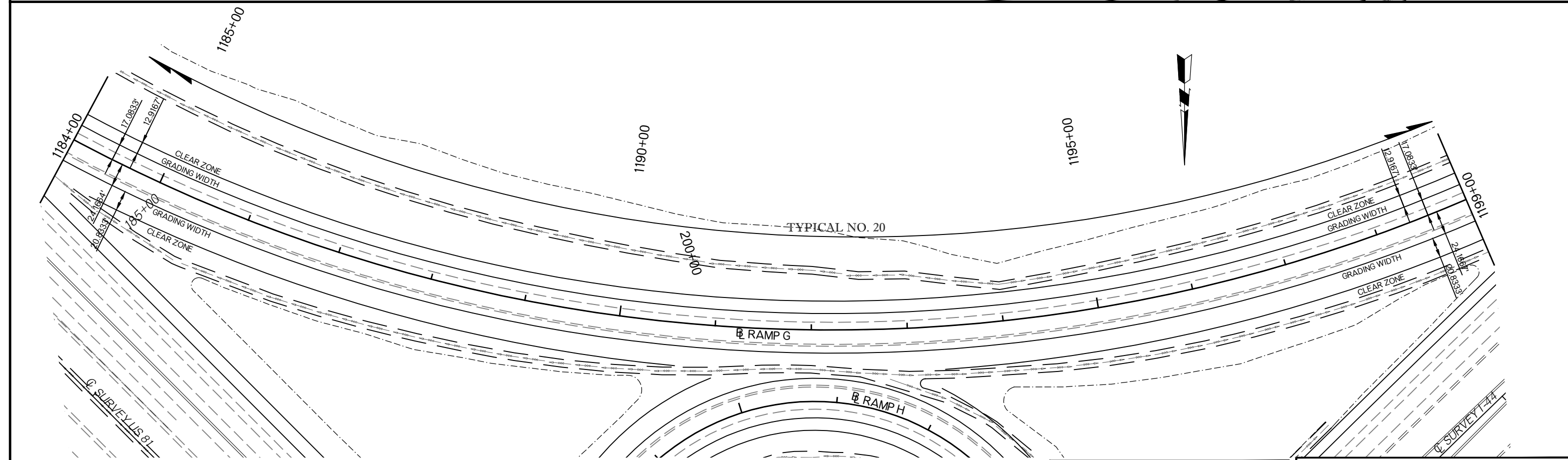
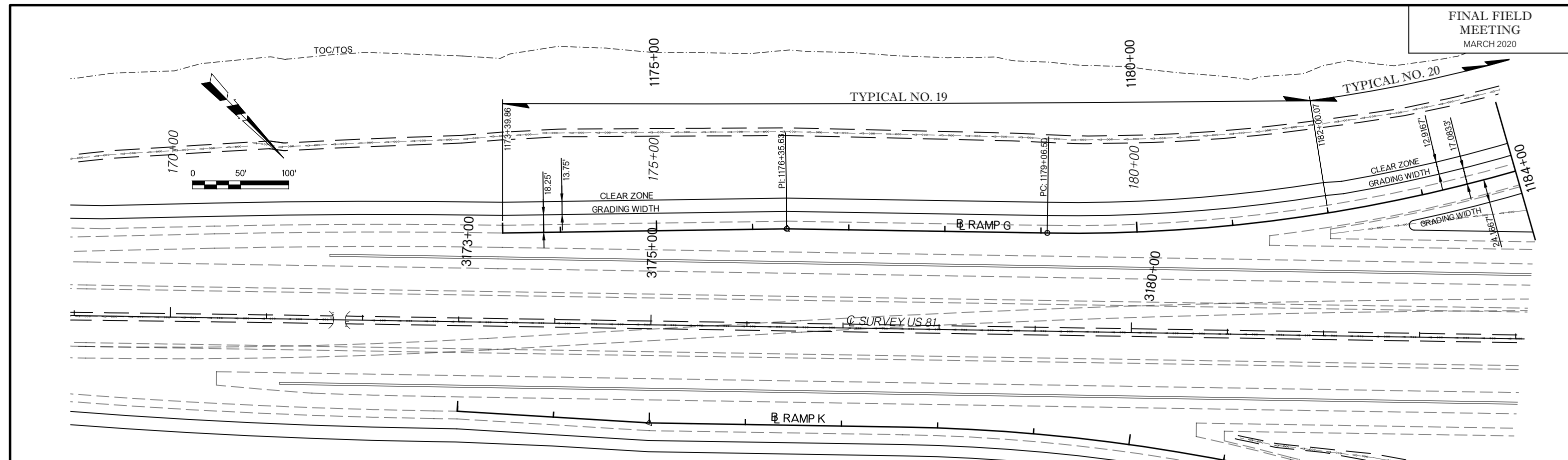
GRADY COUNTY



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
RAMP D
State Job No. 24428(04) Sheet No. R288

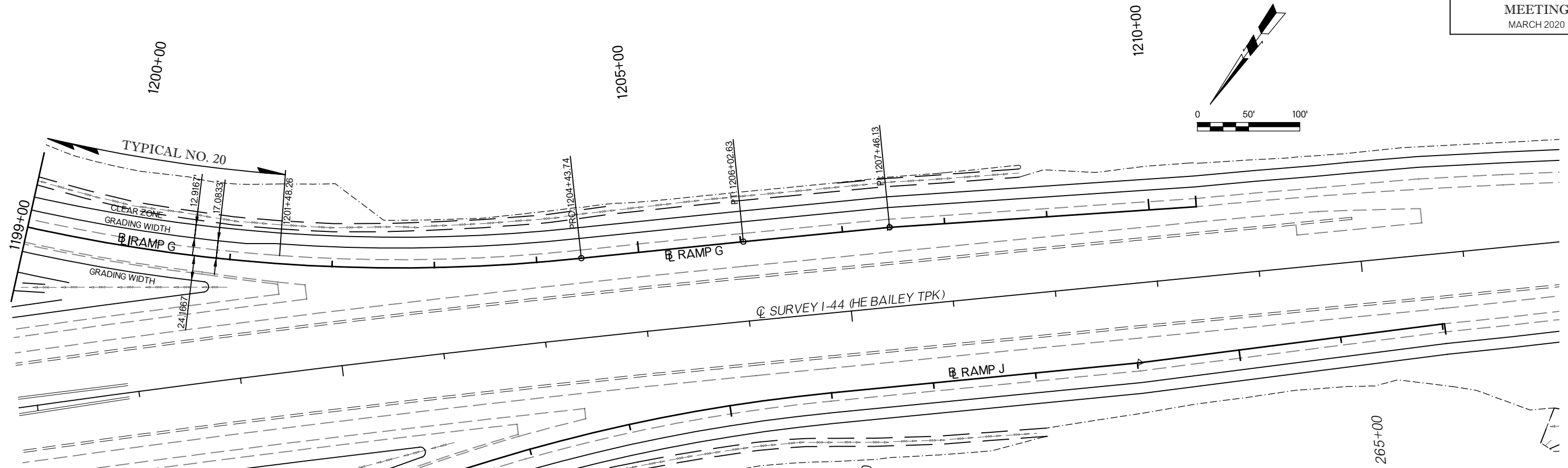
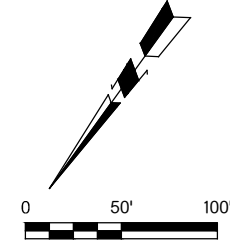
GRADY COUNTY
US 81 REALIGNMENT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
RAMP G
State Job No. 24428(04) Sheet No. R289

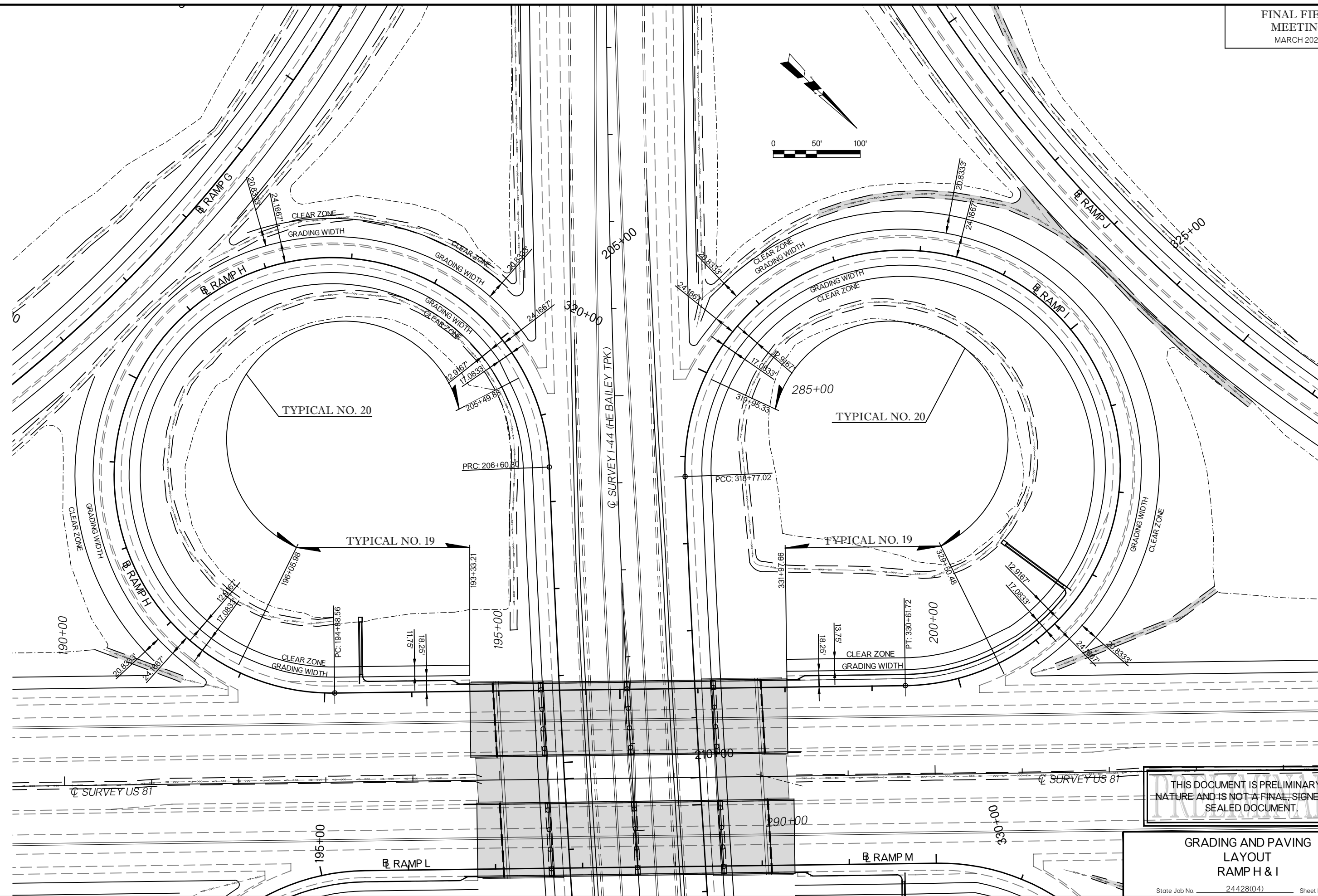
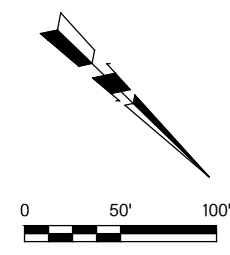
GRADY COUNTY US 81 REALIGNMENT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SURFACING CONSTRUCTION DETAILS
RAMP G
State Job No. 24428(04) Sheet No. R290

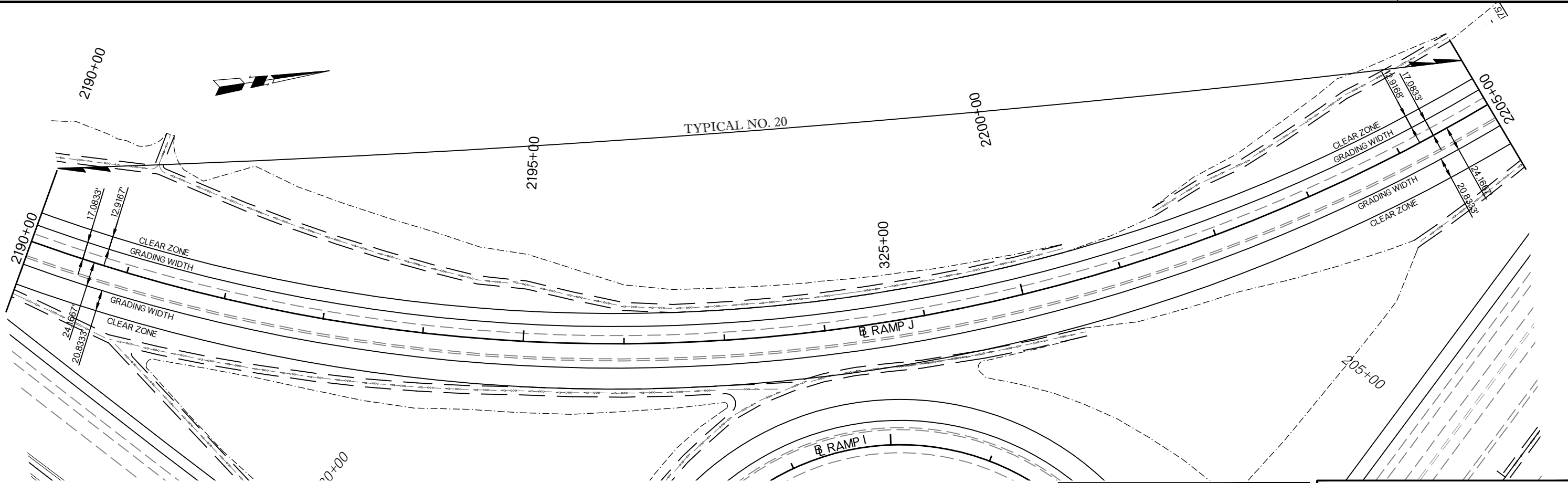
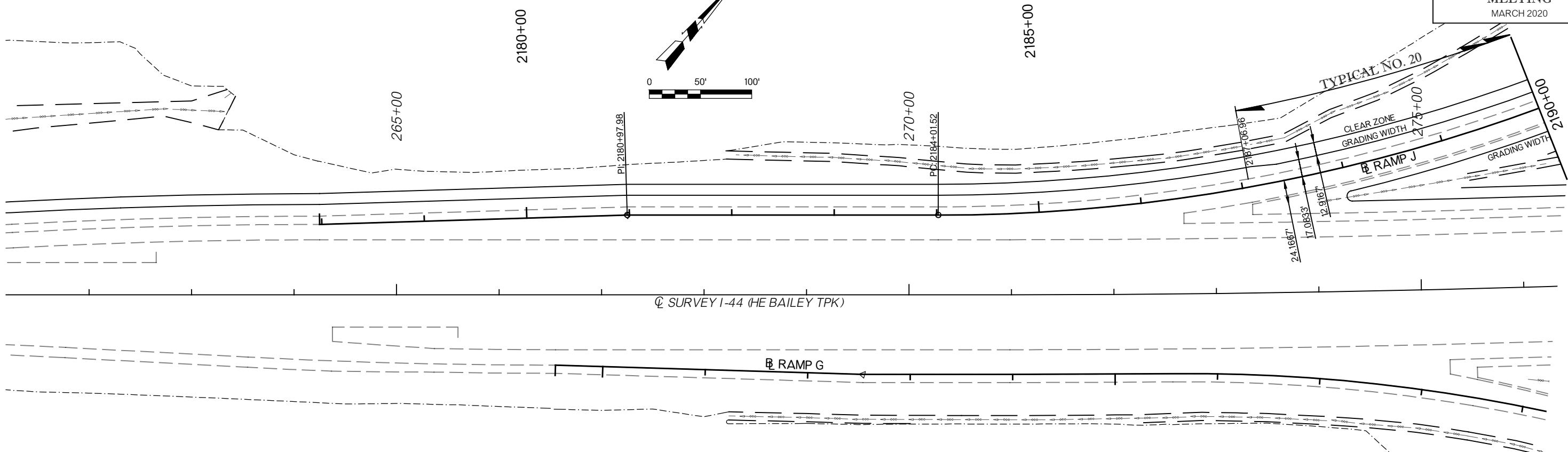
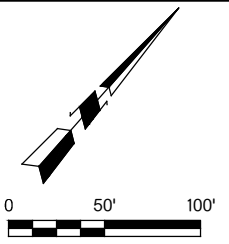
US 81 REALIGNMENT
GRADY COUNTY



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

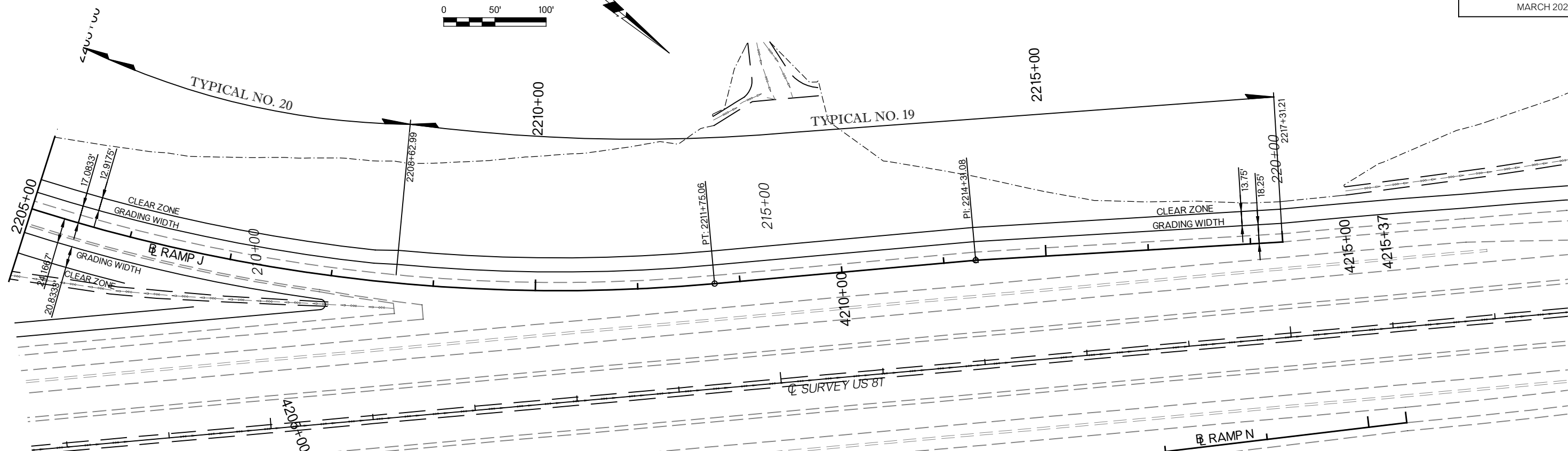
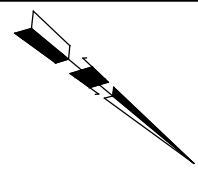
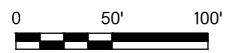
**GRADING AND PAVING LAYOUT
RAMP H & I**

US 81 REALIGNMENT
GRADY COUNTY



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

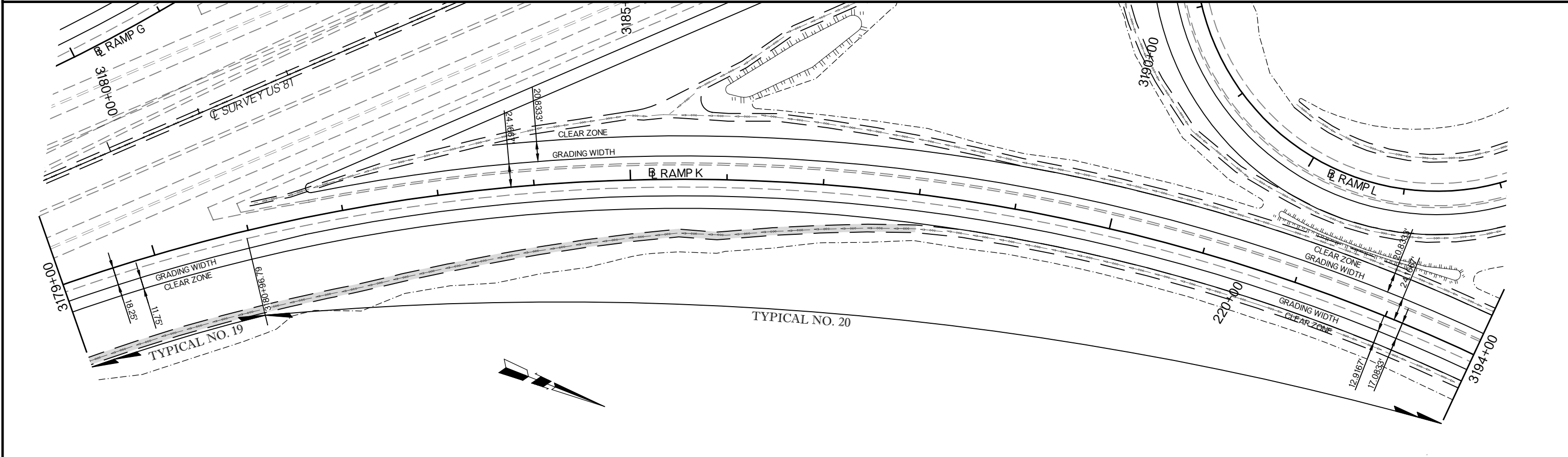
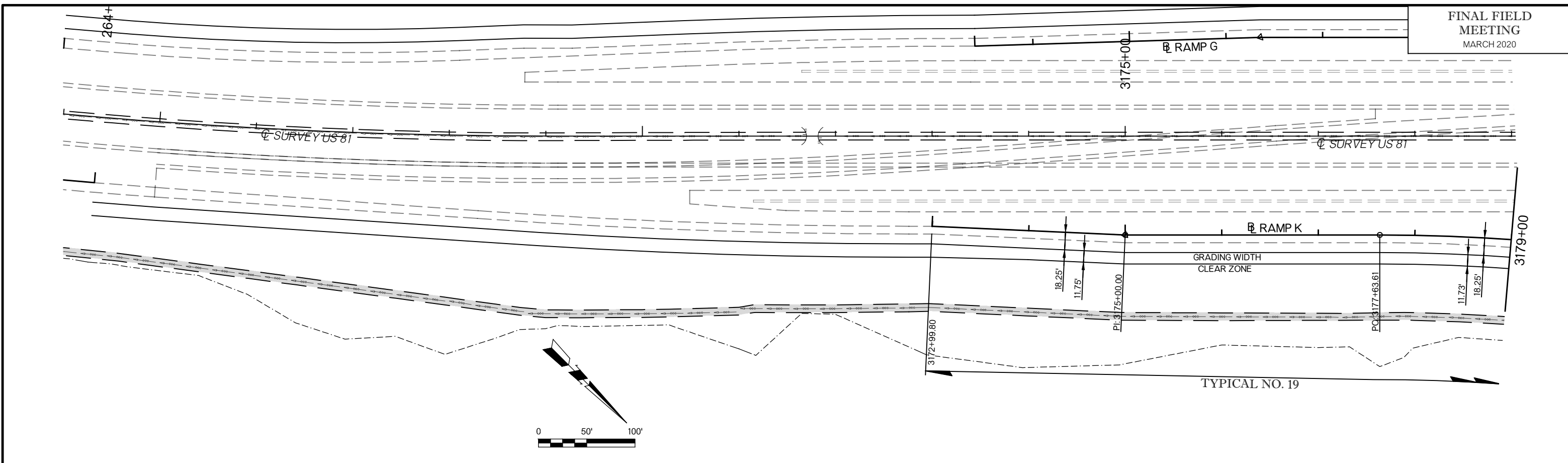
GRADING AND PAVING LAYOUT
RAMP J
State Job No. 24428(04) Sheet No. R292



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

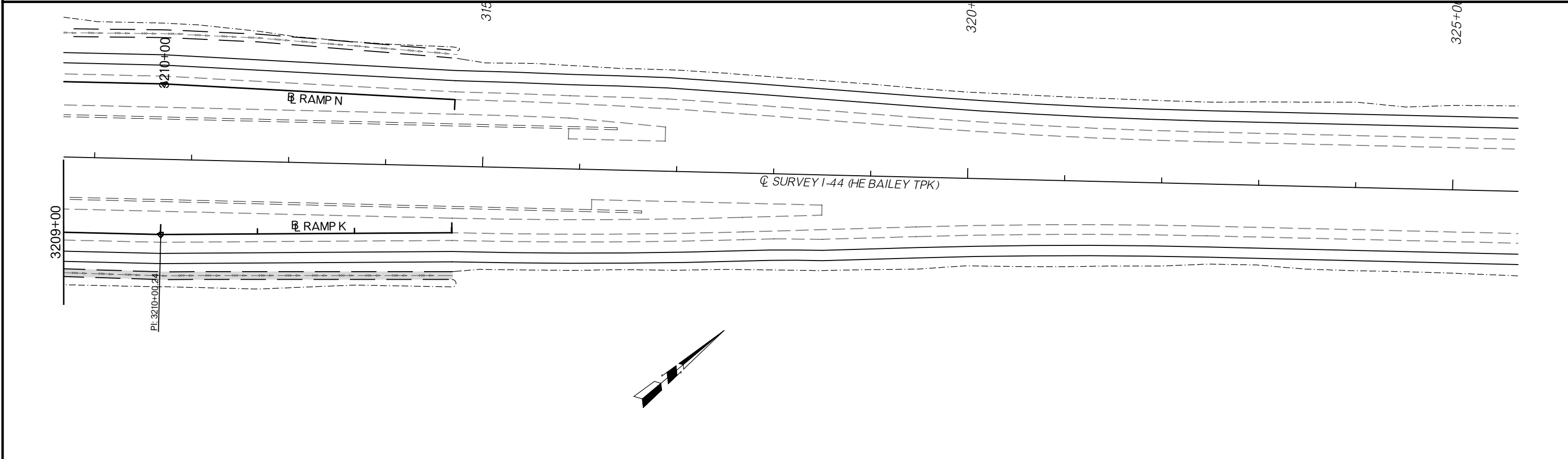
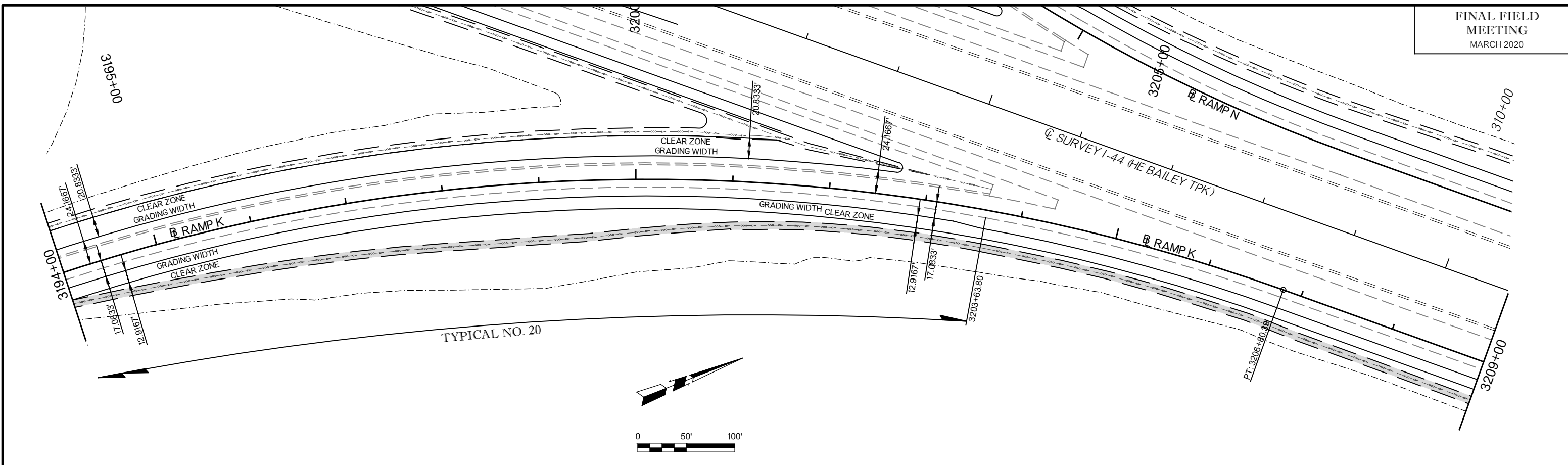
GRADING AND PAVING LAYOUT
RAMP J
State Job No. 24428(04) Sheet No. R293

US 81 REALIGNMENT
GRADY COUNTY



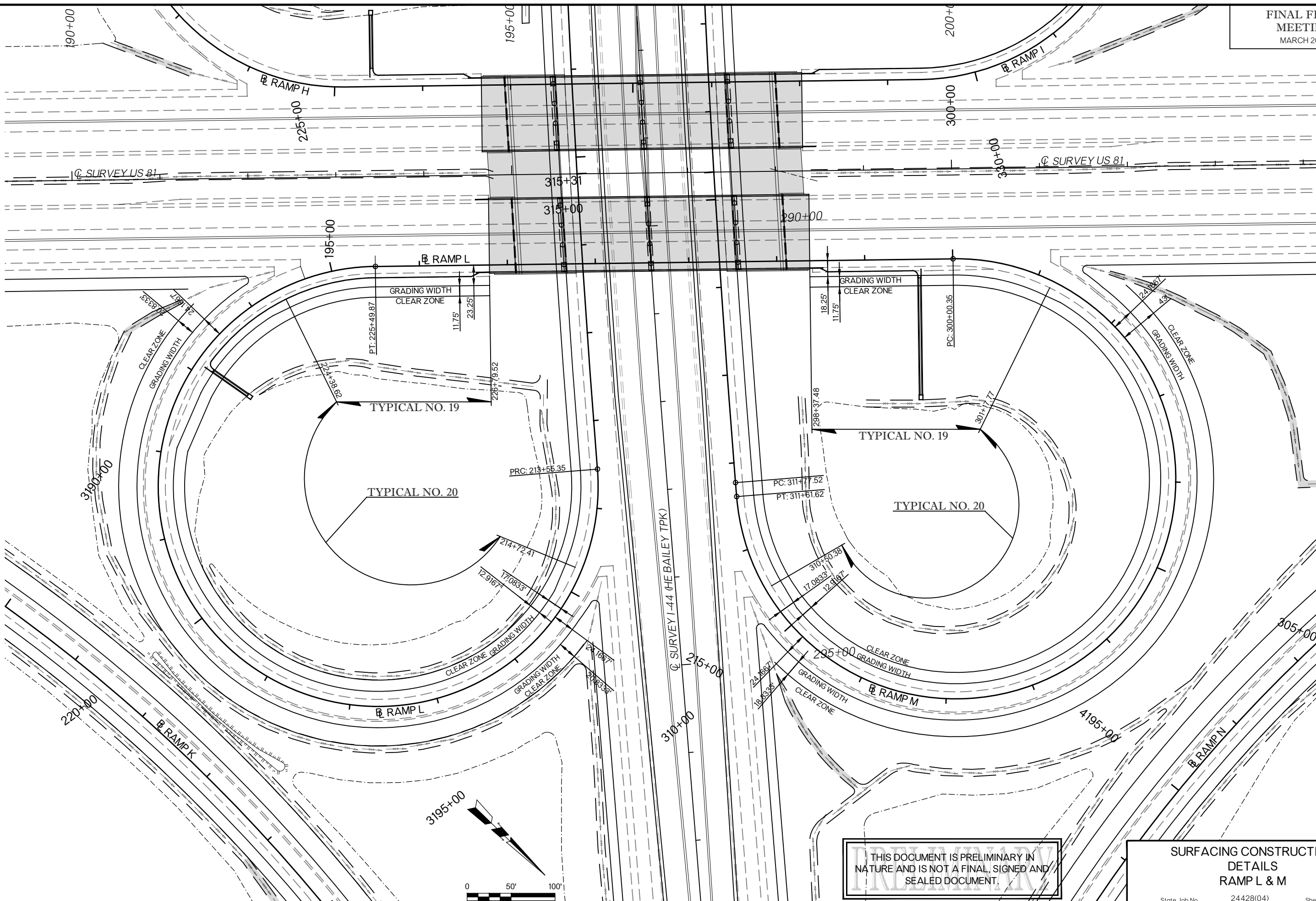
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
RAMP K



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

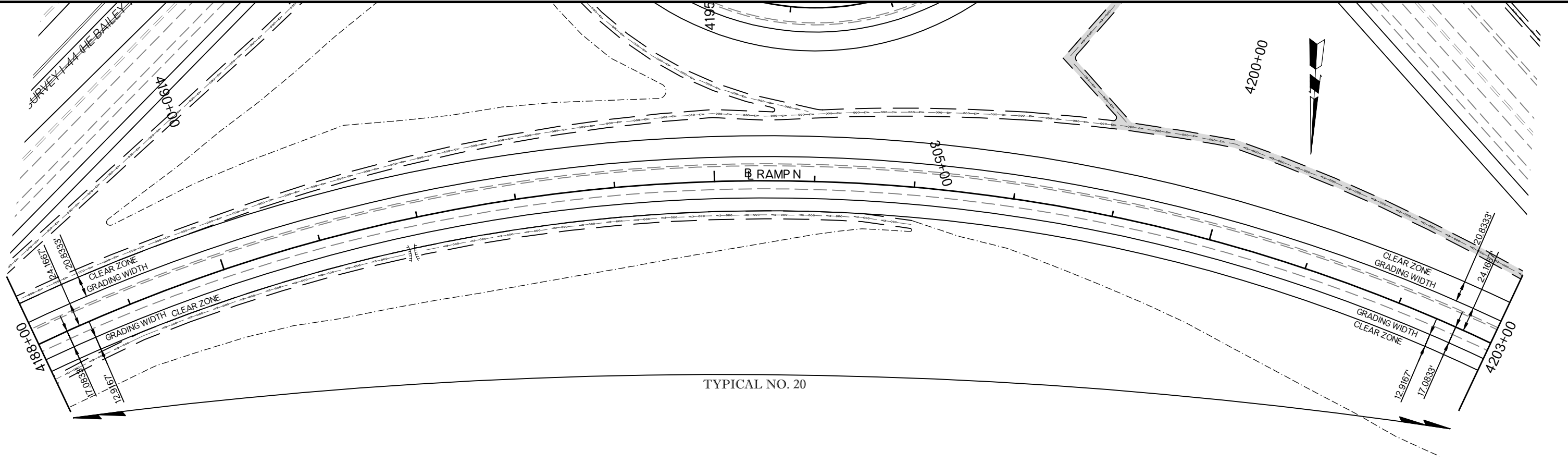
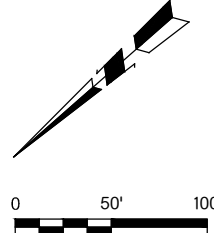
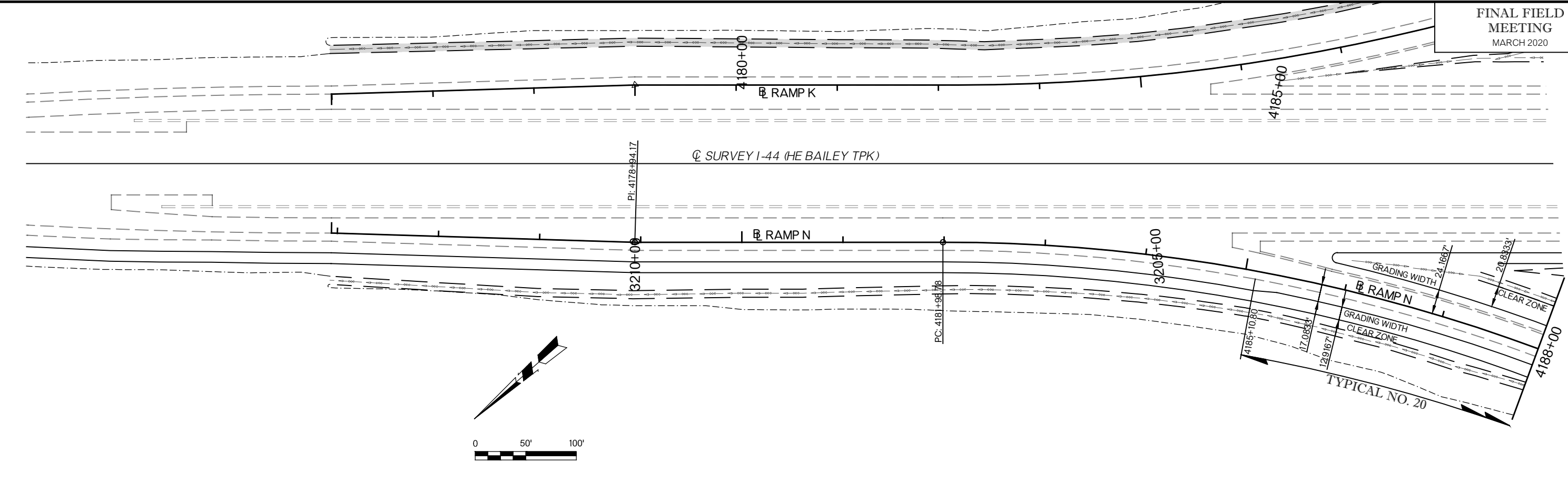
GRADING AND PAVING LAYOUT
RAMP K
State Job No. 24428(04) Sheet No. R295



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

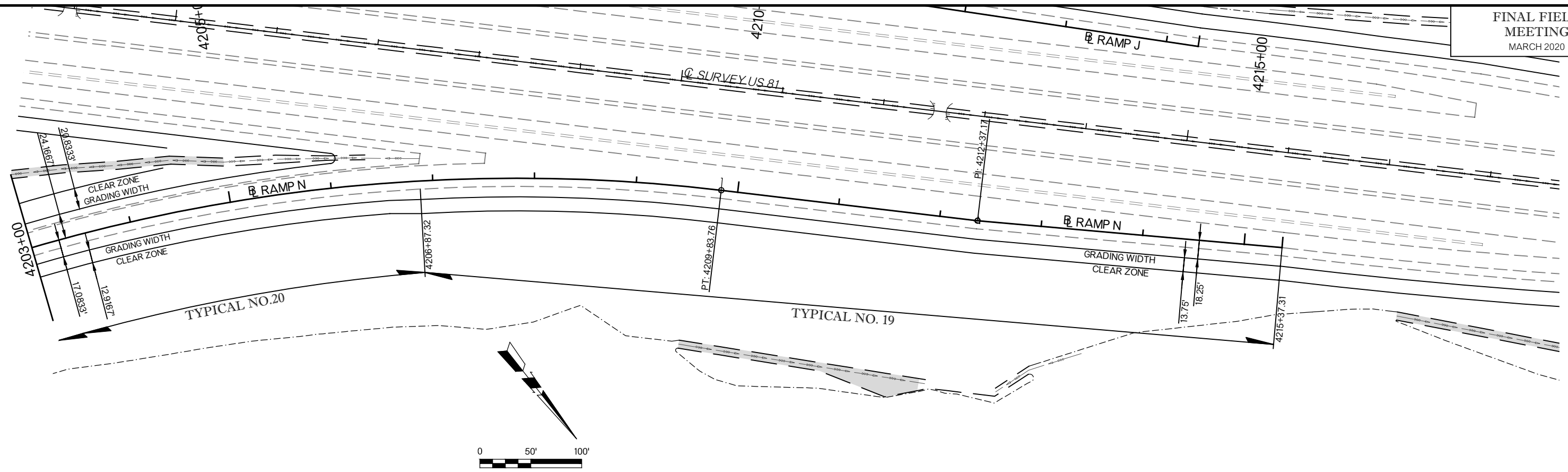
SURFACING CONSTRUCTION DETAILS
RAMP L & M
State Job No. 24428(04) Sheet No. R296

GRADY COUNTY
US 81 REALIGNMENT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

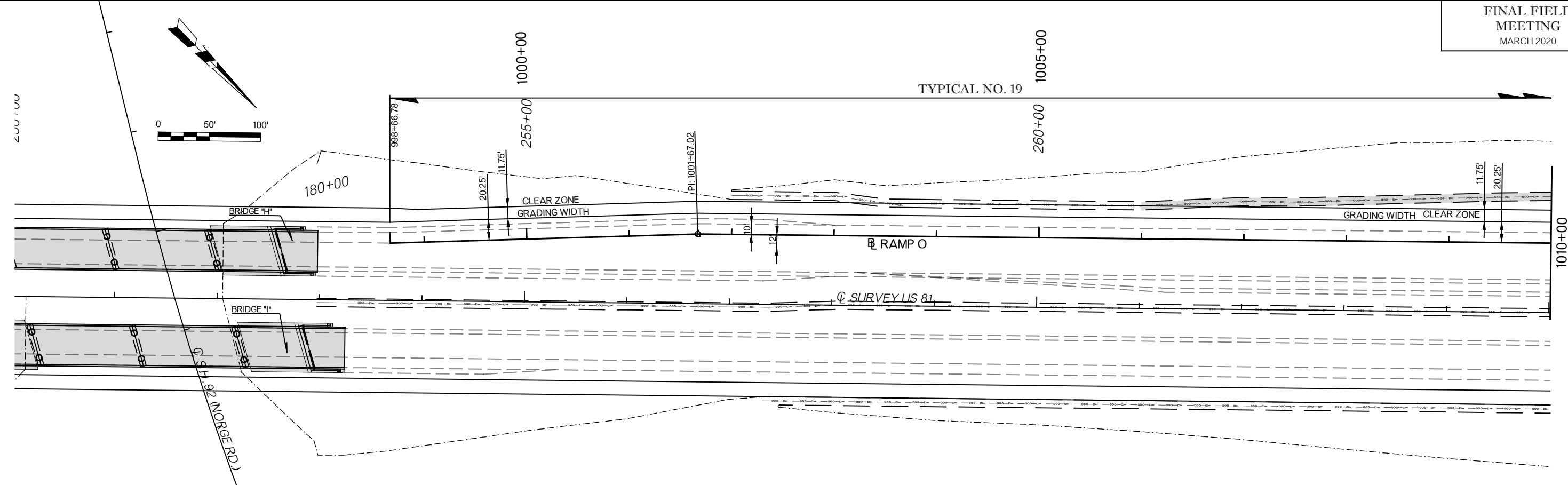
GRADING AND PAVING LAYOUT RAMP N
State Job No. 24428(04) Sheet No. R297



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

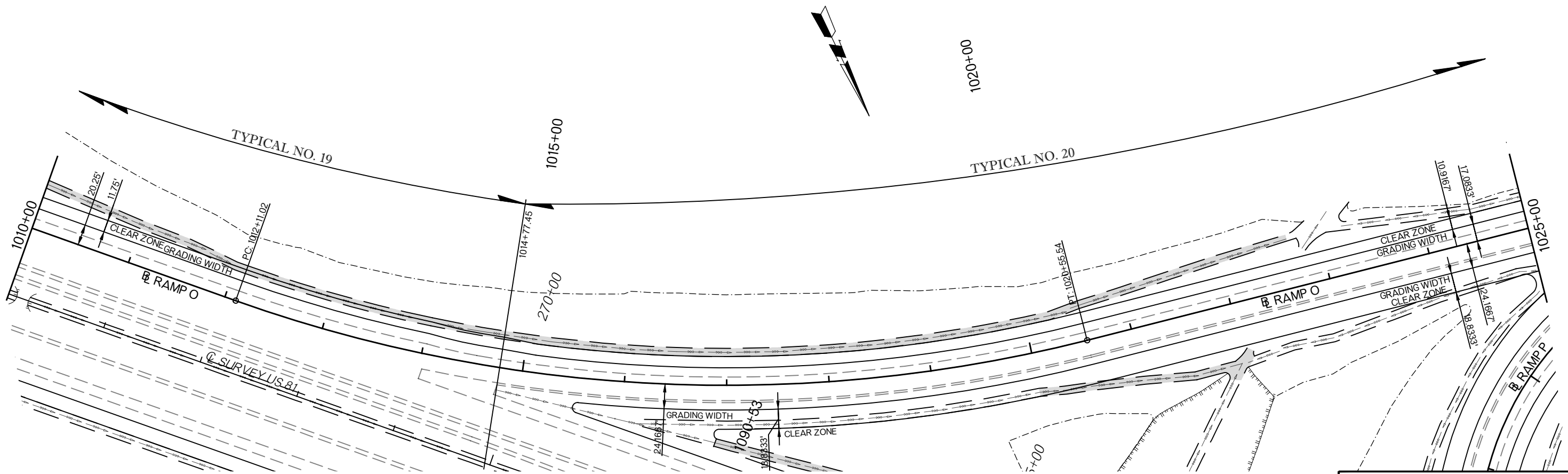
GRADING AND PAVING LAYOUT
RAMP N
State Job No. 24428(04) Sheet No. R298

TYPICAL NO. 19



1020+00

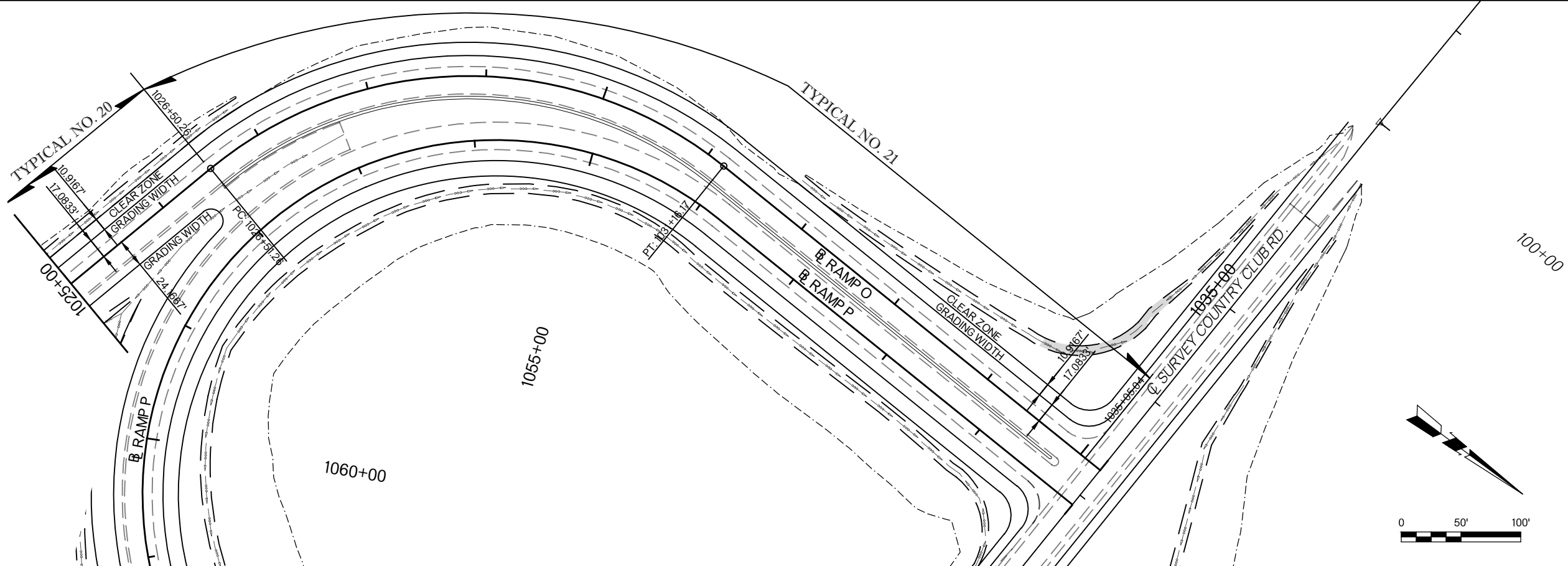
TYPICAL NO. 20



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
RAMP O
State Job No. 24428(04) Sheet No. R299

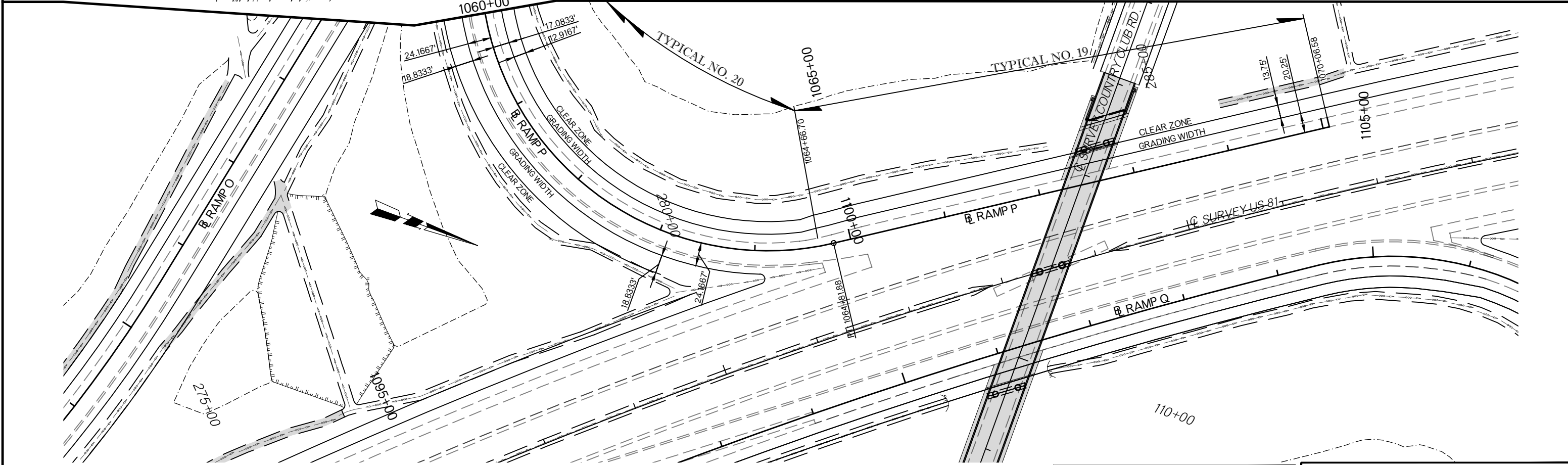
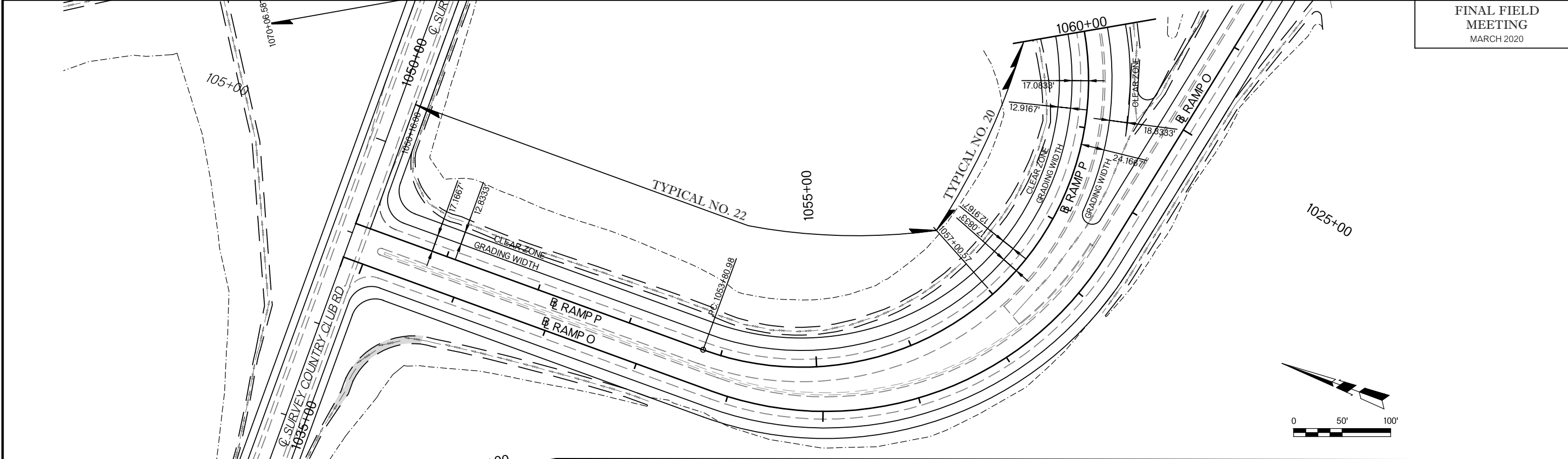
GRADY COUNTY
US 81 REALIGNMENT



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

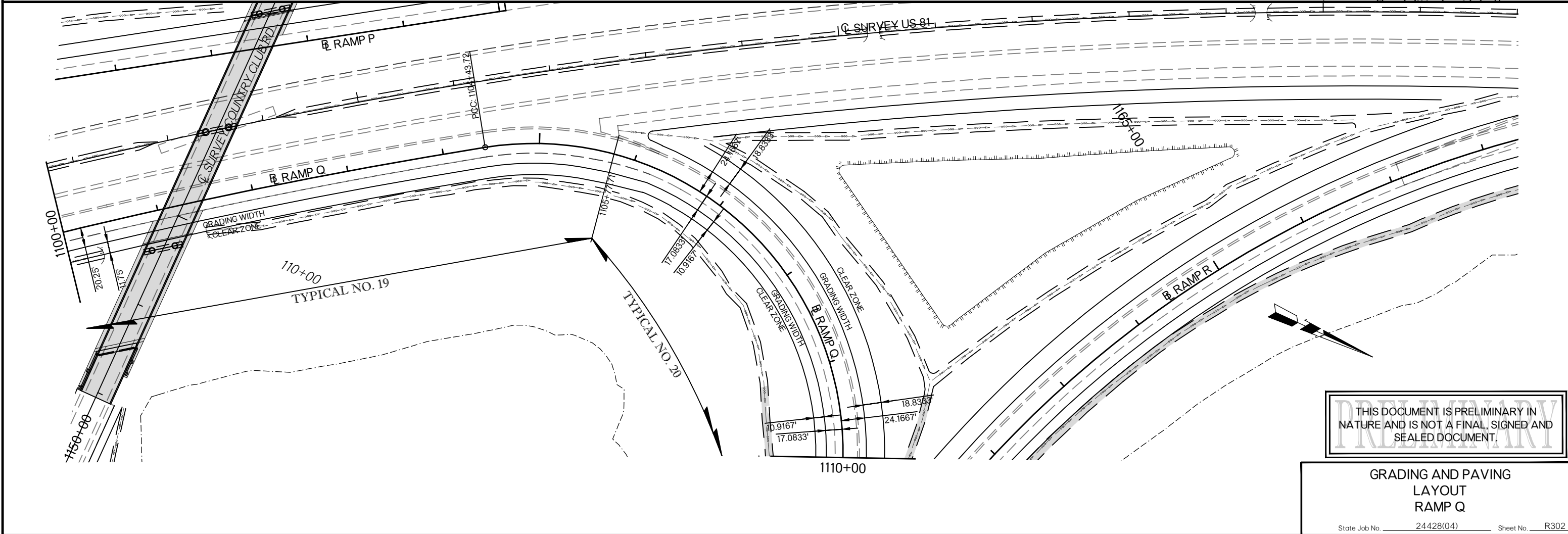
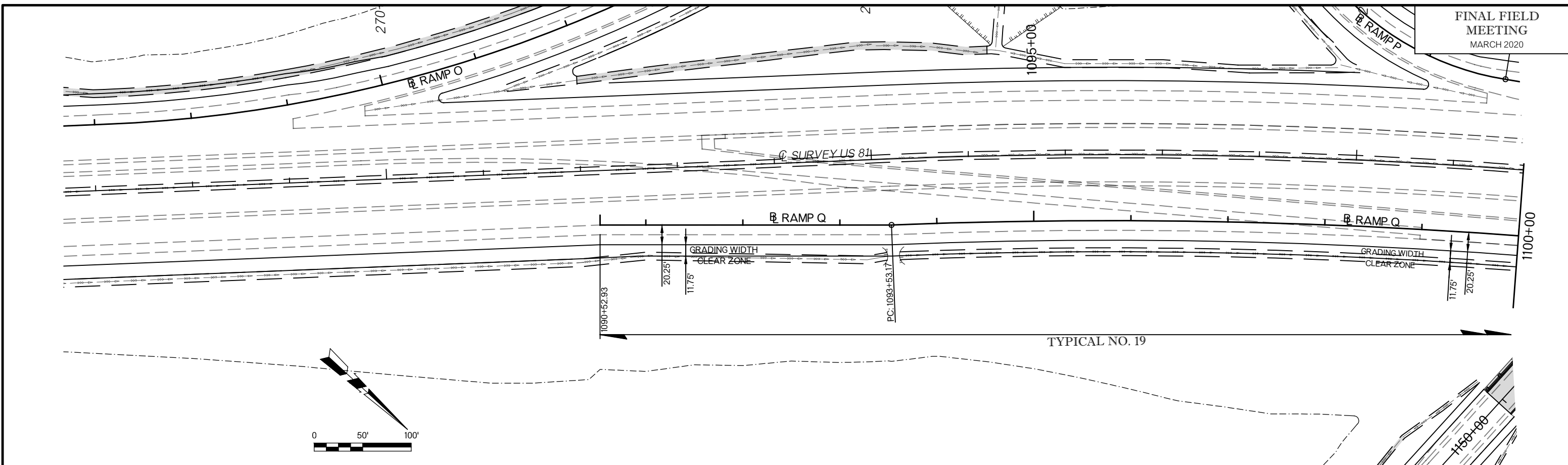
**GRADING AND PAVING
LAYOUT
RAMP O**

US 81 REALIGNMENT
GRADY COUNTY



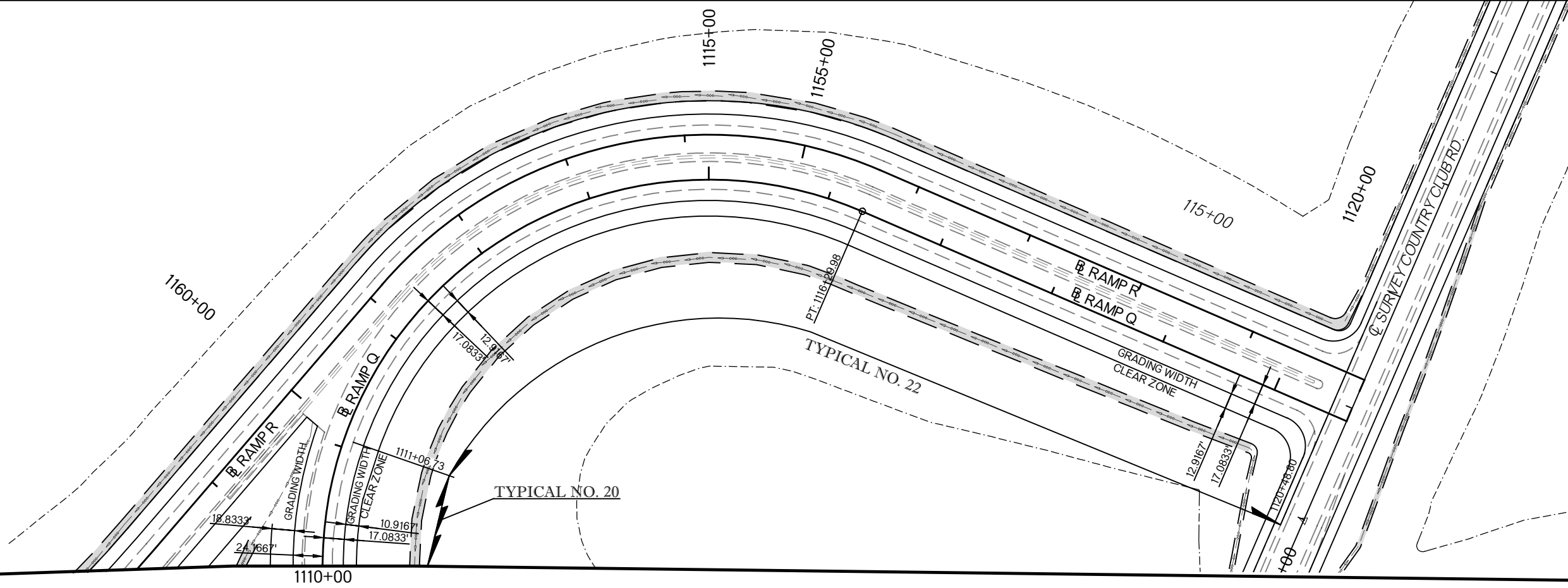
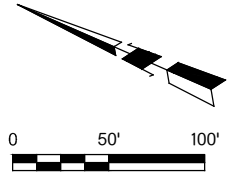
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
RAMP P



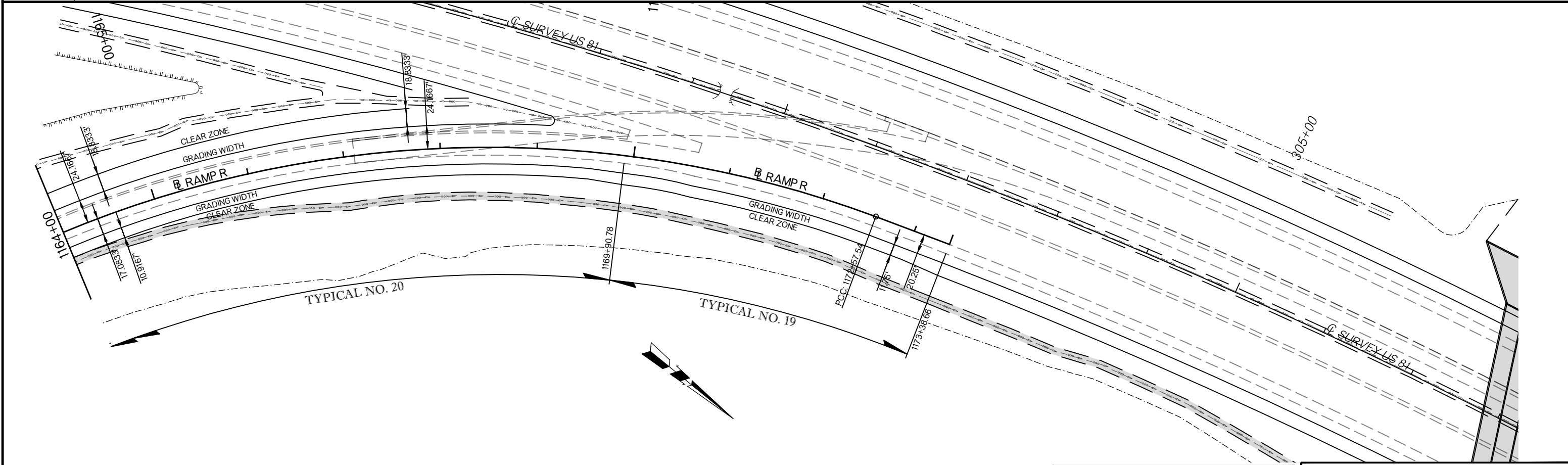
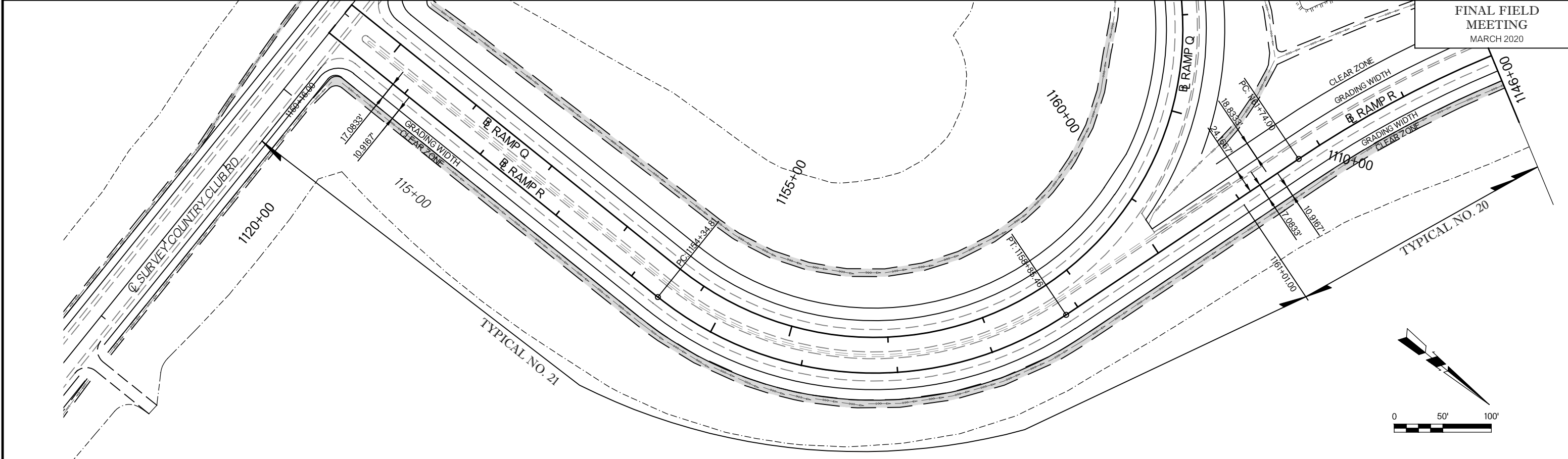
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT RAMP Q



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

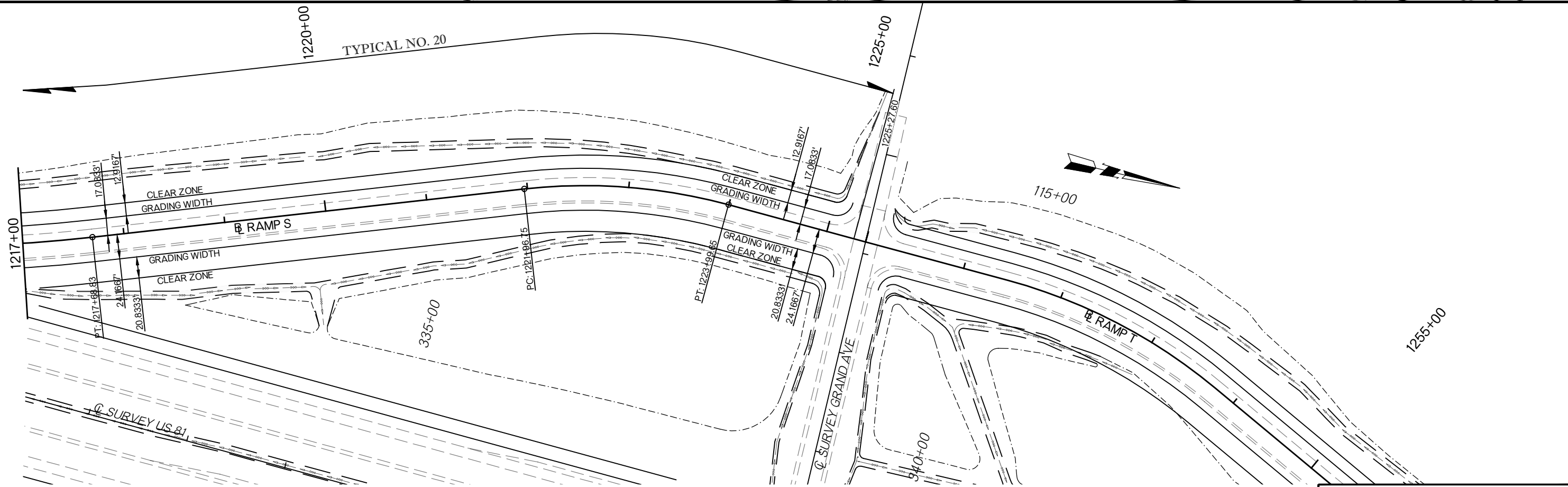
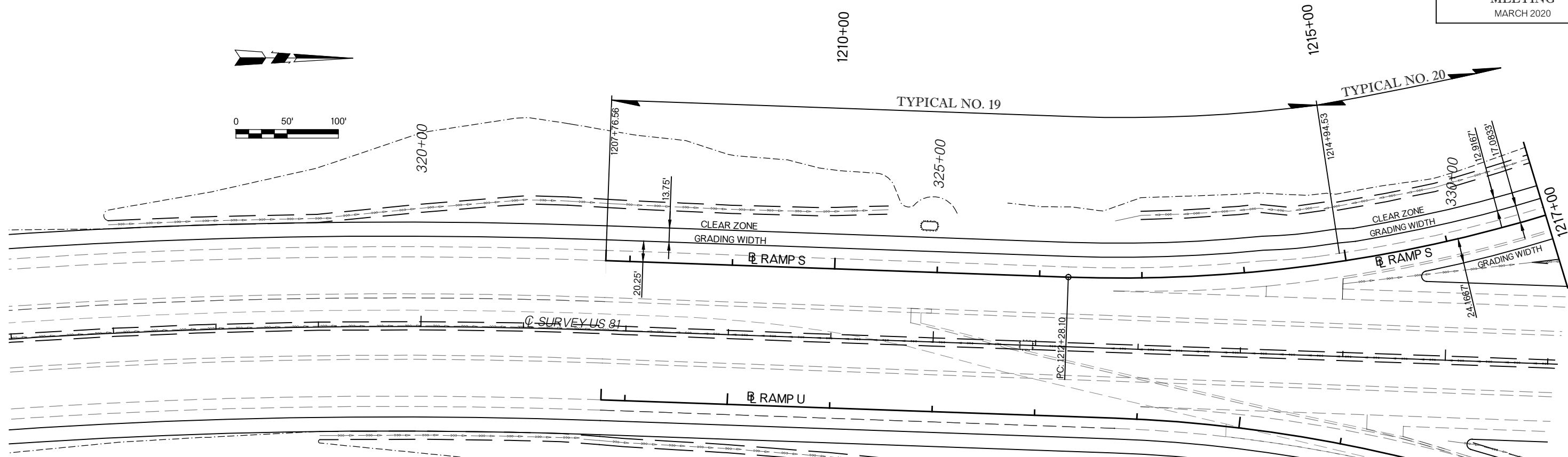
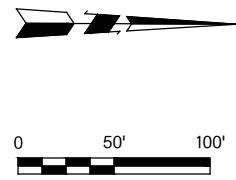
GRADING AND PAVING LAYOUT
RAMP Q



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT RAMP R
State Job No. 24428(04) Sheet No. R304

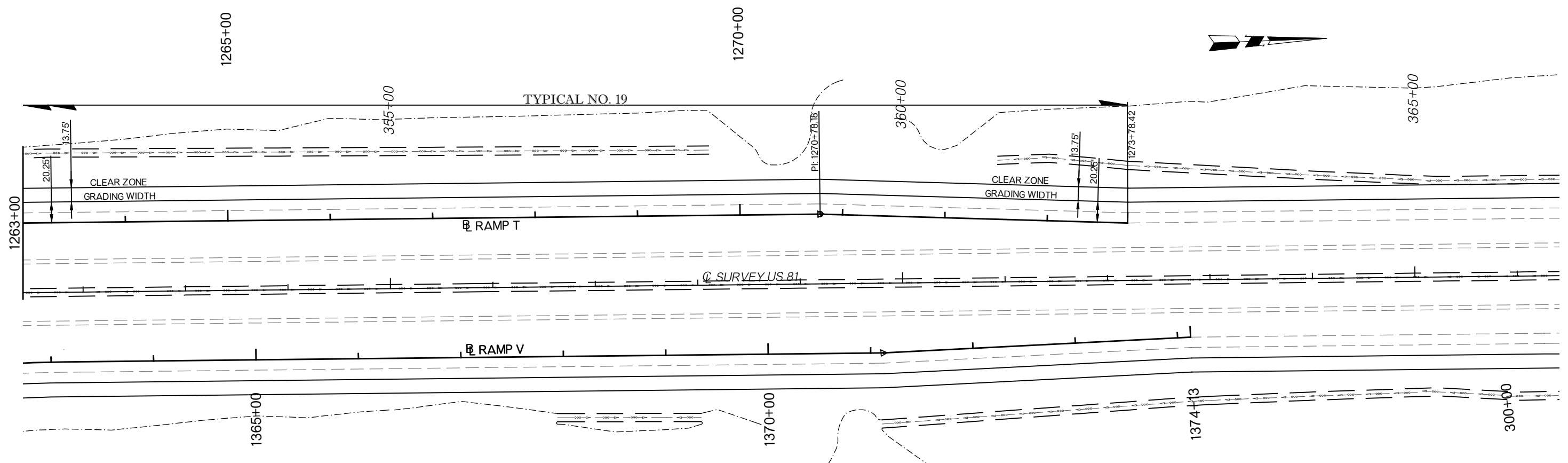
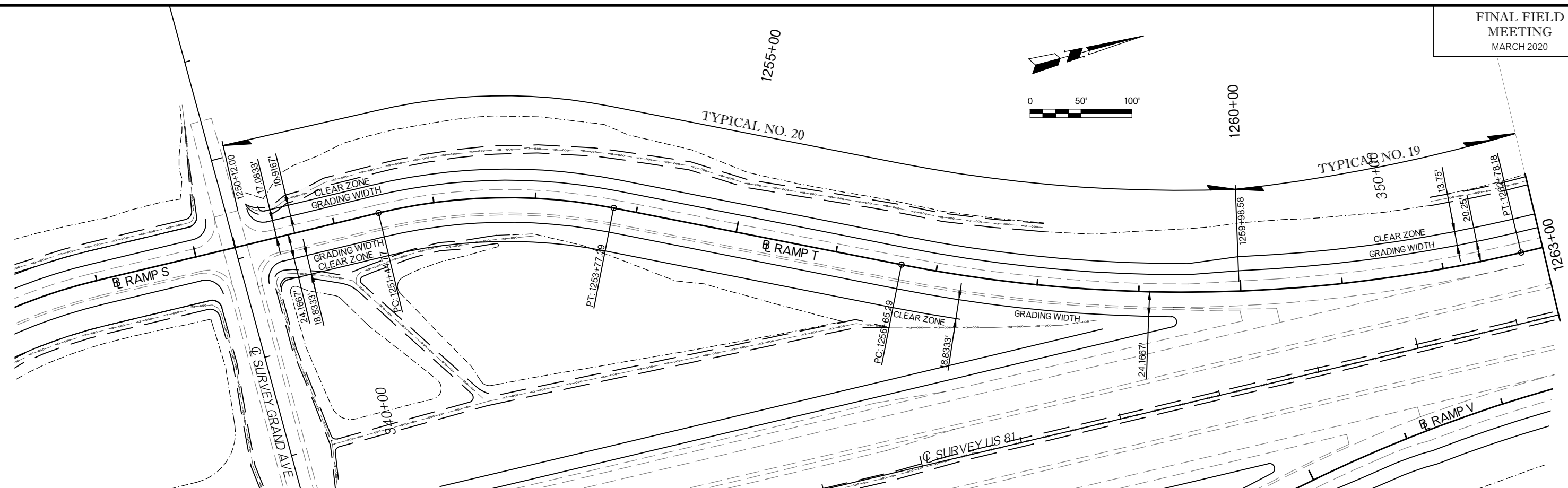
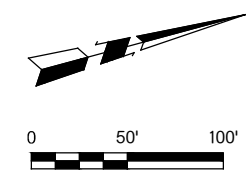
GRADY COUNTY US 81 REALIGNMENT



**GRADING AND PAVING
LAYOUT
RAMP S**

State Job No. 24428(04) Sheet No. R305

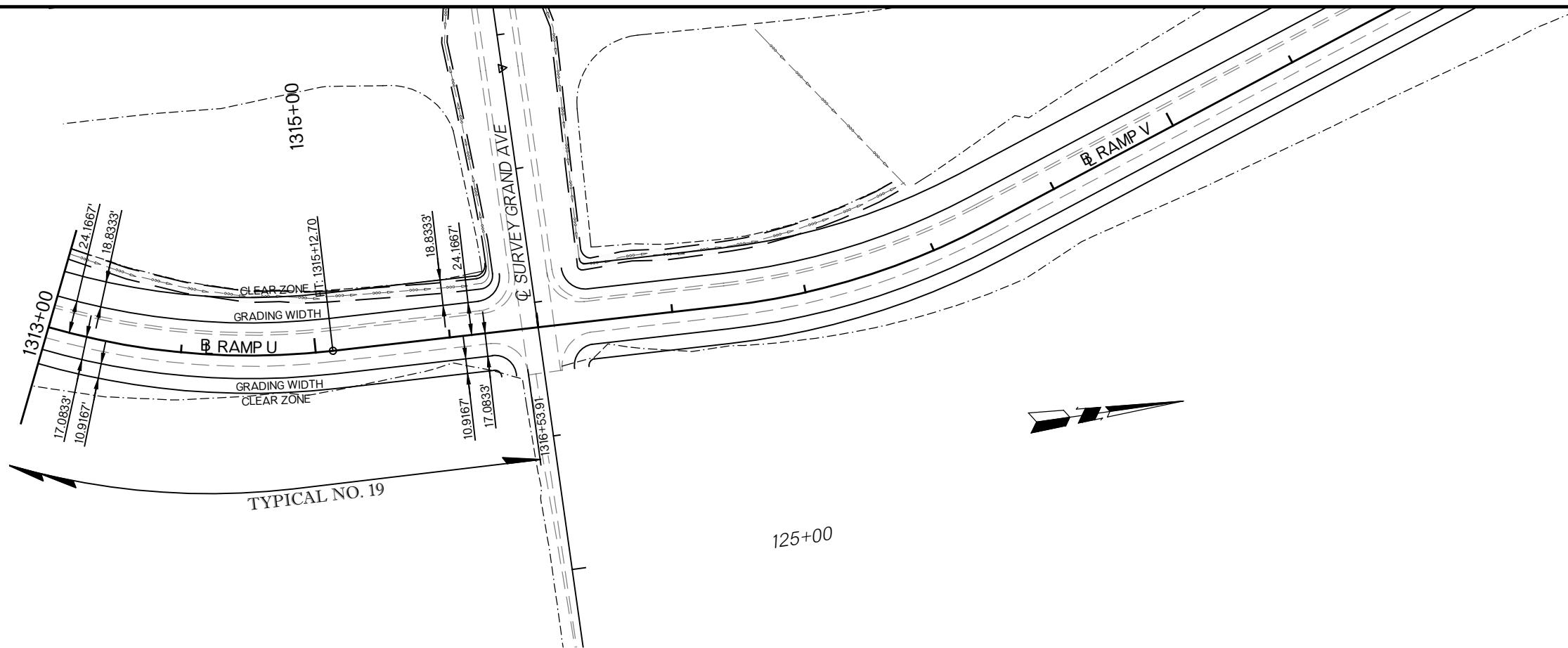
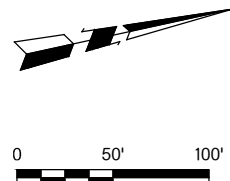
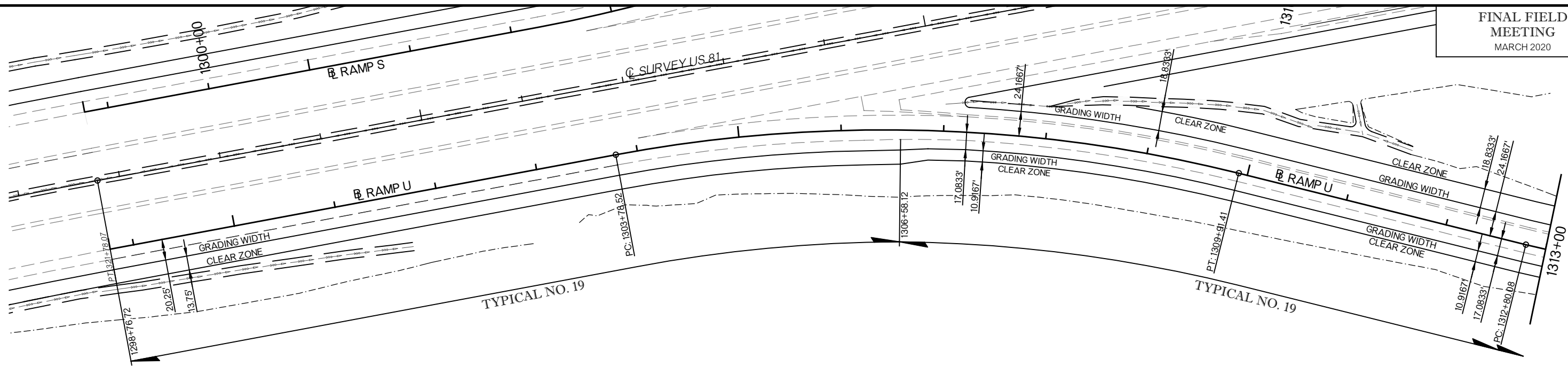
GRADY COUNTY
US 81 REALIGNMENT



PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

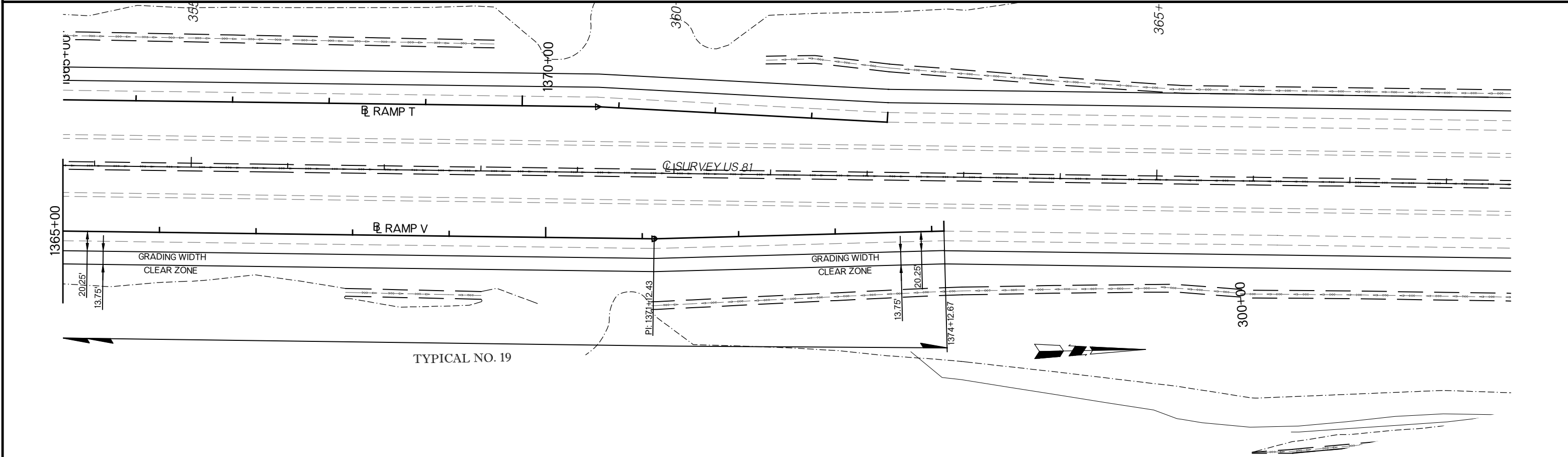
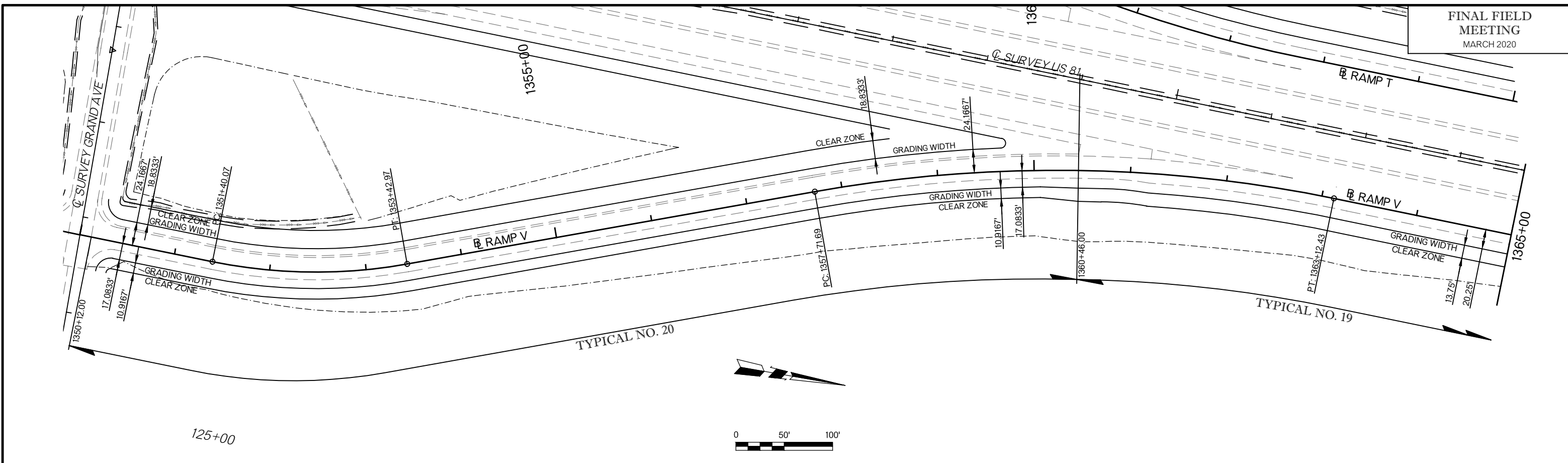
GRADING AND PAVING LAYOUT
RAMP T
State Job No. 24428(04) Sheet No. R306

GRADY COUNTY US 81 REALIGNMENT

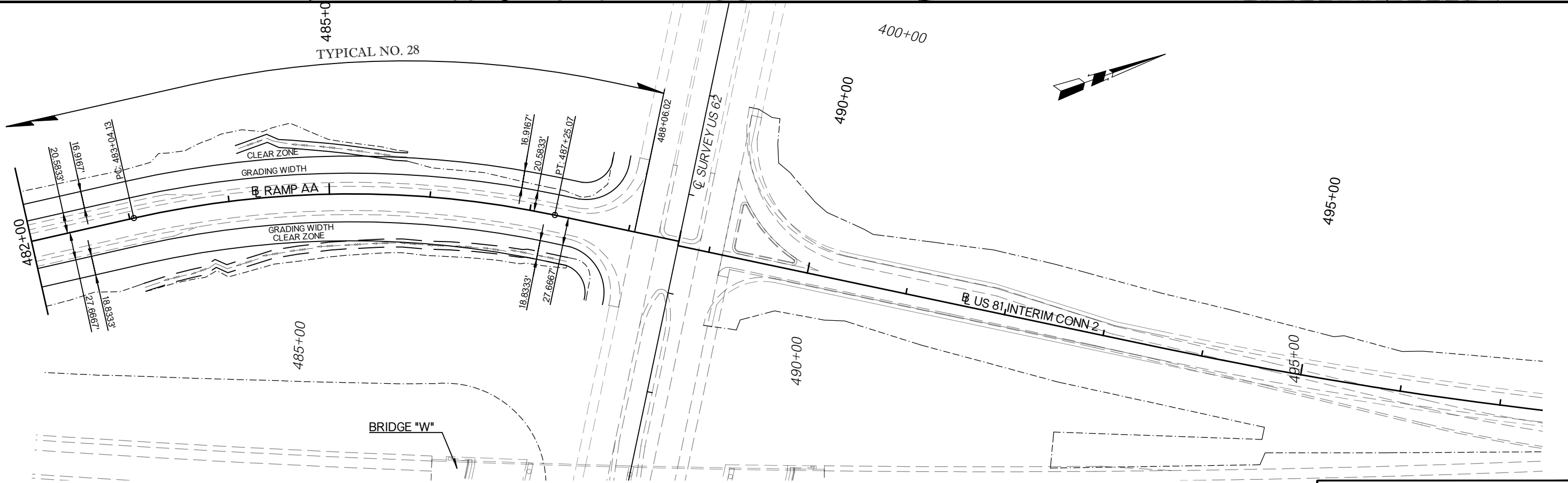
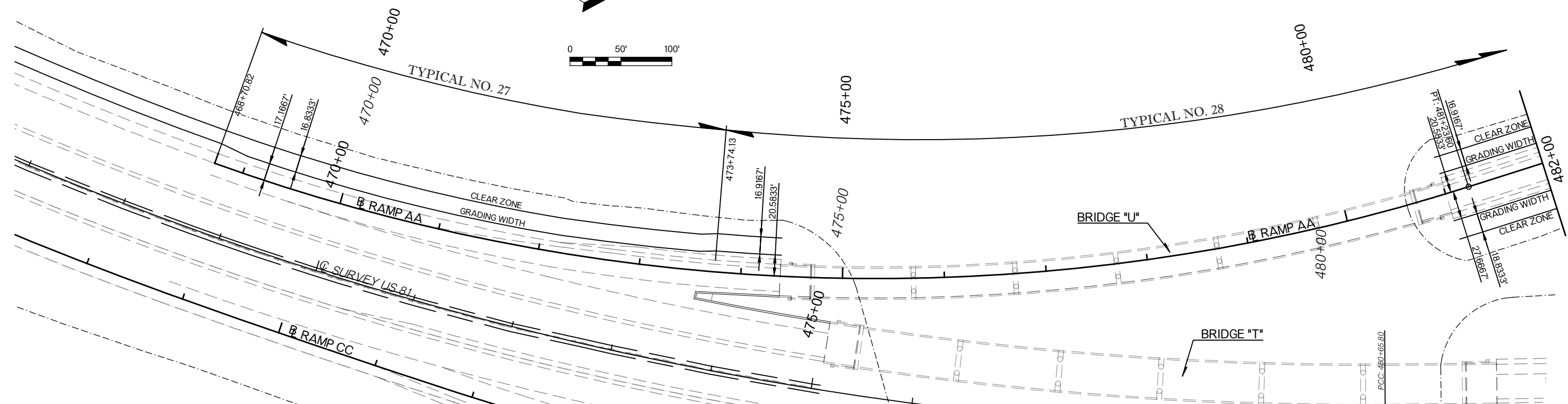
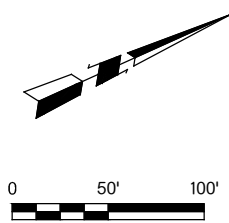


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
RAMP U
State Job No. 24428(04) Sheet No. R307



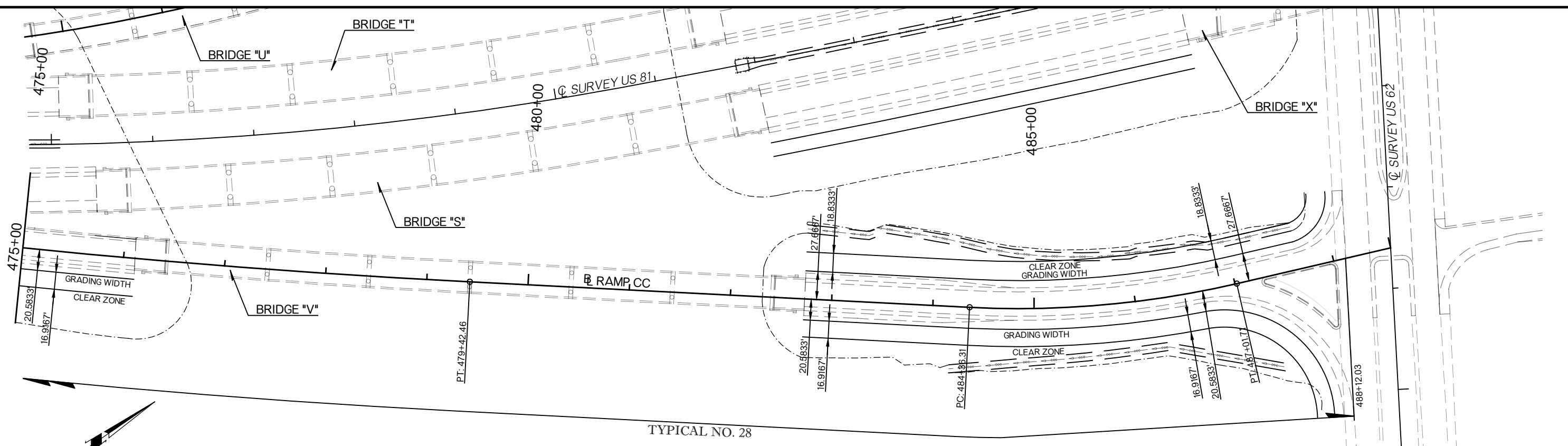
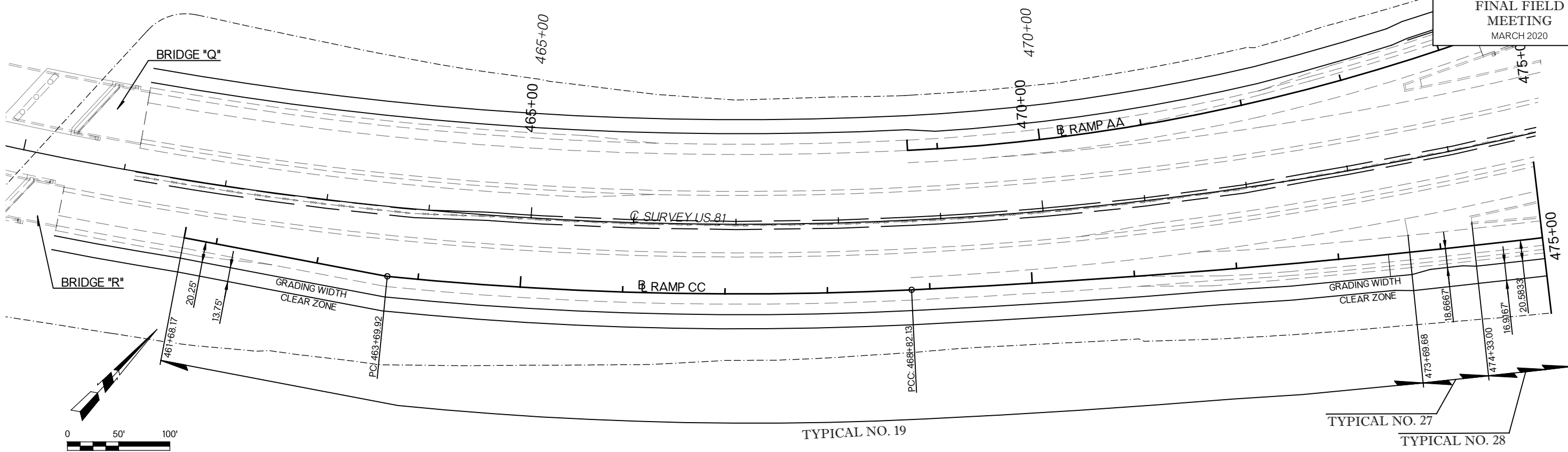
GRADING AND PAVING
LAYOUT
RAMP V



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT
RAMP AA
State Job No. 24428(04) Sheet No. R309

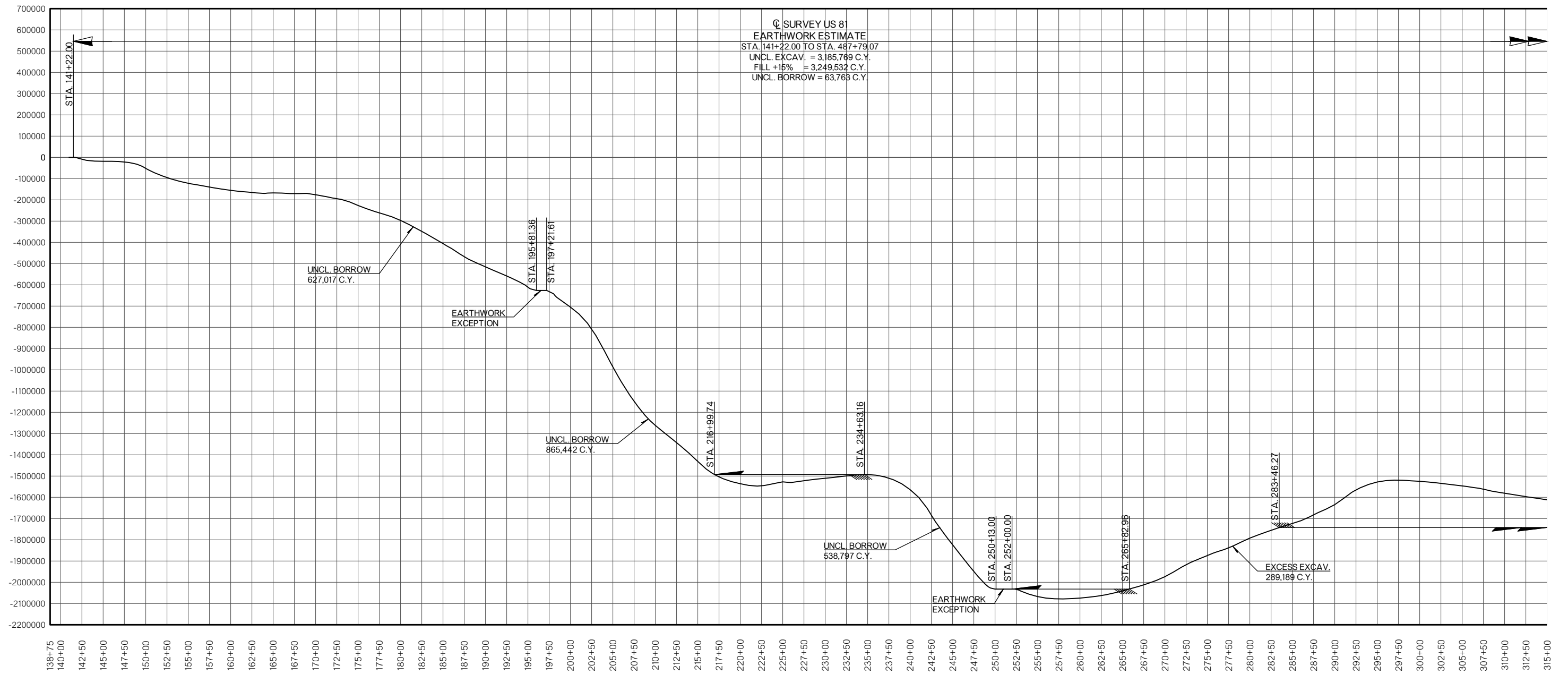
GRADY COUNTY US 81 REALIGNMENT



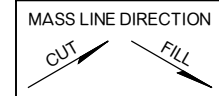
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

GRADING AND PAVING LAYOUT RAMP CC
State Job No. 24428(04) Sheet No. R310

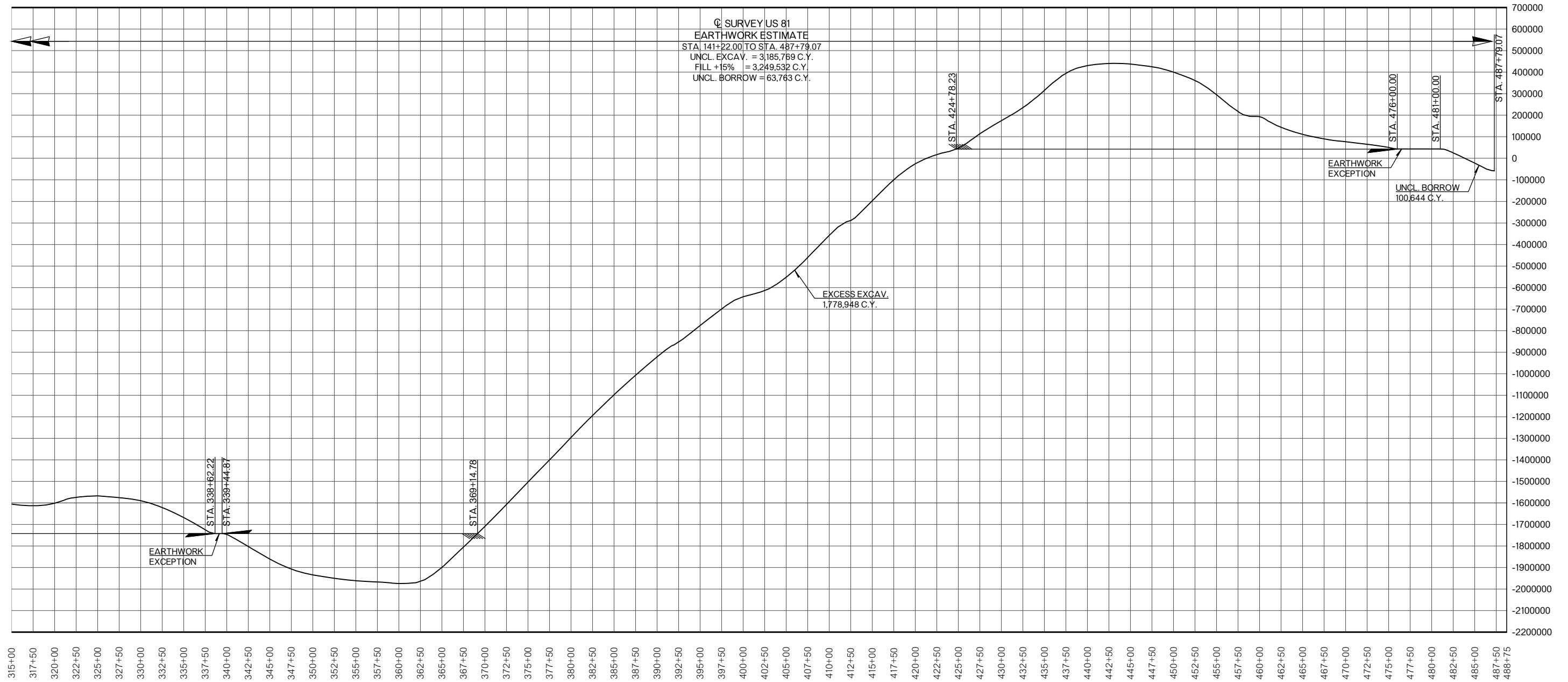
GRADY COUNTY US 81 REALIGNMENT



MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTIONS FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.

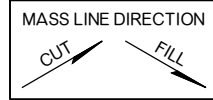


MASS DIAGRAMS

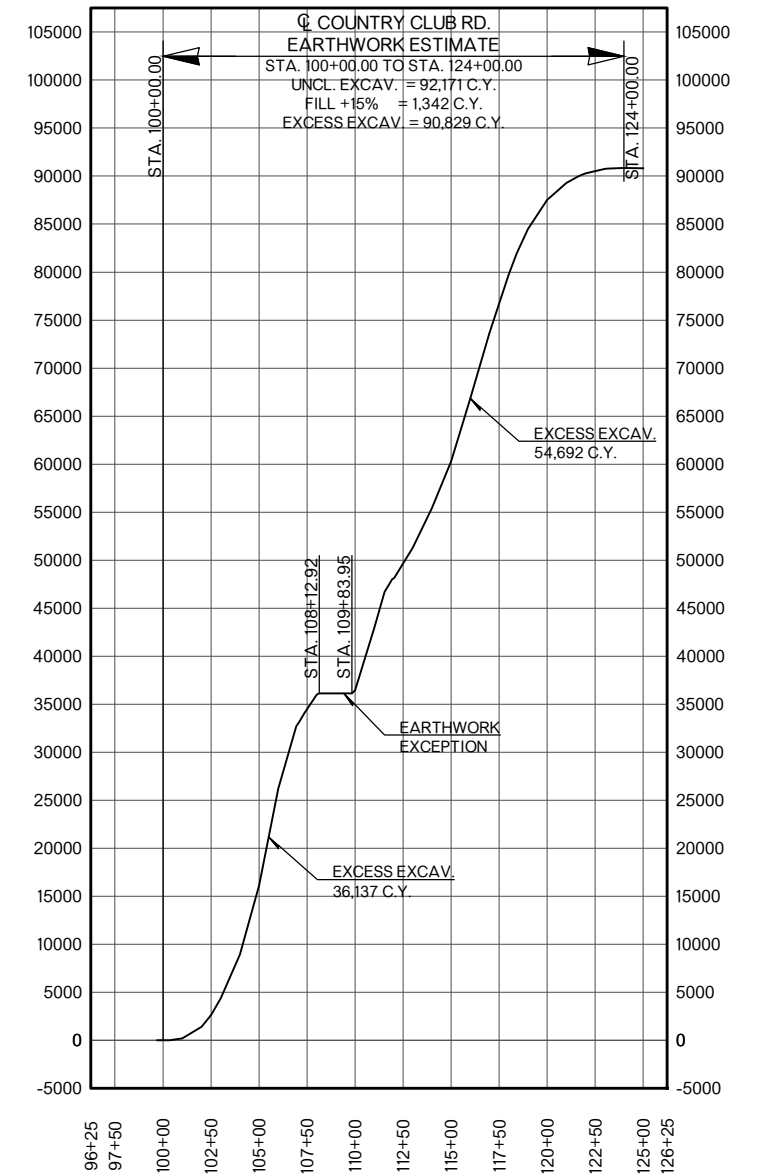
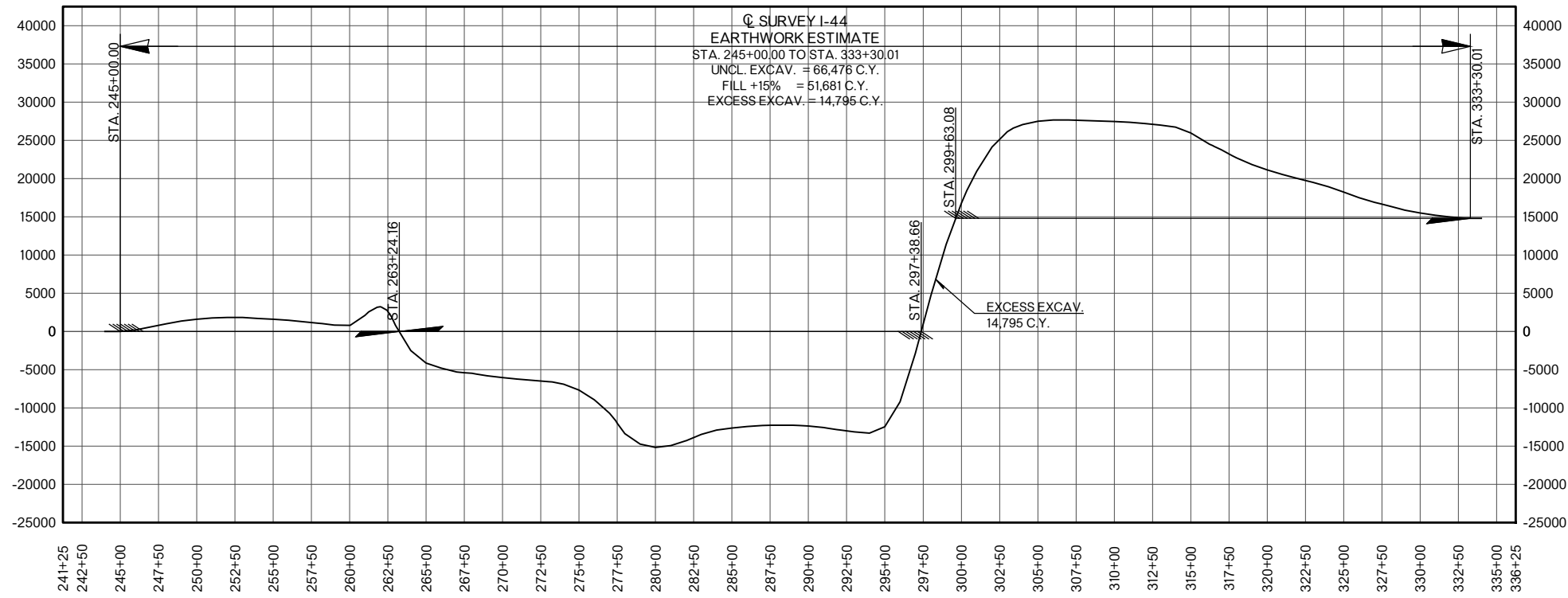


315+00
317+50
320+00
322+50
325+00
327+50
330+00
332+50
335+00
337+50
340+00
342+50
345+00
347+50
350+00
352+50
355+00
357+50
360+00
362+50
365+00
367+50
370+00
372+50
375+00
377+50
380+00
382+50
385+00
387+50
390+00
392+50
395+00
397+50
400+00
402+50
405+00
407+50
410+00
412+50
415+00
417+50
420+00
422+50
425+00
427+50
430+00
432+50
435+00
437+50
440+00
442+50
445+00
447+50
450+00
452+50
455+00
457+50
460+00
462+50
465+00
467+50
470+00
472+50
475+00
477+50
480+00
482+50
485+00
487+50
488+75

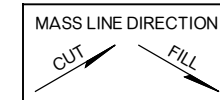
MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTIONS FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.



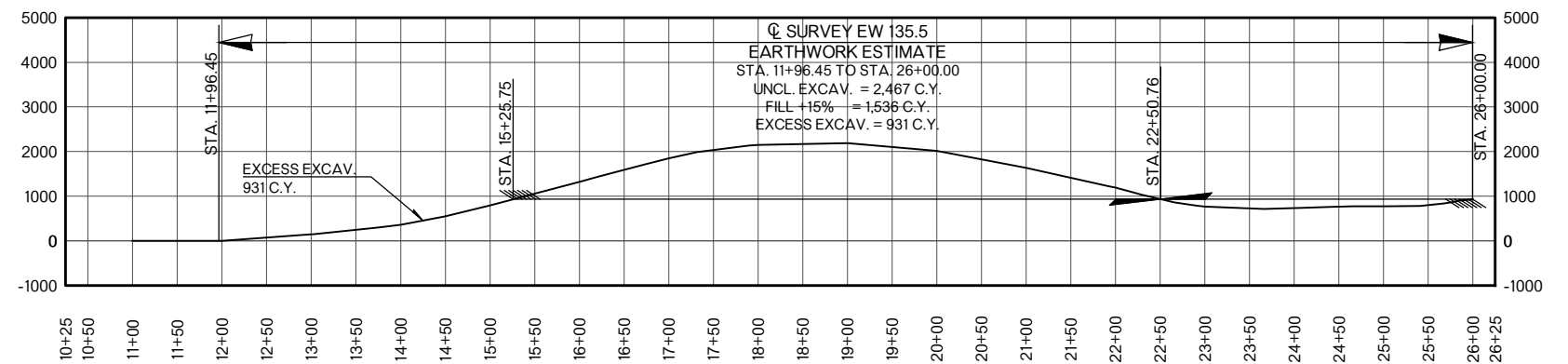
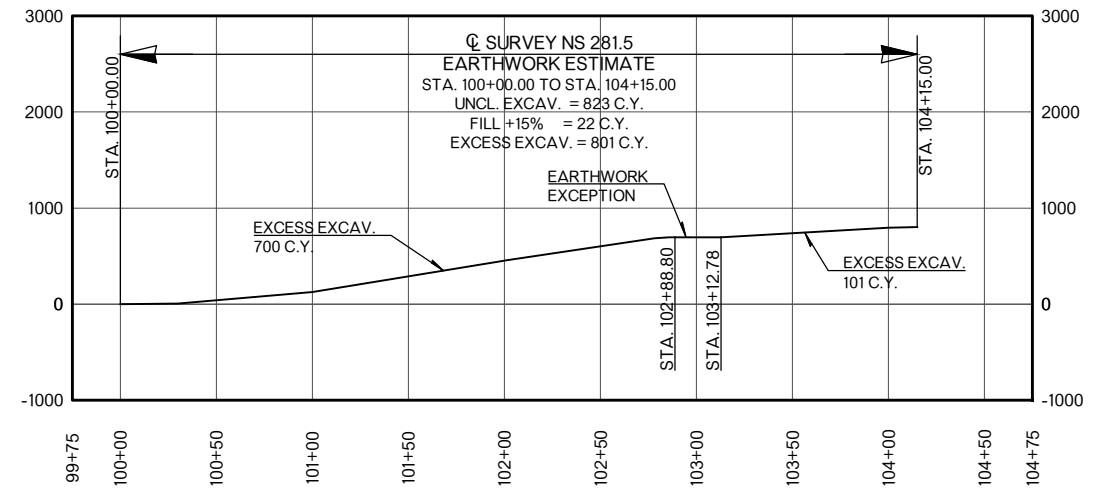
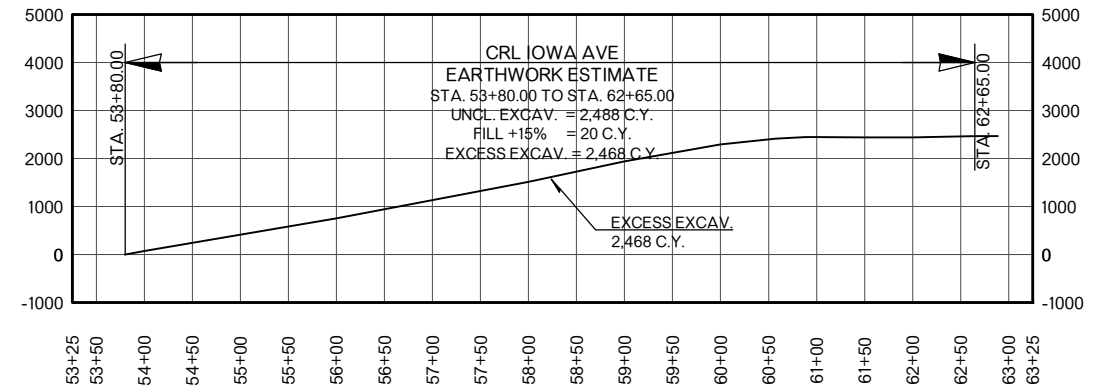
MASS DIAGRAMS



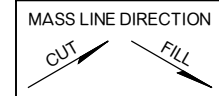
MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTIONS FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.



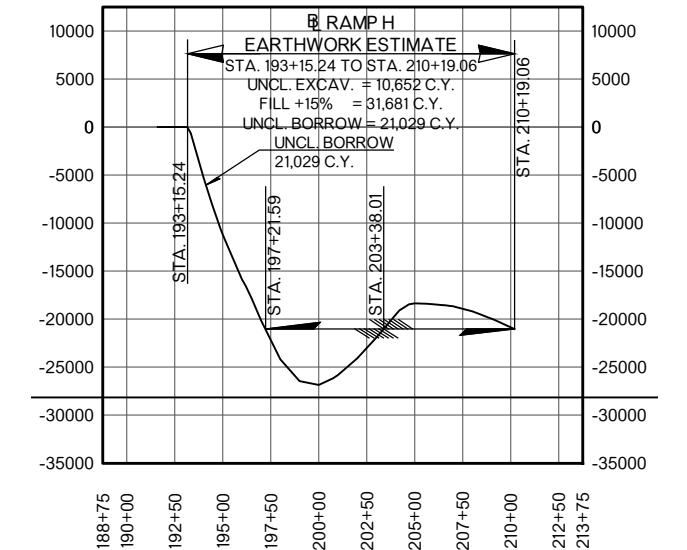
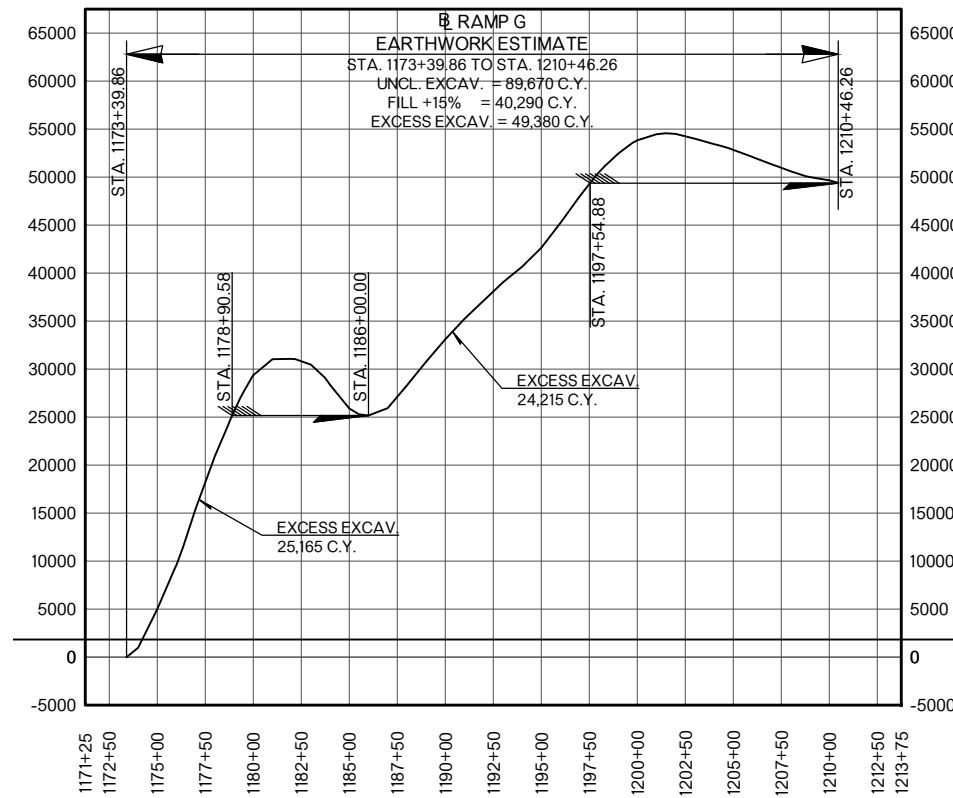
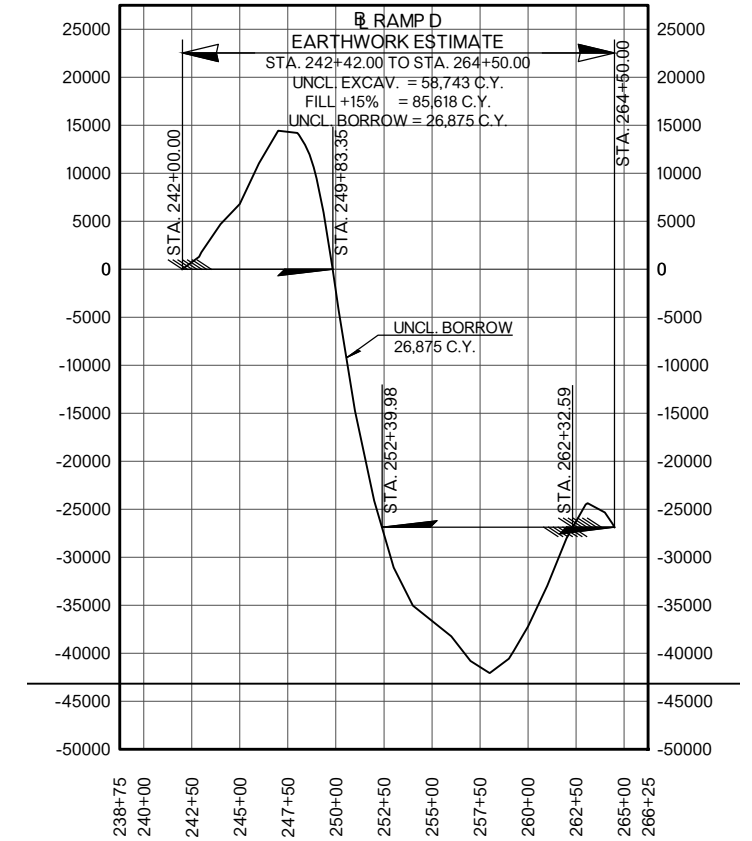
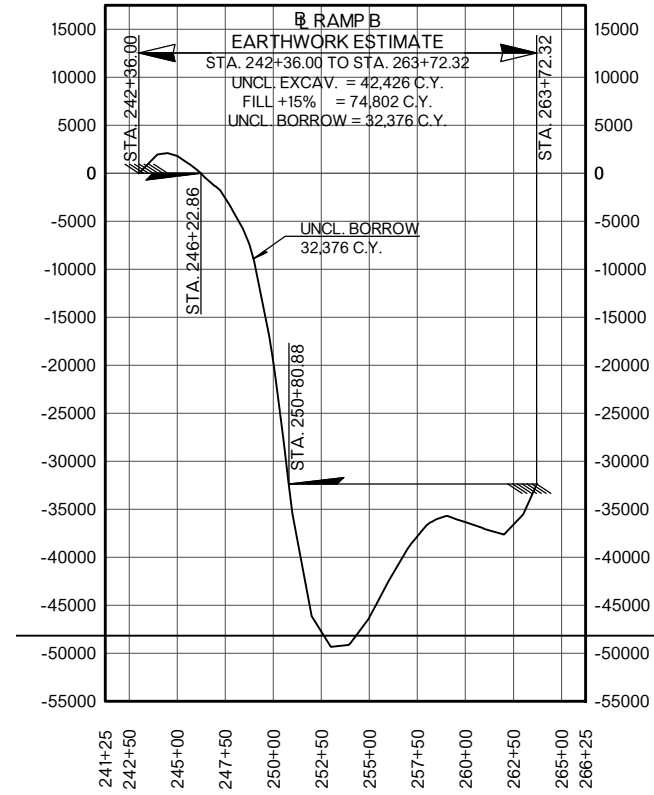
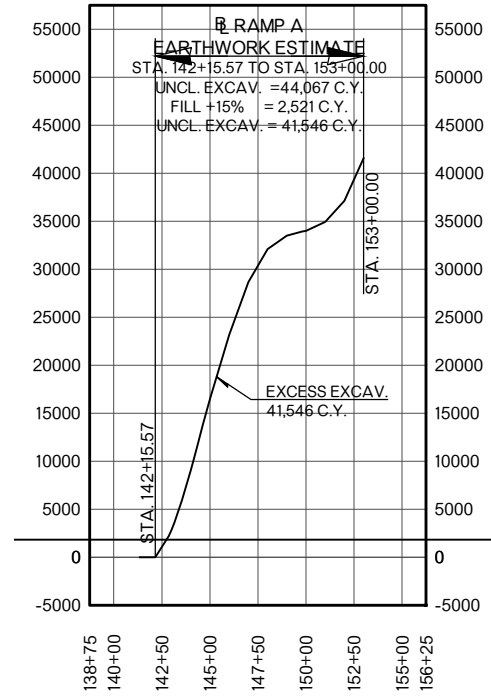
MASS DIAGRAMS



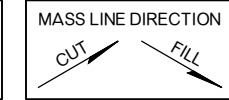
MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTIONS FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.



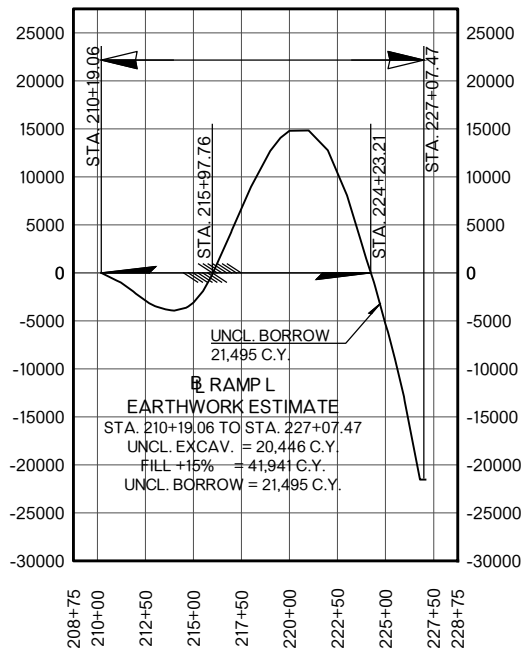
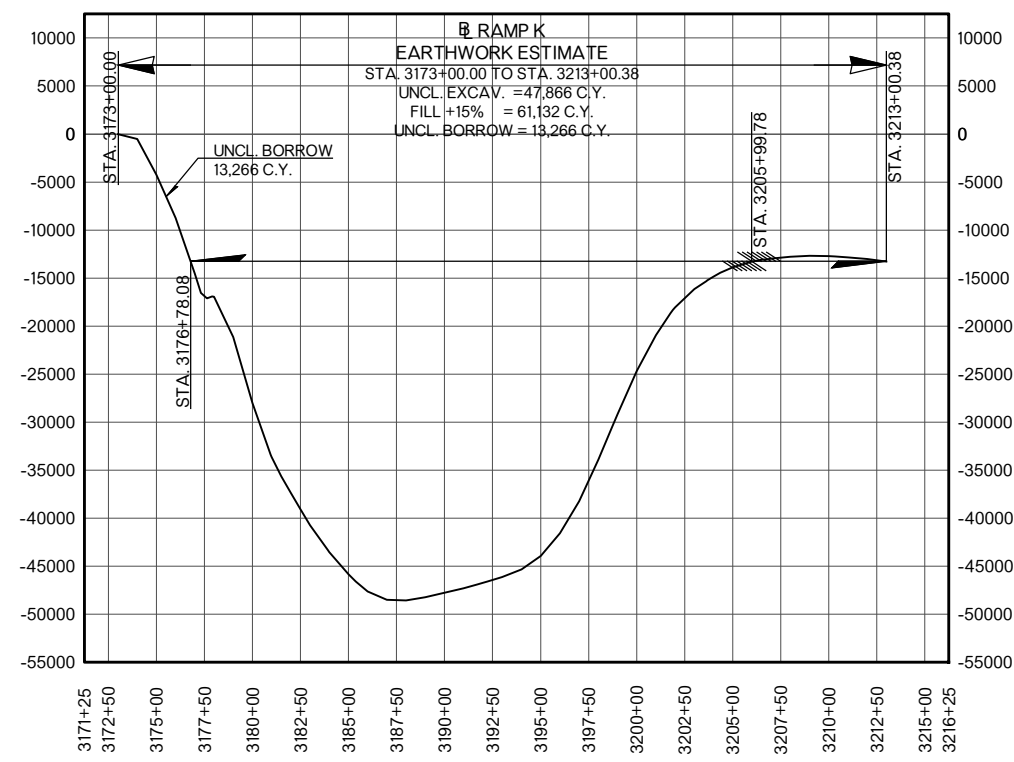
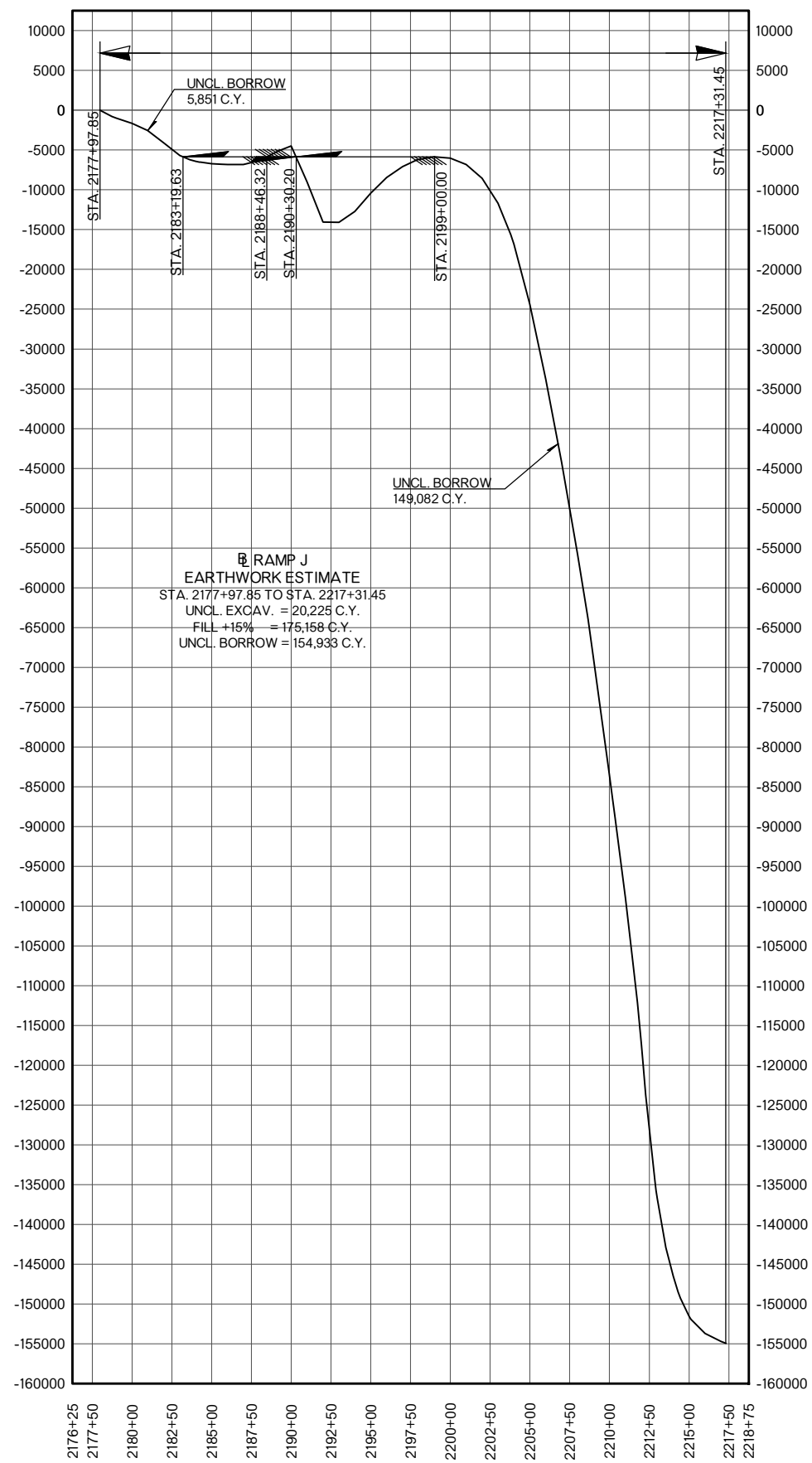
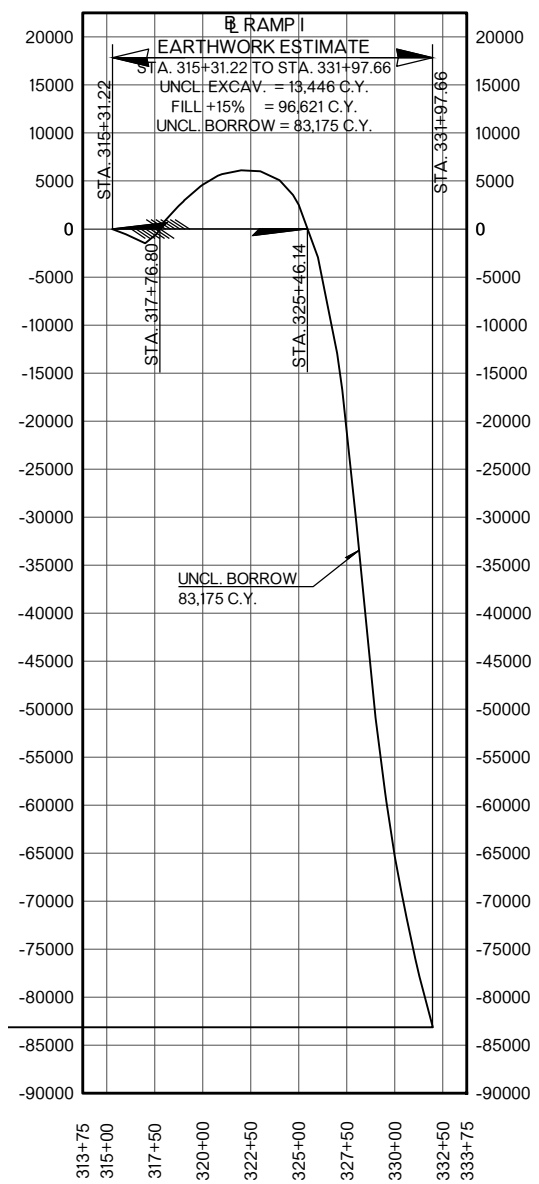
MASS DIAGRAMS



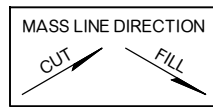
MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTIONS FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.



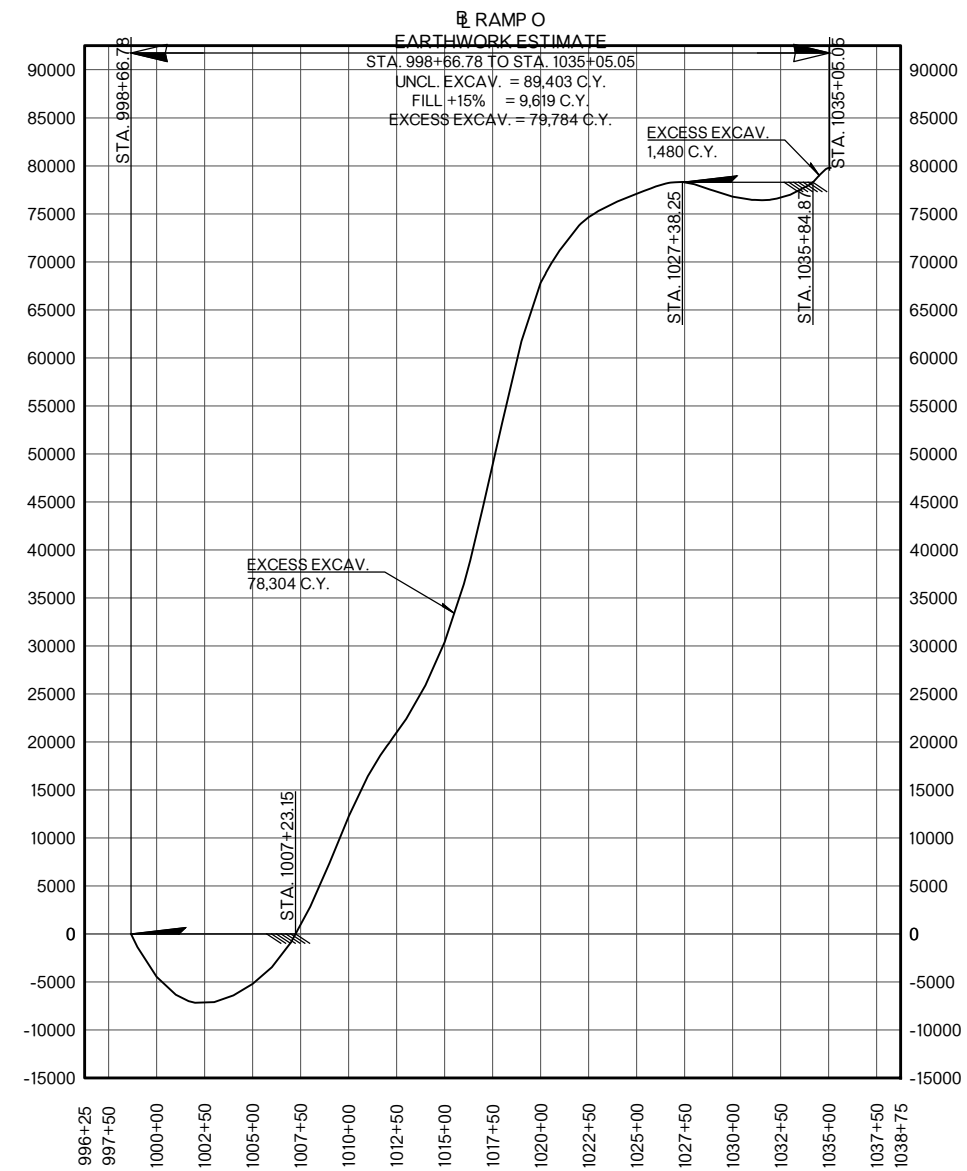
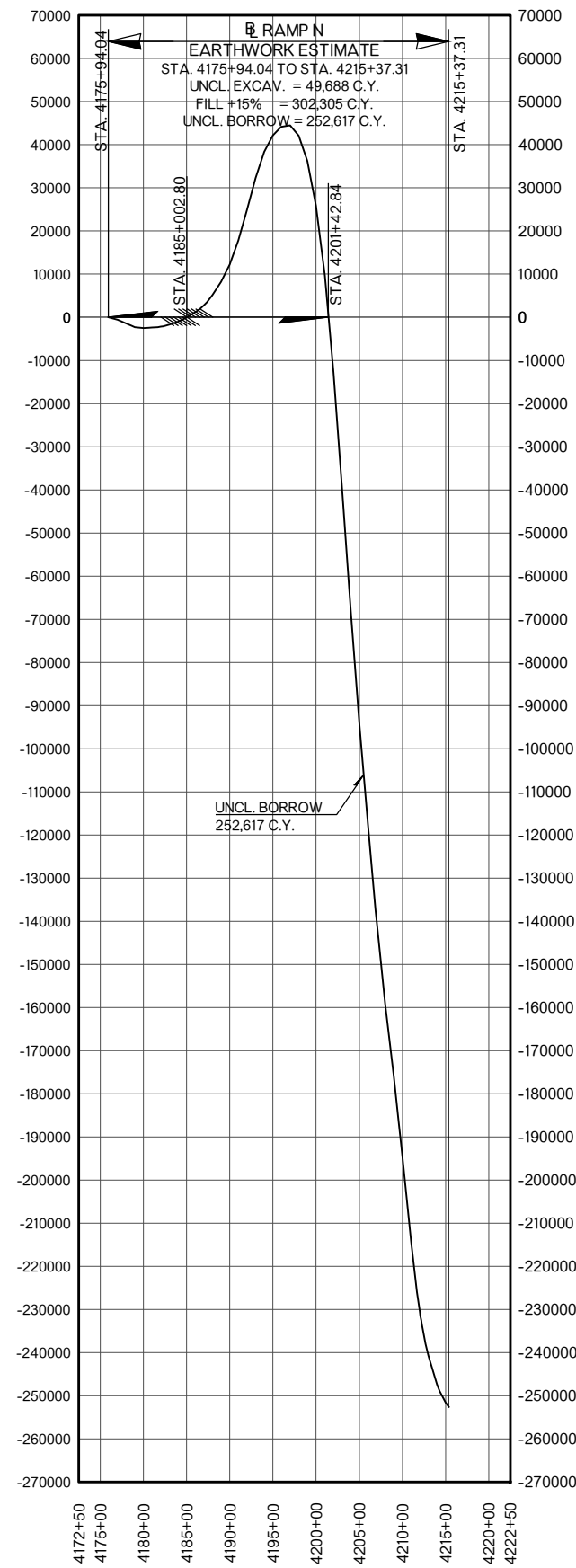
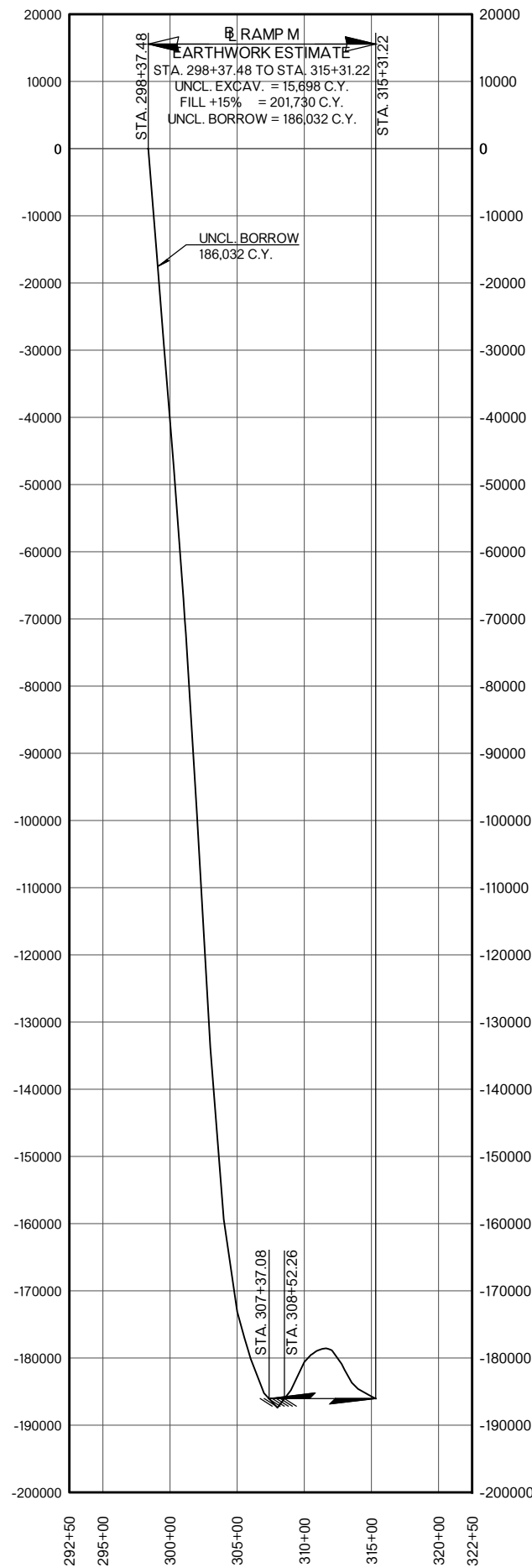
MASS DIAGRAMS



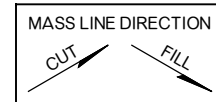
MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTIONS FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.



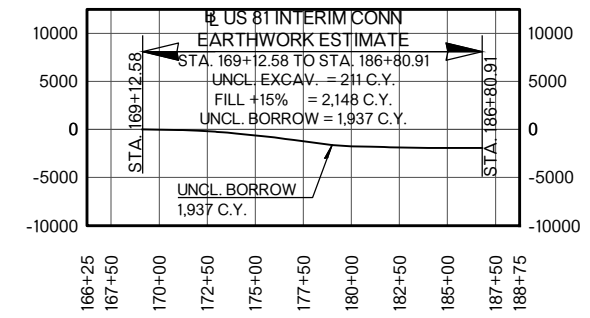
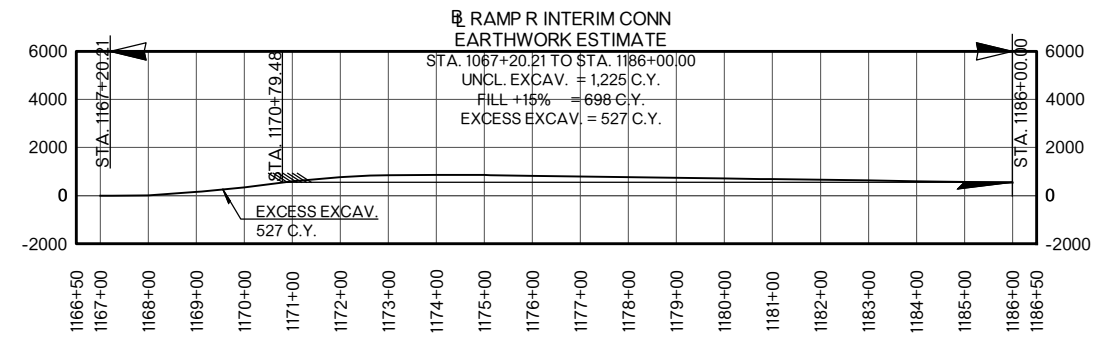
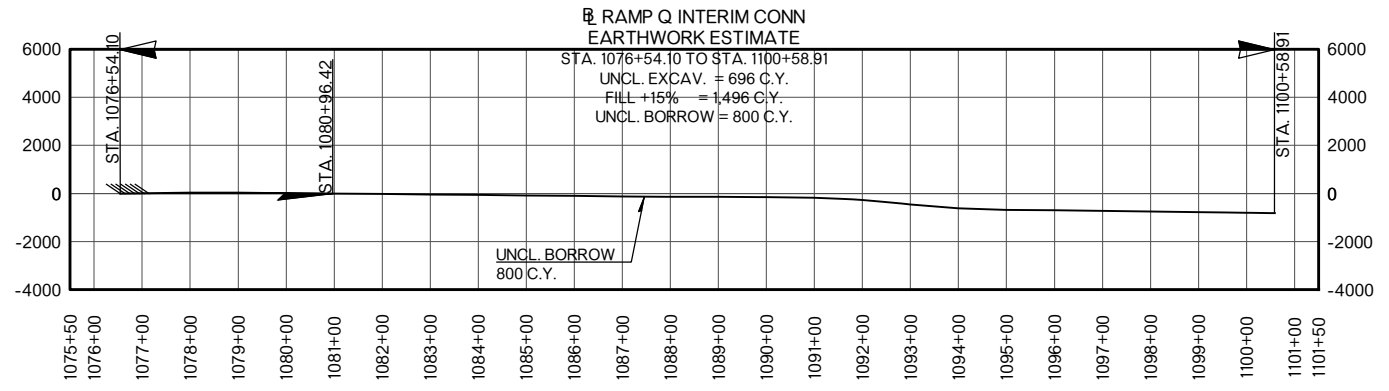
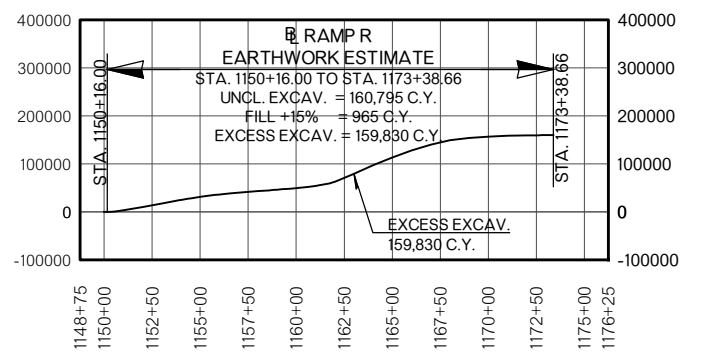
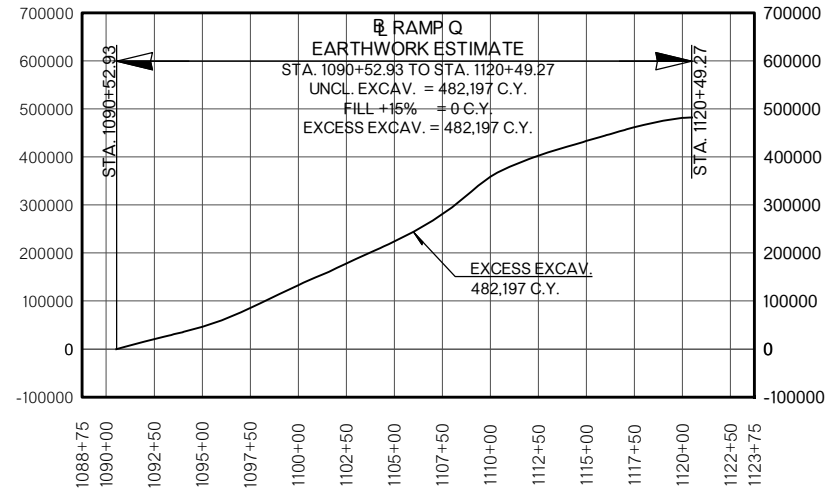
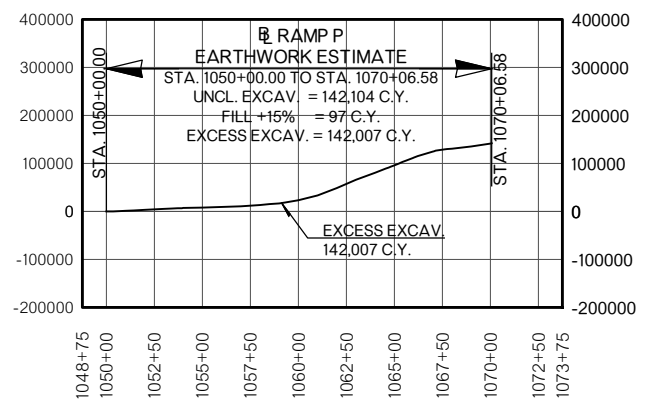
MASS DIAGRAMS



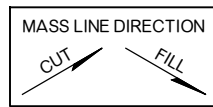
MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTIONS FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.



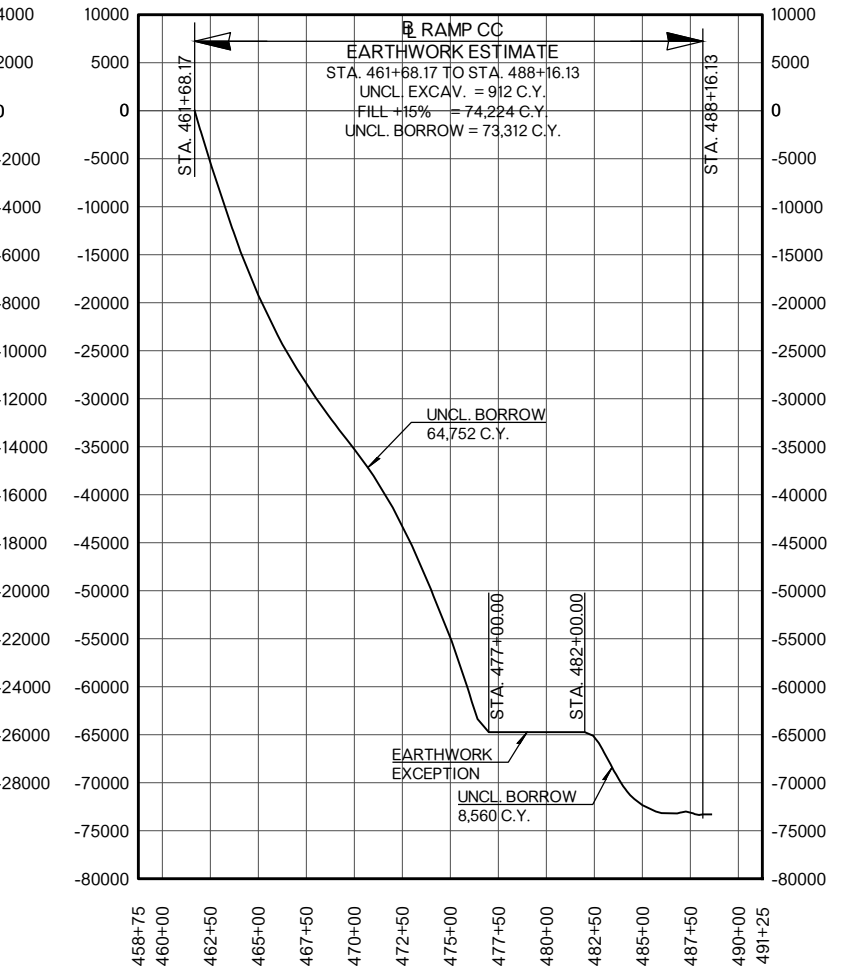
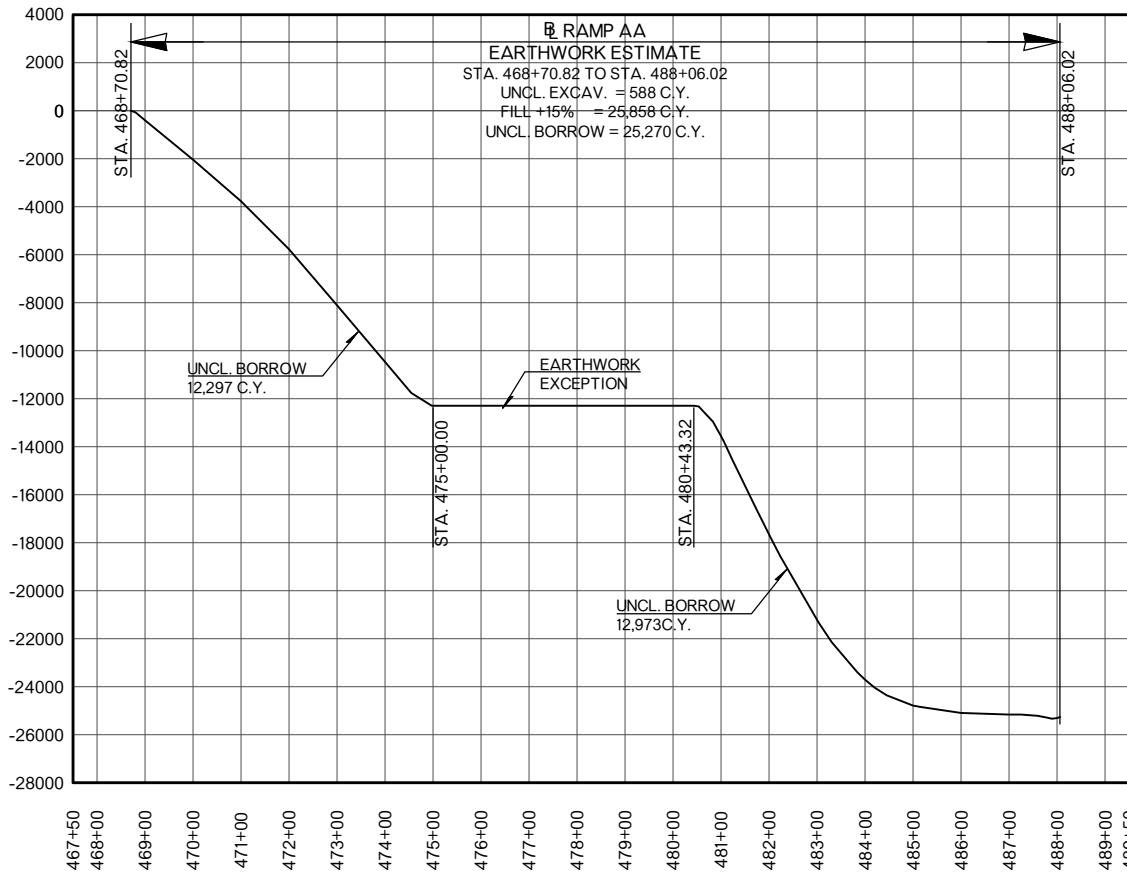
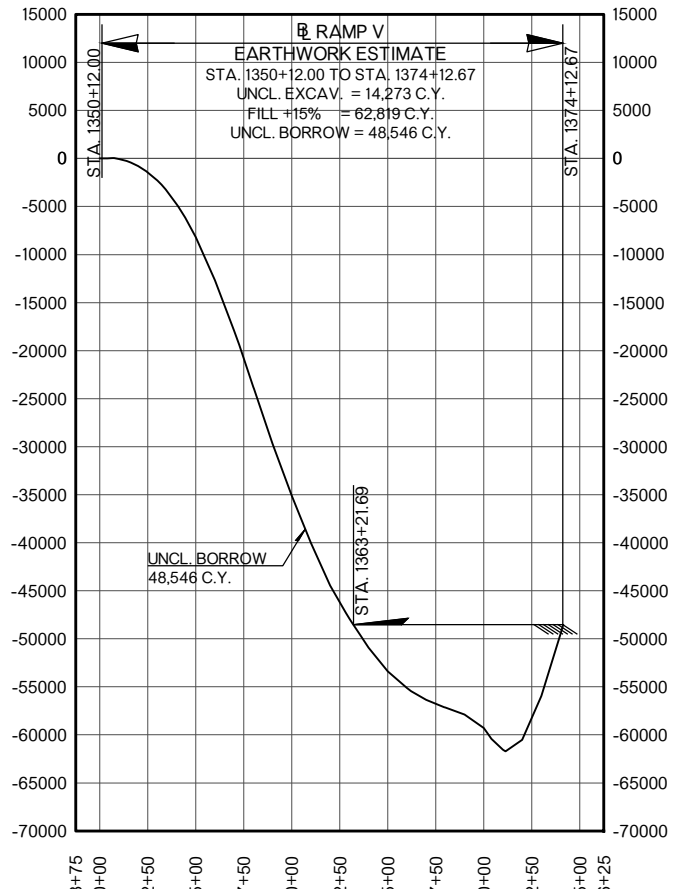
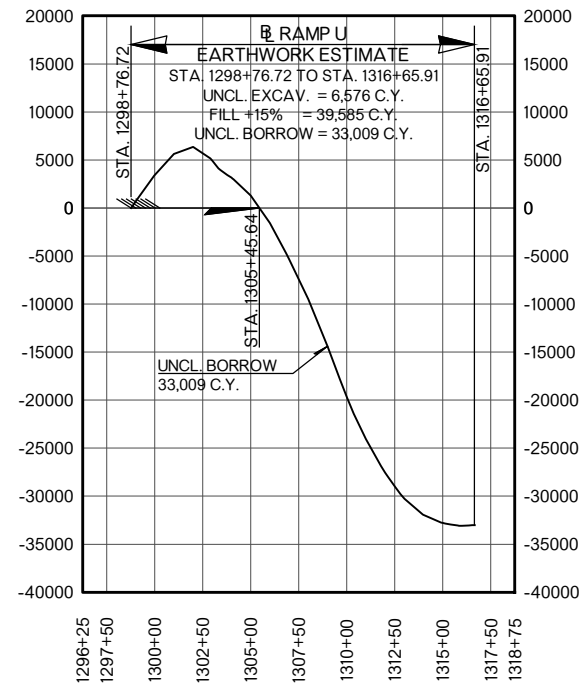
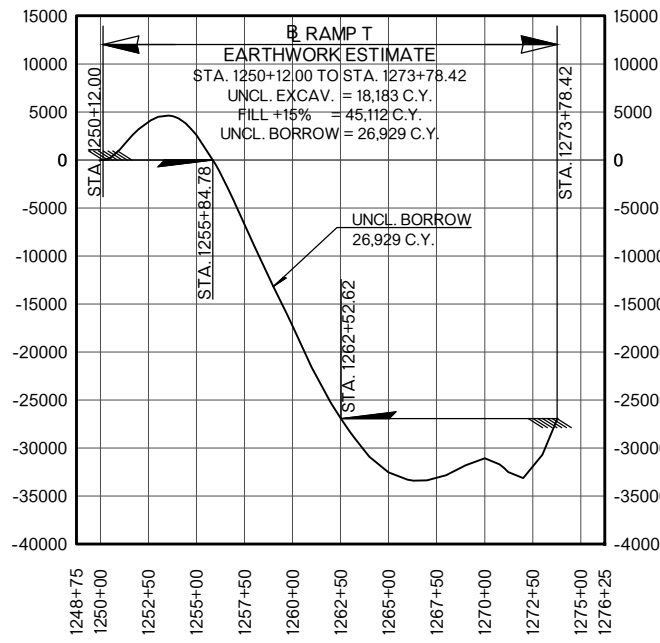
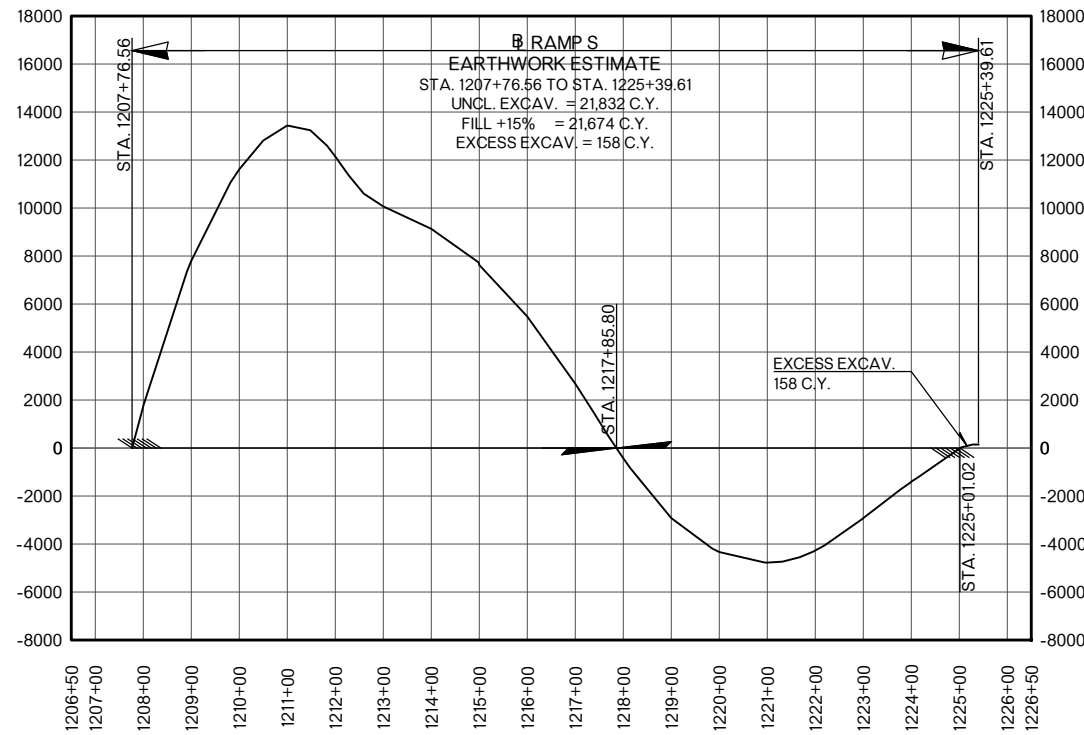
MASS DIAGRAMS



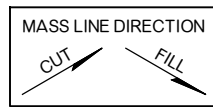
MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTIONS FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.



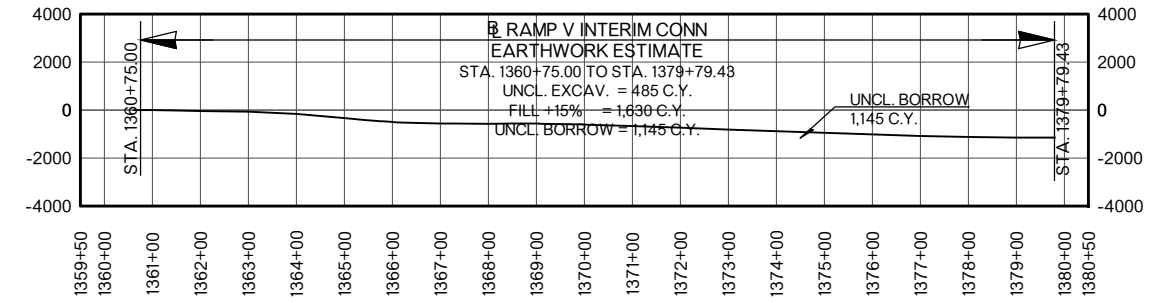
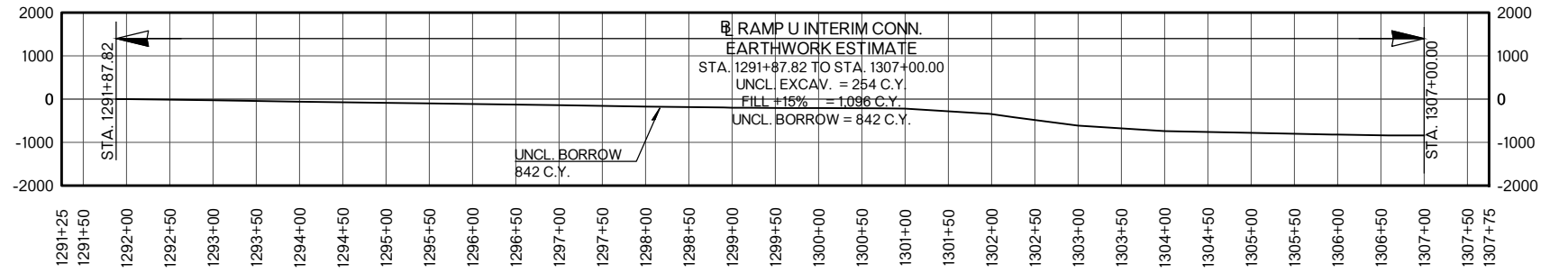
MASS DIAGRAMS



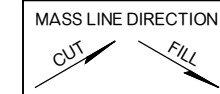
MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTIONS FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.



MASS DIAGRAMS



MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTIONS FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.



MASS DIAGRAMS

LOAD AND RESISTANCE FACTOR DESIGN

DESIGN DATA

CLASS "AA" CONCRETE F'C = 4,000 PSI
REINFORCING STEEL FY = 60,000 PSI

DESIGN: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 8TH EDITION.

NOTES

ALL CONSTRUCTION AND MATERIAL REQUIREMENTS SHALL BE IN ACCORDANCE WITH THE 2009 ODOT STANDARD SPECIFICATIONS, EXCEPT AS MODIFIED BY THE PLANS AND SPECIAL PROVISIONS.

ALL CONCRETE EDGES SHALL HAVE A 1 1/2" CHAMFER UNLESS OTHERWISE SHOWN OR NOTED. ALL CHAMFER STRIPS SHALL BE SIZED LUMBER.

THE CONTRACTOR SHALL MAINTAIN DRAINAGE AT ALL TIMES DURING CONSTRUCTION.

SEE ROADWAY PLAN & PROFILE SHEETS AND CROSS-SECTION SHEETS FOR GRADING DETAILS.

ALL REINFORCING STEEL SHALL HAVE 2" MINIMUM CLEARANCE UNLESS OTHERWISE SHOWN ON PLANS.

TRANSVERSE CONSTRUCTION JOINTS SHALL BE SPACED AT 60 FT. MAX.

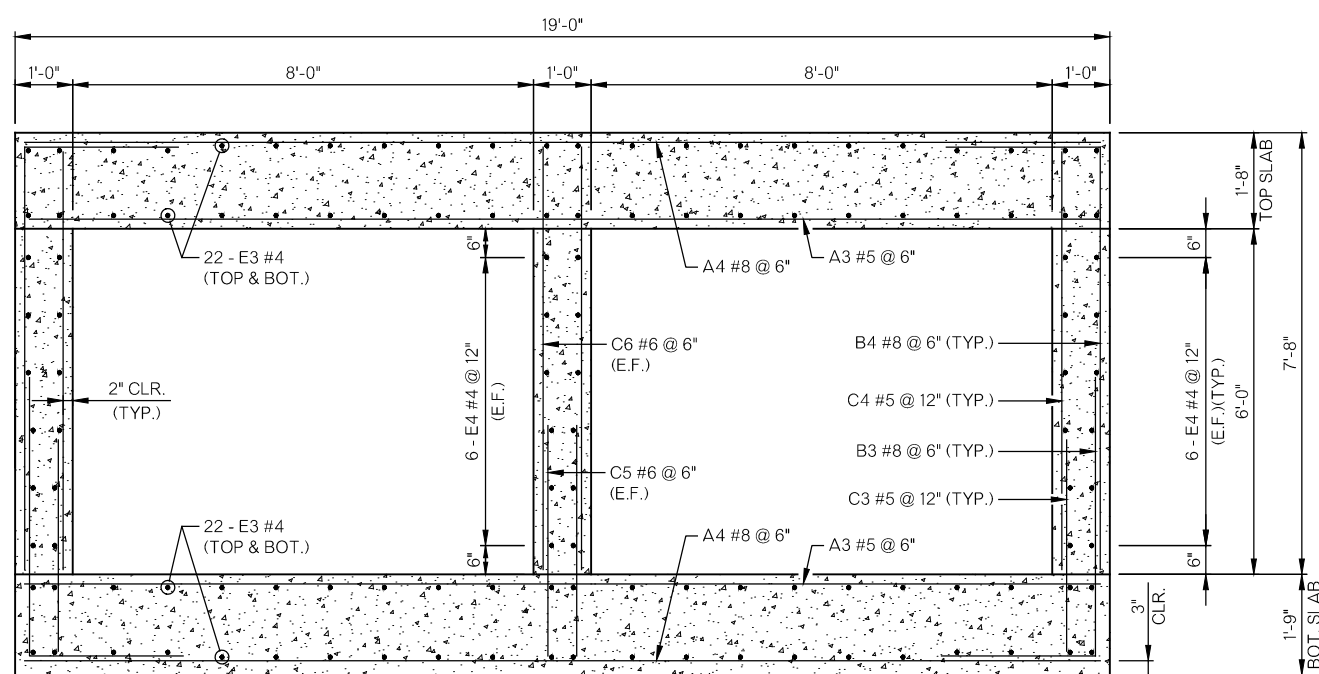
ALL COST FOR CLASS "AA" CONCRETE, LABOR, MATERIALS, EQUIPMENT AND INCIDENTALS NECESSARY TO PLACE CLASS "AA" CONCRETE SHALL BE INCLUDED IN PRICE BID PER CUBIC YARD OF "CLASS AA CONCRETE".

ALL COST FOR REINFORCING STEEL, LABOR, MATERIALS, TOOLS AND OTHER INCIDENTALS NECESSARY TO PLACE REINFORCING STEEL SHALL BE INCLUDED IN PRICE BID PER POUND OF "REINFORCING STEEL".

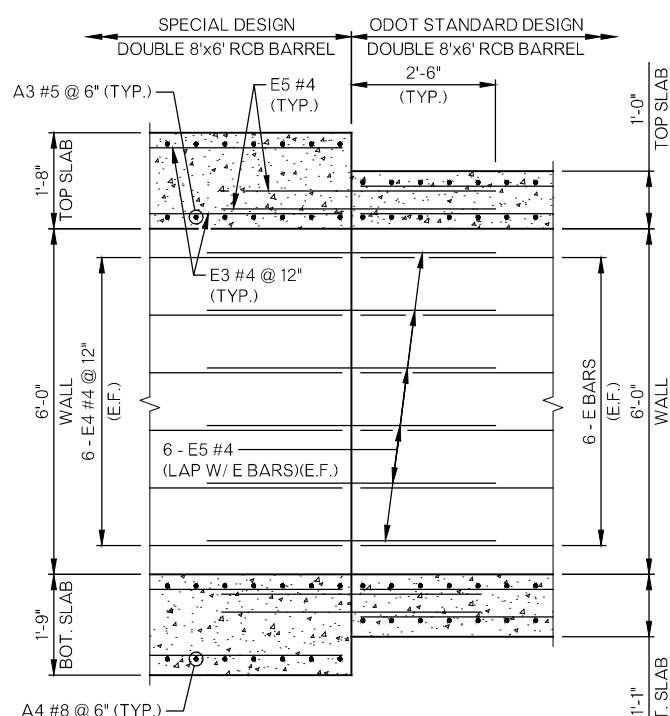
SOME REINFORCING STEEL BAR MARKS ARE REPEATED. THE STRUCTURAL COMPONENT SHOULD ACCOMPANY THE BAR MARK TO ENSURE THAT ALL BARS ARE PLACED WITH THE CORRECT STRUCTURAL COMPONENT.

THE FOLLOWING STANDARDS SHALL BE REQUIRED:

- RCB-C2-8(2-12)-01E
- RCB-E2-H6-0-1-01E
- RCB-E2-H6-0-2-01E
- RCB-CW2-D4-0-01E
- SBI-4-2



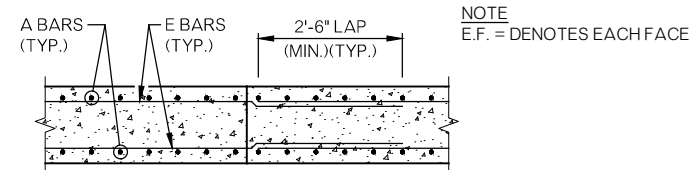
BARREL SECTION



SECTION THRU CONSTRUCTION JOINT BETWEEN SPECIAL AND STANDARD RCB BARRELS

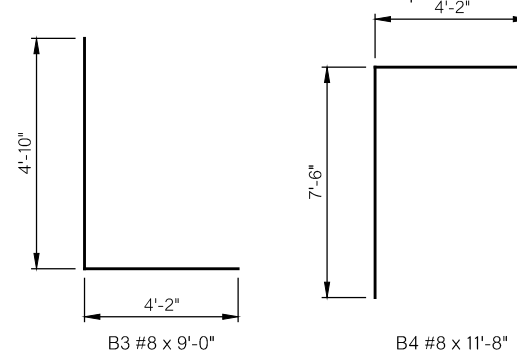
PRELIMINARY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SECTION THRU CONSTRUCTION JOINT IN SPECIAL RCB BARREL

NOTE
E.F. = DENOTES EACH FACE

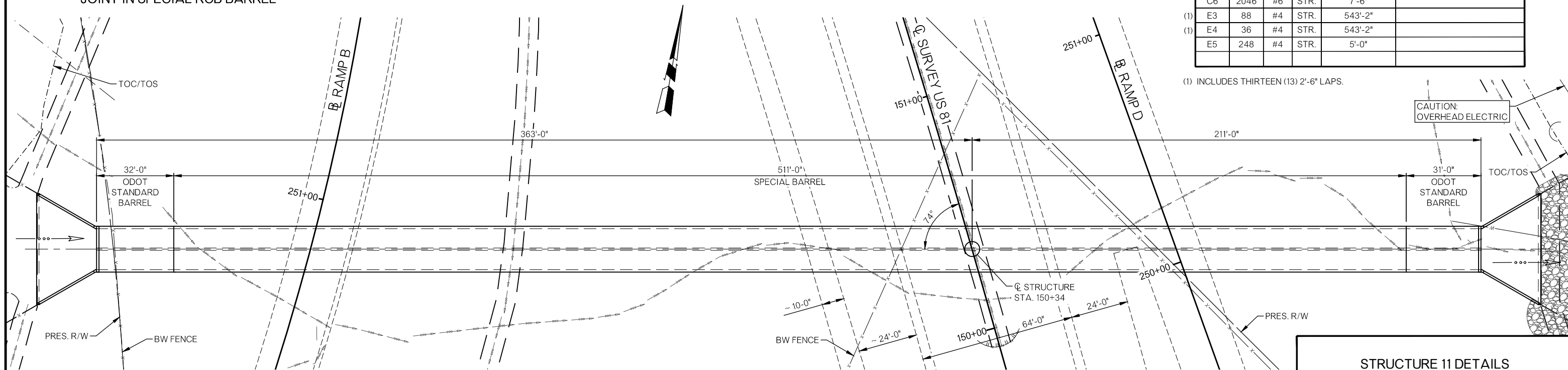


BAR BEND DETAILS

| STRUCTURE 11 QUANTITIES | | |
|-------------------------|------|------------|
| DESCRIPTION | UNIT | TOTAL |
| CLASS AA CONCRETE | CY | 1,789.50 |
| REINFORCING STEEL | LB | 376,740.00 |

| STRUCTURE 11 REINFORCING LIST | | | | | |
|-------------------------------|------|------|------|---------|---------|
| MARK | NO. | SIZE | FORM | LENGTH | REMARKS |
| PLAIN REINFORCING BARS | | | | | |
| A3 | 2046 | #5 | STR. | 18'-8" | |
| A4 | 2046 | #8 | STR. | 18'-8" | |
| B3 | 2046 | #8 | BNT. | 9'-0" | |
| B4 | 2046 | #8 | BNT. | 11'-8" | |
| C3 | 1024 | #5 | STR. | 3'-8" | |
| C4 | 1024 | #5 | STR. | 7'-6" | |
| C5 | 2046 | #6 | STR. | 4'-0" | |
| C6 | 2046 | #6 | STR. | 7'-6" | |
| (1) E3 | 88 | #4 | STR. | 543'-2" | |
| (1) E4 | 36 | #4 | STR. | 543'-2" | |
| E5 | 248 | #4 | STR. | 5'-0" | |

(1) INCLUDES THIRTEEN (13) 2'-6" LAPS.



STRUCTURE 11 PLAN

STRUCTURE 11 DETAILS

LOAD AND RESISTANCE FACTOR DESIGN

DESIGN DATA

CLASS "AA" CONCRETE F'C = 4,000 PSI
REINFORCING STEEL FY = 60,000 PSI

DESIGN: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 8TH EDITION.

NOTES

ALL CONSTRUCTION AND MATERIAL REQUIREMENTS SHALL BE IN ACCORDANCE WITH THE 2009 ODOT STANDARD SPECIFICATIONS, EXCEPT AS MODIFIED BY THE PLANS AND SPECIAL PROVISIONS.

ALL CONCRETE EDGES SHALL HAVE A 1 1/2" CHAMFER UNLESS OTHERWISE SHOWN OR NOTED. ALL CHAMFER STRIPS SHALL BE SIZED LUMBER.

THE CONTRACTOR SHALL MAINTAIN DRAINAGE AT ALL TIMES DURING CONSTRUCTION.

SEE ROADWAY PLAN & PROFILE SHEETS AND CROSS-SECTION SHEETS FOR GRADING DETAILS.

ALL REINFORCING STEEL SHALL HAVE 2" MINIMUM CLEARANCE UNLESS OTHERWISE SHOWN ON PLANS.

TRANSVERSE CONSTRUCTION JOINTS SHALL BE SPACED AT 60 FT. MAX.

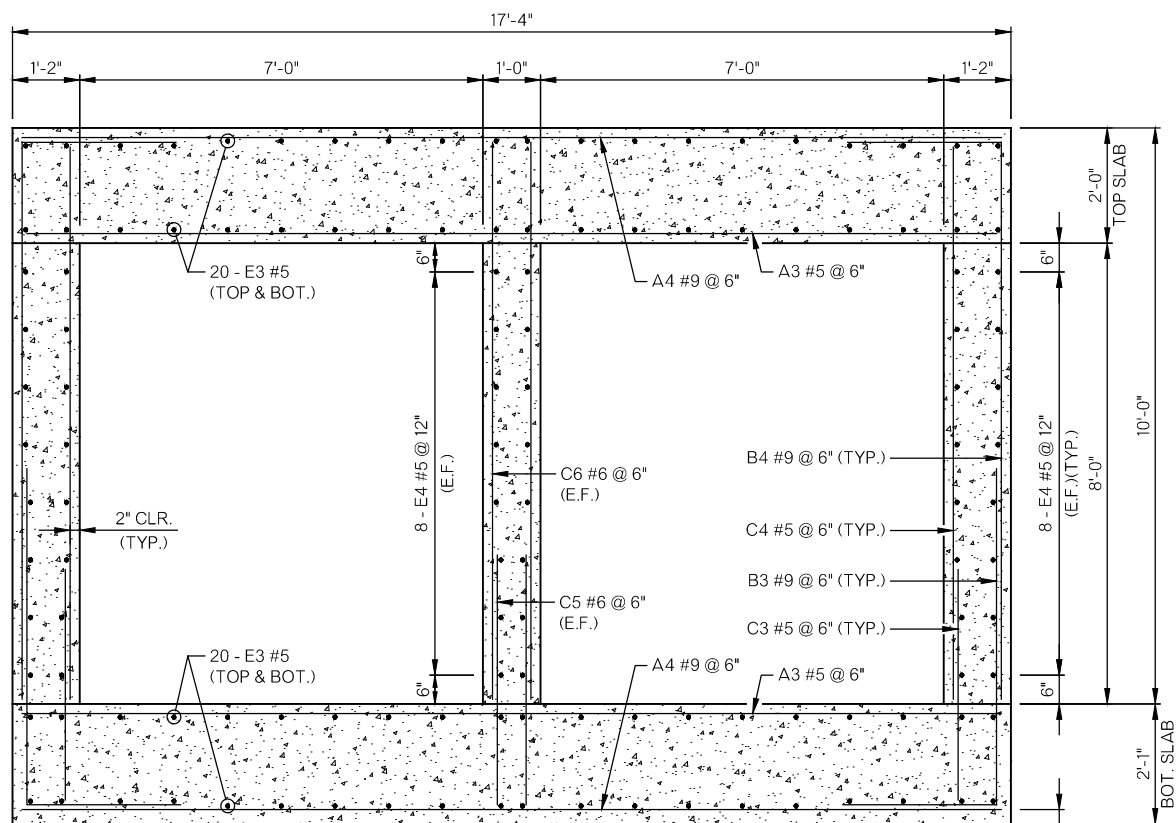
ALL COST FOR CLASS "AA" CONCRETE, LABOR, MATERIALS, EQUIPMENT AND INCIDENTALS NECESSARY TO PLACE CLASS "AA" CONCRETE SHALL BE INCLUDED IN PRICE BID PER CUBIC YARD OF "CLASS AA CONCRETE".

ALL COST FOR REINFORCING STEEL, LABOR, MATERIALS, TOOLS AND OTHER INCIDENTALS NECESSARY TO PLACE REINFORCING STEEL SHALL BE INCLUDED IN PRICE BID PER POUND OF "REINFORCING STEEL".

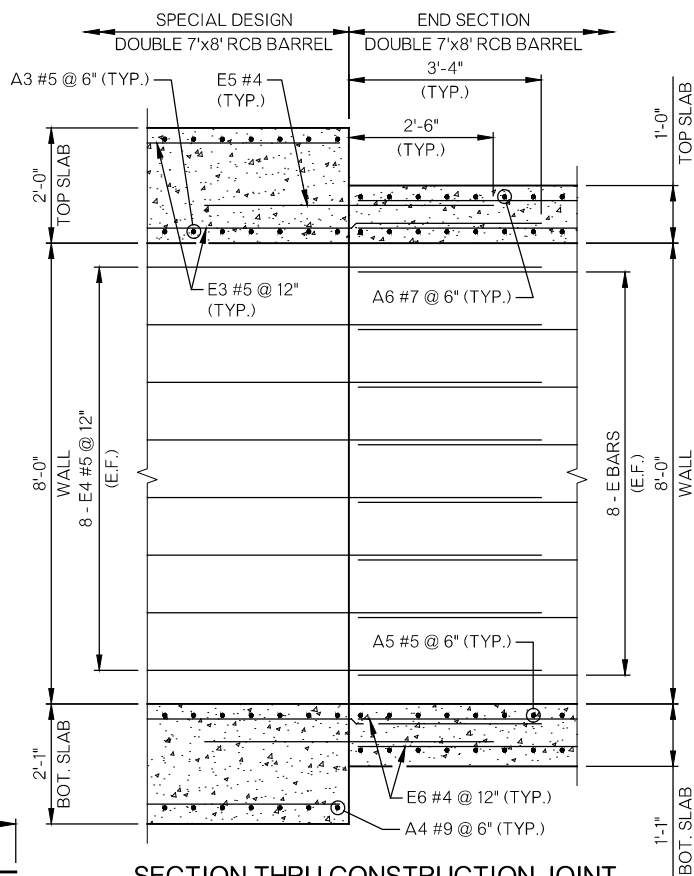
SOME REINFORCING STEEL BAR MARKS ARE REPEATED. THE STRUCTURAL COMPONENT SHOULD ACCOMPANY THE BAR MARK TO ENSURE THAT ALL BARS ARE PLACED WITH THE CORRECT STRUCTURAL COMPONENT.

THE FOLLOWING STANDARDS SHALL BE REQUIRED:

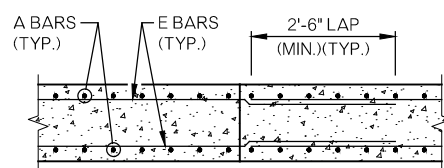
SBI-4-2



BARREL SECTION

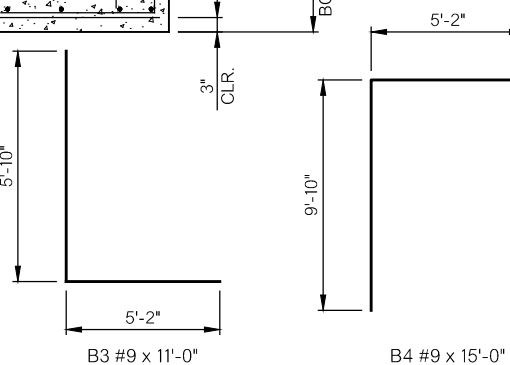


SECTION THRU CONSTRUCTION JOINT BETWEEN SPECIAL RCB BARREL AND END SECTION



SECTION THRU CONSTRUCTION JOINT IN SPECIAL RCB BARREL

NOTE
E.F. = DENOTES EACH FACE

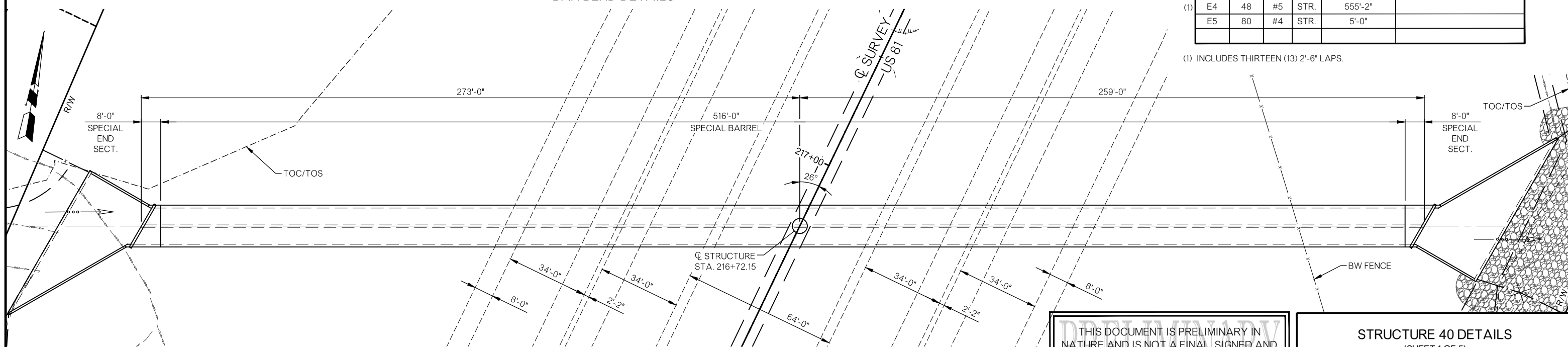


BAR BEND DETAILS

| STRUCTURE 40 QUANTITIES | | |
|-------------------------|------|------------|
| DESCRIPTION | UNIT | TOTAL |
| CLASS AA CONCRETE | CY | 2,050.10 |
| REINFORCING STEEL | LB | 511,940.00 |

| STRUCTURE 40 BARREL REINFORCING LIST | | | | | |
|---|------|------|------|---------|---------|
| MARK | NO. | SIZE | FORM | LENGTH | REMARKS |
| PLAIN REINFORCING BARS | | | | | |
| A3 | 2066 | #5 | STR. | 17'-0" | |
| A4 | 2066 | #9 | STR. | 17'-0" | |
| B3 | 2066 | #9 | BNT. | 11'-0" | |
| B4 | 2066 | #9 | BNT. | 15'-0" | |
| C3 | 2066 | #5 | STR. | 4'-0" | |
| C4 | 2066 | #5 | STR. | 9'-10" | |
| C5 | 2066 | #6 | STR. | 4'-4" | |
| C6 | 2066 | #6 | STR. | 9'-10" | |
| (1) E3 | 80 | #5 | STR. | 555'-2" | |
| (1) E4 | 48 | #5 | STR. | 555'-2" | |
| E5 | 80 | #4 | STR. | 5'-0" | |

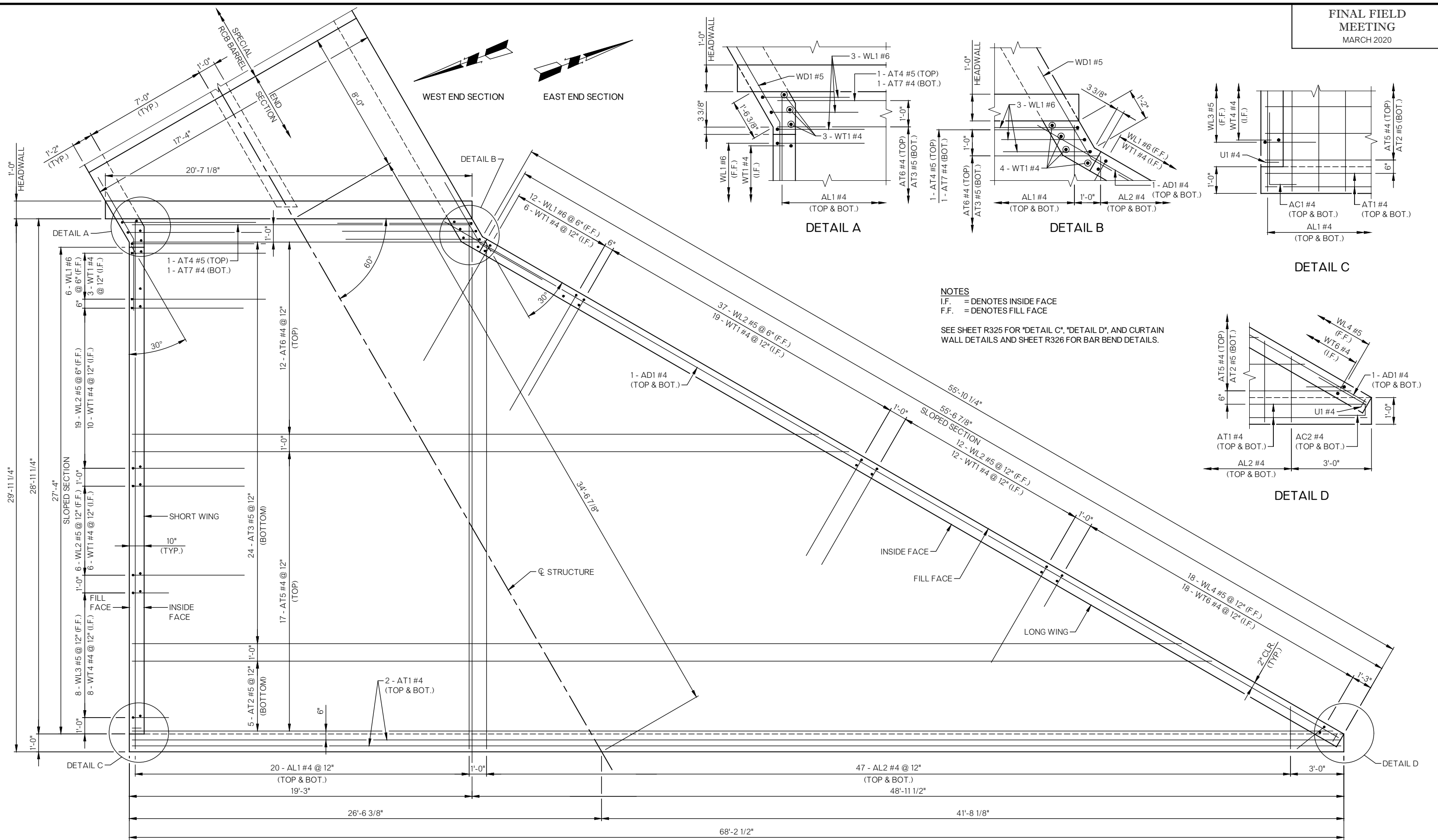
(1) INCLUDES THIRTEEN (13) 2'-6" LAPS.



STRUCTURE 40 PLAN

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

STRUCTURE 40 DETAILS
(SHEET 1 OF 5)



NOTES

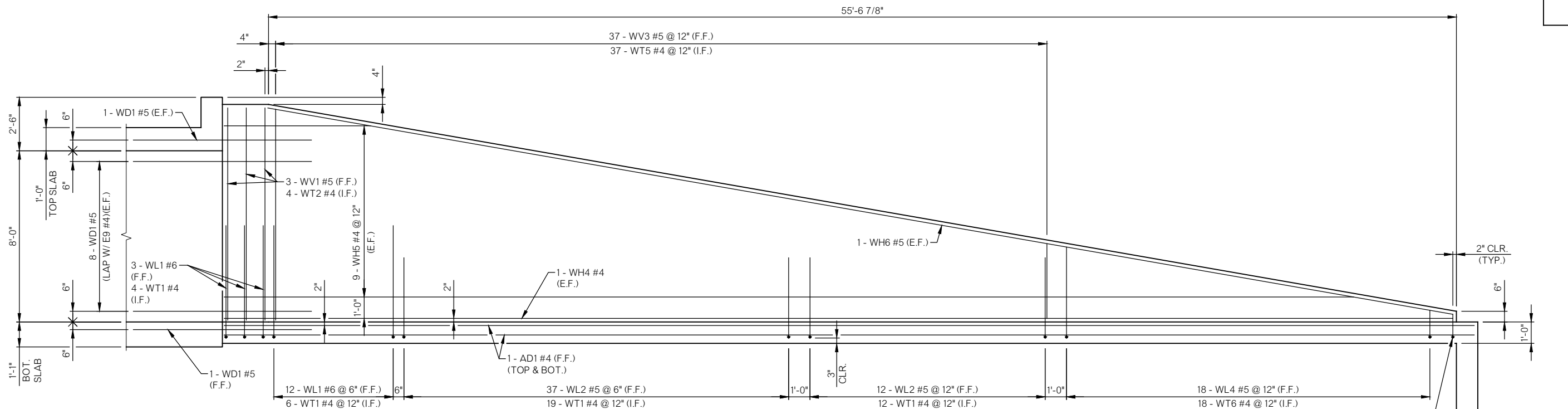
I.F. = DENOTES INSIDE FACE
F.F. = DENOTES FILL FACE

SEE SHEET R325 FOR "DETAIL C", "DETAIL D", AND CURTAIN WALL DETAILS AND SHEET R326 FOR BAR BEND DETAILS.

END SECTION APRON REINFORCING AND LAYOUT PLAN
(END CONNECTION REINFORCING NOT SHOWN FOR CLARITY)

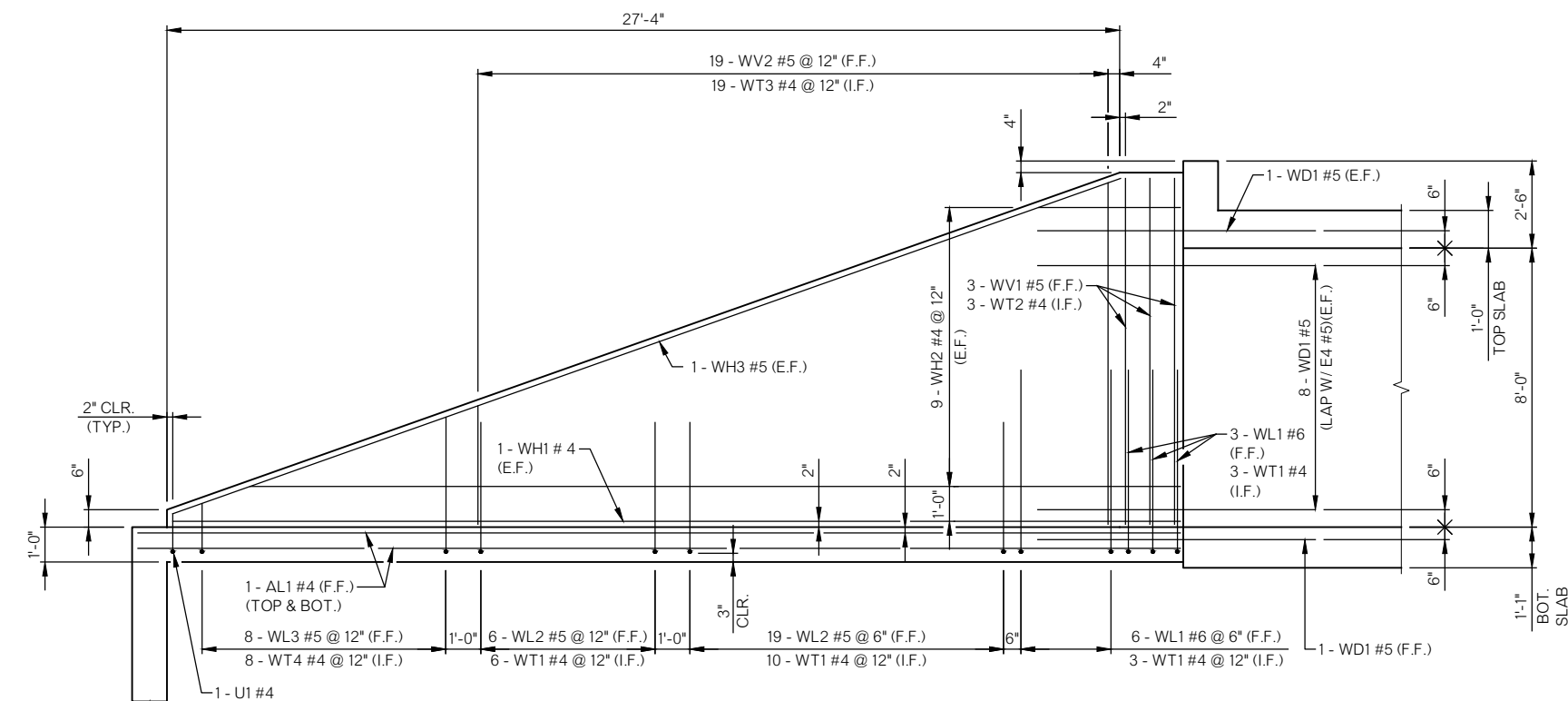
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

STRUCTURE 40 DETAILS
(SHEET 2 OF 5)



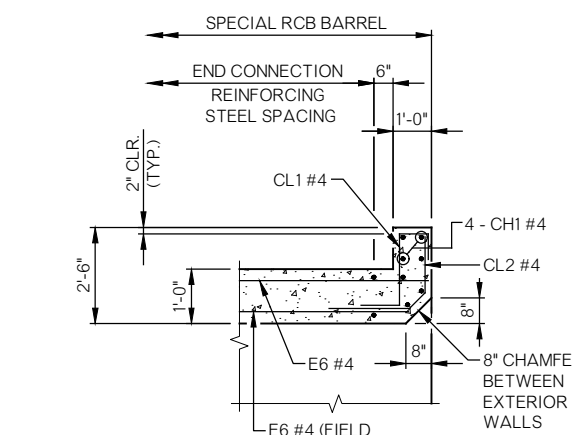
LONG WING ELEVATION

NOTES
E.F. = DENOTES EACH FACE
I.F. = DENOTES INSIDE FACE
F.F. = DENOTES FILL FACE

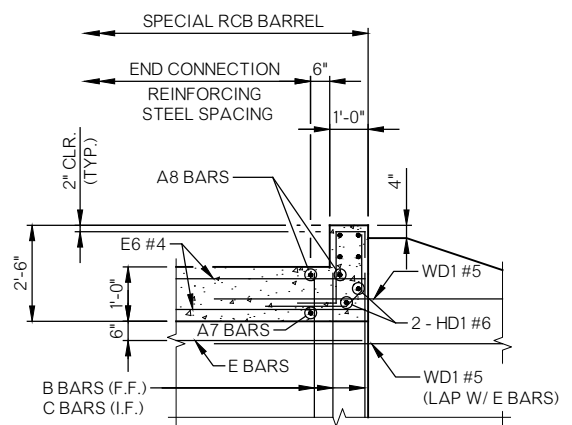


SHORT WING ELEVATION

SEE CURTAIN WALL DETAIL ON SHEET R325

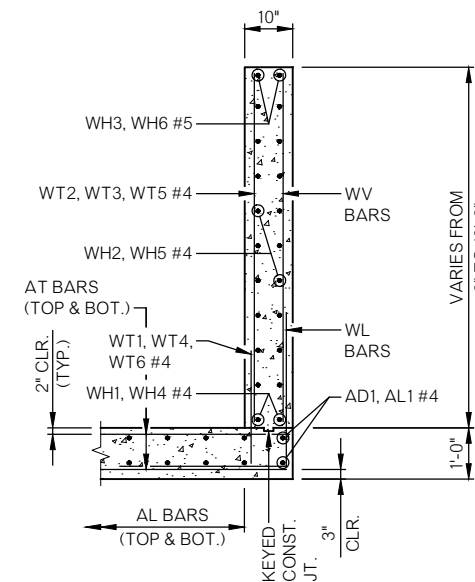


HEADWALL DETAIL AT MIDSPAN



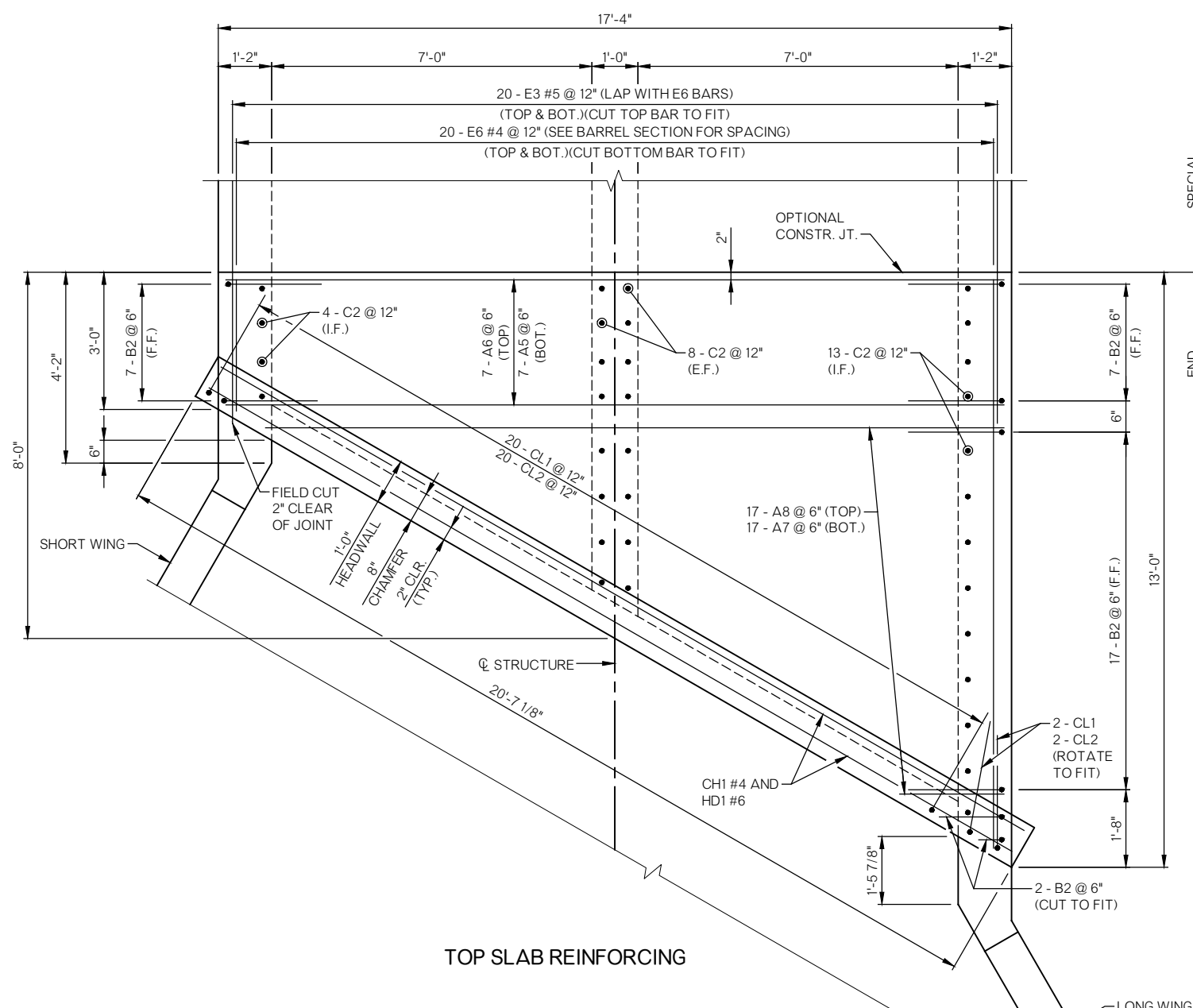
HEADWALL DETAIL AT EXTERIOR WALL

SEE CURTAIN WALL DETAIL ON SHEET R325

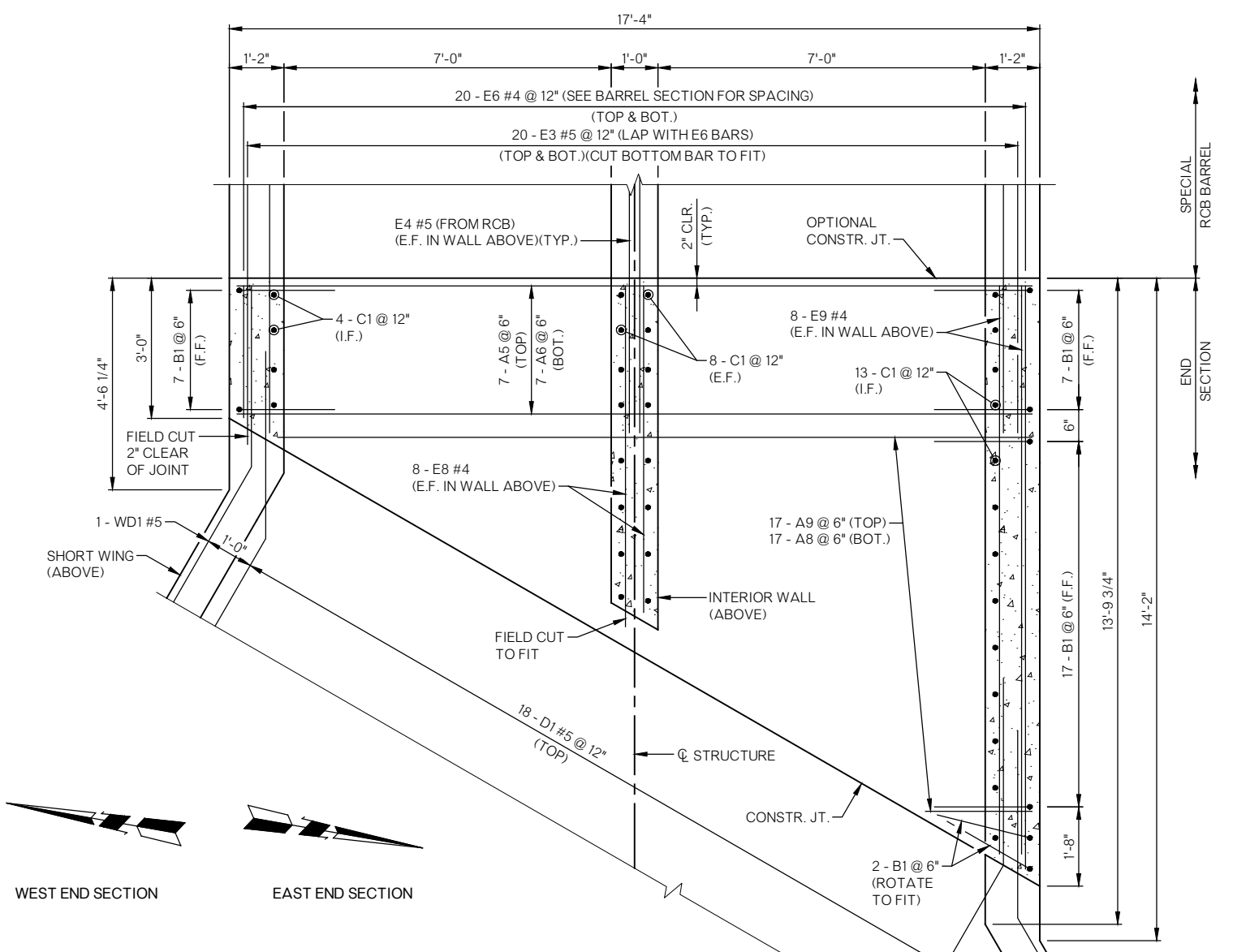


TYPICAL SECTION THRU WING

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

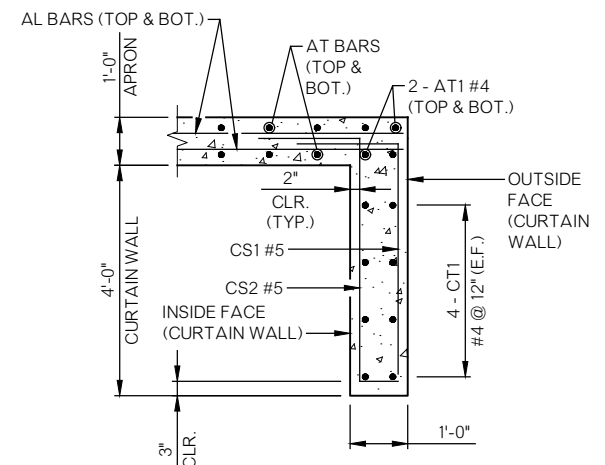


TOP SLAB REINFORCING

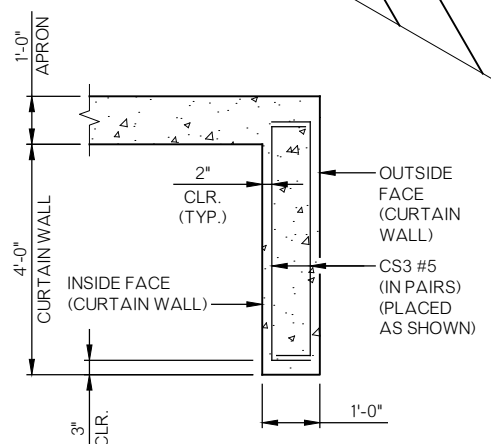


BOTTOM SLAB REINFORCING

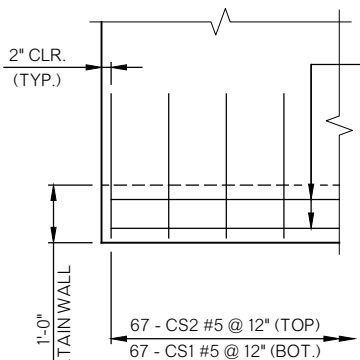
NOTES
E.F. = DENOTES EACH FACE
I.F. = DENOTES INSIDE FACE
F.F. = DENOTES FILL FACE



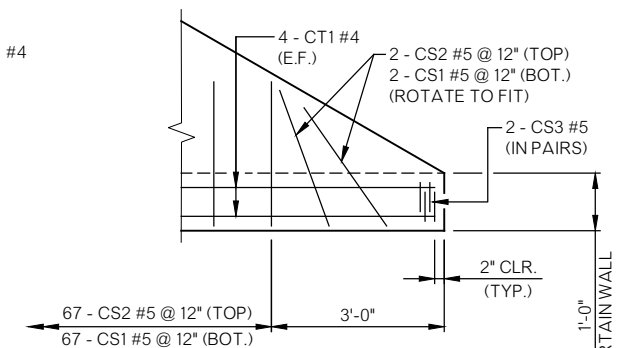
CURTAIN WALL DETAIL



CURTAIN WALL END DETAIL



DETAIL C
(CURTAIN WALL)



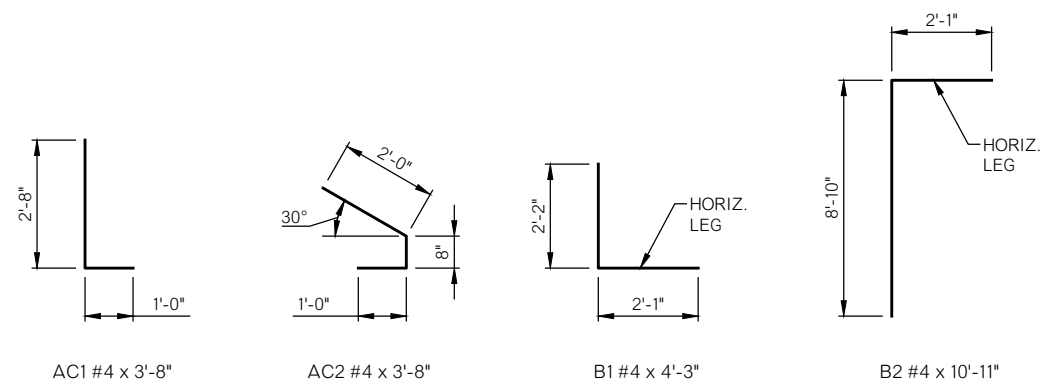
DETAIL D
(CURTAIN WALL)

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

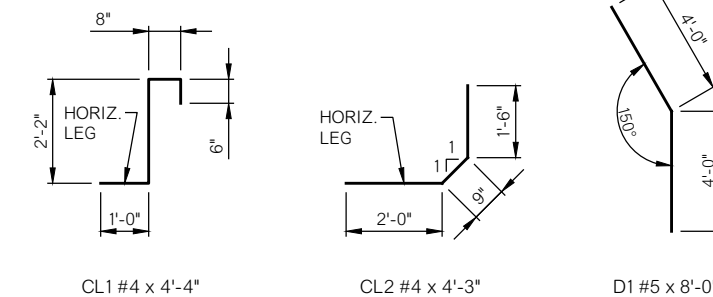
STRUCTURE 40 DETAILS
(SHEET 4 OF 5)

| STRUCTURE 40 APRON REINFORCING LIST (INCLUDES HEADWALL AND CURTAIN WALL) | | | | | |
|--|-----|------|------|-------------|--------------------|
| MARK | NO. | SIZE | FORM | LENGTH | REMARKS |
| PLAIN REINFORCING BARS | | | | | |
| AC1 | 2 | #4 | BNT. | 3'-8" | |
| AC2 | 2 | #4 | BNT. | 3'-8" | |
| AD1 | 2 | #4 | STR. | 60'-3" | |
| AL1 | 40 | #4 | STR. | 29'-7" | |
| (1) AL2 | 94 | #4 | STR. | 15'-4" AVG. | 2'-4" TO 28'-4" |
| (1) AT1 | 4 | #4 | STR. | 70'-4" | |
| (1) AT2 | 5 | #5 | STR. | 66'-6" AVG. | 63'-1" TO 69'-11" |
| AT3 | 24 | #5 | STR. | 39'-4" AVG. | 19'-10" TO 58'-10" |
| AT4 | 1 | #5 | STR. | 19'-1" | |
| (1) AT5 | 17 | #4 | STR. | 56'-4" AVG. | 42'-9" TO 69'-11" |
| AT6 | 12 | #4 | STR. | 29'-2" AVG. | 19'-10" TO 38'-6" |
| AT7 | 1 | #4 | STR. | 19'-1" | |
| CH1 | 4 | #4 | STR. | 20'-3" | |
| CL1 | 22 | #4 | BNT. | 4'-4" | |
| CL2 | 22 | #4 | BNT. | 4'-3" | |
| CS1 | 69 | #5 | BNT. | 5'-9" | |
| CS2 | 69 | #5 | BNT. | 7'-0" | |
| CS3 | 4 | #5 | BNT. | 5'-11" | |
| (1) CT1 | 8 | #4 | STR. | 70'-4" | |
| HD1 | 2 | #6 | STR. | 20'-3" | |

| STRUCTURE 40 END CONNECTION REINFORCING LIST | | | | | |
|---|-----|------|------|-------------|-----------------|
| MARK | NO. | SIZE | FORM | LENGTH | REMARKS |
| PLAIN REINFORCING BARS | | | | | |
| A5 | 14 | #5 | STR. | 17'-0" | |
| A6 | 14 | #7 | STR. | 17'-0" | |
| A7 | 17 | #7 | STR. | 7'-10" AVG. | 11" TO 14'-9" |
| A8 | 34 | #7 | STR. | 9'-2" AVG. | 2'-3" TO 16'-1" |
| A9 | 17 | #7 | STR. | 9'-2" AVG. | 2'-3" TO 16'-1" |
| B1 | 33 | #4 | BNT. | 4'-3" | |
| B2 | 33 | #4 | BNT. | 10'-11" | |
| C1 | 33 | #4 | STR. | 2'-2" | |
| C2 | 33 | #4 | STR. | 8'-10" | |
| D1 | 18 | #5 | BNT. | 8'-0" | |
| E6 | 80 | #4 | STR. | 7'-7" AVG. | 2'-8" TO 12'-6" |
| E8 | 16 | #4 | STR. | 7'-0" | |
| E9 | 16 | #4 | STR. | 12'-1" | |

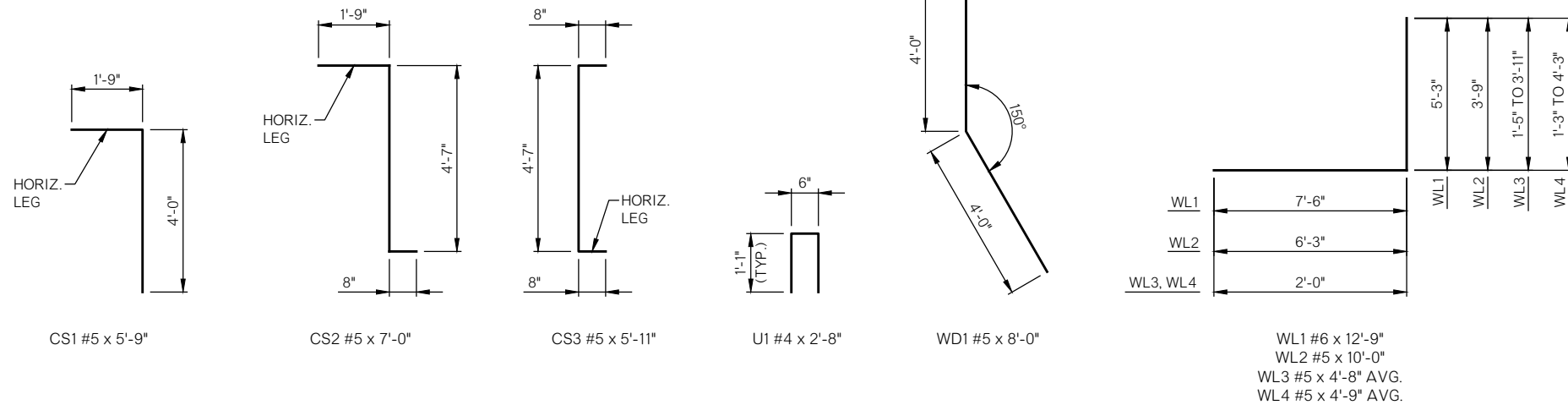


AC1 #4 x 3'-8" AC2 #4 x 3'-8" B1 #4 x 4'-3" B2 #4 x 10'-11"



CL1 #4 x 4'-4" CL2 #4 x 4'-3" D1 #5 x 8'-0"

| STRUCTURE 40 WING REINFORCING LIST | | | | | |
|---------------------------------------|-----|------|------|--------------|-------------------|
| MARK | NO. | SIZE | FORM | LENGTH | REMARKS |
| PLAIN REINFORCING BARS | | | | | |
| WD1 | 38 | #5 | BNT. | 8'-0" | |
| WH1 | 2 | #4 | STR. | 28'-4" | |
| (3) WH2 | 18 | #4 | STR. | 14'-9" AVG. | 3'-5" TO 26'-1" |
| WH3 | 2 | #5 | STR. | 28'-10" | |
| (1) WH4 | 2 | #4 | STR. | 59'-0" | |
| (3) WH5 | 18 | #4 | STR. | 28'-10" AVG. | 5'-10" TO 51'-10" |
| WH6 | 2 | #5 | STR. | 56'-3" | |
| WL1 | 24 | #6 | BNT. | 12'-9" | |
| WL2 | 74 | #5 | BNT. | 10'-0" | |
| WL3 | 8 | #5 | BNT. | 4'-8" AVG. | 3'-5" TO 5'-11" |
| WL4 | 18 | #5 | BNT. | 4'-9" AVG. | 3'-3" TO 6'-3" |
| WT1 | 63 | #4 | STR. | 2'-10" | |
| WT2 | 7 | #4 | STR. | 10'-0" | |
| WT3 | 19 | #4 | STR. | 6'-8" AVG. | 3'-6" TO 9'-10" |
| WT4 | 8 | #4 | STR. | 2'-8" AVG. | 1'-5" TO 3'-11" |
| WT5 | 37 | #4 | STR. | 6'-9" AVG. | 3'-7" TO 9'-11" |
| WT6 | 18 | #4 | STR. | 2'-9" AVG. | 1'-3" TO 4'-3" |
| WV1 | 6 | #5 | STR. | 10'-0" | |
| WV2 | 19 | #5 | STR. | 6'-8" AVG. | 3'-6" TO 9'-10" |
| WV3 | 37 | #5 | STR. | 6'-9" AVG. | 3'-7" TO 9'-11" |
| U1 | 2 | #4 | BNT. | 2'-8" | |



CS1 #5 x 5'-9" CS2 #5 x 7'-0" CS3 #5 x 5'-11" U1 #4 x 2'-8" WD1 #5 x 8'-0"

WL1 #6 x 12'-9" WL2 #5 x 10'-0" WL3 #5 x 4'-8" AVG. WL4 #5 x 4'-9" AVG.

- (1) INCLUDES ONE (1) 2'-6" LAP.
- (2) INCLUDES TWO (2) SETS OF FORTY-SEVEN (47) BARS EACH.
- (3) INCLUDES TWO (2) SETS OF NINE (9) BARS EACH.

BAR BEND DETAILS

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

STRUCTURE 40 DETAILS
(SHEET 5 OF 5)

LOAD AND RESISTANCE FACTOR DESIGN

DESIGN DATA

CLASS "AA" CONCRETE F'C = 4,000 PSI
REINFORCING STEEL FY = 60,000 PSI

DESIGN: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 8TH EDITION.

NOTES

ALL CONSTRUCTION AND MATERIAL REQUIREMENTS SHALL BE IN ACCORDANCE WITH THE 2009 ODOT STANDARD SPECIFICATIONS, EXCEPT AS MODIFIED BY THE PLANS AND SPECIAL PROVISIONS.

ALL CONCRETE EDGES SHALL HAVE A 1/2" CHAMFER UNLESS OTHERWISE SHOWN OR NOTED. ALL CHAMFER STRIPS SHALL BE SIZED LUMBER.

THE CONTRACTOR SHALL MAINTAIN DRAINAGE AT ALL TIMES DURING CONSTRUCTION.

SEE ROADWAY PLAN & PROFILE SHEETS AND CROSS-SECTION SHEETS FOR GRADING DETAILS.

ALL REINFORCING STEEL SHALL HAVE 2" MINIMUM CLEARANCE UNLESS OTHERWISE SHOWN ON PLANS.

TRANSVERSE CONSTRUCTION JOINTS SHALL BE SPACED AT 60 FT. MAX.

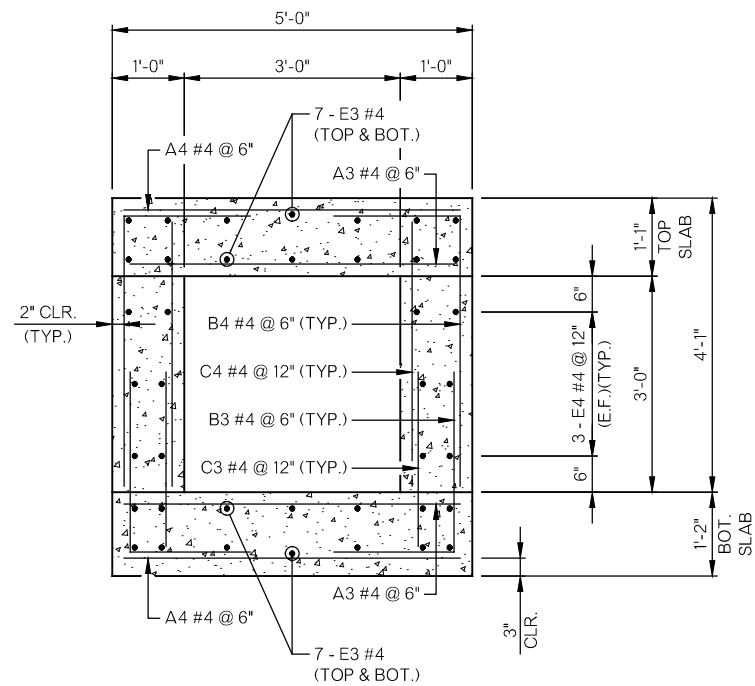
ALL COST FOR CLASS "AA" CONCRETE, LABOR, MATERIALS, EQUIPMENT AND INCIDENTALS NECESSARY TO PLACE CLASS "AA" CONCRETE SHALL BE INCLUDED IN PRICE BID PER CUBIC YARD OF "CLASS AA CONCRETE".

ALL COST FOR REINFORCING STEEL, LABOR, MATERIALS, TOOLS AND OTHER INCIDENTALS NECESSARY TO PLACE REINFORCING STEEL SHALL BE INCLUDED IN PRICE BID PER POUND OF "REINFORCING STEEL".

SOME REINFORCING STEEL BAR MARKS ARE REPEATED. THE STRUCTURAL COMPONENT SHOULD ACCOMPANY THE BAR MARK TO ENSURE THAT ALL BARS ARE PLACED WITH THE CORRECT STRUCTURAL COMPONENT.

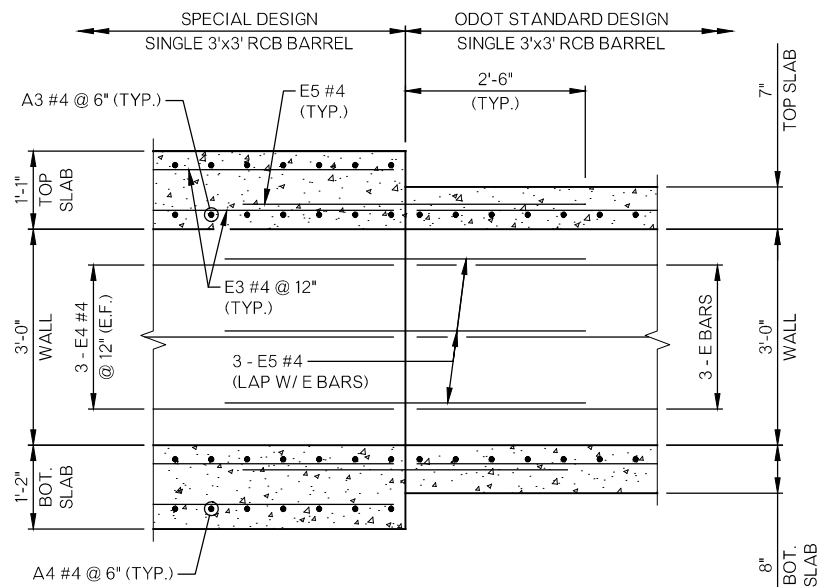
THE FOLLOWING STANDARDS SHALL BE REQUIRED:

- RCB-C1-3&4&5(2-20)-01E
- RCB-E1-H3-0-1-01E
- RCB-E1-H3-0-2-01E
- RCB-CW1-D4-0-01E
- SBI-4-2

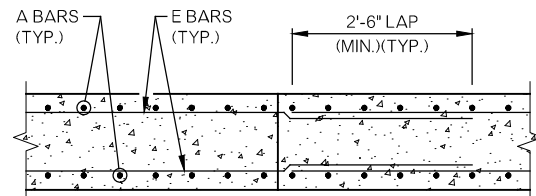


BARREL SECTION

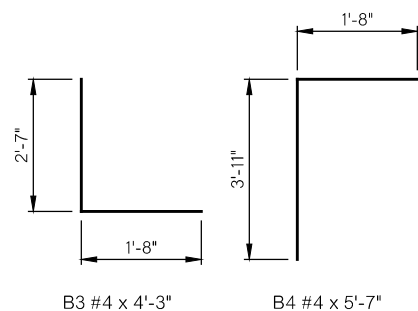
NOTE
E.F. = DENOTES EACH FACE



SECTION THRU CONSTRUCTION JOINT BETWEEN SPECIAL AND STANDARD RCB BARRELS



SECTION THRU CONSTRUCTION JOINT IN SPECIAL RCB BARREL

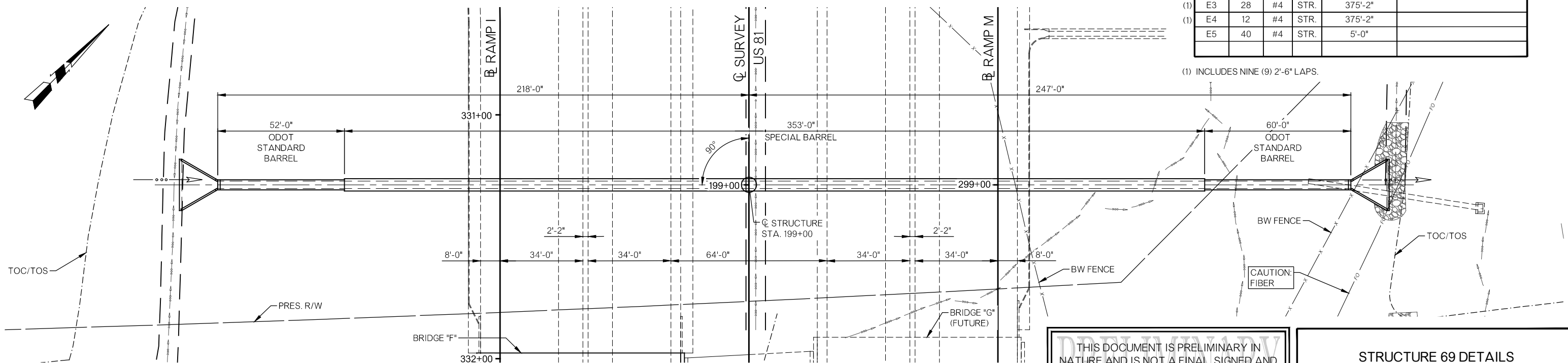


BAR BEND DETAILS

| STRUCTURE 69 QUANTITIES | | |
|-------------------------|------|-----------|
| DESCRIPTION | UNIT | TOTAL |
| CLASS AA CONCRETE | CY | 283.70 |
| REINFORCING STEEL | LB | 39,640.00 |

| STRUCTURE 69 REINFORCING LIST | | | | | |
|-------------------------------|------|------|------|---------|---------|
| MARK | NO. | SIZE | FORM | LENGTH | REMARKS |
| PLAIN REINFORCING BARS | | | | | |
| A3 | 1414 | #4 | STR. | 4'-8" | |
| A4 | 1414 | #4 | STR. | 4'-8" | |
| B3 | 1414 | #4 | BNT. | 4'-3" | |
| B4 | 1414 | #4 | BNT. | 5'-7" | |
| C3 | 708 | #4 | STR. | 2'-7" | |
| C4 | 708 | #4 | STR. | 3'-11" | |
| (1) E3 | 28 | #4 | STR. | 375'-2" | |
| (1) E4 | 12 | #4 | STR. | 375'-2" | |
| E5 | 40 | #4 | STR. | 5'-0" | |

(1) INCLUDES NINE (9) 2'-6" LAPS.



STRUCTURE 69 PLAN

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

STRUCTURE 69 DETAILS

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION

SURVEY OF
U.S. 81, CHICKASHA WEST BYPASS
SWO 4380(1)
J/P NO. 24428(04)
GRADY COUNTY

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION
SURVEY DIVISION

SWO 4380(1) J/P 24428(04) ; CO. Grady

HORIZONTAL CONTROL:
 Oklahoma Coordinate System of 1927 Zone.
 Oklahoma Coordinate System of 1983 South Zone. (CORS96)
 Oklahoma Dept. of Transportation Plane Coordinate System of 1927 Zone.
 Oklahoma Dept. of Transportation Plane Coordinate System of 1983 Zone.
 Arbitrary Coordinate System

HORIZONTAL PLANE DATUM DEFINITION:
 Oklahoma Department of Transportation coordinates were derived by multiplying the Oklahoma Coordinate Systems of 1927 or 1983 by the combined adjustment factor of 1.00010. The ODOT Coordinate System is 2350 feet above sea level.

1. GPS Network adjusted to CORS (1st) Order
 Stations okoa, okar, okcl, okdt, oklv, wmkok
 A) Closure before adjustment X Y Angles 1
 Trav. Length No. Angles : 1:
 B) is Order before adjustment.
 C) Method of Distance Measurement:
 Electronic GPS Triangulation Tied
 D) Instrument used for angles

2. Stations adjusted to Order
 A) Closure before adjustment X Y Angles
 B) is Order; Tied to
 C) Method of Distance Measurement:
 Electronic GPS Triangulation Tied
 D) Instrument used for angles

VERTICAL CONTROL IS 3rd order. Level Line taken from BM 7 & BM 8 SWO 4380 (1)
3rd order and tied to BM 35 & BM 37 SWO 4380 (1) (3rd) order. NGVD 29 datum
 NAVD 88 datum

ACCURACY DEFINITION:
 (1) HORIZONTAL: (3rd Order = Class I = 1 : 10,000'
 (3rd Order = Class II = 1 : 5,000'
 (2) VERTICAL: (1st Order = 0.017 Ft. x sqrt. of Mi.) (2nd Order = 0.035 Ft. x sqrt. of Mi.)
 (3rd Order = 0.050 Ft. x sqrt. of Mi.)

Distribution:
 Copy w/survey reports Bill R. Webb
 Copy in each Alignment Professional Land Surveyor
 and level book 12/21/2015
 (FORM SD #20) Date
 Rev. 11/03

INDEX OF SHEETS

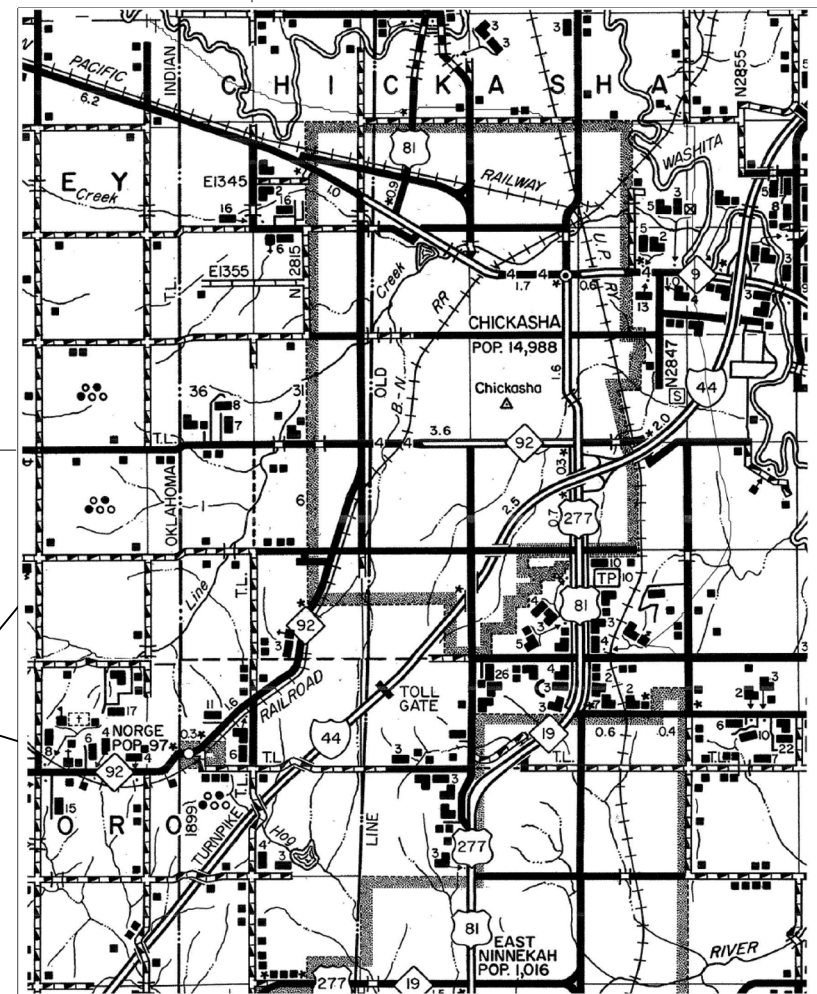
- TITLE SHEET
- KEY PLAN
- HISTORICAL LETTER & WRITTEN REPORT
- LEVEL REPORT
- ALIGNMENT REPORT
- COORDINATE GEOMETRY REPORT
- SURVEY CONTROL NETWORK
- SURVEY DATA SHEETS
- GEOMETRIC DATA SHEETS

SURVEY BEGAN: FEBRUARY 17, 2011
 SURVEY COMPLETED: JUNE 17, 2016

BENHAM DESIGN, LLC

PERSONNEL:
 JOHN T. BIRKHAHN, PROFESSIONAL LAND SURVEYOR
 ANDREW KIS, PARTY CHIEF
 JAMES JACKSON, SURVEY TECHNICIAN
 DANIEL BENNETT, PARTY CHIEF
 CHRIS SHIPMAN, SURVEY TECHNICIAN
 BRANDON HOLLAND, SURVEY TECHNICIAN
 JOHN OKON, SURVEY TECHNICIAN

EQUIPMENT:
 TRIMBLE R8 MODEL 3 RECEIVER
 SOKKIA SET3 TOTAL STATION
 TRIMBLE DINI DIGITAL LEVEL



PROJECT EXTENTS

UTILITIES

| Company Name | Phone No. |
|---|--------------|
| Communication Lines: | |
| Chickasaw Telephone Co. | 580-618-5455 |
| Southwestern Bell | 800-522-6543 |
| AT&T | 800-778-9140 |
| Dobson Technologies | 800-778-9140 |
| Intelq Communications | 800-335-4343 |
| Medicine Park Telephone Co. | 580-529-2700 |
| Electric Lines: | |
| AEP Public Service Co. of Oklahoma | 888-216-3523 |
| Oklahoma Electric Cooperative | 405-321-2024 |
| Water & Sewer Lines: | |
| City of Chickasha Public Works | 405-222-6080 |
| Rural Water District # 6 | 405-459-6626 |
| Rural Water District # 7 | 405-779-6224 |
| Natural Gas Lines: | |
| Centerpoint Energy | 866-275-5265 |
| Natural Gas / Petroleum Pipelines: | |
| Enable Midstream | 800-522-8048 |
| Continuum Energy | 877-587-0026 |
| DCP Midstream | 800-435-1679 |
| Unit Petroleum | 918-493-7700 |
| Sunoco Logistics | 800-753-5531 |
| Keppco Operating Inc. | 855-421-2088 |

U.S. 81, WEST BYPASS,
 MAIN SURVEY PROJECT LENGTH: 45,577.58 Ft. 8.63 MI.
 BEGINNING STATION : 87+22.42
 ENDING STATION : 543+00.00

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION

SWO 4380(1) Job/Piece 24428(04) Engr. Contract No. 1218

LAND SURVEYOR'S CERTIFICATION

I hereby certify that all land and property sub-division distances, angles, corners, and monumentation made or used in conjunction with this survey and depicted or recorded herein or hereon were recovered, established or re-established in substantial conformity with:

- Applicable instructions contained in the U.S. Government Bureau of Land Management publication "Manual of Survey Instruction";
- Its supplement, "Restoration of Lost or Obliterated Corners and Sub-division of Sections";
- "Oklahoma Minimum Standards for the Practice of Land Surveying" as adopted by the State Board of Licensure for Professional Engineers and Land Surveyors; and
- Sound land surveying practices;

including a thorough search, study, analysis and consideration of all existing records and field evidence.

I further certify that all survey monuments depicted exist and that all land survey work was done by me or under my direct supervision.

Dated this 24 day of June, 2016.

Land Surveyor John T. Birkhahn (seal)
 Signature
 John T. Birkhahn
 Printed Name
 Oklahoma Licensed Land Surveyor No. 1738
 Certificate of Authorization No. 3722



THIS SURVEY MEETS THE OKLAHOMA MINIMUM STANDARDS FOR THE PRACTICE OF LAND SURVEYING AS ADOPTED BY THE OKLAHOMA STATE BOARD OF REGISTRATION FOR PROFESSIONAL ENGINEERS AND LAND SURVEYORS, MAY 17, 2010.

Electronic File Transfer Disclaimer:
 These Files, Drawings and/or Notes are provided for information only. The Oklahoma Department of Transportation (ODOT) and the Owner cannot be held responsible for the content or accuracy of these Files, Drawings and/or Notes due to conversions, software translations, or any other manipulation of said Files, Drawings and/or Notes. ODOT expressly disclaims any responsibility arising from any use of these Files, Drawings and/or Notes. To the full extent permitted by applicable law, the recipient of these Files, Drawings and/or Notes hereby agrees to defend, indemnify, and hold harmless ODOT and the Owner from and against any and all claims, suits, actions, damages, loss, liability or costs of every nature or description (including reasonable attorney's fees) arising from, or in any way attributable to or connected with any of these Files, Drawings and/or Notes.

SPECIFICATIONS FOR SURVEYS FOR PRIMARY AND SECONDARY HIGHWAYS DATED JAN 1, 2011 GOVERN. WHERE THERE IS A CONFLICT BETWEEN THESE SPECIAL PROVISIONS AND THE SPECIFICATIONS, THE SPECIAL PROVISIONS SHALL TAKE PRECEDENCE.



| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | |
|--|--------|
| PLS | JTB |
| DRAWN | JTB |
| CHECKED | JTB |
| APPROVED | JTB |
| CREW | BENHAM |

SURVEY DATA SHEET
 SDS 1 OF 76

SWO 4380 (1) PROJECT NO. 24428(04) SHEET NO. S001

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |

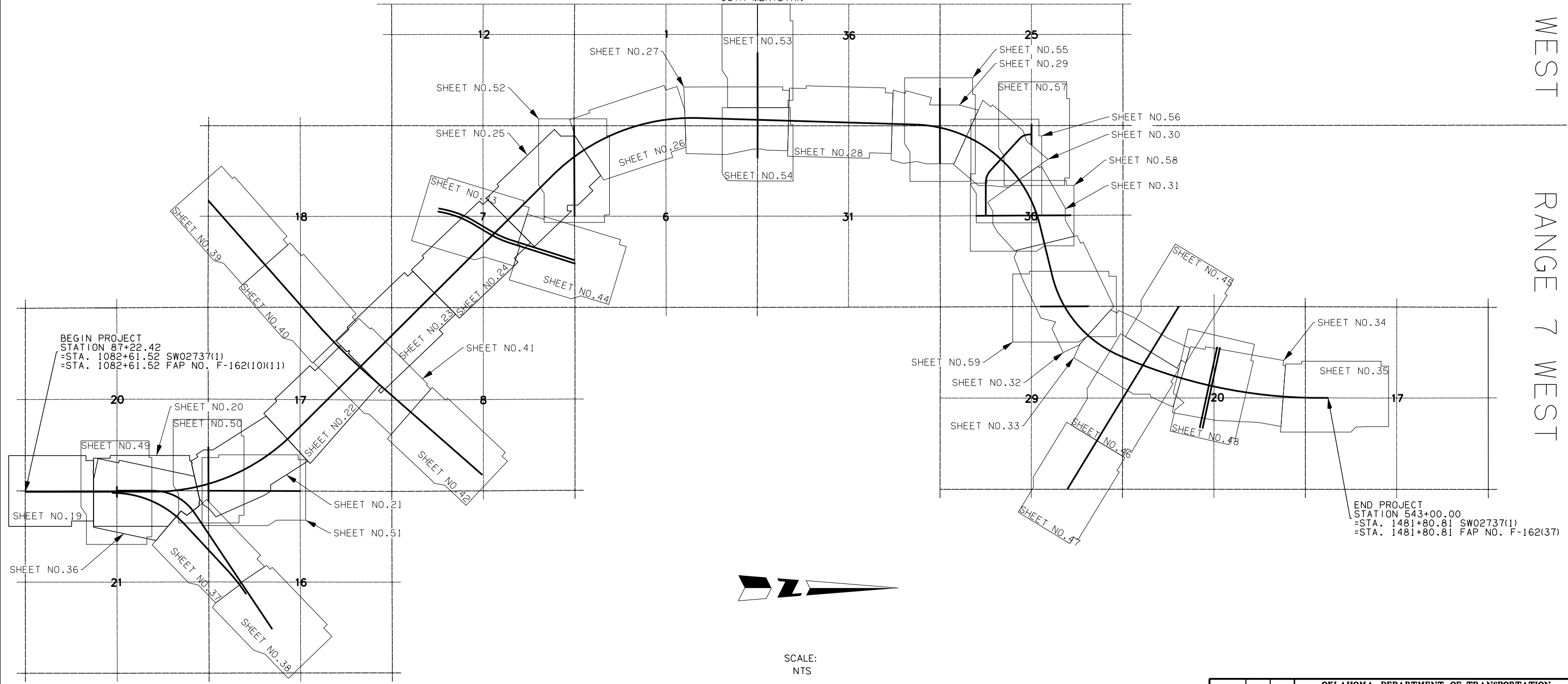
TOWNSHIP 6 NORTH

TOWNSHIP 7 NORTH

RANGE 8 WEST

RANGE 7 WEST

98TH MERIDIAN



BEGIN PROJECT
 STATION 87+22.42
 =STA. 1082+61.52 SW02737(1)
 =STA. 1082+61.52 FAP NO. F-162(10)(11)

END PROJECT
 STATION 543+00.00
 =STA. 1481+80.81 SW02737(1)
 =STA. 1481+80.81 FAP NO. F-162(37)



SCALE:
 NTS

| | | |
|----------|--------|---|
| PLS | JTB | OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET SDS <u>2</u> OF <u>76</u> SWO <u>4380</u> (1) PROJECT NO. <u>24428(04)</u> SHEET NO. <u>S002</u> |
| DRAWN | APK | |
| CHECKED | JTB | |
| APPROVED | JTB | |
| CREW | BENHAM | |

| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|-----------|-------------|-----------|--------------|
| | OKLA | | | | |

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
SURVEY DIVISION (405) 521-2621 FAX 405-522-0364

Date: July 6, 2016

To: Mr. William Tackett, Chief of Surveys
From: John Birkhahn, Professional Land Surveyor
Subject: SWO 4380(1) J/P 24428(04) – Grady County U.S. 81 Chickasha West Bypass.

HISTORICAL LETTER & WRITTEN REPORT

1. GENERAL:

- A. Survey began: February 17, 2011.
Survey completion: June 17, 2016.
- B. The measurement unit for this project will be the U.S. Survey Foot.

2. SURVEY ASSIGNMENT:

The above survey was assigned to me by Mr. Larry Reser, Chief of Surveys in September 2010, and then again by Mr. William Tackett, Chief of Surveys, with revised Survey Special Provisions in November 2014.

3. PURPOSE OF SURVEY:

The purpose of this survey is to develop plans to design a highway on a new highway alignment, to include all road crossings, interchanges, bridges, frontage roads, and other facilities required by the design. The survey will also include sufficient data to design the connections to the existing highway at the beginning and end of the project. The survey will include the Alignment, Topographic/Planimetric data, Surface Features/Digital Terrain Model Data, Land and Property Ties, Utilities, Drainage and all other pertinent information needed to aid in the design.

4. SURVEY LIMITS:

West Bypass Main Survey: Beginning at P.O.T. Sta. 1082+61.52 (on EW-141 Section Line) as established under SWO 2737(1) Section One survey and shown on FAP No. F-162(10)(11) plans, and extending northerly along US 81, then northwesterly on a new alignment (one of two similar alternatives selected on October 29, 2014) continuing Northwest, Northeast, and finally Northerly, tying back to existing US 81, near the present junction of US 81 & US 62, Northwest of Chickasha. From there, the survey continues to and ends at P.T. Sta. 1481+80.81 as established under SWO 2737(1) Section 2 survey and shown on FAP No. F-162(37) plans (approximate centerline length=6.63 miles).

US 81 Northbound Survey: Beginning at P.C. Sta. 1107+80.13 as shown on FAP No. F-162(10)(11) plans, and extending northeasterly along existing US 81 for approximately 4300 feet.

US 81 Southbound Survey: Beginning at P.O.T. Sta. 1109+14.72 as shown on FAP No. F-162(10)(11) plans, and extending northeasterly along existing US 81 for approximately 4500 feet.

H.E. Bailey Turnpike Connection Survey: Beginning approximately 4840 feet Southwest of the centerline of the Main survey and extending Northeast along H.E. Bailey Turnpike approximately 9700 feet

Historical Letter & Written Report
Page 1 of 9

S.H. 92 (Norge Rd.) & BNSF Railroad Connection Survey: Beginning 600 feet Southwest of the centerline of the Main Survey and extending Northeast along S.H. 92 & BNSF Railroad approximately 1200 feet.

U.S. 62 Connection Survey: Beginning at P.O.T. Sta. 385+47.37 (NS-282 Section Line) as established under SWO 2738(1) Section 1 survey & SWO 3609(1) survey and shown on FAP No. STPY-026B(240) plans, and extending Southeasterly along U.S. 62 approximately 6200 feet.

Old U.S. 62 Connection Survey: Beginning 1150 feet Northwest of the centerline of the Main Survey and extending Southeast along Old U.S. 62 approximately 2400 feet.

Union Pacific Railroad Connection Survey: Beginning 1150 feet Northwest of the centerline of the Main Survey and extending Southeast along Old U.S. 62 approximately 2400 feet.

EW-140.5 Section Line Connection Survey: Beginning 300 feet West of the centerline of the Main Survey and extending East 600 feet along Section Line.

EW-140 Section Line Connection Survey: Beginning 1100 feet West of the centerline of the Main Survey and extending East approximately 3400 feet along Section Line.

NS-283 Section Line (16th St.) Connection Survey: Beginning at EW-140 Section Line, extending North approximately 400 feet along Section Line.

EW-138 Section Line (Country Club Rd.) Connection Survey: Beginning at NS-281 Section Line and extending East to NS-281.5 ¼ Section Line, approximately 2620 feet.

EW-137 Section Line (Grand Ave.) Connection Survey: Beginning approximately 1950 feet West of the centerline of the Main Survey and extending East approximately 3050 feet along Section Line.

EW-136 Section Line (Idaho Ave.) Connection Survey: This survey will begin approximately 1150 feet West of the centerline of the Main Survey and will extend East approximately 2200 feet along Section Line.

Iowa Ave. Connection Survey: Beginning at EW-135.5 ¼ Section Line, approximately 250 feet East of NS-281 Section Line, then extending Southeasterly for approximately 3000 feet to the present intersection of Iowa Ave. and C.S. 2815 Rd.

EW-135.5 Section Line (C.R. 1355) Connection Survey: Beginning at NS-281 Section Line and extending East, approximately 600 feet along Section Line.

NS-281.5 Section line (C.S. 2815 Rd.) Connection Survey: Beginning approximately 300 feet South of the centerline of the Main Survey and extending North approximately 1300 feet along Section Line.

NS-282 Section line (29th St.) Connection Survey: Beginning approximately 700 feet South of the centerline of the Main Survey and extending North approximately 1400 feet along Section Line.

Historical Letter & Written Report
Page 2 of 9

ROCK HOLLOW CREEK FEMA FLOOD PLAIN: Parallel flight lines, parallel to U.S. 62, are to be laid out by the Aerial Photo/Mapping sub-consultant to cover the flood plain area approved by ODOT.

5. ALIGNMENTS:

West Bypass Main Survey: This Centerline of Survey is along the proposed alignment submitted to ODOT by Benham Design, LLC (one of two similar alternatives selected on October 29, 2014).

US 81 Northbound Survey: The Centerline of Survey will be along and identical to the Centerline of Survey as shown on FAP No. F-162(10)(11) plans.

US 81 Southbound Survey: The Centerline of Survey will be along and identical to the Centerline of Survey as shown on FAP No. F-162(10)(11) plans.

H.E. Bailey Turnpike Connection Survey: The Centerline of Survey will be along and identical to the Centerline of Survey established under SWO 2737(1) Section One survey and shown in Alignment Book 2.

S.H. 92 (Norge Rd.) & BNSF Railroad Connection Survey: The Centerline of Survey will be along and identical to the centerline of S.H. 92 as established under SWO 1148 survey & SWO 1148(REV.) survey and shown as FAS No. S-219(1) plans. Note: Reference points were found for two P.I.'s at Sta. 146+17.19 and Sta. 182+70.95 from said plans. All curve data and tangent angles were held between these points to determine alignment.

U.S. 62 Connection Survey: The Centerline of Survey will be along and identical to the Centerline of Survey established under SWO 2738(1) Section 1 survey and SWO 3609(1) survey and shown on FAP No. STPY-026B(240) plans.

Old U.S. 62 Connection Survey: The Centerline of Survey will be along and identical to the centerline of Old U.S. 62 as established under SWO 2737(1) Section 2 survey and shown in Alignment Book No. 2.

Union Pacific Railroad Connection Survey: The Centerline of Survey will be along and identical to the centerline of the former C.R.I. & P. Railway as established under SWO 2737(1) Section 2 survey and shown in Alignment Book No. 2.

EW-140.5 Section Line Connection Survey: The Centerline of Survey will be along and identical to EW-140.5 ¼ Section Line.

EW-140 Section Line Connection Survey: The Centerline of Survey will be along and identical to EW-140 Section Line.

NS-283 Section Line (16th St.) Connection Survey: The Centerline of Survey will be along and identical to NS-283 Section Line.

EW-138 Section Line (Country Club Rd.) Connection Survey: The Centerline of Survey will be along and identical to EW-138 Section Line.

EW-137 Section Line (Grand Ave.) Connection Survey: The Centerline of Survey will be along and identical to EW-137 Section Line.

Historical Letter & Written Report
Page 3 of 9

EW-136 Section Line (Idaho Ave.) Connection Survey: The Centerline of Survey will be along and identical to EW-136 Section Line.

Iowa Ave. Connection Survey: This Centerline of Survey is along the proposed alignment submitted to ODOT by Benham Design, LLC (one of two similar alternatives selected on October 29, 2014).

EW-135.5 Section Line (C.R. 1355) Connection Survey: The Centerline of Survey will be along and identical to EW-135.5 ¼ Section Line.

NS-281.5 Section line (C.S. 2815 Rd.) Connection Survey: The Centerline of Survey will be along and identical to NS-281.5 ¼ Section Line.

NS-282 Section line (29th St.) Connection Survey: The Centerline of Survey will be along and identical to NS-282 Section Line.

ROCK HOLLOW CREEK FEMA FLOOD PLAIN: N/A.

6. STATIONING:

West Bypass Main Survey: Stationing for this survey is taken from SWO 2737(1) Section One survey at P.O.T. Sta. 1082+61.52 (at the EW-141 Section Line). Stationing increases Northerly from this point, field measured distance, to the End of Survey without equation, except with previous surveys and plans.

US 81 Northbound Survey: Stationing for this survey is taken from SWO 2737(1) Section One survey at P.C. Sta. 1107+80.13. Stationing increases Northeasterly from this point to the End of Survey without equation, except with previous surveys and plans.

US 81 Southbound Survey: Stationing for this survey is taken from SWO 2737(1) Section One survey at P.O.T. Sta. 1109+14.72 (at the EW-140.5 ¼ Section Line). Stationing increases Northeasterly from this point to the End of Survey without equation, except with previous surveys and plans.

H.E. Bailey Turnpike Connection Survey: Stationing for this survey is taken from SWO 2737(1) Section One survey.

S.H. 92 (Norge Rd.): Stationing for this survey is taken from FAS No. S-219(1) plans at P.C. Sta. 162+81.10. Stationing increases Northeasterly from this point to the End of Survey without equation, except with previous surveys and plans.

BNSF Railroad Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey and stationing increases Northeast to the End of Survey without equation, except with other surveys and plans.

U.S. 62 Connection Survey: Stationing for this survey is taken from SWO 2738(1) Section 1 survey.

Old U.S. 62 Connection Survey: Stationing for this survey is taken from SWO 2737(1) Section 2 survey.

Union Pacific Railroad Connection Survey: Stationing for this survey is taken from SWO 2737(1) Section 2 survey.

Historical Letter & Written Report
Page 4 of 9

EW-140.5 Section Line Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey, and stationing increases East to the End of Survey without equation, except with other surveys and plans.

EW-140 Section Line Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey, and stationing increases East to the End of Survey without equation, except with other surveys and plans.

NS-283 Section Line (16th St.) Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey, and stationing increases North to the End of Survey without equation, except with other surveys and plans.

EW-138 Section Line (Country Club Rd.) Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey, and stationing increases East to the End of Survey without equation, except with other surveys and plans.

EW-137 Section Line (Grand Ave.) Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey, and stationing increases East to the End of Survey without equation, except with other surveys and plans.

EW-136 Section Line (Idaho Ave.) Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey and stationing increases East to the End of Survey without equation, except with other surveys and plans.

Iowa Ave. Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey and stationing increases Southeast to the End of Survey without equation, except with other surveys and plans.

EW-135.5 Section Line (C.R. 1355) Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey, and stationing increases East to the End of Survey without equation, except with other surveys and plans.

NS-281.5 Section line (C.S. 2815 Rd.) Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey and stationing increases North to the End of Survey without equation, except with other surveys and plans.

NS-282 Section line (29th St.) Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey and stationing increases North to the End of Survey without equation, except with other surveys and plans.

ROCK HOLLOW CREEK FEMA FLOOD PLAIN: N/A.

7. HORIZONTAL CONTROL:

- A. Horizontal control for this survey is NGS Oklahoma State Plane Coordinate System NAD83 (CORS 96) (EPOCH: 2002), Lambert Projection, South Zone (3502). Primary Control points for this survey are the following:

Existing Monuments

- "UNION" (From N.G.S. Data Sheet, NAD 83 (2007), PID-FJ1044)
- "11F" (From N.G.S. Data Sheet, NAD 83 (1993), PID-EL1033)
- "CHICK" (From N.G.S. Data Sheet, NAD 83 (1993), PID-FJ1049)
- "D 214" (From N.G.S. Data Sheet, NAD 83, PID-EL0693)
- "POCASSET" (From N.G.S. Data Sheet, NAD 83 (1993), PID-FJ0789)

Historical Letter & Written Report
Page 5 of 9

"K 41 1953" (From N.G.S. Data Sheet, NAD 83, PID-FJ0528)

Set Monuments

ODOT monuments "G-26-865 thru G-26-868" NAD 83 (CORS 96) (by Benham Design, LLC)
ODOT monuments "G-26-1032 thru G-26-1039" (CORS 96) (by Aerial Data Services, Inc.)

ODOT monuments "G-26-865 thru G-26-868" were set 5/8" iron pins with stamped aluminum cap in concrete. They were established using GPS, for three separate static survey data occupations for a minimum of two hours, on three separate days, at different times of day. All data was uploaded to <http://www.ngs.noaa.gov/OPUS/>. The solutions were then averaged to obtain coordinates in NAD 83 (CORS96)(EPOCH: 2002).

ODOT monuments "G-26-1032 thru G-26-1039" were set chiseled "X"s in concrete. They were set using GPS from selected points from OPUS Projects network adjustment shown in SWO4380_1_v2.dgn. Existing N.G.S. monuments listed above were used as a check for primary and secondary control.

A coordinate shift of $\Delta N = 0.002$, $\Delta E = -0.129$ was applied to all NAD 83 (CORS 2011) coordinate solutions to conform to NAD 83 (CORS 96) horizontal datum. This was calculated by uploading identical static sessions for ODOT Monuments "G-26-865 thru G-26-868" to <http://www.ngs.noaa.gov/OPUS/>, and subtracting resulting coordinate differences between CORS 96 and CORS 2011 solutions.

- B. Secondary Control for this survey was established using GPS RTK, based off of primary control points.

8. VERTICAL CONTROL:

Level datum for this survey is NAVD 88. Benchmarks for this survey were set by Aerial Data Services Inc. All bench marks were tied to the following ODOT bench marks from SWO 4380(1): BM 7-13, 15-20, 24-28, 30, 32-34, & 37. Bench marks established or used on this survey meet the requirements of the N.G.S. 3rd order standards as a minimum.

9. PHOTO CONTROLS:

A total of 110 Aerial Targets were set and tied on this project. Aerial targets were set and measured by Aerial Data Service, Inc. Aerial targets are shown in SWO4380_1_v2.dgn, Cogo Points List, and X,Y,Z coordinates were placed in the following file: SWO4380_1_v2_targets.txt

10. TOPOGRAPHY:

The majority of the topography on this survey was obtained from aerial photogrammetry survey data by Aerial Data Service, Inc. The date of flight for the aerial survey was March 11, 2015. GPS RTK and conventional field methods were used to survey the following:

- 1. Drainage structures
- 2. Creek flow lines.
- 3. Ponds

Historical Letter & Written Report
Page 6 of 9

| | | |
|----------|--------|---|
| PLS | JTB | OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET SDS <u>3</u> OF <u>76</u> |
| DRAWN | JTB | |
| CHECKED | JTB | |
| APPROVED | JTB | |
| CREW | BENHAM | |
| | | SWO <u>4380</u> (<u>1</u>) PROJECT NO. <u>24428(04)</u> SHEET NO. <u>S003</u> |

| FED. ROAD DIST. NO. | STATE | PRGJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|-----------|-------------|-----------|--------------|
| | OKLA | | | | |

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
SURVEY DIVISION (405) 521-2621 FAX 405-522-0364

Date: July 6, 2016

To: Mr. William Tackett, Chief of Surveys
From: John Birkhahn, Professional Land Surveyor
Subject: SWO 4380(1) J/P 24428(04) – Grady County U.S. 81 Chickasha West Bypass.

HISTORICAL LETTER & WRITTEN REPORT

1. GENERAL:

- A. Survey began: February 17, 2011.
Survey completion: June 17, 2016.
- B. The measurement unit for this project will be the U.S. Survey Foot.

2. SURVEY ASSIGNMENT:

The above survey was assigned to me by Mr. Larry Reser, Chief of Surveys in September 2010, and then again by Mr. William Tackett, Chief of Surveys, with revised Survey Special Provisions in November 2014.

3. PURPOSE OF SURVEY:

The purpose of this survey is to develop plans to design a highway on a new highway alignment, to include all road crossings, interchanges, bridges, frontage roads, and other facilities required by the design. The survey will also include sufficient data to design the connections to the existing highway at the beginning and end of the project. The survey will include the Alignment, Topographic/Planimetric data, Surface Features/Digital Terrain Model Data, Land and Property Ties, Utilities, Drainage and all other pertinent information needed to aid in the design.

4. SURVEY LIMITS:

West Bypass Main Survey: Beginning at P.O.T. Sta. 1082+61.52 (on EW-141 Section Line) as established under SWO 2737(1) Section One survey and shown on FAP No. F-162(10)(11) plans, and extending northerly along US 81, then northwesterly on a new alignment (one of two similar alternatives selected on October 29, 2014) continuing Northwesterly, Northeasterly, and finally Northerly, tying back to existing US 81, near the present junction of US 81 & US 62, Northwest of Chickasha. From there, the survey continues to and ends at P.T. Sta. 1481+80.81 as established under SWO 2737(1) Section 2 survey and shown on FAP No. F-162(37) plans (approximate centerline length=6.63 miles).

US 81 Northbound Survey: Beginning at P.C. Sta. 1107+80.13 as shown on FAP No. F-162(10)(11) plans, and extending northeasterly along existing US 81 for approximately 4300 feet.

US 81 Southbound Survey: Beginning at P.O.T. Sta. 1109+14.72 as shown on FAP No. F-162(10)(11) plans, and extending northeasterly along existing US 81 for approximately 4500 feet.

H.E. Bailey Turnpike Connection Survey: Beginning approximately 4840 feet Southwest of the centerline of the Main survey and extending Northeast along H.E. Bailey Turnpike approximately 9700 feet

Historical Letter & Written Report
Page 1 of 9

S.H. 92 (Norge Rd.) & BNSF Railroad Connection Survey: Beginning 600 feet Southwest of the centerline of the Main Survey and extending Northeast along S.H. 92 & BNSF Railroad approximately 1200 feet.

U.S. 62 Connection Survey: Beginning at P.O.T. Sta. 385+47.37 (NS-282 Section Line) as established under SWO 2738(1) Section 1 survey & SWO 3609(1) survey and shown on FAP No. STPY-026B(240) plans, and extending Southeasterly along U.S. 62 approximately 6200 feet.

Old U.S. 62 Connection Survey: Beginning 1150 feet Northwest of the centerline of the Main Survey and extending Southeast along Old U.S. 62 approximately 2400 feet.

Union Pacific Railroad Connection Survey: Beginning 1150 feet Northwest of the centerline of the Main Survey and extending Southeast along Old U.S. 62 approximately 2400 feet.

EW-140.5 Section Line Connection Survey: Beginning 300 feet West of the centerline of the Main Survey and extending East 600 feet along Section Line.

EW-140 Section Line Connection Survey: Beginning 1100 feet West of the centerline of the Main Survey and extending East approximately 3400 feet along Section Line.

NS-283 Section Line (16th St.) Connection Survey: Beginning at EW-140 Section Line, extending North approximately 400 feet along Section Line.

EW-138 Section Line (Country Club Rd.) Connection Survey: Beginning at NS-281 Section Line and extending East to NS-281.5 ¼ Section Line, approximately 2620 feet.

EW-137 Section Line (Grand Ave.) Connection Survey: Beginning approximately 1950 feet West of the centerline of the Main Survey and extending East approximately 3050 feet along Section Line.

EW-136 Section Line (Idaho Ave.) Connection Survey: This survey will begin approximately 1150 feet West of the centerline of the Main Survey and will extend East approximately 2200 feet along Section Line.

Iowa Ave. Connection Survey: Beginning at EW-135.5 ¼ Section Line, approximately 250 feet East of NS-281 Section Line, then extending Southeasterly for approximately 3000 feet to the present intersection of Iowa Ave. and C.S. 2815 Rd.

EW-135.5 Section Line (C.R. 1355) Connection Survey: Beginning at NS-281 Section Line and extending East, approximately 600 feet along Section Line.

NS-281.5 Section line (C.S. 2815 Rd.) Connection Survey: Beginning approximately 300 feet South of the centerline of the Main Survey and extending North approximately 1300 feet along Section Line.

NS-282 Section line (29th St.) Connection Survey: Beginning approximately 700 feet South of the centerline of the Main Survey and extending North approximately 1400 feet along Section Line.

Historical Letter & Written Report
Page 2 of 9

ROCK HOLLOW CREEK FEMA FLOOD PLAIN: Parallel flight lines, parallel to U.S. 62, are to be laid out by the Aerial Photo/Mapping sub-consultant to cover the flood plain area approved by ODOT.

5. ALIGNMENTS:

West Bypass Main Survey: This Centerline of Survey is along the proposed alignment submitted to ODOT by Benham Design, LLC (one of two similar alternatives selected on October 29, 2014).

US 81 Northbound Survey: The Centerline of Survey will be along and identical to the Centerline of Survey as shown on FAP No. F-162(10)(11) plans.

US 81 Southbound Survey: The Centerline of Survey will be along and identical to the Centerline of Survey as shown on FAP No. F-162(10)(11) plans.

H.E. Bailey Turnpike Connection Survey: The Centerline of Survey will be along and identical to the Centerline of Survey established under SWO 2737(1) Section One survey and shown in Alignment Book 2.

S.H. 92 (Norge Rd.) & BNSF Railroad Connection Survey: The Centerline of Survey will be along and identical to the centerline of S.H. 92 as established under SWO 1148 survey & SWO 1148(REV.) survey and shown as FAS No. S-219(1) plans. Note: Reference points were found for two P.I.'s at Sta. 146+17.19 and Sta. 182+70.95 from said plans. All curve data and tangent angles were held between these points to determine alignment.

U.S. 62 Connection Survey: The Centerline of Survey will be along and identical to the Centerline of Survey established under SWO 2738(1) Section 1 survey and SWO 3609(1) survey and shown on FAP No. STPY-026B(240) plans.

Old U.S. 62 Connection Survey: The Centerline of Survey will be along and identical to the centerline of Old U.S. 62 as established under SWO 2737(1) Section 2 survey and shown in Alignment Book No. 2.

Union Pacific Railroad Connection Survey: The Centerline of Survey will be along and identical to the centerline of the former C.R.I. & P. Railway as established under SWO 2737(1) Section 2 survey and shown in Alignment Book No. 2.

EW-140.5 Section Line Connection Survey: The Centerline of Survey will be along and identical to EW-140.5 ¼ Section Line.

EW-140 Section Line Connection Survey: The Centerline of Survey will be along and identical to EW-140 Section Line.

NS-283 Section Line (16th St.) Connection Survey: The Centerline of Survey will be along and identical to NS-283 Section Line.

EW-138 Section Line (Country Club Rd.) Connection Survey: The Centerline of Survey will be along and identical to EW-138 Section Line.

EW-137 Section Line (Grand Ave.) Connection Survey: The Centerline of Survey will be along and identical to EW-137 Section Line.

Historical Letter & Written Report
Page 3 of 9

EW-136 Section Line (Idaho Ave.) Connection Survey: The Centerline of Survey will be along and identical to EW-136 Section Line.

Iowa Ave. Connection Survey: This Centerline of Survey is along the proposed alignment submitted to ODOT by Benham Design, LLC (one of two similar alternatives selected on October 29, 2014).

EW-135.5 Section Line (C.R. 1355) Connection Survey: The Centerline of Survey will be along and identical to EW-135.5 ¼ Section Line.

NS-281.5 Section line (C.S. 2815 Rd.) Connection Survey: The Centerline of Survey will be along and identical to NS-281.5 ¼ Section Line.

NS-282 Section line (29th St.) Connection Survey: The Centerline of Survey will be along and identical to NS-282 Section Line.

ROCK HOLLOW CREEK FEMA FLOOD PLAIN: N/A.

6. STATIONING:

West Bypass Main Survey: Stationing for this survey is taken from SWO 2737(1) Section One survey at P.O.T. Sta. 1082+61.52 (at the EW-141 Section Line). Stationing increases Northerly from this point, field measured distance, to the End of Survey without equation, except with previous surveys and plans.

US 81 Northbound Survey: Stationing for this survey is taken from SWO 2737(1) Section One survey at P.C. Sta. 1107+80.13. Stationing increases Northeasterly from this point to the End of Survey without equation, except with previous surveys and plans.

US 81 Southbound Survey: Stationing for this survey is taken from SWO 2737(1) Section One survey at P.O.T. Sta. 1109+14.72 (at the EW-140.5 ¼ Section Line). Stationing increases Northeasterly from this point to the End of Survey without equation, except with previous surveys and plans.

H.E. Bailey Turnpike Connection Survey: Stationing for this survey is taken from SWO 2737(1) Section One survey.

S.H. 92 (Norge Rd.): Stationing for this survey is taken from FAS No. S-219(1) plans at P.C. Sta. 162+81.10. Stationing increases Northeasterly from this point to the End of Survey without equation, except with previous surveys and plans.

BNSF Railroad Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey and stationing increases Northeast to the End of Survey without equation, except with other surveys and plans.

U.S. 62 Connection Survey: Stationing for this survey is taken from SWO 2738(1) Section 1 survey.

Old U.S. 62 Connection Survey: Stationing for this survey is taken from SWO 2737(1) Section 2 survey.

Union Pacific Railroad Connection Survey: Stationing for this survey is taken from SWO 2737(1) Section 2 survey.

Historical Letter & Written Report
Page 4 of 9

EW-140.5 Section Line Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey, and stationing increases East to the End of Survey without equation, except with other surveys and plans.

EW-140 Section Line Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey, and stationing increases East to the End of Survey without equation, except with other surveys and plans.

NS-283 Section Line (16th St.) Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey, and stationing increases North to the End of Survey without equation, except with other surveys and plans.

EW-138 Section Line (Country Club Rd.) Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey, and stationing increases East to the End of Survey without equation, except with other surveys and plans.

EW-137 Section Line (Grand Ave.) Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey, and stationing increases East to the End of Survey without equation, except with other surveys and plans.

EW-136 Section Line (Idaho Ave.) Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey and stationing increases East to the End of Survey without equation, except with other surveys and plans.

Iowa Ave. Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey and stationing increases Southeast to the End of Survey without equation, except with other surveys and plans.

EW-135.5 Section Line (C.R. 1355) Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey, and stationing increases East to the End of Survey without equation, except with other surveys and plans.

NS-281.5 Section line (C.S. 2815 Rd.) Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey and stationing increases North to the End of Survey without equation, except with other surveys and plans.

NS-282 Section line (29th St.) Connection Survey: A value of Sta. 100+00.00 is assigned to the Beginning of Survey and stationing increases North to the End of Survey without equation, except with other surveys and plans.

ROCK HOLLOW CREEK FEMA FLOOD PLAIN: N/A.

7. HORIZONTAL CONTROL:

- A. Horizontal control for this survey is NGS Oklahoma State Plane Coordinate System NAD83 (CORS 96) (EPOCH: 2002), Lambert Projection, South Zone (3502). Primary Control points for this survey are the following:

Existing Monuments

- "UNION" (From N.G.S. Data Sheet, NAD 83 (2007), PID-FJ1044)
- "11T" (From N.G.S. Data Sheet, NAD 83 (1993), PID-EL1033)
- "CHICK" (From N.G.S. Data Sheet, NAD 83 (1993), PID-FJ1049)
- "D 214" (From N.G.S. Data Sheet, NAD 83, PID-EL0693)
- "POCASSET" (From N.G.S. Data Sheet, NAD 83 (1993), PID-FJ0789)

Historical Letter & Written Report
Page 5 of 9

"K 41 1953" (From N.G.S. Data Sheet, NAD 83, PID-FJ0528)

Set Monuments

ODOT monuments "G-26-865 thru G-26-868" NAD 83 (CORS 96) (by Benham Design, LLC)
ODOT monuments "G-26-1032 thru G-26-1039" (CORS 96) (by Aerial Data Services, Inc.)

ODOT monuments "G-26-865 thru G-26-868" were set 5/8" iron pins with stamped aluminum cap in concrete. They were established using GPS, for three separate static survey data occupations for a minimum of two hours, on three separate days, at different times of day. All data was uploaded to <http://www.ngs.noaa.gov/OPUS/>. The solutions were then averaged to obtain coordinates in NAD 83 (CORS96)(EPOCH: 2002).

ODOT monuments "G-26-1032 thru G-26-1039" were set chiseled "X"s in concrete. They were set using GPS from selected points from OPUS Projects network adjustment shown in SWO4380_1_v2.dgn. Existing N.G.S. monuments listed above were used as a check for primary and secondary control.

A coordinate shift of $\Delta N = 0.002$, $\Delta E = -0.129$ was applied to all NAD 83 (CORS 2011) coordinate solutions to conform to NAD 83 (CORS 96) horizontal datum. This was calculated by uploading identical static sessions for ODOT Monuments "G-26-865 thru G-26-868" to <http://www.ngs.noaa.gov/OPUS/>, and subtracting resulting coordinate differences between CORS 96 and CORS 2011 solutions.

- B. Secondary Control for this survey was established using GPS RTK, based off of primary control points.

8. VERTICAL CONTROL:

Level datum for this survey is NAVD 88. Benchmarks for this survey were set by Aerial Data Services Inc. All bench marks were tied to the following ODOT bench marks from SWO 4380(1): BM 7-13, 15-20, 24-28, 30, 32-34, & 37. Bench marks established or used on this survey meet the requirements of the N.G.S. 3rd order standards as a minimum.

9. PHOTO CONTROLS:

A total of 110 Aerial Targets were set and tied on this project. Aerial targets were set and measured by Aerial Data Service, Inc. Aerial targets are shown in SWO4380_1_v2.dgn, Cogo Points List, and X,Y,Z coordinates were placed in the following file: SWO4380_1_v2_targets.txt

10. TOPOGRAPHY:

The majority of the topography on this survey was obtained from aerial photogrammetry survey data by Aerial Data Service, Inc. The date of flight for the aerial survey was March 11, 2015. GPS RTK and conventional field methods were used to survey the following:

- 1. Drainage structures
- 2. Creek flow lines.
- 3. Ponds

Historical Letter & Written Report
Page 6 of 9

| PLS | JTB | OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION |
|----------|--------|---|
| DRAWN | JTB | SURVEY DATA SHEET SDS <u>3</u> OF <u>76</u> |
| CHECKED | JTB | |
| APPROVED | JTB | |
| CREW | BENHAM | |
| | | |
| | | SWO <u>4380</u> (<u>1</u>) PROJECT NO. <u>24428(04)</u> SHEET NO. <u>S003</u> |

Table with columns: FED. ROAD DIST. NO., STATE, PROJ. NO., FISCAL YEAR, SHEET NO., TOTAL SHEETS. Includes 'OKLA' and 'REVISIONS' data.

Table with columns: DESCRIPTION, REVISIONS, DATE.

SWO 4380(1) ALIGNMENT REPORT J/P 24428(04)
Project Name: SWO 4380_1_V2
Description: US81 Existing Alignments
Horizontal Alignment Name: A001
Description: CRL Prop US81 Bypass
Style: proposed 100
STATION EASTING NORTHING
Element: Linear
Element: Circular
Element: Linear
Element: Circular
Element: Linear
Element: Circular

SWO 4380(1) ALIGNMENT REPORT J/P 24428(04)
Radial Direction: N 45°07'21.59" E
Chord Direction: N 21°34'36.93" W
Radial Direction: S 88°16'35.45" E
Tangent Direction: N 1°43'24.55" E
Element: Linear
Element: Circular
Degree of Curvature (Arc):
Element: Linear
Element: Circular
Degree of Curvature (Arc):
Element: Linear
Element: Circular

SWO 4380(1) ALIGNMENT REPORT J/P 24428(04)
Tangent Direction: N 23°57'50.59" E
Element: Circular
Degree of Curvature (Arc):
Element: Linear
Project Name: SWO 4380_1_V2
Description: US81 Existing Alignments
Horizontal Alignment Name: A002
Description: Exist NB US 81
Style: Existing 100
STATION EASTING NORTHING
Element: Circular
Degree of Curvature (Arc):

SWO 4380(1) ALIGNMENT REPORT J/P 24428(04)
Chord Direction: N 23°29'51.10" E
Radial Direction: S 42°56'11.40" E
Tangent Direction: N 47°03'48.60" E
Element: Linear
Element: Circular
Degree of Curvature (Arc):
Element: Linear
Element: Clothoid
Project Name: SWO 4380_1_V2
Description: US81 Existing Alignments
Horizontal Alignment Name: A003
Description: Exist SB US 81
Style: Existing 100
STATION EASTING NORTHING
Element: Linear
Element: Clothoid

SWO 4380(1) ALIGNMENT REPORT J/P 24428(04)
Length: 400.00
Angle: 8°00'00.00" Right
Constant: 756.94
Long Tangent: 266.94
Short Tangent: 133.58
Long Chord: 399.65
Xs: 399.22
Ys: 18.59
P: 4.65
K: 199.87
Tangent Direction: N 0°04'06.40" W
Radial Direction: N 89°55'53.60" E
Chord Direction: N 2°35'52.02" E
Radial Direction: S 82°04'06.40" E
Tangent Direction: N 7°55'53.60" E
Element: Circular
Degree of Curvature (Arc):
Element: Clothoid
Tangent Direction: N 48°14'23.61" E
Radial Direction: S 41°45'36.39" E

Table with columns: PLS, JTJB, OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION, SURVEY DATA SHEET, SDS, 7 OF 76, SWO 4380_(1) PROJECT NO. 24428(04) SHEET NO. S007

| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|-----------|-------------|-----------|--------------|
| | OKLA | | | | |

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
|-------------|-----------|------|

SWO 4380(1) ALIGNMENT REPORT J/P 24428(04)

Element: Linear
 POB (398) 100+00.00 1981439.5820 601412.0549
 PI (464) 112+81.65 1982721.2230 601415.5070
 Tangent Direction: N 89°50'44.43" E
 Tangent Length: 1281.65

Element: Linear
 PI (464) 112+81.65 1982721.2230 601415.5070
 POE (399) 115+87.04 1983026.6137 601416.6165
 Tangent Direction: N 89°47'30.67" E
 Tangent Length: 305.39

Project Name: SWO_4380_1_V2
 Description: US81 Existing Alignments
Horizontal Alignment Name: A012
 Description: 16thSt
 Style: Existing 100

| STATION | EASTING | NORTHING |
|---------|---------|----------|
|---------|---------|----------|

Element: Linear
 POB (464) 100+00.00 1982721.2230 601415.5070
 POE (465) 126+53.46 1982731.3420 604068.9490
 Tangent Direction: N 0°13'06.60" E
 Tangent Length: 2653.46

Project Name: SWO_4380_1_V2
 Description: US81 Existing Alignments
Horizontal Alignment Name: A013
 Description: Country Club Rd
 Style: Existing 100

| STATION | EASTING | NORTHING |
|---------|---------|----------|
|---------|---------|----------|

Element: Linear
 POB (466) 100+00.00 1972152.1270 611997.4360
 POE (467) 126+19.75 1974771.8420 612011.7490
 Tangent Direction: N 89°41'13.07" E
 Tangent Length: 2619.75

Project Name: SWO_4380_1_V2
 Description: US81 Existing Alignments
Horizontal Alignment Name: A014
 Description: Grand Ave
 Style: Existing 100

| STATION | EASTING | NORTHING |
|---------|---------|----------|
|---------|---------|----------|

11

SWO 4380(1) ALIGNMENT REPORT J/P 24428(04)

Element: Linear
 POB (433) 100+00.00 1970029.5777 617309.4145
 PI (468) 121+24.83 1972154.4060 617312.2990
 Tangent Direction: N 89°55'19.99" E
 Tangent Length: 2124.83

Element: Linear
 PI (468) 121+24.83 1972154.4060 617312.2990
 POE (434) 130+53.40 1973082.9725 617310.3684
 Tangent Direction: S 89°52'51.14" E
 Tangent Length: 928.57

Project Name: SWO_4380_1_V2
 Description: US81 Existing Alignments
Horizontal Alignment Name: A015
 Description: Idaho Ave
 Style: Existing 100

| STATION | EASTING | NORTHING |
|---------|---------|----------|
|---------|---------|----------|

Element: Linear
 POB (435) 100+00.00 1971053.5069 622589.7015
 PI (469) 111+00.01 1972153.5150 622587.5440
 Tangent Direction: S 89°53'15.44" E
 Tangent Length: 1100.01

Element: Linear
 PI (469) 111+00.01 1972153.5150 622587.5440
 POE (436) 122+00.01 1973253.5119 622590.1401
 Tangent Direction: N 89°51'53.20" E
 Tangent Length: 1100.00

Project Name: SWO_4380_1_V2
 Description: US81 Existing Alignments
Horizontal Alignment Name: A016
 Description: Iowa Ave
 Style: proposed 100

| STATION | EASTING | NORTHING |
|---------|---------|----------|
|---------|---------|----------|

Element: Linear
 POB (439) 100+00.00 1972401.2398 625238.7968
 PC (440) 101+15.64 1972411.4002 625123.6043
 Tangent Direction: S 5°02'26.40" E
 Tangent Length: 115.64

12

SWO 4380(1) ALIGNMENT REPORT J/P 24428(04)

Element: Circular
 PC (440) 101+15.64 1972411.4002 625123.6043
 PI (441) 102+46.58 1972422.9049 624993.1717
 CC () 1972747.1308 625153.2170
 PT (442) 103+65.42 1972519.4484 624904.7162
 Radius: 337.03
 Delta: 42°27'45.65" Left
 Degree of Curvature(Arc): 16°59'59.99"
 Length: 249.78
 Tangent: 130.94
 Chord: 244.10
 Middle Ordinate: 22.88
 External: 24.54
 Tangent Direction: S 5°02'26.40" E
 Radial Direction: S 84°57'33.60" W
 Chord Direction: S 26°16'19.22" E
 Radial Direction: S 42°29'47.95" W
 Tangent Direction: S 47°30'12.05" E

Element: Linear
 PT (442) 103+65.42 1972519.4484 624904.7162
 PI (341) 111+82.99 1973122.2599 624352.4060
 Tangent Direction: S 47°30'12.05" E
 Tangent Length: 817.57

Element: Linear
 PI (341) 111+82.99 1973122.2599 624352.4060
 PC (443) 116+27.03 1973449.6530 624052.4406
 Tangent Direction: S 47°30'12.02" E
 Tangent Length: 444.03

Element: Circular
 PC (443) 116+27.03 1973449.6530 624052.4406
 PI (444) 118+30.03 1973599.3274 623915.3053
 CC () 1973801.5258 624436.4873
 PT (445) 120+14.16 1973802.3261 623915.6172
 Radius: 520.87
 Delta: 42°35'04.90" Left
 Degree of Curvature(Arc): 11°00'00.00"
 Length: 387.13
 Tangent: 203.00
 Chord: 378.28
 Middle Ordinate: 35.55
 External: 38.16
 Tangent Direction: S 47°30'12.02" E
 Radial Direction: S 42°29'47.95" W
 Chord Direction: S 68°47'44.43" E
 Radial Direction: S 0°05'16.92" E
 Tangent Direction: N 89°54'43.08" E

Element: Linear
 PT (445) 120+14.16 1973802.3261 623915.6172
 POE (446) 129+71.43 1974759.5890 623917.0880

13

SWO 4380(1) ALIGNMENT REPORT J/P 24428(04)

Tangent Direction: N 89°54'43.08" E
 Tangent Length: 957.26

Project Name: SWO_4380_1_V2
 Description: US81 Existing Alignments
Horizontal Alignment Name: A017
 Description: CR 1355
 Style: Default

| STATION | EASTING | NORTHING |
|---------|---------|----------|
|---------|---------|----------|

Element: Linear
 POB (437) 100+00.00 1972102.8245 625238.6000
 PI (470) 100+47.28 1972150.1025 625238.6185
 Tangent Direction: N 89°58'39.45" E
 Tangent Length: 47.28

Element: Linear
 PI (470) 100+47.28 1972150.1025 625238.6185
 POE (438) 105+98.10 1972700.9205 625239.0095
 Tangent Direction: N 89°57'33.60" E
 Tangent Length: 550.82

Project Name: SWO_4380_1_V2
 Description: US81 Existing Alignments
Horizontal Alignment Name: A018
 Description: CS 2815 Rd.
 Style: Existing 100

| STATION | EASTING | NORTHING |
|---------|---------|----------|
|---------|---------|----------|

Element: Linear
 POB (447) 100+00.00 1974761.0641 623620.2837
 PI (471) 116+20.20 1974753.0120 625240.4660
 Tangent Direction: N 0°17'05.10" W
 Tangent Length: 1620.20

Element: Linear
 PI (471) 116+20.20 1974753.0120 625240.4660
 POE (448) 127+55.13 1974747.3442 626375.3838
 Tangent Direction: N 0°17'10.07" W
 Tangent Length: 1134.93

Project Name: SWO_4380_1_V2

14

SWO 4380(1) ALIGNMENT REPORT J/P 24428(04)

Description: US81 Existing Alignments
Horizontal Alignment Name: A019
 Description: 29thSt
 Style: Existing 100

| STATION | EASTING | NORTHING |
|---------|---------|----------|
|---------|---------|----------|

Element: Linear
 POB (449) 100+00.00 1977388.4353 625492.1445
 POE (450) 113+99.84 1977382.5628 626891.9712
 Tangent Direction: N 0°14'25.31" W
 Tangent Length: 1399.84

15

| | | | |
|----------|--------|---|---|
| PLS | JTB | | OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET SDS <u>9</u> OF <u>76</u> |
| DRAWN | JTB | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BENHAM | SWO_4380_(1) PROJECT NO. 24428(04) SHEET NO. S009 | |

Table with columns: PT NO., EASTING, NORTHING for SWO 4380(1) and J/P 24428(04). Includes benchmarks and coordinate point list.

COORDINATE POINT LIST Page 1 of 22

Table with columns: PT NO., EASTING, NORTHING for SWO 4380(1) and J/P 24428(04). Includes benchmarks and coordinate point list.

COORDINATE POINT LIST Page 2 of 22

Table with columns: PT NO., EASTING, NORTHING for SWO 4380(1) and J/P 24428(04). Includes benchmarks, centerline, and coordinate point list.

COORDINATE POINT LIST Page 3 of 22

Table with columns: PT NO., EASTING, NORTHING for SWO 4380(1) and J/P 24428(04). Includes benchmarks and coordinate point list.

COORDINATE POINT LIST Page 4 of 22

Table with columns: PT NO., EASTING, NORTHING for SWO 4380(1) and J/P 24428(04). Includes benchmarks, aerial targets, and coordinate point list.

COORDINATE POINT LIST Page 5 of 22

Table with columns: PT NO., EASTING, NORTHING for SWO 4380(1) and J/P 24428(04). Includes benchmarks, existing right-of-way, control points by others, and coordinate point list.

COORDINATE POINT LIST Page 6 of 22

Form with fields: PLS, DRAWN, CHECKED, APPROVED, CREW, JTB, SWO 4380 () PROJECT NO. 24428(04) SHEET NO. S010

Table with columns: FED. ROAD DIST. NO., STATE, PROJ. NO., FISCAL YEAR, SHEET NO., TOTAL SHEETS

Table with columns: DESCRIPTION, REVISIONS, DATE

Table with columns: SWO 4380(1), COORDINATE POINT LIST, J/P 24428(04), PT NO., EASTING, NORTHING

COORDINATE POINT LIST Page 7 of 22

Table with columns: SWO 4380(1), COORDINATE POINT LIST, J/P 24428(04), PT NO., EASTING, NORTHING

COORDINATE POINT LIST Page 8 of 22

Table with columns: SWO 4380(1), COORDINATE POINT LIST, J/P 24428(04), PT NO., EASTING, NORTHING

COORDINATE POINT LIST Page 9 of 22

Table with columns: SWO 4380(1), COORDINATE POINT LIST, J/P 24428(04), PT NO., EASTING, NORTHING

COORDINATE POINT LIST Page 10 of 23

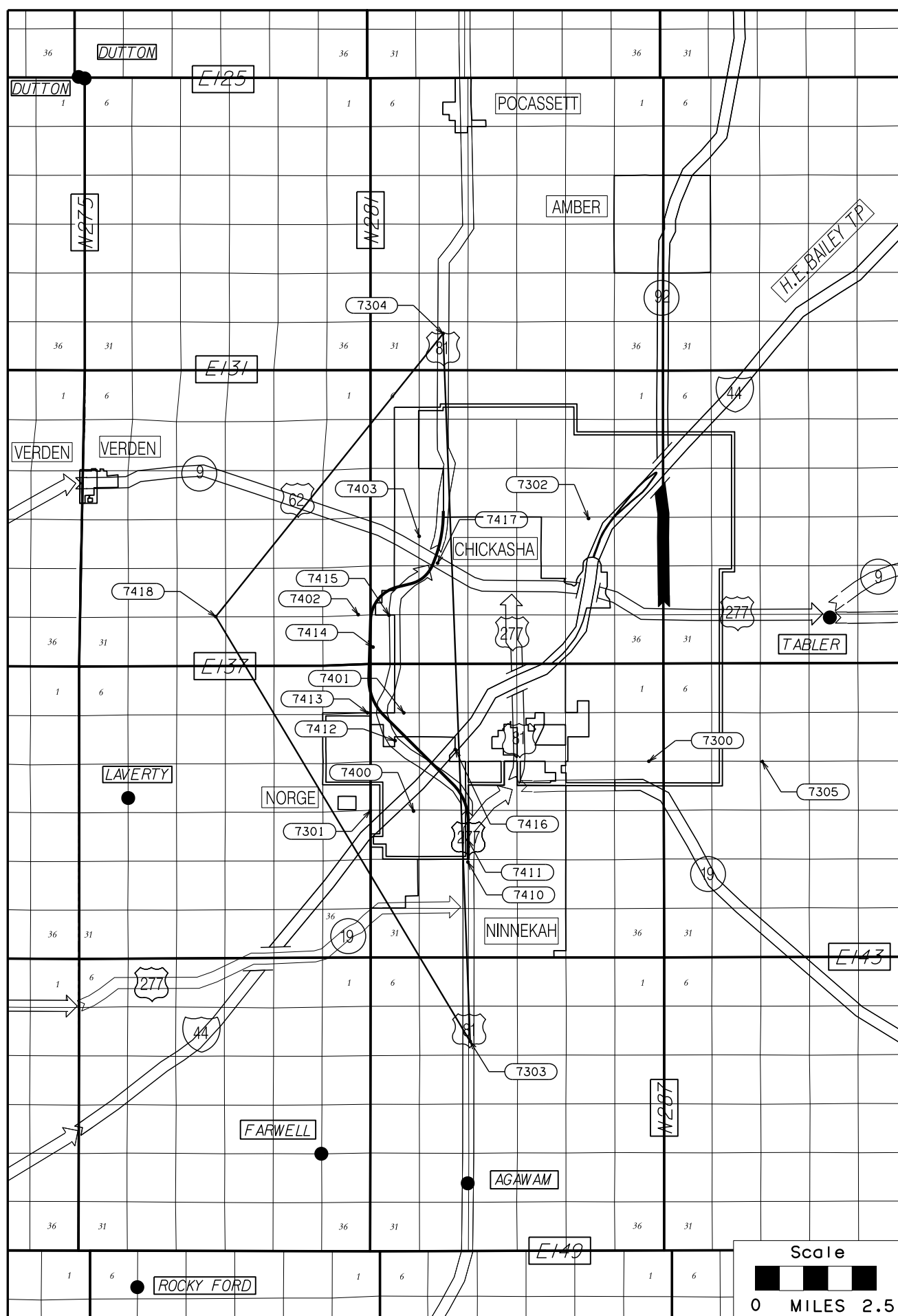
Table with columns: SWO 4380(1), COORDINATE POINT LIST, J/P 24428(04), PT NO., EASTING, NORTHING

COORDINATE POINT LIST Page 11 of 23

Table with columns: SWO 4380(1), COORDINATE POINT LIST, J/P 24428(04), PT NO., EASTING, NORTHING

COORDINATE POINT LIST Page 12 of 23

Table with columns: PLS, JTB, DRAWN, CHECKED, APPROVED, CREW, OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION, SURVEY DATA SHEET, SDS, PROJECT NO., SHEET NO.



Network Adjustment for "D 214" (7303)

NGS OPUS-PROJECTS NETWORK ADJUSTMENT REPORT

All coordinate accuracies reported here are 1 times the formal uncertainties from the solution. For additional information: geodesy.noaa.gov/OPUS/using_OPUS_Projects.html Accuracy

These positions were computed without any knowledge by the National Geodetic Survey regarding the equipment or field operating procedures used.

SUBMITTED BY: bwebb
 SOLUTION FILE NAME: network-d214final.sum
 SOLUTION SOFTWARE: GPSCOM(1210.24)
 SOLUTION DATE: 2015-05-13T10:31:17 UTC
 STANDARD ERROR OF UNIT WEIGHT: 0.577
 TOTAL NUMBER OF OBSERVATIONS: 60076
 TOTAL NUMBER OF MARKS: 9
 NUMBER OF CONSTRAINED MARKS: 8

START TIME: 2015-04-09T17:39:00 GPS
 STOP TIME: 2015-04-09T23:32:00 GPS
 FREQUENCY: L1-ONLY TO ION-FREE [BY BASELINE LENGTH]
 OBSERVATION INTERVAL: 30 s
 ELEVATION CUTOFF: 15 deg
 TROPO INTERVAL: 1800 s [STEP-OFFSET PARAMETERIZATION]
 DD CORRELATIONS: ON

| INCLUDED SOLUTION | RMS | SOFTWARE | RUN DATE |
|-------------------|--------|----------------|----------------------|
| 1) 2015-09-02D14 | 1.2 cm | page5(1404.11) | 2015-05-13T10:05 UTC |
| 2) 2015-09-02D14 | 1.2 cm | page5(1404.11) | 2015-05-13T10:15 UTC |

| BASELINE | LENGTH | RMS | OBS | OMITTED | FIXED IN SOLUTION(S) |
|-----------|------------|--------|------|---------|----------------------|
| 7125-5978 | 13.342 km | 1.8 cm | 4110 | 4.7% | 100.0% 1, 2 |
| okao-5978 | 23.356 km | 1.0 cm | 7976 | 1.4% | 100.0% 1, 2 |
| okao-5978 | 32.132 km | 1.3 cm | 8248 | 1.3% | 100.0% 1, 2 |
| oklu-5978 | 56.750 km | 1.2 cm | 7298 | 10.6% | 100.0% 1, 2 |
| oklt-5978 | 75.334 km | 1.1 cm | 8098 | 2.2% | 100.0% 1, 2 |
| wmok-5978 | 78.382 km | 1.0 cm | 8256 | 1.1% | 94.4% 1, 2 |
| okar-5978 | 109.824 km | 1.4 cm | 8152 | 2.3% | 95.0% 1, 2 |
| okcl-5978 | 112.090 km | 1.1 cm | 7938 | 2.9% | 100.0% 1, 2 |

UNCONSTRAINED MARKS

MARK: 5978 (5978 1)

REF FRAME: NAD_83(2011) (2010.0000) IGS08 (2015.2709)
 X: -724344.945 m 0.001 m -724345.753 m 0.001 m
 Y: -5185621.433 m 0.004 m -5185620.031 m 0.004 m
 Z: 3630542.442 m 0.004 m 3630542.292 m 0.004 m
 LAT: 34 55 02.21320 0.002 m 34 55 02.23293 0.002 m
 E LON: 262 02 53.46469 0.001 m 262 02 53.42551 0.001 m
 W LON: 97 57 06.53531 0.001 m 97 57 06.57449 0.001 m
 EL HGT: 343.311 m 0.005 m 342.179 m 0.005 m
 ORTHO HGT: 369.539 m 0.010 m (H = h - N WHERE N = GEOID12A HGT)

UTM COORDINATES STATE PLANE COORDINATES
 UTM (Zone 14) SPC (3502 OK S)
 NORTHING (Y) 3881775.765 m 193378.324 m
 EASTING (X) 568750.537 m 577574.765 m
 CONVERGENCE 0.43388055 deg -0.13997067 deg
 POINT SCALE 0.99965825 0.99997275
 COMBINED FACTOR 0.99960482 0.99991931

US NATIONAL GRID DESIGNATOR: 145ND6875081775 (NAD 83)

MARK: okar (okar a 3)
 CONSTRN: HOR-ONLY NORMAL
 ADJUST X: -0.007m (0.001m) Y: -0.039m (0.004m) Z: 0.027m (0.004m)
 ADJUST N: 0.000m (0.002m) E: -0.002m (0.001m) H: 0.048m (0.005m)

REF FRAME: NAD_83(2011) (2010.0000) IGS08 (2015.2709)
 X: -659318.976 m 0.001 m -659319.782 m 0.001 m
 Y: -5241679.389 m 0.004 m -5241677.972 m 0.004 m
 Z: 3562055.730 m 0.004 m 3562055.575 m 0.004 m
 LAT: 34 10 06.45066 0.002 m 34 10 06.47030 0.002 m
 E LON: 262 49 50.74924 0.001 m 262 49 50.71110 0.001 m
 W LON: 97 10 09.28976 0.001 m 97 10 09.28890 0.001 m
 EL HGT: 236.971 m 0.005 m 235.604 m 0.005 m
 ORTHO HGT: 263.085 m 0.010 m (H = h - N WHERE N = GEOID12A HGT)

UTM COORDINATES STATE PLANE COORDINATES
 UTM (Zone 14) SPC (3502 OK S)

Network Adjustment for "D 214" (7303)

UNCONSTRAINED MARKS

MARK: 7125 (7125 1)

CONSTRN: VER-ONLY NORMAL
 ADJUST X: -0.004m (0.001m) Y: -0.045m (0.004m) Z: 0.042m (0.003m)
 ADJUST N: 0.008m (0.002m) E: 0.002m (0.001m) H: 0.061m (0.005m)

REF FRAME: NAD_83(2011) (2010.0000) IGS08 (2015.2710)
 X: -714059.069 m 0.001 m -714059.878 m 0.001 m
 Y: -5181677.945 m 0.004 m -5181676.544 m 0.004 m
 Z: 3638070.518 m 0.003 m 3638070.370 m 0.003 m
 LAT: 35 00 01.31683 0.002 m 35 00 01.33667 0.002 m
 E LON: 262 09 13.66416 0.001 m 262 09 13.62501 0.001 m
 W LON: 97 50 46.33584 0.001 m 97 50 46.37499 0.001 m
 EL HGT: 297.023 m 0.005 m 295.892 m 0.005 m
 ORTHO HGT: 323.343 m 0.010 m (H = h - N WHERE N = GEOID12A HGT)

UTM COORDINATES STATE PLANE COORDINATES
 UTM (Zone 14) SPC (3502 OK S)
 NORTHING (Y) 3873691.742 m 184826.481 m
 EASTING (X) 605287.744 m 614039.121 m
 CONVERGENCE 0.66185685 deg 0.08729695 deg
 POINT SCALE 0.99978662 0.99996225
 COMBINED FACTOR 0.99969001 0.99991563

US NATIONAL GRID DESIGNATOR: 145PD052873691 (NAD 83)

MARK: okao (okao a 2)
 CONSTRN: HOR-ONLY NORMAL
 ADJUST X: -0.010m (0.001m) Y: -0.042m (0.004m) Z: 0.027m (0.004m)
 ADJUST N: -0.003m (0.002m) E: -0.004m (0.001m) H: 0.051m (0.005m)

REF FRAME: NAD_83(2011) (2010.0000) IGS08 (2015.2709)
 X: -749498.285 m 0.001 m -749499.090 m 0.001 m
 Y: -5171811.793 m 0.004 m -5171811.389 m 0.004 m
 Z: 3645002.537 m 0.004 m 3645002.390 m 0.004 m
 LAT: 35 04 35.04542 0.002 m 35 04 35.06527 0.002 m
 E LON: 261 45 14.79978 0.001 m 261 45 14.76038 0.001 m
 W LON: 98 14 45.20022 0.001 m 98 14 45.23962 0.001 m
 EL HGT: 340.497 m 0.005 m 339.370 m 0.005 m
 ORTHO HGT: 367.304 m 0.010 m (H = h - N WHERE N = GEOID12A HGT)

UTM COORDINATES STATE PLANE COORDINATES
 UTM (Zone 14) SPC (3502 OK S)
 NORTHING (Y) 3881775.765 m 193378.324 m
 EASTING (X) 568750.537 m 577574.765 m
 CONVERGENCE 0.43388055 deg -0.13997067 deg
 POINT SCALE 0.99965825 0.99997275
 COMBINED FACTOR 0.99960482 0.99991931

US NATIONAL GRID DESIGNATOR: 145ND6875081775 (NAD 83)

MARK: okar (okar a 3)
 CONSTRN: HOR-ONLY NORMAL
 ADJUST X: -0.007m (0.001m) Y: -0.039m (0.004m) Z: 0.027m (0.004m)
 ADJUST N: 0.000m (0.002m) E: -0.002m (0.001m) H: 0.048m (0.005m)

REF FRAME: NAD_83(2011) (2010.0000) IGS08 (2015.2709)
 X: -659318.976 m 0.001 m -659319.782 m 0.001 m
 Y: -5241679.389 m 0.004 m -5241677.972 m 0.004 m
 Z: 3562055.730 m 0.004 m 3562055.575 m 0.004 m
 LAT: 34 10 06.45066 0.002 m 34 10 06.47030 0.002 m
 E LON: 262 49 50.74924 0.001 m 262 49 50.71110 0.001 m
 W LON: 97 10 09.28976 0.001 m 97 10 09.28890 0.001 m
 EL HGT: 236.971 m 0.005 m 235.604 m 0.005 m
 ORTHO HGT: 263.085 m 0.010 m (H = h - N WHERE N = GEOID12A HGT)

UTM COORDINATES STATE PLANE COORDINATES
 UTM (Zone 14) SPC (3502 OK S)

Table with columns: FED. ROAD DIST. NO., STATE, PROJ. NO., FISCAL YEAR, SHEET NO., TOTAL SHEETS. Values include OKLA, PROJ. NO., FISCAL YEAR, SHEET NO., TOTAL SHEETS.

Table with columns: DESCRIPTION, REVISIONS, DATE.

Network Adjustment for "D 214" (7303)

NGS OPUS-PROJECTS NETWORK ADJUSTMENT REPORT
All coordinate accuracies reported here are 1 times the formal uncertainties from the solution. For additional information: geodesy.noaa.gov/OPUS/using_OPUS-Projects.html#accuracy
These positions were computed without any knowledge by the National Geodetic Survey regarding the equipment or field operating procedures used.

Network Adjustment for "Pocasset" (7304)

NGS OPUS-PROJECTS NETWORK ADJUSTMENT REPORT
All coordinate accuracies reported here are 1 times the formal uncertainties from the solution. For additional information: geodesy.noaa.gov/OPUS/using_OPUS-Projects.html#accuracy
These positions were computed without any knowledge by the National Geodetic Survey regarding the equipment or field operating procedures used.

Network Adjustment for "Pocasset" (7304)

POINT SCALE 0.99965825 0.99997275
COMBINED FACTOR 0.99960482 0.99991931
US NATIONAL GRID DESIGNATOR: 145ND0687508175 (NAD 83)
CONSTRAN: HOR-ONLY NORMAL
ADJUST X: -0.010m (0.001m) Y: -0.050m (0.004m) Z: 0.036m (0.004m)

Network Adjustment for "D 214" (7303)

EL HGT: 314.751 m 0.005 m 313.626 m 0.005 m
ORTHOG HGT: 340.377 m 0.010 m (H = h - N WHERE N = GEOD12A HGT)
UTM COORDINATES STATE PLANE COORDINATES
NORTHING (Y) 3928421.957 m 54494.520 m
EASTING (X) 635364.257 m 644673.365 m

Network Adjustment for "Pocasset" (7304)

MARK: 5978 (5978 1)
CONSTRAN: VER-ONLY NORMAL
ADJUST X: -0.004m (0.001m) Y: -0.051m (0.004m) Z: 0.043m (0.003m)
ADJUST N: 0.006m (0.002m) E: 0.003m (0.001m) H: 0.066m (0.005m)
REF FRAME: NAD_83(2011) (2010.0000) IGS08 (2015.2709)

Network Adjustment for "Pocasset" (7304)

UTM COORDINATES STATE PLANE COORDINATES
NORTHING (Y) 3928421.957 m 54494.520 m
EASTING (X) 635364.257 m 644673.365 m
CONVERGENCE 0.33486194 deg -0.5730269 deg
POINT SCALE 0.99960482 0.99997275

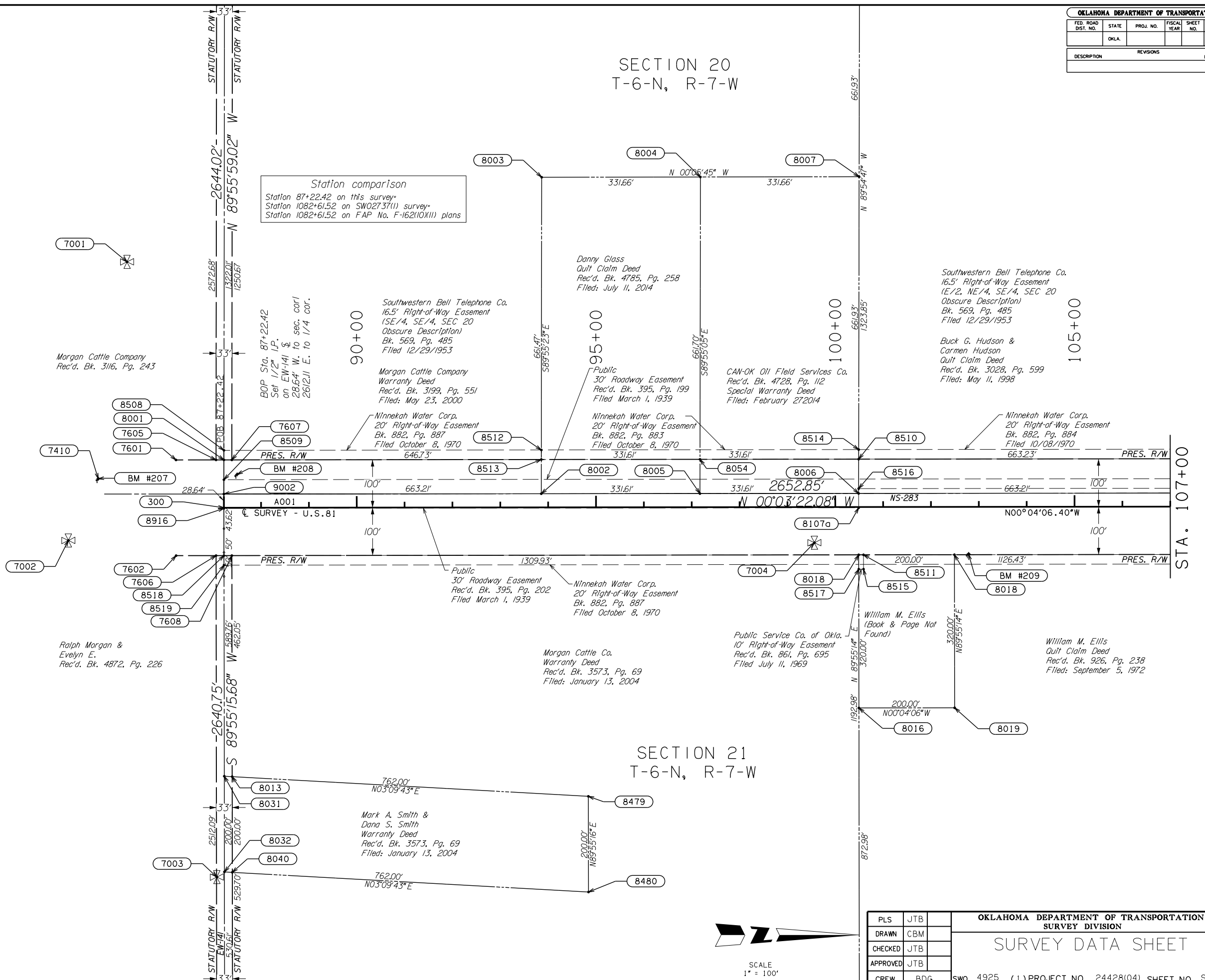
Table with columns: PLS, JTB, OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION, SURVEY DATA SHEET, SDS 15 OF 76, CREW, BENHAM, SWO 4380 (1) PROJECT NO. 24428(04) SHEET NO. S015

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

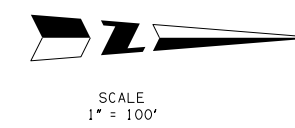
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |
| | | | | | |

SECTION 20
T-6-N, R-7-W

Station comparison
 Station 87+22.42 on this survey*
 Station 1082+61.52 on SW0273(1) survey*
 Station 1082+61.52 on FAP No. F-162(10XII) plans



| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | |
|---|-----|--|--|
| SURVEY DIVISION | | | |
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S019 | | | |



UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

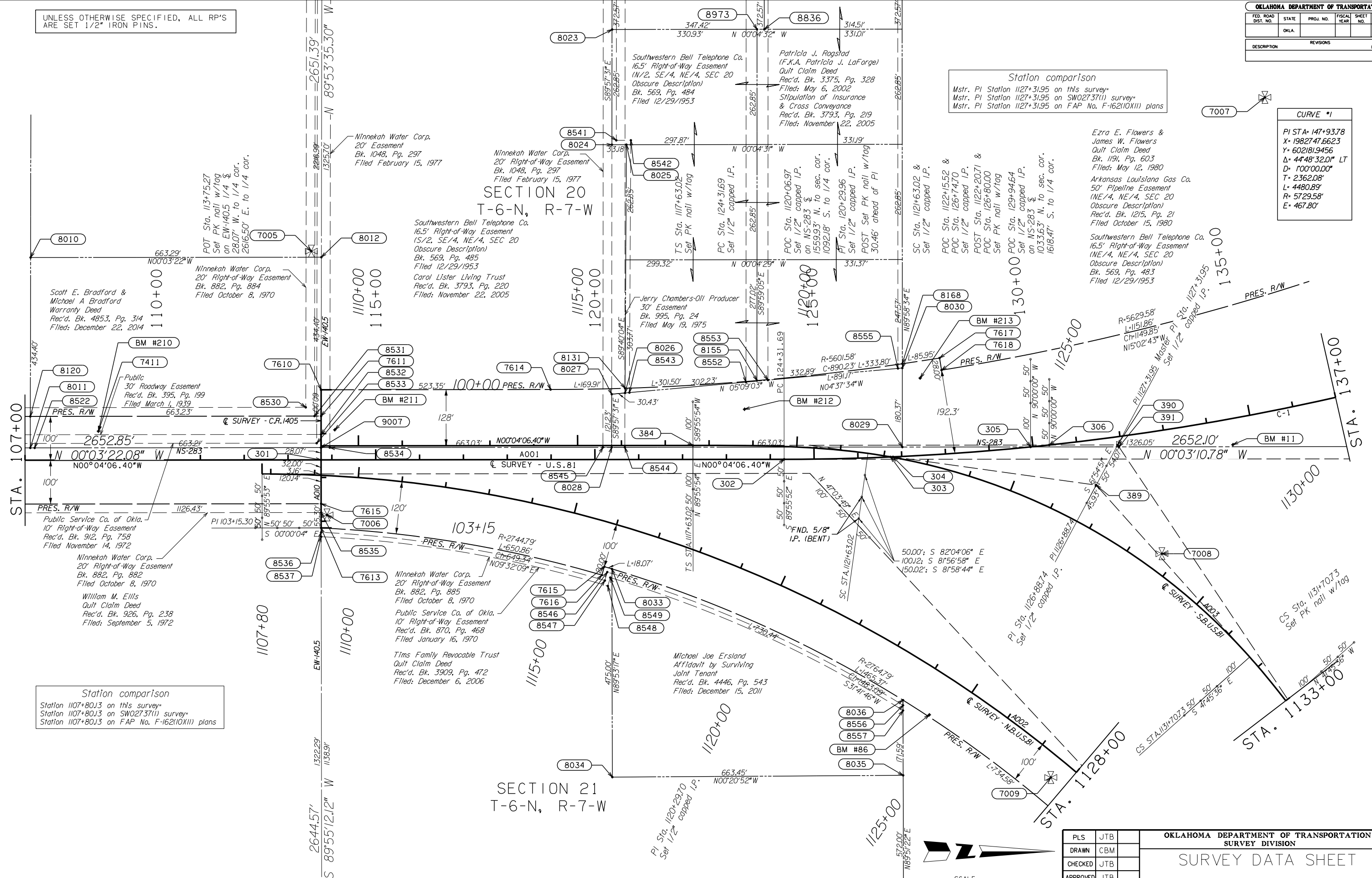
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |

Station comparison
 Mstr. PI Station 1127+31.95 on this survey
 Mstr. PI Station 1127+31.95 on SW02737(1) survey
 Mstr. PI Station 1127+31.95 on FAP No. F-16210(XII) plans

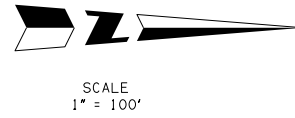
| CURVE #1 |
|--------------------|
| PI STA= 147+93.78 |
| X= 1982747.6623 |
| Y= 602181.9456 |
| Δ= 44°48'32.01" LT |
| D= 100°00.00" |
| T= 2362.08' |
| L= 4480.89' |
| R= 5729.58' |
| E= 467.80' |

SECTION 20
T-6-N, R-7-W

SECTION 21
T-6-N, R-7-W



Station comparison
 Station 1107+80.13 on this survey
 Station 1107+80.13 on SW02737(1) survey
 Station 1107+80.13 on FAP No. F-16210(XII) plans



| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | |
|---|-----|--|--|
| SURVEY DIVISION | | | |
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S020 | | | |

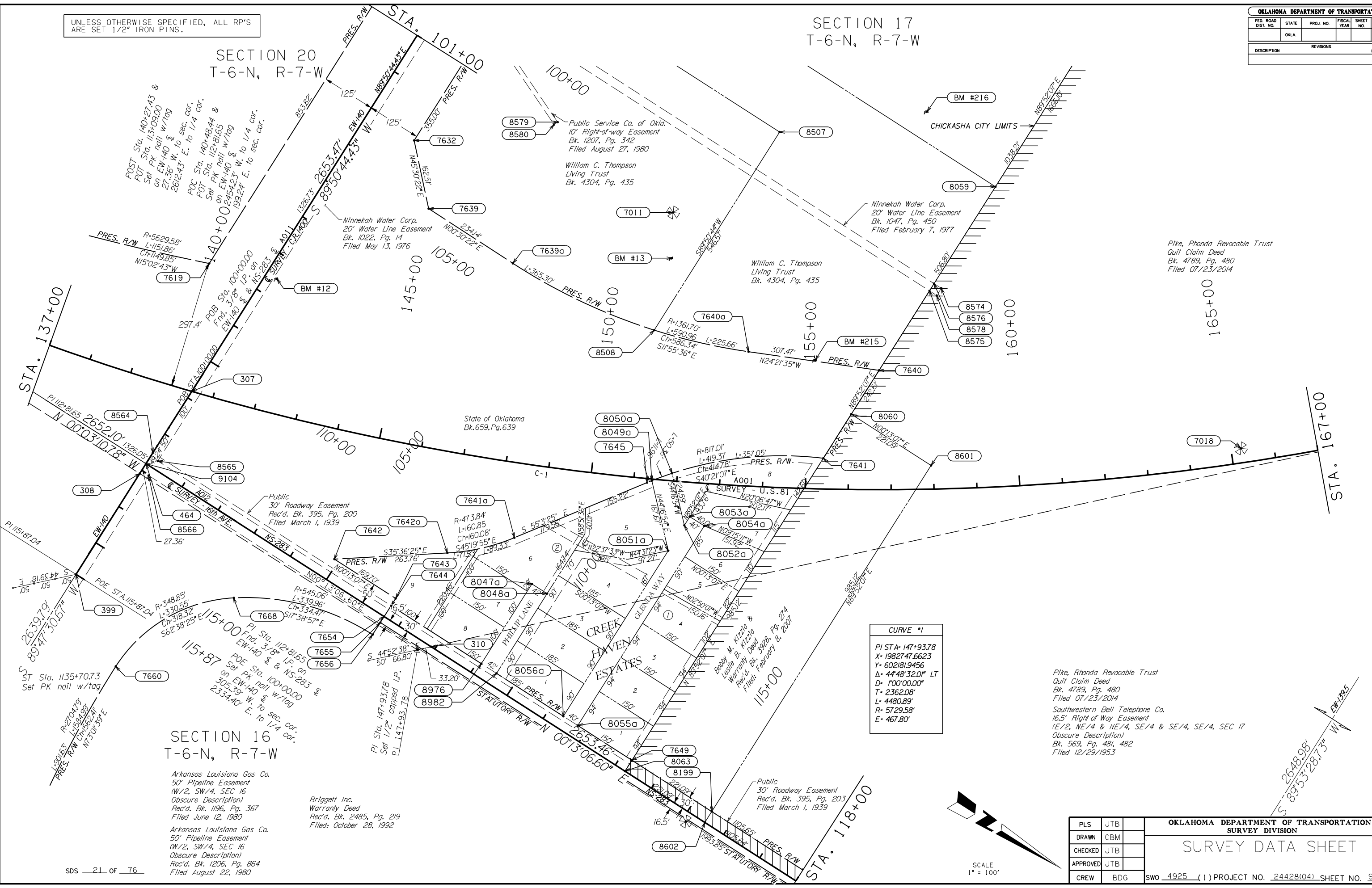
UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |

SECTION 20
T-6-N, R-7-W

SECTION 17
T-6-N, R-7-W

SECTION 16
T-6-N, R-7-W



| CURVE #1 | |
|----------|-----------------|
| PI STA | 147+93.78 |
| X | 1982747.6623 |
| Y | 6021819456 |
| Δ | 44°48'32.01" LT |
| D | 100'00.00" |
| T | 2362.08' |
| L | 4480.89' |
| R | 5729.58' |
| E | 467.80' |

Pike, Rhonda Revocable Trust
Quit Claim Deed
Bk. 4789, Pg. 480
Filed 07/23/2014

Southwestern Bell Telephone Co.
16.5' Right-of-Way Easement
(E/2, NE/4 & NE/4, SE/4 & SE/4, SE/4, SEC 17
Obscure Description)
Bk. 569, Pg. 481, 482
Filed 12/29/1953

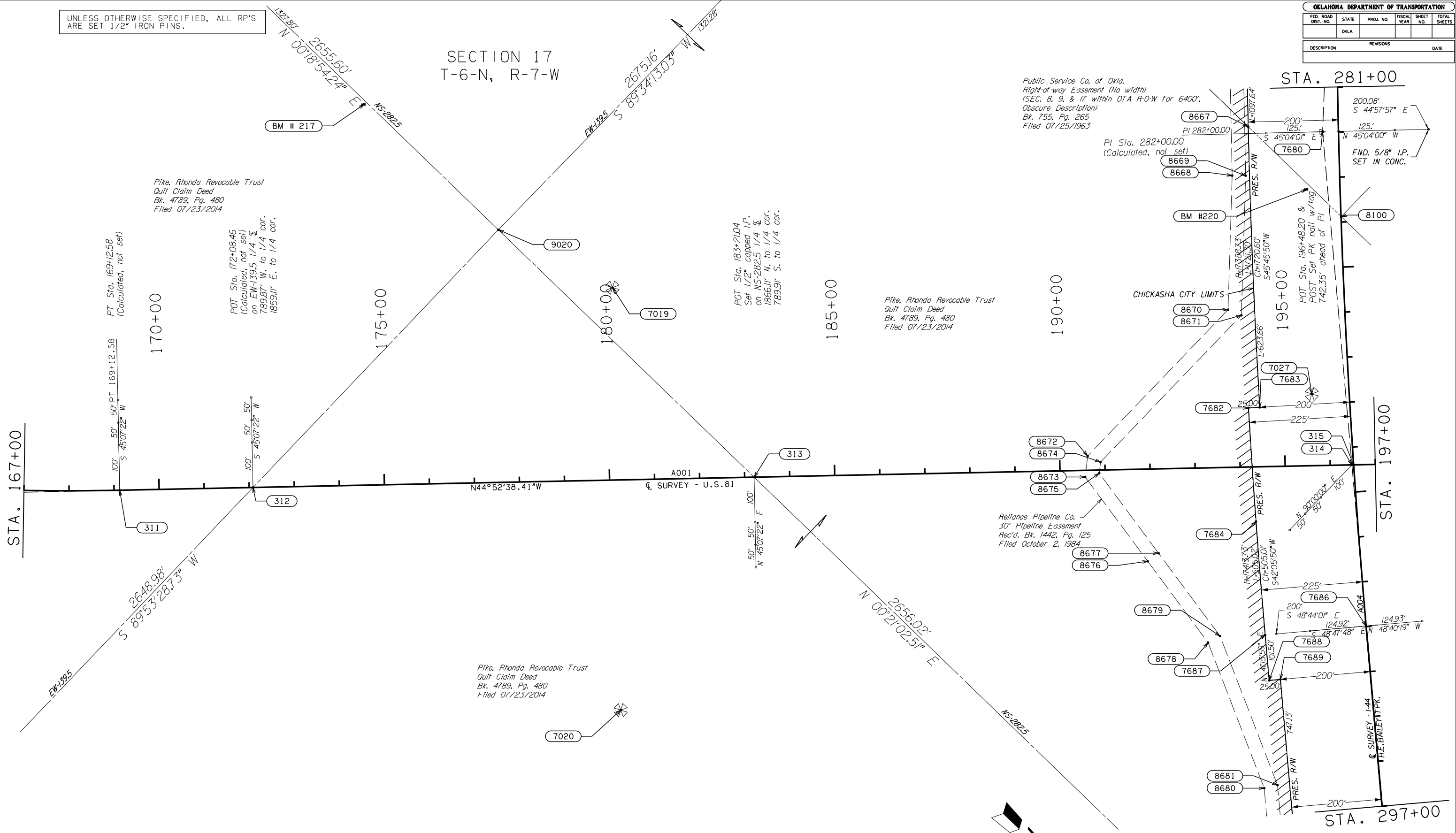
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | |
|---------------------------------------|-----|----------------------|----------------|
| SURVEY DIVISION | | | |
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) | | PROJECT NO. 24428104 | SHEET NO. S021 |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | | DATE |
| | | | | | |

SECTION 17
T-6-N, R-7-W

SECTION 17
T-6-N, R-7-W



Public Service Co. of Okla.
Right-of-way Easement (No width)
(SEC. 8, 9, & 17 within OTA R-O-W for 6400';
Obscure Description)
Bk. 755, Pg. 265
Filed 07/25/1963

Pike, Rhonda Revocable Trust
Quit Claim Deed
Bk. 4789, Pg. 480
Filed 07/23/2014

POT Sta. 172+08.46
(Calculated, not set)
on EW-139.5 1/4 S.
789.87' W. to 1/4 cor.
1859.11' E. to 1/4 cor.

POT Sta. 183+21.04
Set 1/2" capped I.P.
on NS-282.5 1/4 S.
1866.11' N. to 1/4 cor.
789.91' S. to 1/4 cor.

Pike, Rhonda Revocable Trust
Quit Claim Deed
Bk. 4789, Pg. 480
Filed 07/23/2014

Reliance Pipeline Co.
30' Pipeline Easement
Rec'd. Bk. 1442, Pg. 125
Filed October 2, 1984

Pike, Rhonda Revocable Trust
Quit Claim Deed
Bk. 4789, Pg. 480
Filed 07/23/2014

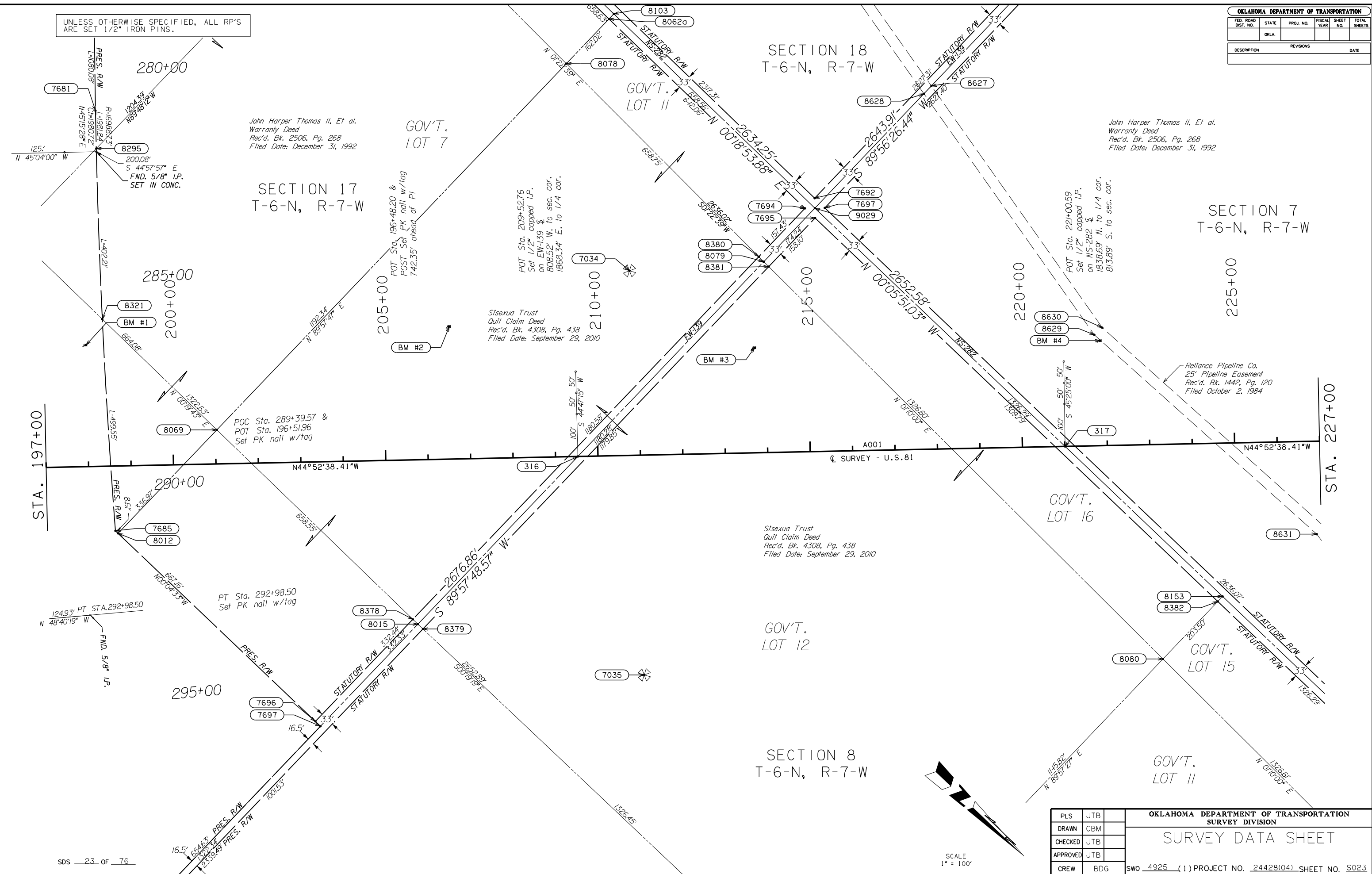
| PLS | JTB | OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | |
|----------|-----|---|--|--|--|
| DRAWN | CBM | SURVEY DIVISION | | | |
| CHECKED | JTB | SURVEY DATA SHEET | | | |
| APPROVED | JTB | | | | |
| CREW | BDG | | | | |
| | | SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S022 | | | |

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |
| | | | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

John Harper Thomas II, Et al.
Warranty Deed
Rec'd. Bk. 2506, Pg. 268
Filed Date: December 31, 1992

John Harper Thomas II, Et al.
Warranty Deed
Rec'd. Bk. 2506, Pg. 268
Filed Date: December 31, 1992



| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | |
|---|-----|--|--|
| SURVEY DIVISION | | | |
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S023 | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

SECTION 7
T-6-N, R-7-W

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |
| | | | | | |

John H. Thomas II, Etal
Warranty Deed
Rec'd. Bk. 2266, Pg. 184
Filed December 26, 1990

Chester Stanton Thomas, Etal
Warranty Deed
Rec'd. Bk. 2506, Pg. 270
Filed December 31, 1992

POT Sta. 176+78.23
Set PK nail w/1tag
on EW-138.5 1/4 &
293.04' W. to 1/4 cor.
2349.37' E. to 1/4 cor.

Public
80' Roadway Easement
Rec'd. Bk. 498, Pg. 377
Filed August 13, 1947

PC Sta. 179+36.62
Set PK nail w/1tag

POC Sta. 180+64.05 &
POT Sta. 251+58.67
Set 1/2" capped I.P.
Set PK nail w/1tag

PI Sta. 182+67.67
Fnd. 1/2" I.P.

Norge Water and Sewer Co. Inc.
20' Easement
Bk. 1020, Pg. 429
Filed April 23, 1976

Public Service Co. of Okla.
Right-of-way Easement
(No width specified)
(SW/4, NE/4, Sec. 7
Obscure Description)
Bk. 818, Pg. 691
Filed August 8, 1966

Chester Stanton Thomas, Etal
Warranty Deed
Rec'd. Bk. 2506, Pg. 270
Filed December 31, 1992

John H. Thomas II, Etal
Warranty Deed
Rec'd. Bk. 2266, Pg. 185
Filed December 26, 1990

Reliance Pipeline Co.
25' Pipeline Easement
Rec'd. Bk. 1534, Pg. 219
Filed April 17, 1985

Southwestern Light & Power Co.
Right-of-Way (No width specified)
Bk. 259, Pg. 76, 86, 90
Filed March 15, 1927

STA. 227+00

STA. 257+00

230+00

235+00

240+00

245+00

115+00

120+00

125+00

185+00

STA. 227+00 A001 SURVEY - U.S. 81

CHICKASHA CITY LIMITS

GOV'T.
LOT 15
Sisexua Trust
Quit Claim Deed
Rec'd. Bk. 4308, Pg. 438
Filed Date: September 29, 2010

SECTION 7
T-6-N, R-7-W

SCALE
1" = 100'

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | |
|---------------------------------------|-----|--------------------------------------|--|
| SURVEY DIVISION | | | |
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) | | PROJECT NO. 24428(04) SHEET NO. S024 | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | | REVISIONS | | DATE |
| | | | | | |

Chester Stanton Thomas, Etal
Warranty Deed
Rec'd. Bk. 2506, Pg. 270
Filed December 31, 1992

POC Sta. 283+51.69 &
POT Sta. 109+02.53
Set PK nail w/tag
on EW-138 &
902.53' W. to sec. cor.
177-22' E. to 1/4 cor.

POST Sta. 284+08.38 &
POT Sta. 108+17.55
Set PK nail w/tag
on EW-138 &
817.55' W. to sec. cor.
1802.20' E. to 1/4 cor.

GOV'T.
LOT 5

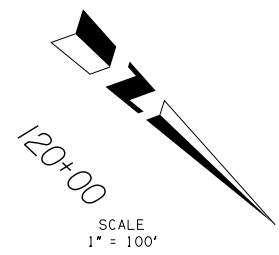
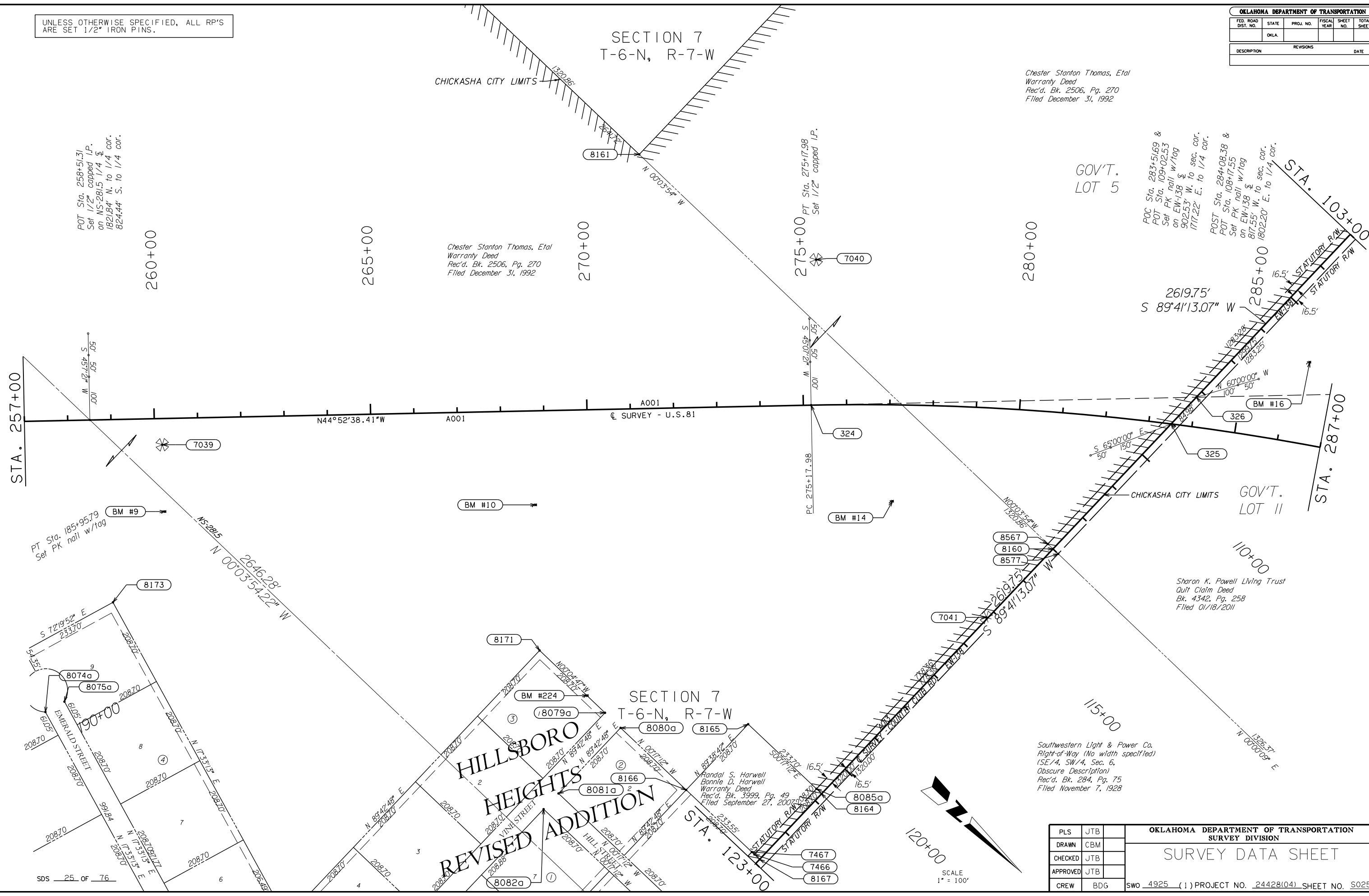
Sharon K. Powell Living Trust
Quit Claim Deed
Bk. 4342, Pg. 258
Filed 01/18/2011

Southwestern Light & Power Co.
Right-of-Way (No width specified)
(SE/4, SW/4, Sec. 6,
Obscure Description)
Rec'd. Bk. 284, Pg. 75
Filed November 7, 1928

SECTION 7
T-6-N, R-7-W

SECTION 7
T-6-N, R-7-W

HILLSBORO
HEIGHTS
REVISED ADDITION

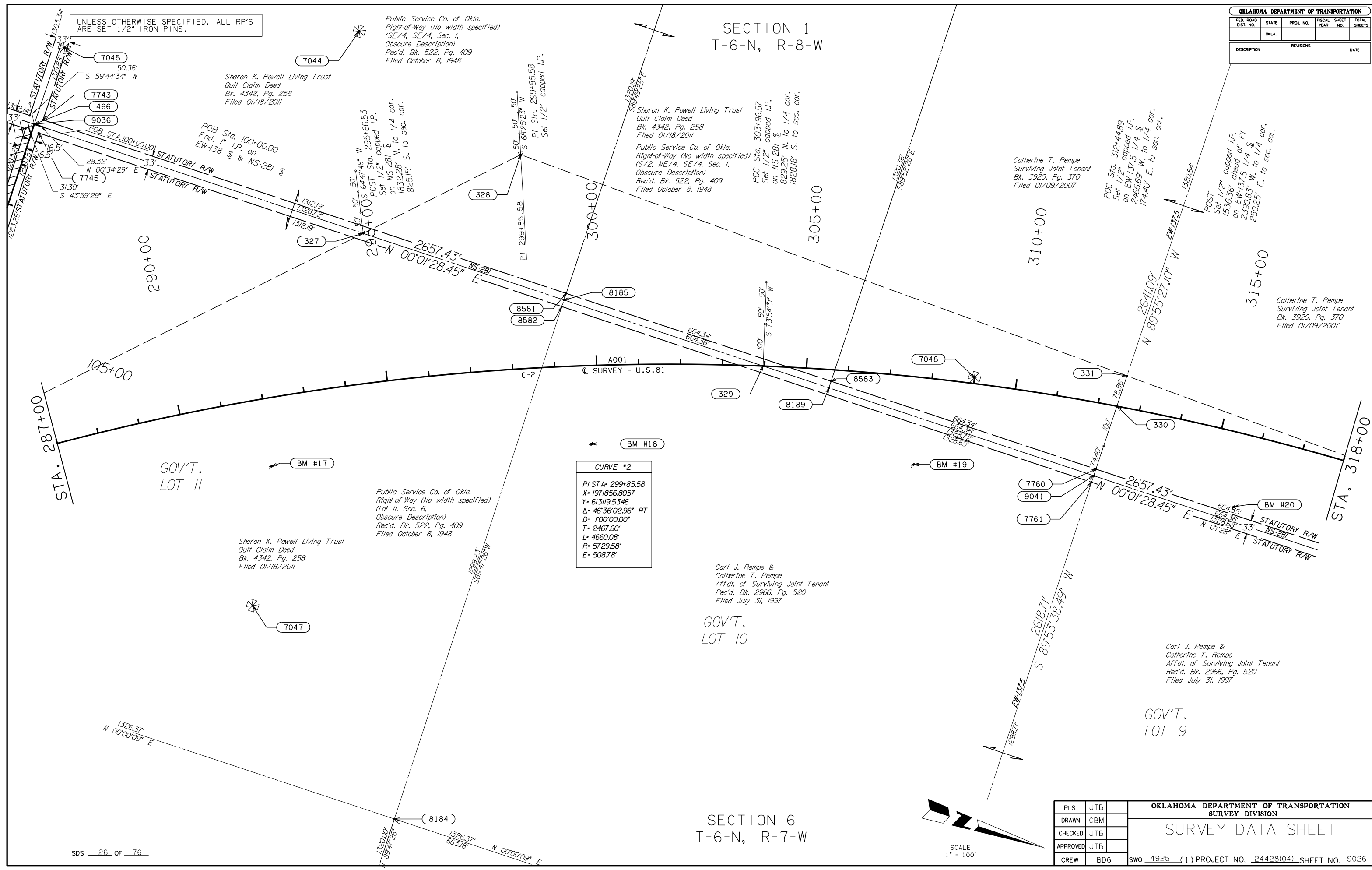


SCALE
1" = 100'

| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | | | |
|--|-----|--|--|
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO_4925_(1)_PROJECT NO. 24428(04)_SHEET NO. S025 | | | |

SECTION 1
T-6-N, R-8-W

SECTION 6
T-6-N, R-7-W



UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

Public Service Co. of Okla.
Right-of-Way (No width specified)
(SE/4, SE/4, Sec. 1,
Obscure Description)
Rec'd. Bk. 522, Pg. 409
Filed October 8, 1948

Sharon K. Powell Living Trust
Quit Claim Deed
Bk. 4342, Pg. 258
Filed 01/18/2011

Sharon K. Powell Living Trust
Quit Claim Deed
Bk. 4342, Pg. 258
Filed 01/18/2011

Public Service Co. of Okla.
Right-of-Way (No width specified)
(SE/2, NE/4, SE/4, Sec. 1,
Obscure Description)
Rec'd. Bk. 522, Pg. 409
Filed October 8, 1948

Catherine T. Rempe
Surviving Joint Tenant
Bk. 3920, Pg. 370
Filed 01/09/2007

POC Sta. 312+44.89
Set 1/2" capped I.P.
on EW-137.5 1/4 §
2486.69' W. to 1/4 §
174.40' E. to sec. cor.

POST
Set 1/2" capped I.P.
1536.36' ahead of P.I.
on EW-137.5 1/4 §
2390.83' W. to 1/4 §
250.25' E. to sec. cor.

Catherine T. Rempe
Surviving Joint Tenant
Bk. 3920, Pg. 370
Filed 01/09/2007

Sharon K. Powell Living Trust
Quit Claim Deed
Bk. 4342, Pg. 258
Filed 01/18/2011

Public Service Co. of Okla.
Right-of-Way (No width specified)
(Lot II, Sec. 6,
Obscure Description)
Rec'd. Bk. 522, Pg. 409
Filed October 8, 1948

Carl J. Rempe &
Catherine T. Rempe
Affdt. of Surviving Joint Tenant
Rec'd. Bk. 2966, Pg. 520
Filed July 31, 1997

Carl J. Rempe &
Catherine T. Rempe
Affdt. of Surviving Joint Tenant
Rec'd. Bk. 2966, Pg. 520
Filed July 31, 1997

| CURVE #2 |
|--------------------|
| PI STA= 299+85.58 |
| X= 1971856.8057 |
| Y= 613119.5346 |
| Δ= 46°36'02.96" RT |
| D= 100°00.00" |
| T= 2467.60' |
| L= 4660.08' |
| R= 5729.58' |
| E= 508.78' |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

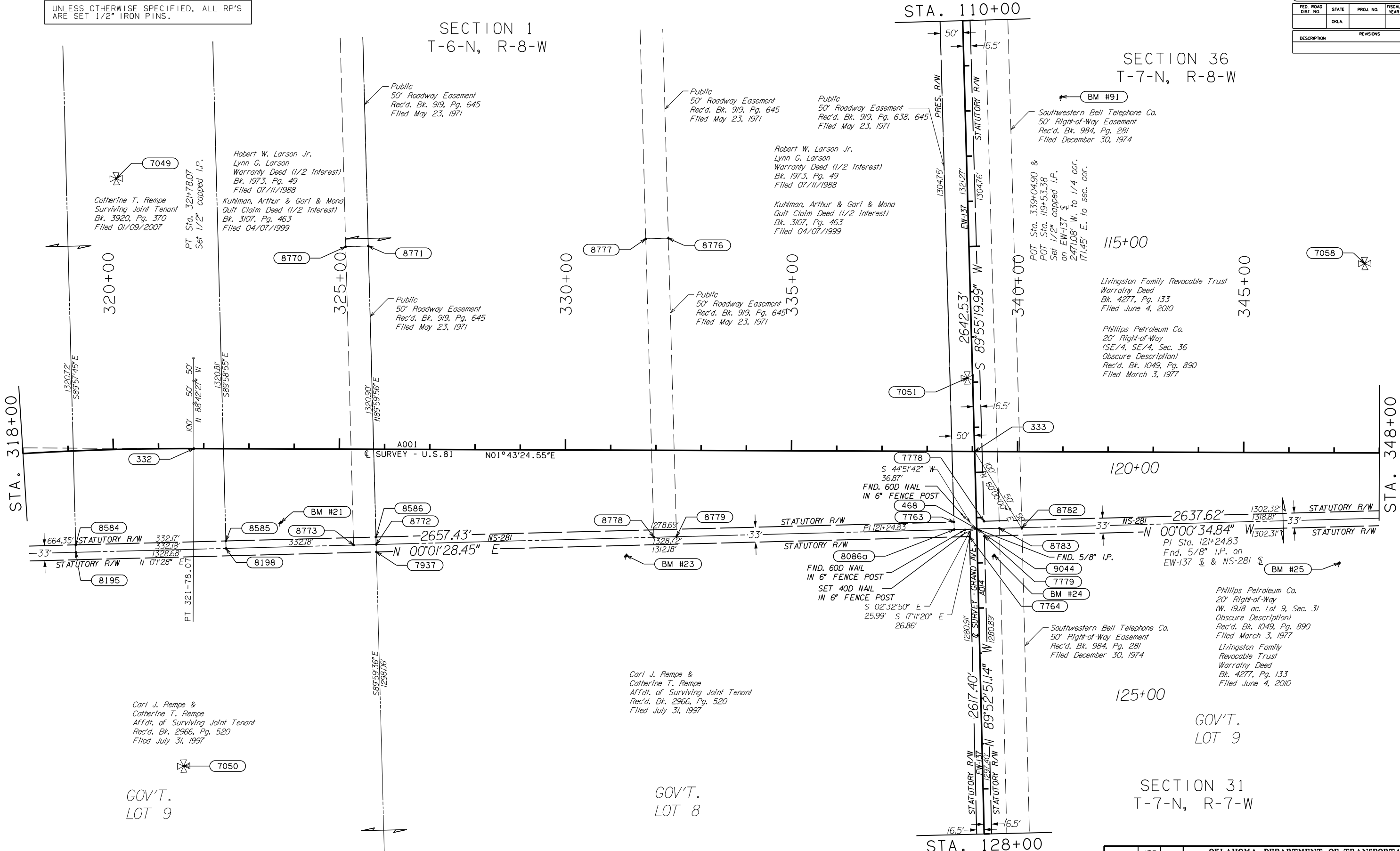
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|--|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS | |
| | OKLA | | | | | |
| DESCRIPTION | | REVISIONS | | DATE | | |
| | | | | | | |

SECTION 1
T-6-N, R-8-W

SECTION 36
T-7-N, R-8-W

SECTION 31
T-7-N, R-7-W

SECTION 6
T-6-N, R-7-W



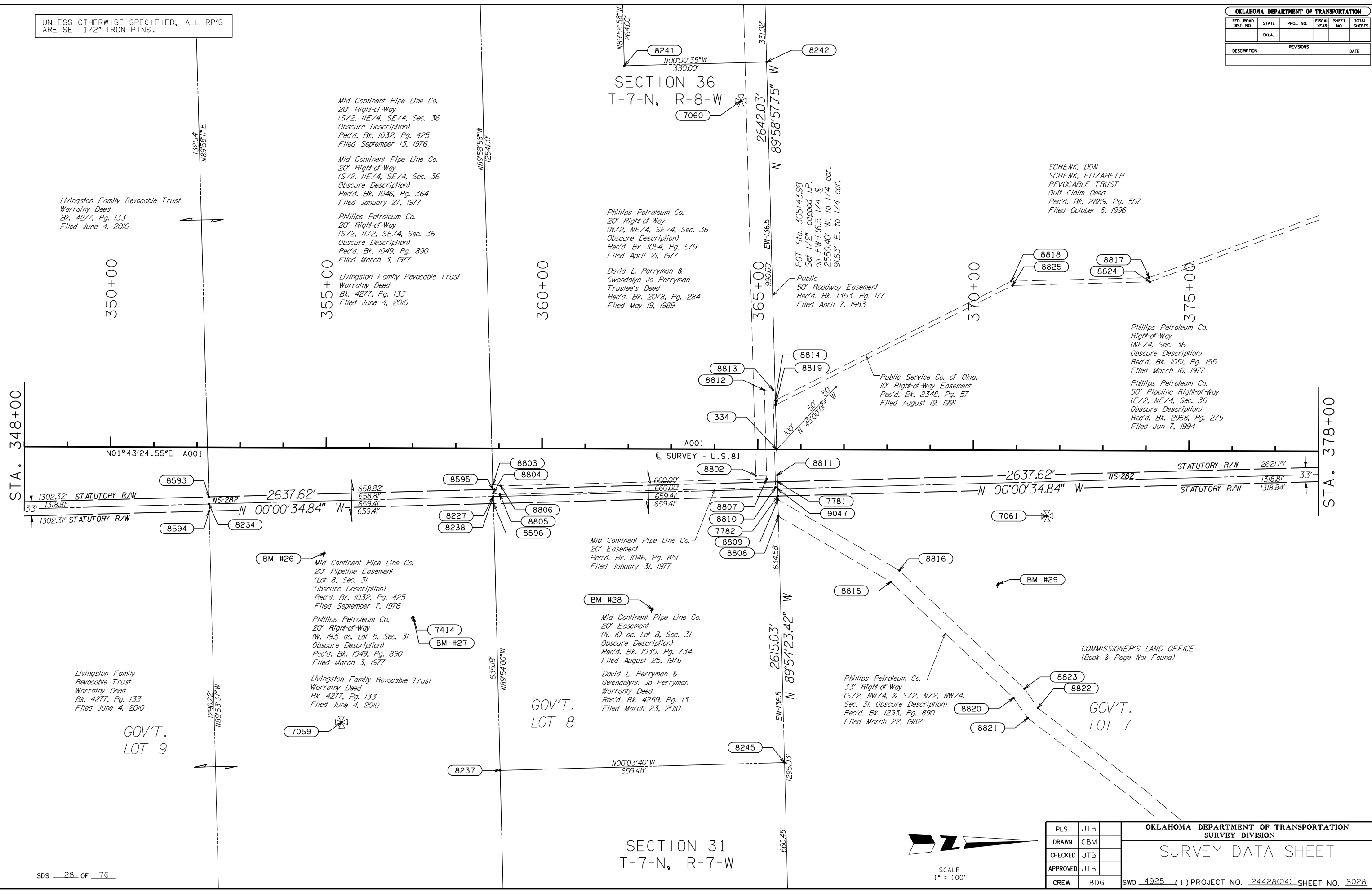
| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | | | |
|--|-----|--|--|
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SURVEY DATA SHEET | | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S027 | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | | | REVISIONS | DATE |

SECTION 36
T-7-N, R-8-W

SECTION 31
T-7-N, R-7-W



Livingston Family Revocable Trust
Warranty Deed
Bk. 4277, Pg. 133
Filed June 4, 2010

Mid Continent Pipe Line Co.
20' Right-of-Way
(S/2, NE/4, SE/4, Sec. 36
Obscure Description)
Rec'd. Bk. 1032, Pg. 425
Filed September 13, 1976

Mid Continent Pipe Line Co.
20' Right-of-Way
(S/2, NE/4, SE/4, Sec. 36
Obscure Description)
Rec'd. Bk. 1046, Pg. 364
Filed January 27, 1977

Phillips Petroleum Co.
20' Right-of-Way
(S/2, N/2, SE/4, Sec. 36
Obscure Description)
Rec'd. Bk. 1049, Pg. 890
Filed March 3, 1977

Livingston Family Revocable Trust
Warranty Deed
Bk. 4277, Pg. 133
Filed June 4, 2010

Phillips Petroleum Co.
20' Right-of-Way
(N/2, NE/4, SE/4, Sec. 36
Obscure Description)
Rec'd. Bk. 1054, Pg. 579
Filed April 21, 1977

David L. Perryman &
Gwendolyn Jo Perryman
Trustee's Deed
Rec'd. Bk. 2078, Pg. 284
Filed May 19, 1989

POT Sta. 365+43.98
Set 1/2" capped I.P.
on EW-136.5 1/4 &
2550.40' W. to 1/4 cor.
9163' E. to 1/4 cor.

Public
50' Roadway Easement
Rec'd. Bk. 1353, Pg. 177
Filed April 7, 1983

SCHENK, DON
SCHENK, ELIZABETH
REVOCABLE TRUST
Quit Claim Deed
Rec'd. Bk. 2889, Pg. 507
Filed October 8, 1996

Phillips Petroleum Co.
Right-of-Way
(NE/4, Sec. 36
Obscure Description)
Rec'd. Bk. 1051, Pg. 155
Filed March 16, 1977

Phillips Petroleum Co.
50' Pipeline Right-of-Way
(E/2, NE/4, Sec. 36
Obscure Description)
Rec'd. Bk. 2968, Pg. 275
Filed Jun 7, 1994

Public Service Co. of Okla.
10' Right-of-Way Easement
Rec'd. Bk. 2348, Pg. 57
Filed August 19, 1991

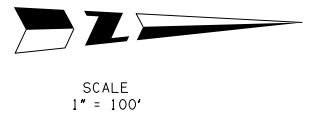
Mid Continent Pipe Line Co.
20' Easement
Rec'd. Bk. 1046, Pg. 851
Filed January 31, 1977

Mid Continent Pipe Line Co.
20' Easement
(N. 10 ac. Lot 8, Sec. 31
Obscure Description)
Rec'd. Bk. 1030, Pg. 734
Filed August 25, 1976

David L. Perryman &
Gwendolyn Jo Perryman
Warranty Deed
Rec'd. Bk. 4259, Pg. 13
Filed March 23, 2010

Phillips Petroleum Co.
33' Right-of-Way
(S/2, NW/4, & S/2, N/2, NW/4,
Sec. 31, Obscure Description)
Rec'd. Bk. 1293, Pg. 890
Filed March 22, 1982

COMMISSIONER'S LAND OFFICE
(Book & Page Not Found)



| | | |
|----------|-----|--|
| PLS | JTB | |
| DRAWN | CBM | |
| CHECKED | JTB | |
| APPROVED | JTB | |
| CREW | BDG | |

| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | | |
|--|--|--|
| SURVEY DATA SHEET | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. 5028 | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

Raw Crude, L.L.C.
25' Pipeline Easement
(NE/4, Sec. 36
Obscure Description)
Rec'd. Bk. 3451, Pg. 386
Filed Jun 22, 2003

UMC Petroleum Corporation
Pipeline Right-of-Way
(No width specified)
(NE/4, Sec. 36
Obscure Description)
Rec'd. Bk. 2857, Pg. 556
Filed May 3, 1996

SCHENK, DON
SCHENK, ELIZABETH
REVOCABLE TRUST
Quit Claim Deed
Rec'd. Bk. 2889, Pg. 507
Filed October 8, 1996

Public Service Co. of Okla.
10' Right-of-Way Easement
Rec'd. Bk. 2348, Pg. 57
Filed August 19, 1991

Mid Continent Pipe Line Co.
50' Right-of-Way
(NE/4, Sec. 36
Obscure Description)
Rec'd. Bk. 1046, Pg. 366
Filed January 27, 1977

Koch Industries Inc.
Right-of-Way
(No width specified)
(NE/4, Sec. 6,
Obscure Description)
Rec'd. Bk. 896, Pg. 23
Filed May 10, 1971

Rural Water, Sewer and
Solid Waste Mgt. Dist. No. 6
20' Right-of-Way Easement
(NE/4, Sec. 36
Obscure Description)
Bk. 1018, Pg. 314
Filed April 1, 1976

Rural Water, Sewer and
Solid Waste Mgt. Dist. No. 6
20' Right-of-Way Easement
Bk. 1323, Pg. 545
Filed 09/28/1982

Rural Water, Sewer and
Solid Waste Mgt. Dist. No. 6
20' Right-of-Way Easement
Bk. 1323, Pg. 539, 542
Filed 09/28/1982

COMMISSIONER'S LAND OFFICE
(Book & Page Not Found)
Rural Water, Sewer and
Solid Waste Mgt. Dist. No. 6
20' Easement
(W/2 Lot 6, Sec. 31
Obscure Description)
Bk. 1018, Pg. 279
Filed April 1, 1976

COMMISSIONER'S LAND
OFFICE (Book & Page Not
Found)

COMMISSIONER'S LAND OFFICE
(Book & Page Not Found)

Public Service Co. of Okla.
5' Right-of-Way Easement
Rec'd. Bk. 1743, Pg. 72
Filed Sept 25, 1986

Gary Don Whitaker &
Donna Fay Whitaker
Quit Claim Deed
Rec'd. Bk. 4329, Pg. 590
Filed December 16, 2010

Phillips Petroleum Co.
33' Right-of-Way
(S/2, NW/4, & S/2, N/2, NW/4,
Sec. 31, Obscure Description)
Rec'd. Bk. 1293, Pg. 890
Filed March 22, 1982

Johnny Paul Franklin &
Holly F. Franklin
Warranty Deed
Rec'd. Bk. 3171, Pg. 165
Filed January 28, 2000

SECTION 36
T-7-N, R-8-W

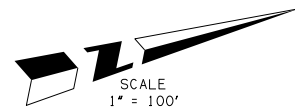
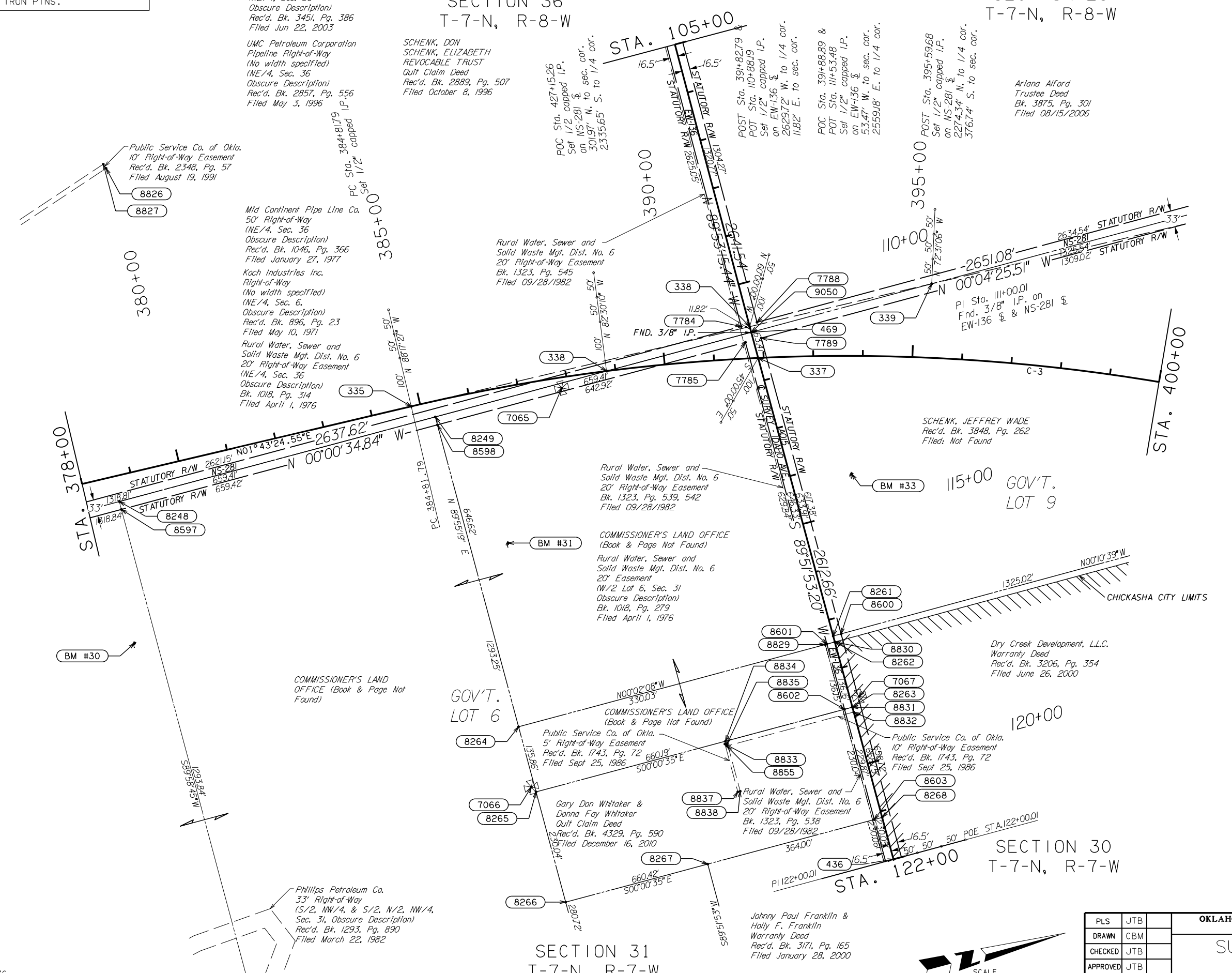
SECTION 25
T-7-N, R-8-W

SECTION 31
T-7-N, R-7-W

SECTION 30
T-7-N, R-7-W

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|--|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS | |
| | OKLA | | | | | |
| DESCRIPTION | | REVISIONS | | DATE | | |

| CURVE #3 | |
|----------|----------------|
| PI STA | 413+90.54 |
| X | 1972208.0949 |
| Y | 624794.3170 |
| Δ | 7434'44.76" RT |
| D | 130'00.00" |
| T | 2908.75' |
| L | 4971.94' |
| R | 3819.72' |
| E | 981.43' |

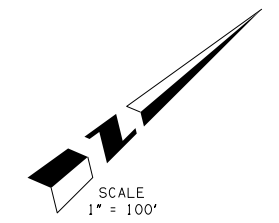
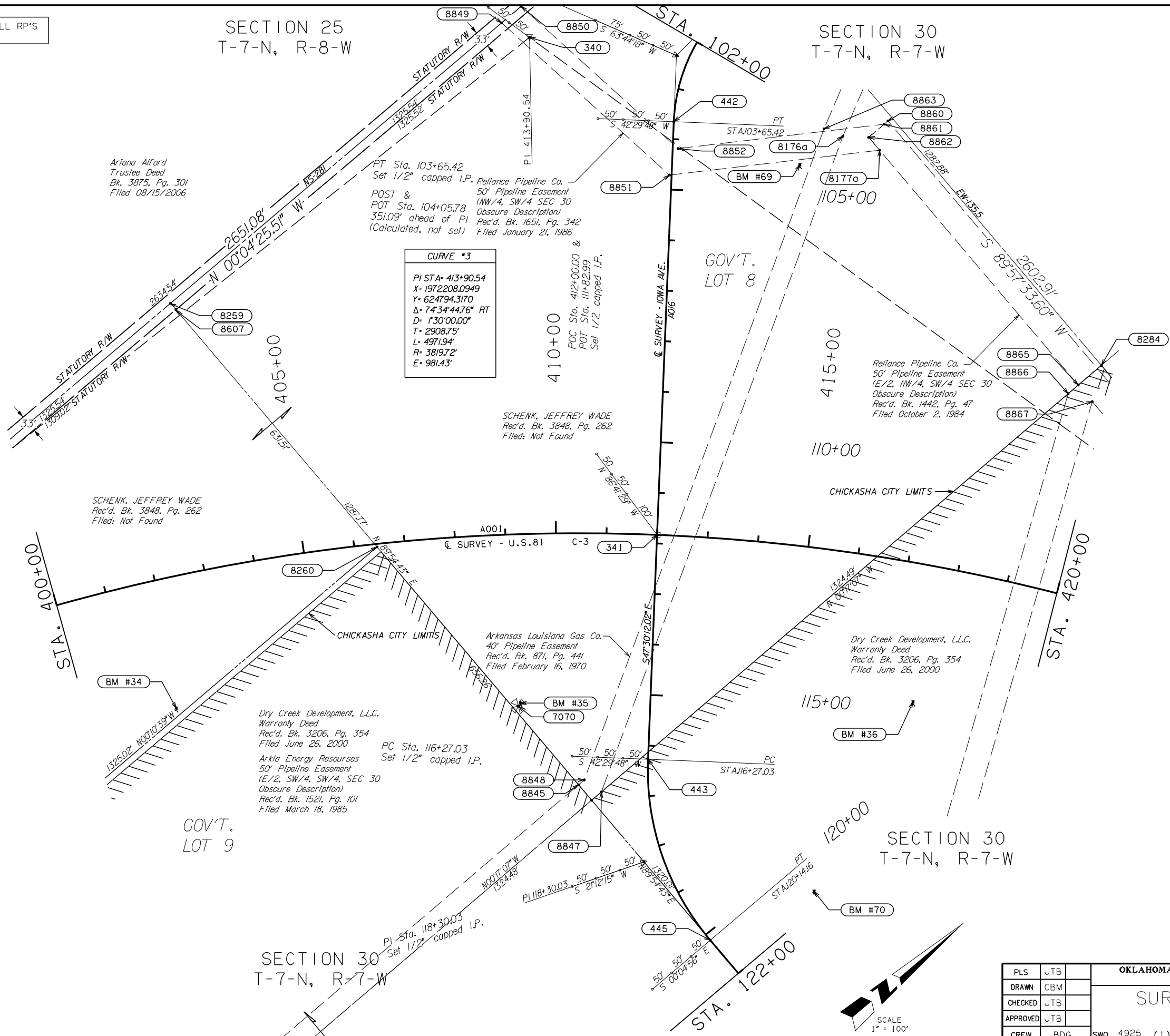


| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | | | |
|--|-----|--|--|
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S029 | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |

SECTION 25 T-7-N, R-8-W SECTION 30 T-7-N, R-7-W

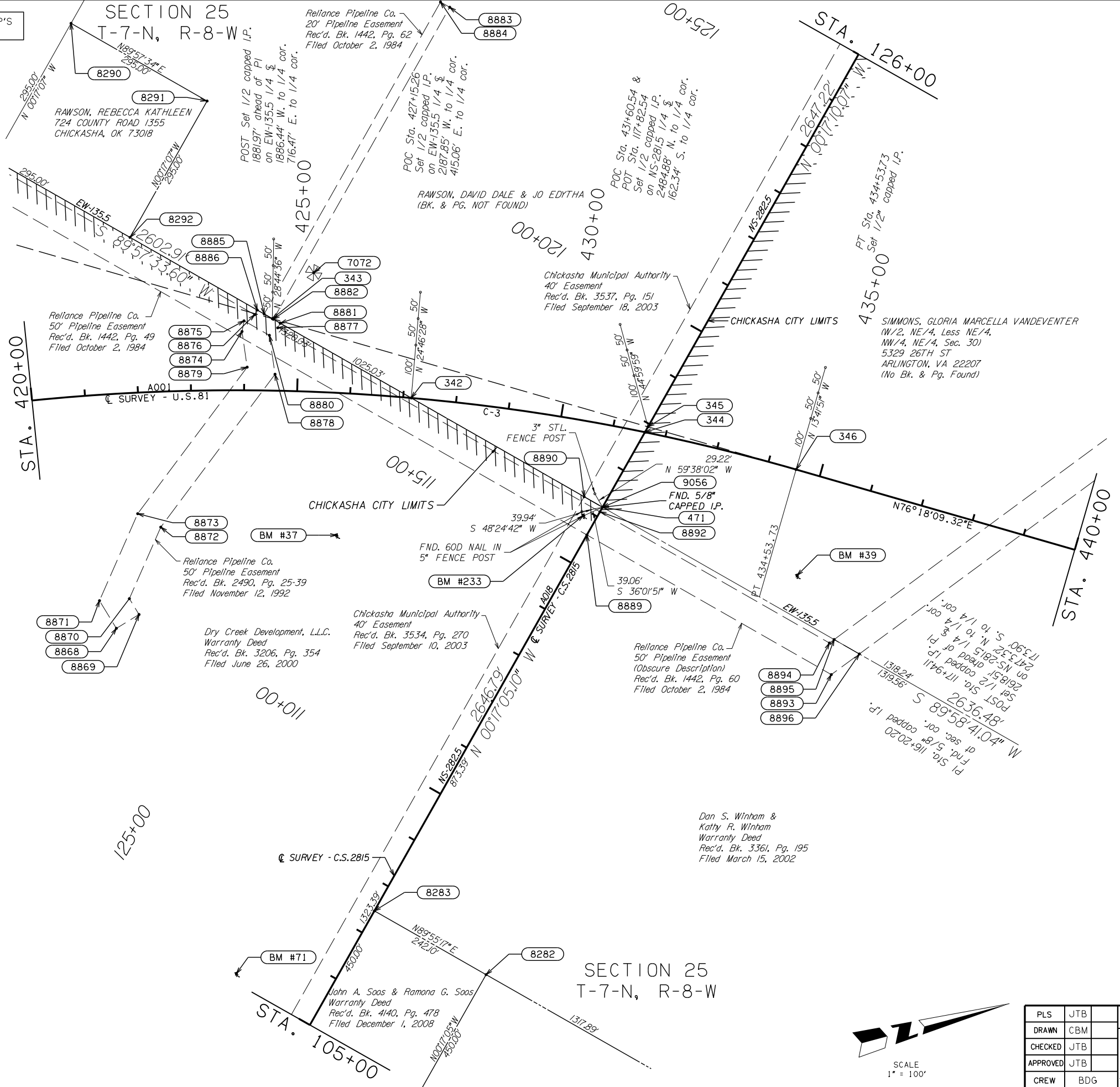


| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | | | |
|---|-----|--|--|
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S030 | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

| CURVE #3 | |
|----------|-----------------|
| PI STA. | 413+90.54 |
| X | 1972208.0949 |
| Y | 624794.3170 |
| Δ | 74°34'44.76" RT |
| D | 1°30'00.00" |
| T | 2908.75' |
| L | 4971.94' |
| R | 3819.72' |
| E | 981.43' |

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |
| | | | | | |



| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | | | | | |
|---|-----|--|--|--|--|
| SURVEY DATA SHEET | | | | | |
| PLS | JTB | | | | |
| DRAWN | CBM | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BDG | | | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S031 | | | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |
| | | | | | |

SECTION 29
T-7-N, R-7-W

GOV'T.
LOT 10

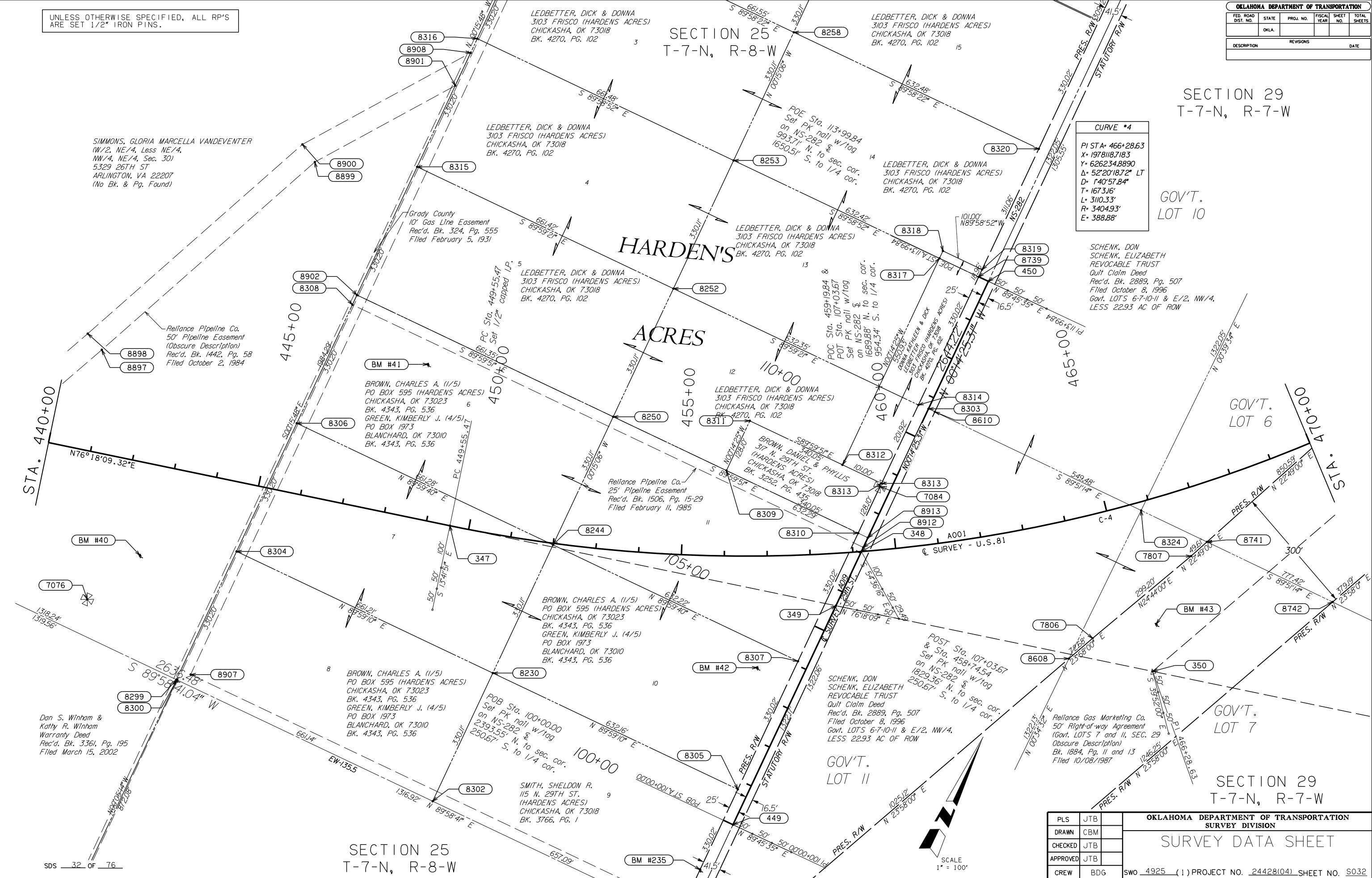
GOV'T.
LOT 6

GOV'T.
LOT 7

SECTION 29
T-7-N, R-7-W

CURVE #4
 PI STA+ 466+28.63
 X+ 1978118.7183
 Y+ 626234.8890
 Δ+ 52°20'18.72" LT
 D+ 140°57.84"
 T+ 1673.16'
 L+ 3110.33'
 R+ 3404.93'
 E+ 388.88'

SCHENK, DON
 SCHENK, ELIZABETH
 REVOCABLE TRUST
 Quit Claim Deed
 Rec'd. Bk. 2889, Pg. 507
 Filed October 8, 1996
 Govt. LOTS 6-7-10-11 & E/2, NW/4,
 LESS 22.93 AC OF ROW



SECTION 25
T-7-N, R-8-W

GOV'T.
LOT II

| | |
|----------|-----|
| PLS | JTB |
| DRAWN | CBM |
| CHECKED | JTB |
| APPROVED | JTB |
| CREW | BDG |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
SURVEY DIVISION

SURVEY DATA SHEET

SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S032

SCALE
1" = 100'

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

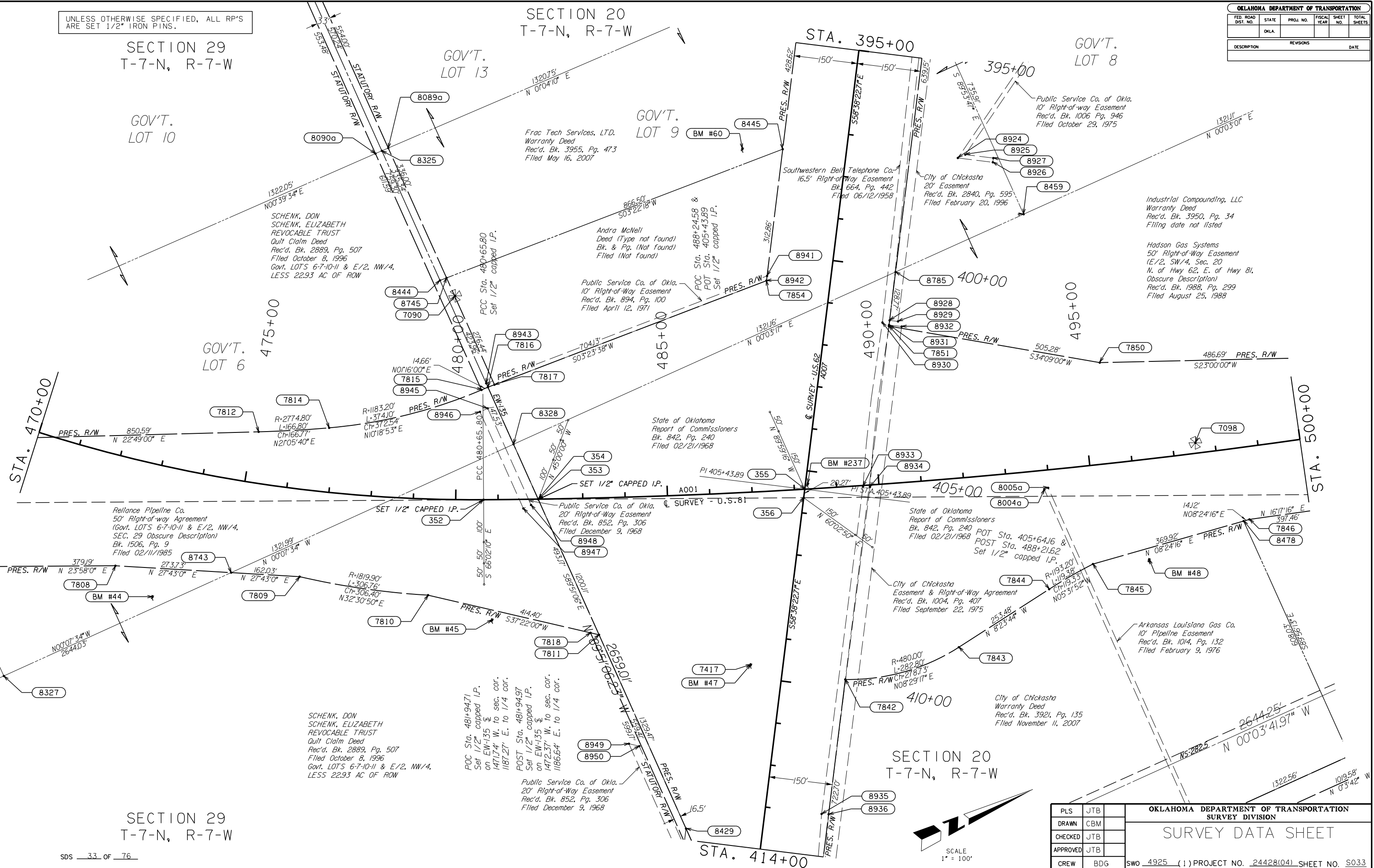
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |
| | | | | | |

SECTION 29
T-7-N, R-7-W

SECTION 20
T-7-N, R-7-W

SECTION 20
T-7-N, R-7-W

SECTION 29
T-7-N, R-7-W

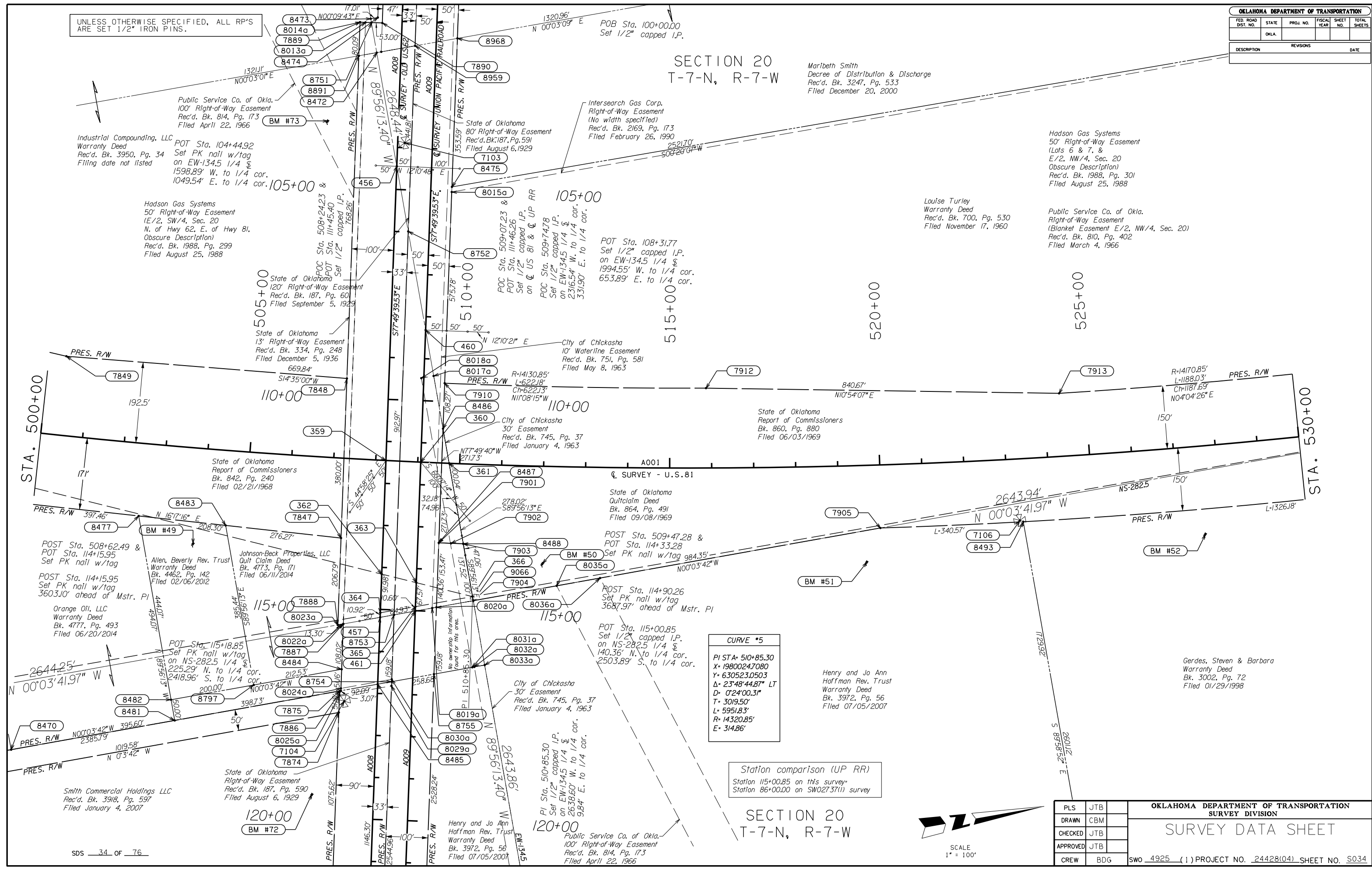


| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-----|--------------|------------------------|----------------|--|
| SURVEY DIVISION | | | | | |
| SURVEY DATA SHEET | | | | | |
| PLS | JTB | | | | |
| DRAWN | CBM | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BDG | SWO 4925 (1) | PROJECT NO. 244281(04) | SHEET NO. S033 | |

SECTION 20
T-7-N, R-7-W

Maribeth Smith
Decree of Distribution & Discharge
Rec'd. Bk. 3247, Pg. 533
Filed December 20, 2000

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.



Hadson Gas Systems
50' Right-of-Way Easement
(Lots 6 & 7, & E/2, NW/4, Sec. 20
Obscure Description)
Rec'd. Bk. 1988, Pg. 301
Filed August 25, 1988

Louise Turley
Warranty Deed
Rec'd. Bk. 700, Pg. 530
Filed November 17, 1960

Public Service Co. of Okla.
Right-of-Way Easement
(Blanket Easement E/2, NW/4, Sec. 20)
Rec'd. Bk. 810, Pg. 402
Filed March 4, 1966

Intersearch Gas Corp.
Right-of-Way Easement
(No width specified)
Rec'd. Bk. 2169, Pg. 173
Filed February 26, 1990

State of Oklahoma
80' Right-of-Way Easement
Rec'd. Bk. 187, Pg. 591
Filed August 6, 1929

Public Service Co. of Okla.
100' Right-of-Way Easement
Rec'd. Bk. 814, Pg. 173
Filed April 22, 1966

Industrial Compounding, LLC
Warranty Deed
Rec'd. Bk. 3950, Pg. 34
Filing date not listed

Hadson Gas Systems
50' Right-of-Way Easement
(E/2, SW/4, Sec. 20
N. of Hwy 62, E. of Hwy 81,
Obscure Description)
Rec'd. Bk. 1988, Pg. 299
Filed August 25, 1988

State of Oklahoma
120' Right-of-Way Easement
Rec'd. Bk. 187, Pg. 600
Filed September 5, 1929

State of Oklahoma
13' Right-of-Way Easement
Rec'd. Bk. 334, Pg. 248
Filed December 5, 1936

City of Chickasha
10' Waterline Easement
Rec'd. Bk. 751, Pg. 581
Filed May 8, 1963

State of Oklahoma
Report of Commissioners
Bk. 860, Pg. 880
Filed 06/03/1969

City of Chickasha
30' Easement
Rec'd. Bk. 745, Pg. 37
Filed January 4, 1963

State of Oklahoma
Report of Commissioners
Bk. 842, Pg. 240
Filed 02/21/1968

State of Oklahoma
Quitclaim Deed
Bk. 864, Pg. 491
Filed 09/08/1969

POST Sta. 509+47.28 &
POT Sta. 114+33.28
Set PK nail w/tag 984.35'
ahead of Mstr. PI

POST Sta. 114+90.26
Set PK nail w/tag 3687.97'
ahead of Mstr. PI

POT Sta. 115+00.85
Set 1/2" capped I.P.
on NS-282.5 1/4 §
140.36' N. to 1/4 cor.
2503.89' S. to 1/4 cor.

City of Chickasha
30' Easement
Rec'd. Bk. 745, Pg. 37
Filed January 4, 1963

Henry and Jo Ann
Hoffman Rev. Trust
Warranty Deed
Bk. 3972, Pg. 56
Filed 07/05/2007

Gerdes, Steven & Barbara
Warranty Deed
Bk. 3002, Pg. 72
Filed 01/29/1998

Smith Commercial Holdings LLC
Rec'd. Bk. 3918, Pg. 597
Filed January 4, 2007

Henry and Jo Ann
Hoffman Rev. Trust
Warranty Deed
Bk. 3972, Pg. 56
Filed 07/05/2007

PI Sta. 510+85.30
Set 1/2" capped I.P.
on EW-134.5 1/4 §
2638.60' W. to 1/4 cor.
9.84' E. to 1/4 cor.

Public Service Co. of Okla.
100' Right-of-Way Easement
Rec'd. Bk. 814, Pg. 173
Filed April 22, 1966



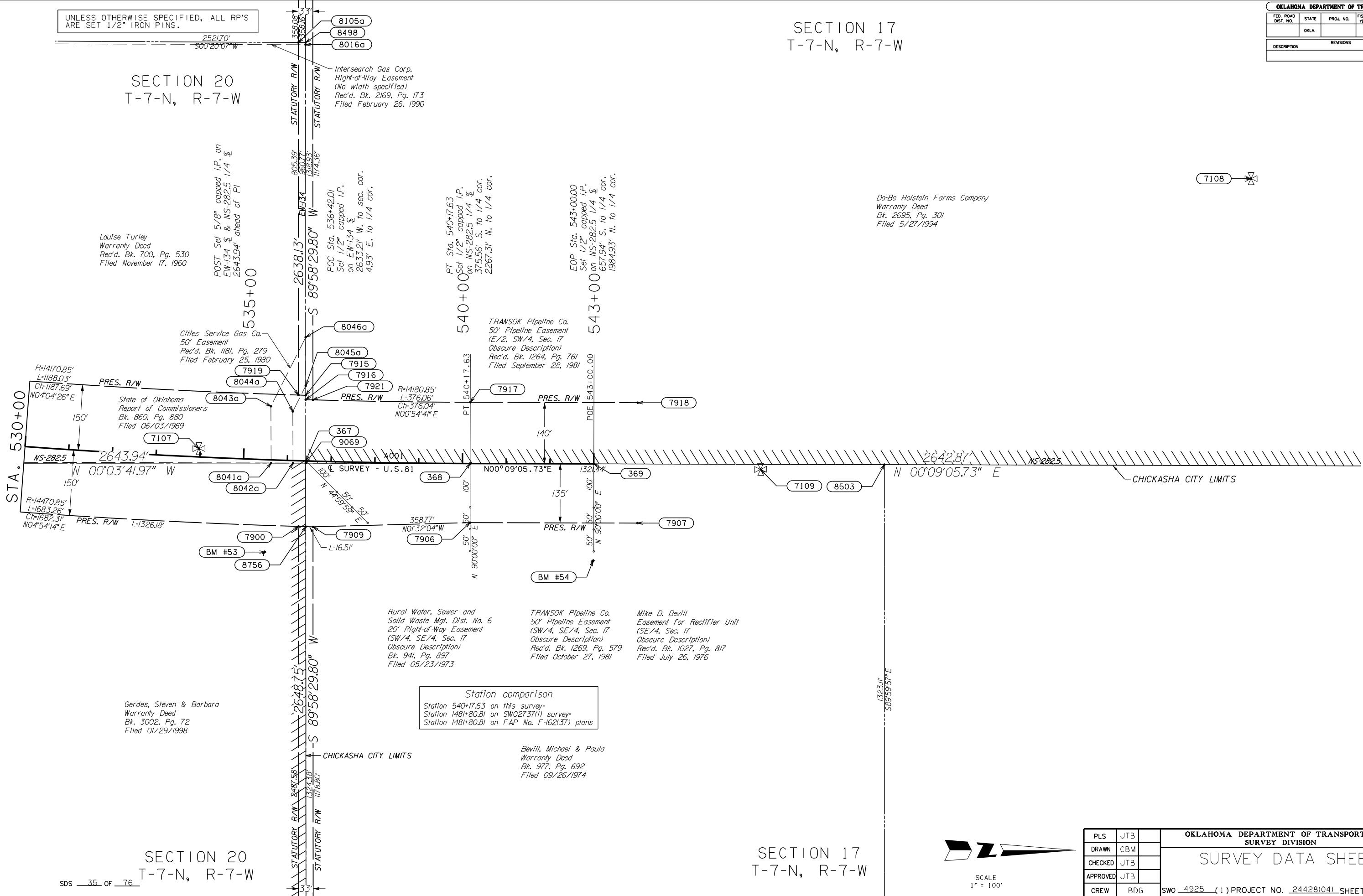
SCALE
1" = 100'

| | | |
|----------|-----|--|
| PLS | JTB | OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION |
| DRAWN | CBM | |
| CHECKED | JTB | |
| APPROVED | JTB | |
| CREW | BDG | |
| | | SURVEY DATA SHEET |
| | | SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S034 |

SECTION 17
T-7-N, R-7-W

SECTION 20
T-7-N, R-7-W

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.



Louise Turley
Warranty Deed
Rec'd. Bk. 700, Pg. 530
Filed November 17, 1960

POST. Set 5/8" capped I.P. on
EW-134 & NS-282.5 1/4 &
2643.94' ahead of PI

FOC Sta. 536+42.01
Set 1/2" capped I.P.
on EW-134 &
2633.21' W. to sec. cor.
4.93' E. to 1/4 cor.

PT Sta. 540+17.63
Set 1/2" capped I.P.
on NS-282.5 1/4 &
375.56' S. to 1/4 cor.
2267.31' N. to 1/4 cor.

EOP Sta. 543+00.00
Set 1/2" capped I.P.
on NS-282.5 1/4 &
657.94' S. to 1/4 cor.
1984.93' N. to 1/4 cor.

Do-Be Holstein Farms Company
Warranty Deed
Bk. 2695, Pg. 301
Filed 5/27/1994

Cites Service Gas Co.
50' Easement
Rec'd. Bk. 1181, Pg. 279
Filed February 25, 1980

TRANSOK Pipeline Co.
50' Pipeline Easement
(E/2, SW/4, Sec. 17
Obscure Description)
Rec'd. Bk. 1264, Pg. 761
Filed September 28, 1981

State of Oklahoma
Report of Commissioners
Bk. 860, Pg. 880
Filed 06/03/1969

Rural Water, Sewer and
Solid Waste Mgt. Dist. No. 6
20' Right-of-Way Easement
(SW/4, SE/4, Sec. 17
Obscure Description)
Bk. 941, Pg. 897
Filed 05/23/1973

TRANSOK Pipeline Co.
50' Pipeline Easement
(SW/4, SE/4, Sec. 17
Obscure Description)
Rec'd. Bk. 1269, Pg. 579
Filed October 27, 1981

Mike D. Beville
Easement for Rectifier Unit
(SE/4, Sec. 17
Obscure Description)
Rec'd. Bk. 1027, Pg. 817
Filed July 26, 1976

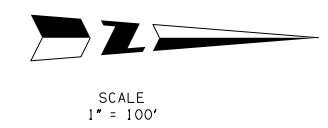
Gerdas, Steven & Barbara
Warranty Deed
Bk. 3002, Pg. 72
Filed 01/29/1998

Station comparison
Station 540+17.63 on this survey
Station 1481+80.81 on SW02737(1) survey
Station 1481+80.81 on FAP No. F-162(37) plans

Beville, Michael & Paula
Warranty Deed
Bk. 977, Pg. 692
Filed 09/26/1974

SECTION 20
T-7-N, R-7-W

SECTION 17
T-7-N, R-7-W



| | | |
|----------|-----|--|
| PLS | JTB | OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION |
| DRAWN | CBM | |
| CHECKED | JTB | |
| APPROVED | JTB | |
| CREW | BDG | |
| | | SURVEY DATA SHEET |
| | | SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S035 |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |

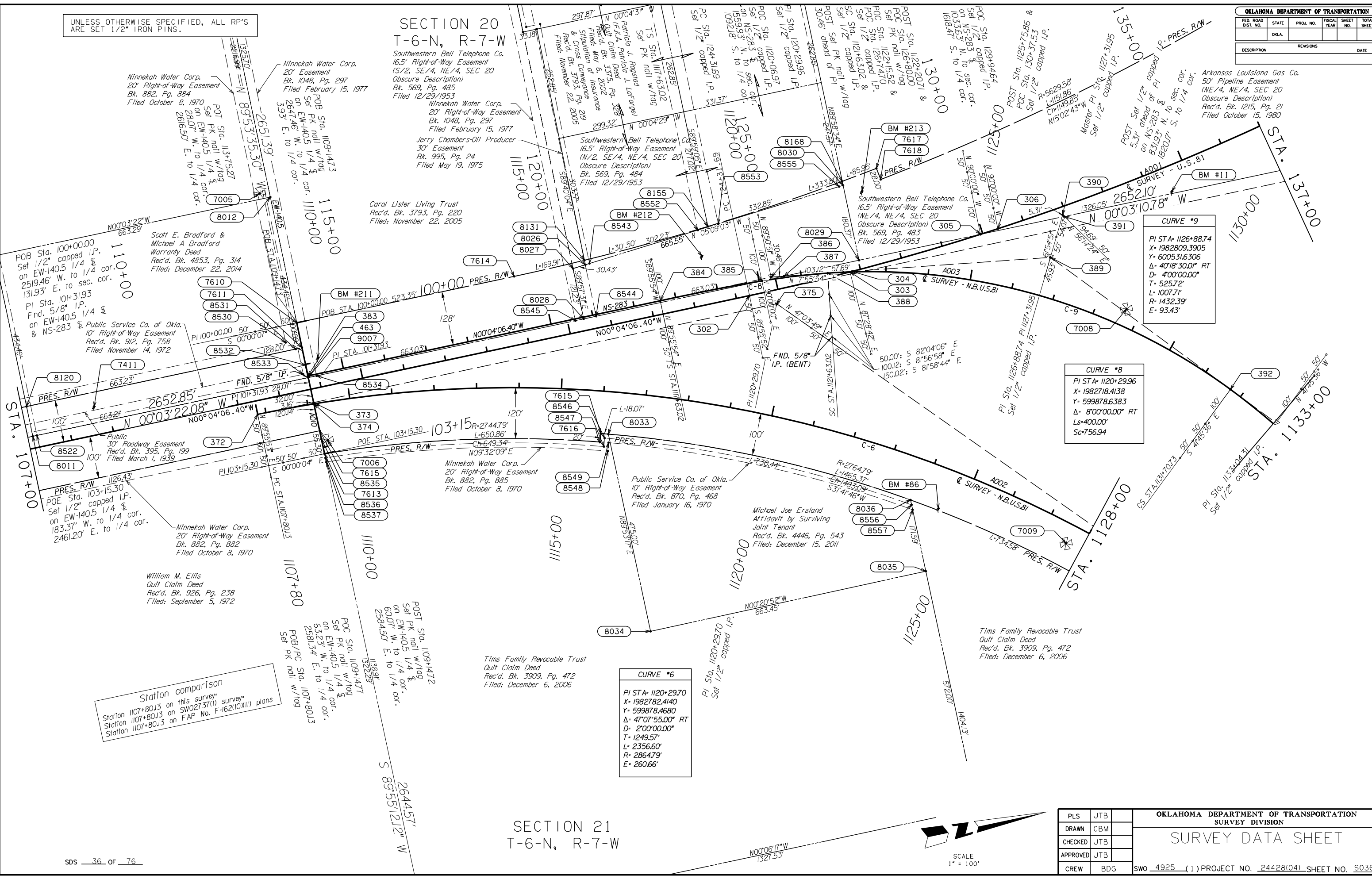
**SECTION 20
T-6-N, R-7-W**

Southwestern Bell Telephone Co.
16.5' Right-of-Way Easement
(S/2, SE/4, NE/4, SEC 20
Obscure Description)
Bk. 569, Pg. 485
Filed 12/29/1953

Ninnekah Water Corp.
20' Right-of-Way Easement
Bk. 1048, Pg. 297
Filed February 15, 1977

Jerry Chambers-Oil Producer
30' Easement
Bk. 995, Pg. 24
Filed May 19, 1975

Carol Lister Living Trust
Rec'd. Bk. 3793, Pg. 220
Filed: November 22, 2005



Station comparison
 Station 1107+80J3 on this survey
 Station 1107+80J3 on SW02737(1) survey
 Station 1107+80J3 on FAP No. F-16210(XII) plans

**SECTION 21
T-6-N, R-7-W**

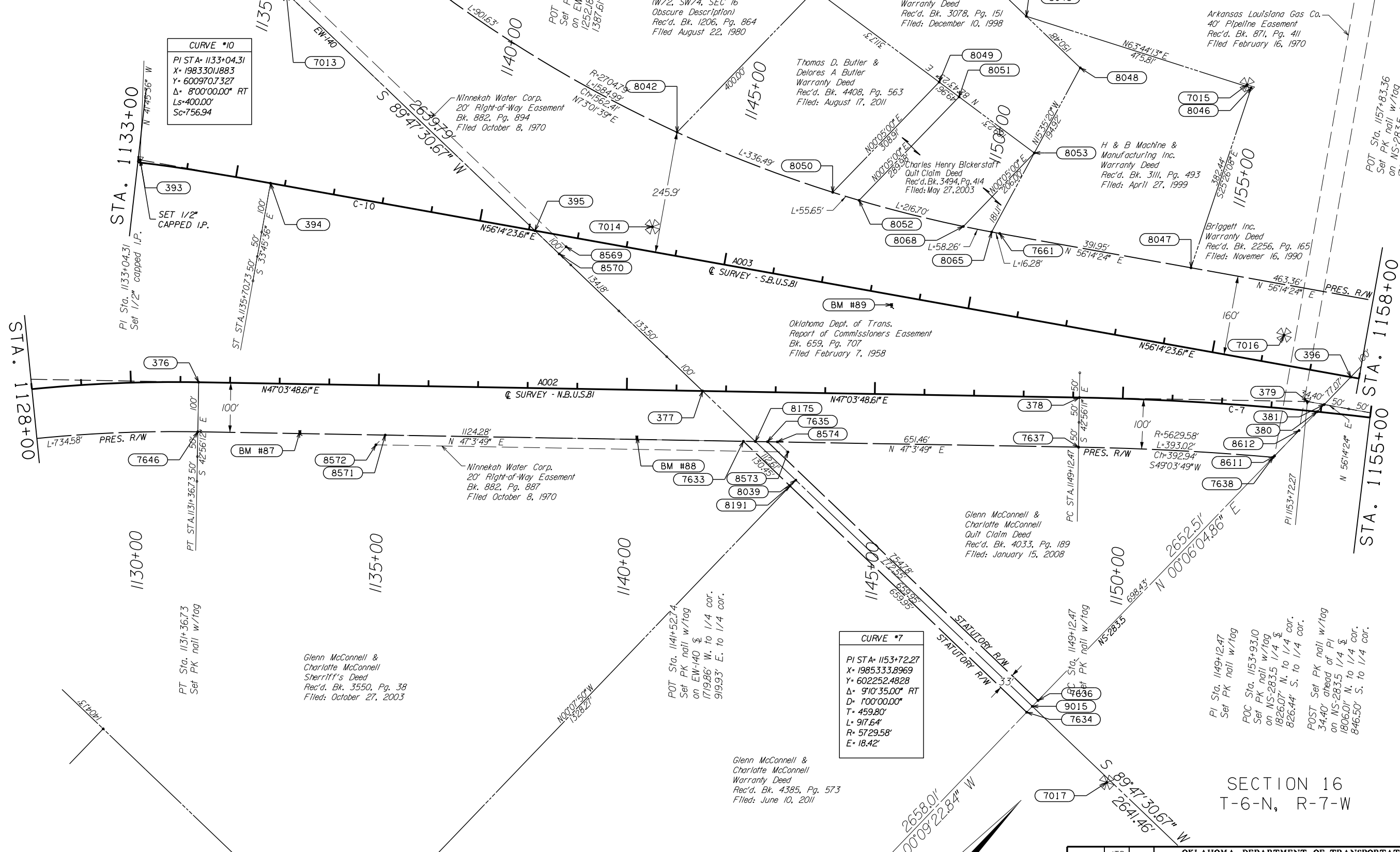
| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | | | |
|--|-----|--|--|
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S036 | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

SECTION 21
T-6-N, R-7-W

SECTION 16
T-6-N, R-7-W

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |



SECTION 21
T-6-N, R-7-W

SECTION 16
T-6-N, R-7-W

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-----|--------------|-----------------------|----------------|--|
| SURVEY DIVISION | | | | | |
| SURVEY DATA SHEET | | | | | |
| PLS | JTB | | | | |
| DRAWN | CBM | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BDG | SWO_4925_(1) | PROJECT NO. 24428(04) | SHEET NO. S037 | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

SECTION 16
T-6-N, R-7-W

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | | REVISIONS | | DATE |
| | | | | | |

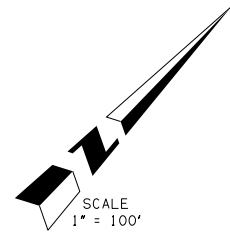
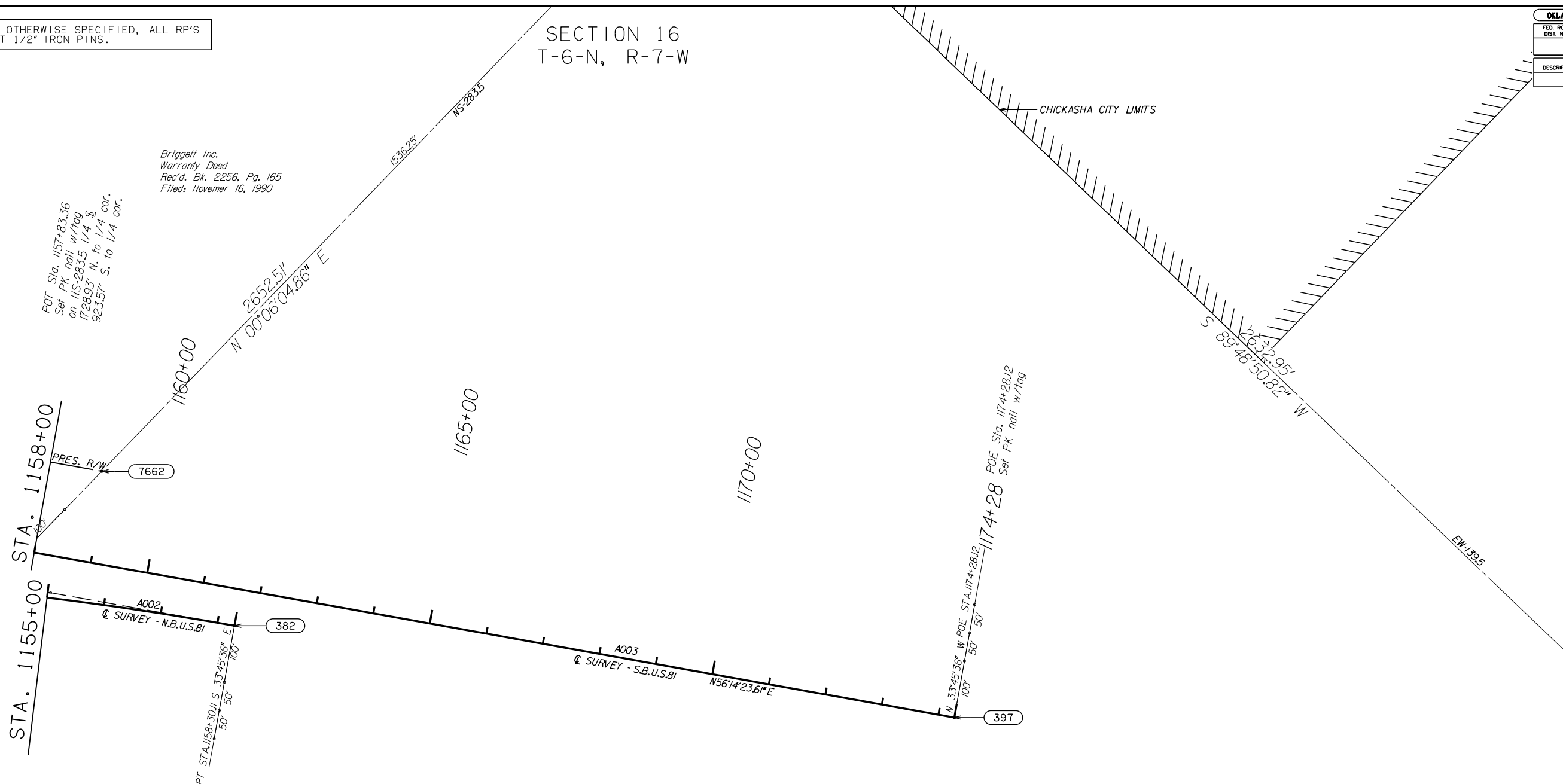
Briggett Inc.
Warranty Deed
Rec'd. Bk. 2256, Pg. 165
Filed: November 16, 1990

POT Sta. 1157+83.36
Set PK nail w/tag
on NS-283.5 1/4 §
1728.93' N. to 1/4 cor.
923.57' S. to 1/4 cor.

POST Set PK nail w/tag
34.40' ahead of PI
on NS-283.5 1/4 §
1806.01' N. to 1/4 cor.
846.50' S. to 1/4 cor.

POE/PT Sta. 1158+30.11
Set PK nail w/tag

CHICKASHA CITY LIMITS



| | |
|----------|-----|
| PLS | JTB |
| DRAWN | CBM |
| CHECKED | JTB |
| APPROVED | JTB |
| CREW | BDG |

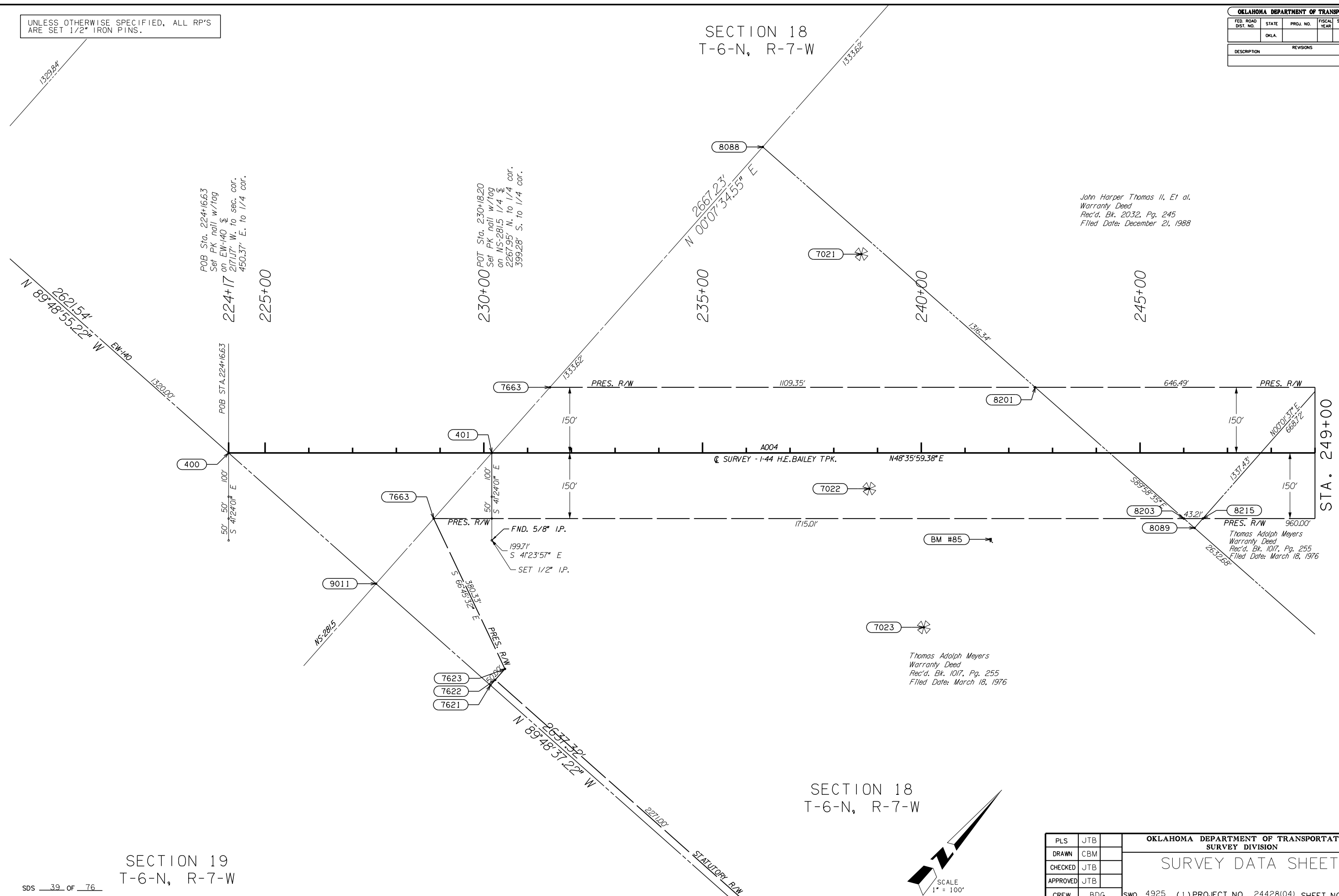
| | |
|--|--------------------------------------|
| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | |
| SURVEY DATA SHEET | |
| SWO 4925 (1) | PROJECT NO. 24428(04) SHEET NO. S038 |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | | REVISIONS | | DATE |
| | | | | | |

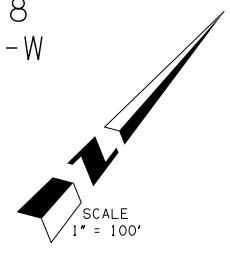
SECTION 18
T-6-N, R-7-W

John Harper Thomas II, Et al.
Warranty Deed
Rec'd. Bk. 2032, Pg. 245
Filed Date: December 21, 1988



SECTION 19
T-6-N, R-7-W

SDS 39 OF 76



| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---|-----|--|--|--|--|
| SURVEY DIVISION | | | | | |
| SURVEY DATA SHEET | | | | | |
| PLS | JTB | | | | |
| DRAWN | CBM | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BDG | | | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S039 | | | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

SECTION 18
T-6-N, R-7-W

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | DATE | | |
| | | | | | |

John Harper Thomas II, Et al.
Warranty Deed
Rec'd. Bk. 2032, Pg. 245
Filed Date: December 21, 1988

John Harper Thomas II, Et al.
Warranty Deed
Rec'd. Bk. 2032, Pg. 245
Filed Date: December 21, 1988

Reliance Pipeline Co.
25' Pipeline Easement
Rec'd. Bk. 1442, Pg. 120
Filed October 2, 1984

John Harper Thomas II, Et al.
Warranty Deed
Rec'd. Bk. 2506, Pg. 268
Filed Date: December 31, 1992

John H. Thomas II, Et al.
Warranty Deed
Rec'd. Bk. 2032, Pg. 245
Filed Date: December 21, 1988

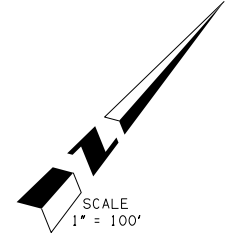
Thomas Adolph Meyers
Warranty Deed
Rec'd. Bk. 1017, Pg. 255
Filed Date: March 18, 1976

Mike & Darlene Crawford
Warranty Deed
Rec'd. Bk. 2906, Pg. 196
Filed Date: December 13, 1996

Pike, Rhonda Revocable Trust
Quit Claim Deed
Bk. 4789, Pg. 480
Filed 07/23/2014

Station comparison
PI Station 282+00.00 on this survey
PI Station 282+00.00 on SW02737(1) survey
PI Station 282+00.00 on Sec. 1-B, No. 206 plans

SECTION 17
T-6-N, R-7-W



| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | |
|---|-----|--|--|
| SURVEY DIVISION | | | |
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S040 | | | |

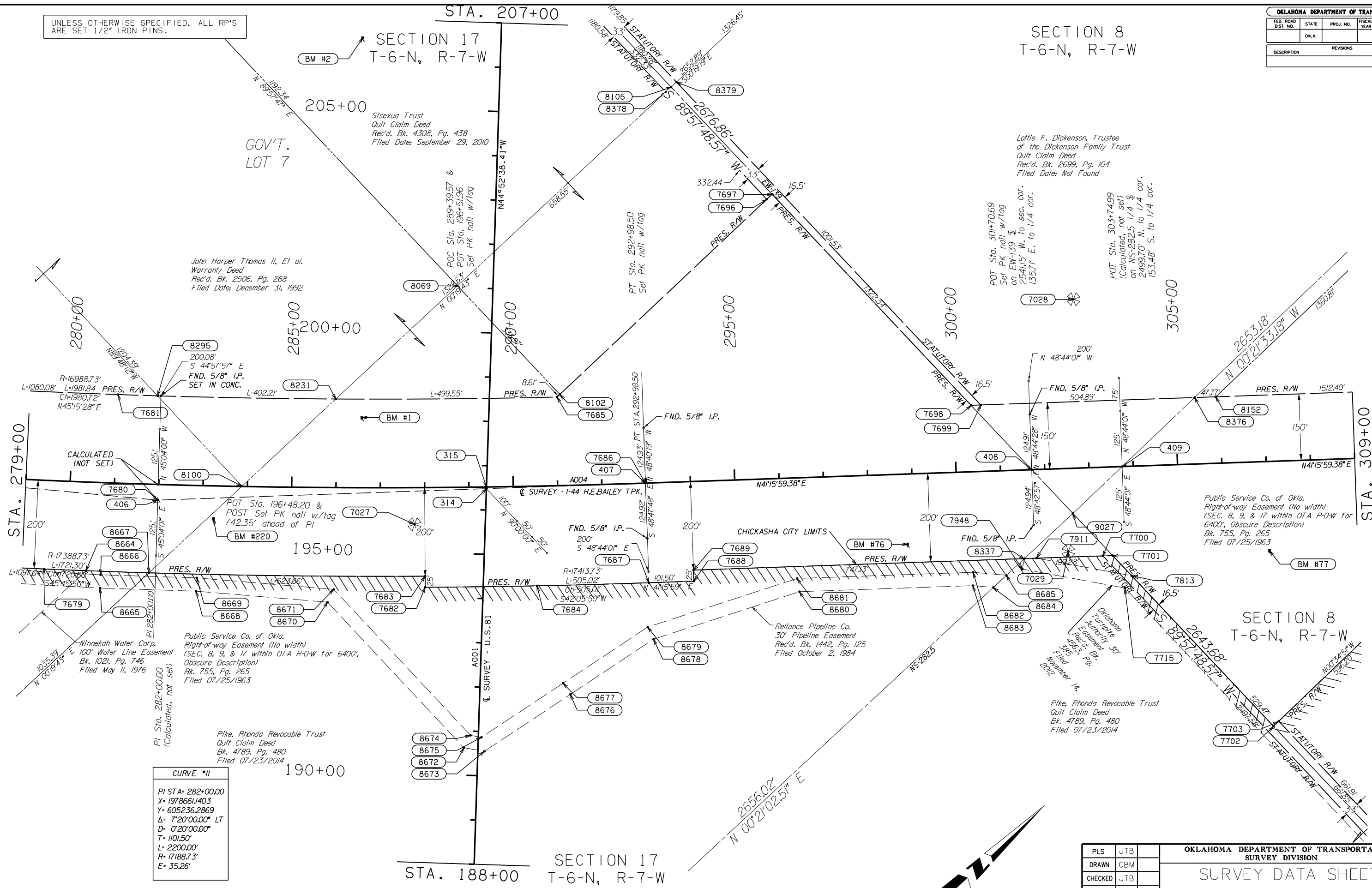
UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |

SECTION 8
T-6-N, R-7-W

STA. 207+00

SECTION 17
T-6-N, R-7-W



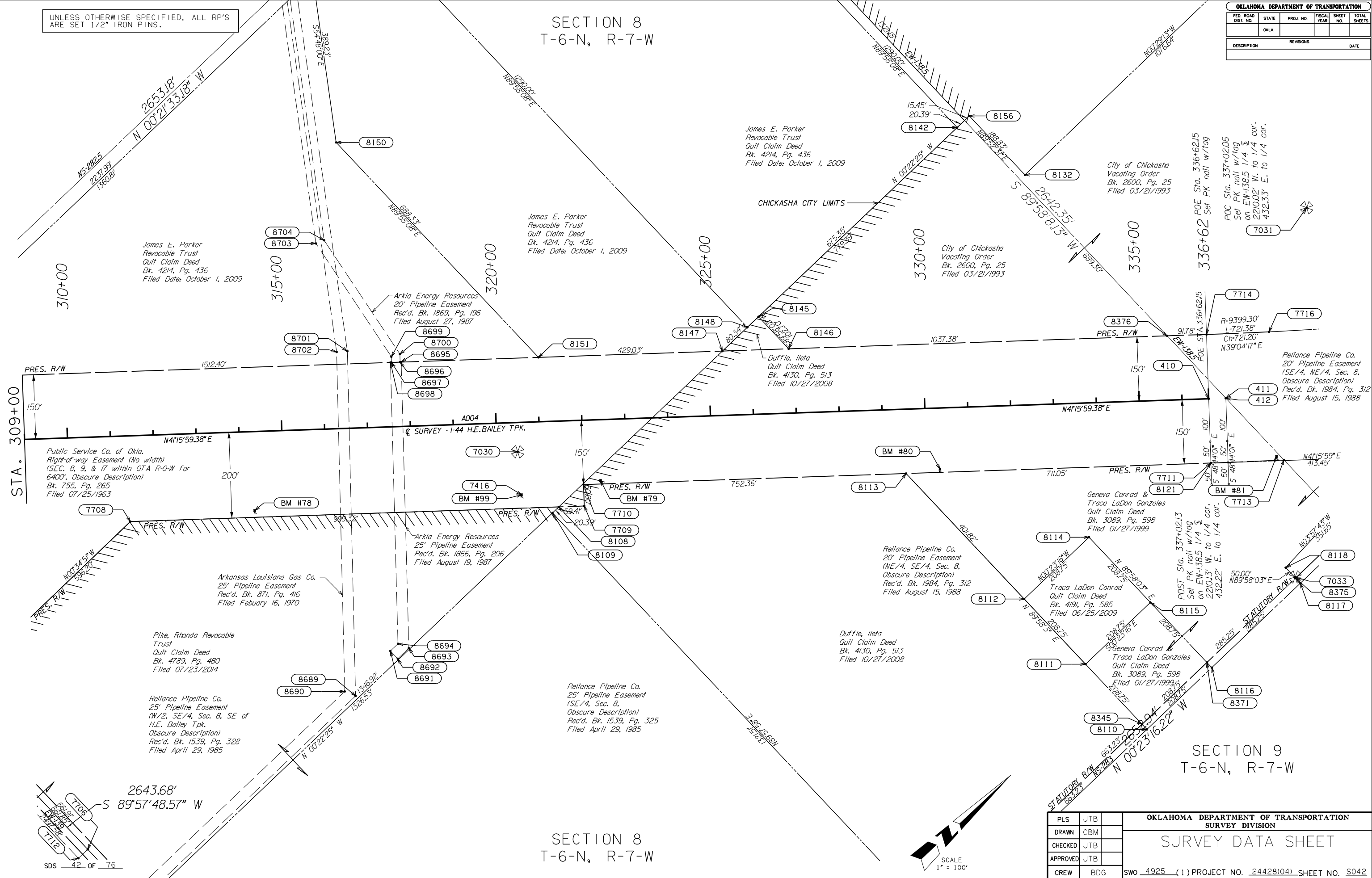
| CURVE #11 |
|-------------------|
| PI STA= 282+00.00 |
| X= 1978661.403 |
| Y= 605236.2869 |
| Δ= 7°20'00.00" LT |
| D= 0°20'00.00" |
| T= 1101.50' |
| L= 2200.00' |
| R= 17188.73' |
| E= 35.26' |

| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | | | |
|--|-----|--------------|--------------------------------------|
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | SWO 4925 (1) | PROJECT NO. 24428(04) SHEET NO. S041 |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

SECTION 8
T-6-N, R-7-W

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |

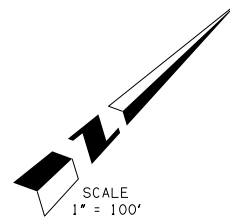


SECTION 8
T-6-N, R-7-W

SECTION 9
T-6-N, R-7-W

2643.68'
S 89°57'48.57" W

SDS 42 OF 76



| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-----|--------------|-----------------------|----------------|--|
| SURVEY DIVISION | | | | | |
| SURVEY DATA SHEET | | | | | |
| PLS | JTB | | | | |
| DRAWN | CBM | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BDG | SWO_4925_(1) | PROJECT NO. 24428(04) | SHEET NO. S042 | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

Station comparison
Station 162+81.0 on this survey
Station 162+81.0 on SW02737(1) survey
Station 162+81.0 on FAS No. 219(1) plans

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | | | | DATE |
| | | | | | |

SECTION 7
T-6-N, R-7-W

CURVE #14
PI STA 168+44.01
X = 1974634.2886
Y = 608636.8678
Δ = 22°13'59.66" RT
D = 2'00"00.00"
T = 562.91'
L = 1111.66'
R = 2864.79'
E = 54.78'

CURVE #15
PI STA 182+67.67
X = 1975369.5550
Y = 609872.7050
Δ = 13°11'00.00" LT
D = 2'00"00.00"
T = 331.05'
L = 659.17'
R = 2864.79'
E = 19.06'

CURVE #13
PI STA 119+79.31
X = 1975451.8275
Y = 609835.6453
Δ = 13°11'00.00" LT
D = 1'59"59.64"
T = 331.06'
L = 659.20'
R = 2864.93'
E = 19.06'

CURVE #12
PI STA 105+62.94
X = 1974720.6841
Y = 608606.0692
Δ = 22°14'00.00" RT
D = 1'59"59.64"
T = 562.94'
L = 1111.72'
R = 2864.93'
E = 54.78'

Bertha B. Thomas
Warranty Deed
Rec'd. Bk. 2831, Pg. 482
Filed January 4, 1996

Chester Stanton Thomas, Etal
Warranty Deed
Rec'd. Bk. 2506, Pg. 270
Filed December 31, 1992

John H. Thomas II, Etal
Warranty Deed
Rec'd. Bk. 2266, Pg. 184
Filed December 26, 1990

Cowboy Country Church Inc.
Warranty Deed
Rec'd. Bk. 4275, Pg. 545
Filed May 27, 2010

Reliance Pipeline Co.
25' Pipeline Easement
Rec'd. Bk. 1534, Pg. 219
Filed April 17, 1985

Norge Water and Sewer Co.
20' Easement
Bk. 1020, Pg. 429
Filed April 23, 1976

Chester Stanton Thomas, Etal
Warranty Deed
Rec'd. Bk. 2506, Pg. 270
Filed December 31, 1992

Chester Stanton Thomas, Etal
Warranty Deed
Rec'd. Bk. 2506, Pg. 270
Filed December 31, 1992

Public Service Co. of Okla.
Right-of-Way Easement
(No width specified)
(SW/4, NE/4, Sec. 7
Obscure Description)
Bk. 818, Pg. 691
Filed August 8, 1966

Southwestern Light & Power Co.
Right-of-Way (No width specified)
Bk. 259, Pg. 76, 86, 90
Filed March 15, 1927

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | |
|---|-----|--|--|--|
| SURVEY DIVISION | | | | |
| SURVEY DATA SHEET | | | | |
| PLS | JTB | | | |
| DRAWN | CBM | | | |
| CHECKED | JTB | | | |
| APPROVED | JTB | | | |
| CREW | BDG | | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S043 | | | | |

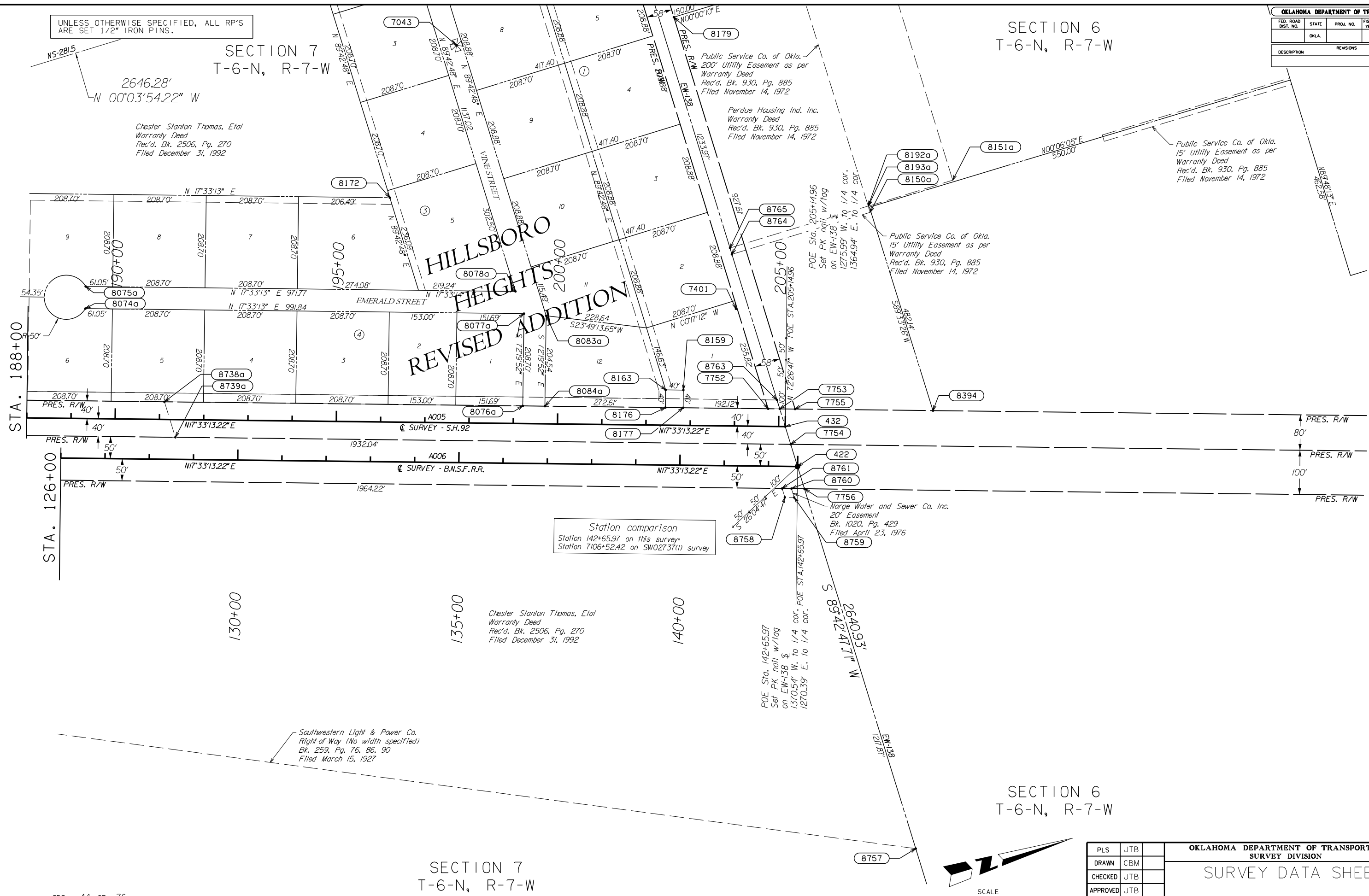
SECTION 7
T-6-N, R-7-W

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

SECTION 7
T-6-N, R-7-W

SECTION 6
T-6-N, R-7-W

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |



Station comparison
Station 142+65.97 on this survey
Station 7106+52.42 on SW02737(1) survey

SECTION 7
T-6-N, R-7-W

SECTION 6
T-6-N, R-7-W

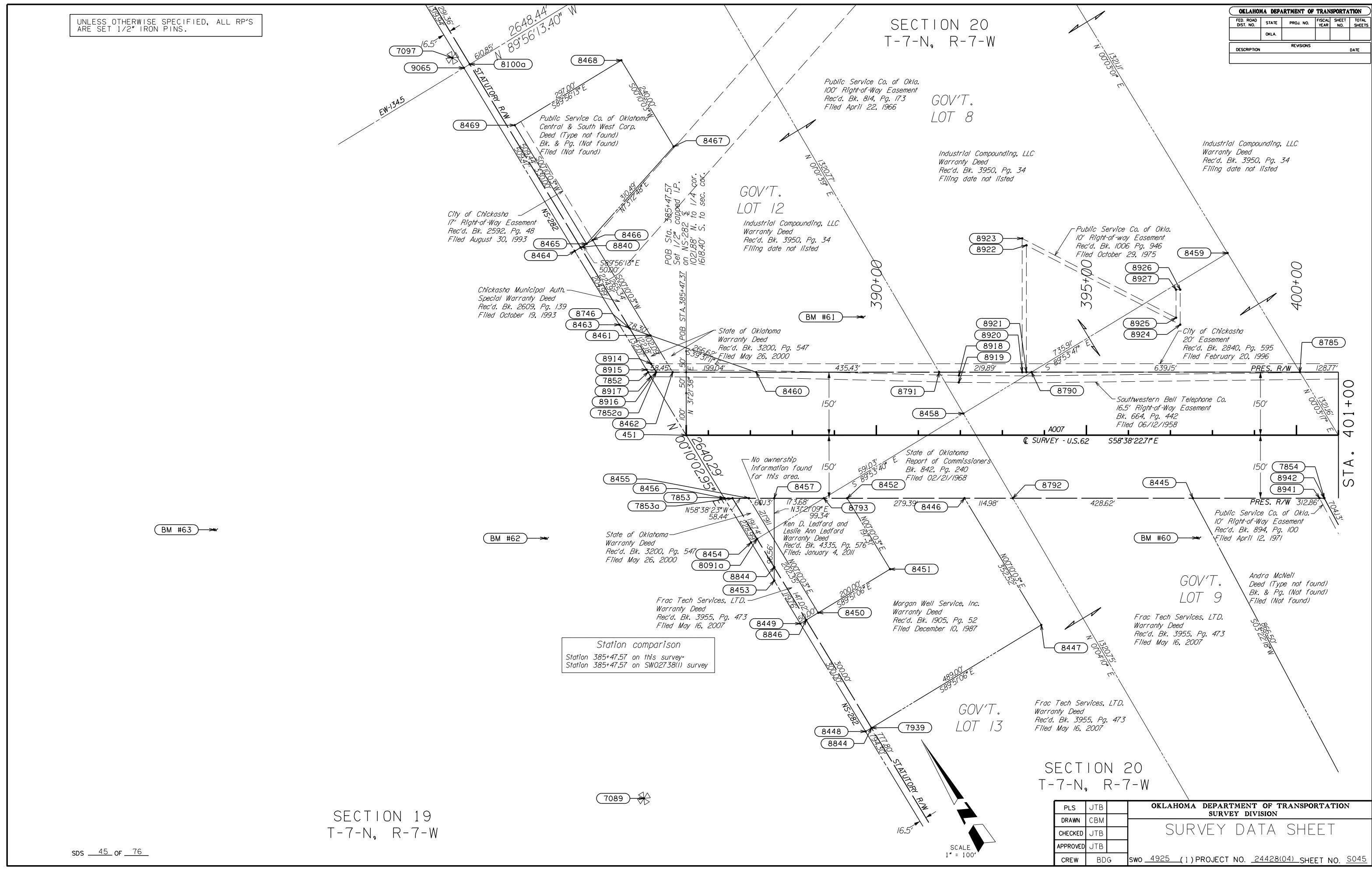


SCALE
1" = 100'

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---|-----|--|--|--|--|
| SURVEY DIVISION | | | | | |
| SURVEY DATA SHEET | | | | | |
| PLS | JTB | | | | |
| DRAWN | CBM | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BDG | | | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S044 | | | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |



Station comparison
 Station 385+47.57 on this survey
 Station 385+47.57 on SW02738(1) survey

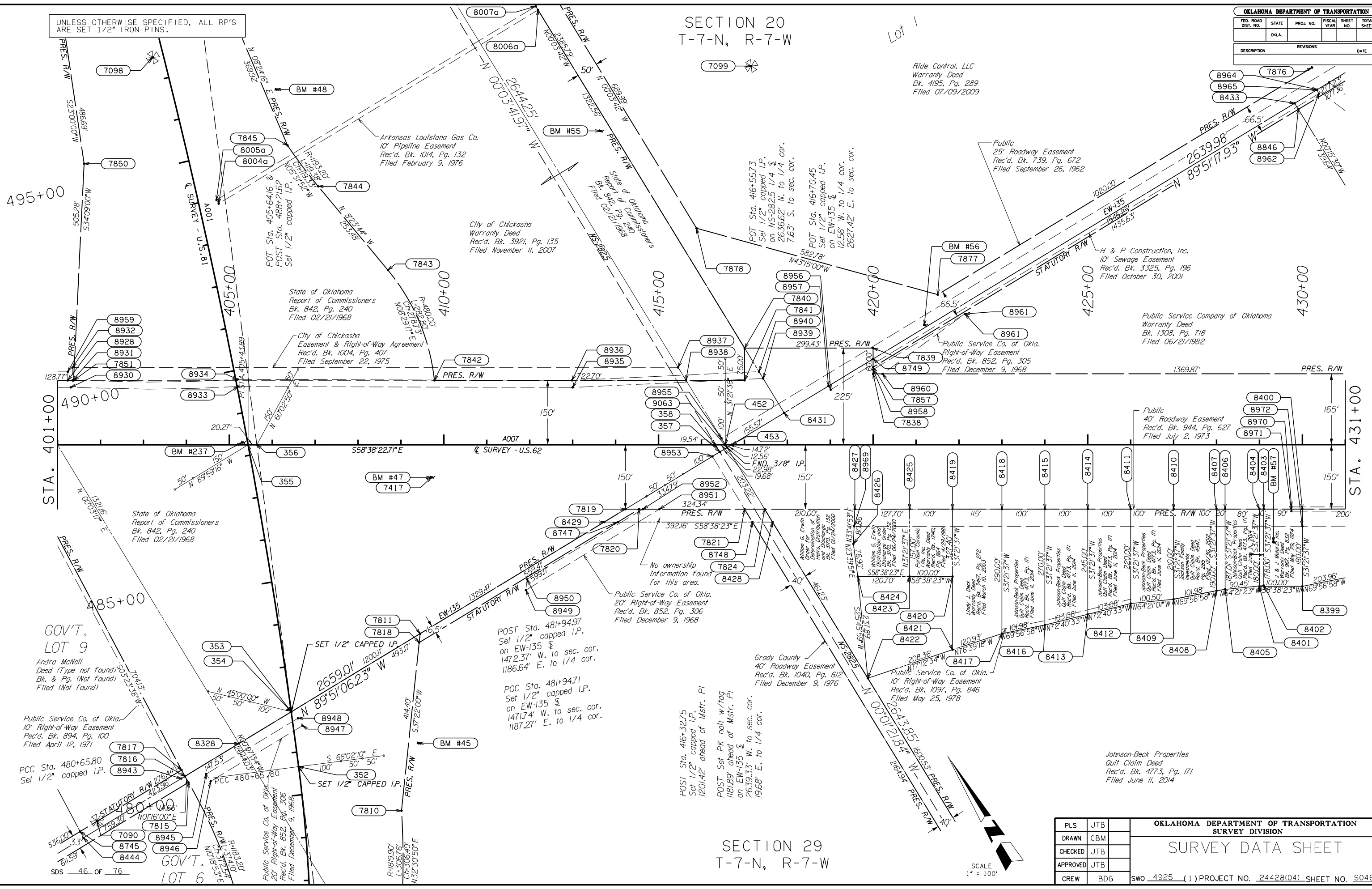
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | |
|---|-----|--|--|
| SURVEY DIVISION | | | |
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S045 | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

SECTION 20
T-7-N, R-7-W

Lot 1

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |



GOV'T. LOT 9
Andra McNeil
Deed (Type not found)
Bk. & Pg. (Not found)
Filed (Not found)

Public Service Co. of Okla.
10' Right-of-Way Easement
Rec'd. Bk. 894, Pg. 100
Filed April 12, 1971

PCC Sta. 480+65.80
Set 1/2" capped I.P.

SDS 46 of 76

SECTION 29
T-7-N, R-7-W

SCALE
1" = 100'

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---|-----|--|--|--|--|
| SURVEY DIVISION | | | | | |
| SURVEY DATA SHEET | | | | | |
| PLS | JTB | | | | |
| DRAWN | CBM | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BDG | | | | |
| SWO_4925_(1) PROJECT NO. 24428(04) SHEET NO. S046 | | | | | |

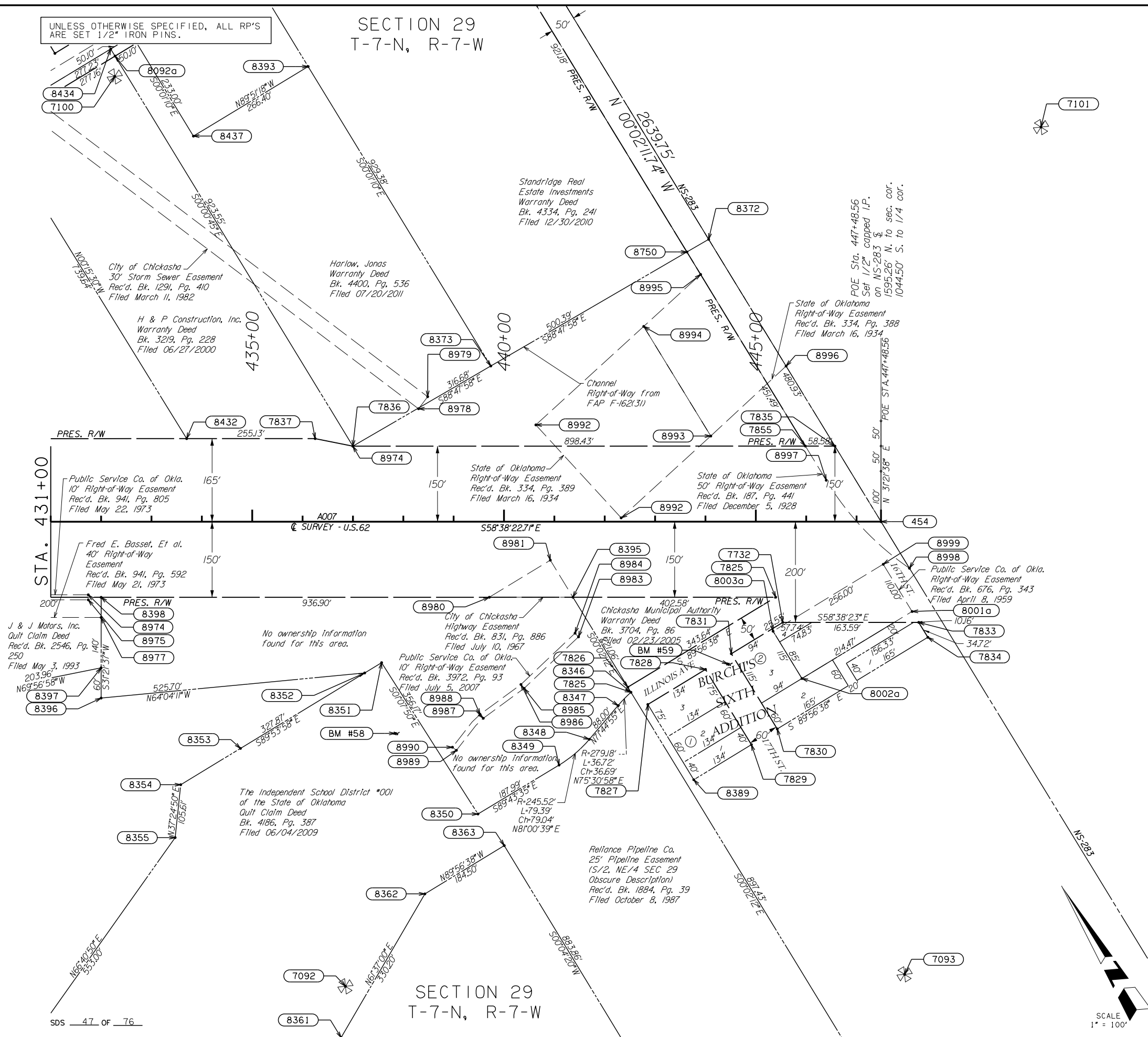
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |

SECTION 29
T-7-N, R-7-W

SECTION 28
T-7-N, R-7-W

SECTION 29
T-7-N, R-7-W

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.



SDS 47 OF 76

SCALE 1" = 100'

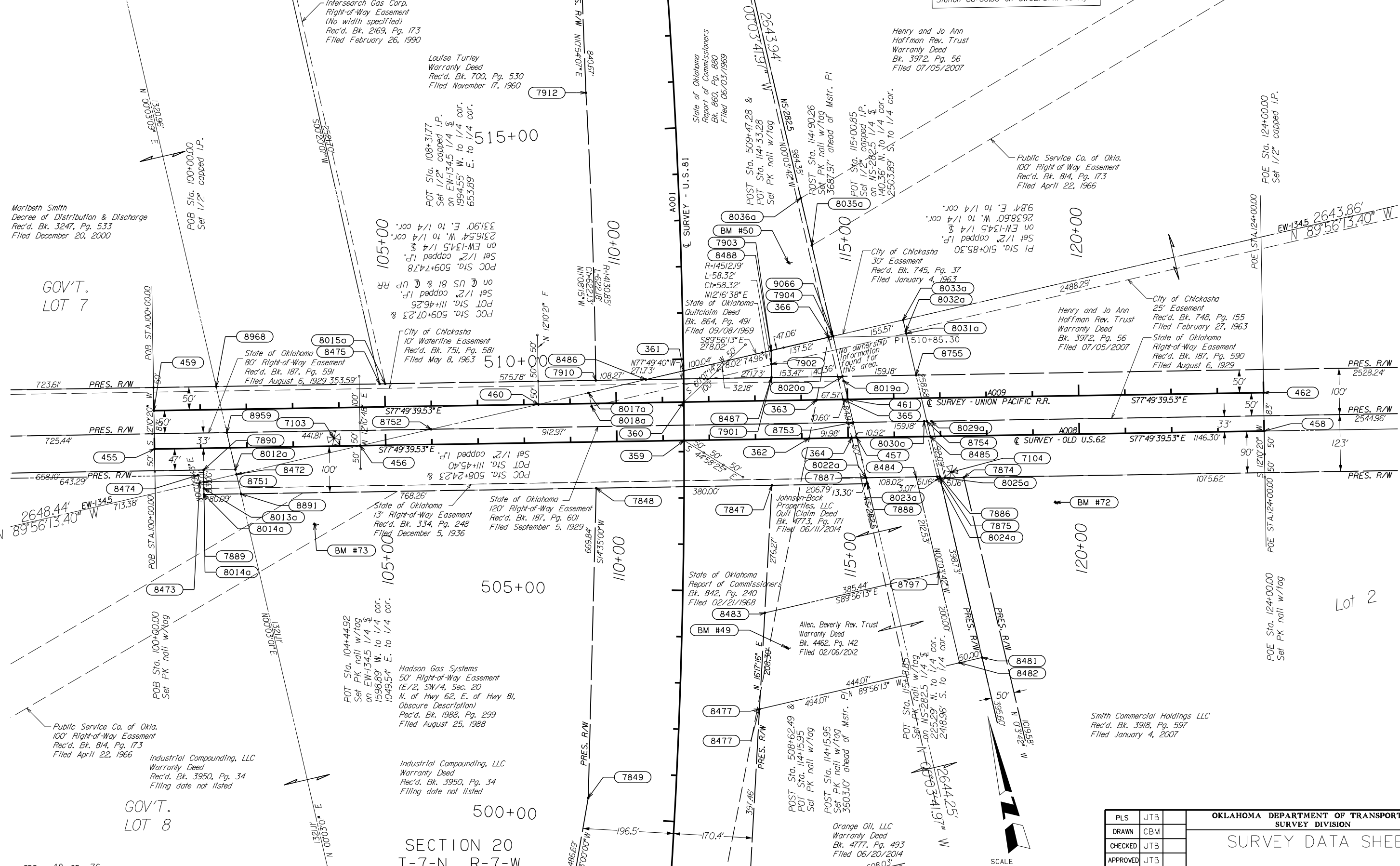
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-----|---|--|--|--|
| SURVEY DIVISION | | | | | |
| SURVEY DATA SHEET | | | | | |
| PLS | JTB | | | | |
| DRAWN | CBM | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BDG | | | | |
| | | SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S047 | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

SECTION 20
T-7-N, R-7-W

Station comparison (UP RR)
Station 115+00.85 on this survey
Station 86+00.00 on SW02737(1) survey

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |



Maribeth Smith
Decree of Distribution & Discharge
Rec'd. Bk. 3247, Pg. 533
Filed December 20, 2000

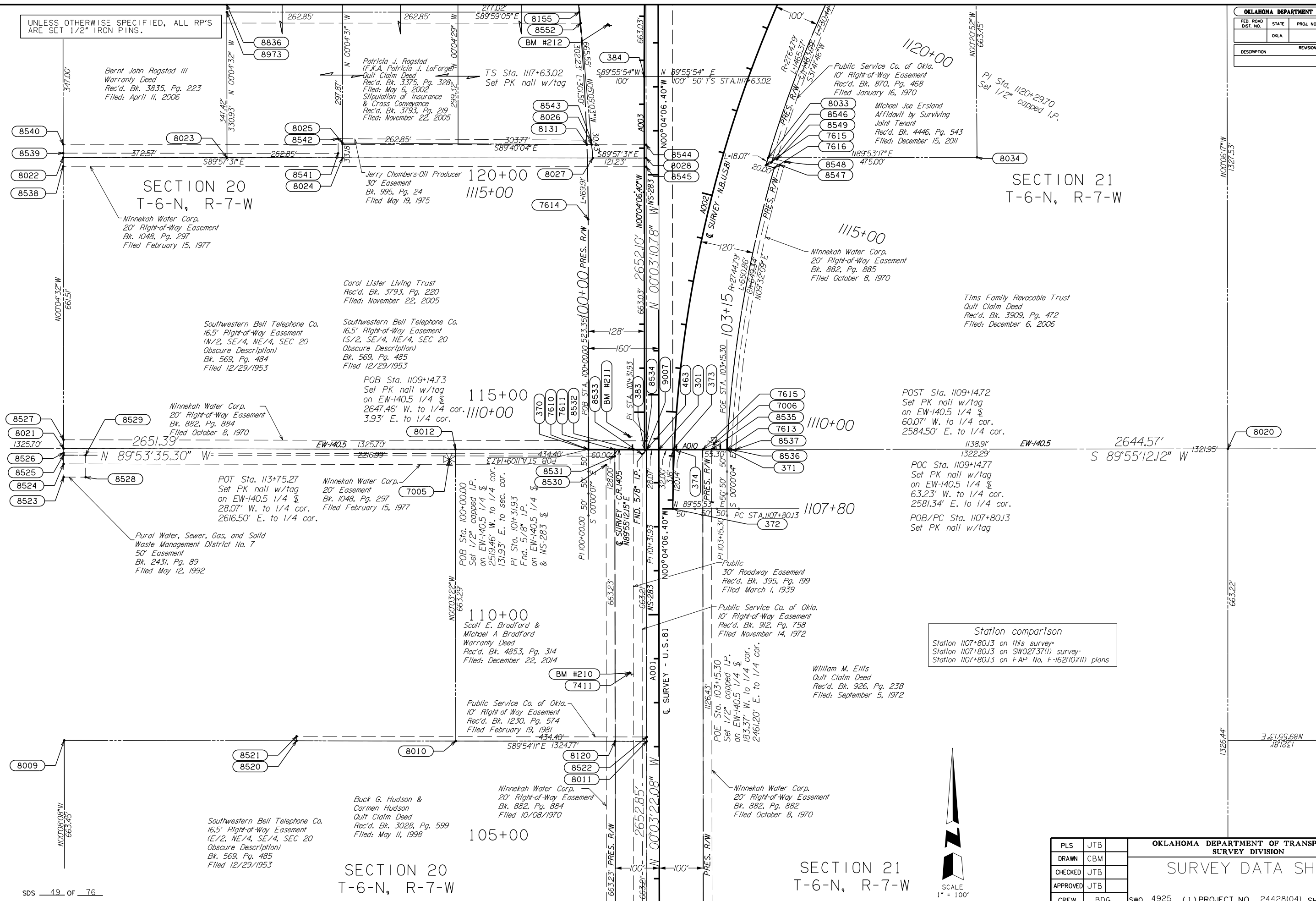
GOV'T.
LOT 7

GOV'T.
LOT 8

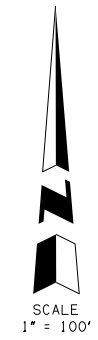
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-----|--------------|-----------------------|----------------|--|
| SURVEY DIVISION | | | | | |
| SURVEY DATA SHEET | | | | | |
| PLS | JTB | | | | |
| DRAWN | CBM | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BDG | SWO_4925_(1) | PROJECT NO. 24428(04) | SHEET NO. S048 | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |
| | | | | | |



Station comparison
 Station 1107+80.13 on this survey
 Station 1107+80.13 on SW02737(1) survey
 Station 1107+80.13 on FAP No. F-162(10X11) plans



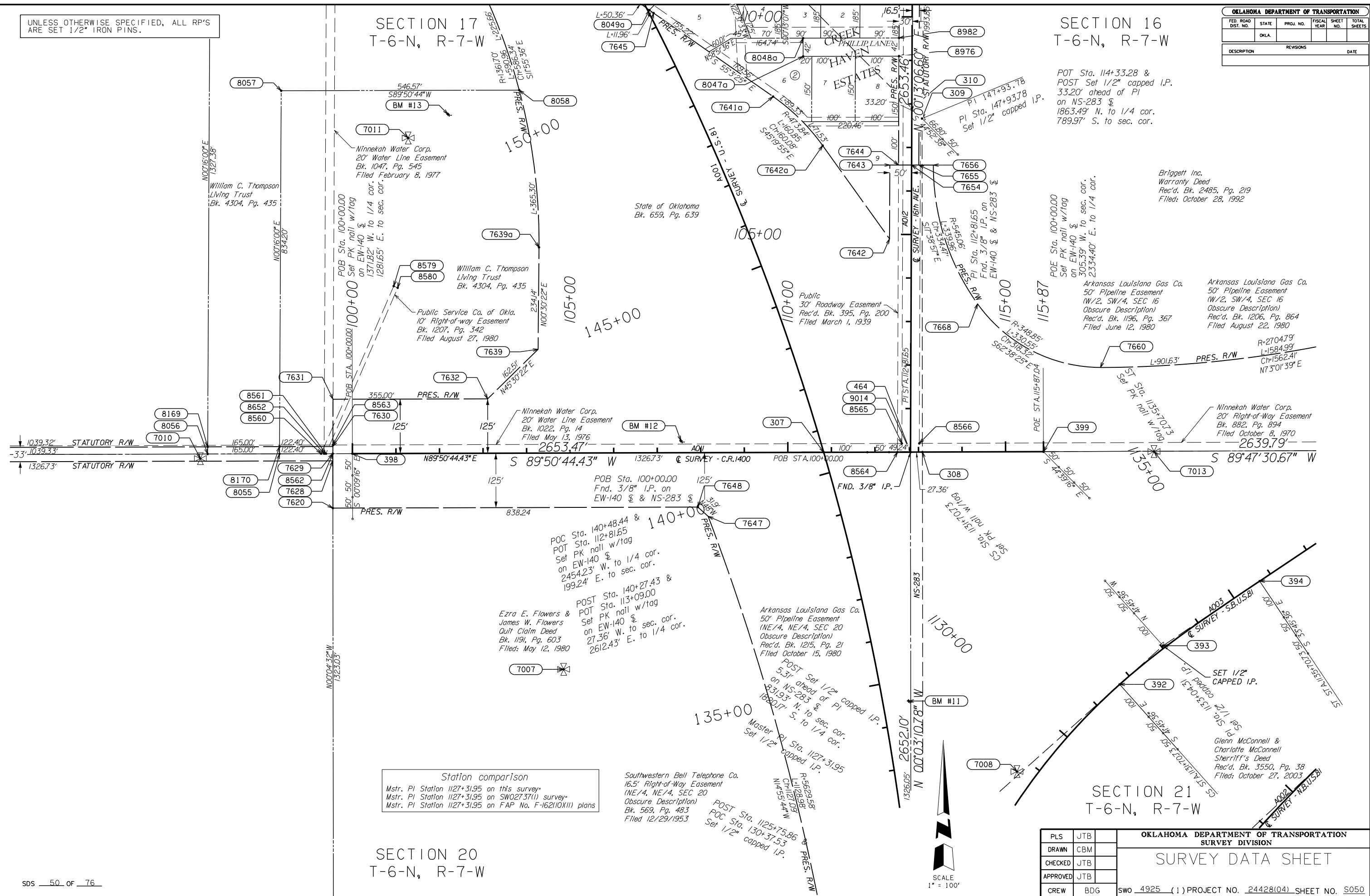
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | |
|---|-----|--|--|
| SURVEY DIVISION | | | |
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S049 | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

SECTION 17
T-6-N, R-7-W

SECTION 16
T-6-N, R-7-W

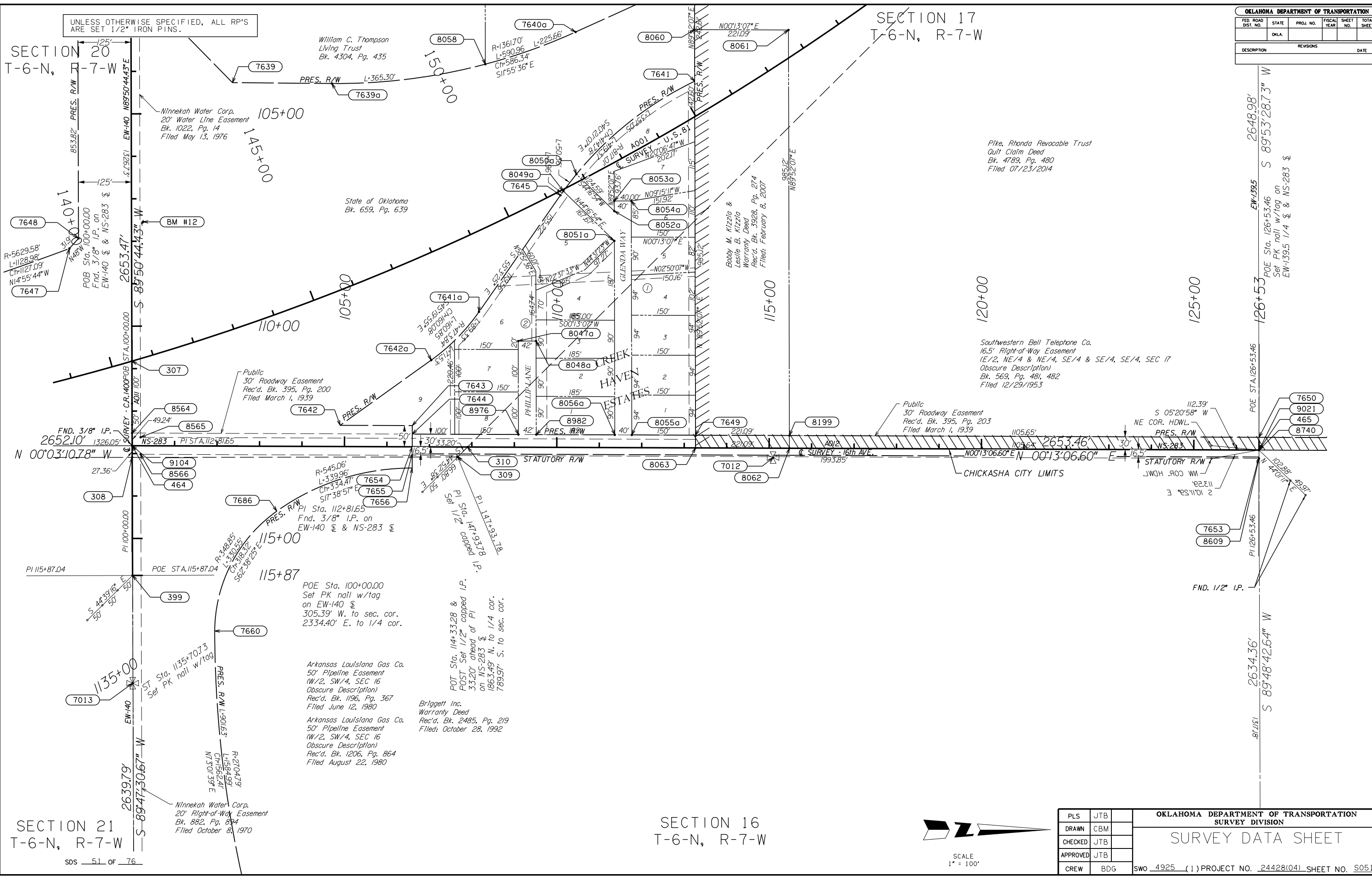
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |
| | | | | | |



SECTION 20
T-6-N, R-7-W

SECTION 21
T-6-N, R-7-W

| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | | | |
|---|-----|--|--|
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S050 | | | |



UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

Pike, Rhonda Revocable Trust
Quit Claim Deed
Bk. 4789, Pg. 480
Filed 07/23/2014

Southwestern Bell Telephone Co.
16.5' Right-of-Way Easement
(E/2, NE/4 & NE/4, SE/4 & SE/4, SE/4, SEC 17
Obscure Description)
Bk. 569, Pg. 481, 482
Filed 12/29/1953

Public
30' Roadway Easement
Rec'd. Bk. 395, Pg. 203
Filed March 1, 1939

William C. Thompson
Living Trust
Bk. 4304, Pg. 435

Ninnekah Water Corp.
20' Water Line Easement
Bk. 1022, Pg. 14
Filed May 13, 1976

State of Oklahoma
Bk. 659, Pg. 639

Public
30' Roadway Easement
Rec'd. Bk. 395, Pg. 200
Filed March 1, 1939

Arkansas Louisiana Gas Co.
50' Pipeline Easement
(W/2, SW/4, SEC 16
Obscure Description)
Rec'd. Bk. 1196, Pg. 367
Filed June 12, 1980

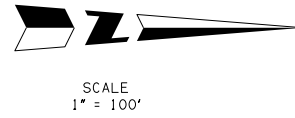
Arkansas Louisiana Gas Co.
50' Pipeline Easement
(W/2, SW/4, SEC 16
Obscure Description)
Rec'd. Bk. 1206, Pg. 864
Filed August 22, 1980

Briggitt Inc.
Warranty Deed
Rec'd. Bk. 2485, Pg. 219
Filed: October 28, 1992

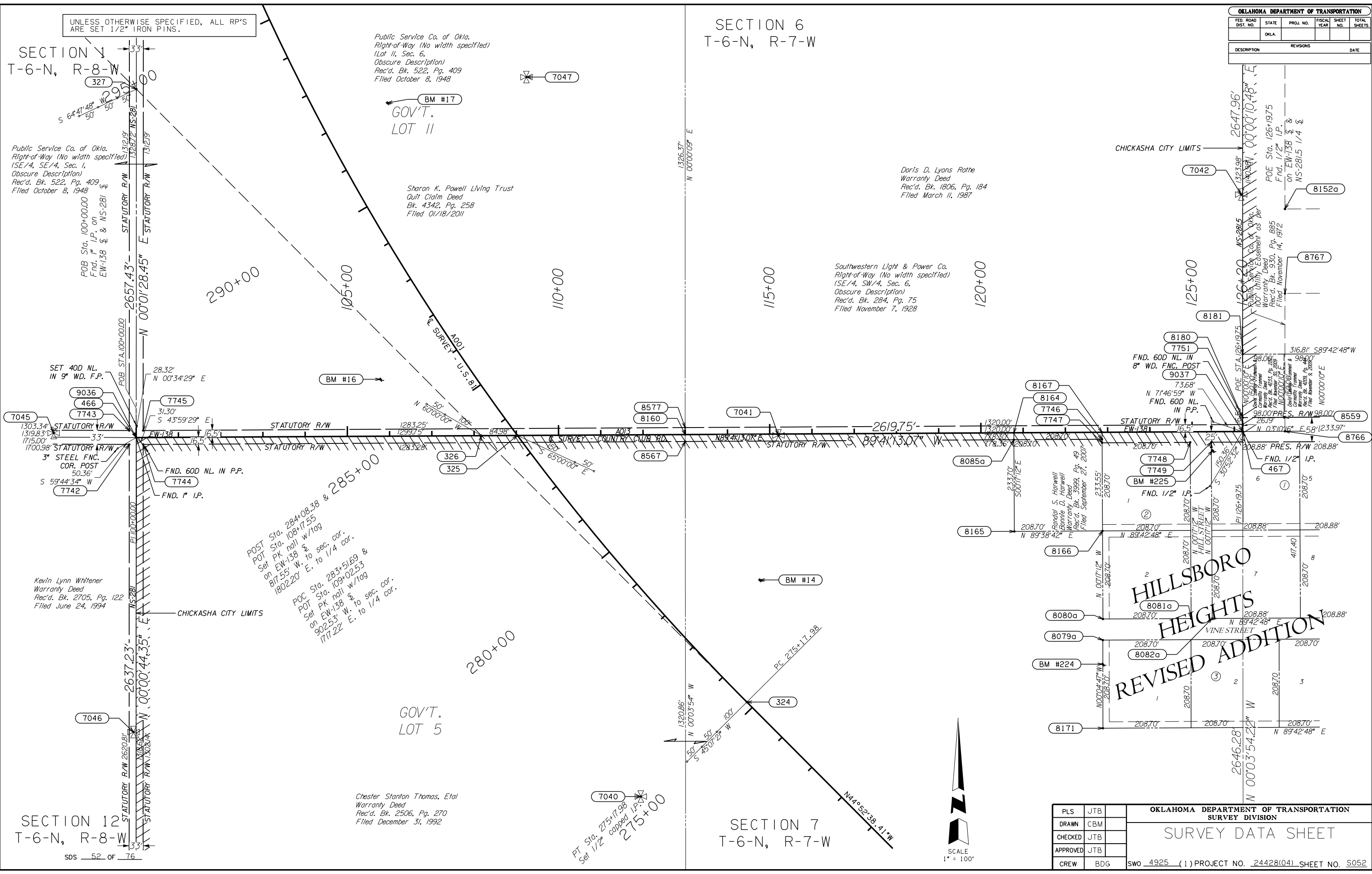
Ninnekah Water Corp.
20' Right-of-Way Easement
Bk. 882, Pg. 884
Filed October 8, 1970

| | |
|----------|-----|
| PLS | JTB |
| DRAWN | CBM |
| CHECKED | JTB |
| APPROVED | JTB |
| CREW | BDG |

| | |
|--|--|
| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | |
| SURVEY DATA SHEET | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S051 | |



| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |



UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

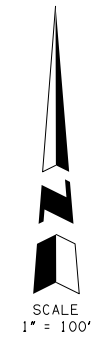
SECTION 1
T-6-N, R-8-W

SECTION 6
T-6-N, R-7-W

SECTION 12
T-6-N, R-8-W

SECTION 7
T-6-N, R-7-W

SDS 52 OF 76



| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---|-----|--|--|--|--|
| SURVEY DIVISION | | | | | |
| SURVEY DATA SHEET | | | | | |
| PLS | JTB | | | | |
| DRAWN | CBM | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BDG | | | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S052 | | | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

SECTION 36
T-7-N, R-8-W

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |
| | | | | | |

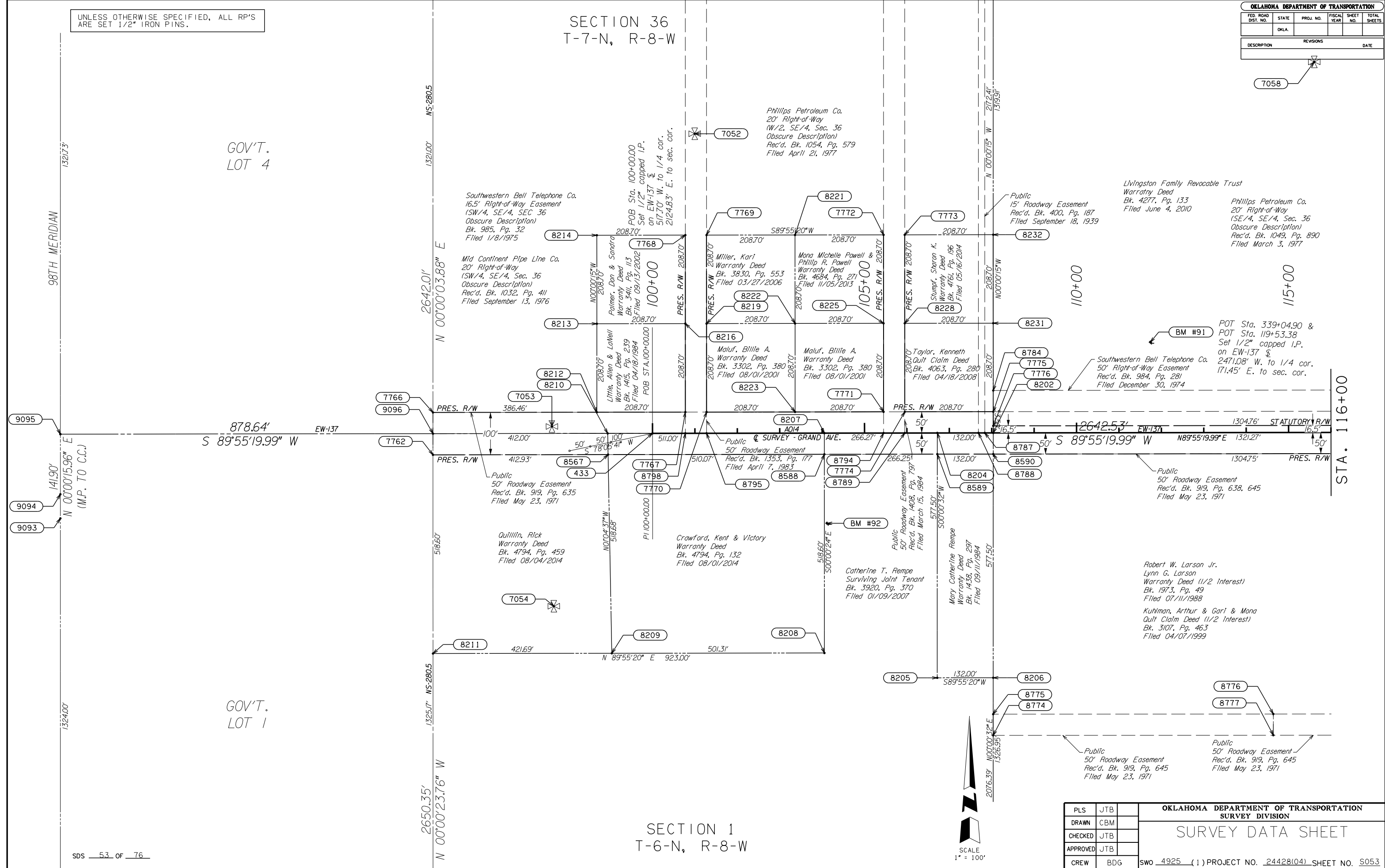
7058

GOV'T.
LOT 4

GOV'T.
LOT 1

98TH MERIDIAN

STA. 116+00



SECTION 1
T-6-N, R-8-W



| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | |
|---|-----|--|--|
| SURVEY DIVISION | | | |
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S053 | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

SECTION 36
T-7-N, R-8-W

SECTION 31
T-7-N, R-7-W

Livingston Family Revocable Trust
Warranty Deed
Bk. 4277, Pg. 133
Filed June 4, 2010

Phillips Petroleum Co.
20' Right-of-Way
(W. 1/4 ac. Lot 9, Sec. 31
Obscure Description)
Rec'd. Bk. 1049, Pg. 890
Filed March 3, 1977

Livingston Family Revocable Trust
Warranty Deed
Bk. 4277, Pg. 133
Filed June 4, 2010

GOV'T.
LOT 9

Livingston Family Revocable Trust
Warranty Deed
Bk. 4277, Pg. 133
Filed June 4, 2010

345+00

Phillips Petroleum Co.
20' Right-of-Way
(SE/4, SE/4, Sec. 36
Obscure Description)
Rec'd. Bk. 1049, Pg. 890
Filed March 3, 1977

120+00

125+00

130+00

STA. 116+00

340+00

1304.76
1321.27

335+00

SECTION 1
T-6-N, R-8-W

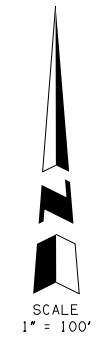
330+00

Carl J. Rempe &
Catherine T. Rempe
Affdt. of Surviving Joint Tenant
Rec'd. Bk. 2966, Pg. 520
Filed July 31, 1997

GOV'T.
LOT 8

SECTION 6
T-6-N, R-7-W

SDS 54 of 76



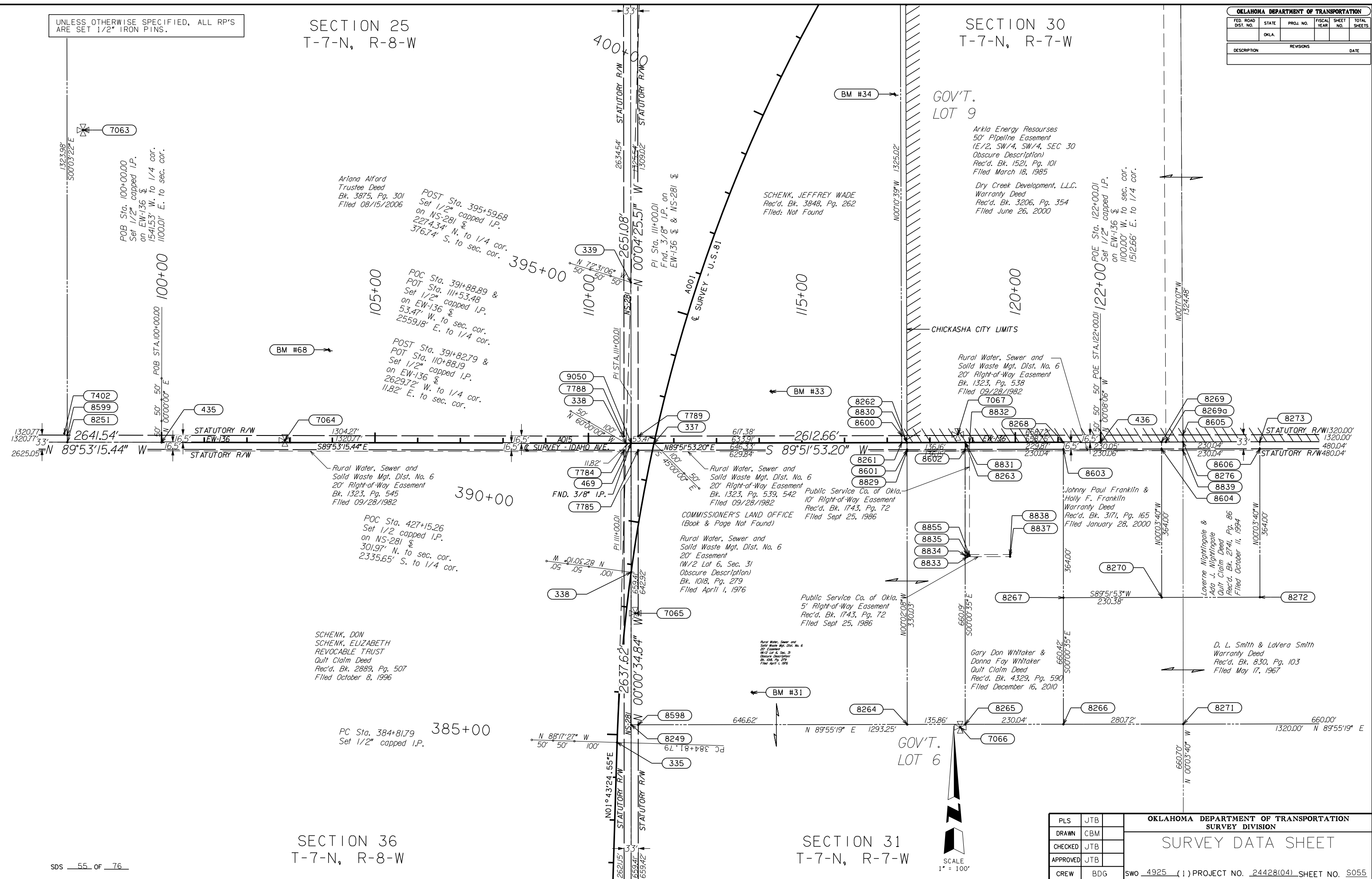
| | | |
|----------|-----|--|
| PLS | JTB | OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION |
| DRAWN | CBM | |
| CHECKED | JTB | |
| APPROVED | JTB | |
| CREW | BDG | |
| | | SURVEY DATA SHEET |
| | | SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S054 |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

SECTION 25
T-7-N, R-8-W

SECTION 30
T-7-N, R-7-W

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |

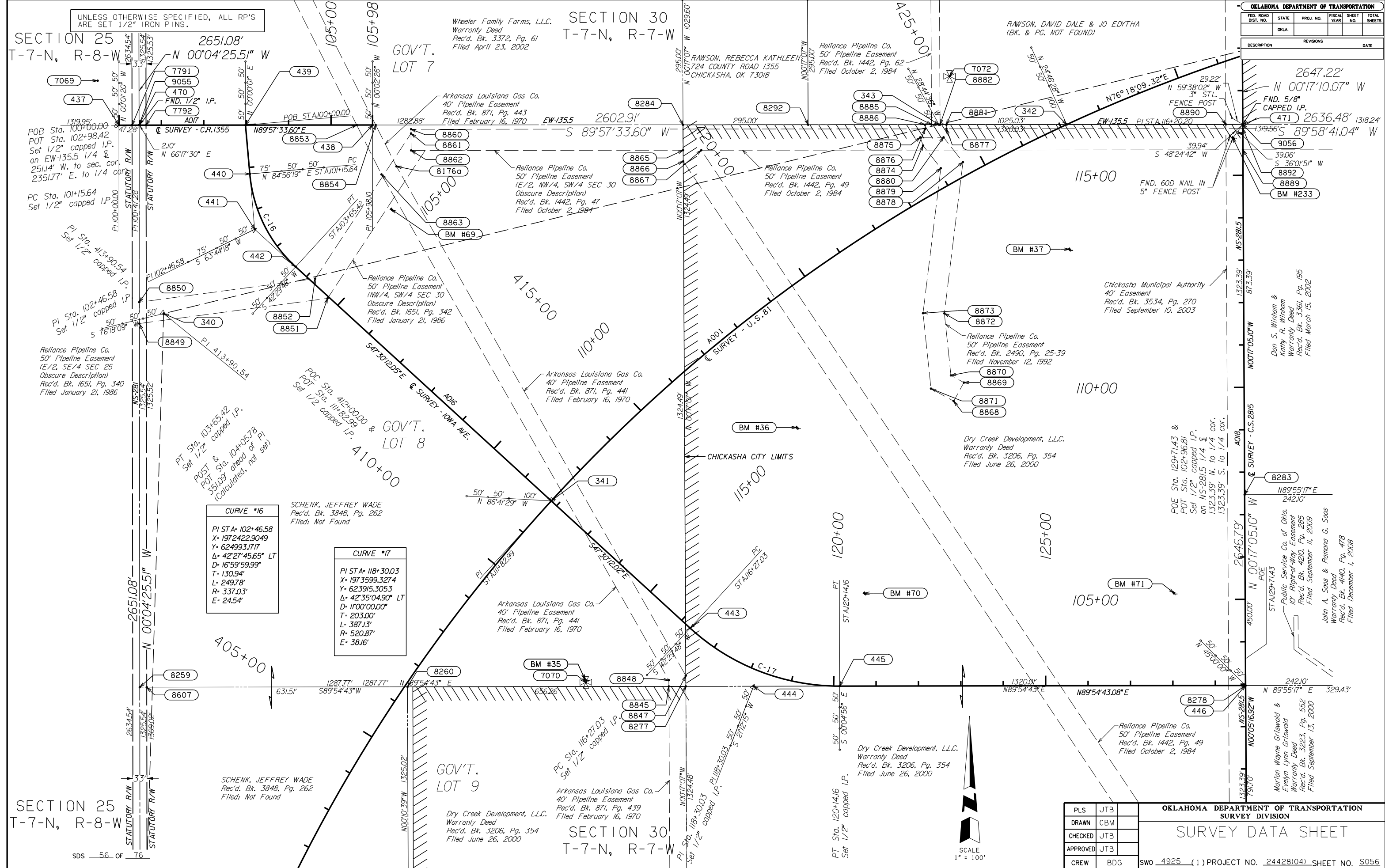


SECTION 36
T-7-N, R-8-W

SECTION 31
T-7-N, R-7-W

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---|-----|--|--|--|--|
| SURVEY DIVISION | | | | | |
| SURVEY DATA SHEET | | | | | |
| PLS | JTB | | | | |
| DRAWN | CBM | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BDG | | | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S055 | | | | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.



| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |

| DESCRIPTION | REVISIONS | DATE |
|-------------|-----------|------|
| | | |

CURVE #16

PI STA+ 102+46.58
 X= 1972422.9049
 Y= 624993.1717
 Δ= 42°27'45.65" LT
 D= 16°59'59.99"
 T= 130.94'
 L= 249.78'
 R= 337.03'
 E= 2454'

CURVE #17

PI STA+ 118+30.03
 X= 1973599.3274
 Y= 623915.3053
 Δ= 42°35'04.90" LT
 D= 11°00'00.00"
 T= 203.00'
 L= 387.13'
 R= 520.87'
 E= 3816'

| | | |
|--|-----|--|
| PLS | JTB | OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION |
| DRAWN | CBM | |
| CHECKED | JTB | |
| APPROVED | JTB | |
| CREW | BDG | |
| SWO_4925_(1) PROJECT NO. 244281(04) SHEET NO. S056 | | |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

SECTION 25
T-7-N, R-8-W

SECTION 30
T-7-N, R-7-W

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |

Wheeler Family Farms, L.L.C.
Warranty Deed
Rec'd. Bk. 3372, Pg. 61
Filed April 23, 2002

| CURVE *16 |
|--------------------|
| PI STA= 102+46.58 |
| X= 19724229049 |
| Y= 6249931717 |
| Δ= 42°27'45.65" LT |
| D= 16°59'59.99" |
| T= 130.94' |
| L= 249.78' |
| R= 337.03' |
| E= 24.54' |

2639.90'
S 89°58'39.45" W

1319.95'

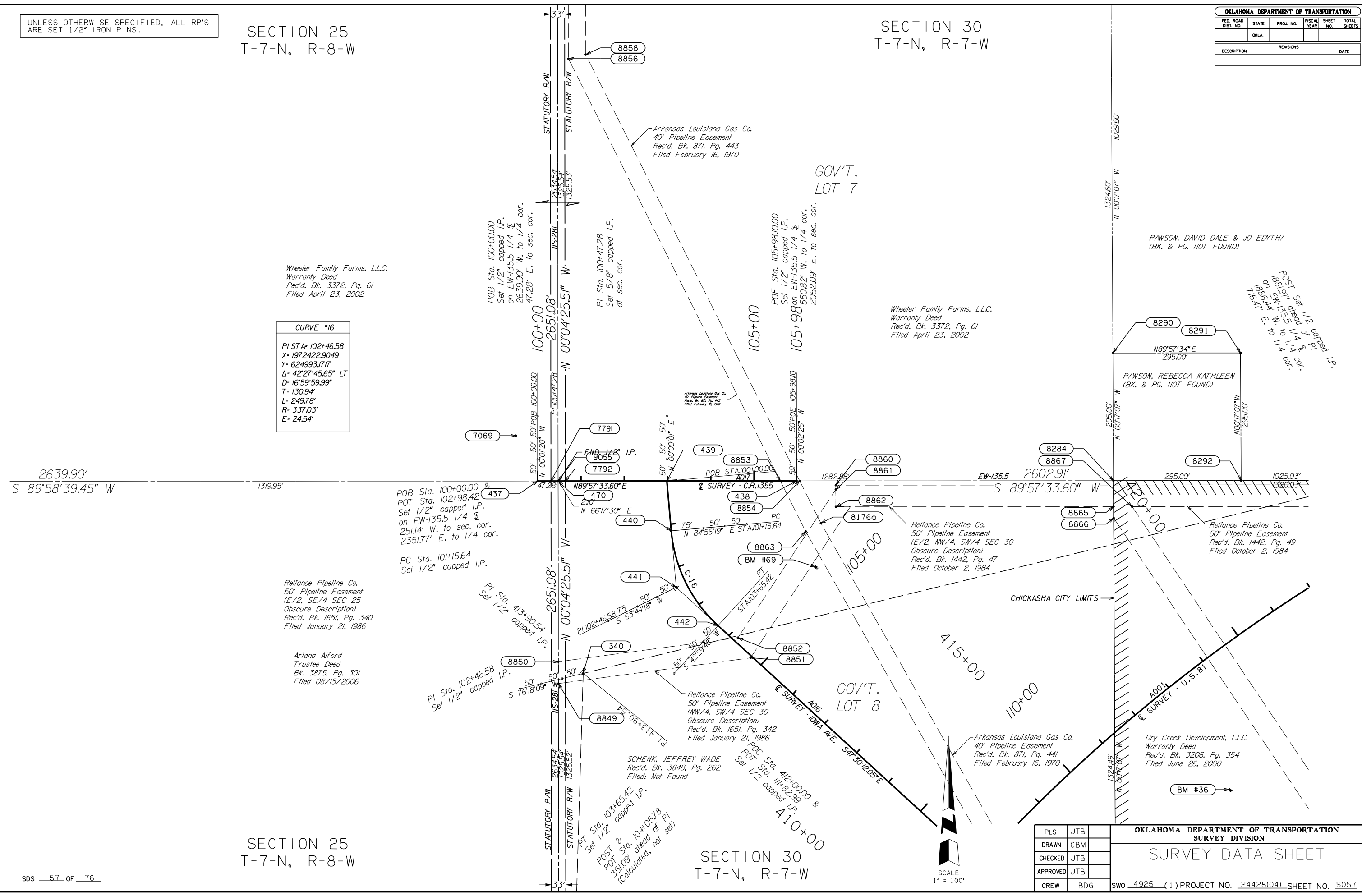
SECTION 25
T-7-N, R-8-W

SECTION 30
T-7-N, R-7-W

| | |
|----------|-----|
| PLS | JTB |
| DRAWN | CBM |
| CHECKED | JTB |
| APPROVED | JTB |
| CREW | BDG |

OKLAHOMA DEPARTMENT OF TRANSPORTATION
SURVEY DIVISION

SURVEY DATA SHEET

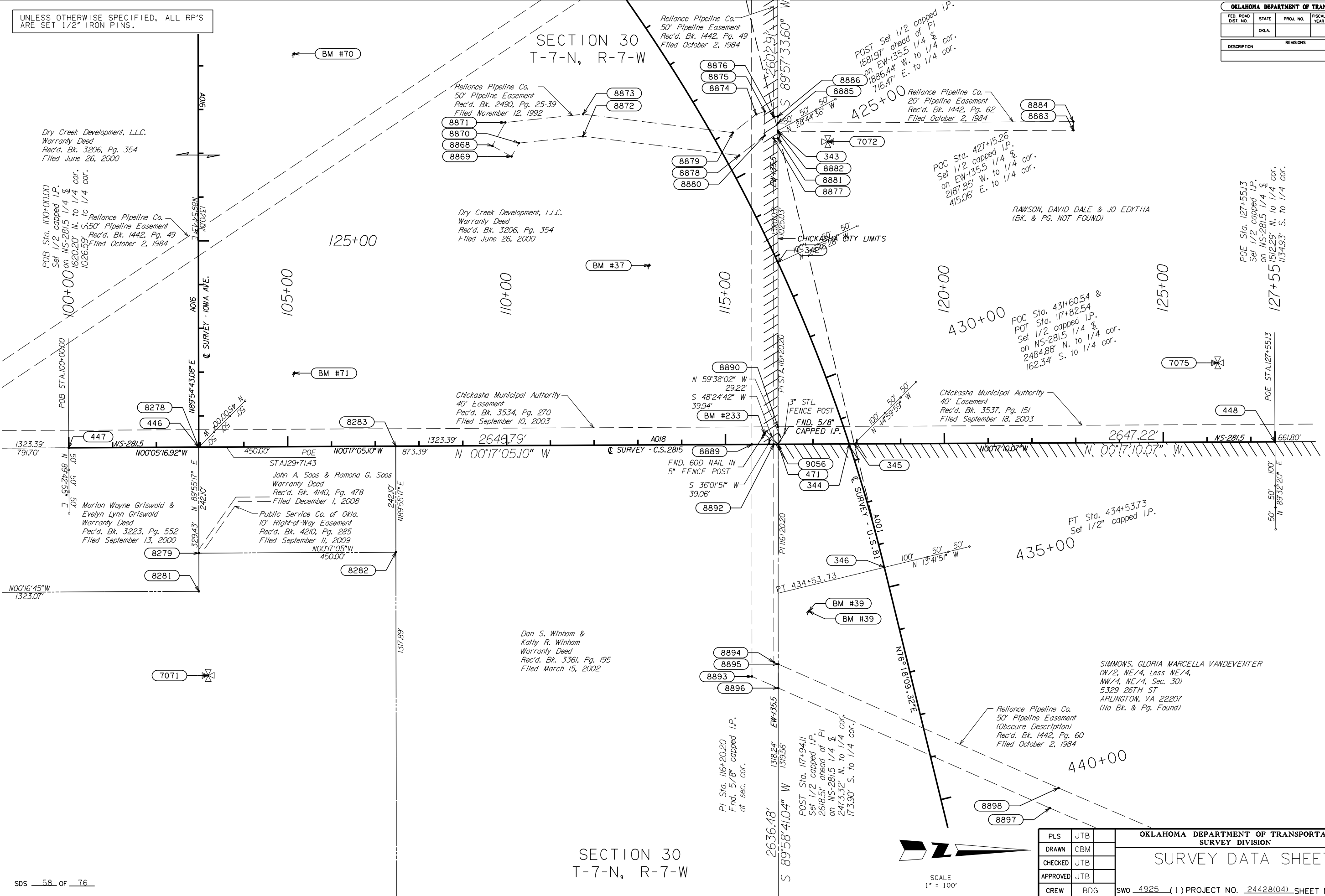


UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

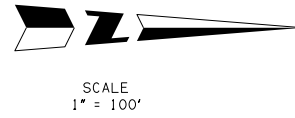
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |
| | | | | | |

SECTION 30
T-7-N, R-7-W

SECTION 30
T-7-N, R-7-W



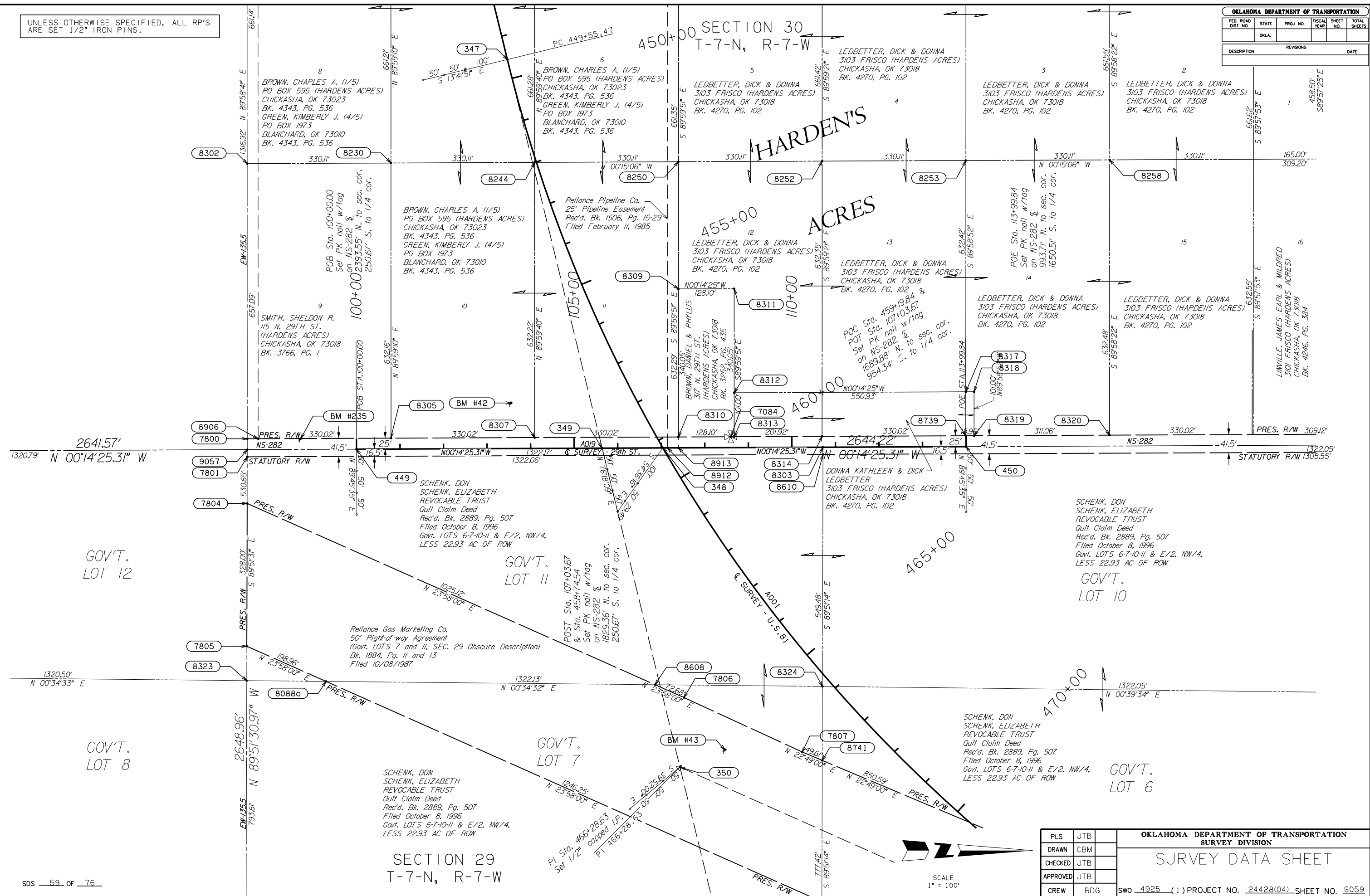
SDS 58 of 76



| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | | | |
|--|-----|-----------------------|----------------|
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) | | PROJECT NO. 24428(04) | SHEET NO. S058 |

UNLESS OTHERWISE SPECIFIED, ALL RP'S ARE SET 1/2" IRON PINS.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PRJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA | | | | |
| DESCRIPTION | | REVISIONS | | DATE | |
| | | | | | |

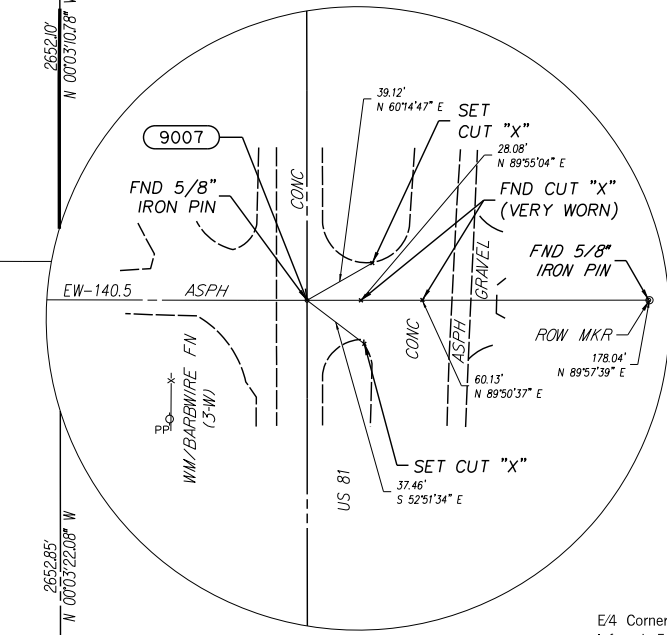
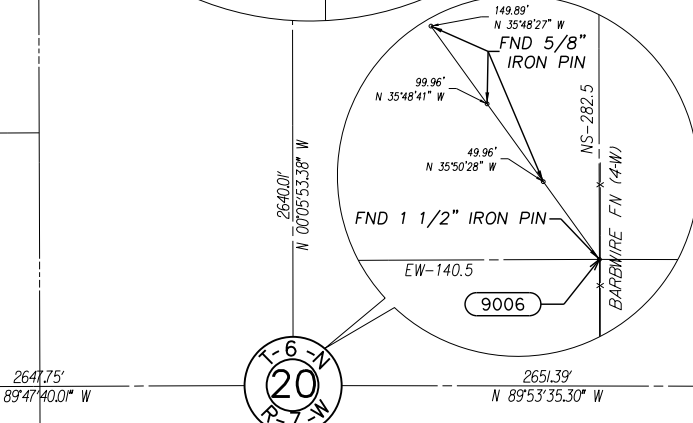
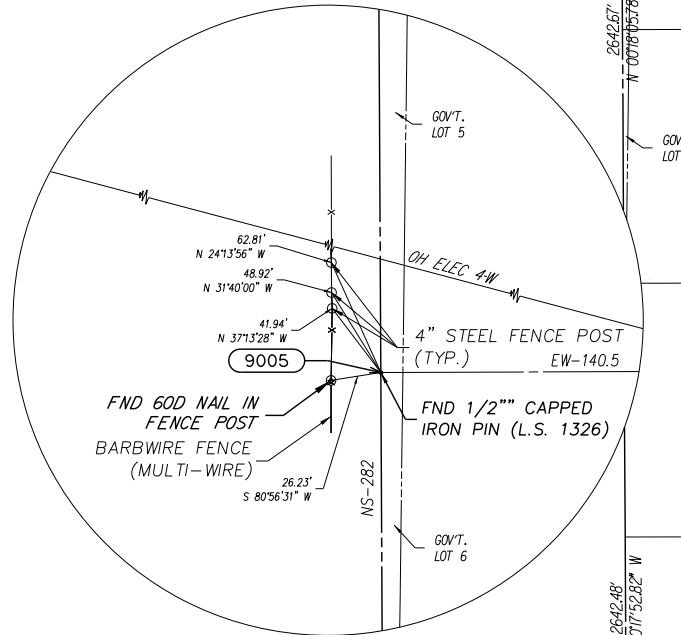
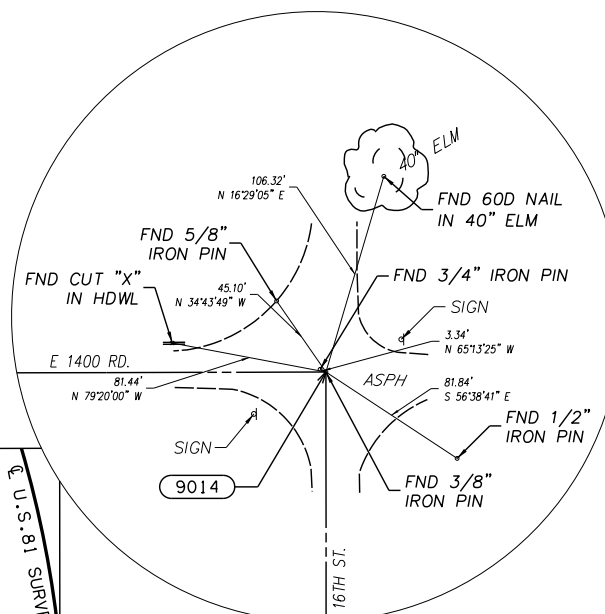
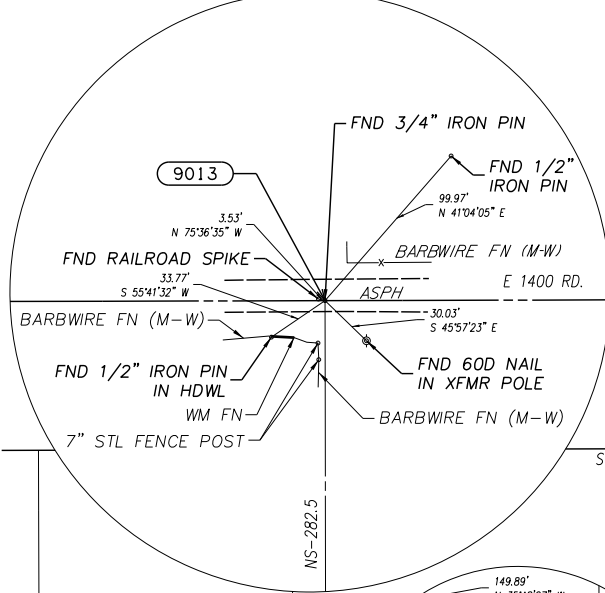
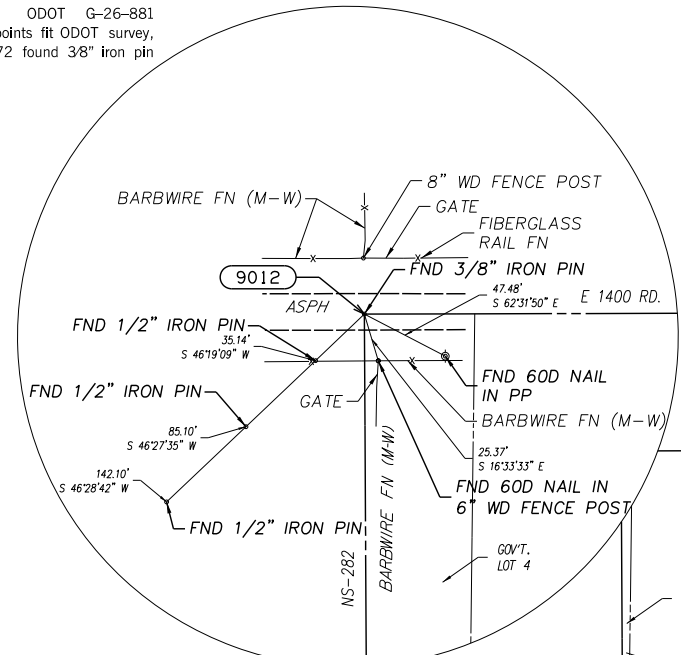


SOS 59 of 76

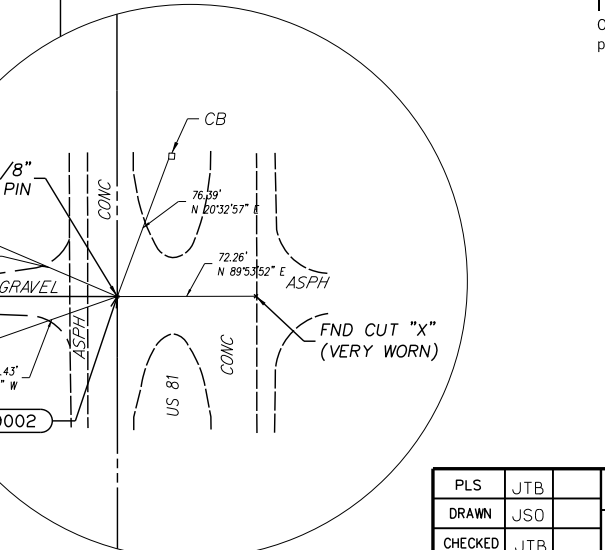
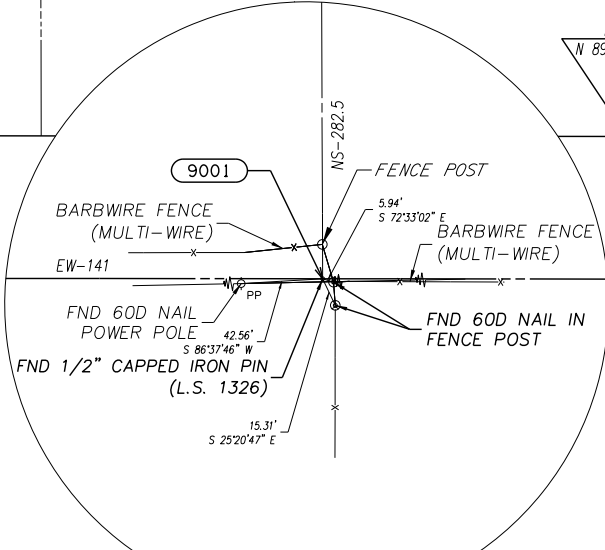
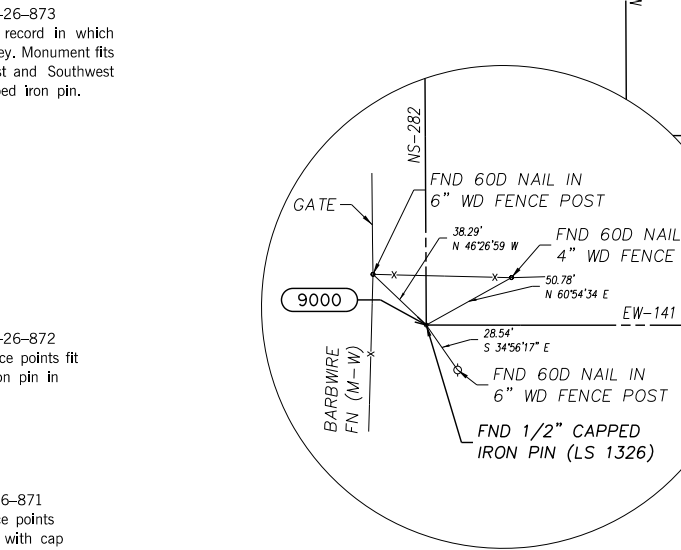
| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | | | |
|---|-----|--|--|
| SURVEY DATA SHEET | | | |
| PLS | JTB | | |
| DRAWN | CBM | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BDG | | |
| SWO 4925 (1) PROJECT NO. 24428(04) SHEET NO. S059 | | | |

NW Corner of Section 20, T-6N, R-7W ODOT G-26-881
 I found 3/8" iron pin. Monument and reference points fit ODOT survey, SWO 2737(1), and corner record in which L.S. 1272 found 3/8" iron pin in 1998 survey. Nothing set. I used 3/8" iron pin.

Angle Of Variance
 At Sta. 9006 (C/4 Cor.)
 X: 1980072.2880
 Y: 598768.3500
 Lat.: 34°58'43.06084"
 Long.: 97°57'40.93182"
 θ = 0°01'18.93742"



W/4 Corner of Section 20, T-6N, R-7W ODOT G-26-873
 I found 1/2" capped iron pin. Monument fits corner record in which L.S. 1326 set 1/2" iron pin with cap in 2012 survey. Monument fits single proportionate method between the Northwest and Southwest corners of Section 20. Nothing set. I used 1/2" capped iron pin.



SW Corner of Section 20, T-6N, R-7W ODOT G-26-872
 I found 1/2" capped iron pin. Monument and reference points fit corner record in which L.S. 1326 set 1/2" capped iron pin in 2007 survey. Nothing set. I used 1/2" capped iron pin.

S/4 Corner of Section 20, T-6N, R-7W ODOT G-26-871
 I found 1/2" capped iron pin. Monument and reference points fit corner record in which L.S. 1326 set 1/2" iron pin with cap in 2012 survey. Monument fits single proportionate method between the southwest and southeast corners of Section 20. Nothing set. I used 1/2" capped iron pin.

N/4 Corner of Section 20, T-6N, R-7W ODOT G-26-880
 I found 3/4" iron pin and railroad spike. 3/4" iron pin and reference points fit ODOT survey, SWO 2737(1). Railroad spike and reference fit corner record in which L.S. 1326 found railroad spike in 2012 survey. The railroad spike had been set using the Southwest corner and one of two monuments at the Southeast corner of Section 17. Railroad spike was set by others for new fence line to the south in 2011. Nothing set. I used 3/4" iron pin.

NE Corner of Section 20, T-6N, R-7W ODOT G-26-879
 I found 3/8" iron pin and 3/4" iron pin. The 3/4" iron pin does not fit ODOT survey, SWO 2737(1), but it does fit the following corner records:

L.S. 1272 found 3/4" iron pin in 1998 survey
 L.S. 1384 found #6 rebar in 2008 survey.

The 3/8" iron pin and reference points fit said ODOT survey. Nothing set. I used 3/8" iron pin.

C/4 Corner of Section 20, T-6N, R-7W ODOT G-26-874
 I found 1 1/2" iron pin. Monument and reference points fit ODOT survey, SWO 2737(1). Nothing set. I used 1 1/2" iron pin.

E/4 Corner of Section 20, T-6N, R-7W ODOT G-26-163
 I found 5/8" iron pin. Monument and reference points fit ODOT survey, SWO 2737(1). Nothing set. I used 5/8" iron pin and set additional reference points.

SE Corner of Section 20, T-6N, R-7W ODOT G-26-162
 I found 5/8" iron pin. Monument and reference point fit ODOT survey, SWO 2737(1). Nothing set. I used 5/8" iron pin.

SCALE: 1" = 50'
 LAND CORNER DETAILS ARE SHOWN AT 1" = 50'

| OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION | | | |
|---|--------|---|--|
| PLS | JTB | | |
| DRAWN | JSO | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BENHAM | | |
| | | SURVEY DATA SHEET | |
| | | SDS 60 OF 76 | |
| | | SWO 4380 (1) PROJECT NO. 24428(04) SHEET NO. S060 | |

N4 Corner of Section 21, T-6N, R-7W ODOT G-26-880
 I found 3/4" iron pin and railroad spike. 3/4" iron pin and reference points fit ODOT survey, SWO 2737(1). Railroad spike and reference points fit corner record in which L.S. 1326 found railroad spike in monuments at the Southeast corner of Section 17. Railroad spike was set by others for new fence line to the South in 2011. Nothing set. I used 3/4" iron pin.



Angle Of Variance
 At Sta. 9008 (C/4 Cor.)
 X = 1985368.2480
 Y = 598767.0960
 Lat. = 34°58'43.02381"
 Long. = 97°56'37.28846"
 θ = 0°01'55.06245"

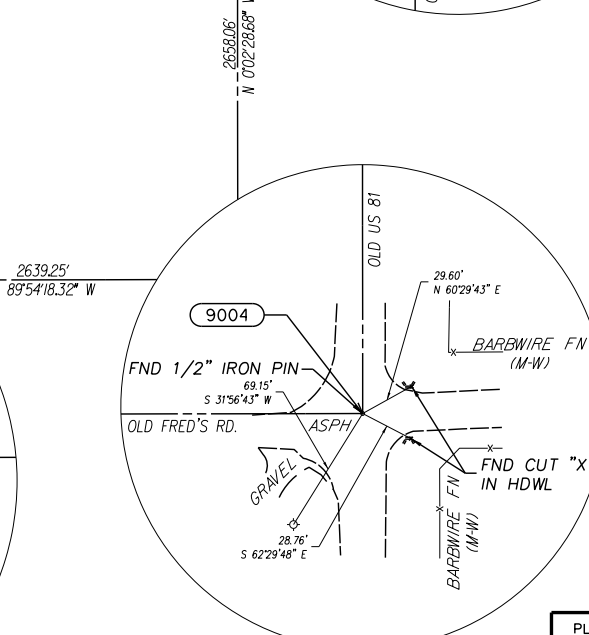
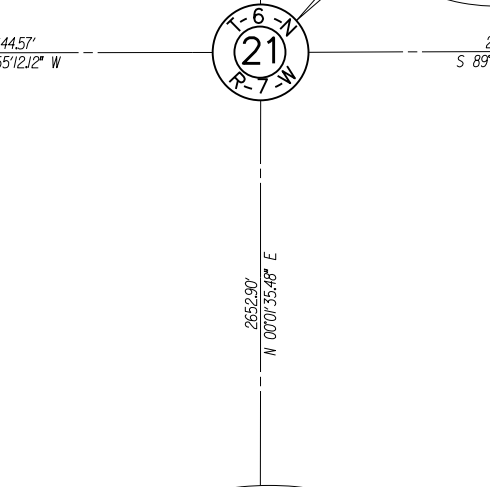
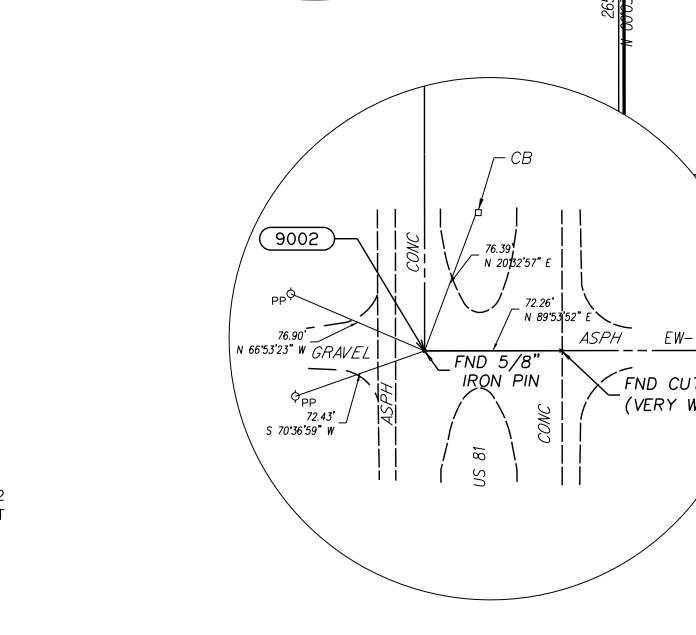
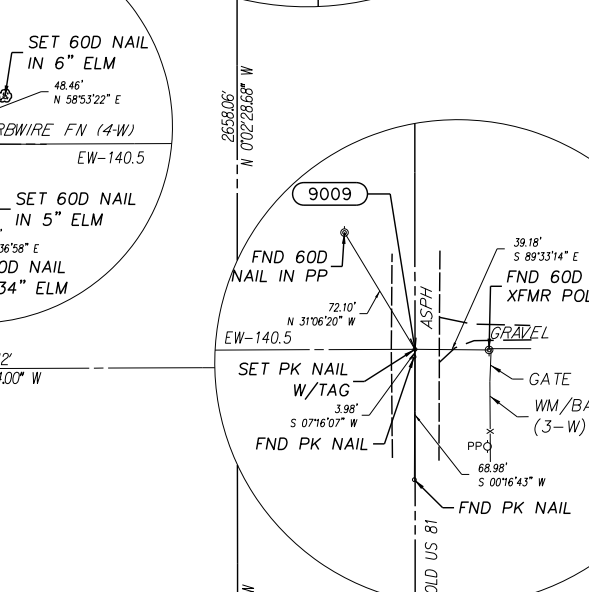
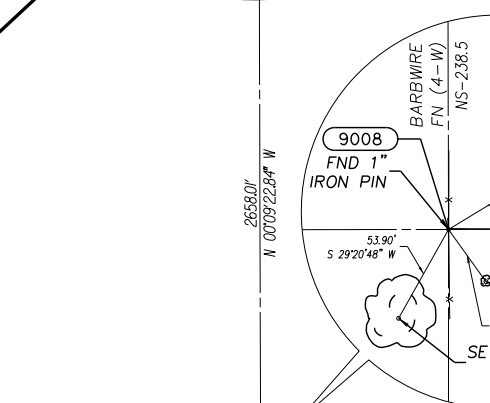
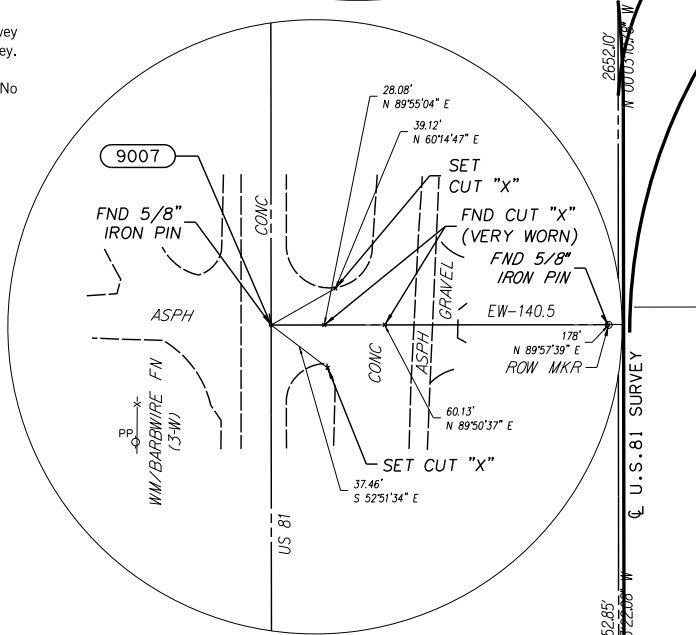
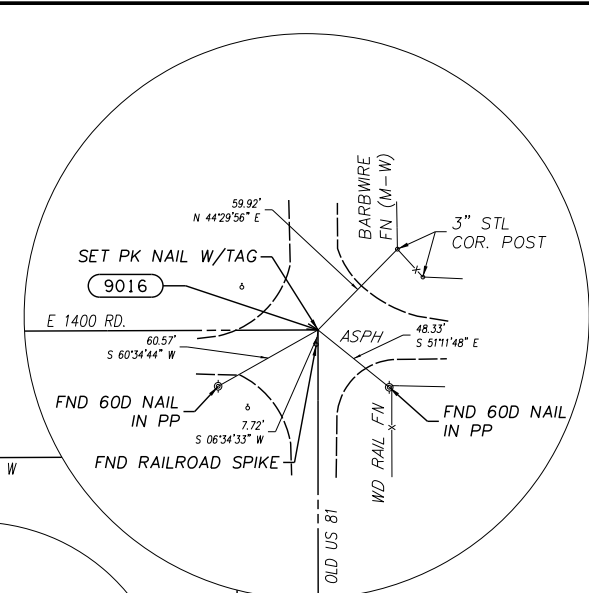
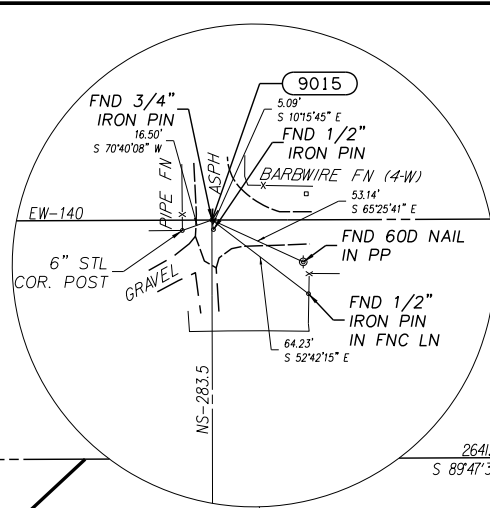
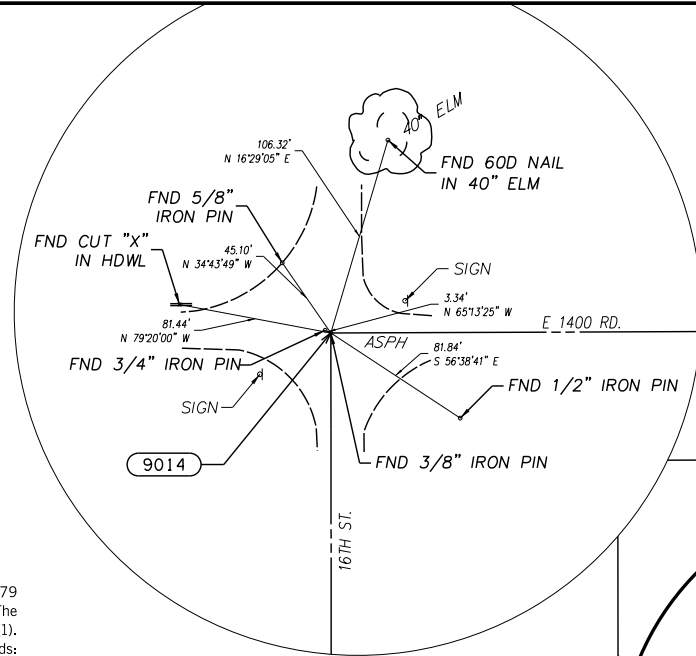
NW Corner of Section 21, T-6N, R-7W ODOT G-26-879
 I found 3/8" iron pin and 3/4" iron pin. The 3/4" iron pin does not fit ODOT survey, SWO 2737(1). It does fit the following corner records:

L.S. 1272 found 3/4" iron pin in 1998 survey
 L.S. 1384 found #6 rebar in 2008 survey.

The 3/8" iron pin and reference points fit said ODOT survey. No evidence set. I used 3/8" iron pin.

W4 Corner of Section 21, T-6N, R-7W ODOT G-26-163
 I found 5/8" iron pin. Monument and reference points fit ODOT survey, SWO 2737(1). Nothing set. I used 5/8" iron pin and set additional reference points.

SW Corner of Section 21, T-6N, R-7W ODOT G-26-162
 I found 5/8" iron pin. Monument and reference point fits ODOT survey, SWO 2737(1). Nothing set. I used 5/8" iron pin.



NE Corner of Section 21, T-6N, R-7W ODOT G-26-877
 I found R.R. Spike. Monument and references do not fit ODOT survey, SWO 2028(1). Monument and references fit the following C.C.R.'s:

L.S. 1071 found PK nail in 1989 survey
 L.S. 1272 found R.R. Spike in 1997 survey
 L.S. 696 found R.R. Spike in 1999 survey.

I set PK nail with tag using measurements from said ODOT survey. I did not accept R.R. Spike since it did not fit G.L.O. Notes or ODOT survey.

C/4 Corner of Section 21, T-6N, R-7W ODOT G-26-875
 I found 1" iron pin. Monument fits measurements from ODOT survey, SWO 2737(1). Monument position fits existing fence lines. Nothing set. I used 1" iron pin and set reference points

E/4 Corner of Section 21, T-6N, R-7W ODOT G-26-876
 I found PK nail. Monument and reference points fit corner record in which L.S. 1326 set PK nail in 2001 survey. Monument had been set using single proportionate method using a questionable railroad spike at the Northeast corner of Section 21. Monument does not fit measurements from ODOT survey, SWO 2028(1), or existing fence lines. I set PK nail with tag. I set monument using single proportionate method using the northeast and southeast corners of Section 21. Monument location fits said ODOT survey and existing fence lines.

SE Corner of Section 21, T-6N, R-7W ODOT G-26-869
 I found 1/2" iron pin. Monument and references fit corner record in which L.S. 1272 found 1/2" iron pin in 1997 survey. Nothing set. I used 1/2" iron pin.

S/4 Corner of Section 21, T-6N, R-7W ODOT G-26-870
 I found 1" iron pin and railroad spike. The railroad spike fits corner record in which L.S. 1326 set railroad spike in 2001 survey. The 1" iron pin and reference points fit ODOT survey, SWO 2737(1). Nothing set. I used 1" iron pin.

SCALE:
 1" = 500'
 LAND CORNER DETAILS ARE SHOWN AT 1"=50'

| | | | |
|----------|--------|--|--|
| PLS | JTB | | OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET SDS <u>61</u> OF <u>76</u> |
| DRAWN | JSD | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BENHAM | | |

SWO 4380 (1) PROJECT NO. 24428(04) SHEET NO. S061

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |
| | | | | | |

NW Corner of Section 18, T-6N, R-7W ODOT G-26-897
I found 3/4" iron pin. Corner location and found reference points match the following corner records. L.S. 449 found iron pin in 1982 survey. L.S. 1272 found 3/8" iron pin in 1998 survey. L.S. 1272 found iron pin in 2001 survey. Nothing set. I used 3/4" iron pin.

N4 Corner of Section 18, T-6N, R-7W ODOT G-26-896
I found original stone. No certified corner records found, although monument is referenced on C.C.R.'s filed on the SW and N4 corner of Section 7. No original reference points were found. The stone was marked "1/4" on the north face and looked undisturbed. Nothing set. I used original stone and set reference points.



Angle Of Variance
At Sta. 9018 (C/4 Cor.)
X = 1974779.2248
Y = 604077.4383
Lat. = 34°59'35.58920"
Long. = 97°58'44.52714"
θ = 0°00'42.83966"

NE Corner of Section 18, T-6N, R-7W ODOT G-26-895
I found 1/2" iron pin. This monument was set by others with no corner record filed. I did not find any supporting evidence for this corner location. This corner was used in the construction of a new barbed-wire fence running north & south. New fence location deviates from the old fence location by approximately 4 feet to the east. I set 5/8" capped iron pin. I used measurements from ODOT survey, SWO 2737(1) to set corner. Corner location fits old fence line to the south.

C4 Corner of Section 18, T-6N, R-7W ODOT G-26-885
No corner evidence found. I set 5/8" capped iron pin. I used measurements from Oklahoma Turnpike Authority Plans, Contract No. 206 to set monument. Position fits old fence line to the north.

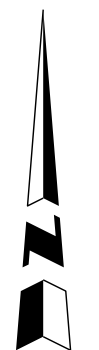
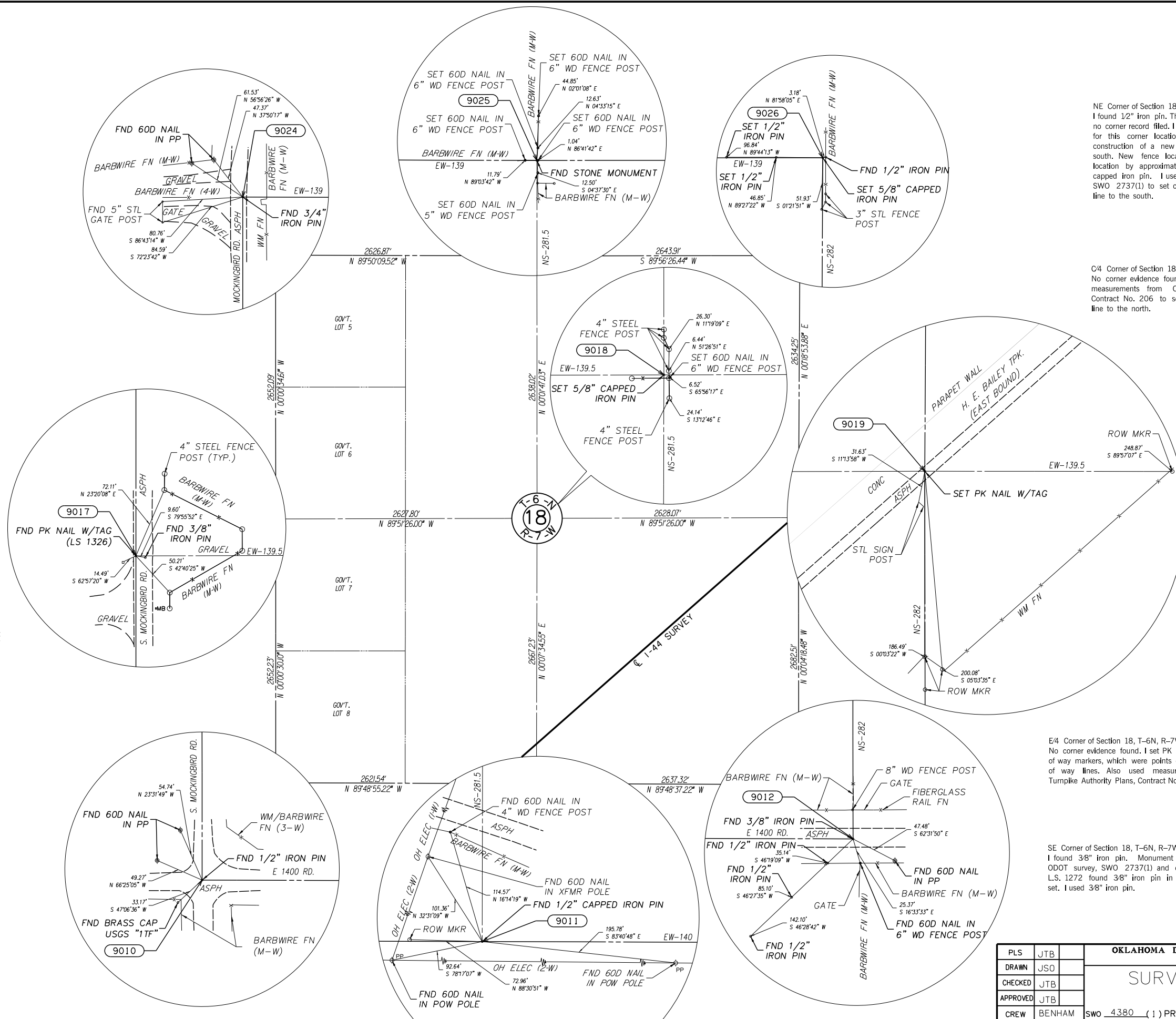
W4 Corner of Section 18, T-6N, R-7W ODOT G-26-884
I found PK nail with tag set by L.S. 1326. Monument and reference points fit corner record in which L.S. 615 found a 1/2" iron pin and set another 1/2" iron pin in 2001 survey. Monument fits single proportionate method measurements to NW and SW corners of Section 18. Nothing set. I used PK nail with tag.

SW Corner of Section 18, T-6N, R-7W ODOT G-26-883
I found 1/2" iron pin. Monument and reference points fit the following corner records. L.S. 449 found iron pin in 1981 survey. L.S. 1082 found iron pin in 1994 survey. L.S. 1272 found 3/8" iron pin in 1998 survey. Monument also fits measurements from Oklahoma Turnpike Authority Plans, Contract 206. No Corner evidence found. I used 1/2" iron pin.

E4 Corner of Section 18, T-6N, R-7W ODOT G-26-886
No corner evidence found. I set PK nail with tag. I used right of way markers, which were points on section lines and right of way lines. Also used measurements from Oklahoma Turnpike Authority Plans, Contract No. 206 to set monument.

SE Corner of Section 18, T-6N, R-7W ODOT G-26-881
I found 3/8" iron pin. Monument and reference points fit ODOT survey, SWO 2737(1) and corner record in which L.S. 1272 found 3/8" iron pin in 1998 survey. Nothing set. I used 3/8" iron pin.

S4 Corner of Section 18, T-6N, R-7W ODOT G-26-882
I found 1/2" capped iron pin. Monument and references fit corner record in which L.S. 1326 set 1/2" iron pin with cap in 2012 survey. Monument also fits single proportionate method from G.L.O. Notes. Nothing set. I used 1/2" capped iron pin.



SCALE:
1" = 500'

LAND CORNER DETAILS ARE SHOWN AT 1"=50'

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|--------|--------------|-----------------------|----------------|--|
| SURVEY DIVISION | | | | | |
| SURVEY DATA SHEET | | | | | |
| | | | SDS 62 OF 76 | | |
| PLS | JTB | | | | |
| DRAWN | JSD | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BENHAM | SWO 4380 (1) | PROJECT NO. 24428(04) | SHEET NO. S062 | |

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | | DATE |
| | | | | | |

N/4 Corner of Section 17, T-6N, R-7W ODOT G-26-892
I found 1/2" iron pin. Monument and references fit the following C.C.R.'s.:

L.S. 1326 found 1/2" iron pin in 2005 survey
L.S. 189 found #3 rebar in 2009 survey.

Nothing set. I used 1/2" iron pin.

NW Corner of Section 17, T-6N, R-7W ODOT G-26-895
I found 1/2" iron pin. This monument was set by others with no corner record filed. I did not find any supporting evidence for this corner location. This corner was used in the construction of a new barbed-wire fence running north and south. New fence location deviates from the old fence location by approximately 4 feet to the east. I set 5/8" capped iron pin. I used measurements from ODOT survey, SWO 2737(1), to set corner. Corner location fits old fence line to the south

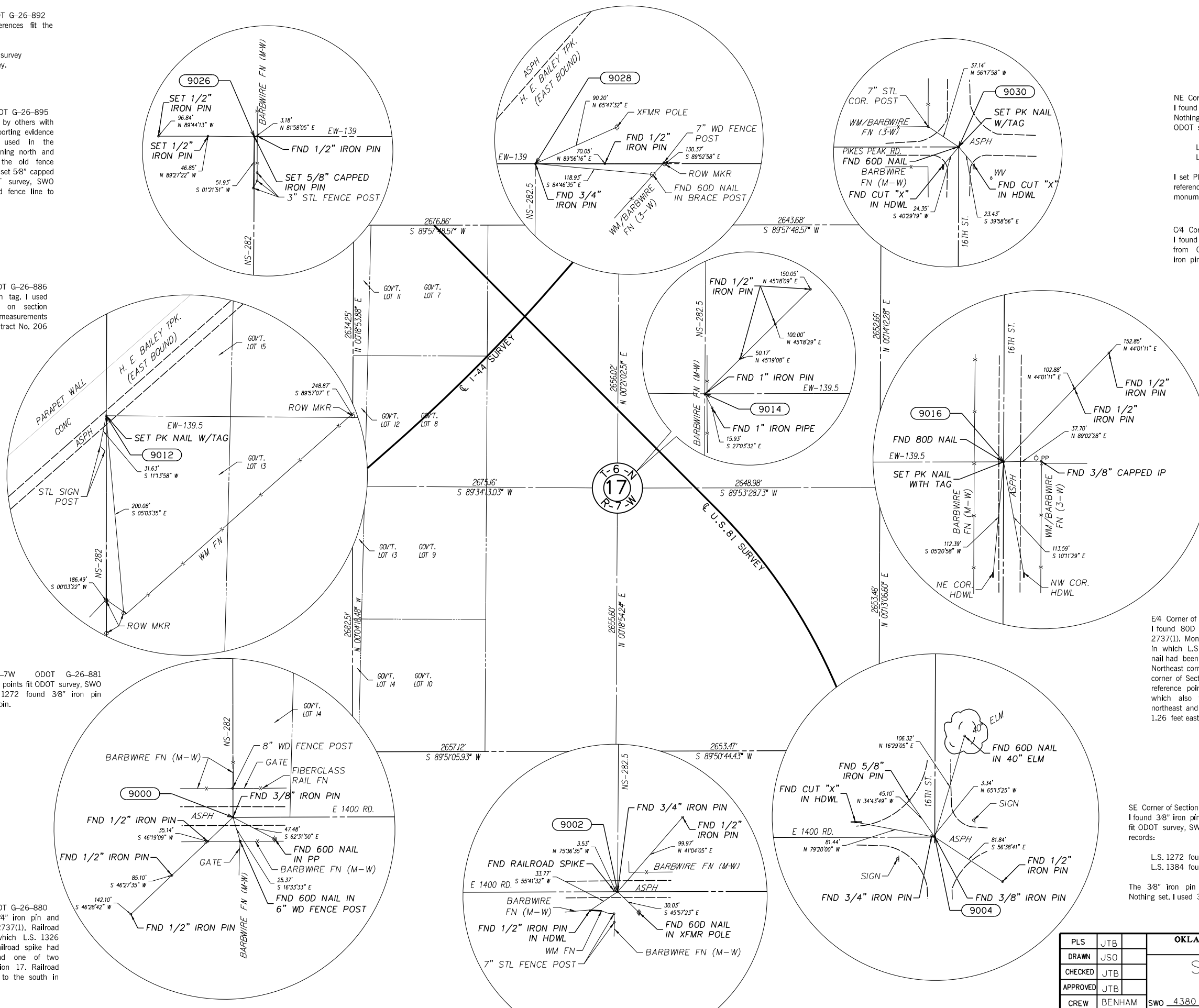
W/4 Corner of Section 17, T-6N, R-7W ODOT G-26-886
No corner evidence found. I set PK nail with tag. I used right of way markers, which were points on section lines and right of way lines. Also used measurements from Oklahoma Turnpike Authority Plans, Contract No. 206 to set monument.



Angle Of Variance
At Sta. 9020 (C/4 Cor.)
X = 1980082.3680
Y = 604063.9240
Lat. = 34°59'35.44004"
Long. = 97°57'40.78631"
θ = 0°01'19.02001"

SW Corner of Section 17, T-6N, R-7W ODOT G-26-881
I found 3/8" iron pin. Monument and reference points fit ODOT survey, SWO 2737(1) and corner record in which L.S. 1272 found 3/8" iron pin in 1998 survey. Nothing set. I used 3/8" iron pin.

S/4 Corner of Section 17, T-6N, R-7W ODOT G-26-880
I found 3/4" iron pin and railroad spike. 3/4" iron pin and reference points fit ODOT survey, SWO 2737(1). Railroad spike and reference fit corner record in which L.S. 1326 found railroad spike in 2012 survey. The railroad spike had been set using the southwest corner and one of two monuments at the southeast corner of Section 17. Railroad spike was set by others for new fence line to the south in 2011. Nothing set. I used 3/4" iron pin.



NE Corner of Section 17, T-6N, R-7W ODOT G-26-893
I found 60D nail. Removed 60D nail to look for iron pin. Nothing found below nail. Monument and references fit ODOT survey and the following C.C.R.'s:

L.S. 1272 found 3/8" iron pin in 1998 survey
L.S. 1384 found #3 rebar in 2008 survey.

I set PK nail with tag back in place of 60D nail. I used references from ODOT survey, SWO 2028(1), to check monument location.

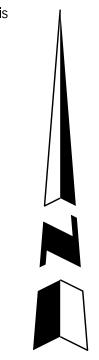
C/4 Corner of Section 17, T-6N, R-7W ODOT G-26-887
I found 1" iron pin. Found monument and reference points from ODOT survey, SWO 2737(1). Nothing set. I used 1" iron pin

E/4 Corner of Section 17, T-6N, R-7W ODOT G-26-888
I found 80D nail. Monument does not fit ODOT survey, SWO 2737(1). Monument and reference points fit the corner record in which L.S. 1384 set 80D nail in 2008 survey. The 80D nail had been set using single proportionate method using the Northeast corner and one of two monuments at the southeast corner of Section 17. I set PK nail with tag. I used existing reference points from said ODOT survey to set monument, which also fits single proportionate method between the northeast and southeast corners of Section 17. Monument is 1.26 feet east and 0.48 feet south of found 80D nail.

SE Corner of Section 17, T-6N, R-7W ODOT G-26-879
I found 3/8" iron pin and 3/4" iron pin. The 3/4" iron pin does not fit ODOT survey, SWO 2737(1), but it does fit the following corner records:

L.S. 1272 found 3/4" iron pin in 1998 survey
L.S. 1384 found #6 rebar in 2008 survey.

The 3/8" iron pin and reference points fit said ODOT survey. Nothing set. I used 3/8" iron pin.



SCALE:
1" = 50'

LAND CORNER DETAILS ARE SHOWN AT 1"=50'

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|--------|---------------------|------------------------------|-----------------------|--|
| SURVEY DIVISION | | | | | |
| SURVEY DATA SHEET | | | | | |
| | | | SDS <u>63</u> OF <u>76</u> | | |
| PLS | JTB | | | | |
| DRAWN | JSO | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BENHAM | SWO <u>4380</u> (1) | PROJECT NO. <u>24428(04)</u> | SHEET NO. <u>S063</u> | |

N4 Corner of Section 16, T-6N, R-7W ODOT G-26-892
I found 1/2" iron pin. Monument and references fit the following C.C.R.'s:

L.S. 1326 found 1/2" iron pin in 2005 survey
L.S. 189 found #3 rebar in 2009 survey.

Nothing set. I used 1/2" iron pin.

NW Corner of Section 16, T-6N, R-7W ODOT G-26-893
I found 60D nail. Removed 60D nail to look for iron pin.
Nothing found below nail. Monument and references fit ODOT survey and the following C.C.R.'s:

L.S. 1272 found 3/8" iron pin in 1998 survey
L.S. 1384 found #3 rebar in 2008 survey.

I set PK nail with tag back in place of 60D nail. I used references from ODOT survey, SWO 2028(1), to check monument location.

Angle Of Variance
At Sta. 9022 (C/4 Cor.)
X = 1985365.6870
Y = 604077.6000
Lot. = 34°59'35.55072"
Long. = 97°56'37.28364"
θ = 0°01'55.06519"

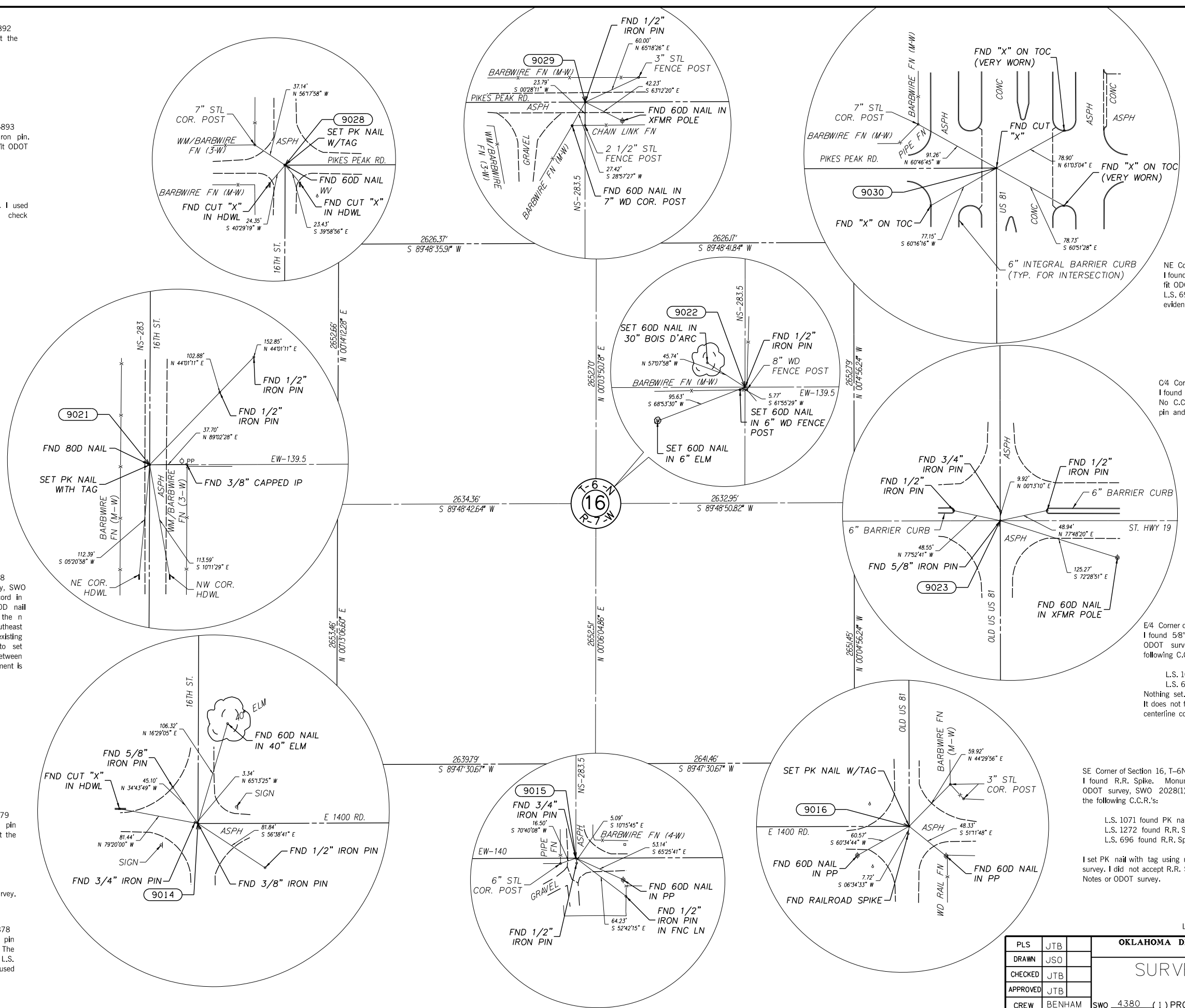
W4 Corner of Section 16, T-6N, R-7W ODOT G-26-888
I found 80D nail. Monument does not fit ODOT survey, SWO 2737(1). Monument and reference points fit the corner record in which L.S. 1384 set 80D nail in 2008 survey. The 80D nail had been set using single proportionate method using the northeast corner and one of two monuments at the southeast corner of Section 17. I set PK nail with tag. I used existing reference points from ODOT survey, SWO 2737(1), to set monument, which also fits single proportionate method between the northeast and southeast corners of Section 17. Monument is 1.26 feet east and 0.48 feet south of found 80D nail.

SW Corner of Section 16, T-6N, R-7W ODOT G-26-879
I found 3/8" iron pin and 3/4" iron pin. The 3/4" iron pin does not fit ODOT survey, SWO 2737(1), but it does fit the following corner records:

L.S. 1272 found 3/4" iron pin in 1998 survey
L.S. 1384 found #6 rebar in 2008 survey.

The 3/8" iron pin and reference points fit said ODOT survey. Nothing set. I used 3/8" iron pin.

S4 Corner of Section 16, T-6N, R-7W ODOT G-26-878
I found 3/4" iron pin and 1/2" iron pin. The 3/4" iron pin and reference point fit ODOT survey, SWO 2737(1). The 1/2" iron pin and reference points fit C.C.R. in which L.S. found 3/8" iron pin in 1997 survey. Nothing set. I used 3/4" iron pin.



| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | | DATE |
| | | | | | |

NE Corner of Section 16, T-6N, R-7W ODOT G-26-891
I found cut "X" in concrete. Monument and references fit ODOT survey, SWO 2028(1), and the C.C.R. in which L.S. 696 found cut "X" in 1999 survey. No corner evidence set. I used cut "X".

C4 Corner of Section 16, T-6N, R-7W ODOT G-26-889
I found 1/2" iron pin. Monument fits ODOT survey 2737 (1). No C.C.R.'s found for corner. Nothing set. I used 1/2" iron pin and set references.

E/4 Corner of Section 16, T-6N, R-7W ODOT G-26-890
I found 5/8" iron pin and 3/4" iron pin. The 5/8" iron pin fits ODOT survey, SWO 2028(1). The 3/4" iron pin fits the following C.C.R.'s:

L.S. 1071 found 5/8" iron pin in 1989 survey
L.S. 696 found R.R. spike in 1999 survey.
Nothing set. I used 5/8" iron pin. I did not accept 3/4" iron pin. It does not fit with G.L.O. notes or ODOT survey. It is possibly a centerline control point for S.H. 19.

SE Corner of Section 16, T-6N, R-7W ODOT G-26-877
I found R.R. Spike. Monument and references do not fit ODOT survey, SWO 2028(1). Monument and references fit the following C.C.R.'s:

L.S. 1071 found PK nail in 1989 survey
L.S. 1272 found R.R. Spike in 1997 survey
L.S. 696 found R.R. Spike in 1999 survey.

I set PK nail with tag using measurements from said ODOT survey. I did not accept R.R. Spike, since it did not fit G.L.O. Notes or ODOT survey.

SCALE:
1" = 50'

LAND CORNER DETAILS ARE SHOWN AT 1"=50'

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|--------|--------------|-----------------------|----------------|--|
| SURVEY DIVISION | | | | | |
| SURVEY DATA SHEET | | | | | |
| | | | SDS 64 OF 76 | | |
| PLS | JTB | | | | |
| DRAWN | JSO | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BENHAM | SWO 4380 (1) | PROJECT NO. 24428(04) | SHEET NO. S064 | |

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |
| | | | | | |

NW Corner of Section 7, T-6N, R-7W ODOT G-26-907
I found 1/2" iron pin, railroad spike, and 1" iron pin. 1/2" iron pin and found reference points fit corner record in which L.S. 1272 found 1/2" iron pin in 2001 survey with no supporting evidence listed. No corner record was found for railroad spike. 1" iron pin fits measurements from ODOT survey, SWO 2737(1). Nothing set. I used 1" iron pin.

N4 Corner of Section 7, T-6N, R-7W ODOT G-26-908
I found 1/2" iron pin. Corner location and found reference points fit ODOT survey, SWO 2737(1), and corner record in which L.S. 1272 found 1/2" iron pin in 2001 survey. Nothing set. I used 1" iron pin.

Angle Of Variance
At Sta. 9032 (C/4 Cor.)
X = 1974774.8469
Y = 609365.4703
Lat. = 35°00'27.89363"
Long. = 97°58'44.56656"
θ = 0°00'42.81728"

W4 Corner of Section 7, T-6N, R-7W ODOT G-26-898
No corner evidence found. I set PK nail with tag. I used single proportion method to set the corner between the northwest and southwest corner of Section 7.

SW Corner of Section 7, T-6N, R-7W ODOT G-26-897
I found 3/4" iron pin. Corner location and found reference points match the following corner records:

- L.S. 449 found iron pin (CCR filed June 8, 1982)
- L.S. 1082 found 3/4" iron pin (CCR filed APR 18, 1994)
- L.S. 1272 found 3/8" iron pin (CCR filed Dec 7, 1998)
- L.S. 1272 found iron pin (CCR filed Aug 31, 2001).

Nothing set. I used 3/4" iron pin.

S4 Corner of Section 7, T-6N, R-7W ODOT G-26-896
I found original stone. No certified corner records found, although monument is referenced on C.C.R.'s filed on the southwest and N4 corner of Section 7. No original reference points were found. Stone was marked, "14" on north face, and looked undisturbed. Nothing set. I used original stone and set reference points.

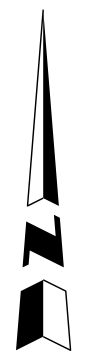
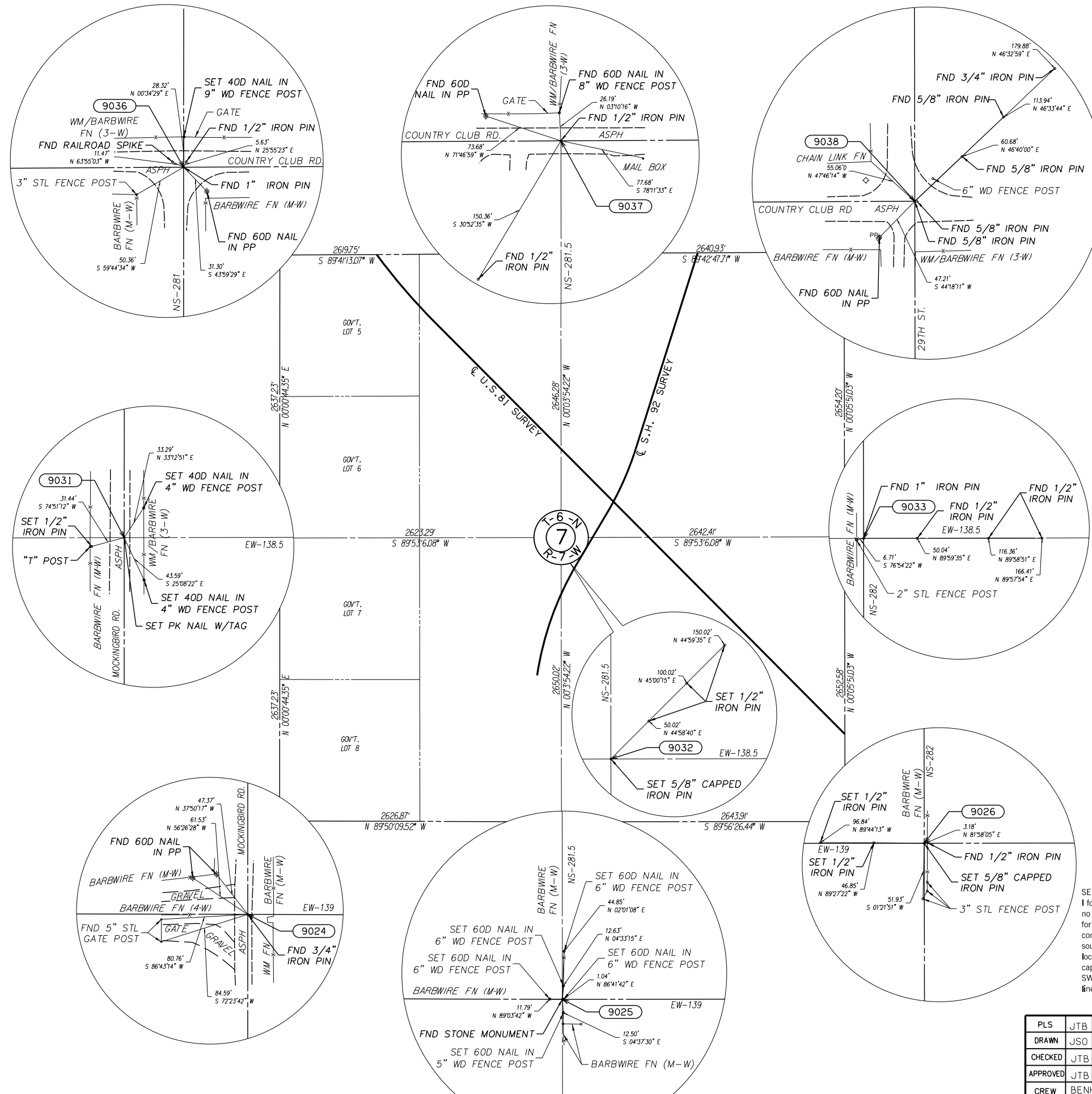
NE Corner of Section 7, T-6N, R-7W ODOT G-26-905
I found 5/8" iron pin. Corner location and found reference points fit ODOT survey, SWO 2737(1). Also found 5/8" iron pin 2 feet east of corner, which is a P.O.T. point for Country Club Road Alignment from same survey. Corner location and found reference points do not fit the following corner records, in which supporting evidence was not listed:

- L.S. 449 found 1/2" iron pin in 1981 survey
- L.S. 1272 found R.R. spike in 1998 survey.

C4 Corner of Section 7, T-6N, R-7W ODOT G-26-899
No corner evidence found. No corner records found. I set 5/8" iron pin. I used the intersection of lines between opposite quarter section corners to set center of section.

E4 Corner of Section 7, T-6N, R-7W ODOT G-26-900
I found 1" iron pin. Monument and found reference points match ODOT survey, SWO 2737(1). Nothing set. I used 1" iron pin

SE Corner of Section 7, T-6N, R-7W ODOT G-26-895
I found 1/2" iron pin. This monument was set by others with no corner record filed. I did not find any supporting evidence for this corner location. This corner was used in the construction of a new barbed-wire fence running north and south. New fence location deviates from the old fence location by approximately 4 feet to the east. I set 5/8" capped iron pin. I used measurements from ODOT survey, SWO 2737(1), to set corner. Corner location fits old fence line to the south



SCALE:
1" = 500'

LAND CORNER DETAILS ARE SHOWN AT 1"=50'

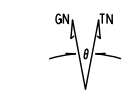
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | |
|---------------------------------------|--------|--------------|--------------------------------------|
| SURVEY DIVISION | | | |
| SURVEY DATA SHEET | | | |
| | | SDS 65 OF 76 | |
| PLS | JTB | | |
| DRAWN | JSO | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BENHAM | SWO 4380 (1) | PROJECT NO. 24428(04) SHEET NO. S065 |

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |

NW Corner of Section 8, T-6N, R-7W ODOT G-26-905
I found 5/8" iron pin. Corner location and found reference points fit ODOT survey, SWO 2737(1). Also found 5/8" iron pin 2 feet east of corner which is a P.O.T. point for Country Club Road Alignment from same survey. Corner location and found reference points do not fit the following corner records, in which supporting evidence was not listed:

- L.S. 449 found 1/2" iron pin (CCR filed Apr 23, 1981)
- L.S. 1272 found railroad spike (CCR filed June 26, 1998).

N4 Corner of Section 8, T-6N, R-7W ODOT G-26-904
I found 5/8" iron pin. Corner and found reference points fit ODOT survey, SWO 2737(1). No previous corner records found. Nothing set. I used 5/8" iron pin.

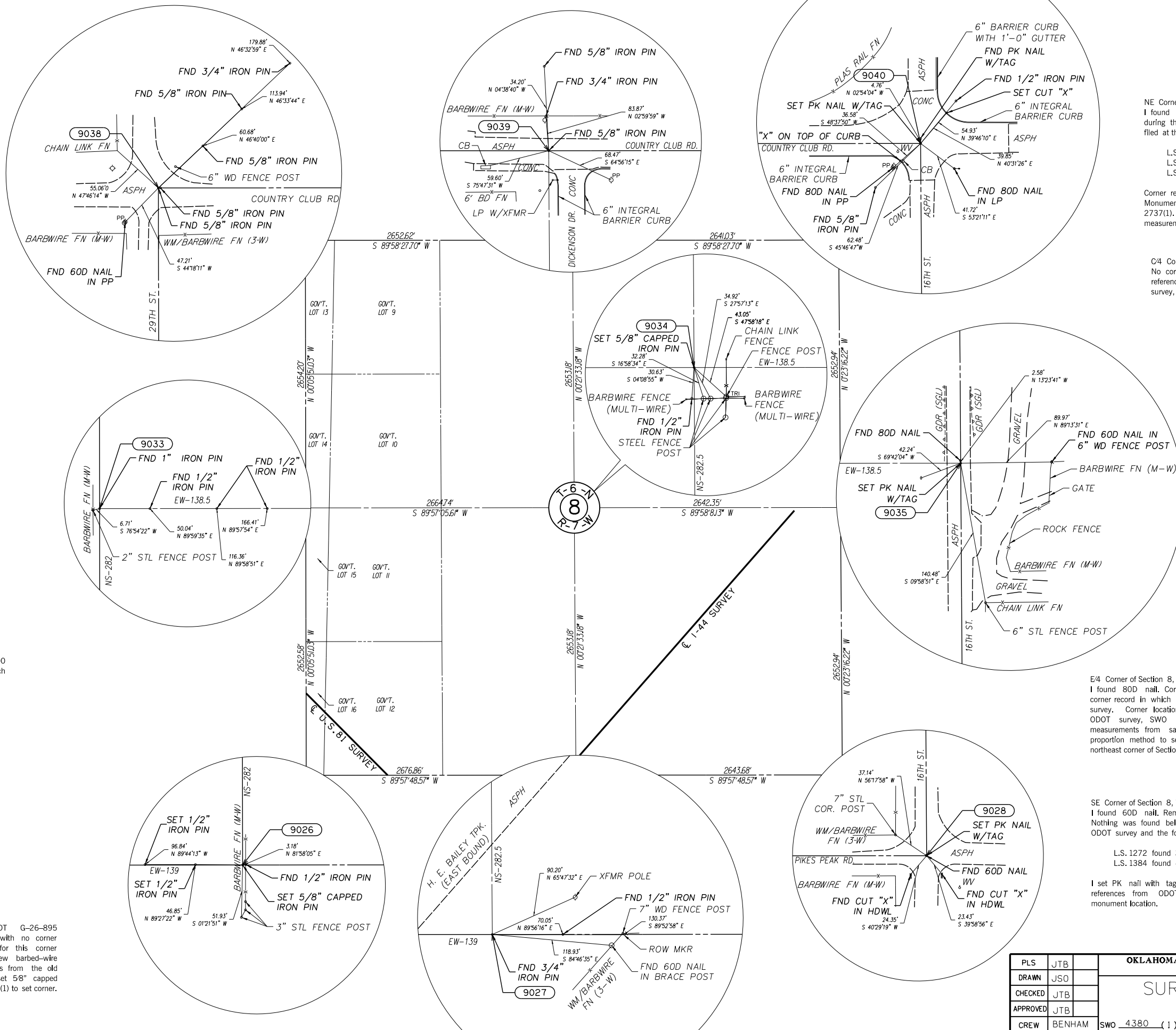


Angle Of Variance
At Sta. 9034 (C/4 Cor.)
X = 1980081.9910
Y = 609373.0260
Lat. = 35°00'27.95285"
Long. = 97°57'40.76639"
θ = 0°01'19.03132"

W4 Corner of Section 8, T-6N, R-7W ODOT G-26-900
I found 1" iron pin. Monument and found reference points match ODOT survey, SWO 2737(1). Nothing set. I used 1" iron pin.

S4 Corner of Section 8, T-6N, R-7W ODOT G-26-894
I found 3/4" iron pin. Corner location and reference points fits ODOT survey, SWO 2737(1), and corner record in which L.S. 1706 found 3/4" iron pin in 2011 survey. Nothing set. I used 3/4" iron pin.

SW Corner of Section 8, T-6N, R-7W ODOT G-26-895
I found 1/2" iron pin. This monument was set by others with no corner record filed. I did not find any supporting evidence for this corner location. This corner was used in the construction of a new barbed-wire fence running north and south. New fence location deviates from the old fence location by approximately 4 feet to the east. I set 5/8" capped iron pin. I used measurements from ODOT survey, SWO 2737(1) to set corner. Corner location fits old fence line to the south



NE Corner of Section 8, T-6N, R-7W ODOT G-26-903
I found PK nail with tag (C.A. 3949). Monument was set during the course of this survey. No corner record had been filed at the time. Monument fit corner records listed below:

- L.S. 449 found iron pin (CCR filed Aug 15, 1984)
- L.S. 1272 found PK nail in 1998 survey
- L.S. 1407 found PK nail in 2002 survey.

Corner records did not have any supporting evidence listed. Monument and corner records did not fit ODOT survey, SWO 2737(1). I set PK nail with tag. I used reference points and measurements from said ODOT survey to set corner.

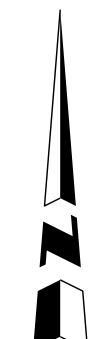
C4 Corner of Section 8, T-6N, R-7W ODOT G-26-901
No corner evidence found. I set 5/8" capped iron pin and references as shown. I used measurements from ODOT survey, SWO 2737(1), to set center of section.

E4 Corner of Section 8, T-6N, R-7W ODOT G-26-902
I found 80D nail. Corner and found references matched corner record in which L.S. 1326 found 80D nail in 2005 survey. Corner location does not fit measurements from ODOT survey, SWO 2737(1). I set PK nail. I used measurements from said ODOT survey and used single proportion method to set corner between the southeast and northeast corner of Section 8.

SE Corner of Section 8, T-6N, R-7W ODOT G-26-893
I found 60D nail. Removed 60D nail to look for iron pin. Nothing was found below nail. Monument and references fit ODOT survey and the following CCR's:

- L.S. 1272 found 3/8" iron pin in 1998 survey
- L.S. 1384 found #3 rebar in 2008 survey.

I set PK nail with tag back in place of 60D nail. I used references from ODOT survey, SWO 2028(1), to check monument location.



SCALE:
1" = 500'

LAND CORNER DETAILS ARE SHOWN AT 1"=50'

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | |
|---------------------------------------|--------|--|--|
| SURVEY DIVISION | | | |
| SURVEY DATA SHEET | | | |
| | | SDS <u>66</u> OF <u>76</u> | |
| PLS | JTB | | |
| DRAWN | JSO | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BENHAM | SWO <u>4380</u> (1) PROJECT NO. <u>24428(04)</u> SHEET NO. <u>S066</u> | |

N4 Corner of Section 6, T-6N, R-7W ODOT G-26-912
 I found 3/4" iron pin. Monument and reference points fit ODOT survey, SWO 2737(1), and corner record in which L.S. 1200 found 5/8" iron pin in 2012 survey. Nothing set. I used 3/4" iron pin.

NW Corner of Section 6, T-6N, R-7W ODOT G-26-913
 I found 5/8" iron pin. Corner location and found reference points fit ODOT survey, SWO 2737(1), and corner record in which L.S. 1378 found 5/8" iron pin in 1995 survey. Nothing set. I used 5/8" iron pin.

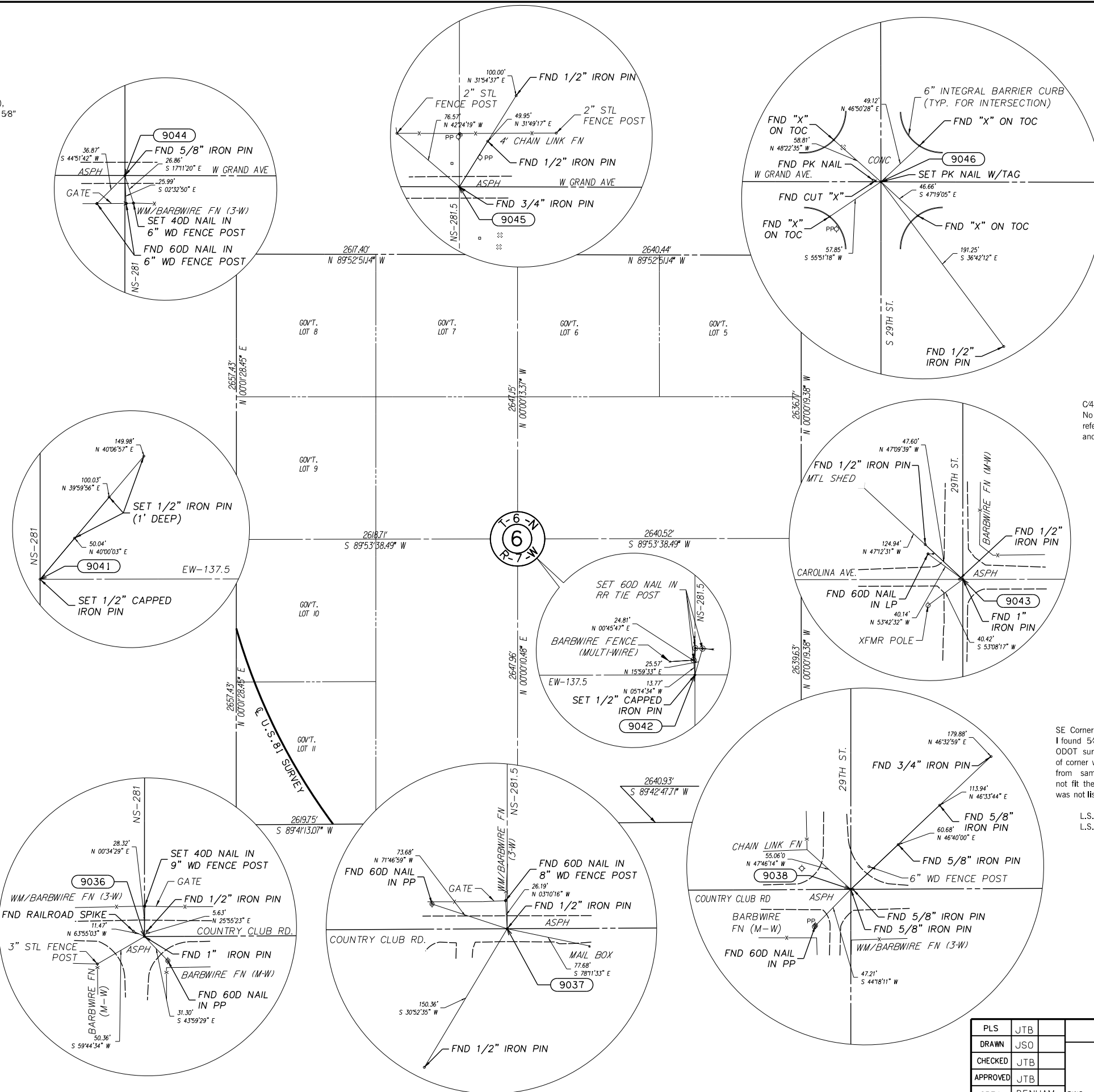


Angle Of Variance
 At Sta. 9042 (C/4 Cor.)
 X = 1974771.9766
 Y = 614659.7111
 Lat. = 35°01'20.25923"
 Long. = 97°58'44.58786"
 θ = 0°00'42.80519"

W4 Corner of Section 6, T-6N, R-7W ODOT G-26-908
 No corner evidence found. I set 5/8" iron pin and references. I set corner using single proportionate method.

SW Corner of Section 6, T-6N, R-7W ODOT G-26-907
 I found 1/2" iron pin, railroad spike, and 1" iron pin. 1/2" iron pin and found reference points fit corner record in which L.S. 1272 found 1/2" iron pin in 2001 survey, with no supporting evidence listed. No corner record found for railroad spike. 1" iron pin fits measurements from ODOT survey, SWO 2737(1). Nothing set. I used 1" iron pin.

S4 Corner of Section 6, T-6N, R-7W ODOT G-26-908
 I found 1/2" iron pin. Corner location and found reference points fit ODOT survey, SWO 2737(1), and corner record in which L.S. 1272 found 1/2" iron pin in 2001 survey. Nothing set. I used 1" iron pin.



NE Corner of Section 6, T-6N, R-7W ODOT G-26-911
 I found a chiseled "X" and nail. Monuments and reference points do not fit ODOT survey, SWO 2737(1), or the following corner records. L.S. 955 found 1" iron pin in 1985 survey. L.S. 1272 found an iron pin in 1988 survey. Monuments and reference points fit the following corner records. L.S. 1272 found a chiseled "X" in 1998 survey, with no supporting evidence listed. L.S. 1200 found a nail at an "X" in intersection in 2012 survey. I set a PK nail with tag. I used found reference points and measurements from said ODOT survey to set monument. I did not find supporting evidence for the chiseled "X", although it was used for subdivision surveys to the Southwest. I did not find supporting evidence for the nail that was found at the intersection of two paving joints, which was placed there by others during the course of this survey.

C4 Corner of Section 6, T-6N, R-7W ODOT G-26-909
 No corner evidence found. I set 1/2" capped iron pin and references. I used the intersection of opposite quarter corners, and measurements from ODOT Survey, SWO 2737(1).

E4 Corner of Section 6, T-6N, R-7W ODOT G-26-910
 I found 1/2" iron pin & 1" iron pin. 1/2" iron pin and found reference points fit the following corner records. L.S. 449 found 1/2" rebar in 1981 survey. L.S. 1272 found iron pin in 1998 survey (C.C.R. filed June 26, 1998). These corner records did not have any supporting evidence listed. 1" iron pin and found reference points fit measurements from ODOT survey, SWO 2737(1). Nothing set. I used 1" iron pin.

SE Corner of Section 6, T-6N, R-7W ODOT G-26-905
 I found 5/8" iron pin. Corner location and found reference points fit ODOT survey, SWO 2737(1). Also found 5/8" iron pin 2 feet east of corner which is a P.O.T. point for Country Club Road alignment from same survey. Corner location and found reference points do not fit the following corner records, in which supporting evidence was not listed:

L.S. 449 found 1/2" iron pin (C.C.R. filed Apr 23, 1981)
 L.S. 1272 found railroad spike (C.C.R. filed June 26, 1998).



SCALE:
 1" = 500'

LAND CORNER DETAILS ARE SHOWN AT 1"=50'

| | | |
|--|--------|--|
| PLS | JTB | OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET SDS <u>67</u> OF <u>76</u> |
| DRAWN | JSO | |
| CHECKED | JTB | |
| APPROVED | JTB | |
| CREW | BENHAM | |
| SWO <u>4380</u> (1) PROJECT NO. <u>24428(04)</u> SHEET NO. <u>S067</u> | | |

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |
| | | | | | |

N/4 Corner of Section 31, T-7N, R-7W ODOT G-26-919
I found 3/4" iron pin. Corner location and found references fit ODOT survey, SWO 2737(1) and the following corner records: L.S. 1281 found 3/4" rebar in 1994 survey and L.S. 1378 found R.R. Spike in 1996 survey. Nothing set. I used 3/4" iron pin.

NW Corner of Section 31, T-7N, R-7W ODOT G-26-920
I found 3/8" iron pin. Monument and reference points fit ODOT survey, SWO 2737(1), and the following corner records:

L.S. 1378 found 1" iron pin in 1996 survey
L.S. 1272 found iron pin in 1998 survey
L.S. 1200 found 3/8" iron pin in 2012 survey.

Nothing set. I used 3/8" iron pin



Angle Of Variance
At Sta. 9048 (C/4 Cor.)
X = 1974768.9904
Y = 619945.6544
Lot. = 35°02'12.54254"
Long. = 97°58'44.61058"
θ = 0°00'42.79229"

W/4 Corner of Section 31, T-7N, R-7W ODOT G-26-914
I found 3/8" iron pin. Monument and references fit corner record in which L.S. 1200 set 3/8" iron pin in 2012 survey. Nothing set. I used 3/8" iron pin.

S/4 Corner of Section 31, T-7N, R-7W ODOT G-26-912
I found 3/4" iron pin. Monument and reference points fit ODOT survey, SWO 2737(1), and corner record in which L.S. 1200 found 5/8" iron pin in 2012 survey. Nothing set. I used 3/4" iron pin.

SW Corner of Section 31, T-7N, R-7W ODOT G-26-913
I found 5/8" iron pin. Corner location and found reference points fit ODOT survey, SWO 2737(1), and corner record in which L.S. 1378 found 5/8" iron pin in 1995 survey. Nothing set. I used 5/8" iron pin.

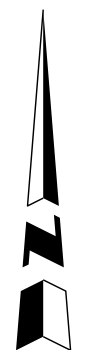
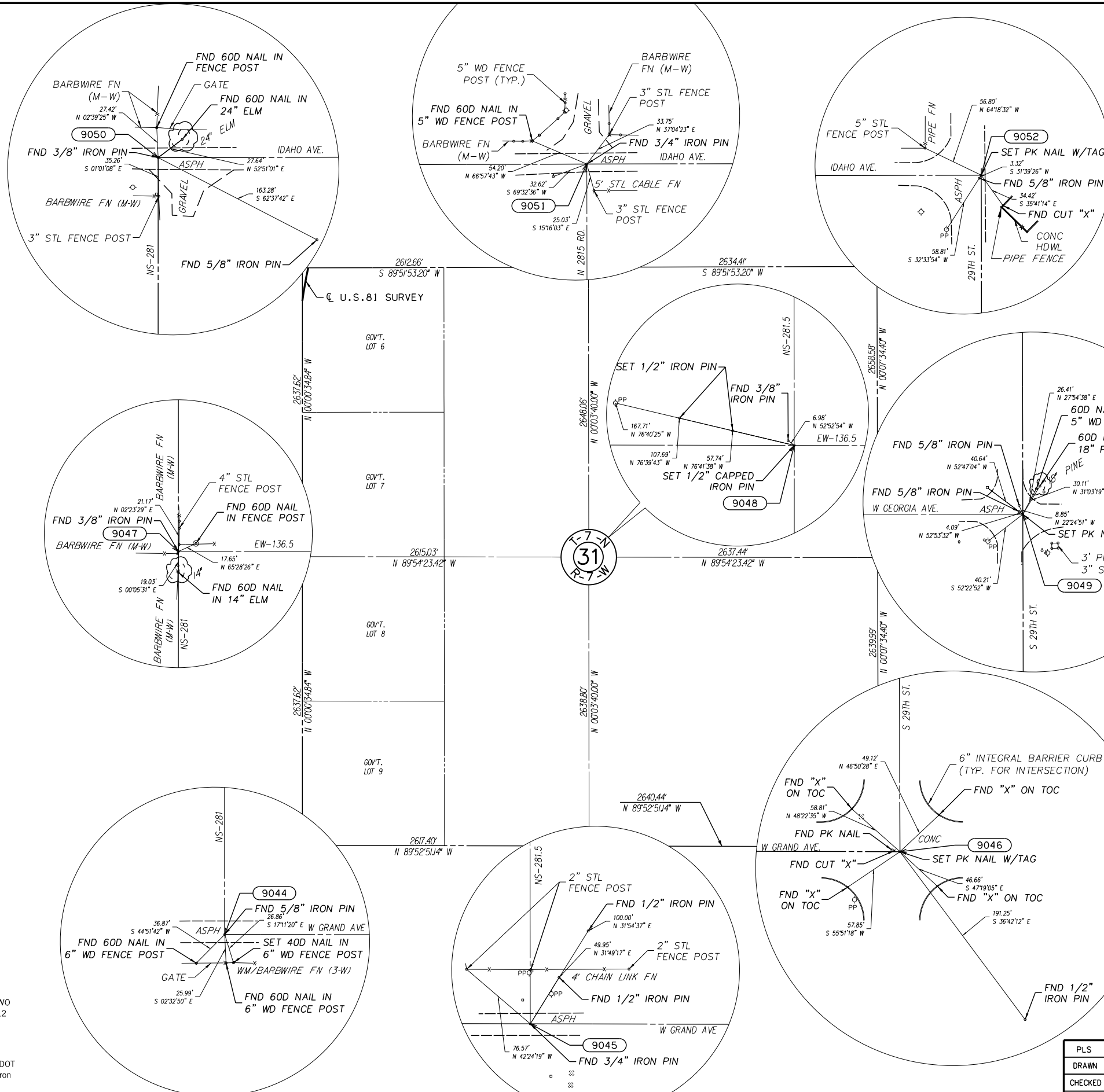
NE Corner of Section 31, T-7N, R-7W ODOT G-26-918
I found 5/8" iron pin. This monument and found references do not fit ODOT survey, SWO 2737(1), or corner record in which L.S. 1272 found iron pin in 1988 survey. Monument and reference points fit corner record in which L.S. 1200 found 5/8" iron pin in 2012 survey. I set PK nail with tag. I used measurements from said ODOT survey to set corner.

C/4 Corner of Section 31, T-7N, R-7W ODOT G-26-915
I found 3/8" iron pin. No corner record or supporting evidence found for this monument. This iron pin appeared to be set from one of the two iron pins found at the E/4 corner of Section 31. Supporting evidence was not found for the iron pins at the said E/4 corner, and their locations did not fit measurements from ODOT survey, SWO 2737(1). I set 1/2" capped iron pin. I used the intersection of opposite 1/4 section corners to set center of section. The E-W 1/4 section line also fits 1/16 corners and P.O.T. iron pins (on Georgia Ave.) Found from said ODOT survey.

E/4 Corner of Section 31, T-7N, R-7W ODOT G-26-916
I found two 5/8" iron pins. One monument was set by others, and one was found by L.S. 449 in 1984 survey (no supporting evidence listed). These monuments do not fit ODOT survey, SWO 2737(1). I set PK nail with tag. I used measurements from said ODOT survey to set corner.

SE Corner of Section 31, T-7N, R-7W ODOT G-26-911
I found chiseled "X" and nail. Monuments and reference points do not fit ODOT survey, SWO 2737(1), or the following corner records. L.S. 955 found 1" iron pin in 1985 survey. L.S. 1272 found an iron pin in 1988 survey. Monuments and reference points fit the following corner records. L.S. 1272 found chiseled "X" in 1998 survey, with no supporting evidence listed. L.S. 1200 found nail at "X" in intersection in 2012 survey. I set PK nail with tag. I used found reference points and measurements from said ODOT survey to set monument. I did not find supporting evidence for the chiseled "X," although it was used for subdivision surveys to the Southwest. I did not find supporting evidence for the nail that was found at the intersection of two paving joints, which was placed there by others during the course of this survey.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |



SCALE:
1" = 500'

LAND CORNER DETAILS ARE SHOWN AT 1"=50'

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|--------|--------------|-----------------------|----------------|--|
| SURVEY DIVISION | | | | | |
| SURVEY DATA SHEET | | | | | |
| SDS 68 OF 76 | | | | | |
| PLS | JTB | | | | |
| DRAWN | JSO | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BENHAM | SWO 4380 (1) | PROJECT NO. 24428(04) | SHEET NO. S068 | |

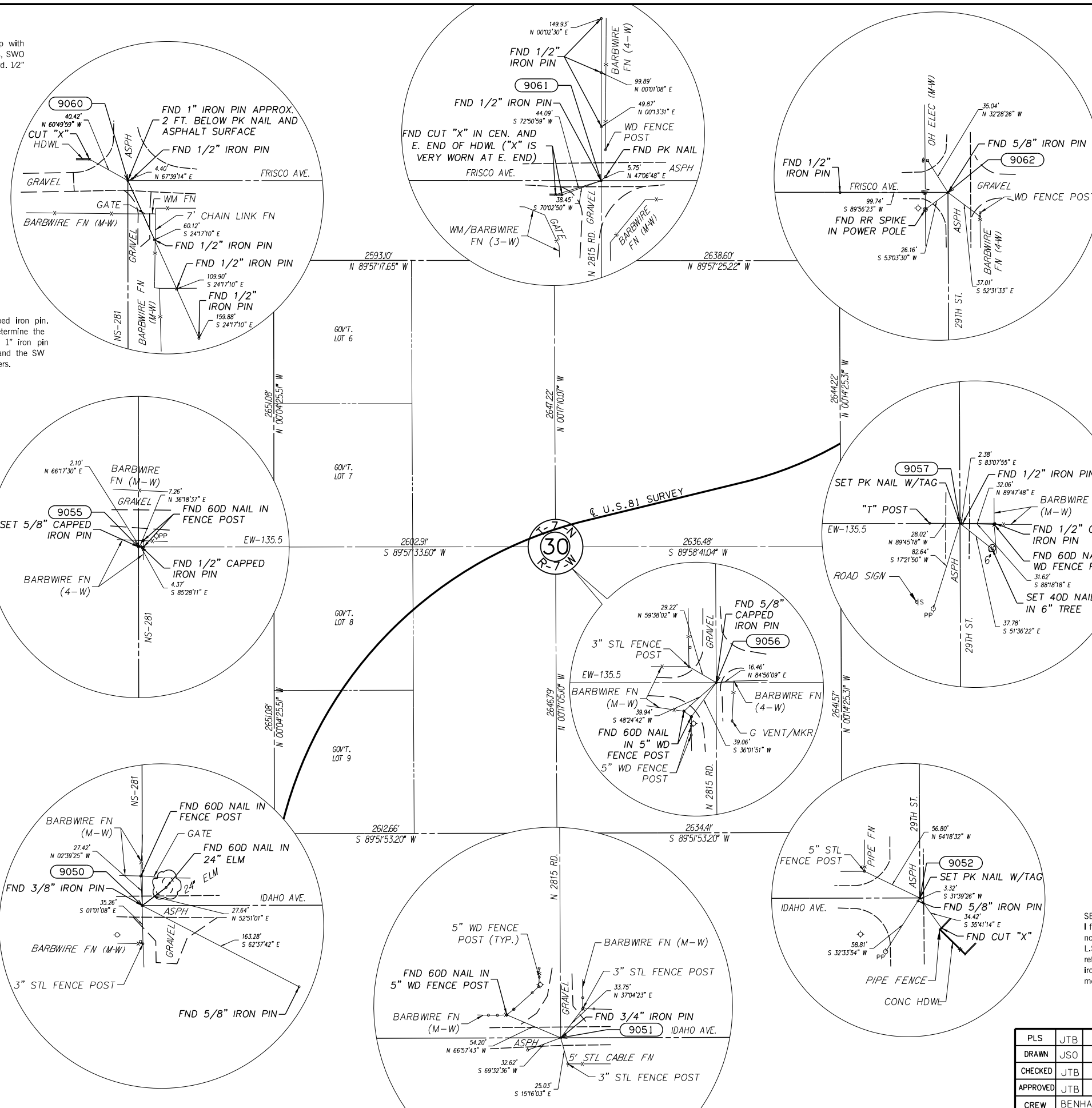
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |
| | | | | | |

N4 Corner of Section 30, T-7N, R-7W ODOT G-26-445
I found PK nail & 1/2" iron pin no corner record was found for PK nail (lines up with property fence to the north). 1/2" iron pin fits measurements from ODOT surveys, SWO 2737(1) and SWO 3609(1), and reference points from the following corner record. 1/2" iron pin set by L.S. 1155 in 1991 survey. Nothing set. I used 1/2" iron pin.

NW Corner of Section 30, T-7N, R-7W ODOT G-26-444
I found 1" iron pin, PK nail, & 1/2" iron pin. 1/2" iron pin was found by L.S. 189 in 1984 survey, with no supporting evidence listed. No corner record was filed for PK nail, which I removed to find 1" iron pin two feet below asphalt surface. 1" iron pin and found references fit measurements from ODOT survey, SWO 3609(1), and corner record which L.S. 1155 found 1" iron pin in 1991 survey. Nothing set. I used 1" iron pin.

W4 Corner of Section 30, T-7N, R-7W ODOT G-26-921
I found 1/2" capped iron pin set by L.S. 1326 in 2005 survey. I set 5/8" capped iron pin. L.S. 1326 used a 1/2" iron pin, located at the NW corner of Section 30 to determine the W4 corner. I did not find supporting evidence for that 1/2" iron pin. I used a 1" iron pin found at the NW corner of Section 30, which fits ODOT survey, SWO 3609(1), and the SW corner. I used single proportion method to set point on line and between said corners.

Angle Of Variance
At Sta. 9056 (C/4 Cor.)
X = 1974753.0120
Y = 625240.4660
Lat. = 35°03'04.91337"
Long. = 97°58'44.78955"
θ = 0°00'42.69070"



NE Corner of Section 30, T-7N, R-7W ODOT G-26-450
I found 5/8" iron pin. Monument fits measurements from ODOT surveys, SWO 2737(1) and SWO 3609(1). It also fits found reference points and measurements from the following corner records: 1/2" iron pin was recovered by L.S. 449 in 1979 survey, 1" iron pin was recovered by L.S. 1155 in 1991 survey, and 1" iron pin was recovered by L.S. 696 in 1997 survey. Nothing set. I used 5/8" iron pin.

C4 Corner of Section 30, T-7N, R-7W ODOT G-26-922
I found 5/8" capped iron pin. Corner location and reference points fit 1/2" capped iron pin set by L.S. 1326 in 2000 and 2001 survey. Nothing set. I used 5/8" capped iron pin.

E4 Corner of Section 30, T-7N, R-7W ODOT G-26-923
I found 1/2" iron pin. Monument and reference points do not fit measurements from ODOT survey, SWO 2737(1), or corner record in which L.S. 1089 set PK nail in 1994 survey. I set PK nail with tag. I used measurements from said ODOT survey to set corner. Corner location fits existing fence lines and reference points from corner record by L.S. 1089.

SE Corner of Section 30, T-7N, R-7W ODOT G-26-918
I found 5/8" iron pin. This monument and found references does not fit ODOT survey, SWO 2737(1), or corner record in which L.S. 1272 found iron pin in 1988 survey. Monument and reference points fit corner record in which L.S. 1200 found 5/8" iron pin in 2012 survey. I set PK nail with tag. I used measurements from said ODOT survey to set corner.

SW Corner of Section 30, T-7N, R-7W ODOT G-26-920
I found 3/8" iron pin. Monument and reference points fit ODOT survey, SWO 2737(1), and the following corner records. L.S. 1378 found 1" iron pin in 1996 survey. L.S. 1272 found iron pin in 1998 survey. L.S. 1200 found 3/8" iron pin in 2012 survey. Nothing set. I used 3/8" iron pin.

S4 Corner of Section 30, T-7N, R-7W ODOT G-26-919
I found 3/4" iron pin. Corner location and found references fit ODOT survey, SWO 2737(1), and the following corner records. L.S. 1281 found 3/4" rebar in 1994 survey. L.S. 1378 found R.R. Spike in 1996 survey. Nothing set. I used 3/4" iron pin.



SCALE:
1" = 500'

LAND CORNER DETAILS ARE SHOWN AT 1"=50'

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | |
|---------------------------------------|--------|--------------|--------------------------------------|
| SURVEY DIVISION | | | |
| SURVEY DATA SHEET | | | |
| | | SDS 69 OF 76 | |
| PLS | JTB | | |
| DRAWN | JSD | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BENHAM | SWO 4380 (1) | PROJECT NO. 24428(04) SHEET NO. S069 |

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |
| | | | | | |

NW Corner of Section 29, T-7N, R-7W ODOT G-26-450
I found 5/8" iron pin. Monument fits measurements from ODOT survey, SWO 2737(1) and SWO 3609(1), and fits found reference points and measurements from the following corner records:

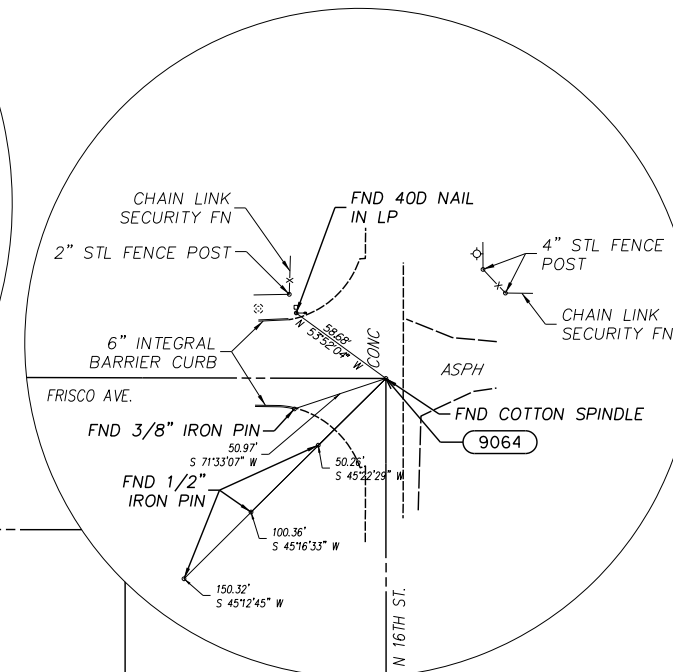
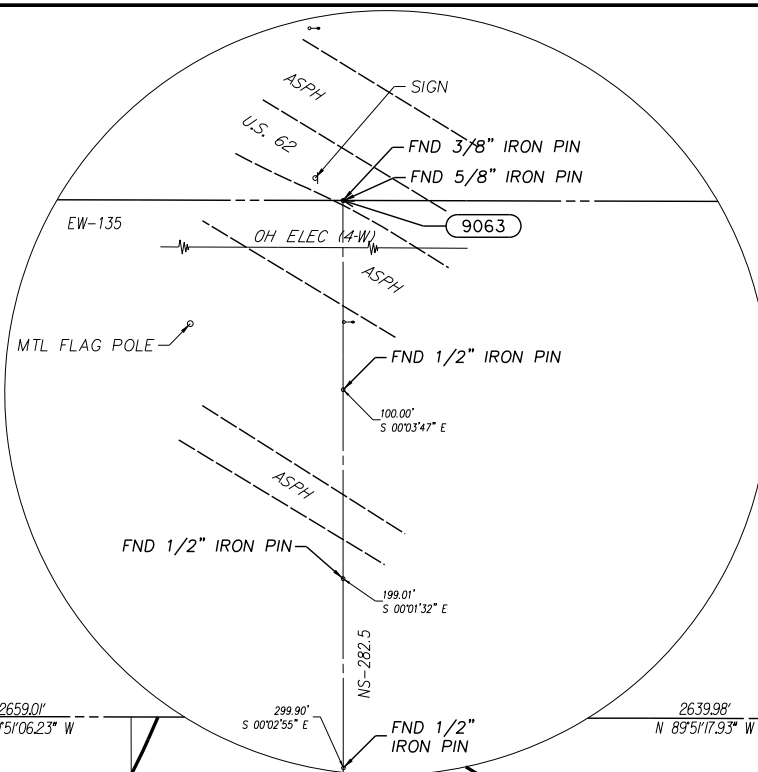
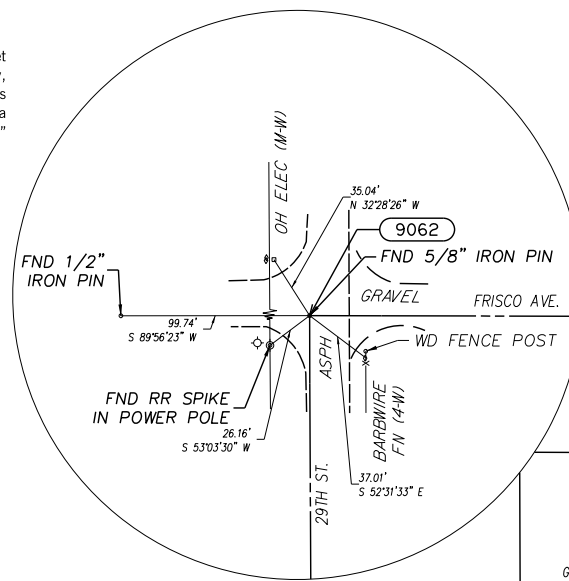
- 1/2" iron pin was recovered by L.S. 449 in 1979 survey
- 1" iron pin was recovered by L.S. 1155 in 1991 survey
- 1" iron pin was recovered by L.S. 696 in 1997 survey.

Nothing set. I used 5/8" iron pin.

N4 Corner of Section 29, T-7N, R-7W ODOT G-26-451
I found 5/8" iron pin and 3/8" iron pin. Monuments are only 0.2 feet apart. The 3/8" monument fits measurements from ODOT survey, SWO 2737(1) and SWO 3609(1), and fits found reference points and measurements from the following corner record: L.S. 1155 set a 5/8" capped iron pin in 1991 ODOT survey. Nothing set. I used 3/8" iron pin.



Angle Of Variance
At Sta. 9058 (C/4 Cor.)
X: 1980038.4430
Y: 625234.9380
Lat.: 35°03'04.84330"
Long.: 97°57'41.21708"
θ = 0°01'18.77550"

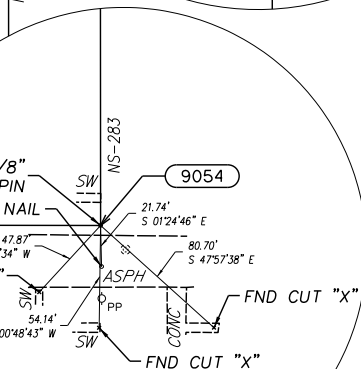
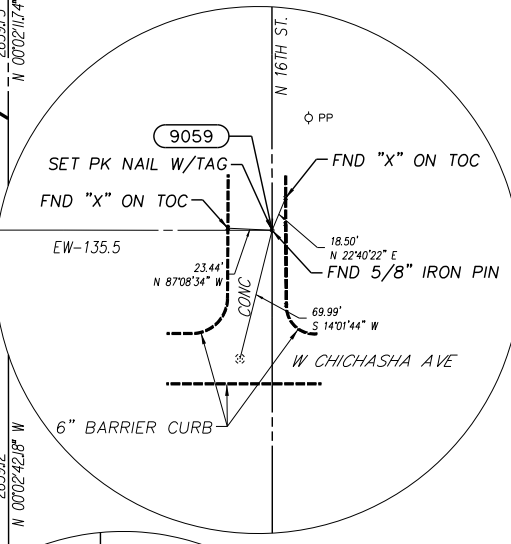
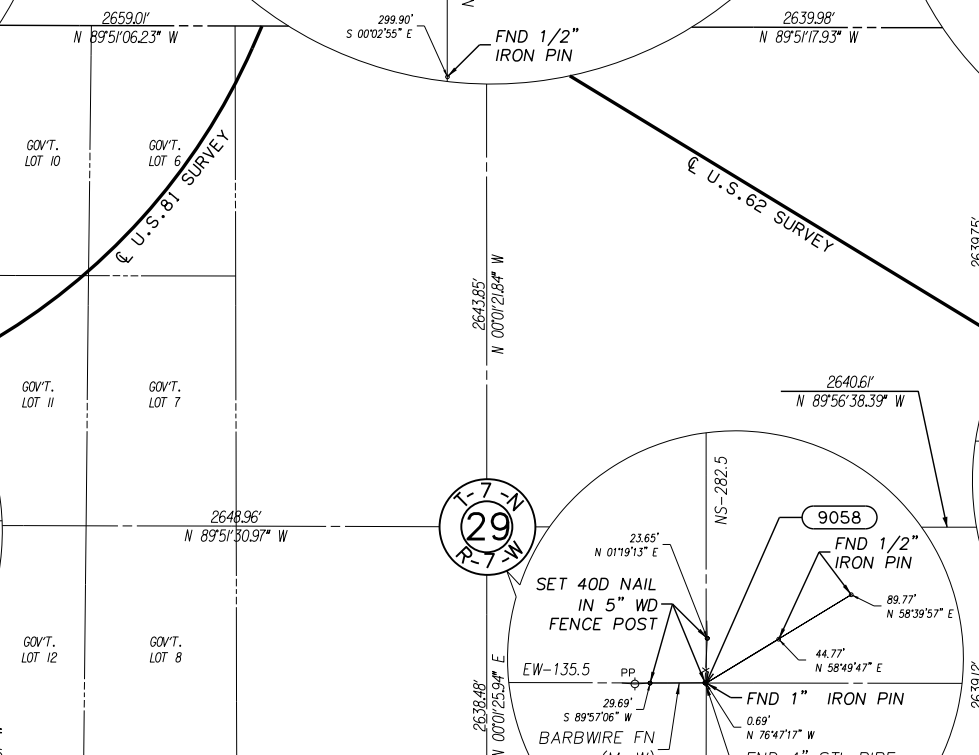
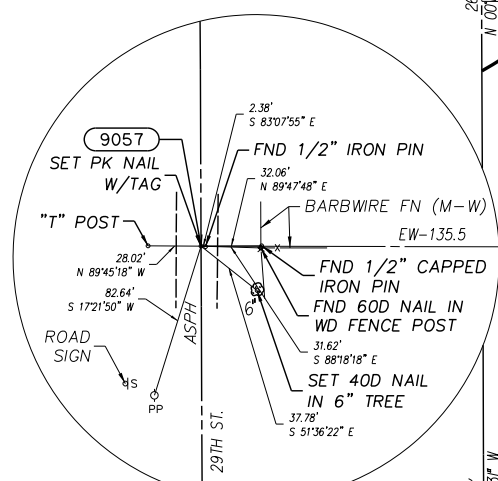


NE Corner of Section 29, T-7N, R-7W ODOT G-26-48
I found cotton spindle. Monument was found inside an impression of concrete made by a brass cap, and it fits found references and measurements from ODOT surveys, SWO 2737(1) and SWO 3609(1). ODOT brass cap was recovered by L.S. 1155 in 1991 ODOT survey and by L.S. 1407 in 1997 survey. Nothing set. I used cotton spindle.

C/4 Corner of Section 29, T-7N, R-7W ODOT G-26-924
I found 1" iron pin monument. It fits found reference points and measurements from ODOT survey, SWO 2738(1). No C.C.R.'s found for this corner. Nothing set. I used 1" iron pin.

E/4 Corner of Section 29, T-7N, R-7W ODOT G-26-925
I found 5/8" iron pin. It was later obliterated during the course of this survey. I also found references from corner record in which L.S. 1326 set mag nail in 2012. Survey mag nail not found. I set PK nail with tag. I used previously measured coordinates and reference points to restore the location of the 5/8" iron pin. This monument fits ODOT survey, SWO 2738(1). Monument location is 0.32 feet south and 0.19 feet west of the mid-point between the northeast and southeast corner of Section 29.

W/4 Corner of Section 29, T-7N, R-7W ODOT G-26-923
I found 1/2" iron pin. Monument and reference points do not fit measurements from ODOT survey, SWO 2737(1), or corner record in which L.S. 1089 set PK nail in 1994 survey. I set PK nail with tag. I used measurements from said ODOT survey to set corner. Corner location fits existing fence lines and reference points from corner record by L.S. 1089.

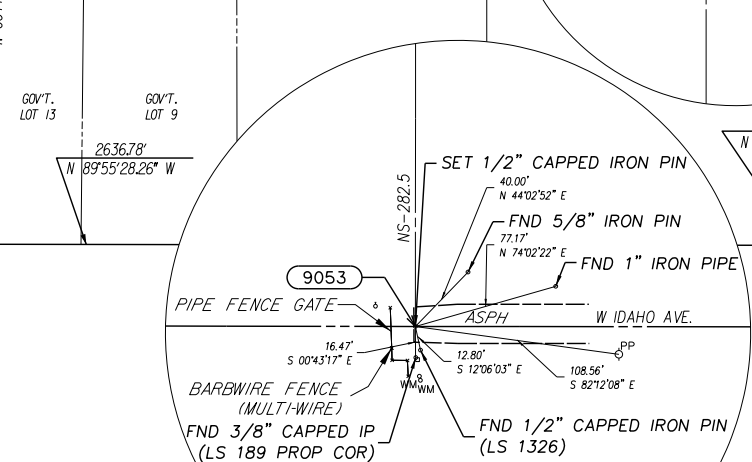
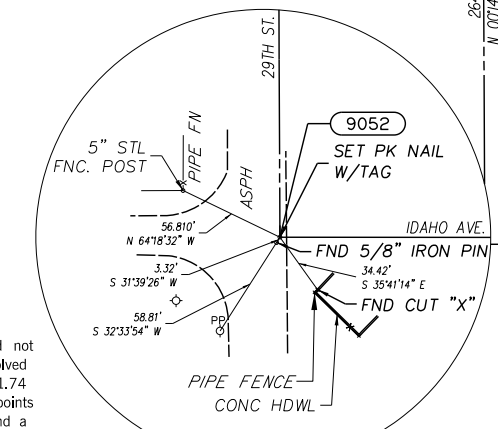


SE Corner of Section 29, T-7N, R-7W ODOT G-26-154
I found PK nail and 5/8" iron pin. PK nail that was found was set by L.S. 1407 in 1997 survey but found none of the supporting evidence listed. 5/8" iron pin matches measurements from ODOT survey, SWO 2737(1) and found reference points from the following corner records:

- 5/8" iron pin recovered by Mr. J. P. Andrews, location engineer (SD 11 form dated April 3, 1967)
- 5/8" iron pin recovered by L.S. 189 in 2006 survey.

Nothing set. I used 5/8" iron pin.

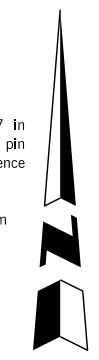
SW Corner of Section 29, T-7N, R-7W ODOT G-26-918
I found 5/8" iron pin. This monument and found references do not fit ODOT survey, SWO 2737(1), or corner record in which L.S. 1272 found iron pin in 1988 survey. Monument and reference points fit corner record in which L.S. 1200 found 5/8" iron pin in 2012 survey. I set PK nail with tag. I used measurements from said ODOT survey to set corner.



S/4 Corner of Section 29, T-7N, R-7W ODOT G-26-917
I found 1/2" capped iron pin, set by L.S. 1326 in 2004 survey. I could not accept this monument, because the method used to calculate this point, involved using a PK nail at the SE corner of Section 29, which was found to be 21.74 feet south of actual corner. I set PK nail with tag using found reference points from ODOT survey, SWO 2737(1), and a corner record where L.S. 189 found a car axle in 2006 survey.

| | | | |
|----------|--------|--|--|
| PLS | JTB | | OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET SDS <u>70</u> OF <u>76</u> |
| DRAWN | JSD | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BENHAM | | |

SWO 4380 (1) PROJECT NO. 24428(04) SHEET NO. 5070

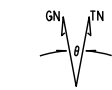


SCALE:
1" = 500'

LAND CORNER DETAILS ARE SHOWN AT 1"=50'

N4 Corner of Section 20, T-7N, R-7W ODOT G-26-453
I found 3/8" iron pin set by others. No corner record or supporting evidence found for this iron pin. 1/2" iron pin and reference points that was set by L.S. 1155 were not found from 1991 survey. I set 5/8" capped iron pin. I applied single proportionate method between the northwest and northeast corners of Section 20. Monument fits measurements from ODOT surveys, SWO 2737(1) and SWO 3609(1).

NW Corner of Section 20, T-7N, R-7W ODOT G-26-448
I found 1" iron pin that was recovered by L.S. 1155 in 1991 ODOT survey. Reference points were not found. Monument fits measurements to other section corners, from ODOT surveys, SWO 2737(1) and 3609(1). Nothing set. I used 1" iron pin.



Angle Of Variance
At Sta. 9066 (C/4 Cor.)
X = 1980034.5485
Y = 630523.0395
Lat. = 35°03'57.14750"
Long. = 97°57'41.23963"
θ = 0°01'18.76270"

W4 Corner of Section 20, T-7N, R-7W ODOT G-26-449
I found PK nail with tag that was set by others. No corner record was filed for this PK nail. Monument fits the found reference points and 3/4" iron pin that was recovered by L.S. 1155 in 1991 ODOT survey. Nothing set. I used PK nail with tag.

SW Corner of Section 20, T-7N, R-7W ODOT G-26-450
I found 5/8" iron pin. Monument fits measurements from ODOT survey SWO 2737(1) and SWO 3609(1) and fits found reference points and measurements from the following corner records:

1/2" iron pin was recovered by L.S. 449 in 1979 survey
1" iron pin was recovered by L.S. 1155 in 1991 survey
1" iron pin was recovered by L.S. 696 in 1997 survey.

Nothing set. I used 5/8" iron pin.

S4 Corner of Section 20, T-7N, R-7W ODOT G-26-451
I found 5/8" iron pin and 3/8" iron pin. Monuments are only 0.2 feet apart. The 3/8" monument fits measurements from ODOT surveys, SWO 2737(1) and SWO 3609(1). It also fits found reference points and measurements from the corner record in which L.S. 1155 set a 5/8" capped iron pin in 1991 ODOT survey. Nothing set. I used 3/8" iron pin.

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |
| | | | | | |

NE Corner of Section 20, T-7N, R-7W ODOT G-26-454
I found 1/2" iron pin that was set by L.S. 1155 in 1991 survey. Corner location and reference points fit ODOT surveys, SWO 2737(1) and SWO 3609(1). Nothing set. I used 1/2" iron pin.

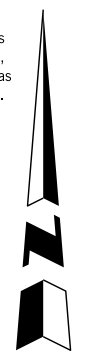
C/4 Corner of Section 20, T-7N, R-7W ODOT G-26-452
No corner evidence found. I set 5/8" iron pin using intersecting lines from 1/4 corners and measurements from ODOT surveys, SWO 2737(1) and SWO 3609(1). Calculated point fits found reference point and 5/8" iron pin that was set by L.S. 1155 in 1991 ODOT survey. Set reference points.

E/4 Corner of Section 20, T-7N, R-7W ODOT G-26-455
I found 3/4" iron pin. Current monument fits measurements from ODOT surveys, SWO 2737(1) and SWO 3609(1). It also fits found reference points and measurements from the following corner records:

OHD brass MON recovered by Mr. J.P. Andrews, location engineer (SD form 11, dated Aug 10, 1966)
3/4" iron pin was recovered by L.S. 1155 1991 survey.

Nothing set. I used 3/4" iron pin.

SE Corner of Section 20, T-7N, R-7W ODOT G-26-48
I found cotton spindle. Monument was found inside an impression of concrete made by a brass cap, and fits found references and measurements from ODOT surveys, SWO 2737(1) and SWO 3609(1). ODOT brass cap was recovered by L.S. 1155 in 1991 ODOT survey and by L.S. 1407 in 1997 survey. Nothing set. I used cotton spindle.



SCALE:
1" = 500'

LAND CORNER DETAILS ARE SHOWN AT 1"=50'

OKLAHOMA DEPARTMENT OF TRANSPORTATION
SURVEY DIVISION

SURVEY DATA SHEET

SDS 71 OF 76

| | |
|----------|--------|
| PLS | JTB |
| DRAWN | JSD |
| CHECKED | JTB |
| APPROVED | JTB |
| CREW | BENHAM |

SWO 4380 (1) PROJECT NO. 24428(04) SHEET NO. S071

NW Corner of Section 17, T-7N, R-7W ODOT G-26-931
I found 1" iron pin. Monument and reference points fit corner record in which L.S. 1407 found a 1" iron pin in 1999 and 2001 surveys. Monument fits measurements from ODOT survey, SWO 2737(1). Nothing set. I used 1" iron pin.

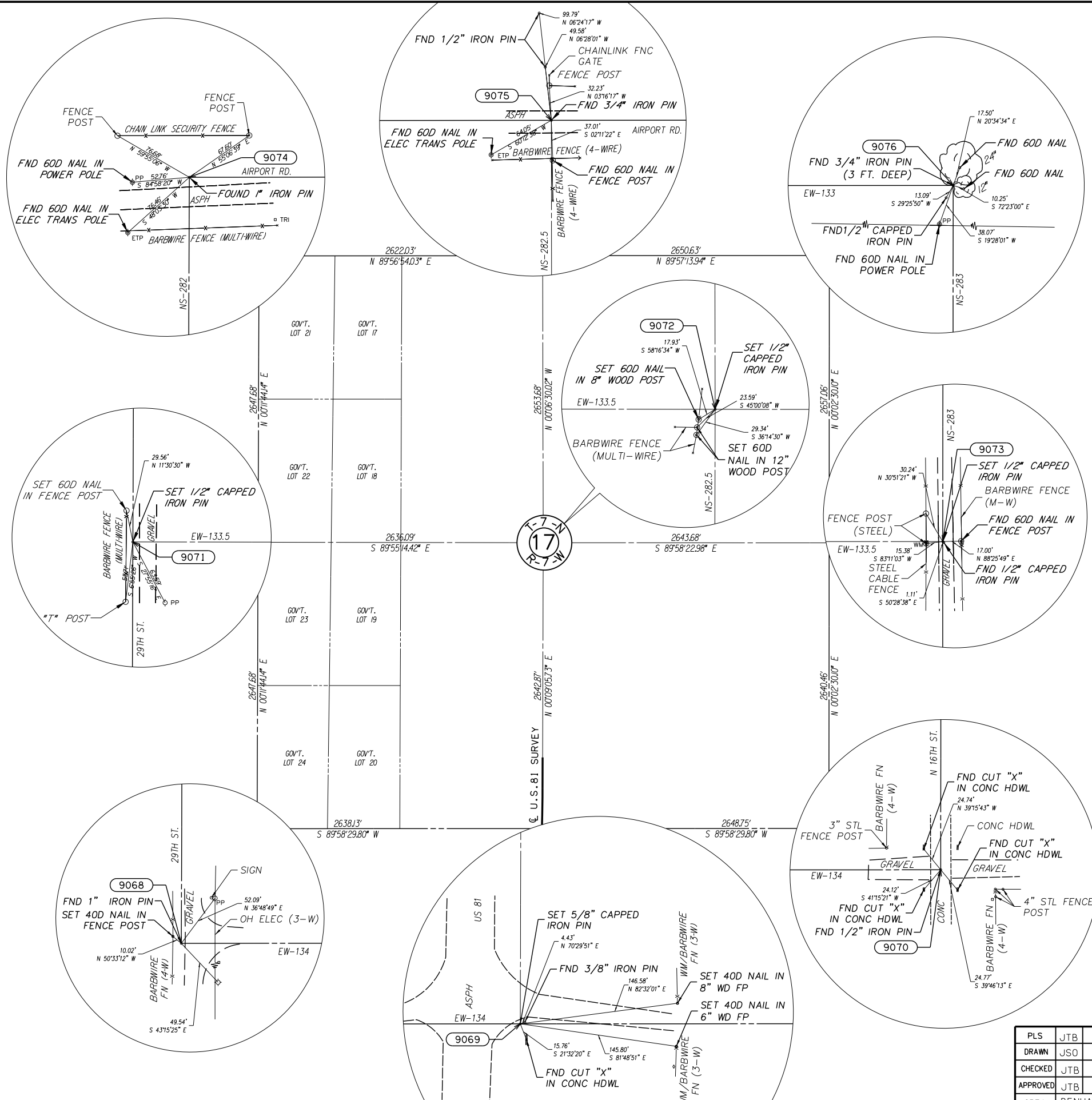
N4 Corner of Section 17, T-7N, R-7W ODOT G-26-930
I set 3/4" iron pin. Monument and reference points fit corner record in which L.S. 1272 found 3/4" iron pin in 2003 survey. Monument and reference points fit measurements from ODOT, SWO survey 2737(1). Nothing set. I used 3/4" iron pin.

W4 Corner of Section 17, T-7N, R-7W ODOT G-26-926
No corner evidence found. I set 5/8" capped iron pin. Monument was set using single proportionate method between the northwest and southwest corners of Section 17.

Angle Of Variance
At Sta. 9072 (C/4 Cor.)
X = 1980038.6957
Y = 635809.8418
Lat. = 35°04'49.43858"
Long. = 97°57'41.16544"
θ = 0°01'18.80481"

SW Corner of Section 17, T-7N, R-7W ODOT G-26-448
I found 1" iron pin that was recovered by L.S. 1155 in 1991 ODOT survey. Reference points were not found. Monument fits measurements to other section corners from ODOT surveys, SWO 2737(1) and 3609(1). Nothing set. I used 1" iron pin.

S4 Corner of Section 17, T-7N, R-7W ODOT G-26-453
I found 3/8" iron pin set by others. No corner record or supporting evidence found for this iron pin. 1/2" iron pin and reference points were not found from 1991 survey, set by L.S. 1155. I set 5/8" capped iron pin. I applied single proportionate method between the northwest and northeast corners of Section 20. Monument fits measurements from ODOT surveys, SWO 2737(1) and SWO 3609 (1).



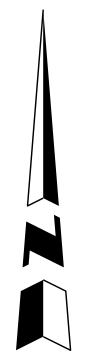
| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |

NE Corner of Section 17, T-7N, R-7W ODOT G-26-929
I found 1/2" capped iron pin and 3/4" iron pin. The 1/2" capped iron pin and reference points fit corner record in which L.S. 1326 set 1/2" capped iron pin in 2012 survey using double proportionate method. The 3/4" iron pin was found in place three feet below surface. The measurements fit ODOT survey, SWO 2737(1). Nothing set. I used 3/4" iron pin.

C4 Corner of Section 17, T-7N, R-7W ODOT G-26-927
No corner evidence found. I set 1/2" capped iron pin. Monument was set using measurements from ODOT survey, SWO 2737(1).

E4 Corner of Section 17, T-7N, R-7W ODOT G-26-928
I found 1/2" capped iron pin. I did not find supporting evidence for this monument or any corner records. The information on the cap could not be determined. I set 1/2" capped iron pin. Monument was set with single proportionate method between the northeast and southeast corners of Section 17. I used the measurements from ODOT survey, SWO 2737(1).

SE Corner of Section 17, T-7N, R-7W ODOT G-26-454
I found 1/2" iron pin that was set by L.S. 1155 in 1991 survey. Corner location and reference points fit ODOT surveys, SWO 2737(1) and SWO 3609(1). Nothing set. I used 1/2" iron pin.



SCALE:
1" = 500'

| | | |
|--|--------|--|
| PLS | JTB | OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET SDS <u>72</u> OF <u>76</u> |
| DRAWN | JSO | |
| CHECKED | JTB | |
| APPROVED | JTB | |
| CREW | BENHAM | |
| SWO <u>4380</u> (1) PROJECT NO. <u>24428(04)</u> SHEET NO. <u>S072</u> | | |

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |
| | | | | | |

NW Closing Corner of Section 25, T-7N, R-8W ODOT G-26-964
No monument found. Reference points were found from C.C.R. filed by L.S. 1155 in 1991 ODOT survey. I set 1/2" capped iron pin and reference points as shown. I used found reference points from C.C.R. filed by L.S. 1155 to set monument. However, the distance between the northwest closing corner of Section 25 and milepost 72 should be 2.39 chains, according to the May 22, 1900 G.L.O. Plat, instead of 2.59 chains, which had been used in the 1991 ODOT survey.

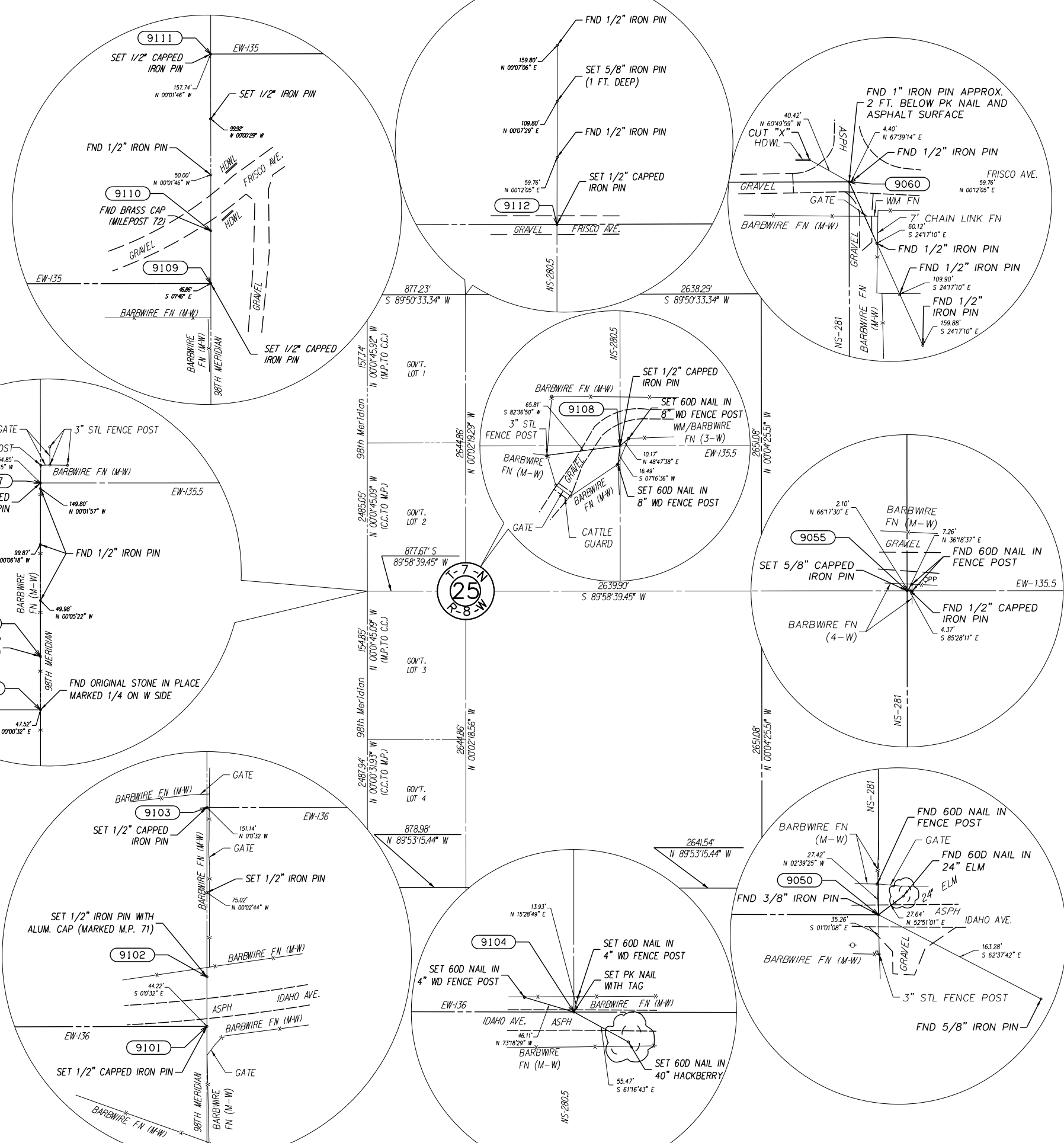
Milepost 72 Corner of Section 25, T-7N, R-8W ODOT G-26-436
I found ODOT brass cap set in concrete L.S. 1155 set monument and filed C.C.R. in 1991 ODOT survey. I used ODOT brass monument and set reference points as shown.

NE Closing Corner of Section 25, T-7N, R-8W ODOT G-26-437
No monument found. Reference points were found from C.C.R. filed by L.S. 1155 in 1991 ODOT survey. I set 1/2" capped iron pin and reference points as shown.

W Closing Corner of Section 25, T-7N, R-8W ODOT G-26-961
Nothing set in 1900 or 1901 survey. No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, north of milepost 71 1/2.

Milepost 71.5 Corner of Section 25, T-7N, R-8W ODOT G-26-43
I found 4" iron pipe with brass cap set in concrete marked "MP 71 1/2," and reference points. L.S. 1155 found monument and filed C.C.R. in 1991 ODOT survey. I used brass cap and set reference points as shown.

Angle Of Variance
At Sta. 9108 (C/4 Cor.)
X: 1969510.2000
Y: 625237.5875
Lot. = 35°03'04.89113"
Long. = 97°59'47.84944"
θ = 0°00'06.89686"



N4 Corner of Section 25, T-7N, R-8W ODOT G-26-963
No monument found. Reference points were found from C.C.R. filed by L.S. 1155 in 1991 ODOT survey. I did not agree with the position of this monument. It was based on an incorrectly placed monument milepost 72 instead of 2.39 chains as shown on May 22, 1900 G.L.O. Plat. This affected the position of the north quarter corner of Section 25. Therefore I did not use reference points to reset the monument. I set 1/2" capped iron pin and reference point as shown. I used single proportionate method between the northwest closing corner and northeast corner of Section 25 using distances from G.L.O. Notes and Plat.

NE Corner of Section 25, T-7N, R-8W ODOT G-26-444
I found 1" iron pin, PK nail, and 1/2" iron pin. 1/2" iron pin was found by L.S. 189 in 1984 survey with no supporting evidence listed. No corner record was filed for PK nail, which I removed to find 1" iron pin two feet below asphalt surface. 1" iron pin and found references fit measurements from ODOT survey, SWO 3609(1), and corner record which L.S. 1155 found 1" iron pin in 1991 survey. Nothing set. I used 1" iron pin.

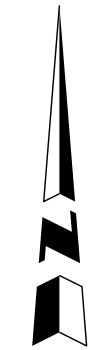
C/4 Corner of Section 25, T-7N, R-8W ODOT G-26-960
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was set on intersecting lines between north and south quarter corners and the west closing corner and east quarter corner of Section 25.

E4 Corner of Section 25, T-7N, R-8W ODOT G-26-921
I found 1/2" capped iron pin set by L.S. 1326 in 2005 survey. I set 5/8" capped iron pin. L.S. 1326 used a 1/2" iron pin located at the NW corner of Section 30 to determine the W/4 corner. I did not find supporting evidence for that 1/2" iron pin. I used a 1" iron pin found at the NW corner of section 30 (fits ODOT survey, SWO 3609), and the SW corner, and used single proportion to set point on line and between said corners.

SE Corner of Section 25, T-7N, R-8W ODOT G-26-920
I found 3/8" iron pin. Monument and reference points fit ODOT survey, SWO 2737(1), and the following corner records:

L.S. 1378 found 1" iron pin in 1996 survey
L.S. 1272 found iron pin in 1998 survey
L.S. 1200 found 3/8" iron pin in 2012 survey.

Nothing set. I used 3/8" iron pin.



SCALE:
1" = 500'

LAND CORNER DETAILS ARE SHOWN AT 1"=50'

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|--------|--------------|-----------------------|----------------|--|
| SURVEY DATA SHEET | | | | | |
| PLS | JTB | | | | |
| DRAWN | JSO | | | | |
| CHECKED | JTB | | | | |
| APPROVED | JTB | | | | |
| CREW | BENHAM | SWO 4380 (1) | PROJECT NO. 24428(04) | SHEET NO. S073 | |

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |
| | | | | | |

NW Closing Corner of Section 36, T-7N, R-8W ODOT G-26-957
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, north of milepost 71.

Milepost 71 Corner of Section 36, T-7N, R-8W ODOT G-26-958
No corner evidence found. I set 1/2" iron pin with aluminum cap, stamped MP 71 and G-26-958. I set reference points as shown. I set monument using single proportionate method between found mileposts 70 1/2 and 71 1/2.



Angle Of Variance
At Sta. 9100 (C/4 Cor.)
X = 1969511.9271
Y = 619950.7188
Lat. = 35°02'12.59890"
Long. = 97°59'47.83080"
θ = 0°00'06.90744"

NE Closing Corner of Section 36, T-7N, R-8W ODOT G-26-959
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, south of milepost 71.

W Closing Corner of Section 36, T-7N, R-8W ODOT G-26-953
Nothing set in 1900 or 1901 survey. No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, north of milepost 70 1/2.

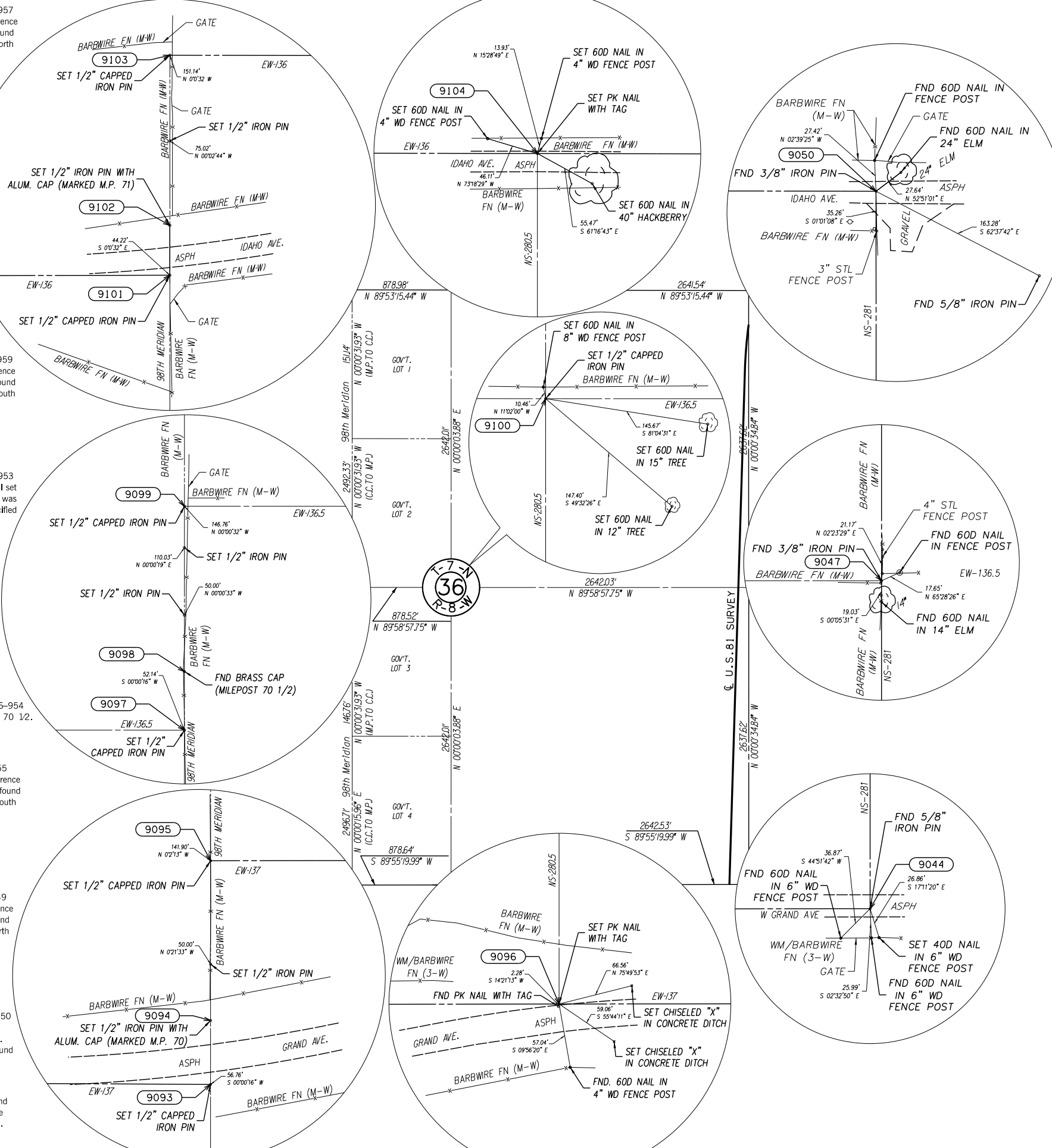
Milepost 70.5 Corner of Section 36, T-7N, R-8W ODOT G-26-954
I found 4" iron pipe with brass cap set in concrete marked MP 70 1/2. I used brass cap and set reference points as shown.

E Closing Corner of Section 36, T-7N, R-8W ODOT G-26-955
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, south of milepost 70 1/2.

SW Closing Corner of Section 36, T-7N, R-8W ODOT G-26-949
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, north of milepost 70.

Milepost 70 Corner of Section 36, T-7N, R-8W ODOT G-26-950
No corner evidence found. I set 1/2" iron pin with aluminum cap, stamped MP 70 and G-26-950. I set reference points as shown. I set monument using single proportionate method between found mileposts 68 and 70 1/2.

SE Closing Corner of Section 36, T-7N, R-8W ODOT G-26-951
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, south of milepost 70.



N4 Corner of Section 36, T-7N, R-8W ODOT G-26-956
No corner evidence found. I set PK nail with tag and reference points as shown. I used single proportionate method between the southwest closing corner and southeast corner of Section 25 using distances from G.L.O. Notes and Plat.

NE Corner of Section 36, T-7N, R-8W ODOT G-26-920
I found 3/8" iron pin. Monument and reference points fit ODOT survey, SWO 2737(1), and the following corner records:

- L.S. 1378 found 1" iron pin in 1996 survey
- L.S. 1272 found iron pin in 1998 survey
- L.S. 1200 found 3/8" iron pin in 2012 survey.

Nothing set. I used 3/8" iron pin

C4 Corner of Section 36, T-7N, R-8W ODOT G-26-952
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was set on intersecting lines between north and south quarter corners and the west closing corner and east quarter corner of Section 36.

E/4 Corner of Section 36, T-7N, R-8W ODOT G-26-914
I found 3/8" iron pin. Monument and references fit corner record in which L.S. 1200 set 3/8" iron pin in 2012 survey. Nothing set. I used 3/8" iron pin.

SE Corner of Section 36, T-7N, R-8W ODOT G-26-913
I found 5/8" iron pin. Corner location and found reference points fit ODOT survey, SWO 2737(1), and corner record in which L.S. 1378 found 5/8" iron pin in 1995 survey. Nothing set. I used 5/8" iron pin.

S/4 Corner of Section 36, T-7N, R-8W ODOT G-26-948
I found PK nail with tag. L.S. 1326 set PK nail and filed C.C.R. in 2003 survey. Monument did not fit proportionate distances between southwest closing corner and southeast corner of Section 36. I set PK nail with tag and reference points as shown. I used single proportionate method between the southwest closing corner and southeast corner of Section 36 using distances from G.L.O. Notes and Plat.



SCALE:
1" = 500'
LAND CORNER DETAILS ARE SHOWN AT 1"=50'

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | |
|---------------------------------------|--------|--------------|--------------------------------------|
| SURVEY DIVISION | | | |
| SURVEY DATA SHEET | | | |
| | | SDS 74 OF 76 | |
| PLS | JTB | | |
| DRAWN | JSD | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BENHAM | SWO 4380 (1) | PROJECT NO. 24428(04) SHEET NO. 5074 |

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | DATE | |
| | | | | | |

NW Closing Corner of Section 1, T-6N, R-8W ODOT G-26-949
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, north of milepost 70.

Milepost 70 Corner of Section 1, T-6N, R-8W ODOT G-26-950
No corner evidence found. I set 1/2" capped iron pin with aluminum cap, stamped MP 70 and G-26-950. I set reference points as shown. I set monument using single proportionate method between found mileposts 68 and 70 1/2.

NE Closing Corner of Section 1, T-6N, R-8W ODOT G-26-951
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, south of milepost 70.

W Closing Corner of Section 1, T-6N, R-8W ODOT G-26-945
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, north of milepost 69 1/2.

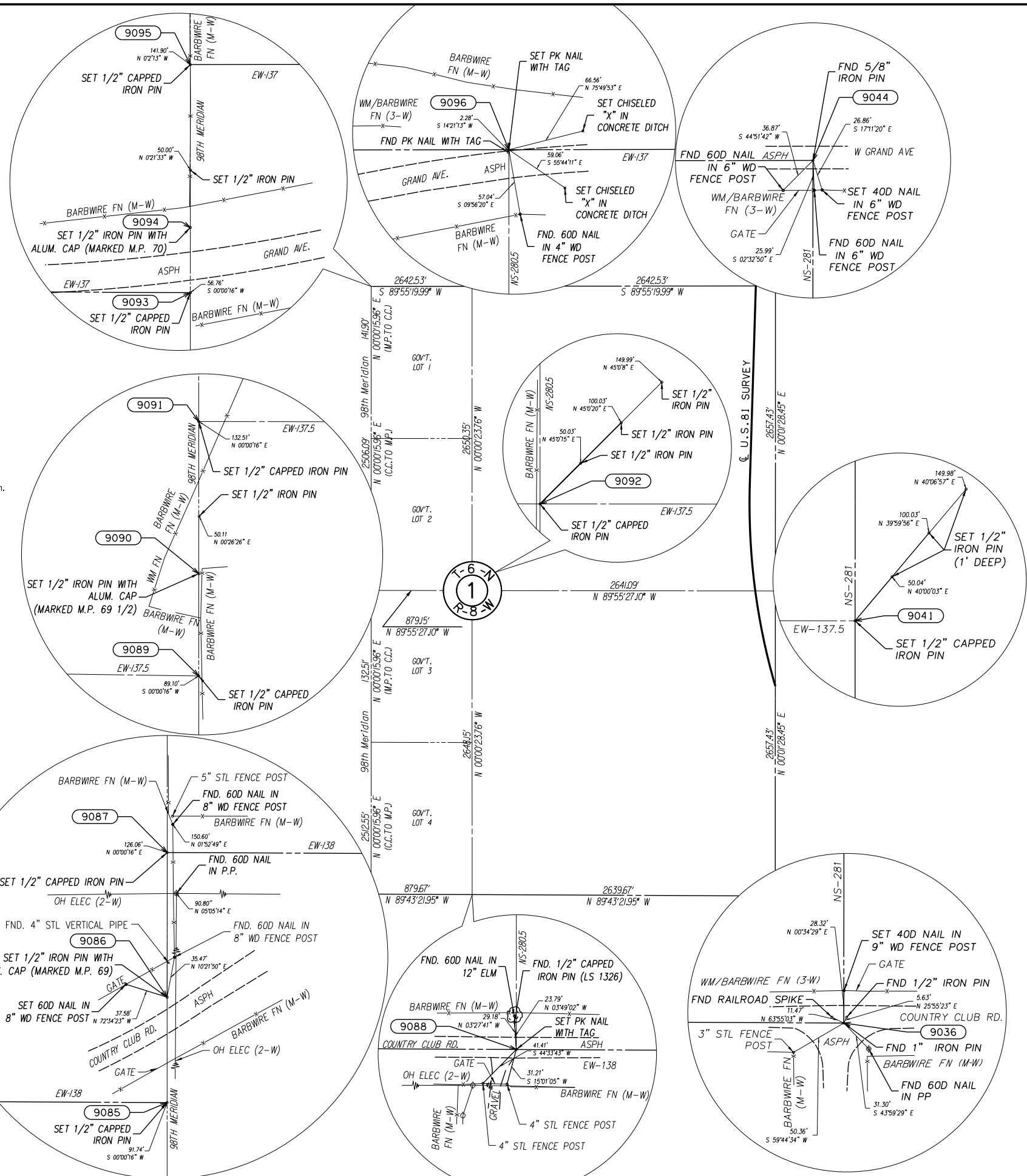
Milepost 69.5 Corner of Section 1, T-6N, R-8W ODOT G-26-946
No Corner evidence found. I set 1/2" iron pin with aluminum cap, stamped MP 69 1/2 and G-26-946. I set reference points as shown. I set monument using single proportionate method between found mileposts 68 and 70 1/2.

E Closing Corner of Section 1, T-6N, R-8W ODOT G-26-947
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, south of milepost 69 1/2.

SW Closing Corner of Section 1, T-6N, R-8W ODOT G-26-941
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, north of milepost 69

Milepost 69 Corner of Section 1, T-6N, R-8W ODOT G-26-942
I found 4" steel post with no brass cap that was found by L.S. 1082 in 1994 survey. The C.C.R. by L.S. 1082 did not list any supporting evidence. I found milepost 68 to the south and milepost 70 1/2 to the north and the 4" steel post did not fit the proportionate distance by nearly 30 feet. I determined that the 4" steel post was most likely a fence post that used to be connected to old fence line to the north. I set 1/2" iron pin with aluminum cap, stamped MP 69 and G-26-942. I set reference points as shown. I set monument using single proportionate method between found mileposts 68 and 70 1/2.

SE Closing Corner of Section 1, T-6N, R-8W ODOT G-26-943
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, south of milepost 69.



N/4 Corner of Section 1, T-6N, R-8W ODOT G-26-948
I found PK nail with tag. L.S. 1326 set PK nail and filled C.C.R. in 2003 survey. Monument did not fit proportionate distances between southwest closing corner and southeast corner of Section 36. I set PK nail with tag and reference points as shown. I used single proportionate method between the southwest closing corner and southeast corner of Section 36 using distances from G.L.O. Notes and Plat.

NE Corner of Section 1, T-6N, R-8W ODOT G-26-913
I found 5/8" iron pin. Corner location and found reference points fit ODOT survey, SWO 2737(1), and corner record in which L.S. 1378 found 5/8" iron pin in 1995 survey. Nothing set. I used 5/8" iron pin.

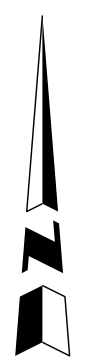
C/4 Corner of Section 1, T-6N, R-8W ODOT G-26-944
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was set on intersecting lines between north and south quarter corners and the west closing corner and east quarter corner of Section One.

E/4 Corner of Section 1, T-6N, R-8W ODOT G-26-908
No corner evidence found. I set 5/8" iron pin and references. I set corner using single proportionate method.

SE Corner of Section 1, T-6N, R-8W ODOT G-26-907
I found 1/2" iron pin, railroad spike, & 1" iron pin. 1/2" iron pin and found reference points fit corner record in which L.S. 1272 found 1/2" iron pin in 2001 survey, with no supporting evidence listed. No corner record found for railroad spike. 1" iron pin fits measurements from ODOT survey, SWO 2737(1). Nothing set. I used 1" iron pin.

S/4 Corner of Section 1, T-6N, R-8W ODOT G-26-940
Found 1/2" capped iron pin marked L.S. 1326. This monument is from a C.C.R. in which L.S. 1326 set a 1/2" capped iron pin in a 2014 survey. The monument was set using an incorrectly identified monument at milepost 69 in a C.C.R. by L.S. 1082 in a 1994 survey. This is further evidenced by the monument at the south quarter corner of Section One not fitting the section line road and fences. Set PK nail with tag. I set monument using single proportionate method between the southwest closing corner and southeast corner of Section One using distances from G.L.O. Notes and Plat.

Angle Of Variance
At Sta. 9092 (C/4 Cor.)
X= 1969512.1828
Y= 614658.3618
Lat.= 35°01'20.25216"
Long.= 97°59'47.82985"
θ = 0°00'06.90798"



| | | |
|---|--------|--|
| PLS | JTB | OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET SDS 75 OF 76 |
| DRAWN | JSD | |
| CHECKED | JTB | |
| APPROVED | JTB | |
| CREW | BENHAM | |
| SWO 4380 (1) PROJECT NO. 24428(04) SHEET NO. 5075 | | |

| OKLAHOMA DEPARTMENT OF TRANSPORTATION | | | | | |
|---------------------------------------|-------|-----------|-------------|-----------|--------------|
| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| | OKLA. | | | | |
| DESCRIPTION | | | REVISIONS | | DATE |
| | | | | | |

NW Closing Corner of Section 12, T-6N, R-8W ODOT G-26-941
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, north of milepost 69

Milepost 69 Corner of Section 12, T-6N, R-8W ODOT G-26-942
I found 4" steel post with no brass cap that was found by L.S. 1082 in 1994 survey. The C.C.R. by L.S. 1082 did not list any supporting evidence. I found milepost 68 to the south and milepost 70 1/2 to the north and the 4" steel post did not fit the proportionate distance by nearly 30 feet. I determined that the 4" steel post was most likely a fence post that used to be connected to old fence line to the north. I set 1/2" iron pin with aluminum cap, stamped MP 69 and G-26-942. I set reference points as shown. I set monument using single proportionate method between found mileposts 68 and 70 1/2.

NE Closing Corner of Section 12, T-6N, R-8W ODOT G-26-943
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, south of milepost 69.

W Closing Corner of Section 12, T-6N, R-8W ODOT G-26-937
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, north of milepost 68 1/2

Milepost 68.5 Corner of Section 12, T-6N, R-8W ODOT G-26-938
No corner evidence found. I set 1/2" iron pin with aluminum cap, stamped MP 68 1/2 and G-26-938. I set reference points as shown. I set monument using single proportionate method between found mileposts 68 and 70 1/2.

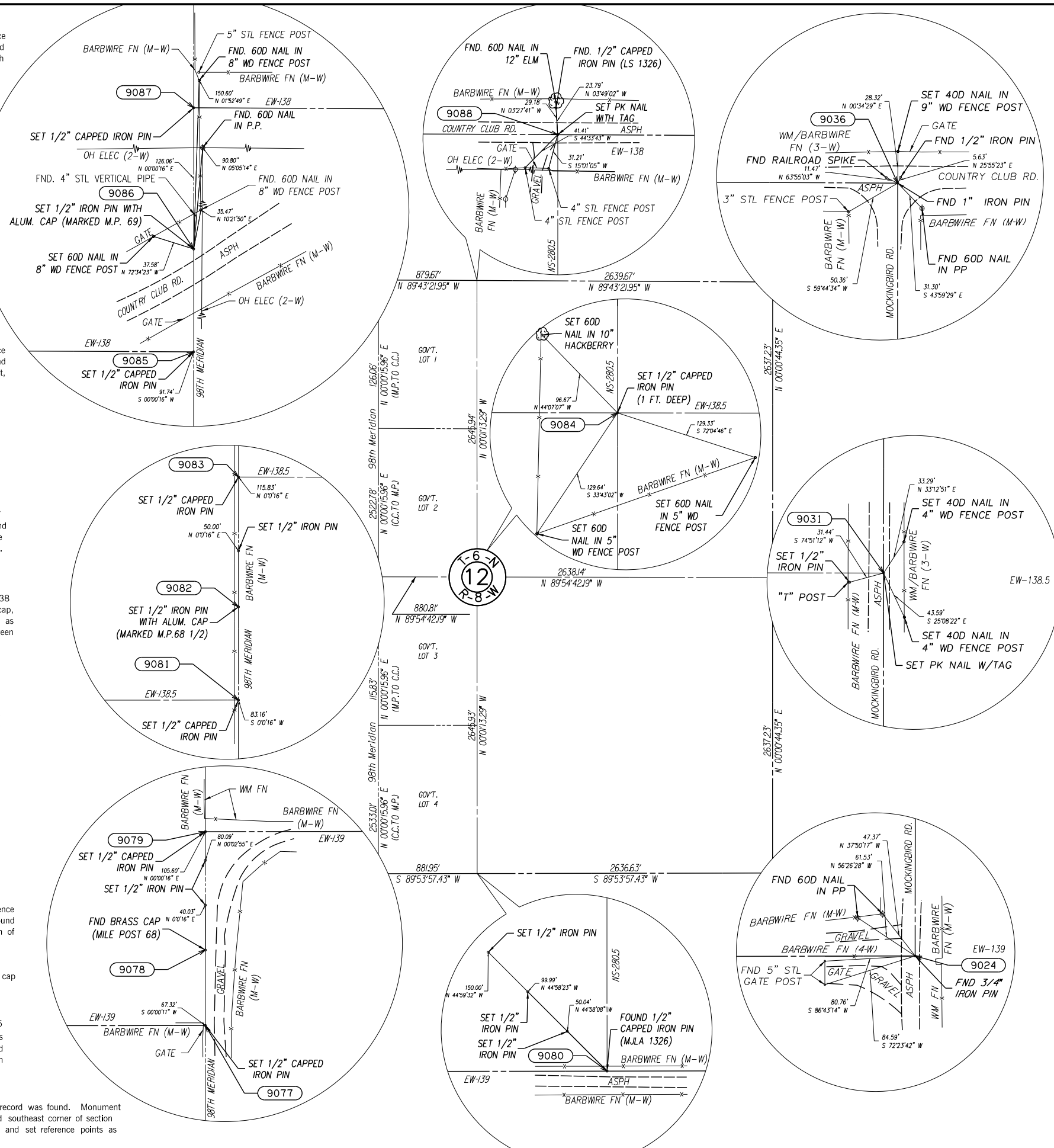
E Closing Corner of Section 12, T-6N, R-8W ODOT G-26-939
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, south of milepost 68 1/2.

SW Closing Corner of Section 12, T-6N, R-8W ODOT G-26-933
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, north of milepost 68

Milepost 68 Corner of Section 12, T-6N, R-8W ODOT G-26-934
Corner evidence found brass cap, marked, "MILE 68." I used brass cap and set reference points as shown.

SE Closing Corner of Section 12, T-6N, R-8W ODOT G-26-935
No evidence found. Set 1/2" capped iron pin and reference points as shown. Monument was placed on a line between found mileposts, at the distance specified on G.L.O. Notes and Plat, south of milepost 68.

S/4 Corner of Section 12, T-6N, R-8W ODOT G-26-932
I found 1/2" capped iron pin marked, "M/JLA 1326". No corner record was found. Monument fits proportionate distances between southwest closing corner and southeast corner of section 12, from G.L.O. notes and Plat. I used 1/2" capped iron pin and set reference points as shown.



N/4 Corner of Section 12, T-6N, R-8W ODOT G-26-940
Found 1/2" capped iron pin marked L.S. 1326. This monument is from a C.C.R. in which L.S. 1326 set a 1/2" capped iron pin in a 2014 survey. The monument was set using an incorrectly identified monument at milepost 69 in a C.C.R. by L.S. 1082 in a 1994 survey. This is further evidenced by the monument at the south quarter corner of section one not fitting the section line road and fences. Set PK nail with tag. I set monument using single proportionate method between the southwest closing corner and southeast corner of Section One using distances from G.L.O. Notes and Plat.

NE Corner of Section 12, T-6N, R-8W ODOT G-26-907
I found 1/2" iron pin, railroad spike, & 1" iron pin. 1/2" iron pin and found reference points fit corner record in which L.S. 1272 found 1/2" iron pin in 2001 survey, with no supporting evidence listed. No corner record found for railroad spike. 1" iron pin fits measurements from ODOT survey, SWO 2737 (1). Nothing set. I used 1" iron pin.

C/4 Corner of Section 12, T-6N, R-8W ODOT G-26-936
No corner evidence found. I set 1/2" capped iron pin and reference points as shown. Monument was set on intersecting lines between north and south quarter corners and the west closing corner and east quarter corner of Section 12.

E/4 Corner of Section 12, T-6N, R-8W ODOT G-26-898
No corner evidence found. I set PK nail with tag. I used single proportion method to set the corner between the northwest and southwest corner of Section 7.

SE Corner of Section 12, T-6N, R-8W ODOT G-26-897
I found 3/4" iron pin. Corner location and found reference points match the corner records filed by the following surveyors:

- L.S. 449 found iron pin (C.C.R. filed June 8, 1982)
- L.S. 1082 found 3/4" iron pin (C.C.R. filed APR 18, 1994)
- L.S. 1272 found 3/8" iron pin (C.C.R. filed Dec 7, 1998)
- L.S. 1272 found iron pin (C.C.R. filed Aug 31, 2001).

Nothing set. I used 3/4" iron pin.



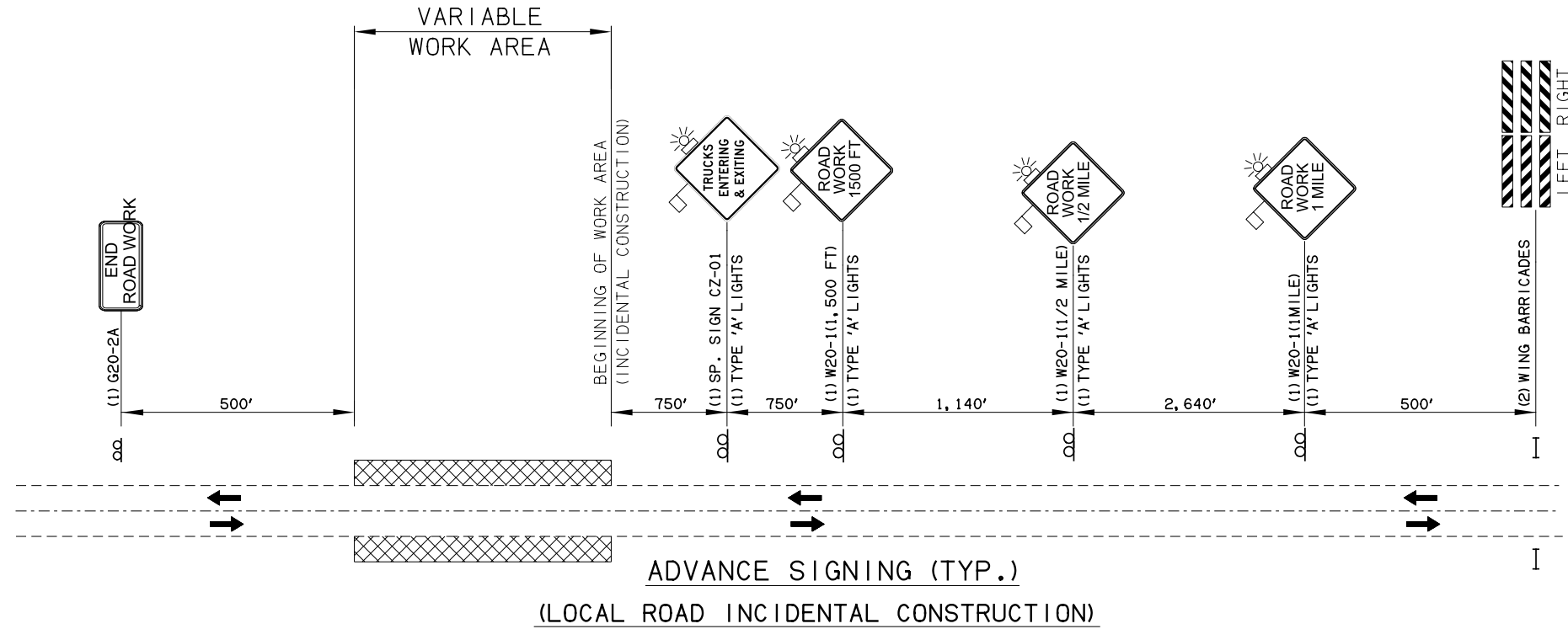
Angle of Variance
At Sta. 9084 (C/4 Cor.)
X = 1969513.4280
Y = 609364.2708
Lot. = 35°00'27.88803"
Long. = 97°59'47.81702"
θ = 0°00'06.91526"



SCALE:
1" = 500'

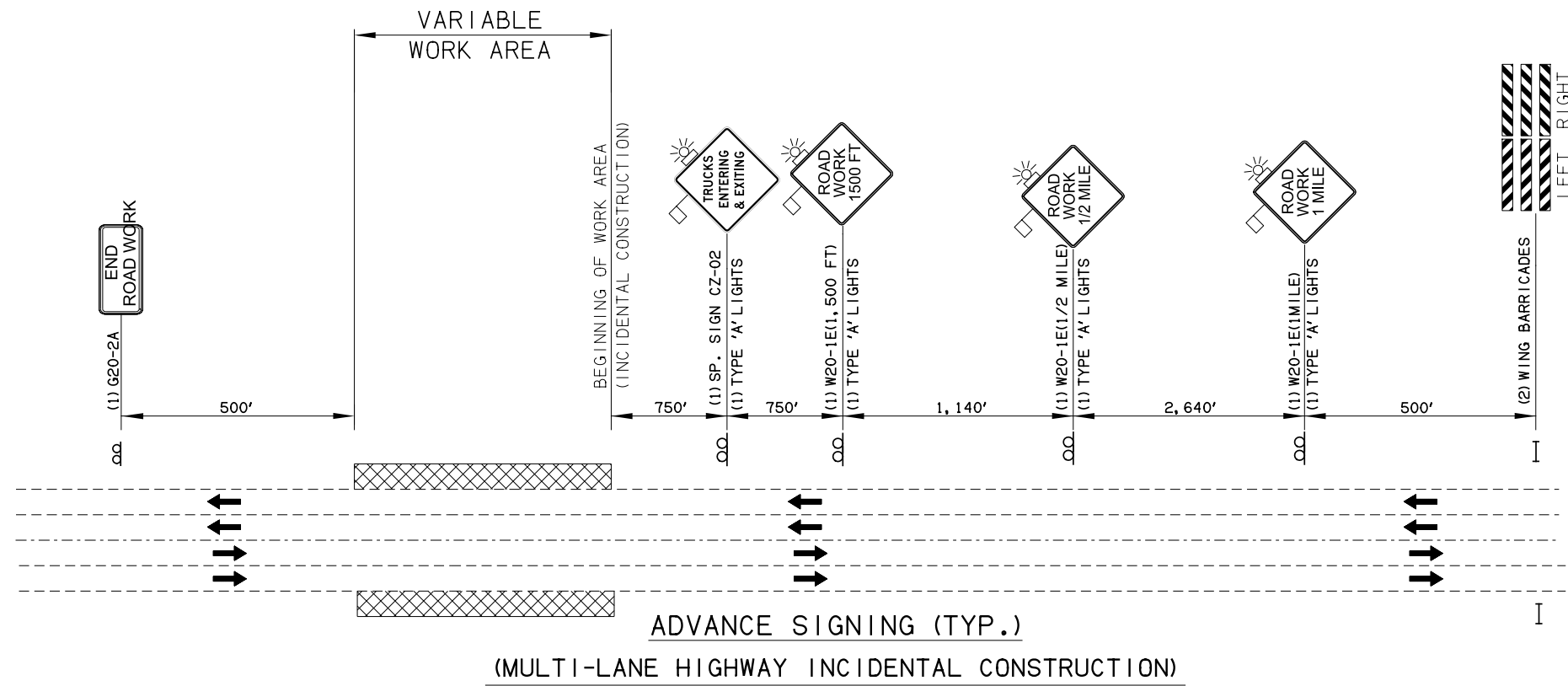
LAND CORNER DETAILS ARE SHOWN AT 1"=50'

| | | | |
|--|--------|--|--|
| PLS | JTB | | OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET SDS <u>76</u> OF <u>76</u> |
| DRAWN | JSD | | |
| CHECKED | JTB | | |
| APPROVED | JTB | | |
| CREW | BENHAM | | |
| SWO <u>4380</u> (1) PROJECT NO. <u>24428(04)</u> SHEET NO. <u>S076</u> | | | |



ADVANCE SIGNING (LOCAL ROAD INCIDENTAL CONSTRUCTION) WILL APPLY TO BOTH DIRECTIONS OF TRAFFIC FOR THE FOLLOWING ROADS:

- IDAHO AVENUE
- 29TH STREET
- NORGE ROAD
- GRAND AVENUE
- COUNTRY CLUB ROAD
- QUAIL ROAD
- CS 2830
- CS 2815



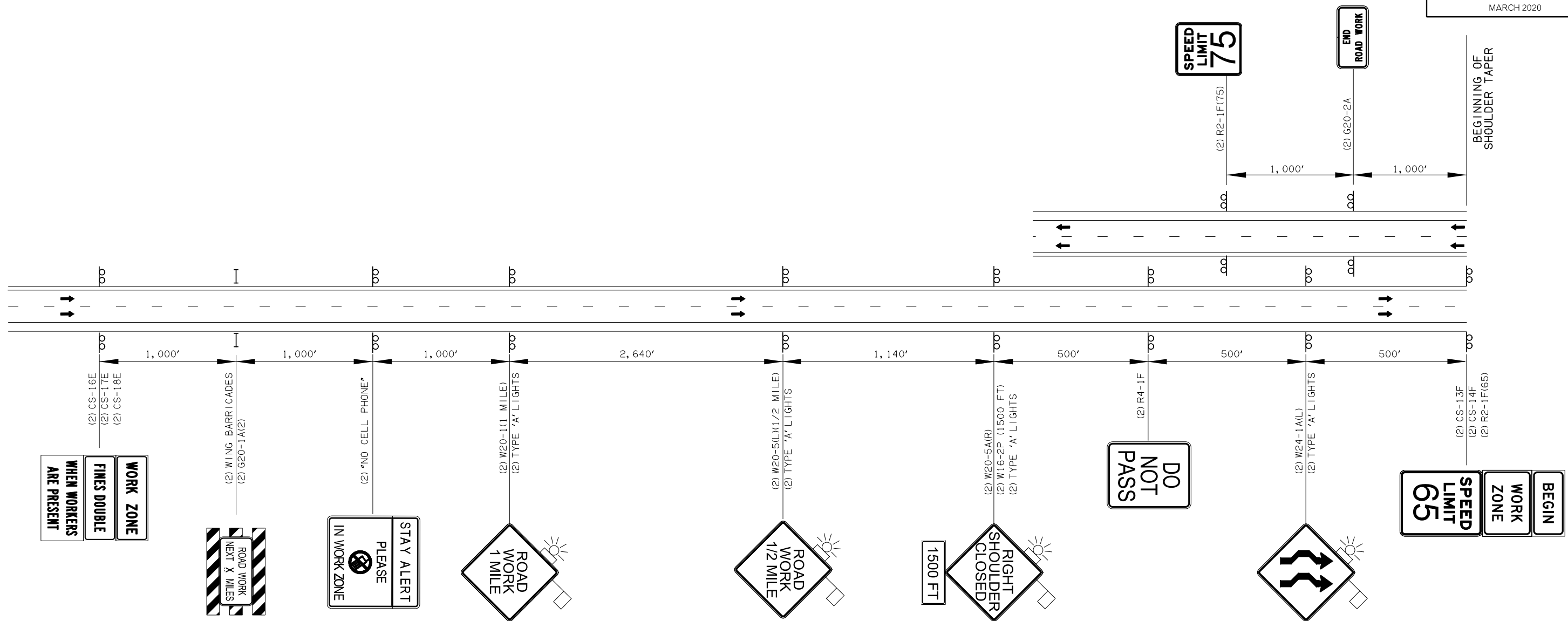
ADVANCE SIGNING (MULTI-LANE HIGHWAY INCIDENTAL CONSTRUCTION) WILL APPLY TO BOTH DIRECTIONS OF TRAFFIC FOR THE FOLLOWING HIGHWAYS:

- US-62
- I-44
- US-81

ONE CHANGEABLE MESSAGE SIGN SHALL BE PROVIDED IN EACH DIRECTION AT LOCATION(S) PROVIDED BY THE ENGINEER.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUGGESTED TRAFFIC CONTROL - ADVANCE SIGNING



BORDER
R=1.5"
TH=0.75"
IN=0.75"

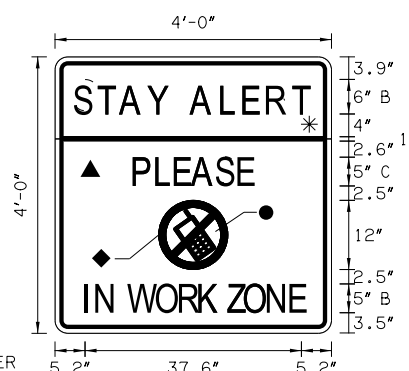
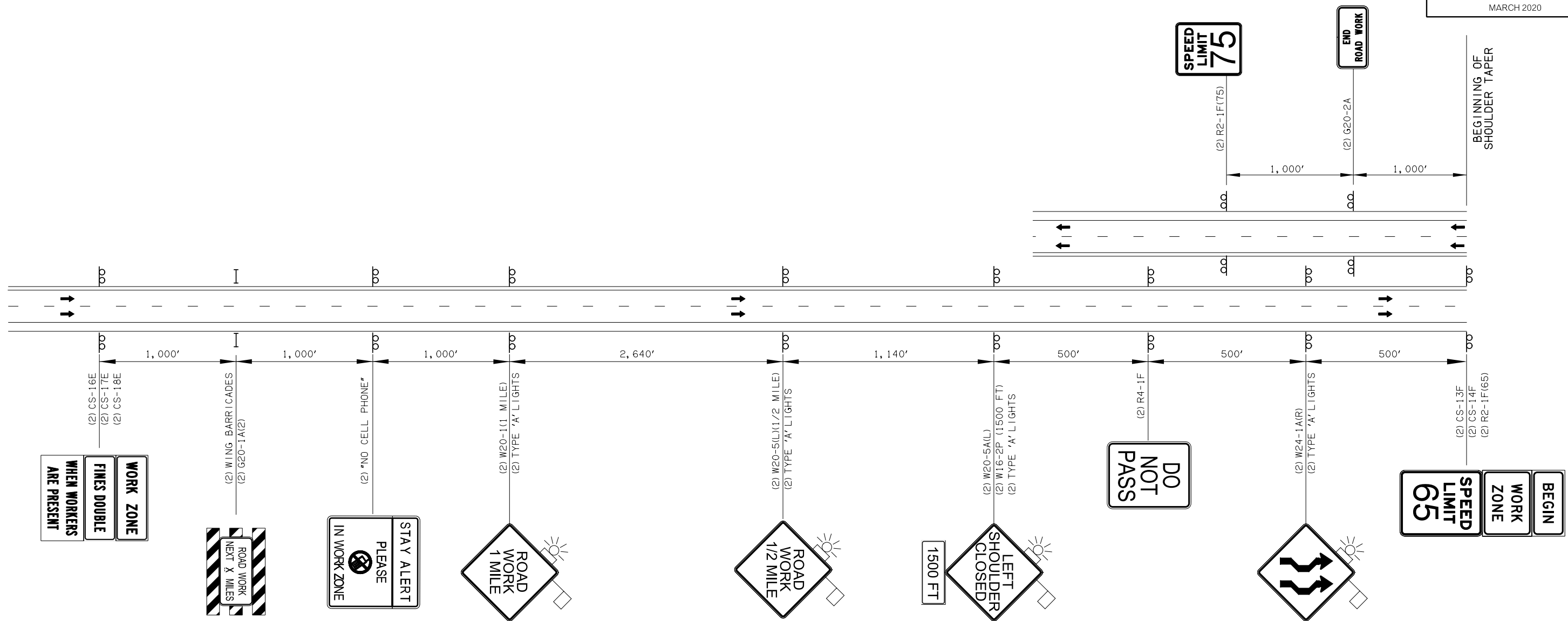
COLOR:
LEGEND, SYMBOL AND BORDER:
BLACK (NON-REFLECTORIZED)
BACKGROUND:
FLUORESCENT ORANGE (REFLECTORIZED)
FLUORESCENT YELLOW (REFLECTORIZED)
WHITE (REFLECTORIZED)
CIRCLE AND DIAGONAL:
RED (TRANSPARENT REFLECTORIZED)

TYPICAL ADVANCE SIGNING
SHIFTING TAPER - TO INSIDE
(MULTIPLE PHASES - 1-44)

- NOTES:
1. TRAFFIC CONTROL ADVANCE WARNING SIGNS SHALL BE MIRRORED FOR OPPOSITE DIRECTION.
 2. TWO (2) PORTABLE CHANGEABLE MESSAGE SIGNS PER APPROACH SHALL BE PROVIDED. THESE SIGNS WILL BE USED AT THE DISCRETION OF THE ENGINEER.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUGGESTED TRAFFIC CONTROL - ADVANCE SIGNING



BORDER
R=1.5"
TH=0.75"
IN=0.75"

COLOR:
LEGEND, SYMBOL AND BORDER:
BLACK (NON-REFLECTORIZED)
BACKGROUND:
▲ FLUORESCENT ORANGE (REFLECTORIZED)
* FLUORESCENT YELLOW (REFLECTORIZED)
● WHITE (REFLECTORIZED)
◆ CIRCLE AND DIAGONAL:
RED (TRANSPARENT REFLECTORIZED)

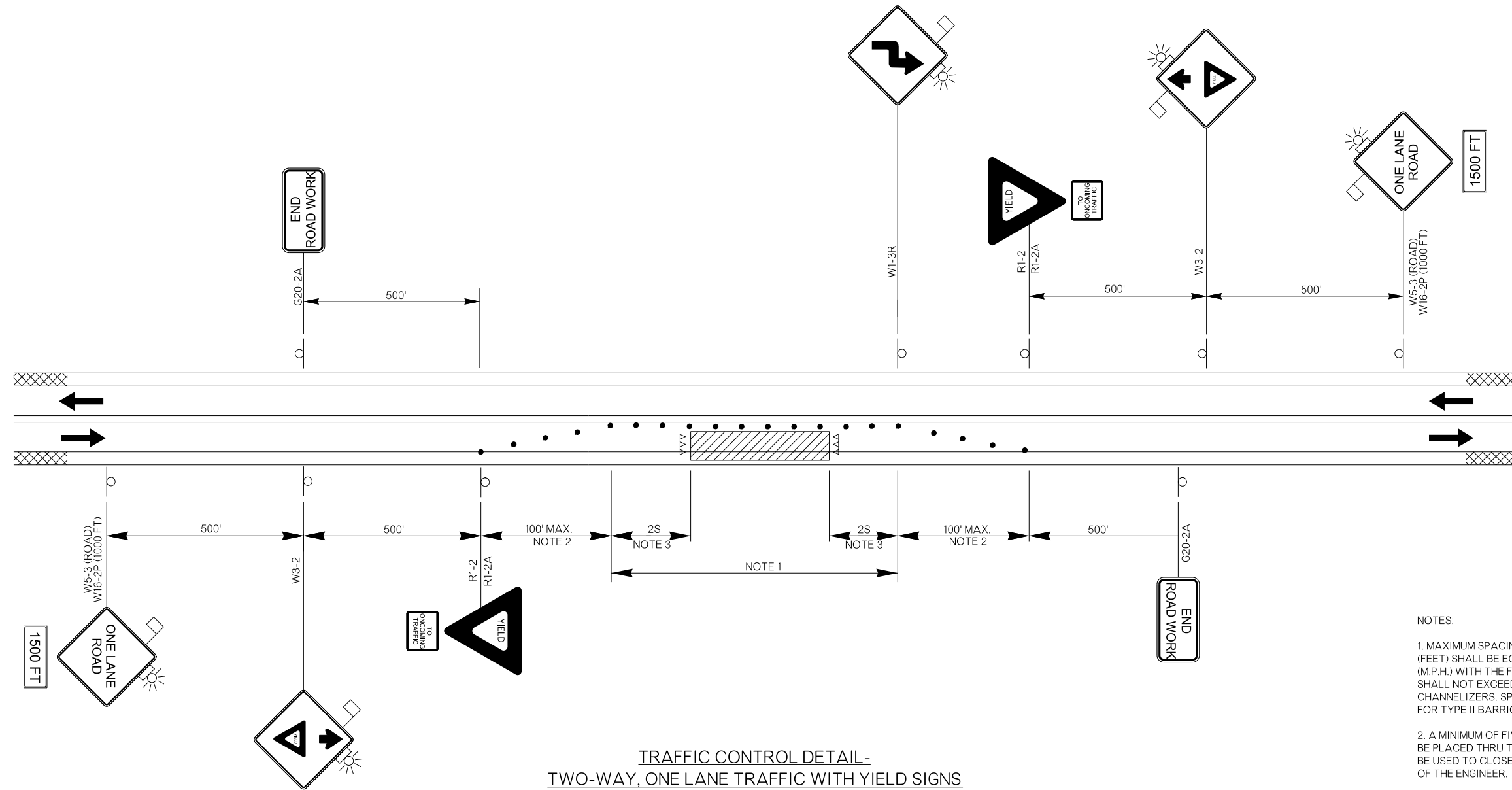
TYPICAL ADVANCE SIGNING
SHIFTING TAPER - TO OUTSIDE
(MULTIPLE PHASES - 1-44)

- NOTES:
1. TRAFFIC CONTROL ADVANCE WARNING SIGNS SHALL BE MIRRORED FOR OPPOSITE DIRECTION.
 2. TWO (2) PORTABLE CHANGEABLE MESSAGE SIGNS PER APPROACH SHALL BE PROVIDED. THESE SIGNS WILL BE USED AT THE DISCRETION OF THE ENGINEER.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUGGESTED TRAFFIC CONTROL - ADVANCE SIGNING

3/13/2020 F:\2016\1001-1500\1016-1182-A-40-Design\Microstation\000T\000T\000T\2442804_1_Traffic\CON_04.dwg 3:13 AM



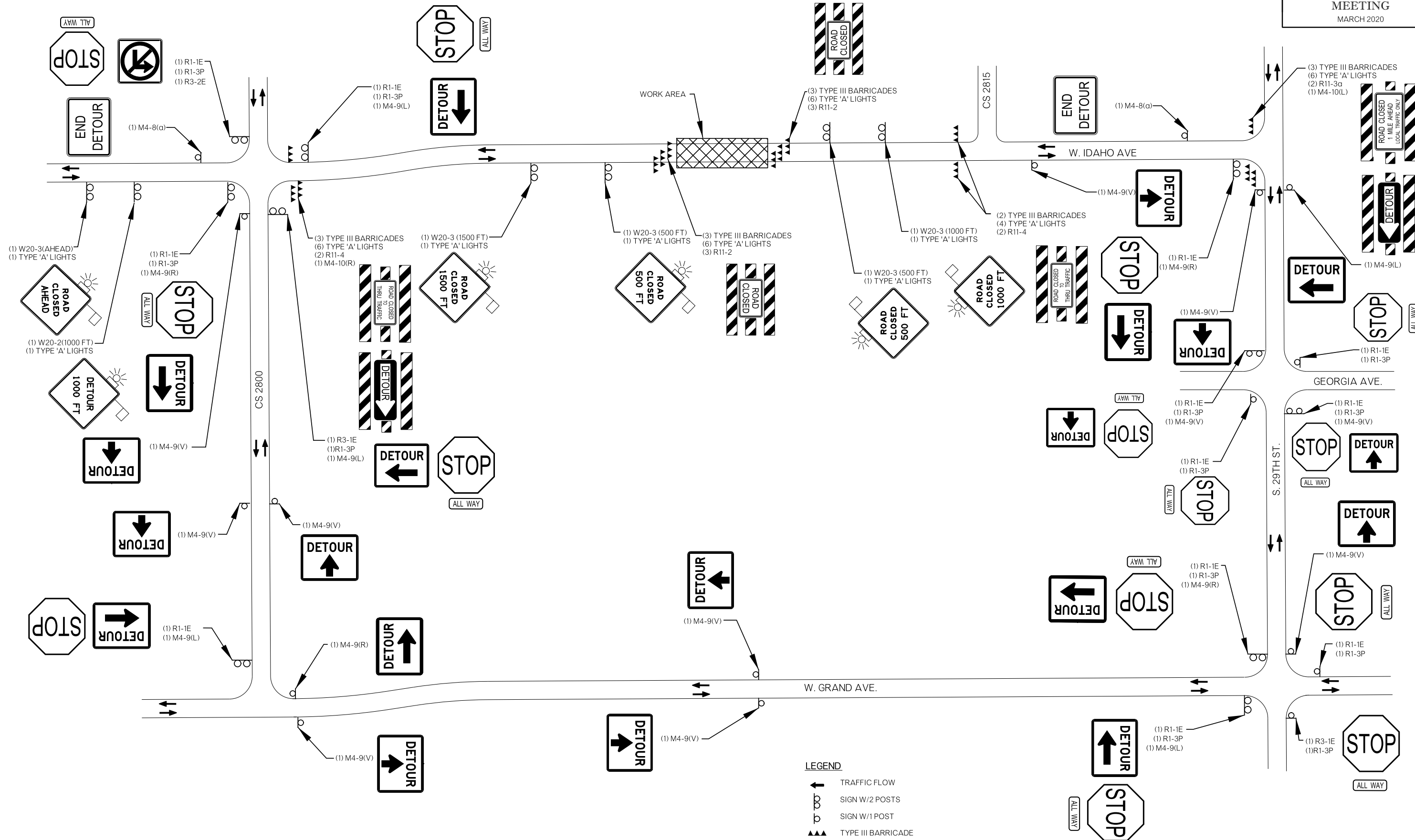
TRAFFIC CONTROL DETAIL-
TWO-WAY, ONE LANE TRAFFIC WITH YIELD SIGNS

- NOTES:
1. MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES (FEET) SHALL BE EQUAL TO THE POSTED SPEED LIMIT (M.P.H.) WITH THE FOLLOWING EXCEPTIONS. SPACING SHALL NOT EXCEED 25 FEET FOR CONES OR TUBE CHANNELIZERS. SPACING SHALL NOT EXCEED 50 FEET FOR TYPE II BARRICADES, VERTICAL PANELS OR DRUMS.
 2. A MINIMUM OF FIVE (5) CHANNELIZING DEVICES SHALL BE PLACED THRU THIS AREA. ADDITIONAL DRUMS MAY BE USED TO CLOSE THE SHOULDER WITH THE APPROVAL OF THE ENGINEER.
 3. DISTANCE 2S IS EQUAL TO TWICE THE POSTED SPEED LIMIT IN FEET.
 4. THIS TEMPORARY TRAFFIC CONTROL ZONE APPLICATION MAY BE USED AS AN ALTERNATE TRAFFIC CONTROL PLAN TO A STANDARD LANE CLOSURE WITH FLAGGERS WHEN THE FOLLOWING CONDITIONS EXIST AND WITH APPROVAL OF THE ENGINEER.
 - (A) TRAFFIC VOLUME IS SUCH THAT SUFFICIENT GAPS EXIST FOR TRAFFIC THAT MUST YIELD.
 - (B) DRIVERS FROM BOTH DIRECTIONS MUST BE ABLE TO SEE APPROACHING TRAFFIC THROUGH AND BEYOND THE WORK AREA.

- KEY:
- SIGN
 - CHANNELIZING DEVICE
 - ▨ WORK AREA
 - ▲▲▲ TYPE III BARRICADE
 - S = POSTED SPEED LIMIT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUGGESTED TRAFFIC CONTROL-
TWO-WAY, ONE LANE TRAFFIC
WITH YIELD SIGNS

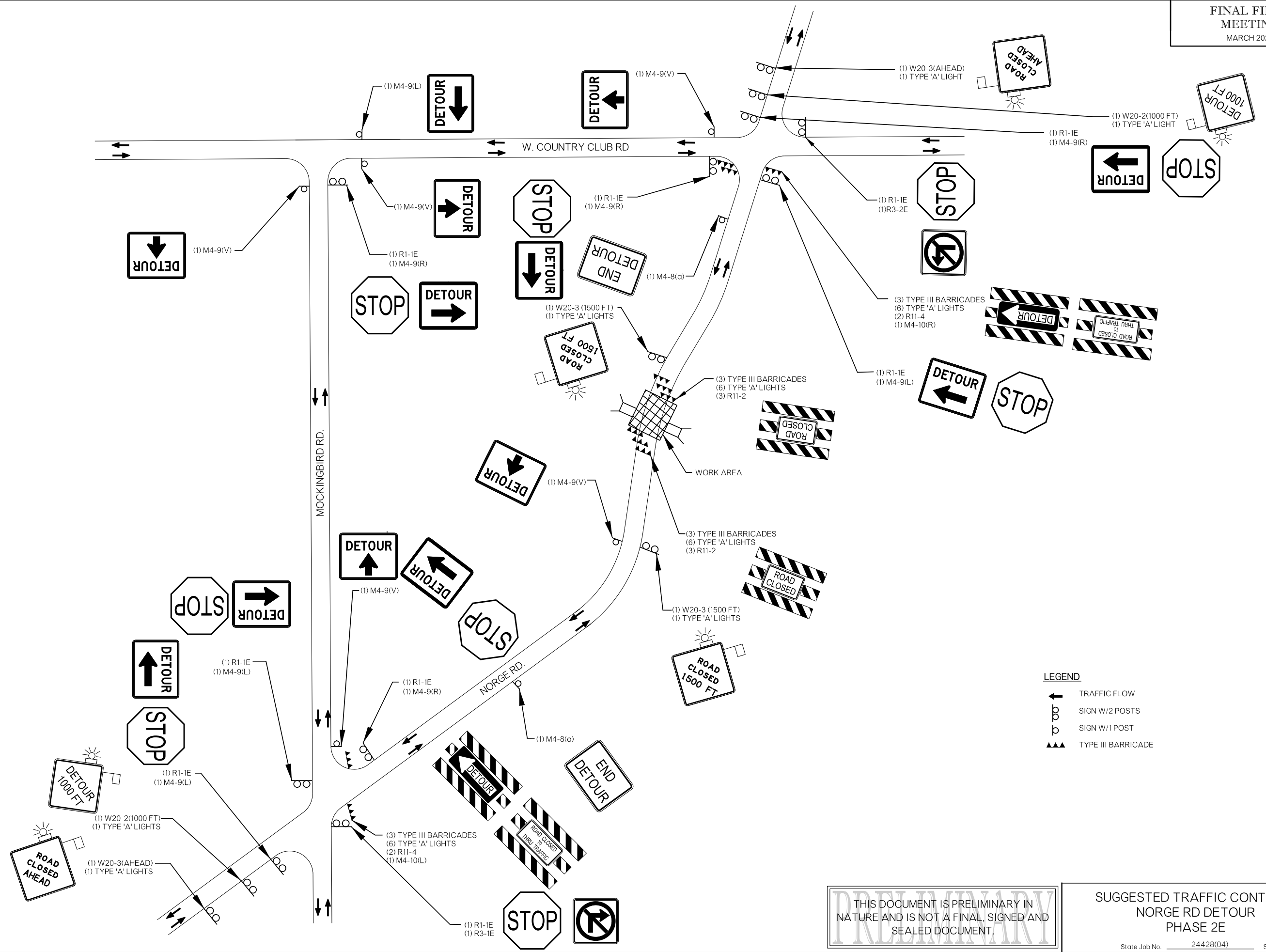


LEGEND

- ↑ TRAFFIC FLOW
- SIGN W/2 POSTS
- SIGN W/1 POST
- ▲ TYPE III BARRICADE

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

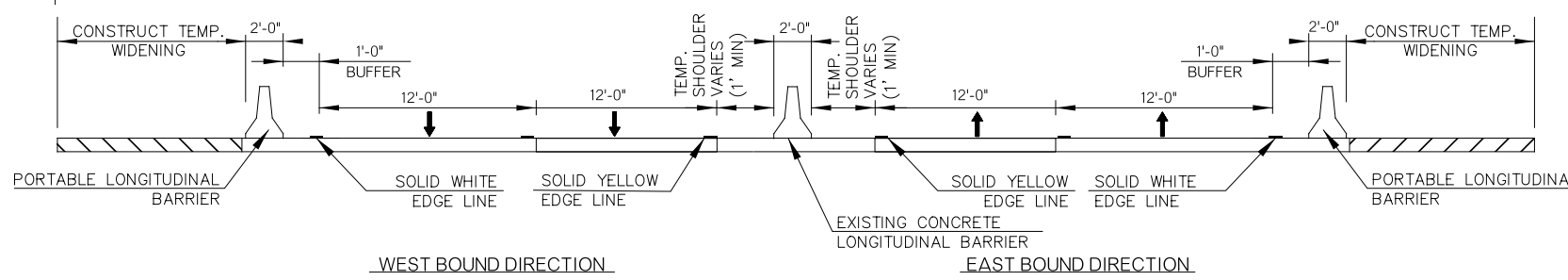
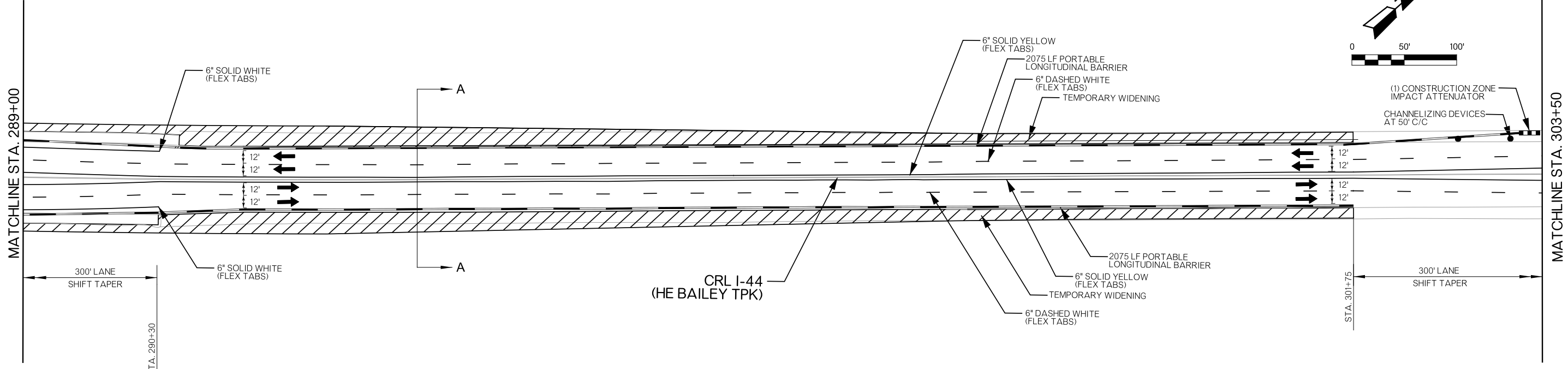
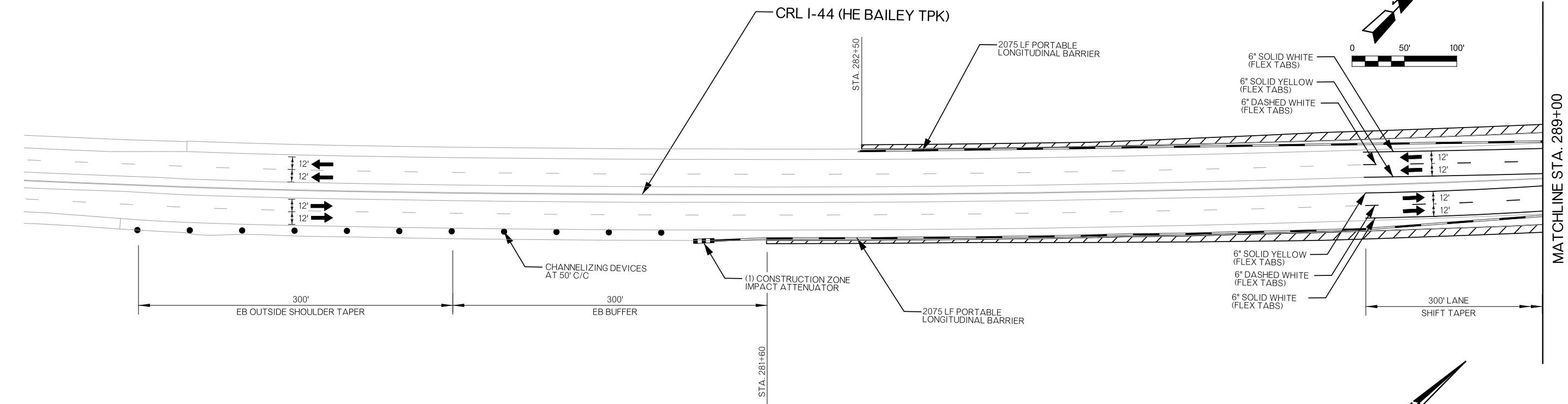
SUGGESTED TRAFFIC CONTROL- IDAHO AVE DETOUR PHASE 1G



LEGEND
← TRAFFIC FLOW
b SIGN W/2 POSTS
b SIGN W/1 POST
▲▲ TYPE III BARRICADE

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUGGESTED TRAFFIC CONTROL-
NORGE RD DETOUR
PHASE 2E



LEGEND

- ← FLOW OF TRAFFIC
- ▬ CZ IMPACT ATTENUATOR
- ▬ PLB (PORT. LONG. BARR)
- ▨ WORK AREA
- CHANNELIZING DEVICE

NOTES:

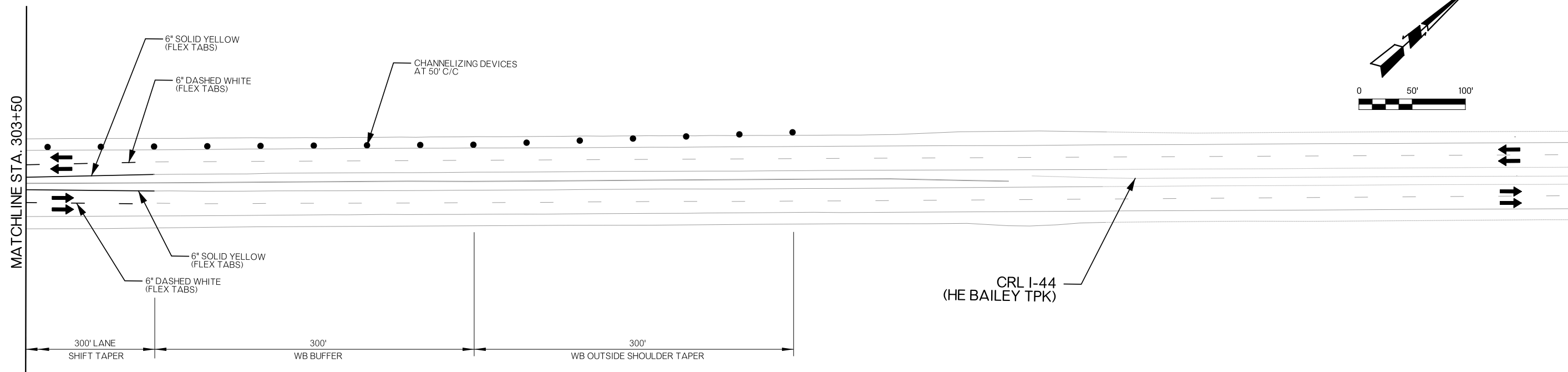
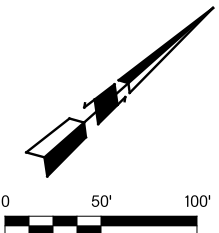
- SEE ADVANCE SIGNING SHEETS FOR ADDITIONAL SIGNS REQUIRED ON I-44.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUGGESTED TRAFFIC CONTROL - I-44 (HE BAILEY TPK) PHASES 3B, 3C

3/13/2020

FINAL FIELD MEETING
MARCH 2020



CRL I-44
(HE BAILEY TPK)

LEGEND

- ← FLOW OF TRAFFIC
- CZ IMPACT ATTENUATOR
- PLB (PORT. LONG. BARR)
- WORK AREA
- CHANNELIZING DEVICE

NOTES:

1. SEE ADVANCE SIGNING SHEETS FOR ADDITIONAL SIGNS REQUIRED ON I-44.

THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

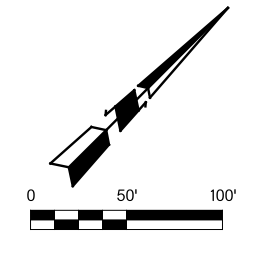
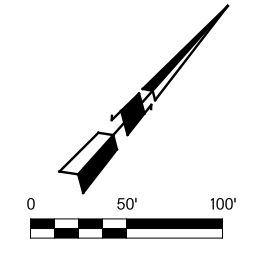
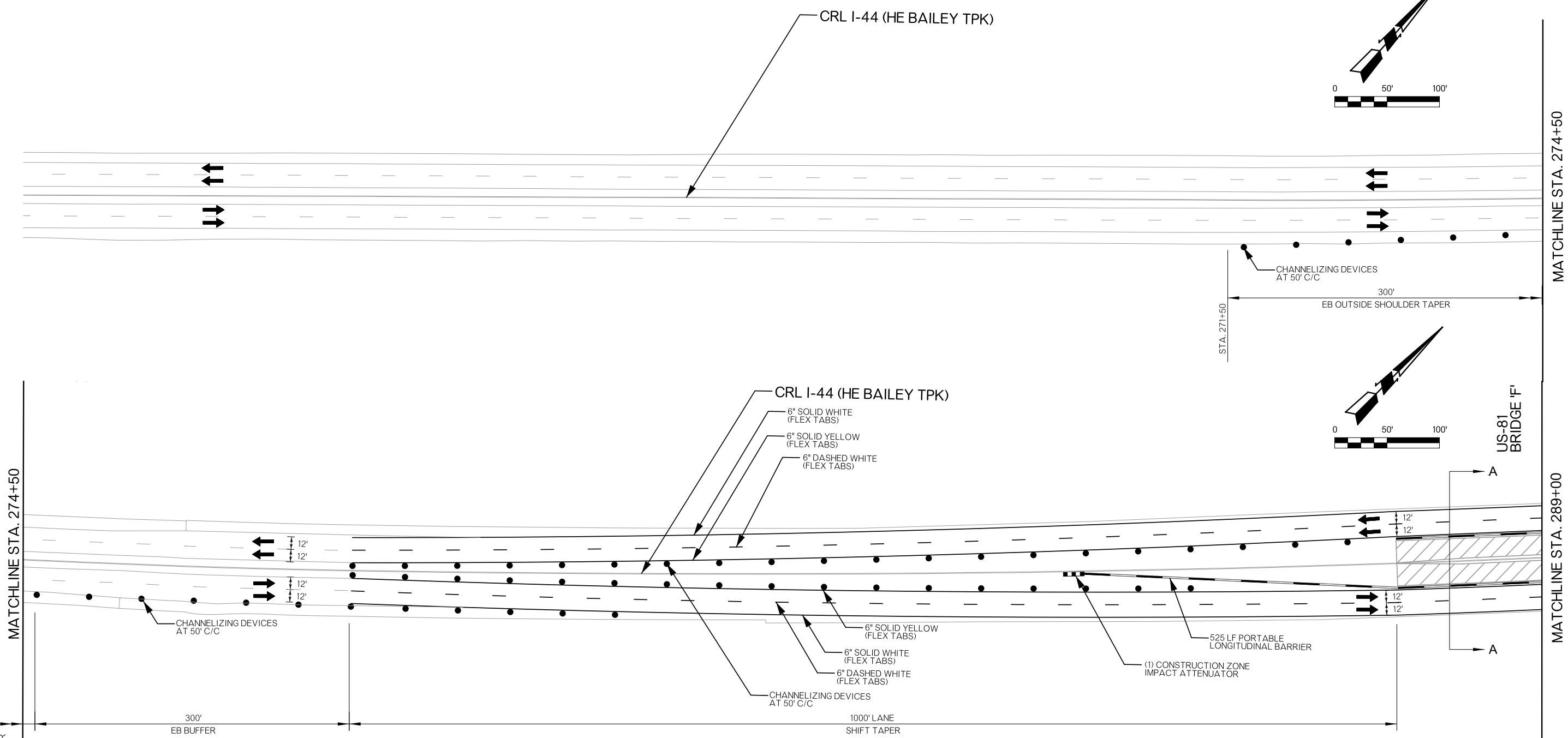
**SUGGESTED TRAFFIC CONTROL -
I-44 (HE BAILEY TPK)
PHASES 3B, 3C**

State Job No. 24428(04) Sheet No. T008

F:\2016\1001\500\016\182-A\40-Design\Microsoft\000\000\24428\04 - Grading - Draft to US-62\CV\2442804 - TRAFCON_0848003\MAC02.dgn

US-81 REALIGNMENT

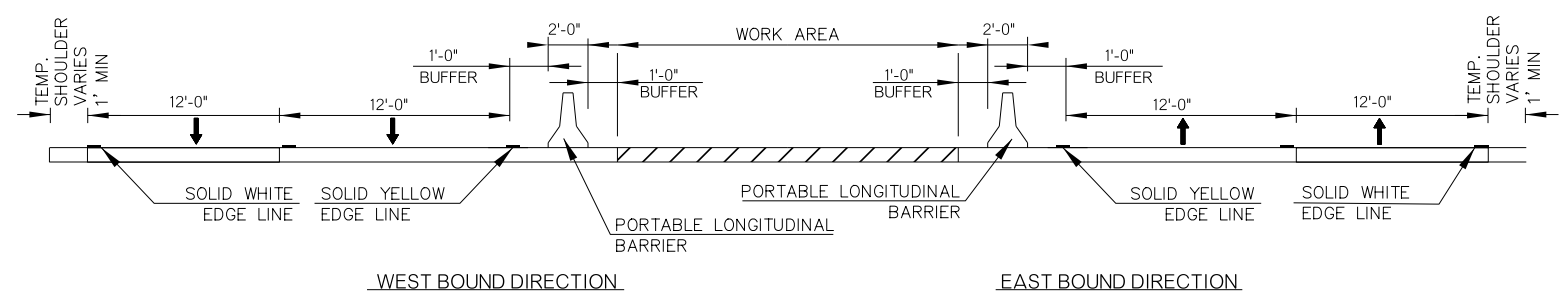
GRADY COUNTY



MATCHLINE STA. 274+50

MATCHLINE STA. 274+50

MATCHLINE STA. 289+00



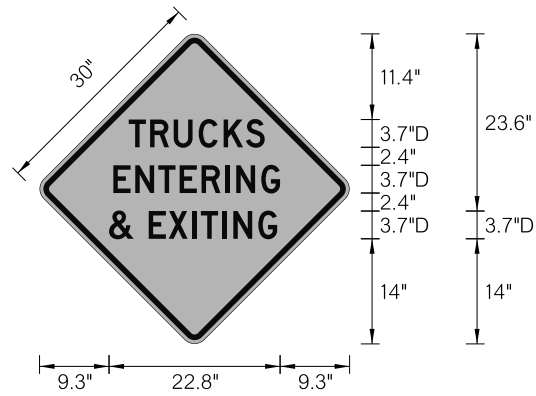
- LEGEND**
- FLOW OF TRAFFIC
 - CZ IMPACT ATTENUATOR
 - PLB (PORT. LONG. BARR)
 - WORK AREA
 - CHANNELIZING DEVICE

NOTES:
 1. SEE ADVANCE SIGNING SHEETS FOR ADDITIONAL SIGNS REQUIRED ON I-44.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUGGESTED TRAFFIC CONTROL - I-44 (HE BAILEY TPK) PHASE 3E

SIGN DETAIL N.T.S.



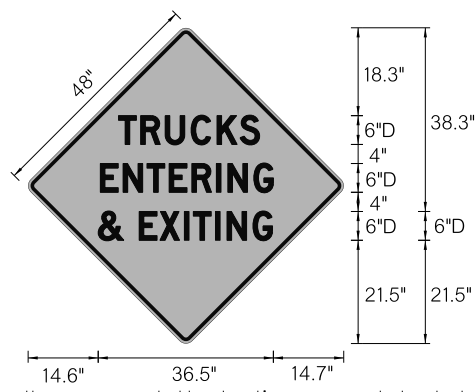
| SIGN NUMBER | SPECIAL SIGN CZ-01 |
|---------------|--------------------------------------|
| WIDTH x HGHT. | 30" X 30" |
| BORDER WIDTH | 0.75" |
| CORNER RADIUS | 1.375" |
| MOUNTING | GROUND |
| SIGN AREA | 6.25 SQ. FT. |
| BACKGROUND | TYPE: REFLECTIVE COLOR: ORANGE |
| LEGEND/BORDER | TYPE: NON-REFLECTIVE COLOR: BLACK |

| SYMBOL | X | Y | WID | HT |
|--------|---|---|-----|----|
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |

Dimensions are in Inches.tenths Letter locations are paneledge to lower left corner

| LETTER POSITIONS (X) | | | | | | | | | | | | | LENGTH | SERIES/SIZE |
|----------------------|------|------|------|------|------|------|------|--|--|--|--|--|--------|-------------|
| T | R | U | C | K | S | | | | | | | | | D 2000 |
| 11.9 | 14.7 | 17.9 | 21.2 | 24.6 | 27.5 | | | | | | | | 18.1 | |
| E | N | T | E | R | I | N | G | | | | | | | D 2000 |
| 9.8 | 12.8 | 15.8 | 18.7 | 21.6 | 24.8 | 26.2 | 29.6 | | | | | | 22.2 | |
| & | E | X | I | T | I | N | G | | | | | | | D 2000 |
| 9.3 | 14.6 | 17.2 | 20.4 | 21.5 | 24.4 | 25.8 | 29.2 | | | | | | 22.4 | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |

SIGN DETAIL N.T.S.



| SIGN NUMBER | SPECIAL SIGN CZ-02 |
|-----------------|--------------------------------------|
| WIDTH x HEIGHT | 48" x 48" |
| BORDER WIDTH | 1.5" |
| CORNER RADIUS | 3" |
| MOUNTING | GROUND |
| SIGN AREA | 16.0 SQ. FT. |
| BACKGROUND | TYPE: REFLECTIVE COLOR: ORANGE |
| LEGEND / BORDER | TYPE: NON-REFLECTIVE COLOR: BLACK |

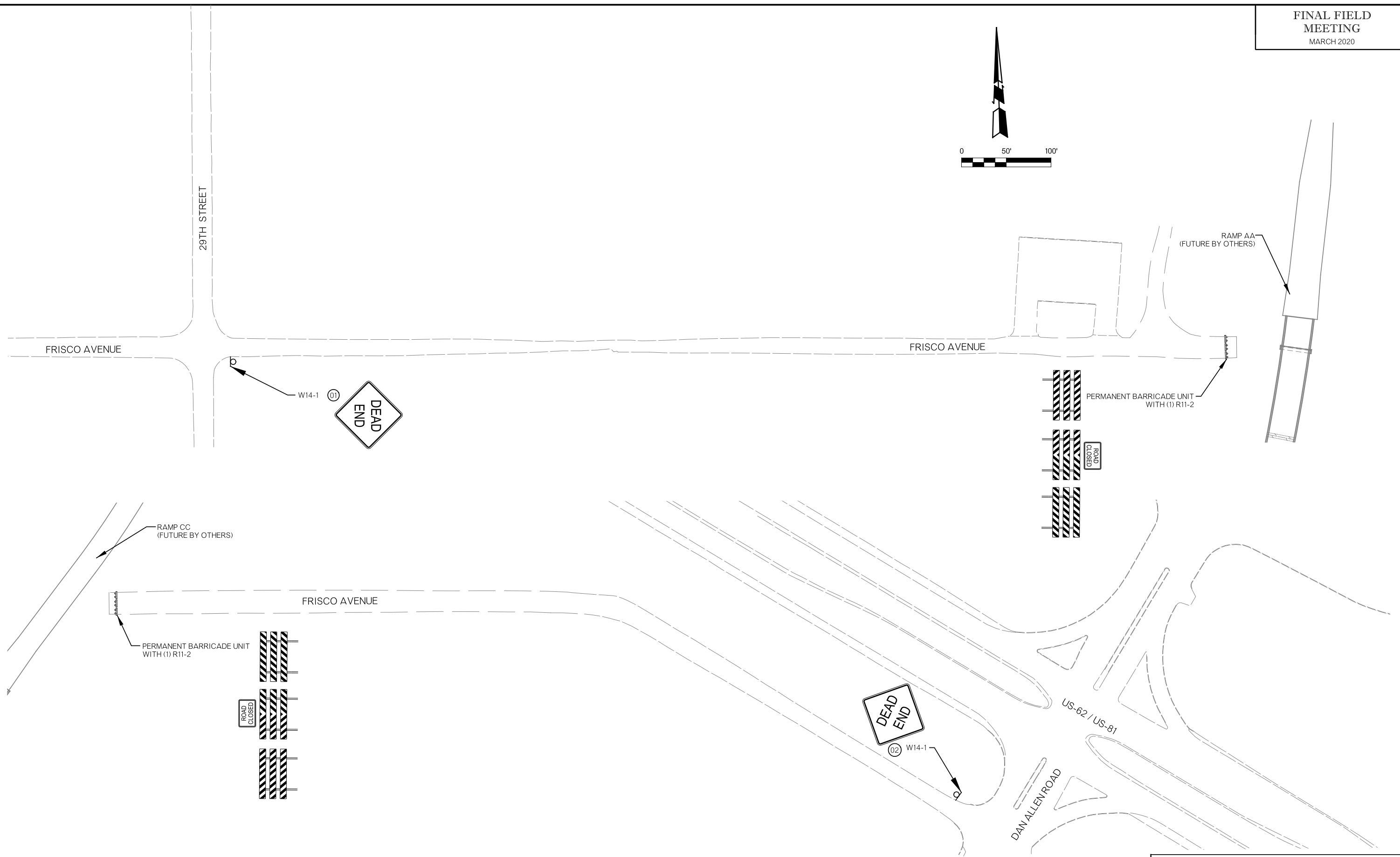
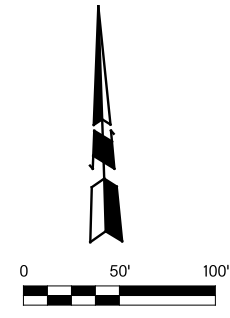
| SYMBOL | X | Y | WID | HT |
|--------|---|---|-----|----|
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |
| | | | | |

Dimensions are in Inches.tenths Letter locations are paneledge to lower left corner

| LETTER POSITIONS (X) | | | | | | | | | | | | | LENGTH | SERIES/SIZE |
|----------------------|------|------|------|------|------|------|------|--|--|--|--|--|--------|-------------|
| T | R | U | C | K | S | | | | | | | | | D 2000 |
| 18.8 | 23.5 | 28.6 | 34 | 39.4 | 44.2 | | | | | | | | 29.4 | |
| E | N | T | E | R | I | N | G | | | | | | | D 2000 |
| 15 | 19.8 | 24.8 | 29.4 | 34.1 | 39.2 | 41.6 | 47 | | | | | | 36.1 | |
| & | E | X | I | T | I | N | G | | | | | | | D 2000 |
| 14.6 | 23.1 | 27.5 | 32.6 | 34.4 | 39 | 41.4 | 46.8 | | | | | | 36.3 | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | |

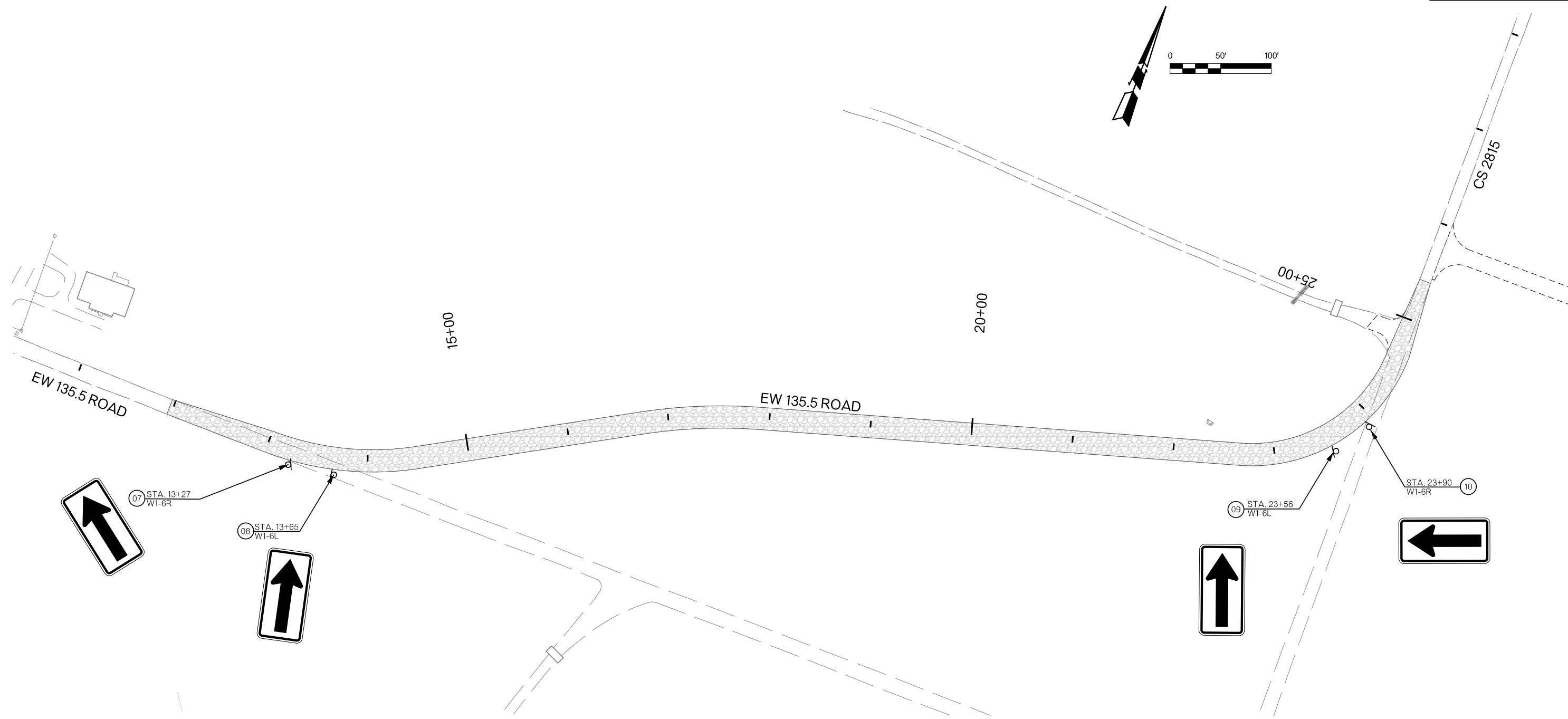
PRELIMINARY THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SUGGESTED TRAFFIC CONTROL - SPECIAL SIGN LAYOUTS



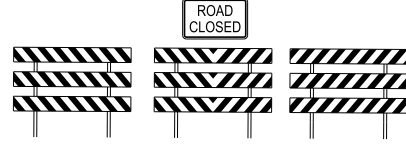
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SIGNING & STRIPING (1) - FRISCO AVENUE
State Job No. 24428(04) Sheet No. T013

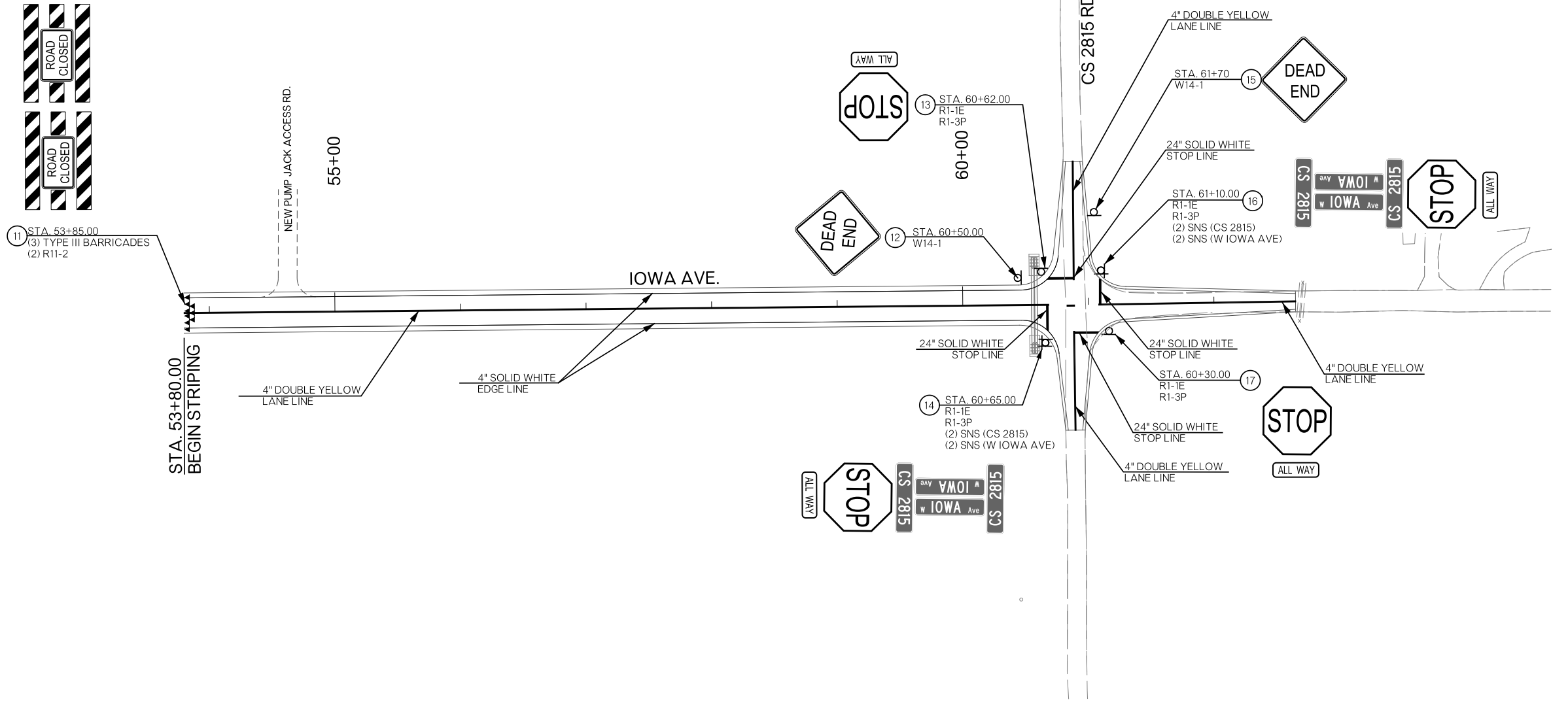
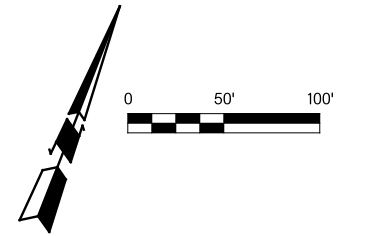


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SIGNING & STRIPING (3)- EW 135.5
State Job No. 24428(04) Sheet No. T015

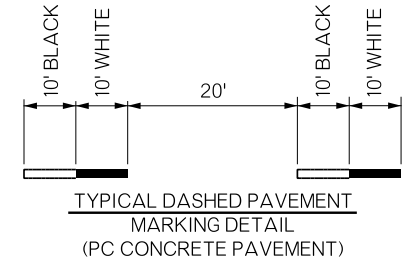
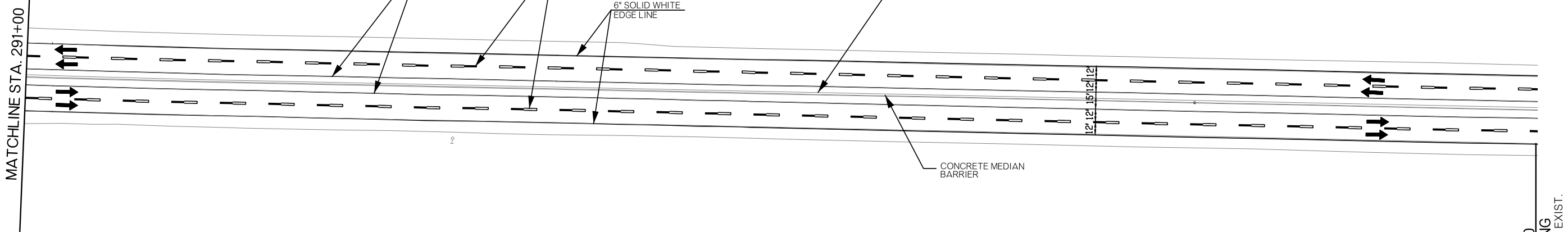
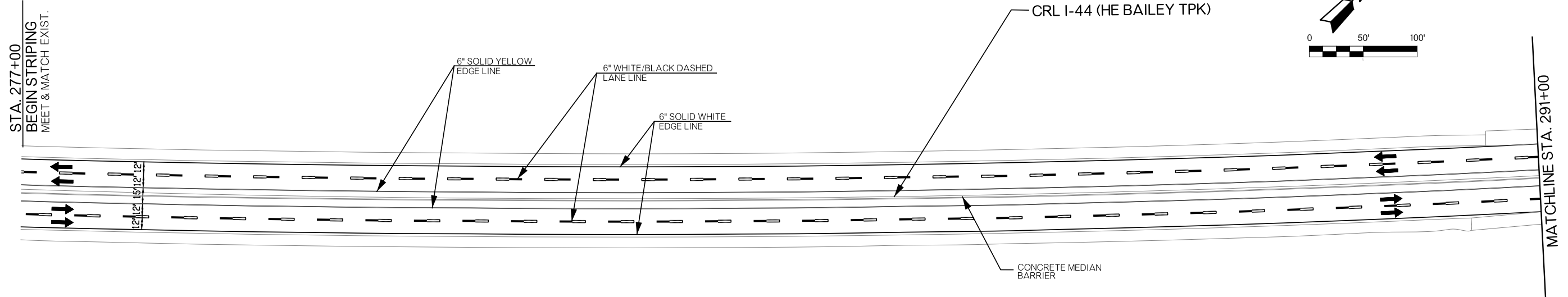
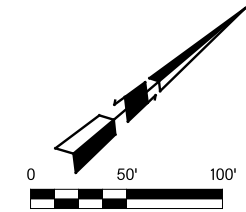
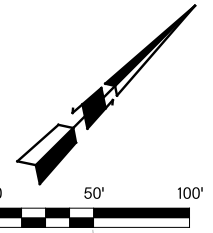


PERMANENT BARRICADE UNIT WITH (1) R11-2
(TO BE INSTALLED AT US-81 PRES. R/W)



PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

**SIGNING & STRIPING (4)-
IOWA AVE.**

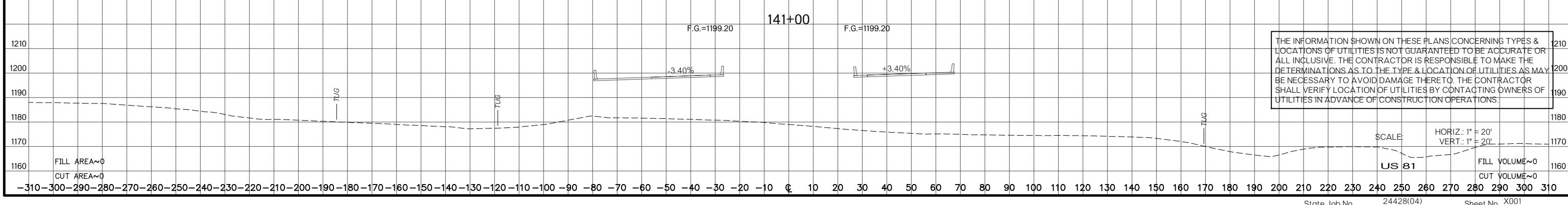
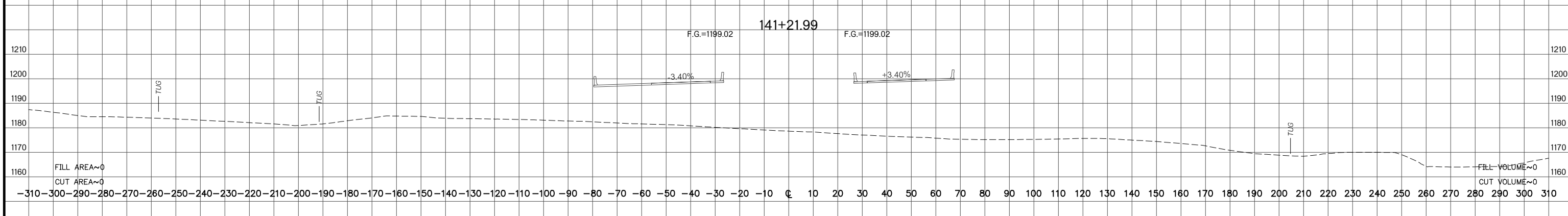
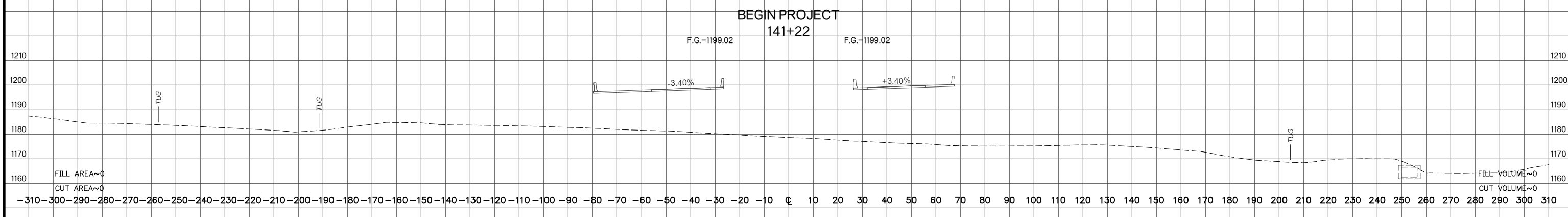
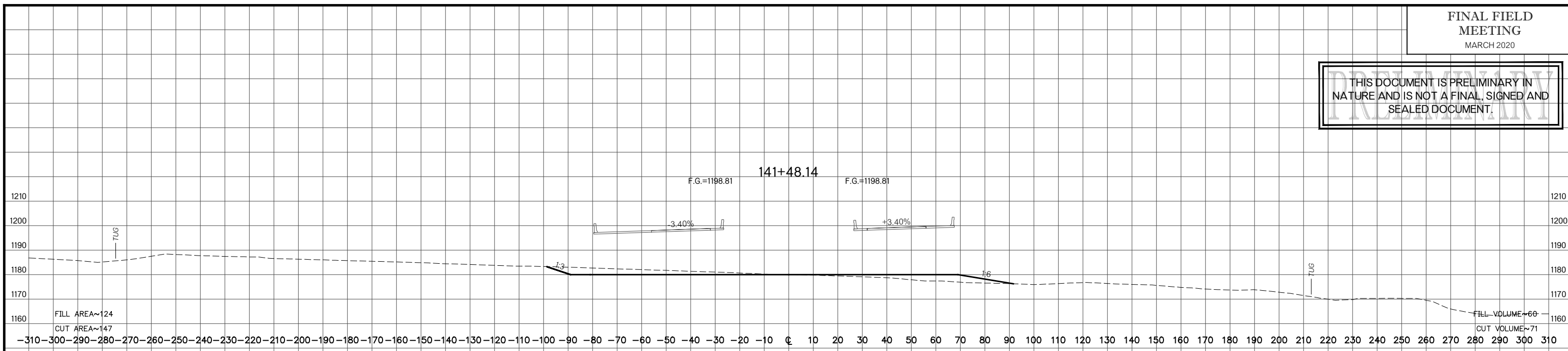


NOTES:
 STRIPING TO BE REPLACED AS DIRECTED BY THE ENGINEER TO RESTORE ORIGINAL STRIPING LAYOUT WHERE STRIPING IS REMOVED DUE TO TEMPORARY TRAFFIC CONTROL.
 FOR WHITE/BLACK DASHED AND/OR DOTTED STRIPING, WHITE STRIPE SHALL ALWAYS BE LOCATED IMMEDIATELY UPSTREAM OF BLACK STRIPE IN ACCORDANCE WITH ODOT STANDARDS.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

SIGNING & STRIPING (5)-I-44 (HE BAILEY TPK)
 State Job No. 24428(04) Sheet No. T017

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

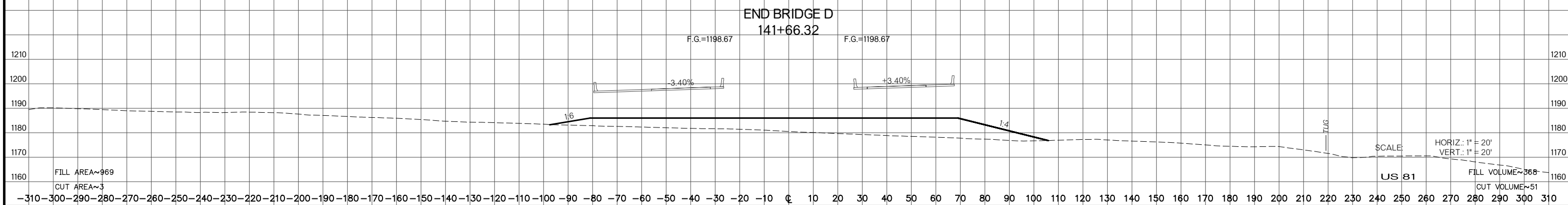
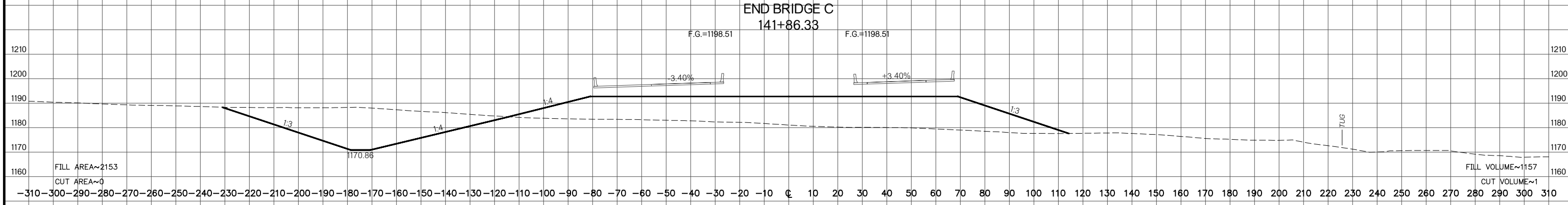
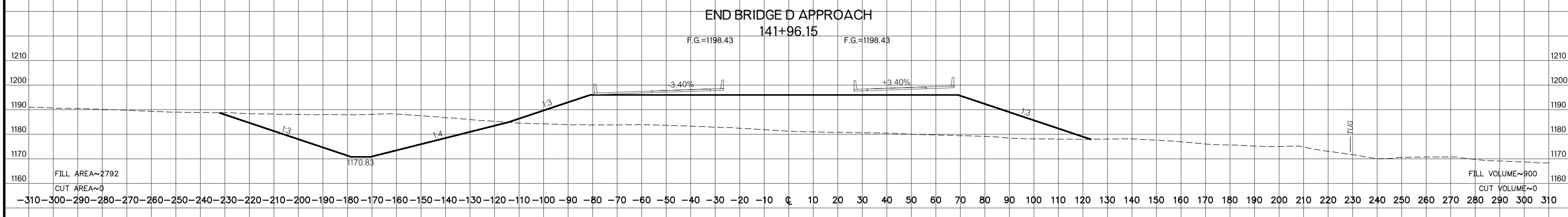
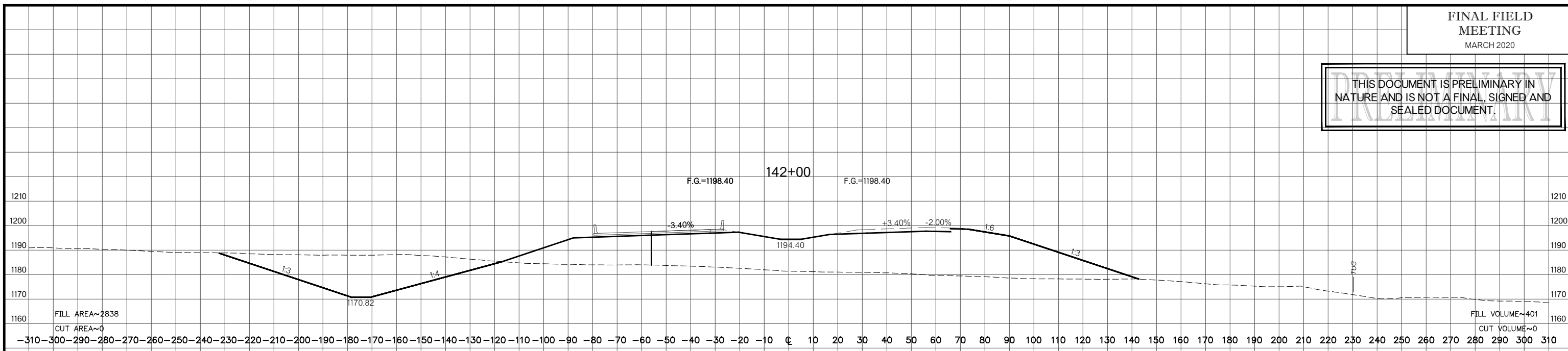


THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

SCALE
US 81
HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

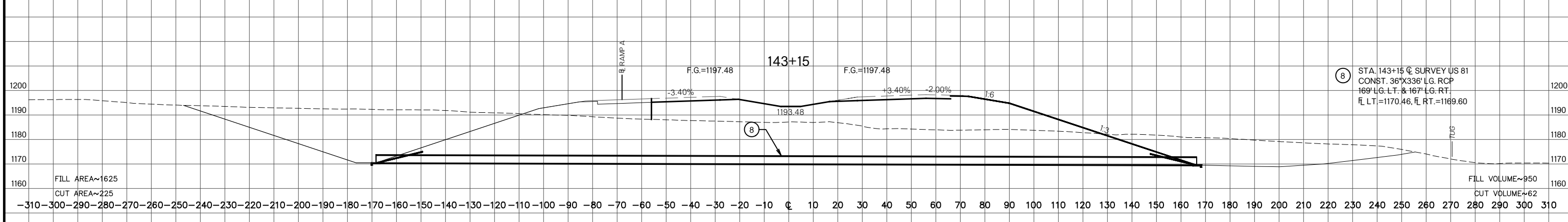
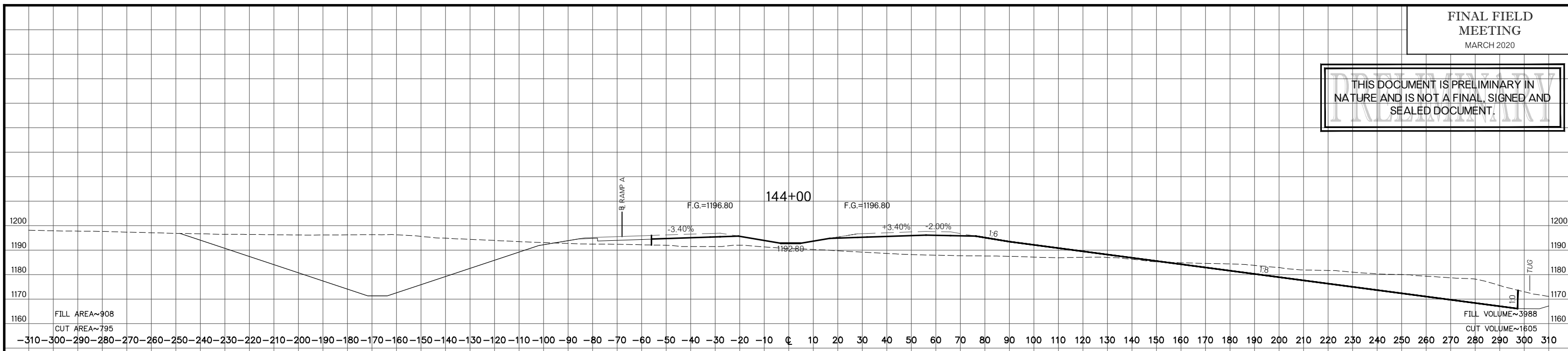
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



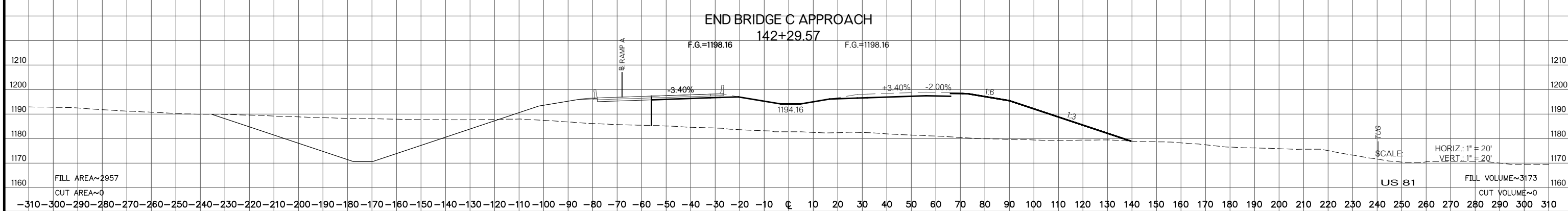
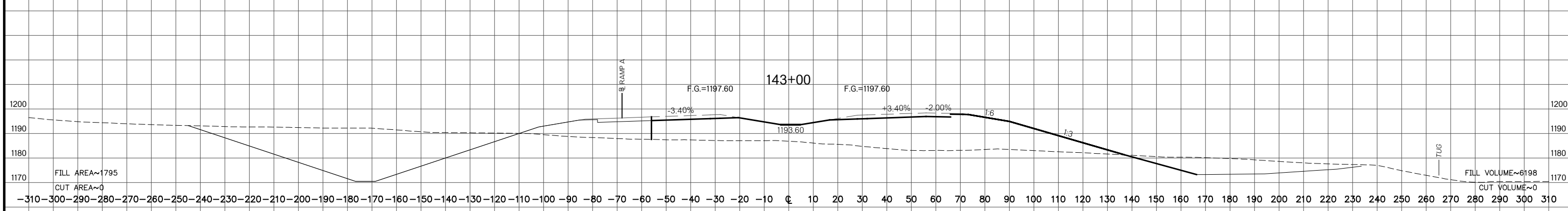
SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



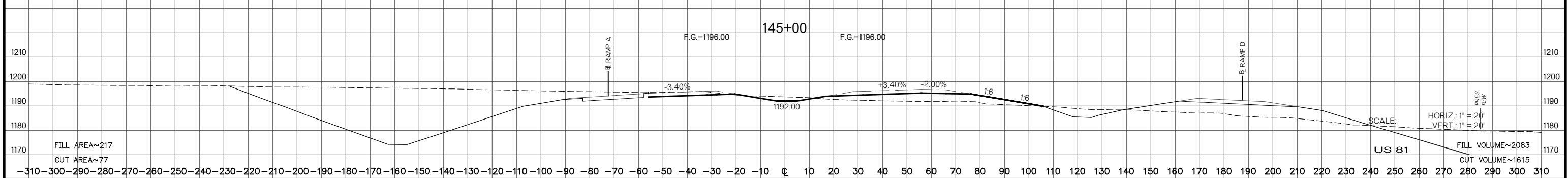
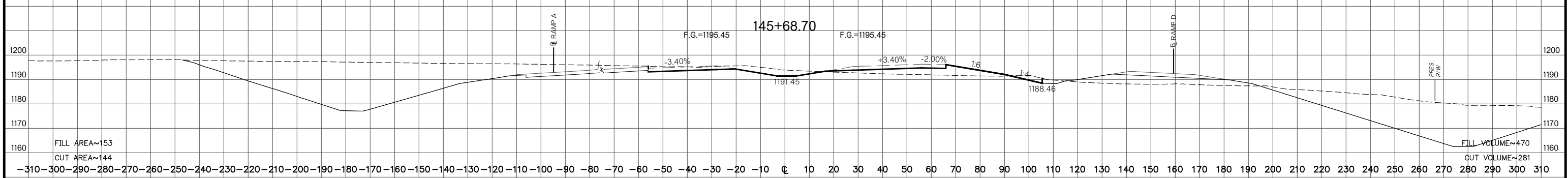
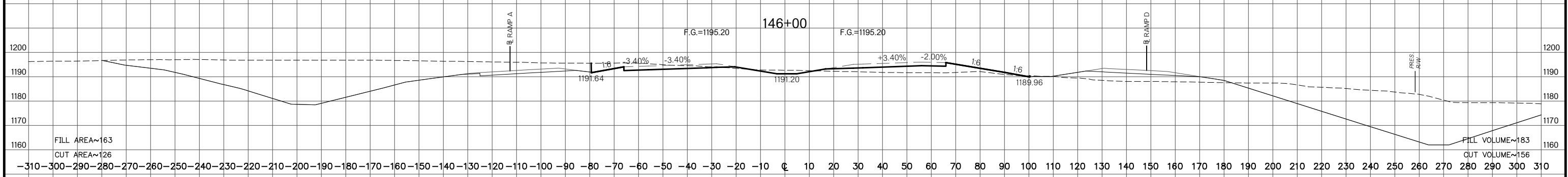
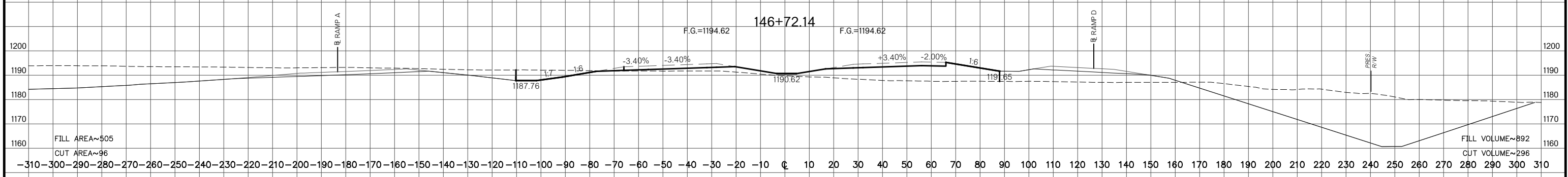
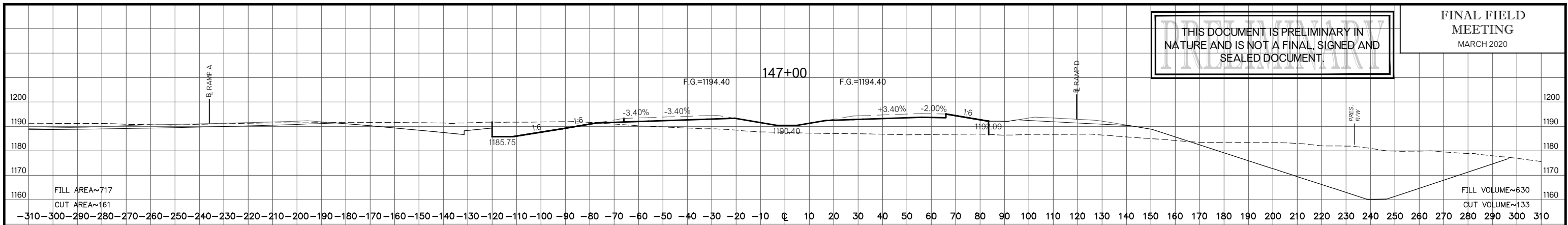
8 STA. 143+15 Q SURVEY US 81
 CONST. 36"X336' LG. RCP
 169' LG. LT. & 167' LG. RT.
 FL LT.=1170.46, FL RT.=1169.60



SCALE
 US 81
 HORIZ.: 1" = 20'
 VERT.: 1" = 20'

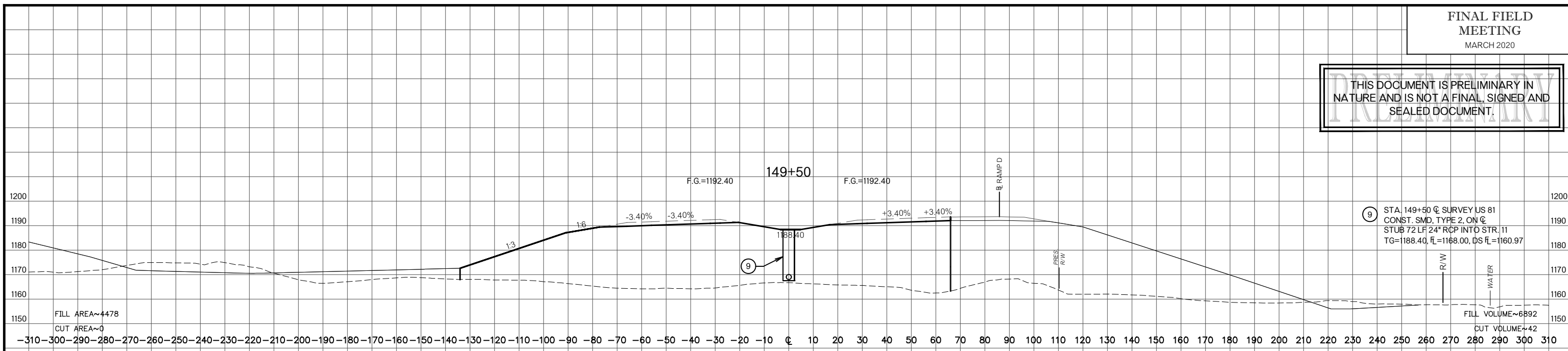
US 81 REALIGNMENT
 GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

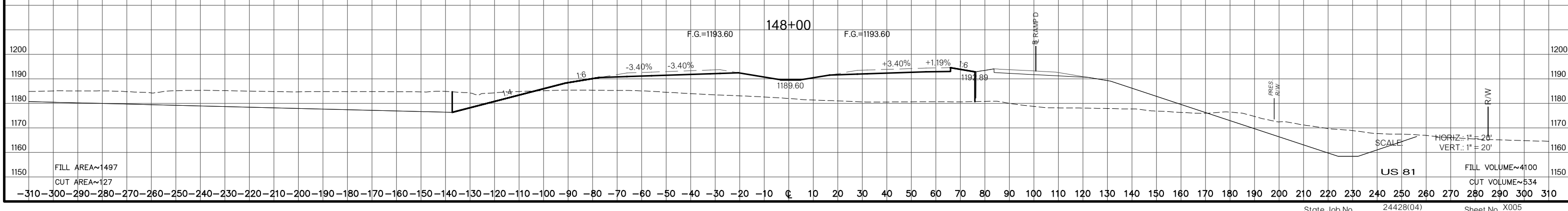
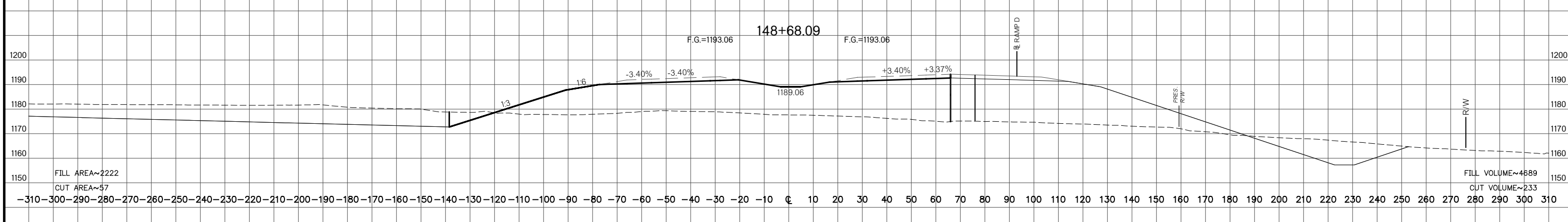
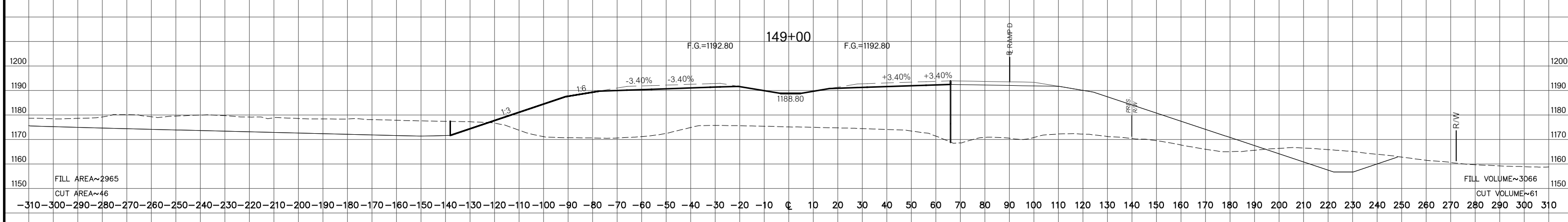


US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



9 STA. 149+50 @ SURVEY US 81
CONST. SMD, TYPE 2, ON @
STUB 72 LF 24" RCP INTO STR. 11
TG=1188.40, FL=1168.00, DS FL=1160.97

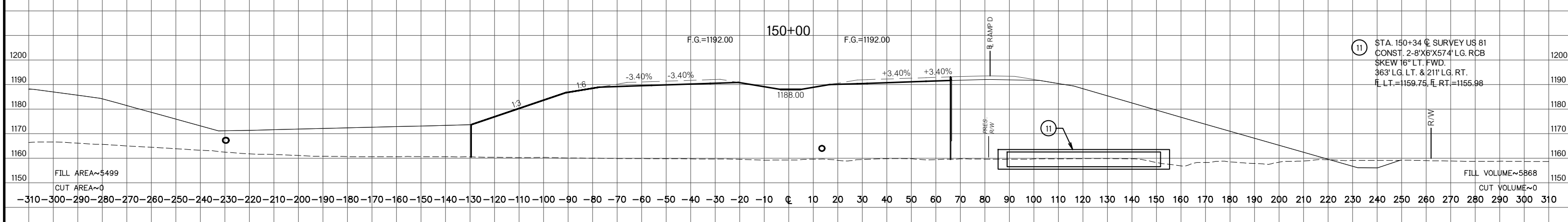
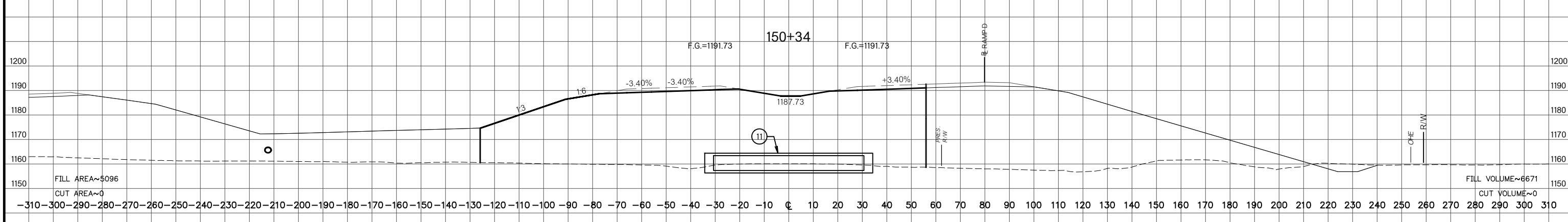
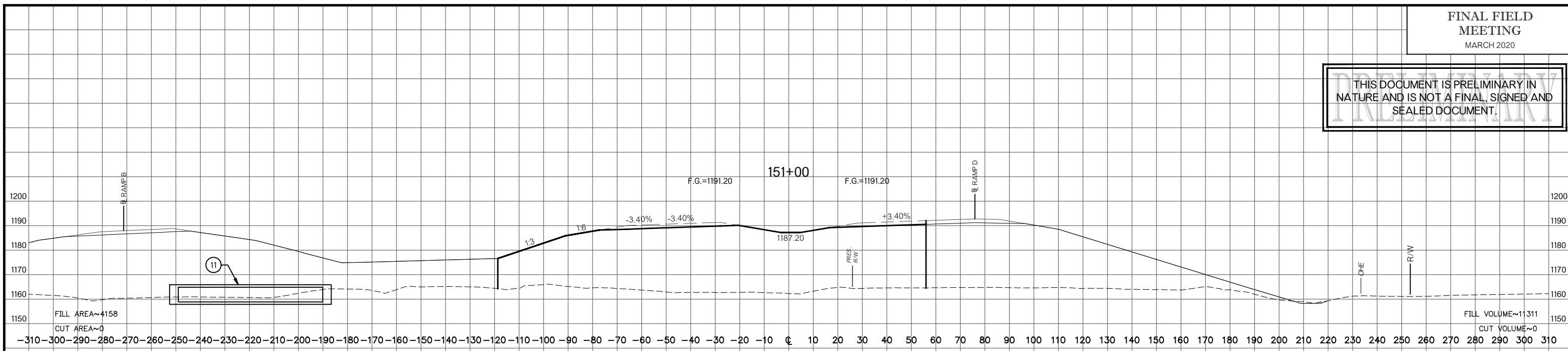


SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

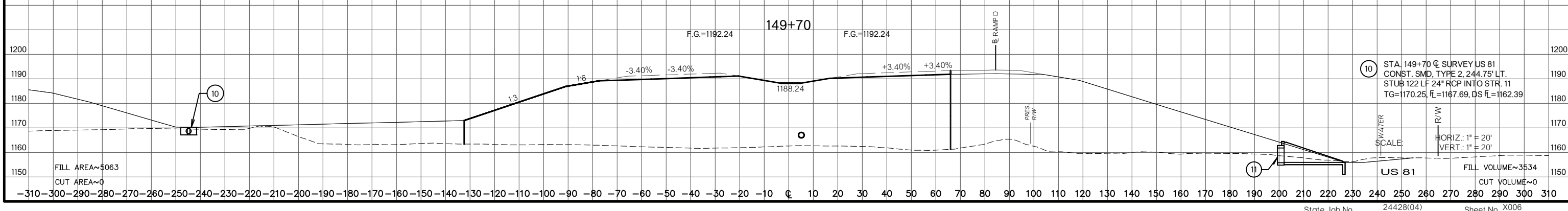
US 81
FILL VOLUME~4100
CUT VOLUME~534

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



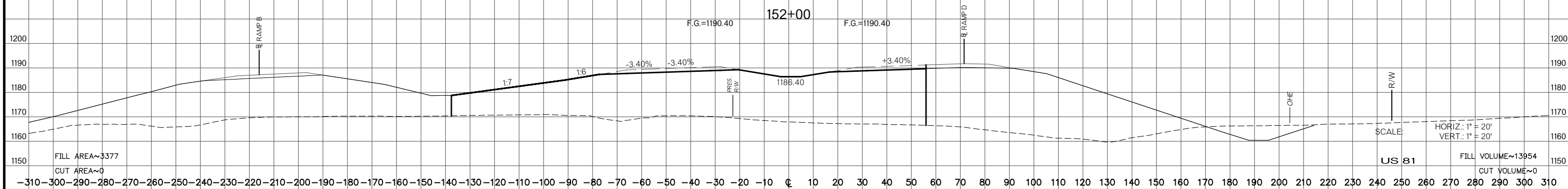
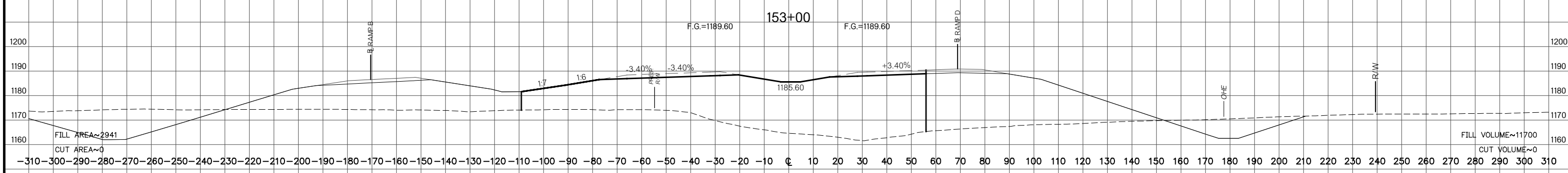
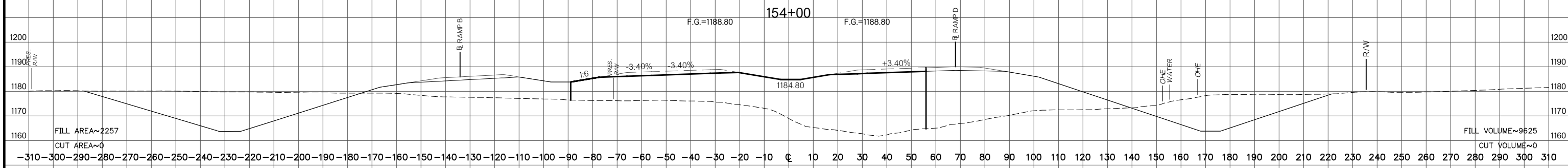
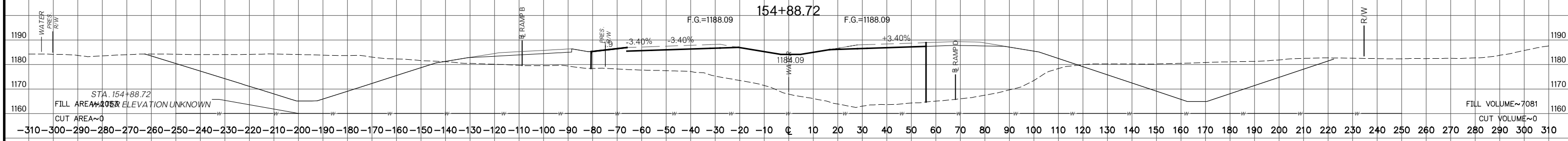
11 STA. 150+34 @ SURVEY US 81
CONST. 2-8'X6'X574' LG. RCB
SKEW 16° LT. FWD.
363' LG. LT. & 211' LG. RT.
FL. LT. = 1159.75, FL. RT. = 1155.98



10 STA. 149+70 @ SURVEY US 81
CONST. SMD. TYPE 2, 244.75' LT.
SUB 122 LF 24" RCP INTO STR. 11
TG = 1170.25, FL = 1167.69, DS FL = 1162.39

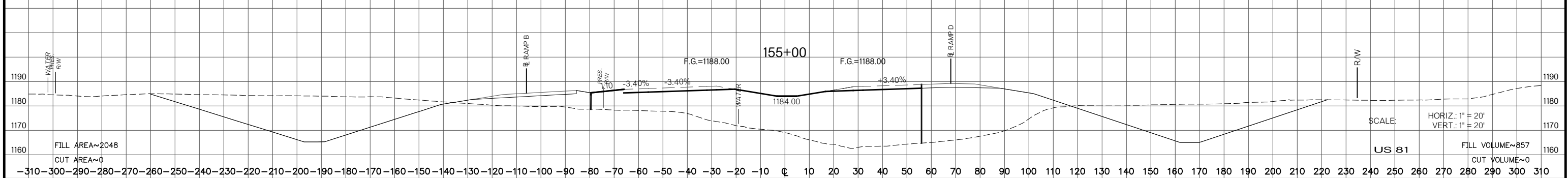
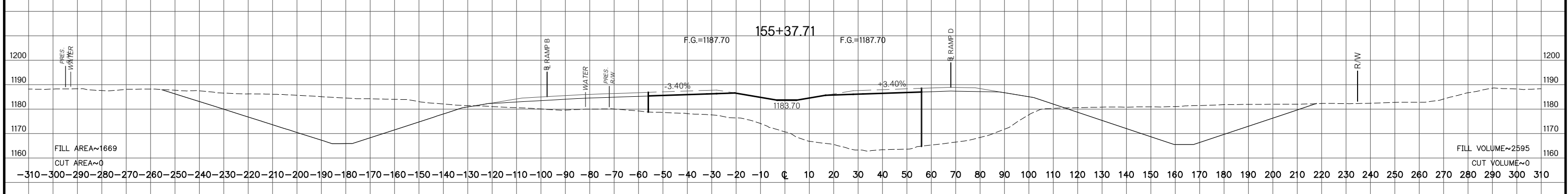
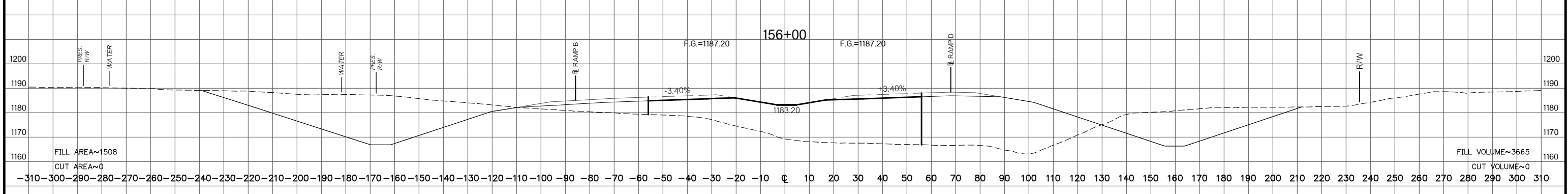
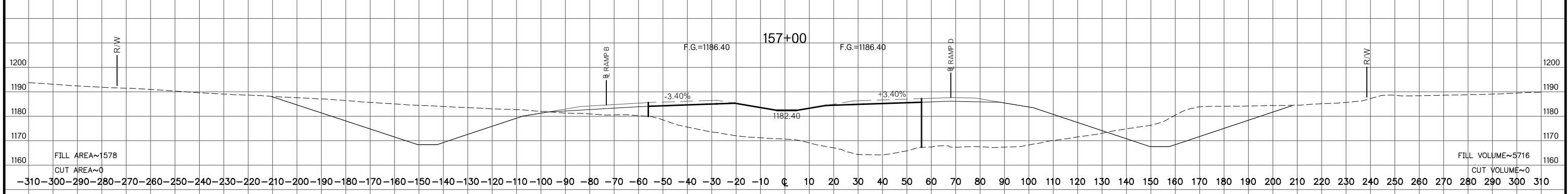
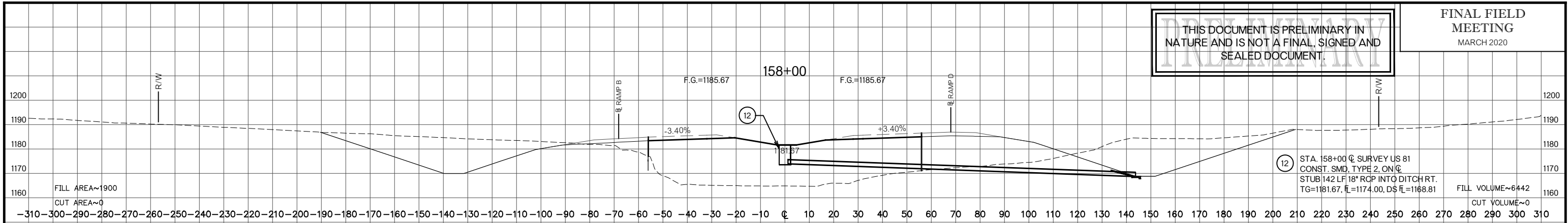
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



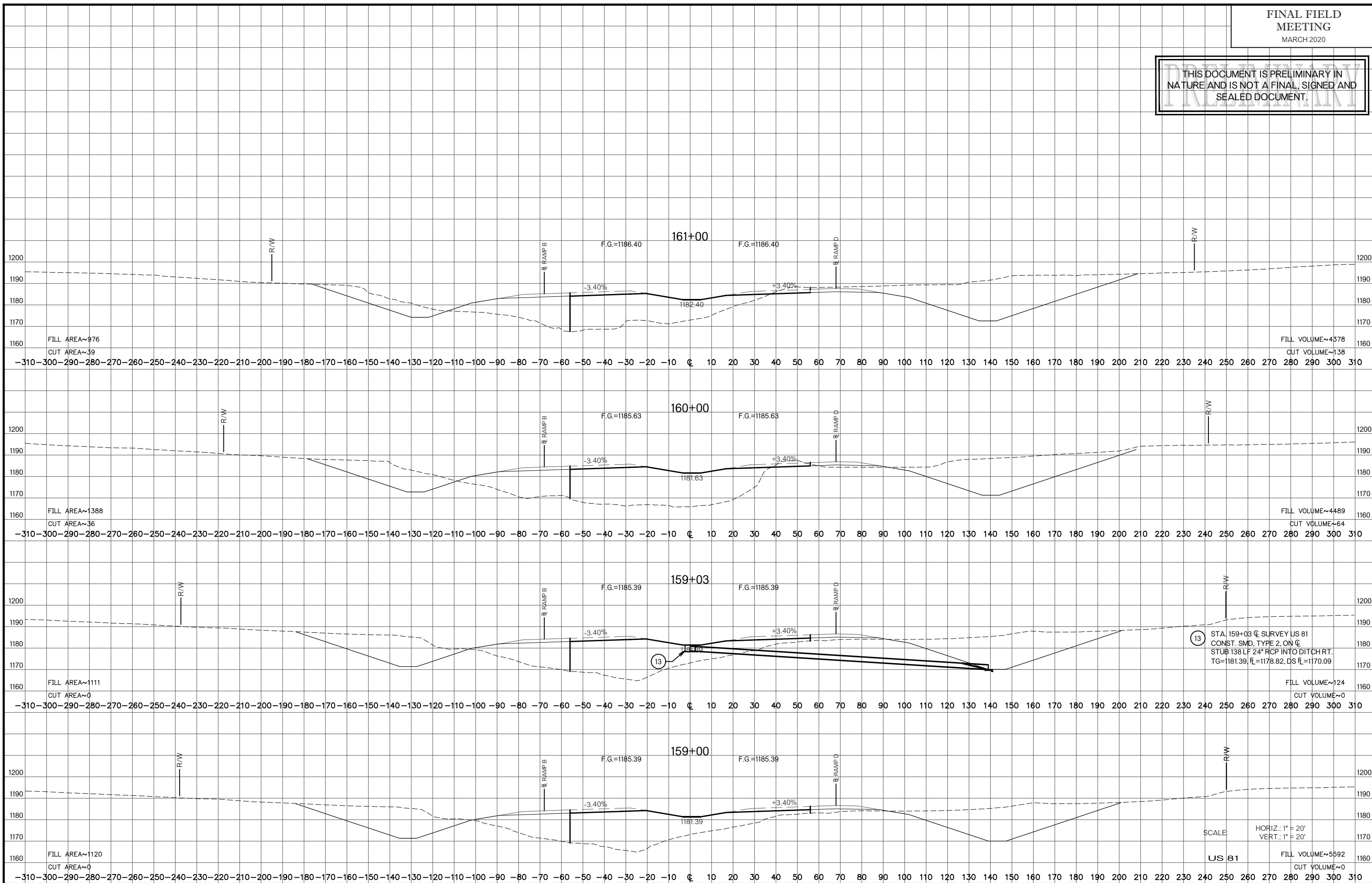
GRADY COUNTY
US 81 REALIGNMENT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

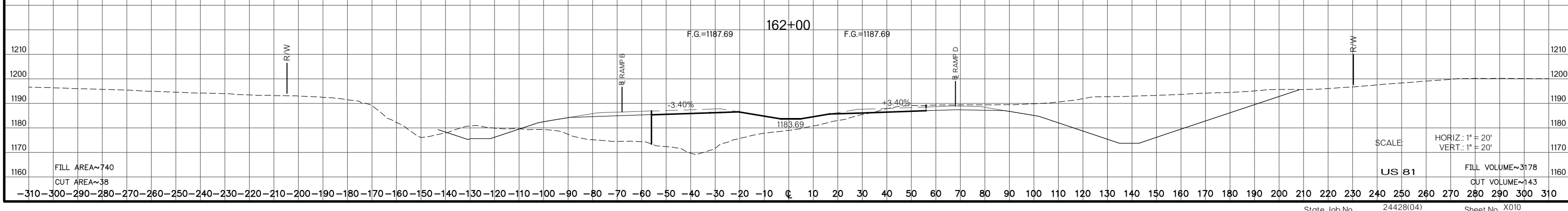
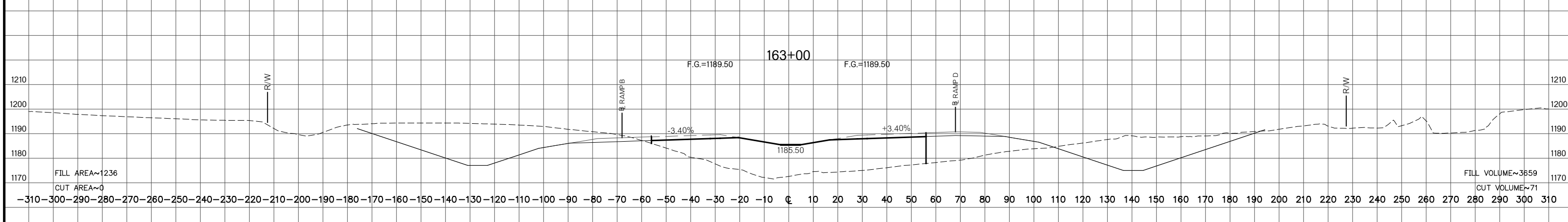
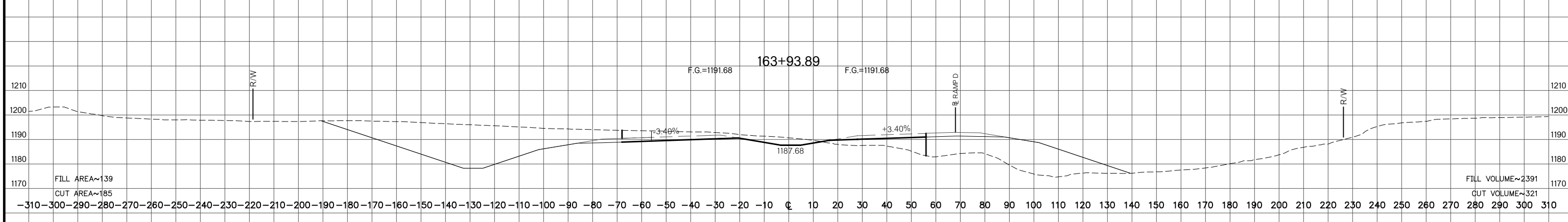
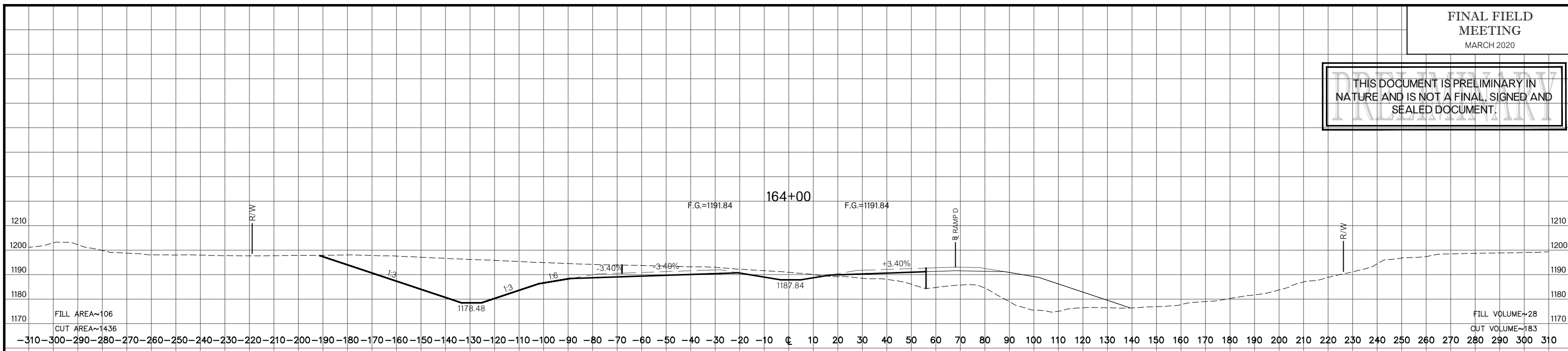


US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

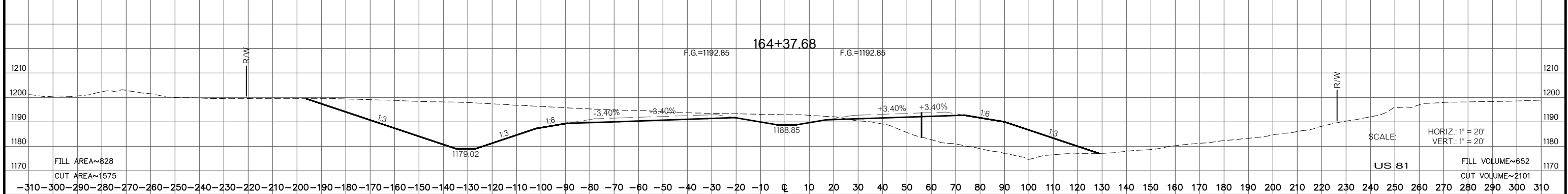
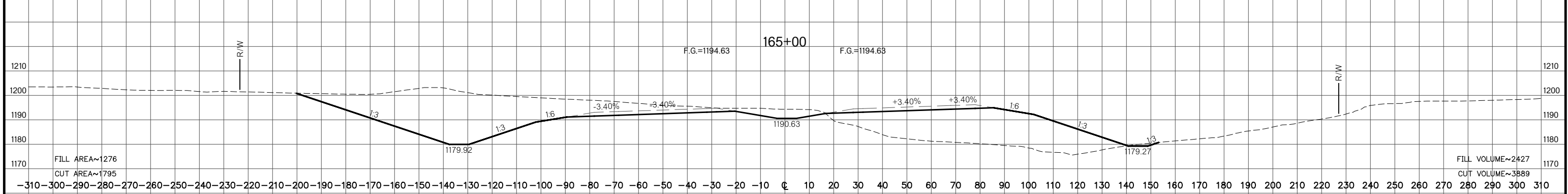
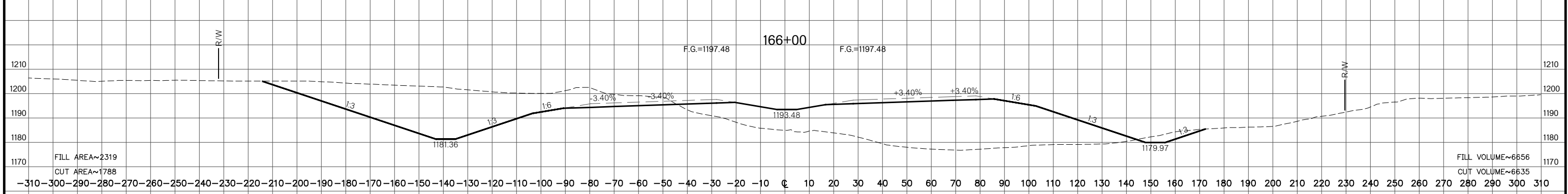
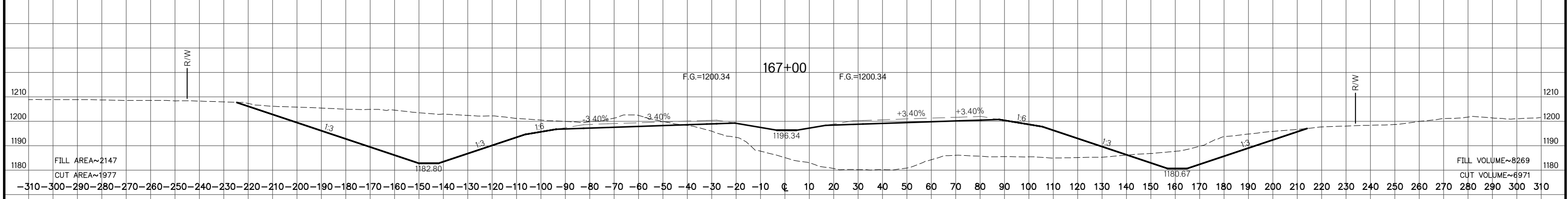
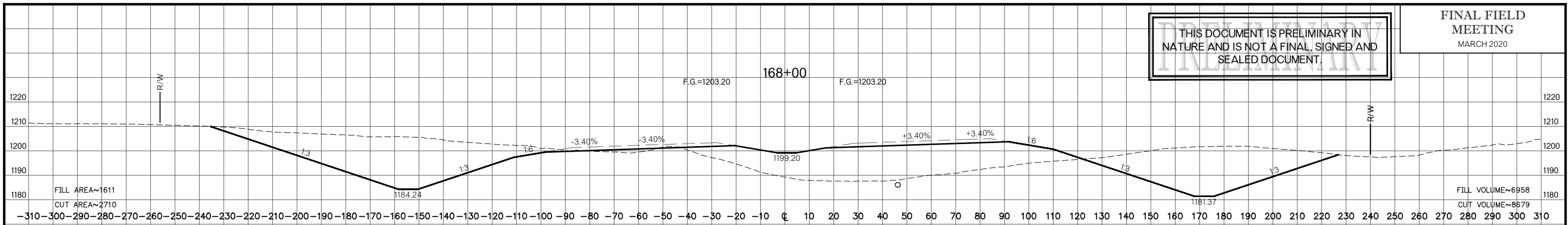


SCALE HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 FILL VOLUME~3178
CUT VOLUME~143

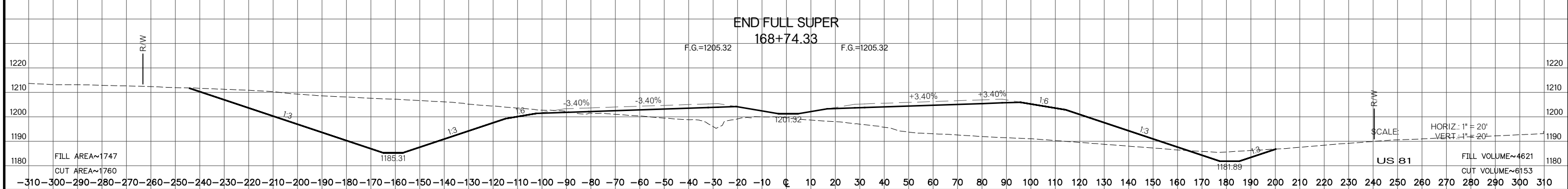
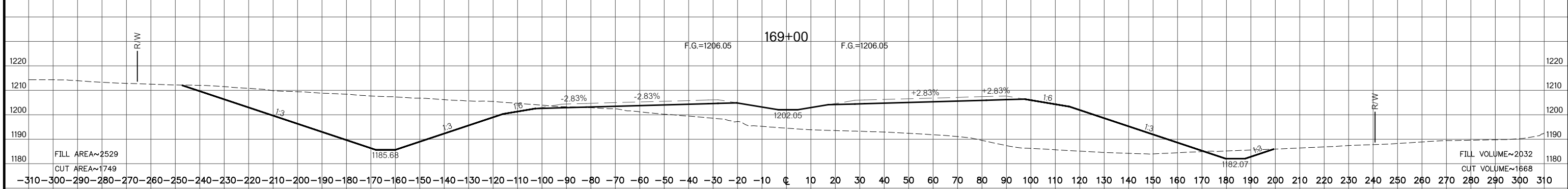
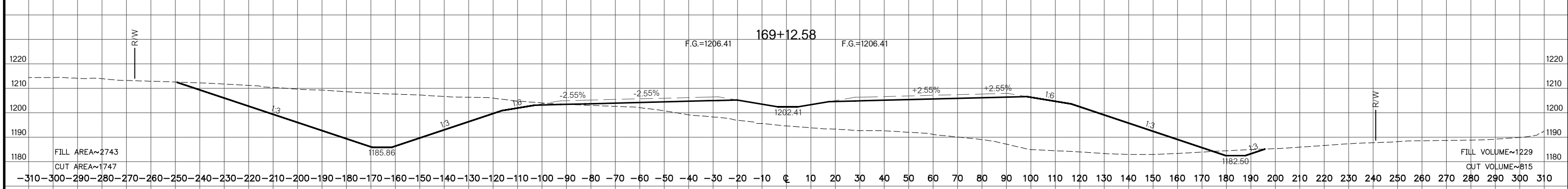
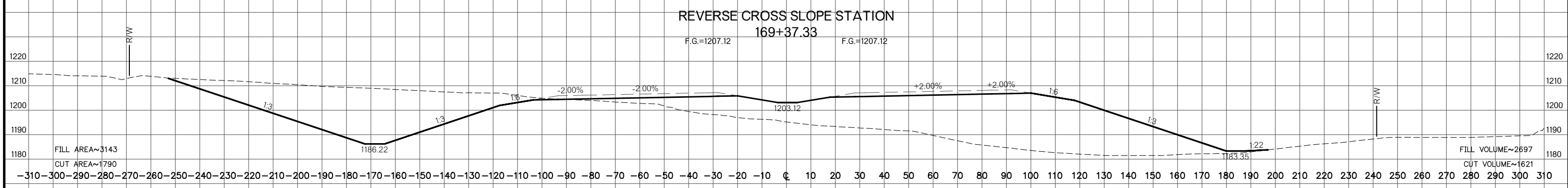
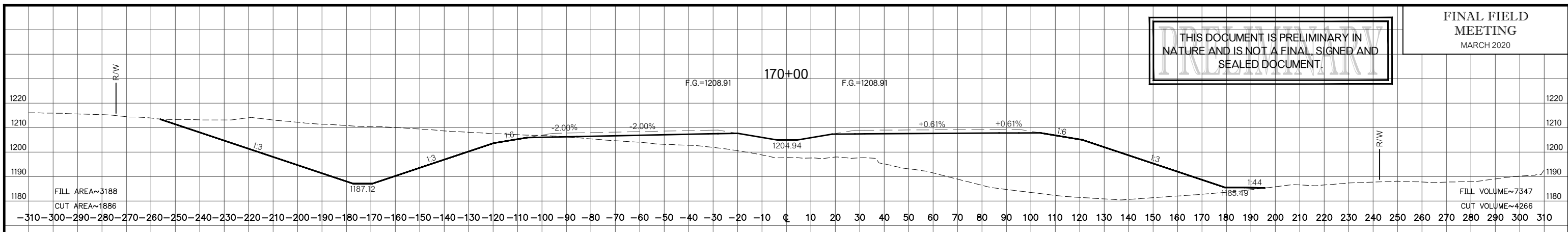
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT GRADY COUNTY

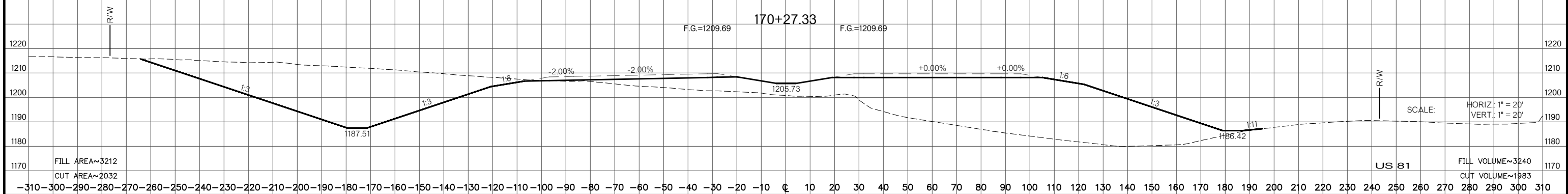
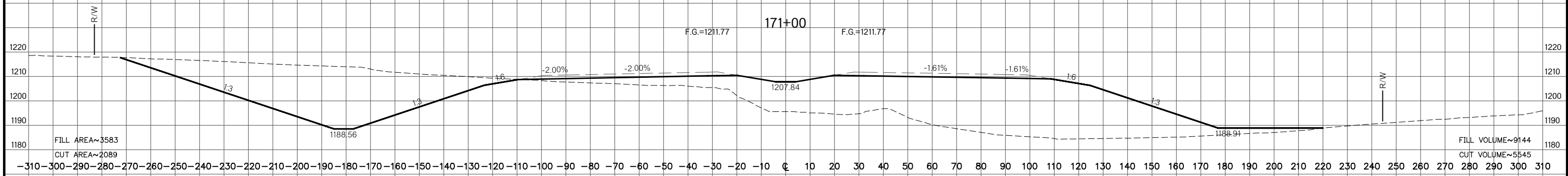
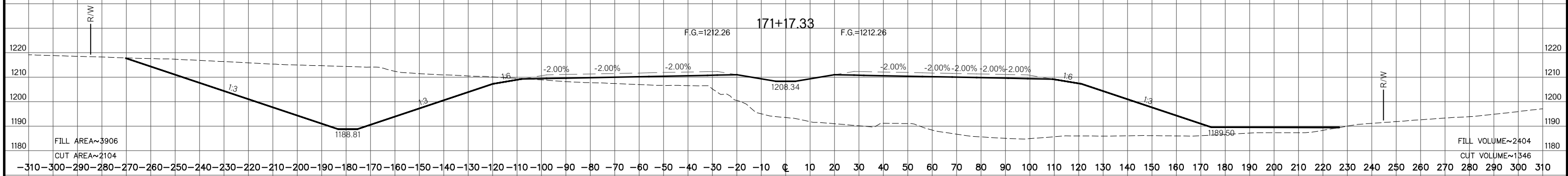
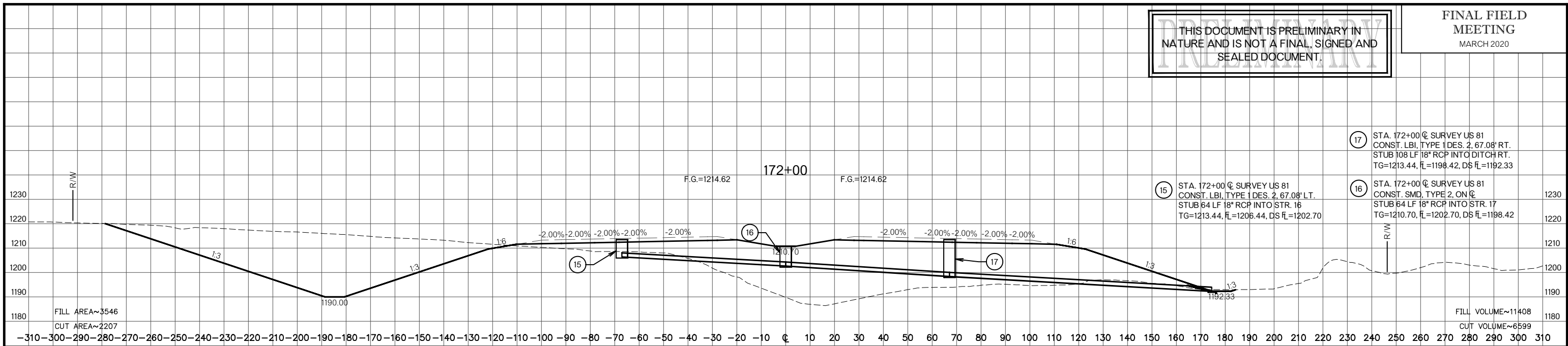
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT GRADY COUNTY

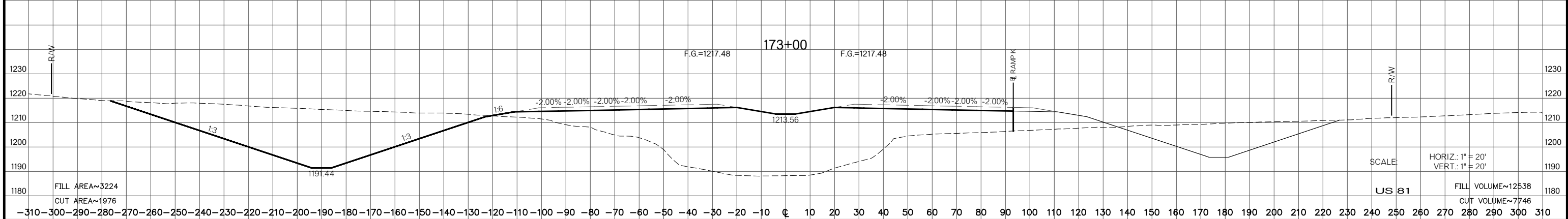
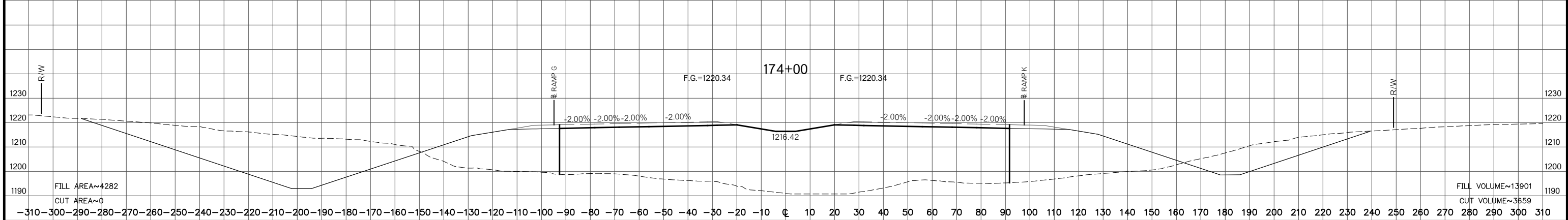
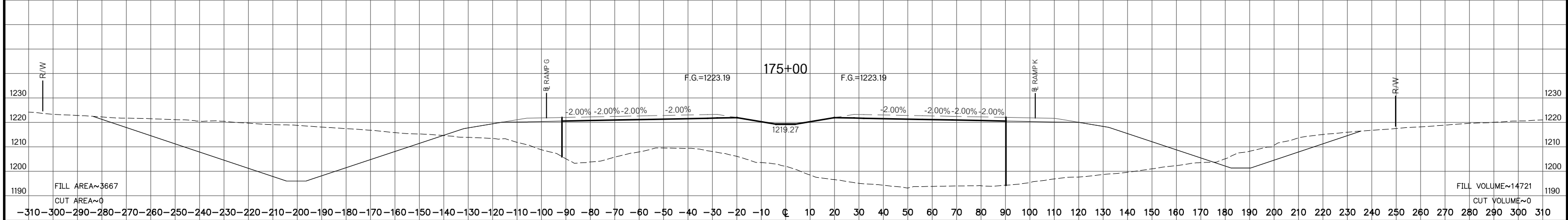
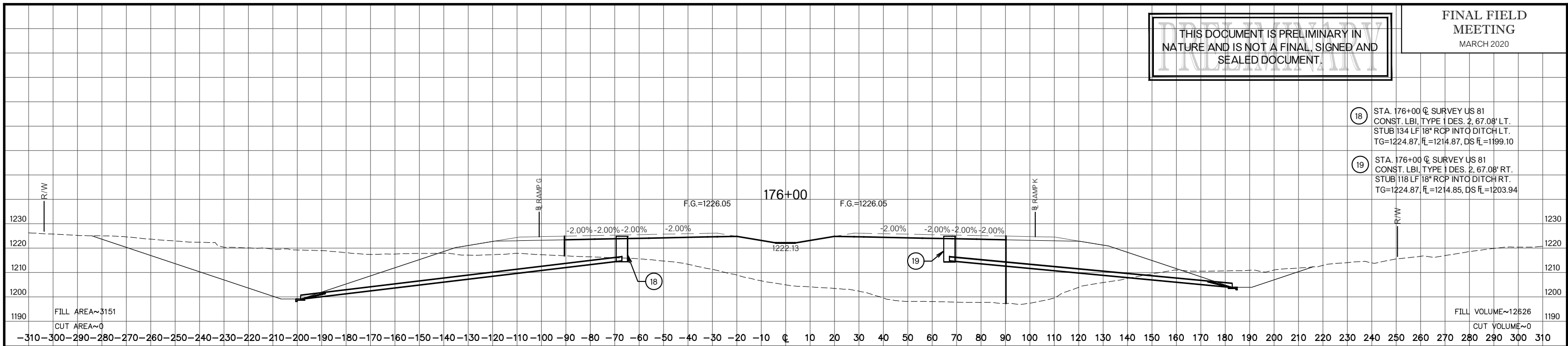
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

- 18 STA. 176+00 @ SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' LT.
STUB 134 LF 18" RCP INTO DITCH LT.
TG=1224.87, \bar{f}_L =1214.87, DS \bar{f}_L =1199.10
- 19 STA. 176+00 @ SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' RT.
STUB 118 LF 18" RCP INTO DITCH RT.
TG=1224.87, \bar{f}_L =1214.85, DS \bar{f}_L =1203.94



SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

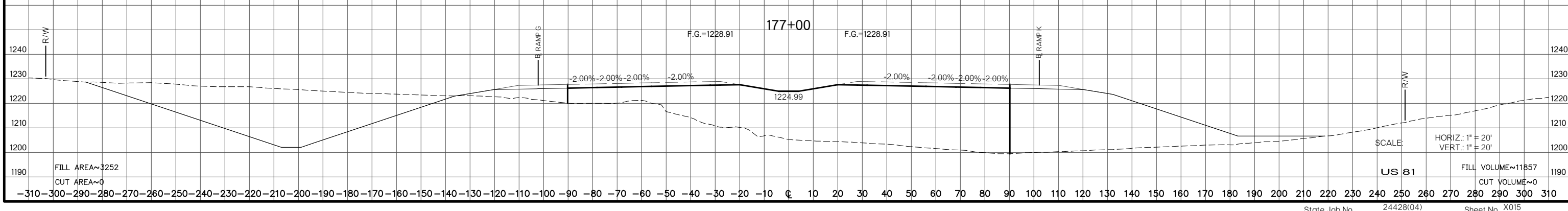
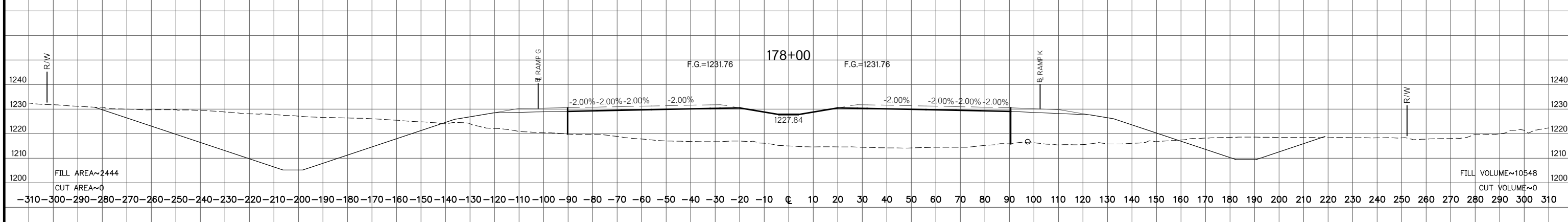
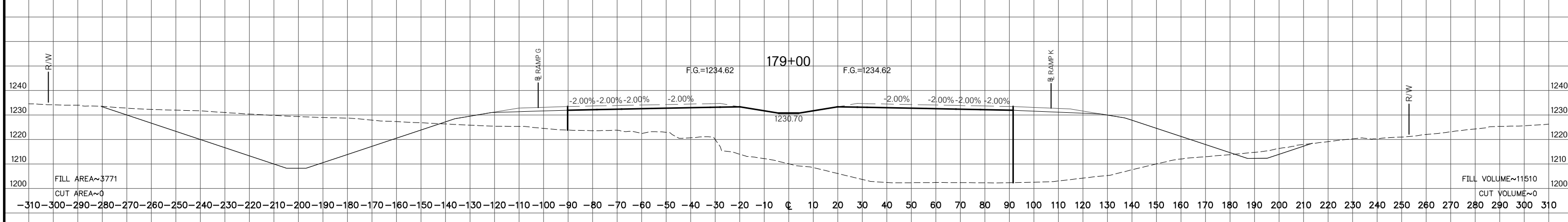
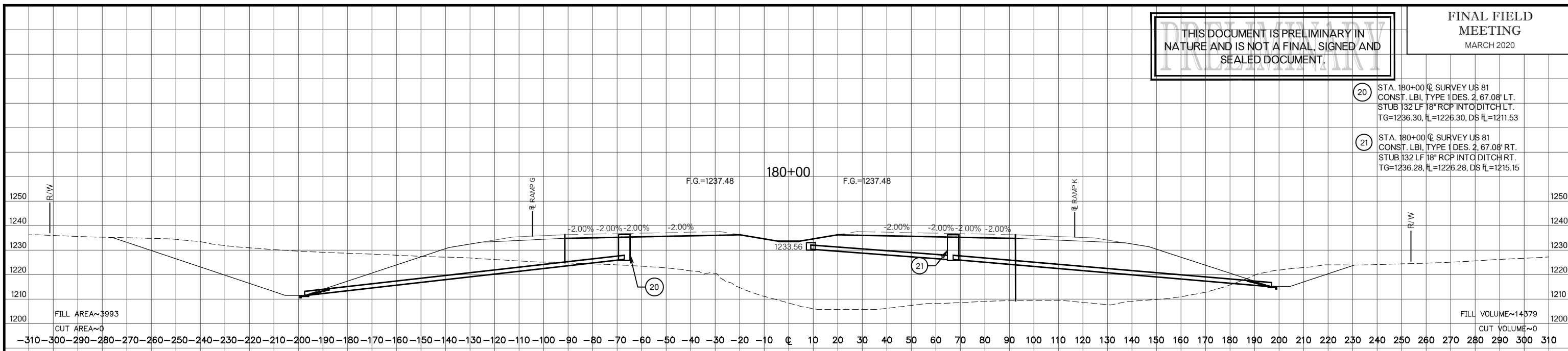
US 81
FILL VOLUME~12538
CUT VOLUME~7746

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

(20) STA. 180+00 Q SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' LT.
STUB 132 LF 18" RCP INTO DITCH LT.
TG=1236.30, f_L=1226.30, DS f_L=1211.53

(21) STA. 180+00 Q SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' RT.
STUB 132 LF 18" RCP INTO DITCH RT.
TG=1236.28, f_L=1226.28, DS f_L=1215.15

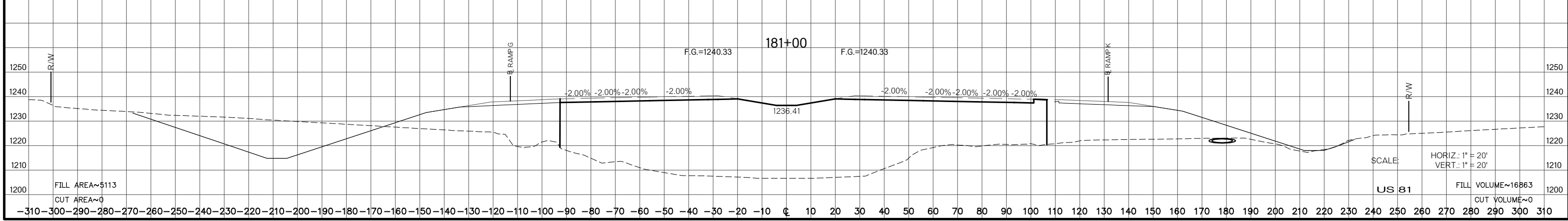
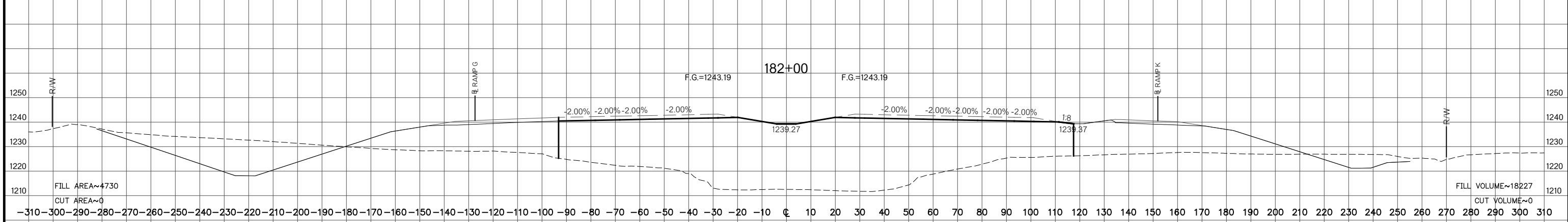
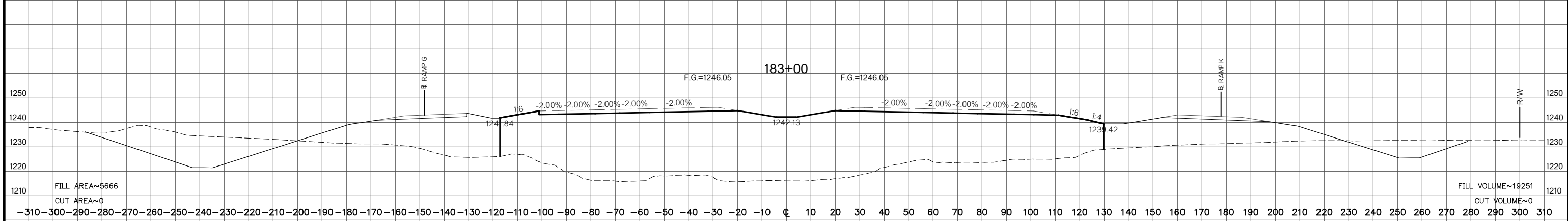
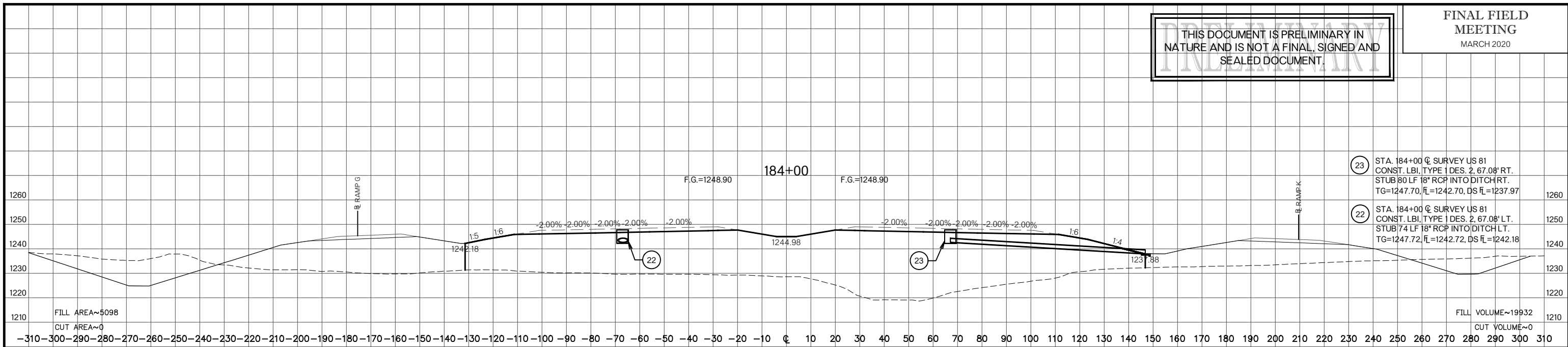


SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

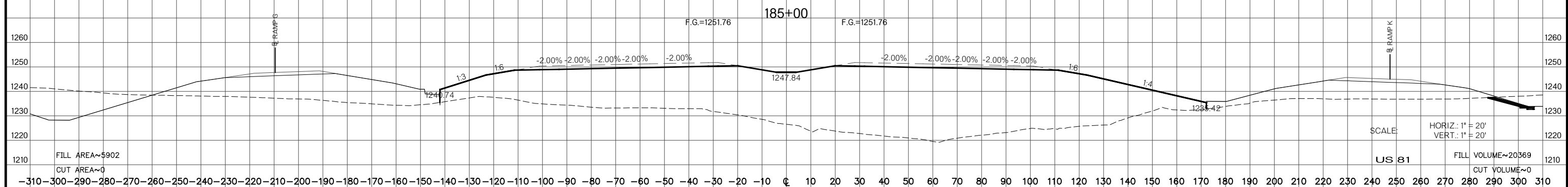
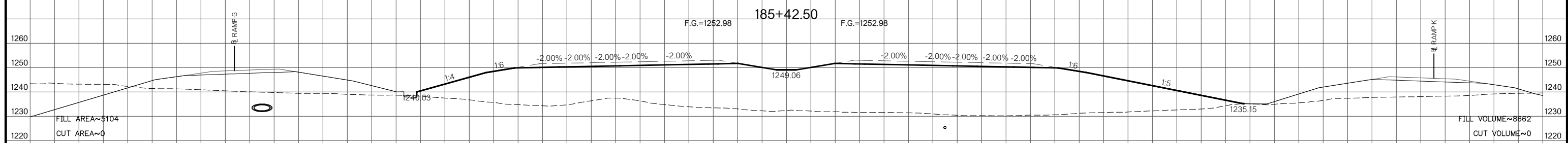
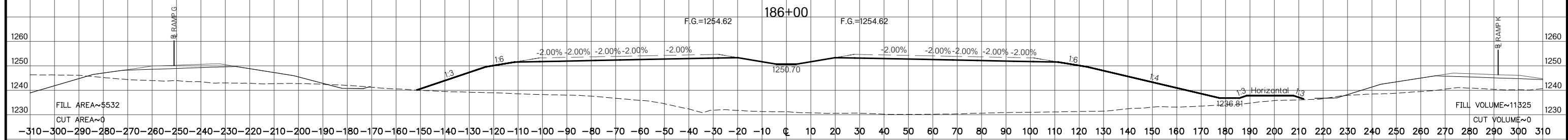
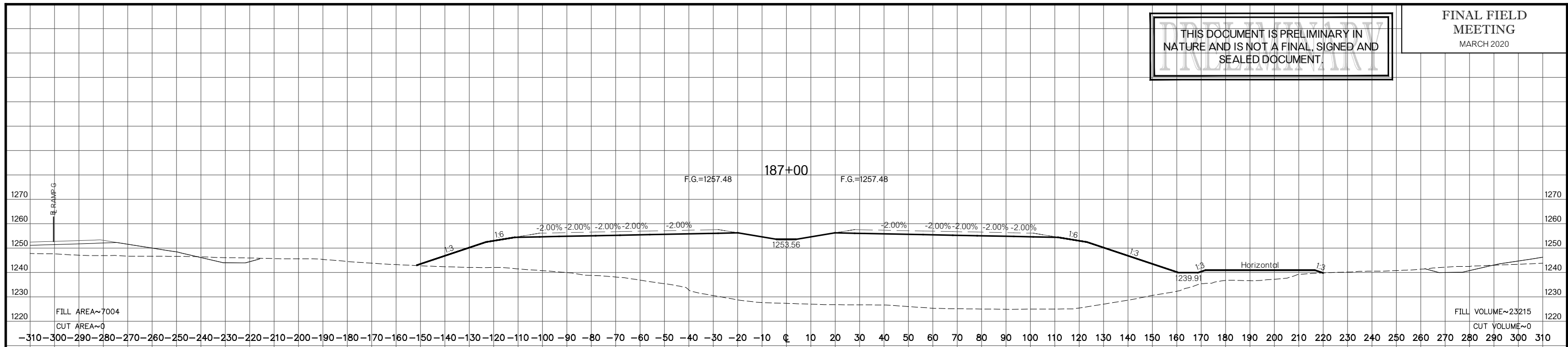


SCALE HORIZ.: 1" = 20'
 VERT.: 1" = 20'

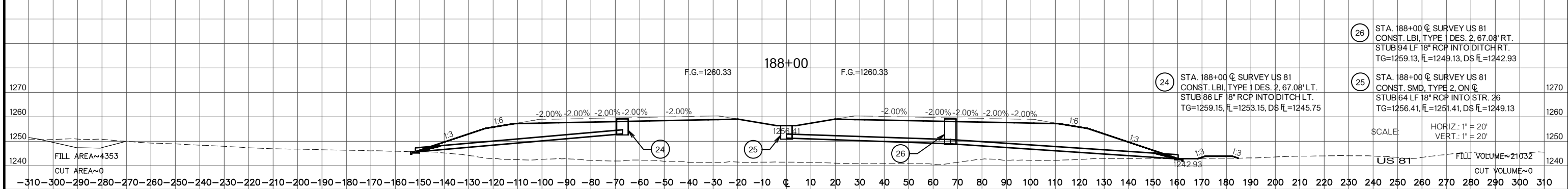
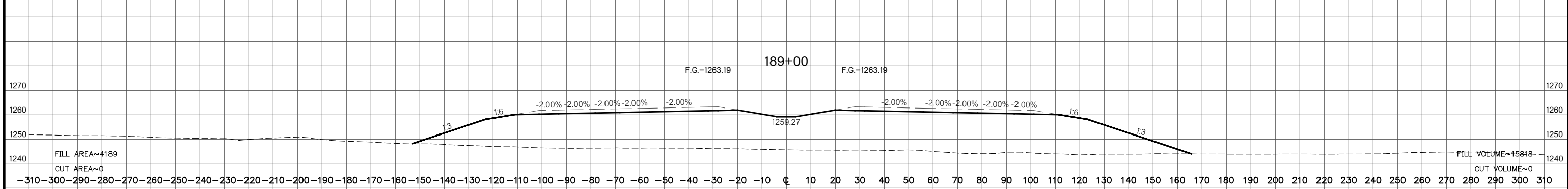
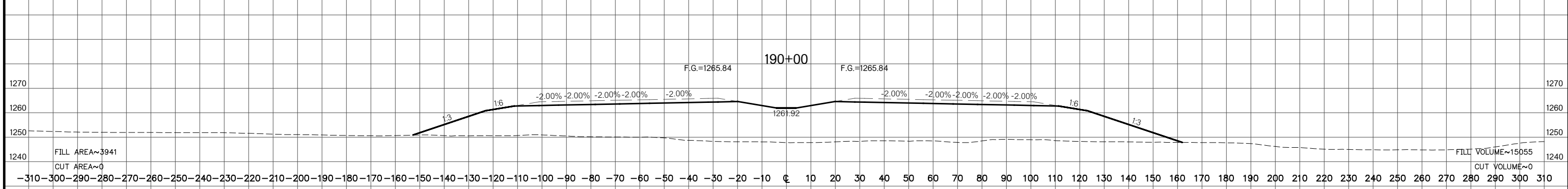
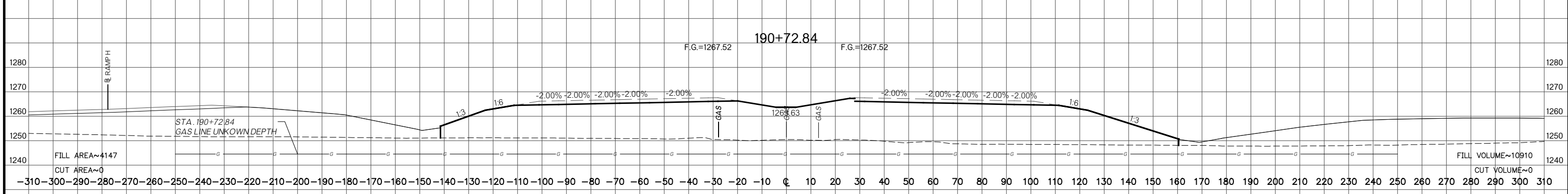
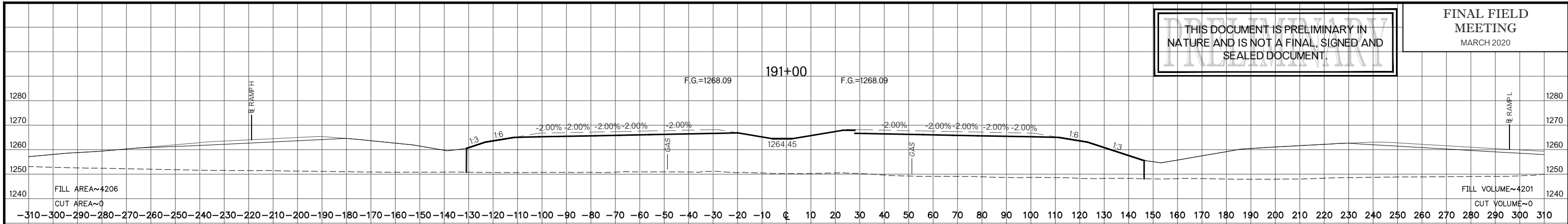
US 81

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



24 STA. 188+00 @ SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' LT.
STUB 86 LF 18" RCP INTO DITCH LT.
TG=1259.15, FL=1253.15, DS FL=1245.75

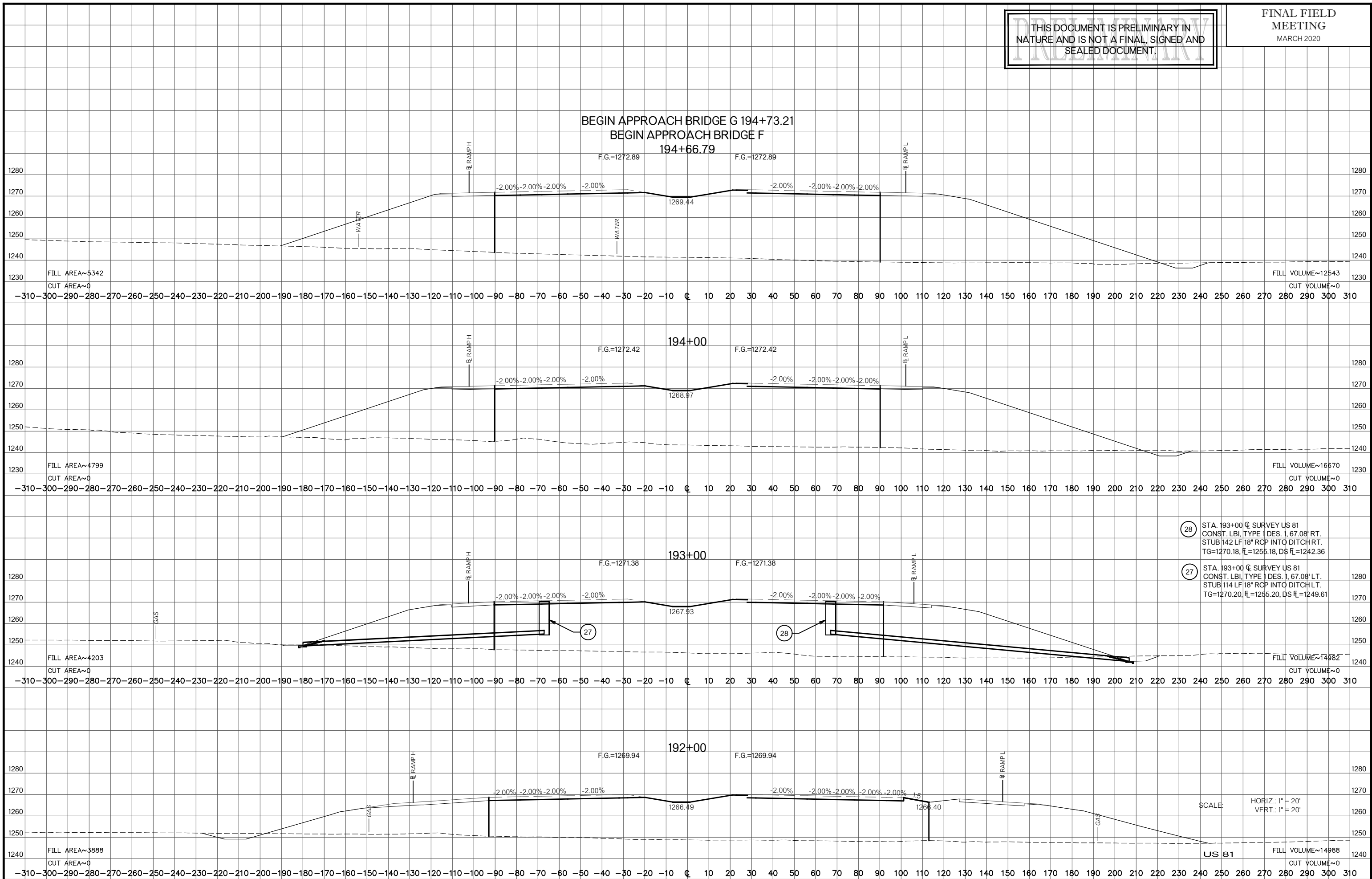
26 STA. 188+00 @ SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' RT.
STUB 94 LF 18" RCP INTO DITCH RT.
TG=1259.13, FL=1249.13, DS FL=1242.93

25 STA. 188+00 @ SURVEY US 81
CONST. SMD, TYPE 2, ON @
STUB 64 LF 18" RCP INTO STR. 26
TG=1256.41, FL=1251.41, DS FL=1249.13

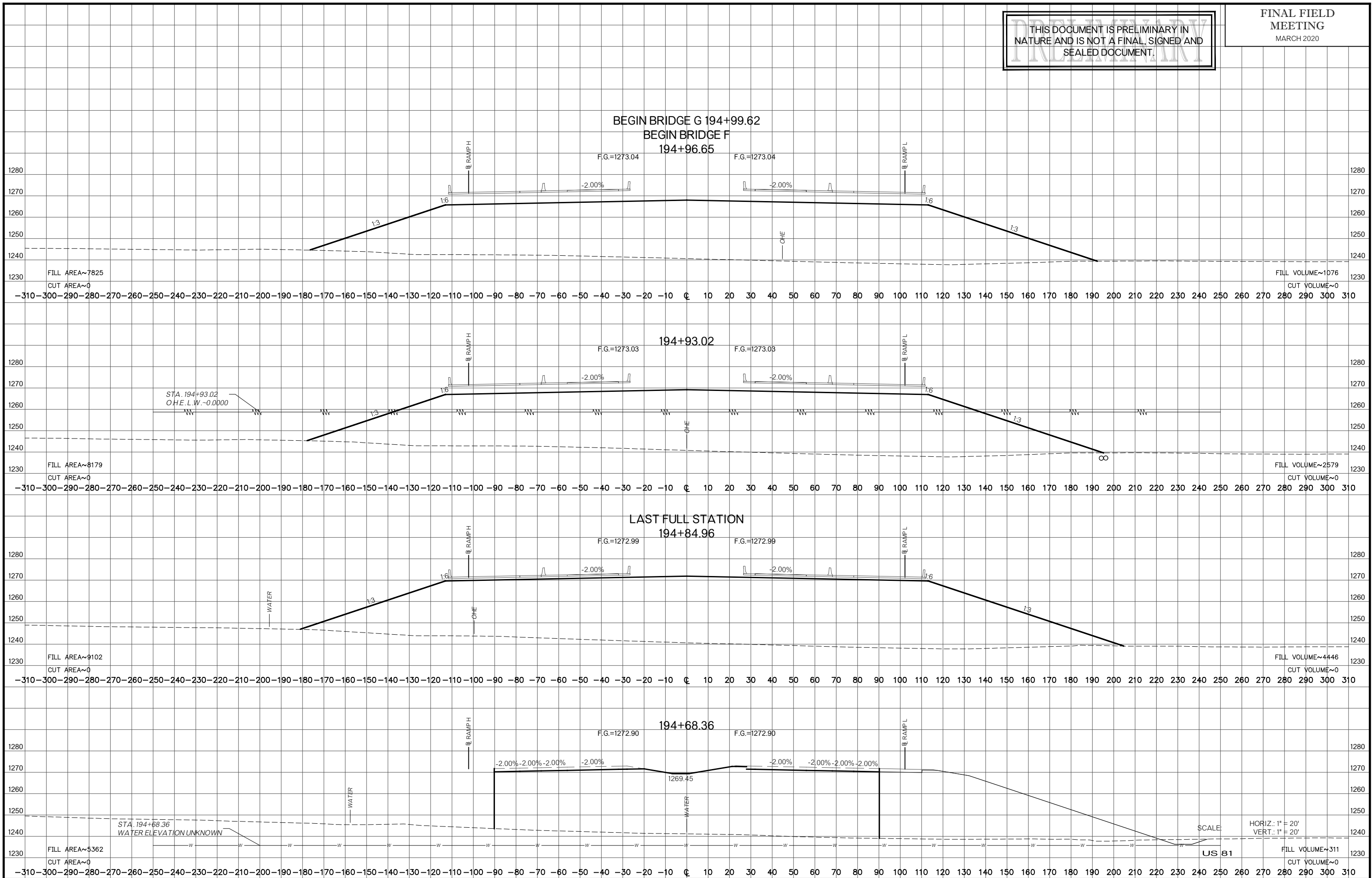
SCALE HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

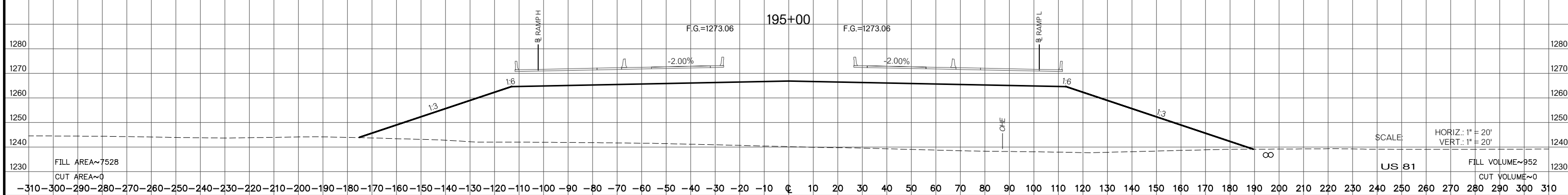
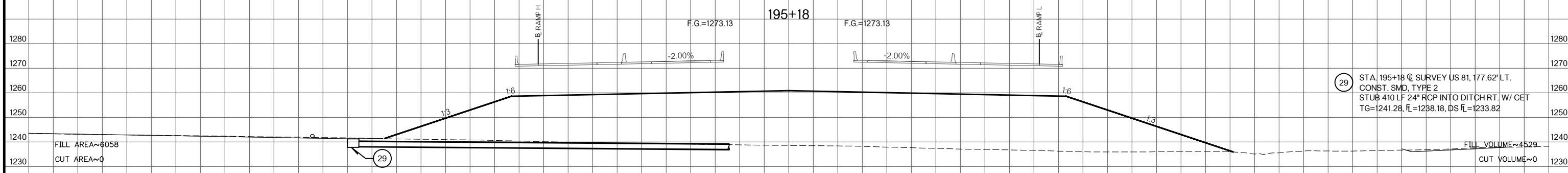
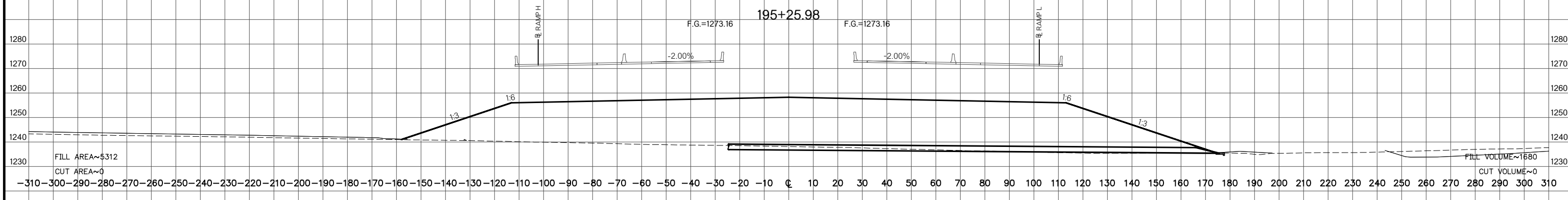
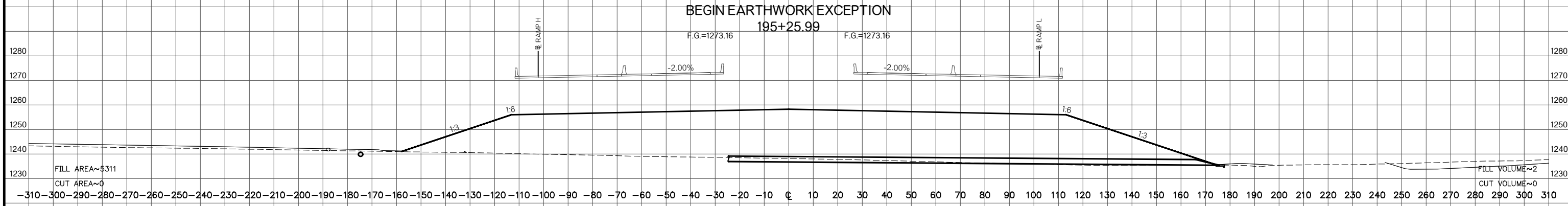
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

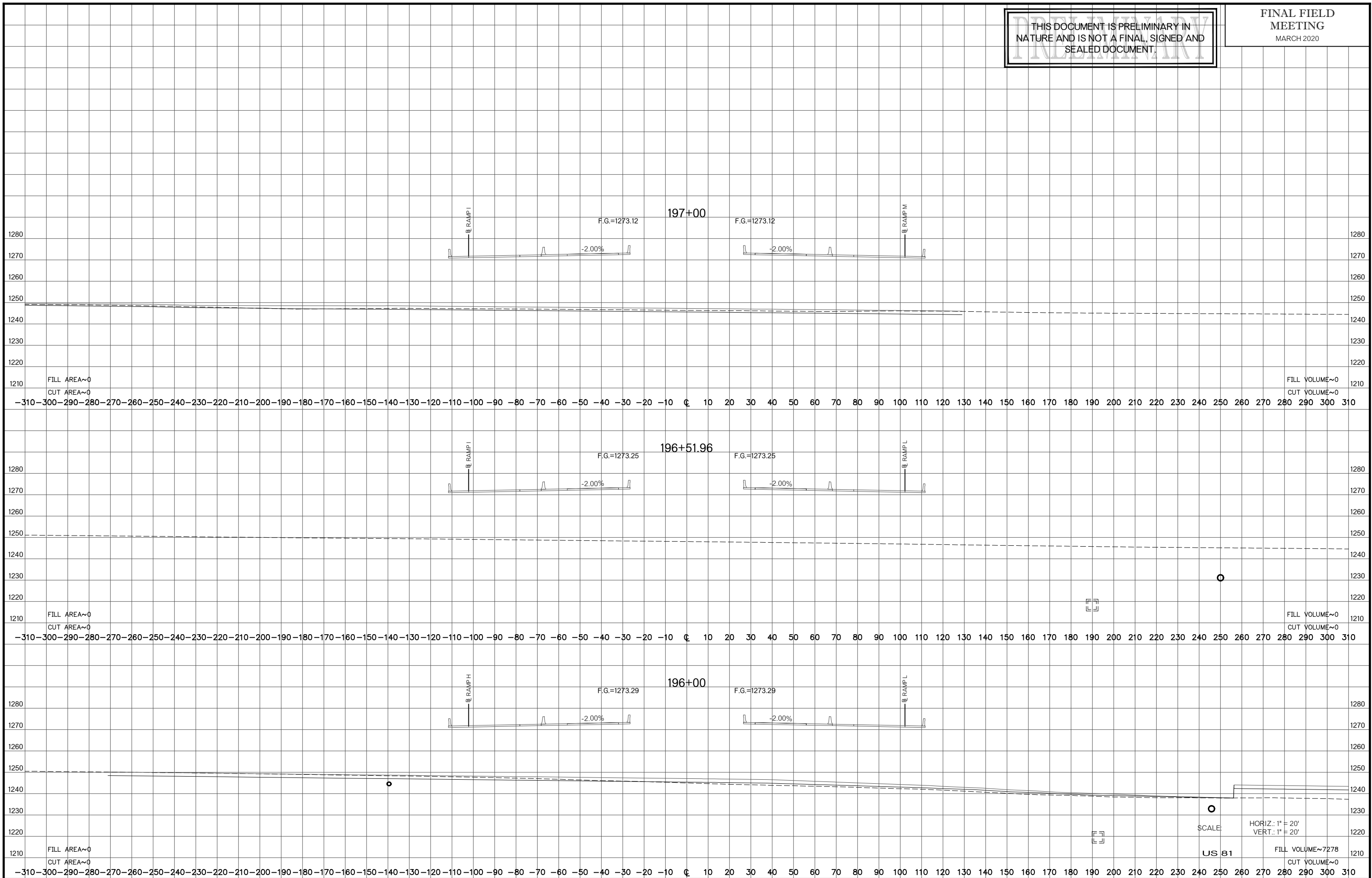


SCALE
US 81
HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

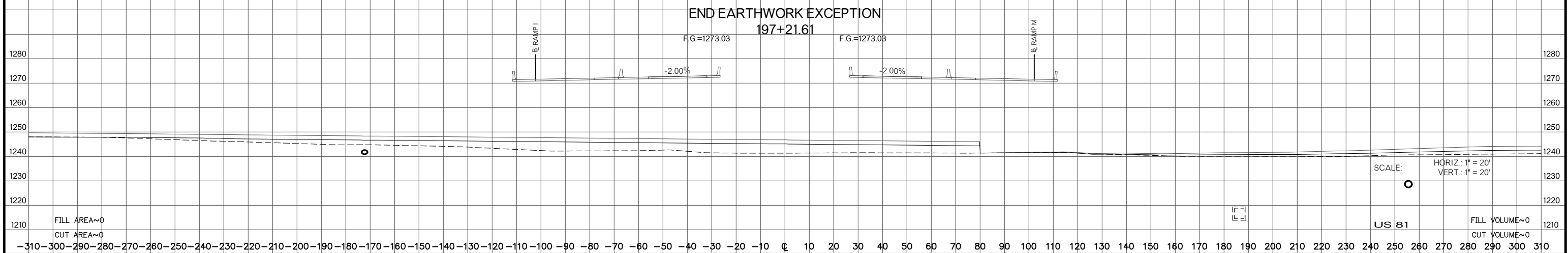
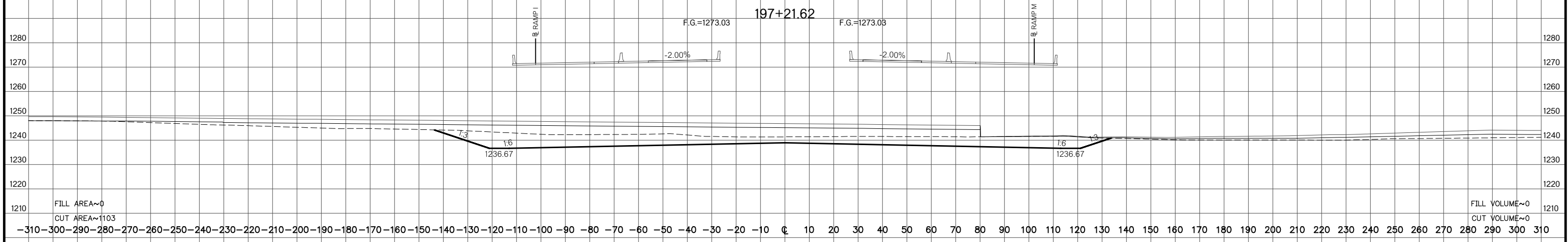
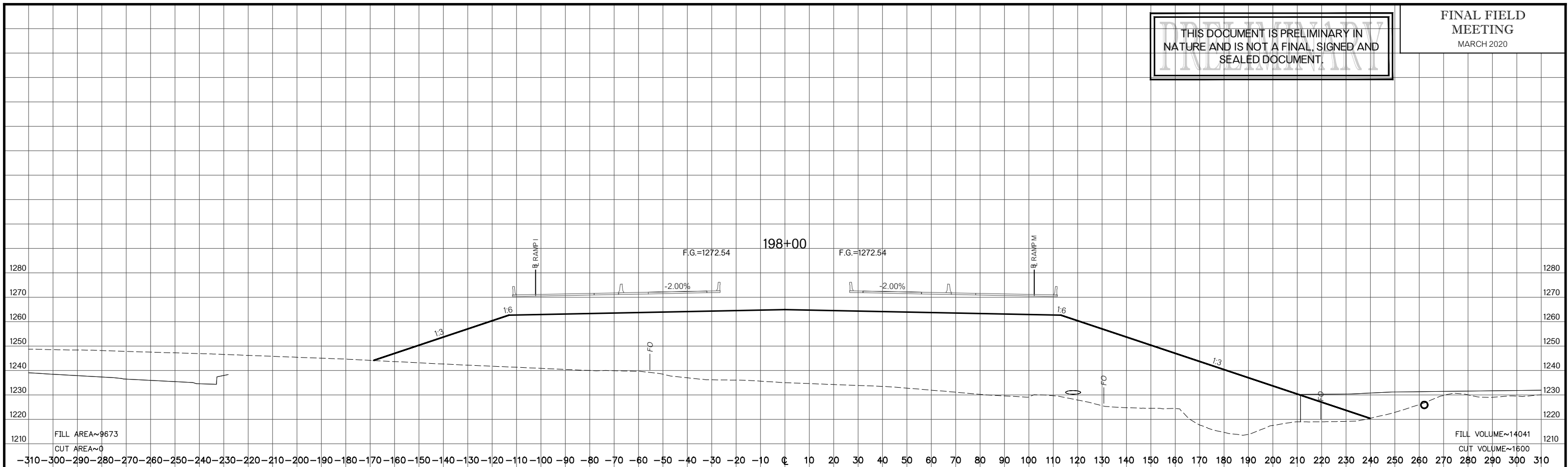
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

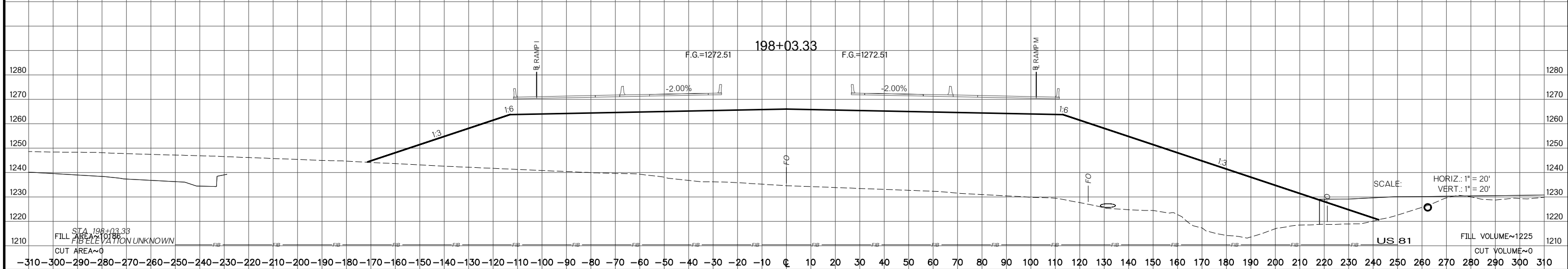
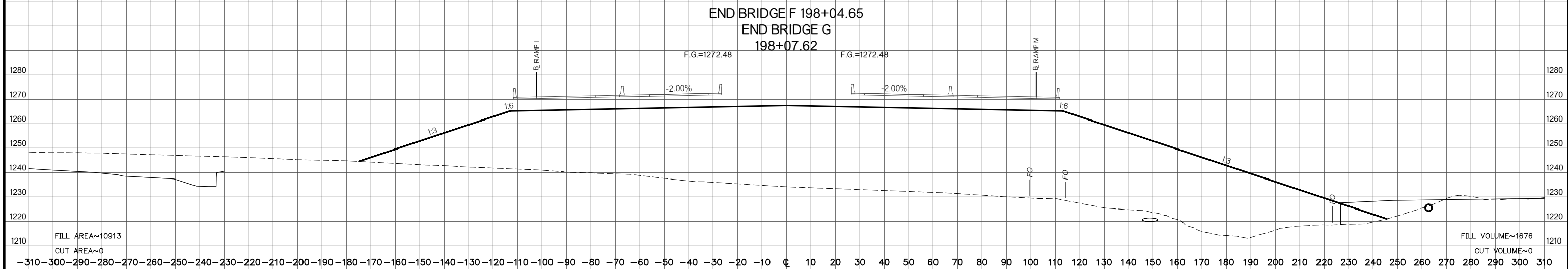
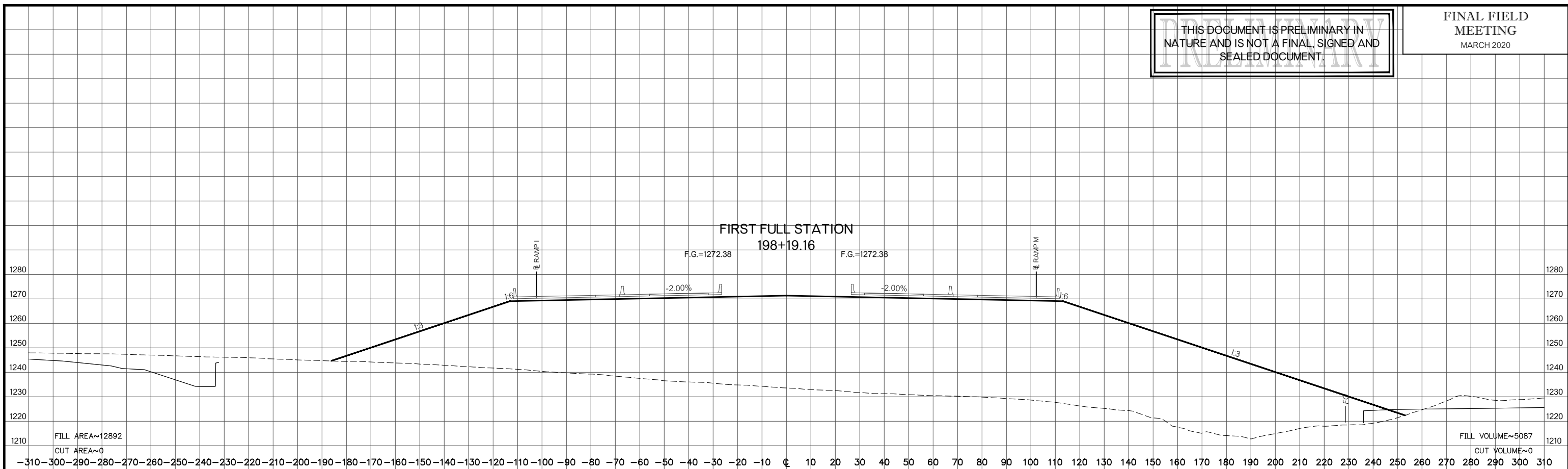
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

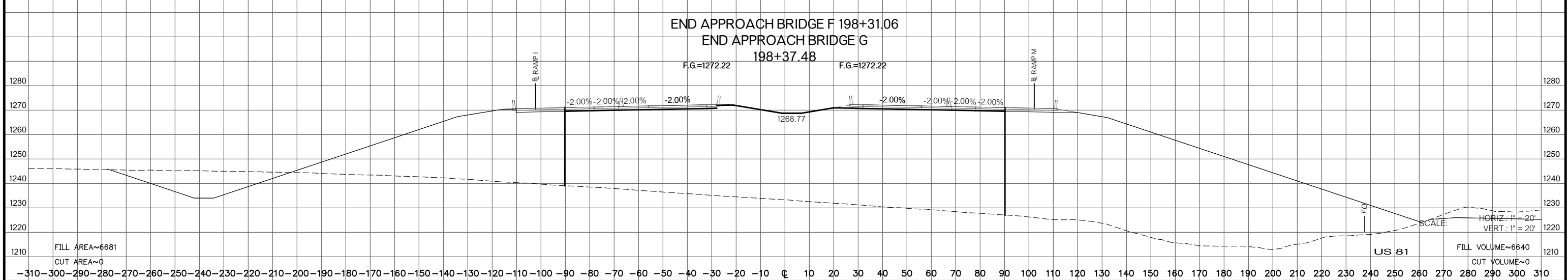
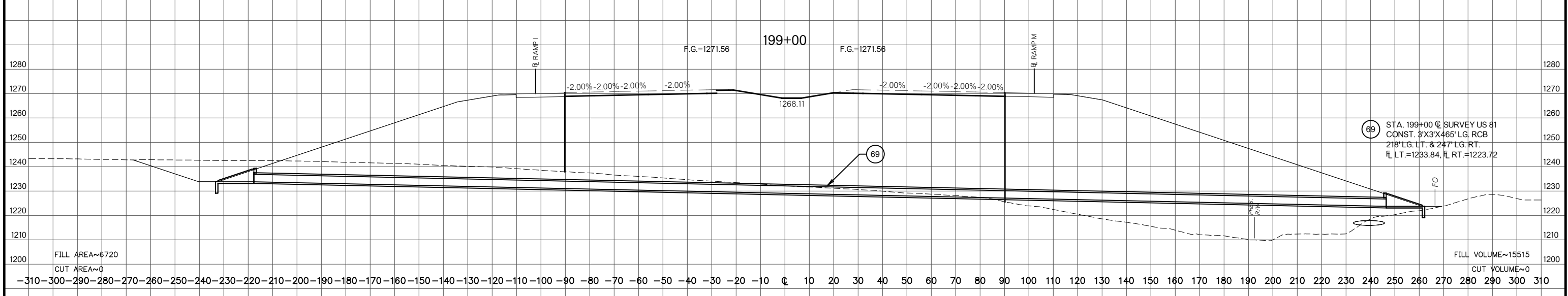
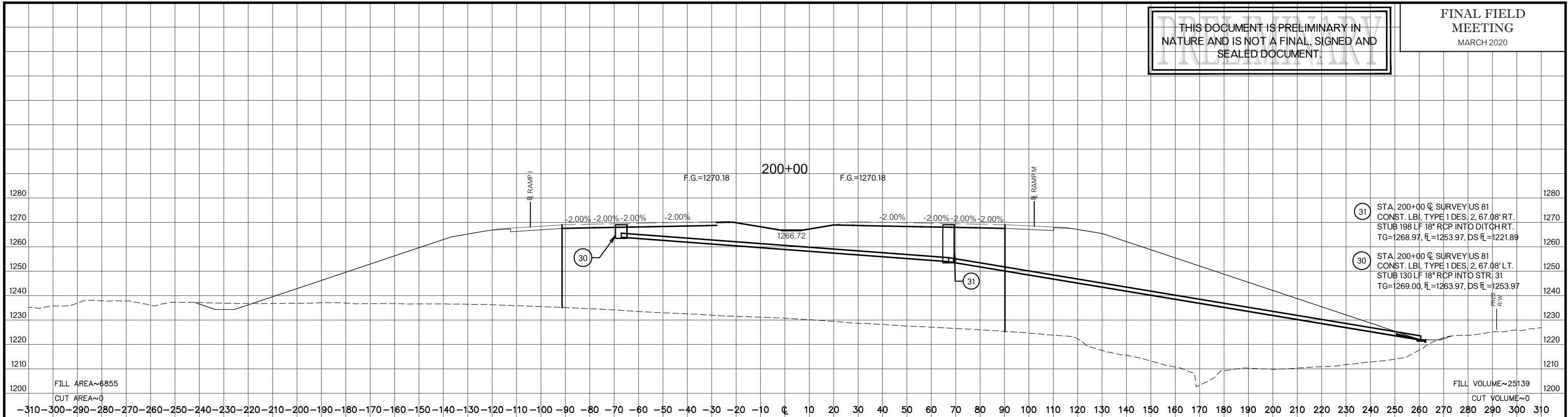
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

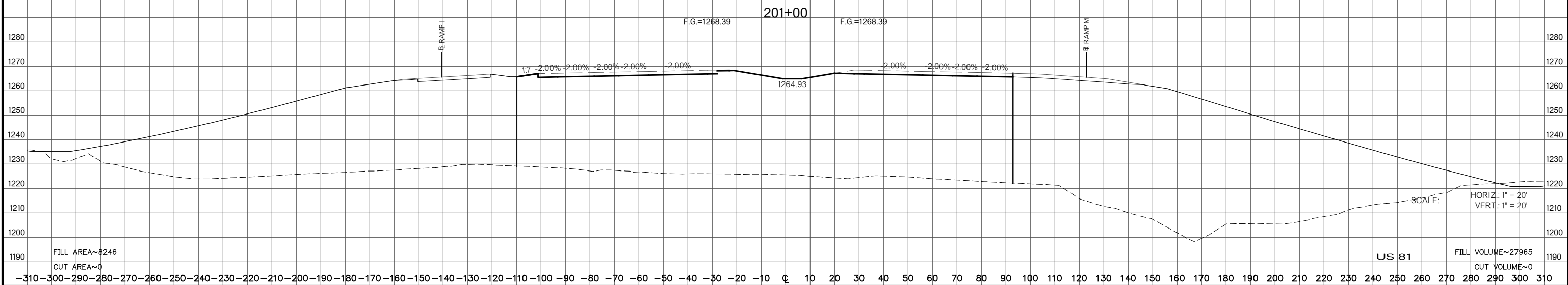
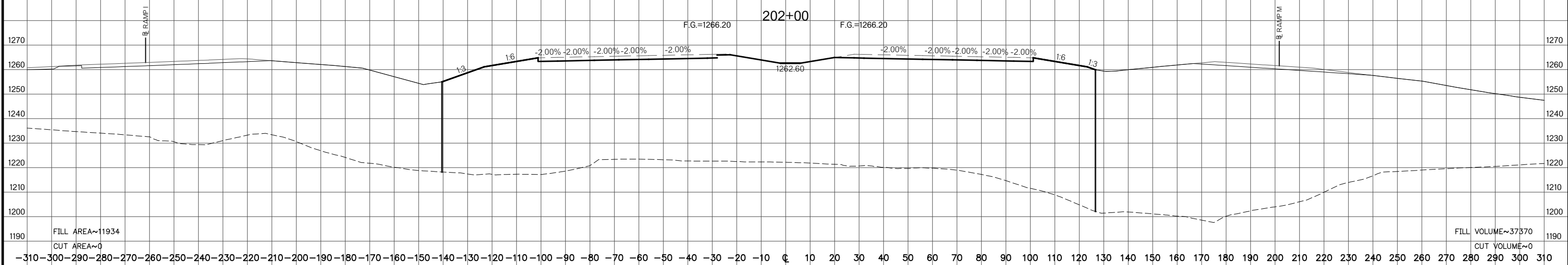
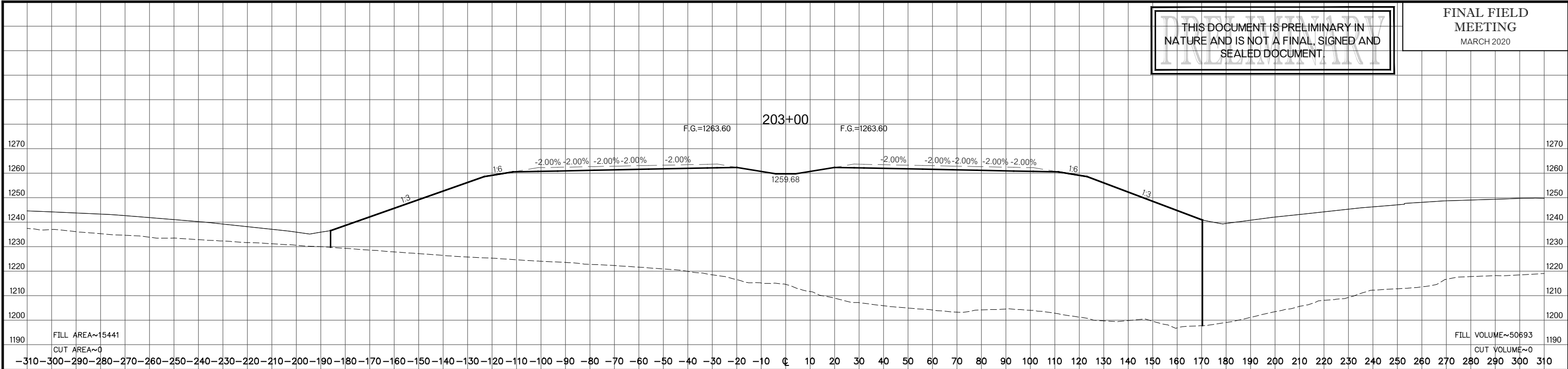


US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



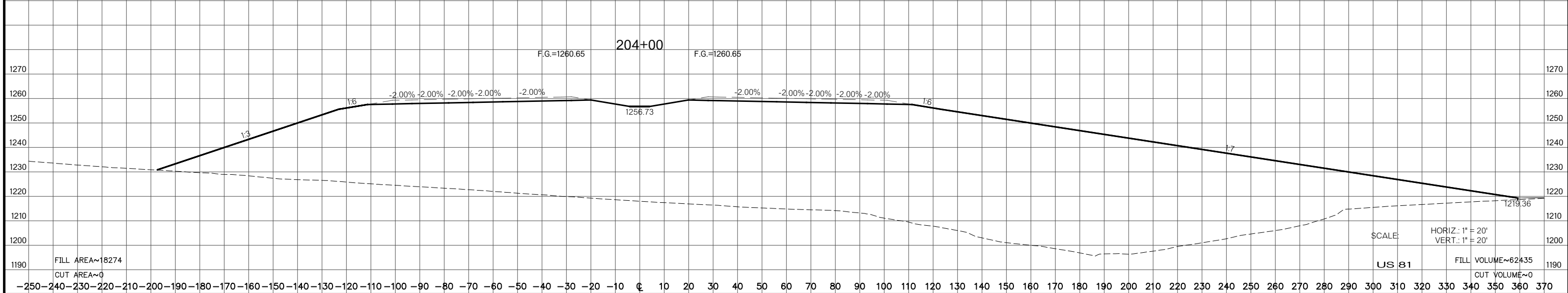
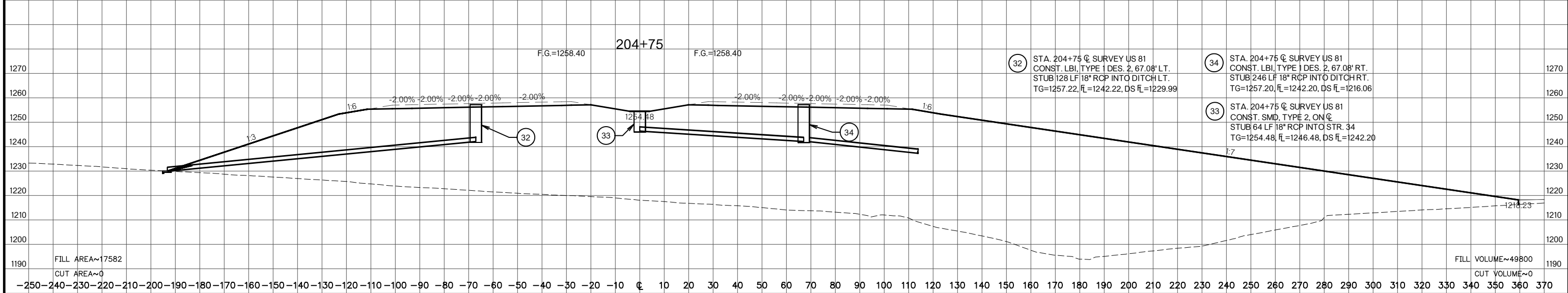
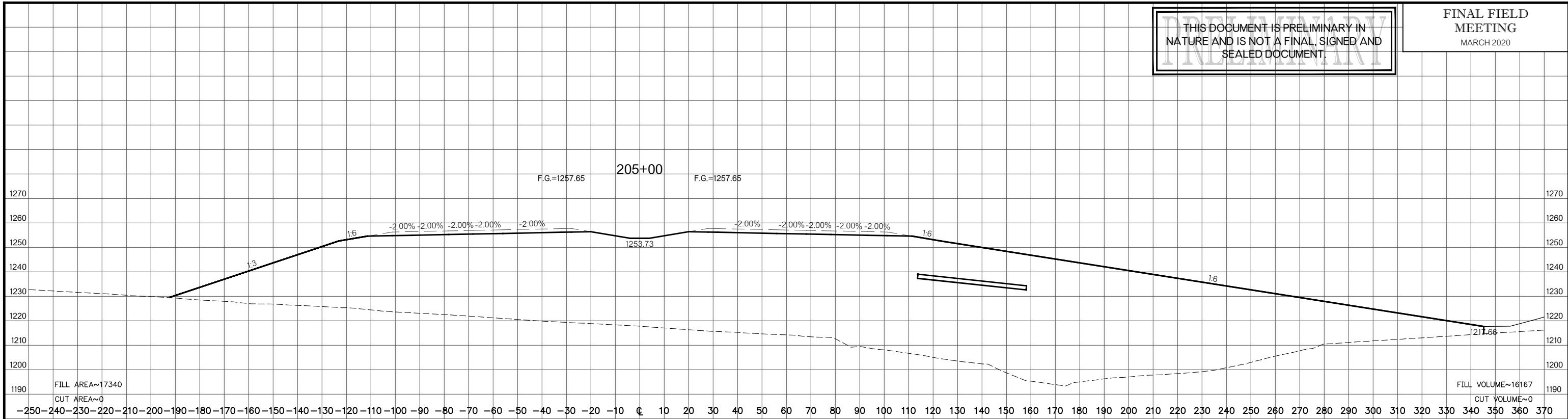
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

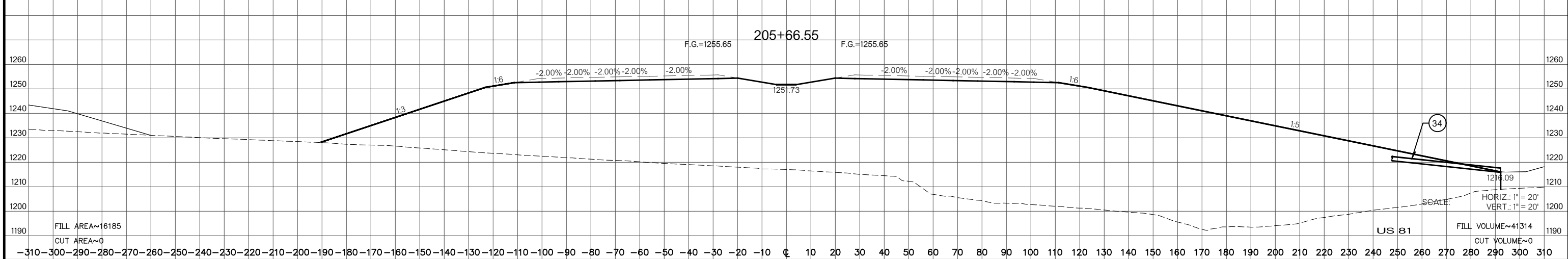
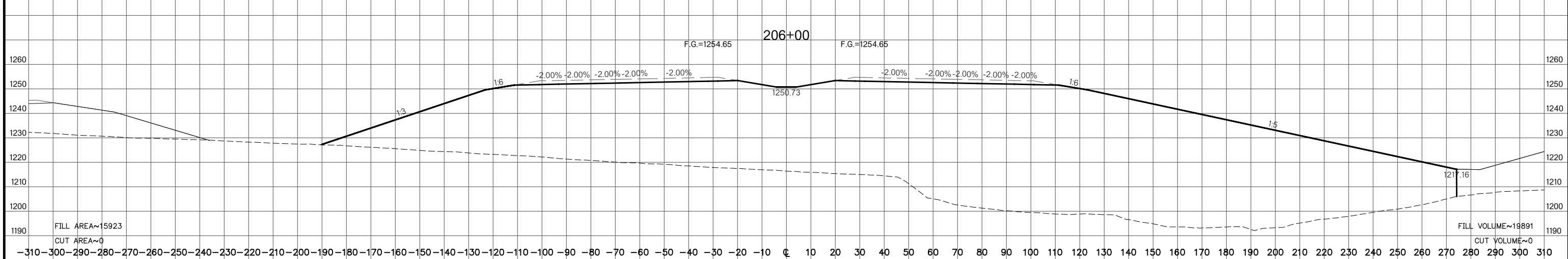
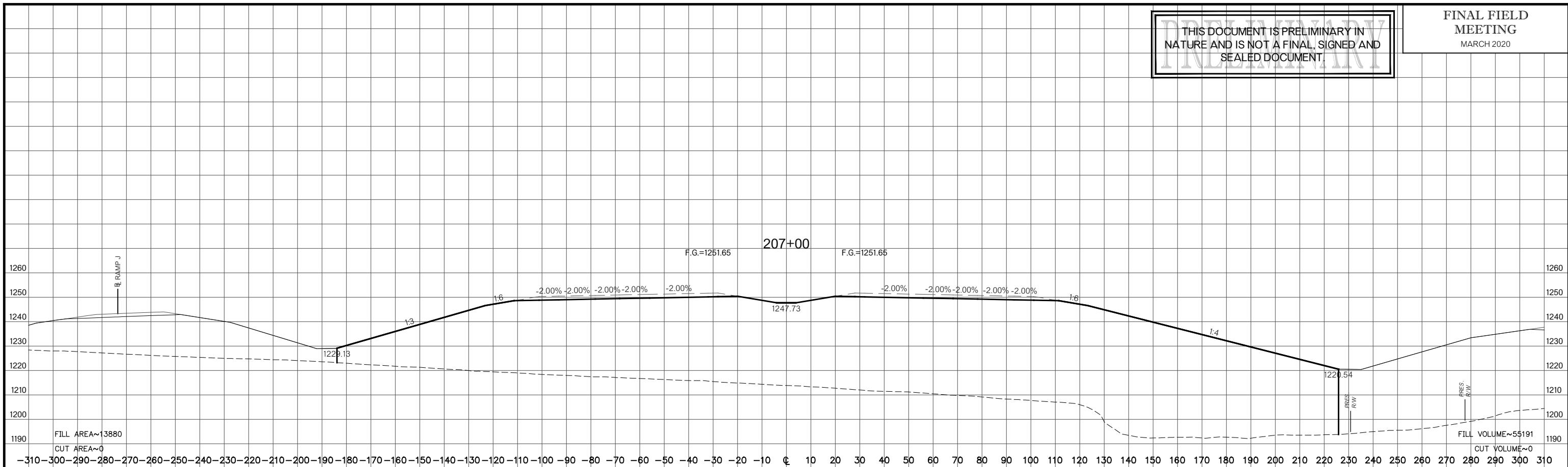
FINAL FIELD MEETING
MARCH 2020



SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

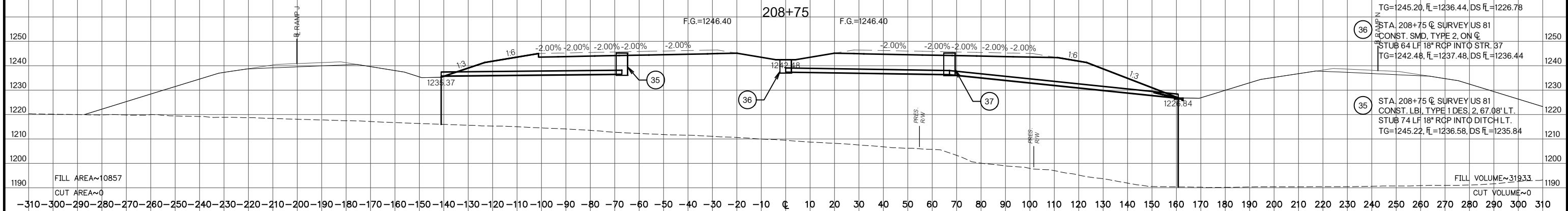
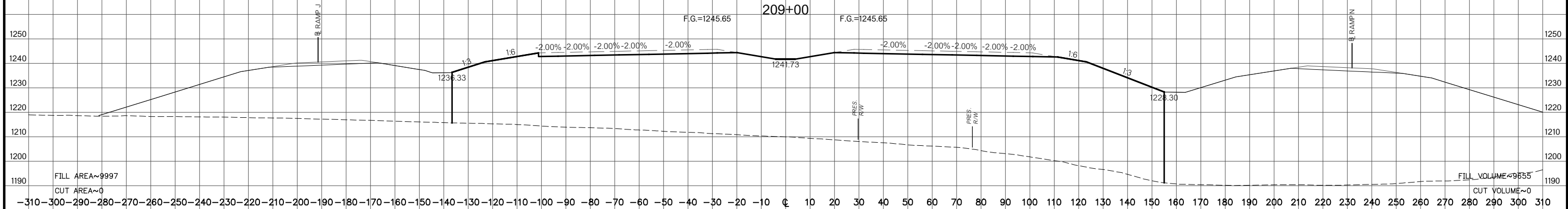
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

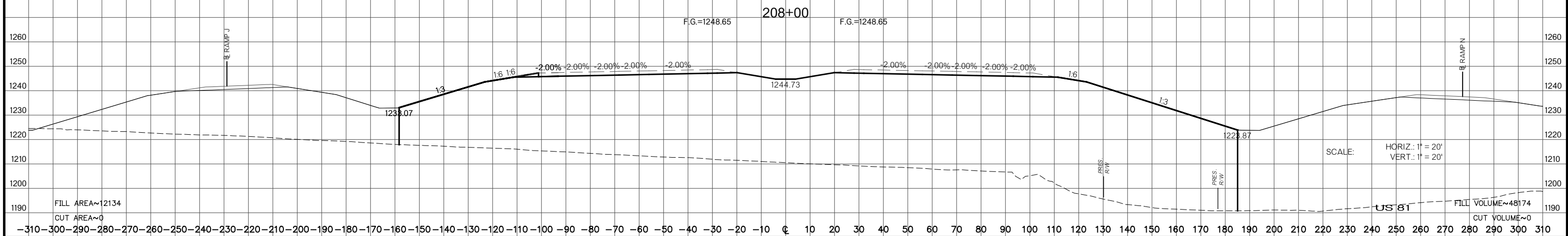


US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



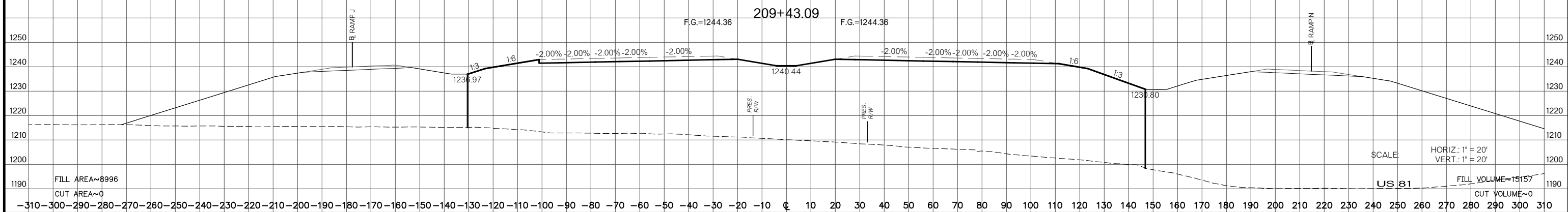
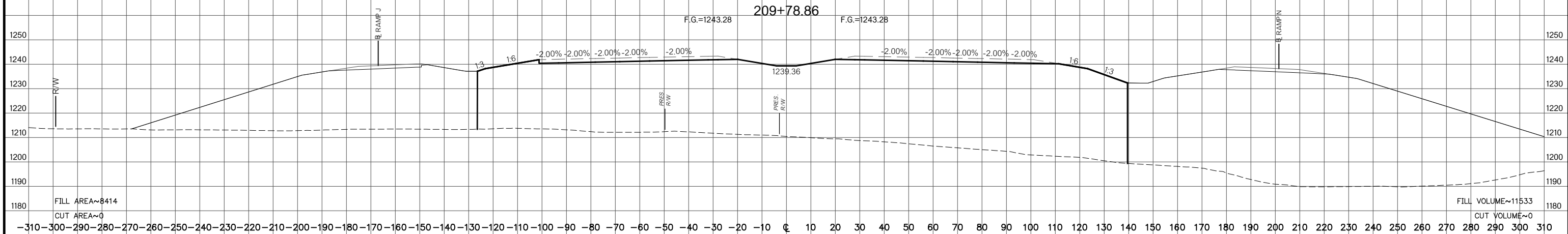
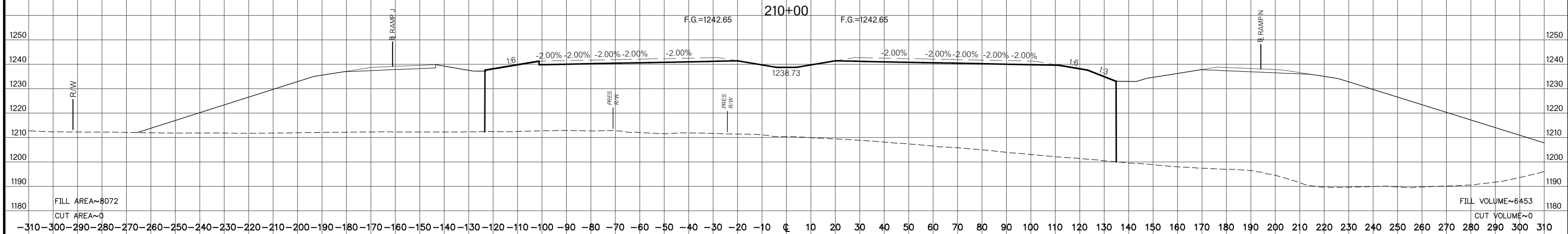
- 37 STA. 208+75 C SURVEY US 81
CONST. LBI, TYPE 1 DES, 2, 67.08' RT.
STUB 96 LF 18" RCP INTO DITCH RT.
TG=1245.20, FL=1236.44, DS FL=1226.78
- 36 STA. 208+75 C SURVEY US 81
CONST. SMD, TYPE 2, ON C
STUB 64 LF 18" RCP INTO STR. 37
TG=1242.48, FL=1237.48, DS FL=1236.44
- 35 STA. 208+75 C SURVEY US 81
CONST. LBI, TYPE 1 DES, 2, 67.08' LT.
STUB 74 LF 18" RCP INTO DITCH LT.
TG=1245.22, FL=1236.58, DS FL=1235.84



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

GRADY COUNTY US 81 REALIGNMENT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

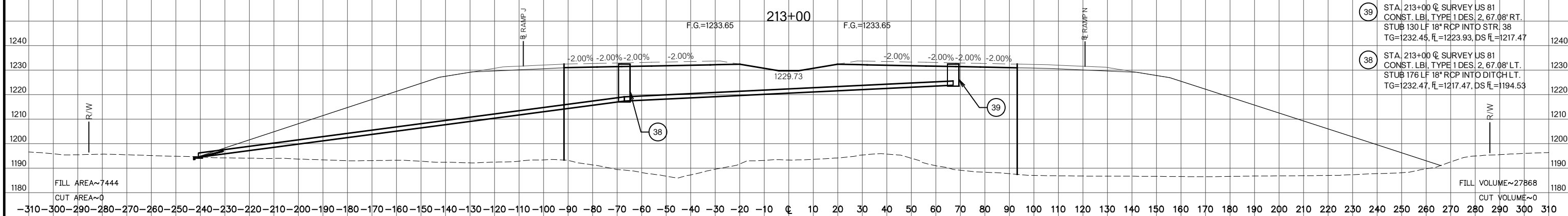


SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

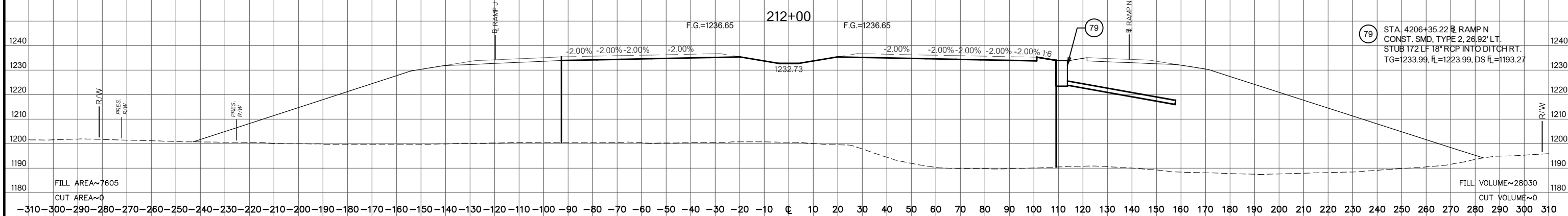
US 81

US 81 REALIGNMENT
GRADY COUNTY

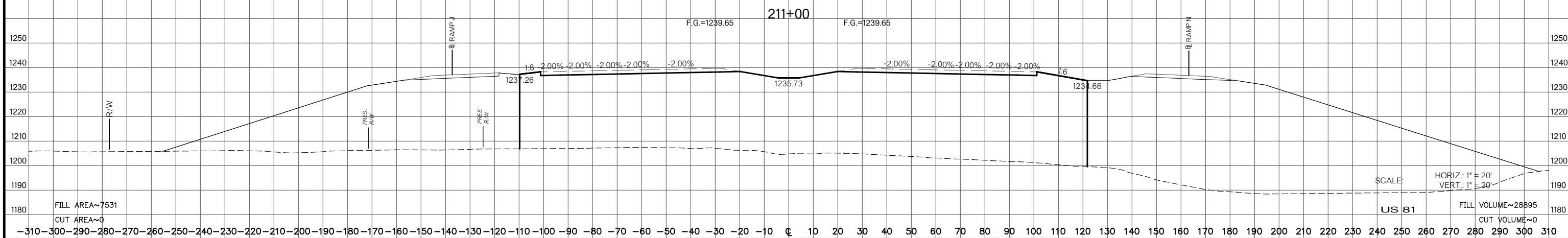
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



- 39 STA. 213+00 @ SURVEY US 81
CONST. LBI, TYPE 1 DES, 2, 67.08' RT.
STUB 130 LF 18" RCP INTO STR. 38
TG=1232.45, FL=1223.93, DS FL=1217.47
- 38 STA. 213+00 @ SURVEY US 81
CONST. LBI, TYPE 1 DES, 2, 67.08' LT.
STUB 176 LF 18" RCP INTO DITCH LT.
TG=1232.47, FL=1217.47, DS FL=1194.53



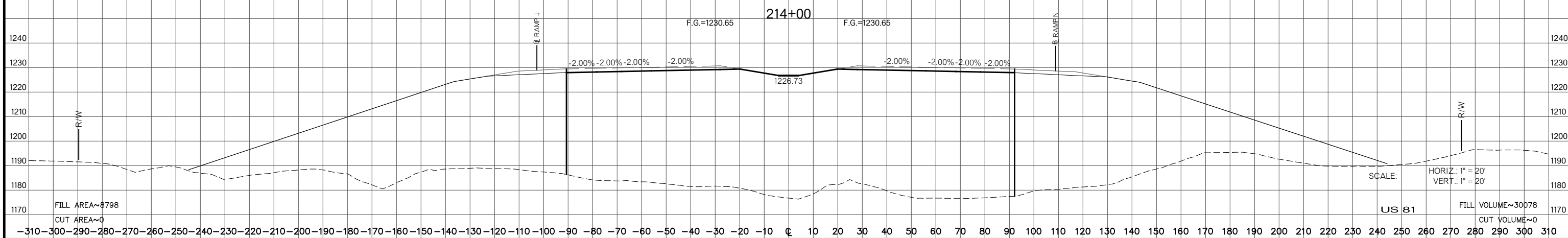
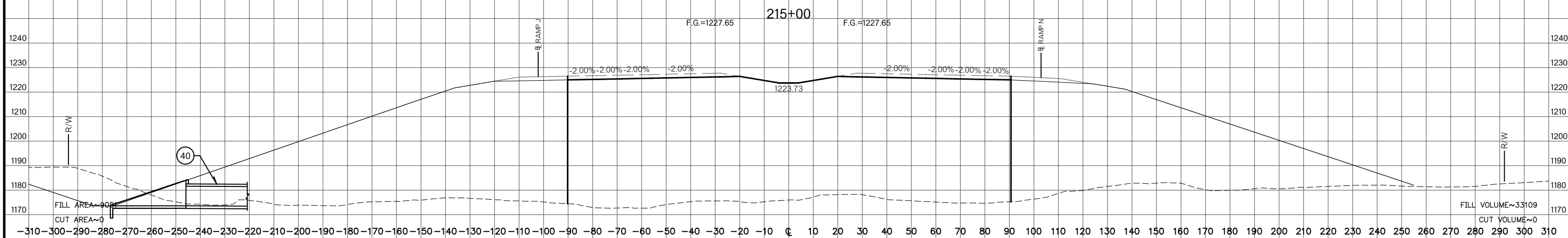
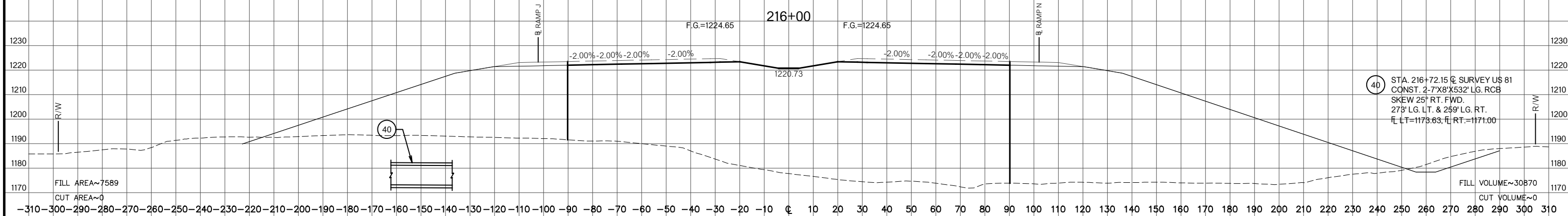
- 79 STA. 4206+35.22 @ RAMP N
CONST. SMD, TYPE 2, 26.92' LT.
STUB 172 LF 18" RCP INTO DITCH RT.
TG=1233.99, FL=1223.99, DS FL=1193.27



SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

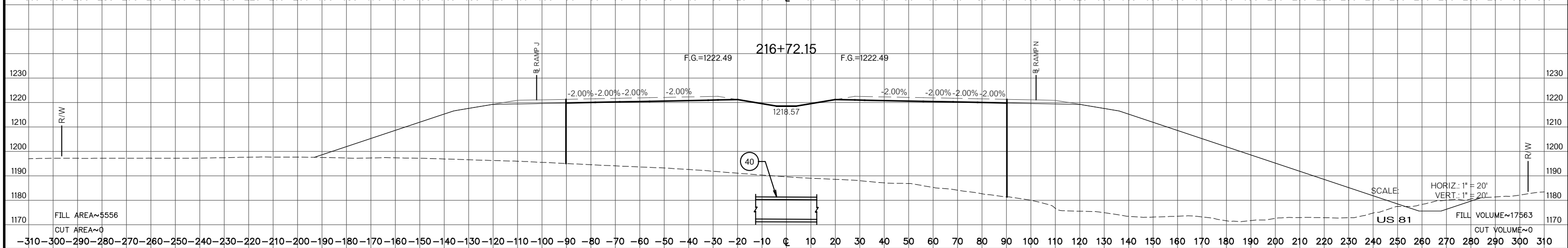
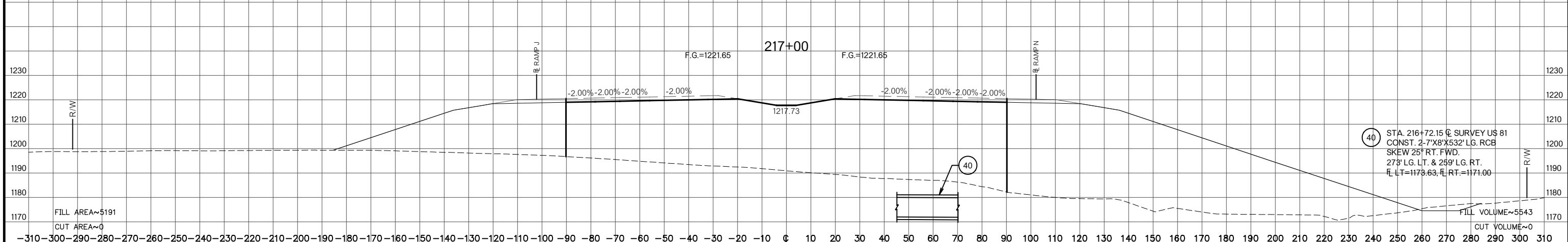
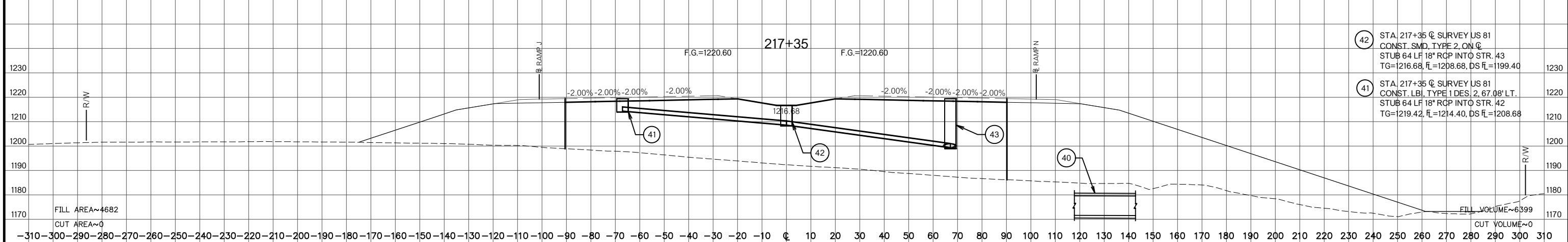
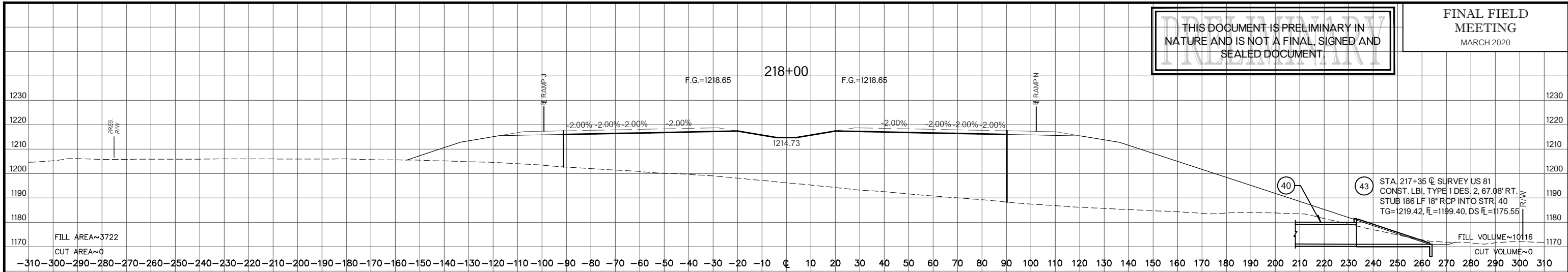
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

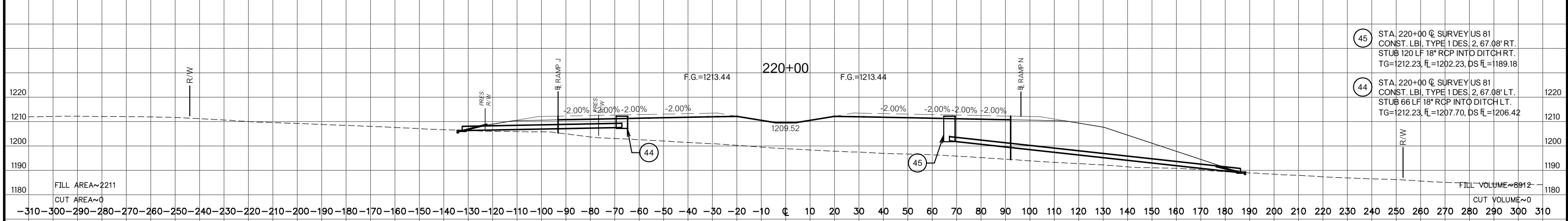
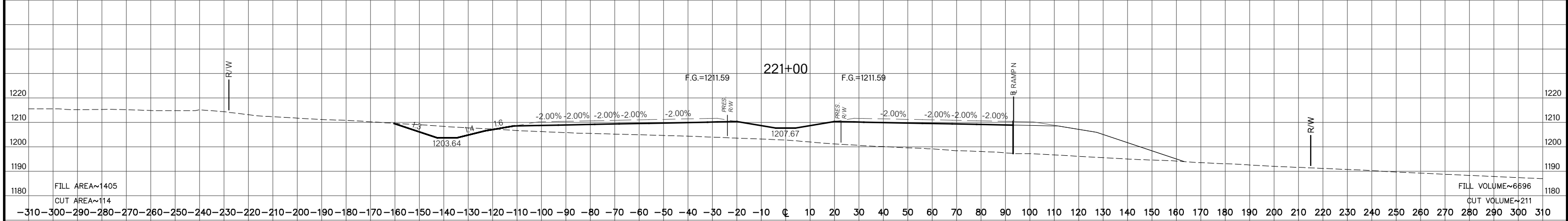
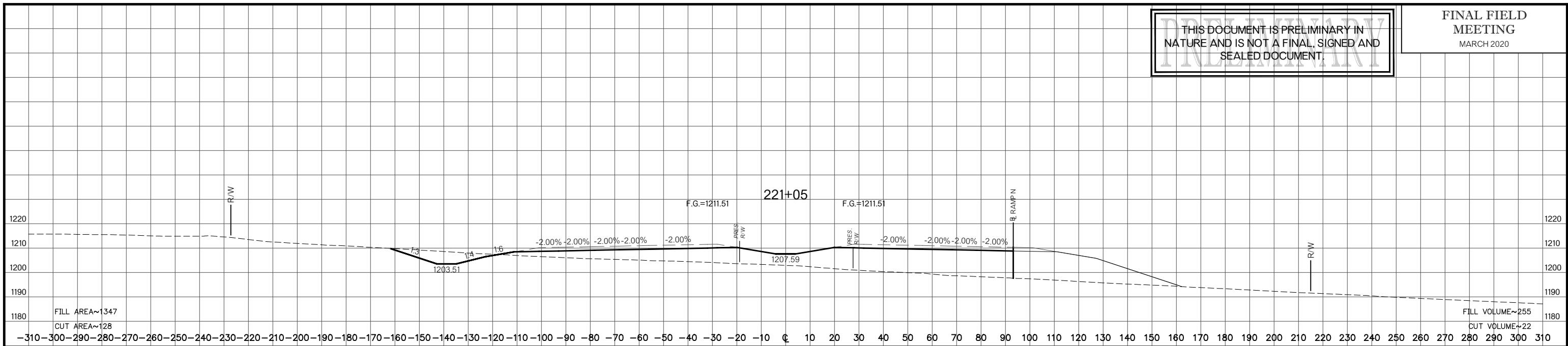
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



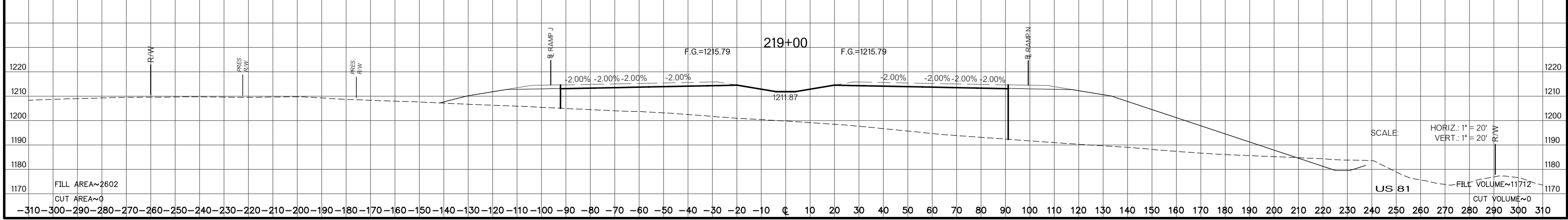
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



45 STA. 220+00 \bar{C} SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' RT.
STUB 120 LF 18" RCP INTO DITCH RT.
TG=1212.23, \bar{f}_i =1202.23, DS \bar{f}_i =189.18

44 STA. 220+00 \bar{C} SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' LT.
STUB 66 LF 18" RCP INTO DITCH LT.
TG=1212.23, \bar{f}_i =1207.70, DS \bar{f}_i =1206.42

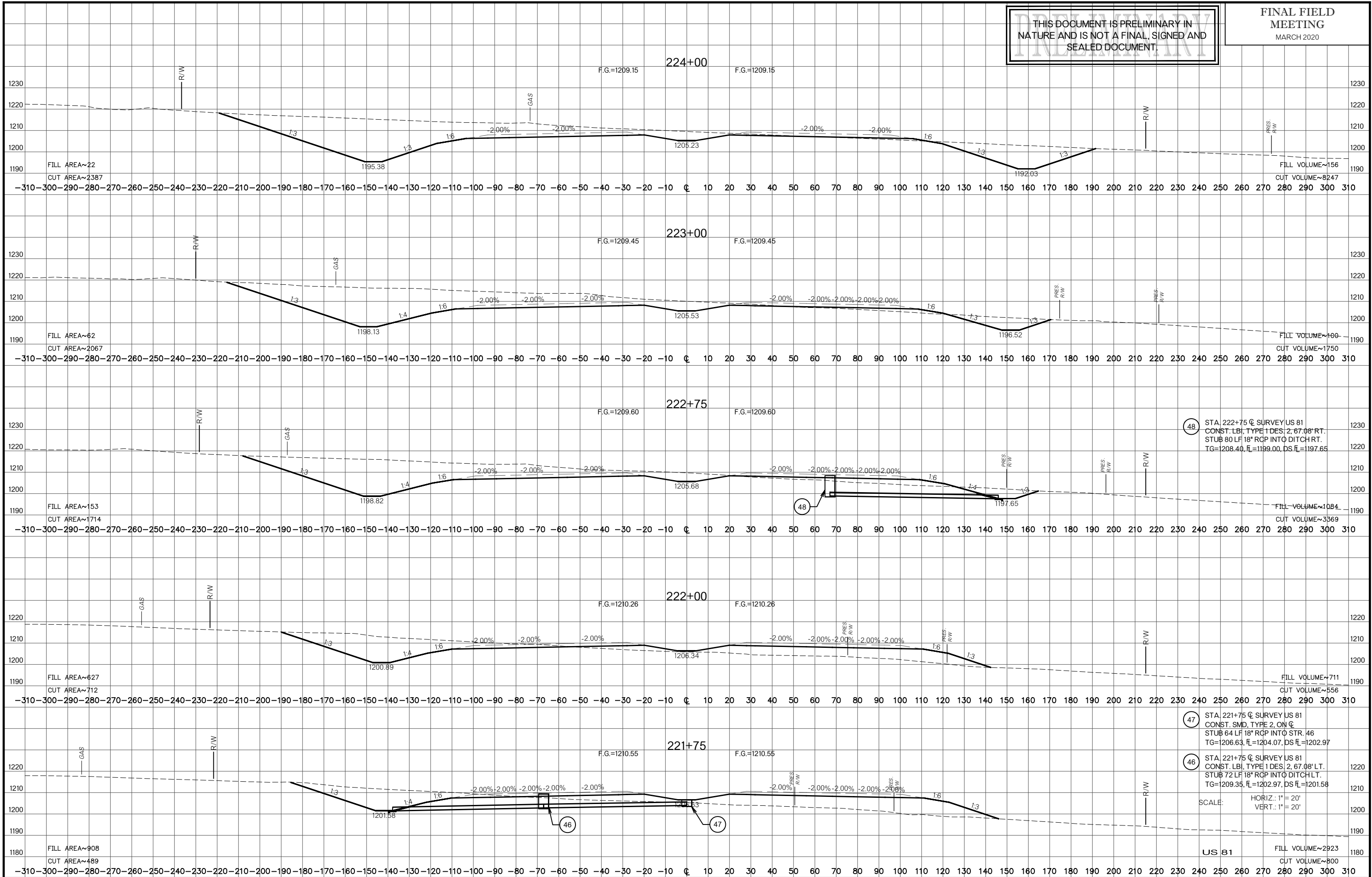


SCALE: HORIZ.: 1" = 20' R/W
VERT.: 1" = 20' R/W

US 81
FILL VOLUME~11712
CUT VOLUME~0

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



48 STA. 222+75 Q SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' RT.
STUB 80 LF 18" RCP INTO DITCH RT.
TG=1208.40, FL=1199.00, DS FL=1197.65

47 STA. 221+75 Q SURVEY US 81
CONST. SMD, TYPE 2, ON Q
STUB 64 LF 18" RCP INTO STR. 46
TG=1206.63, FL=1204.07, DS FL=1202.97

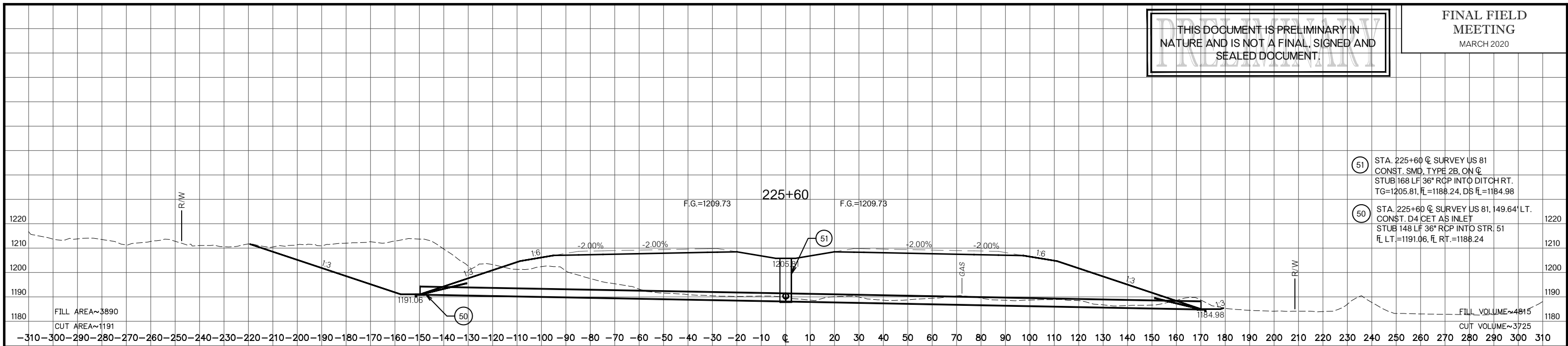
46 STA. 221+75 Q SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' LT.
STUB 72 LF 18" RCP INTO DITCH LT.
TG=1209.35, FL=1202.97, DS FL=1201.58

SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

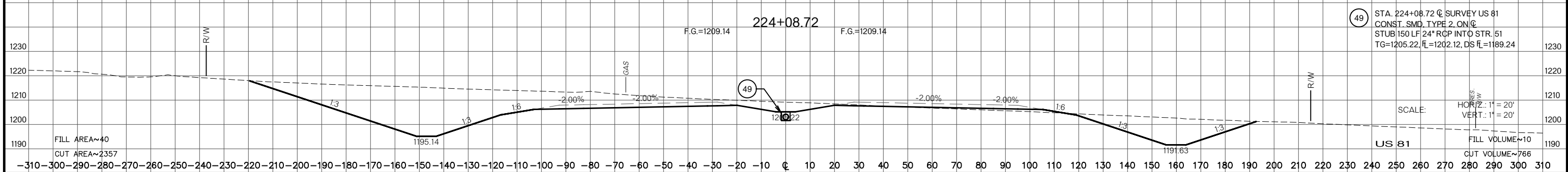
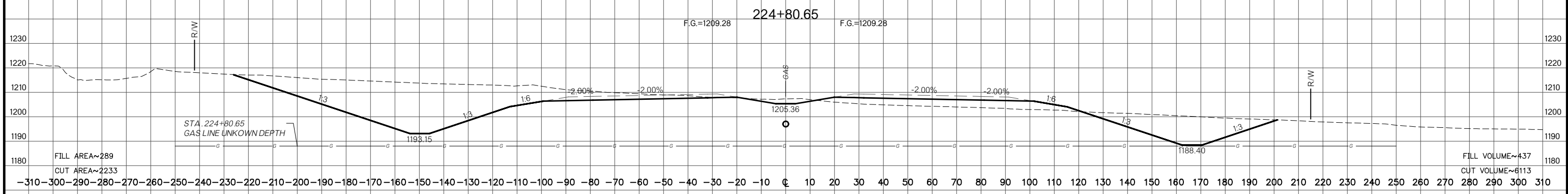
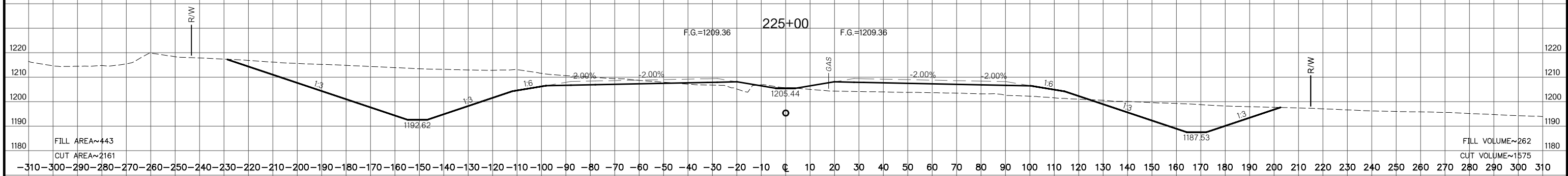
US 81
FILL VOLUME~2923
CUT VOLUME~800

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



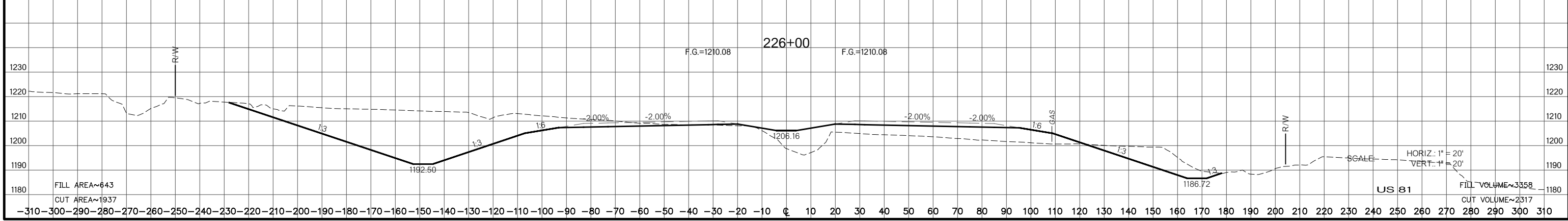
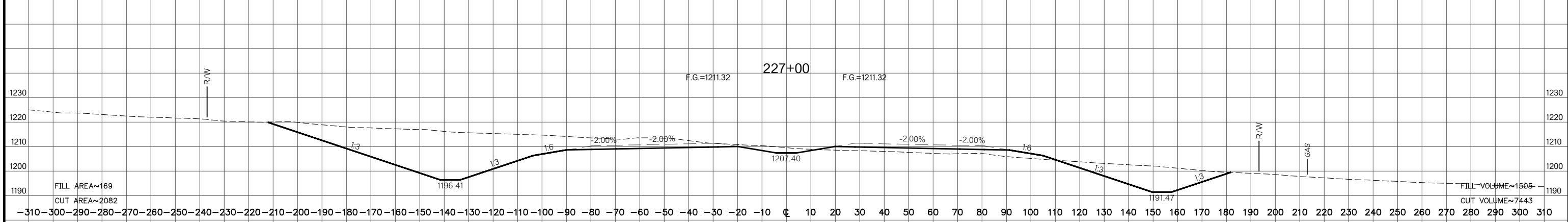
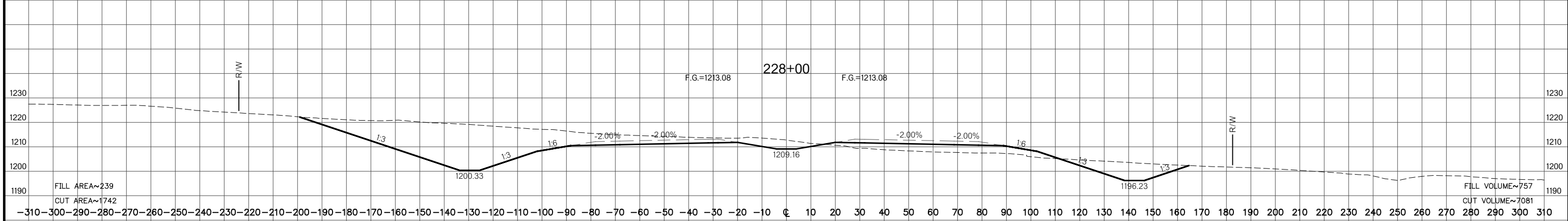
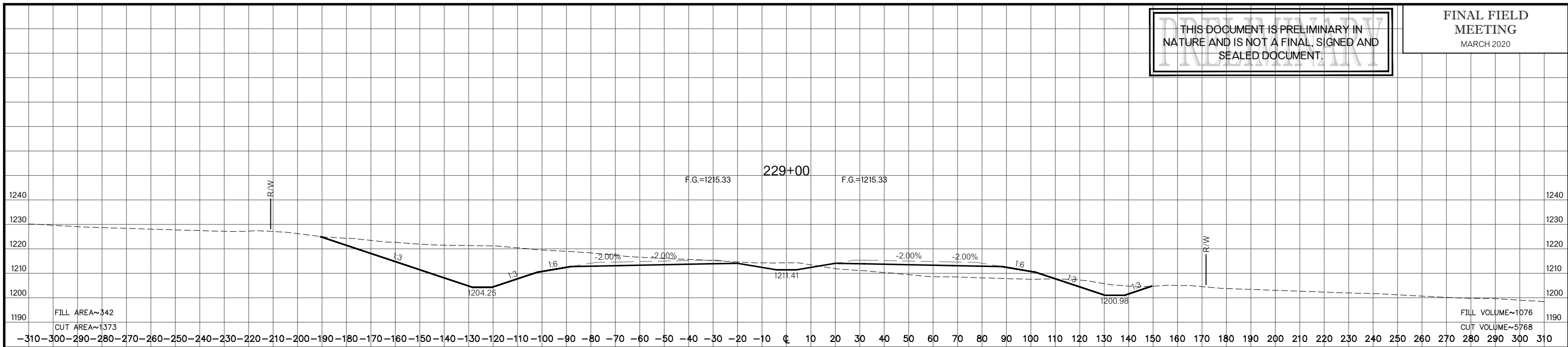
- 51 STA. 225+60 @ SURVEY US 81
CONST. SMD. TYPE 2B, ON @
STUB 168 LF 36" RCP INTO DITCH RT.
TG=1205.81, FL=1188.24, DS FL=1184.98
- 50 STA. 225+60 @ SURVEY US 81, 149.64' LT.
CONST. D4 CET AS INLET
STUB 148 LF 36" RCP INTO STR. 51
FL LT=1191.06, FL RT=1188.24



- 49 STA. 224+08.72 @ SURVEY US 81
CONST. SMD. TYPE 2, ON @
STUB 150 LF 24" RCP INTO STR. 51
TG=1205.22, FL=1202.12, DS FL=1189.24

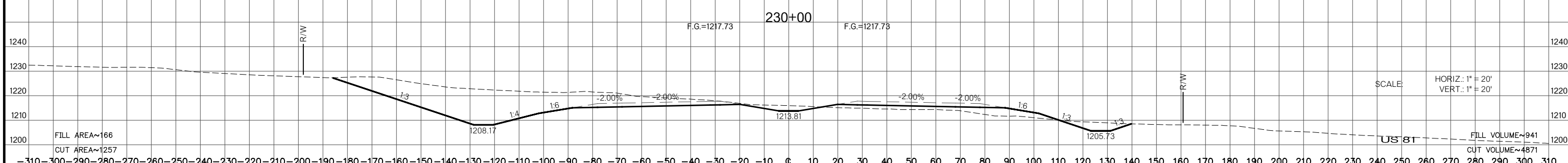
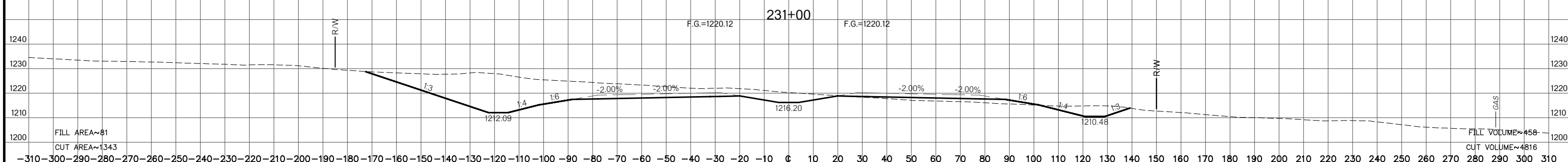
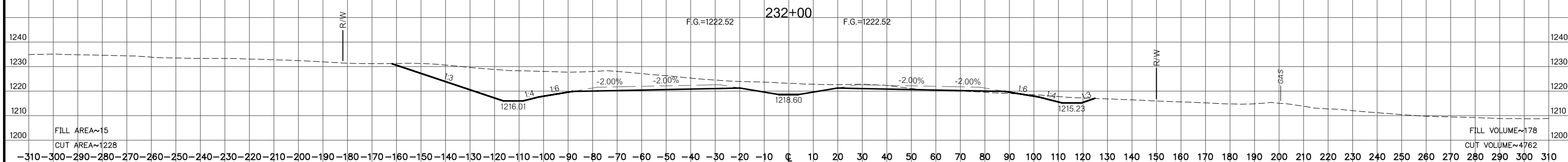
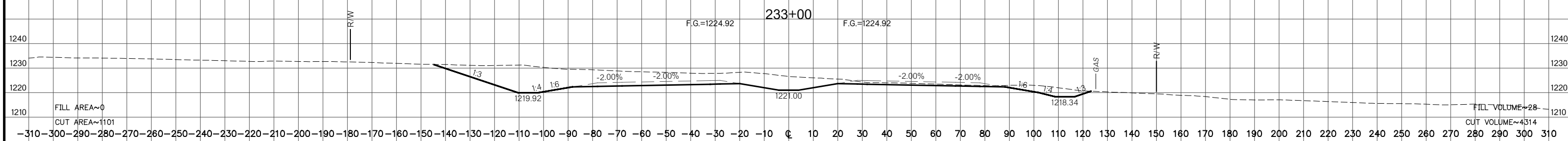
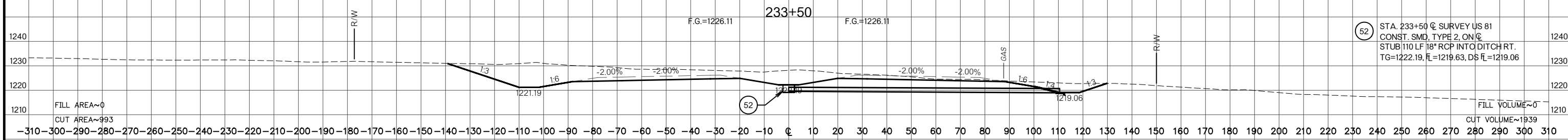
SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

52 STA. 233+50 @ SURVEY US 81 CONST. SMD, TYPE 2, ON @ STUB 110 LF 18" RCP INTO DITCH RT. TG=1222.19, FL=1219.63, DS FL=1219.06



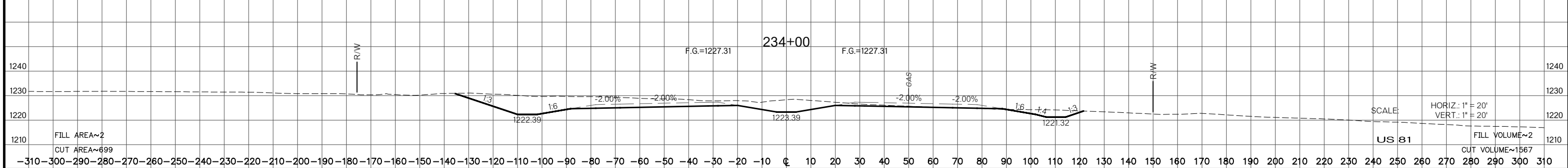
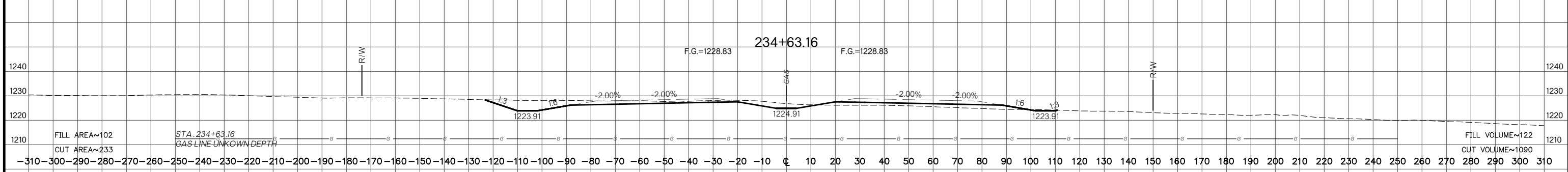
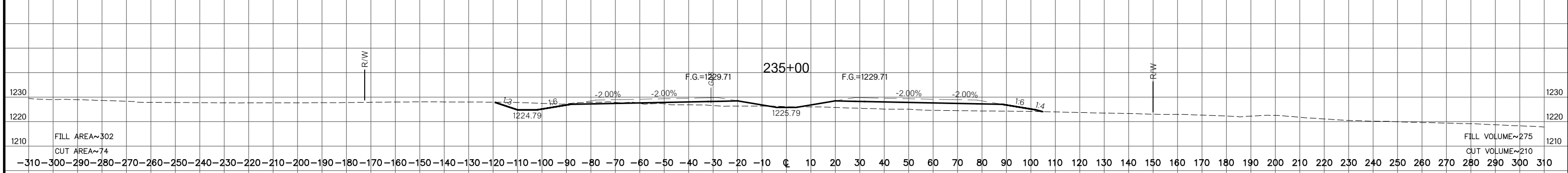
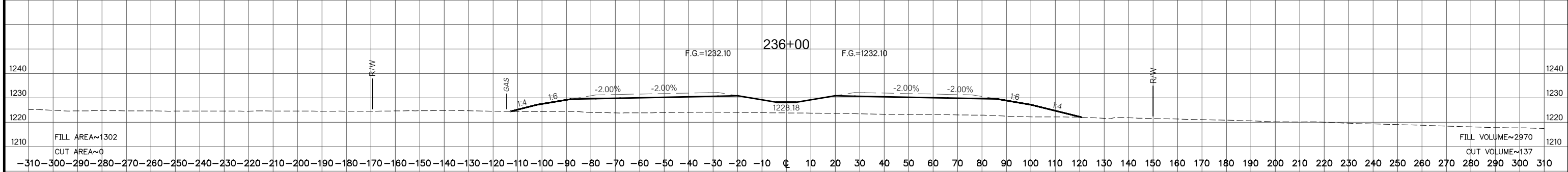
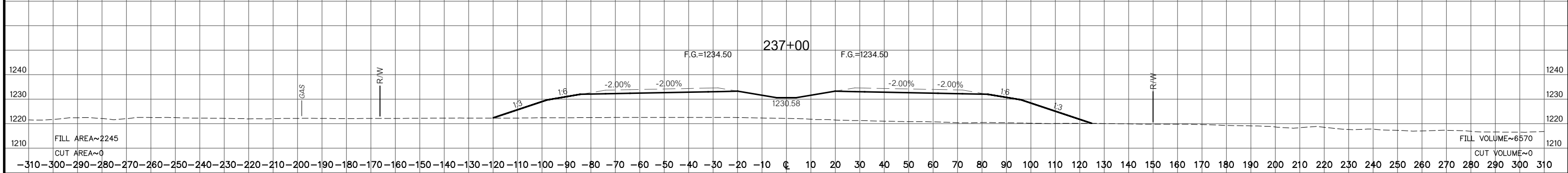
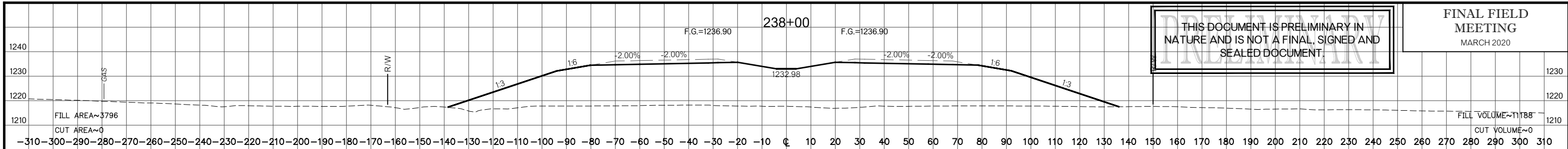
SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

US 81

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020

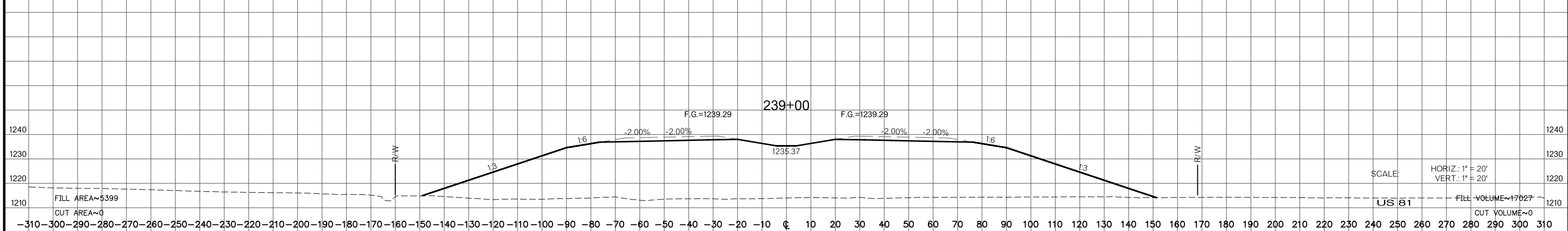
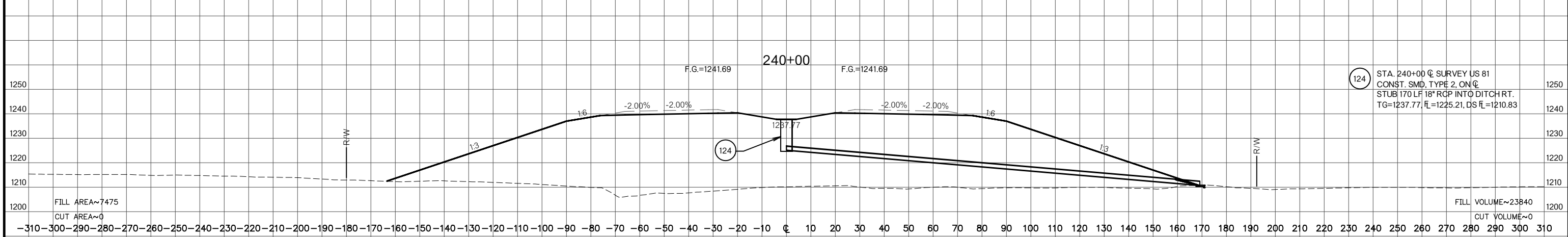
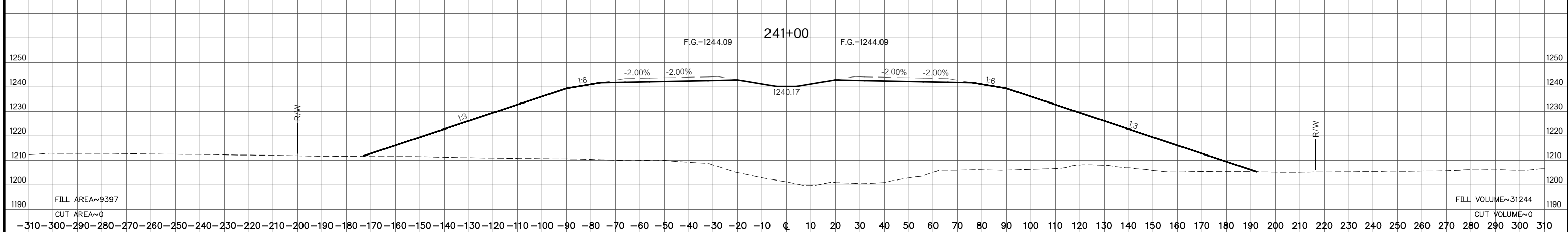
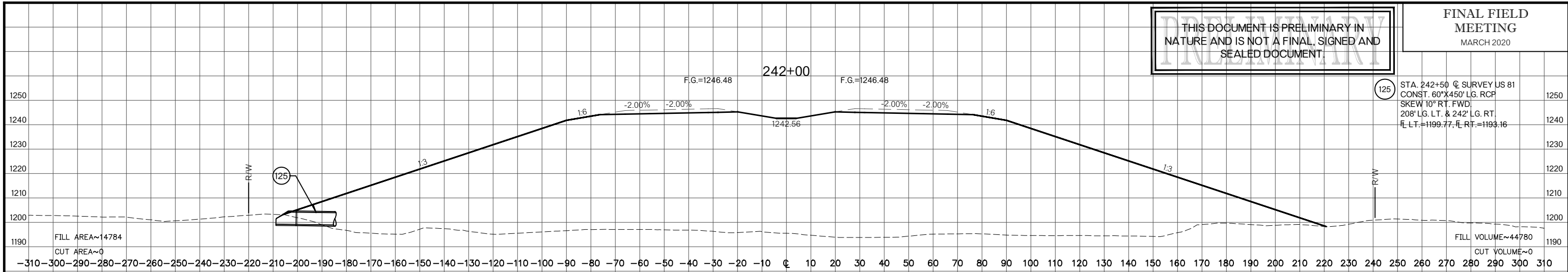


SCALE US 81 HORIZ.: 1" = 20' VERT.: 1" = 20'

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020

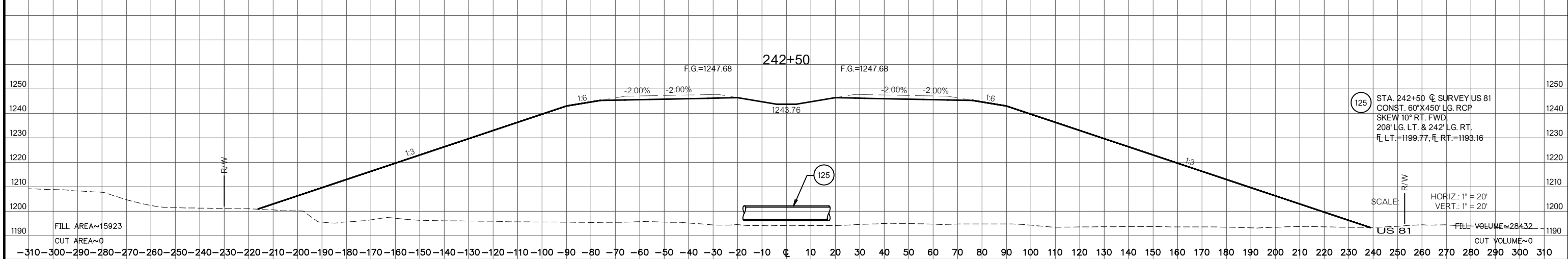
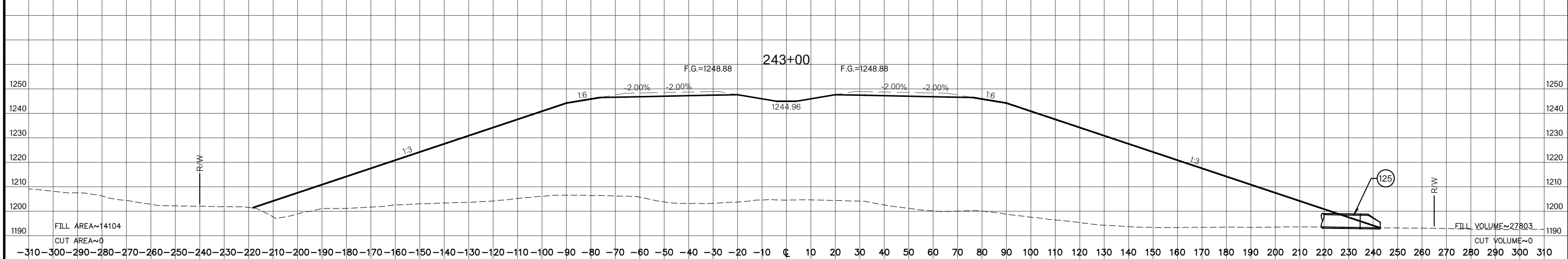
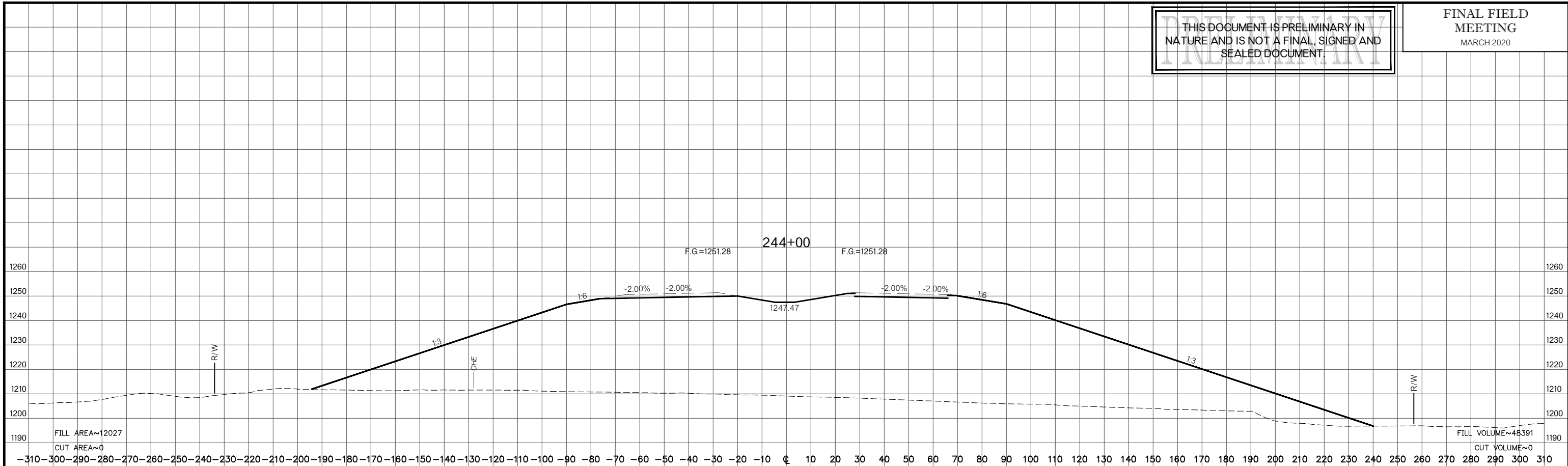


SCALE US 81 HORIZ.: 1" = 20' VERT.: 1" = 20'

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



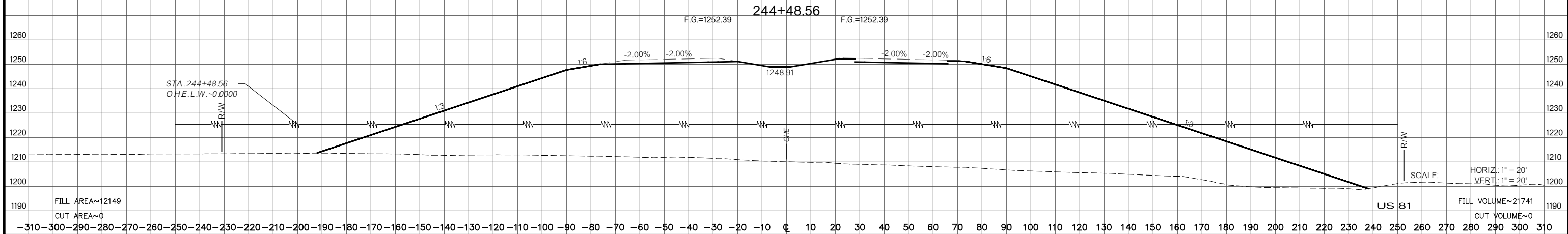
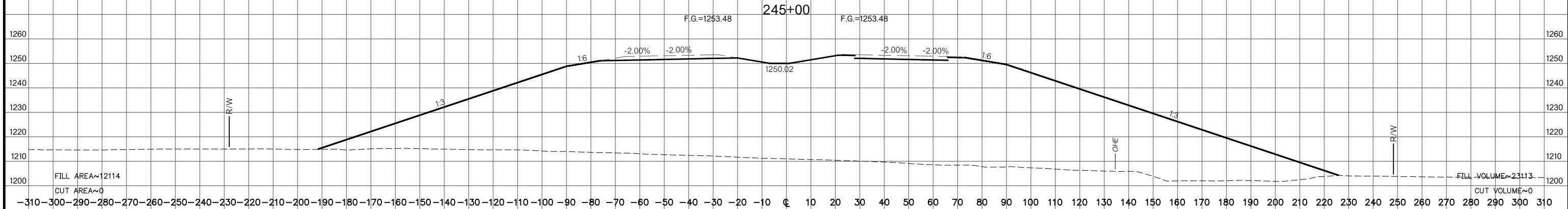
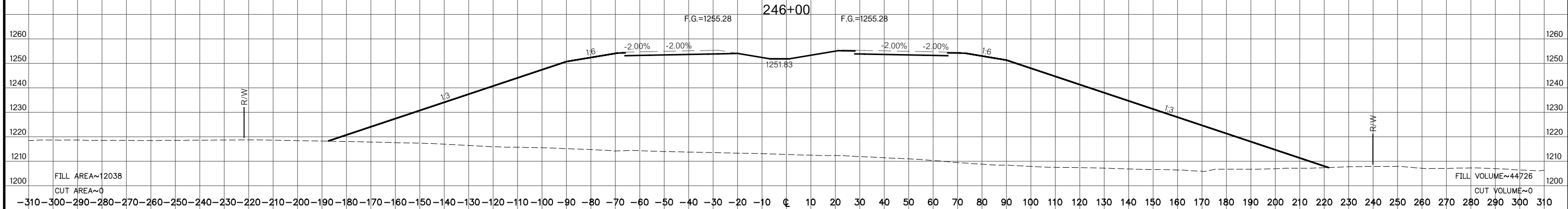
125 STA. 242+50 @ SURVEY US 81
 CONST. 60"X450' LG. RCP
 SKEW 10° RT. FWD.
 208' LG. LT. & 242' LG. RT.
 FL LT.=1199.77, FL RT.=1199.16

SCALE R/W
 US 81
 HORIZ.: 1" = 20'
 VERT.: 1" = 20'

US 81 REALIGNMENT
 GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

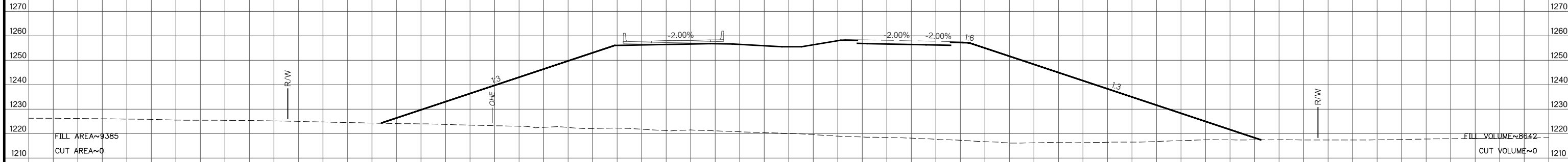


GRADY COUNTY
US 81 REALIGNMENT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

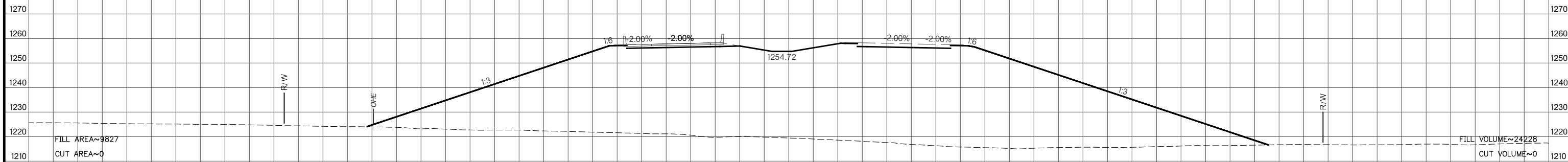
FIRST FULL STATION 248+88.39

F.G.=1258.30 F.G.=1258.30



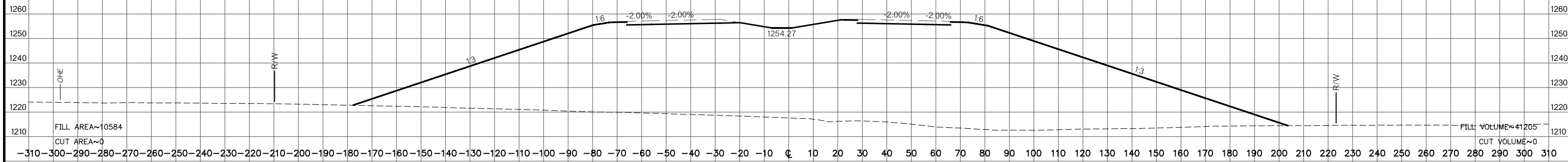
BEGIN APPROACH BRIDGE H 248+64.10

F.G.=1258.17 F.G.=1258.17



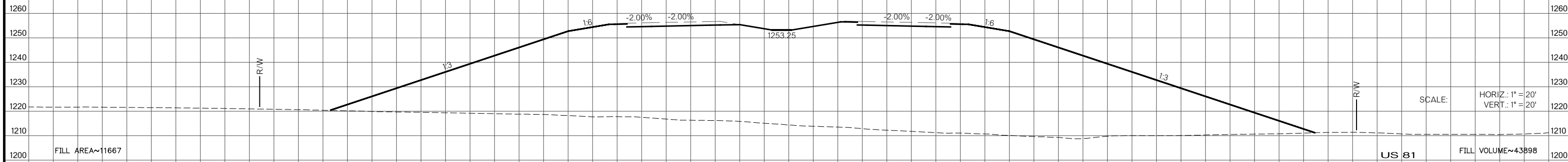
248+00

F.G.=1257.72 F.G.=1257.72



247+00

F.G.=1256.70 F.G.=1256.70

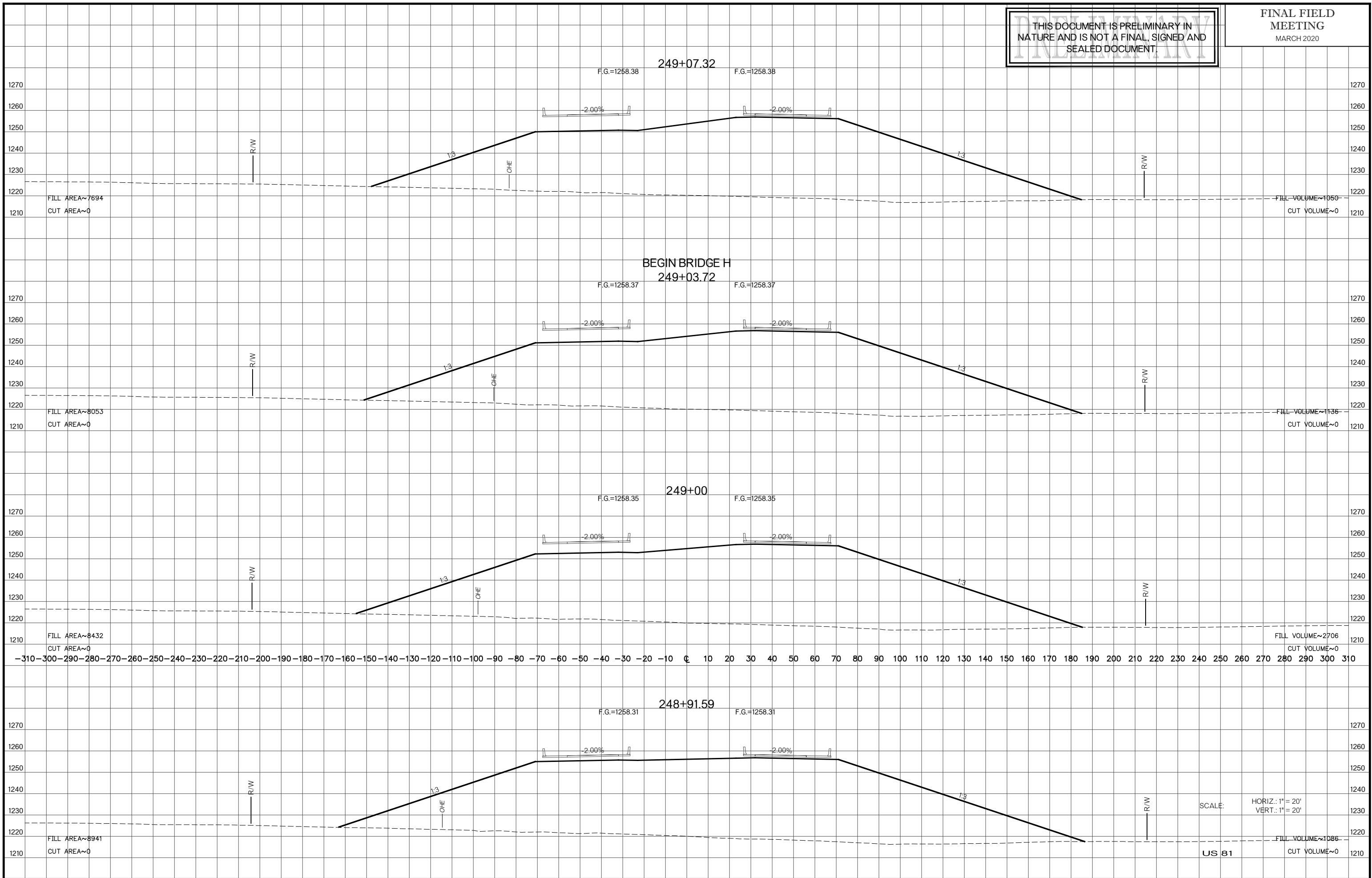


SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

US 81

GRADY COUNTY US 81 REALIGNMENT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

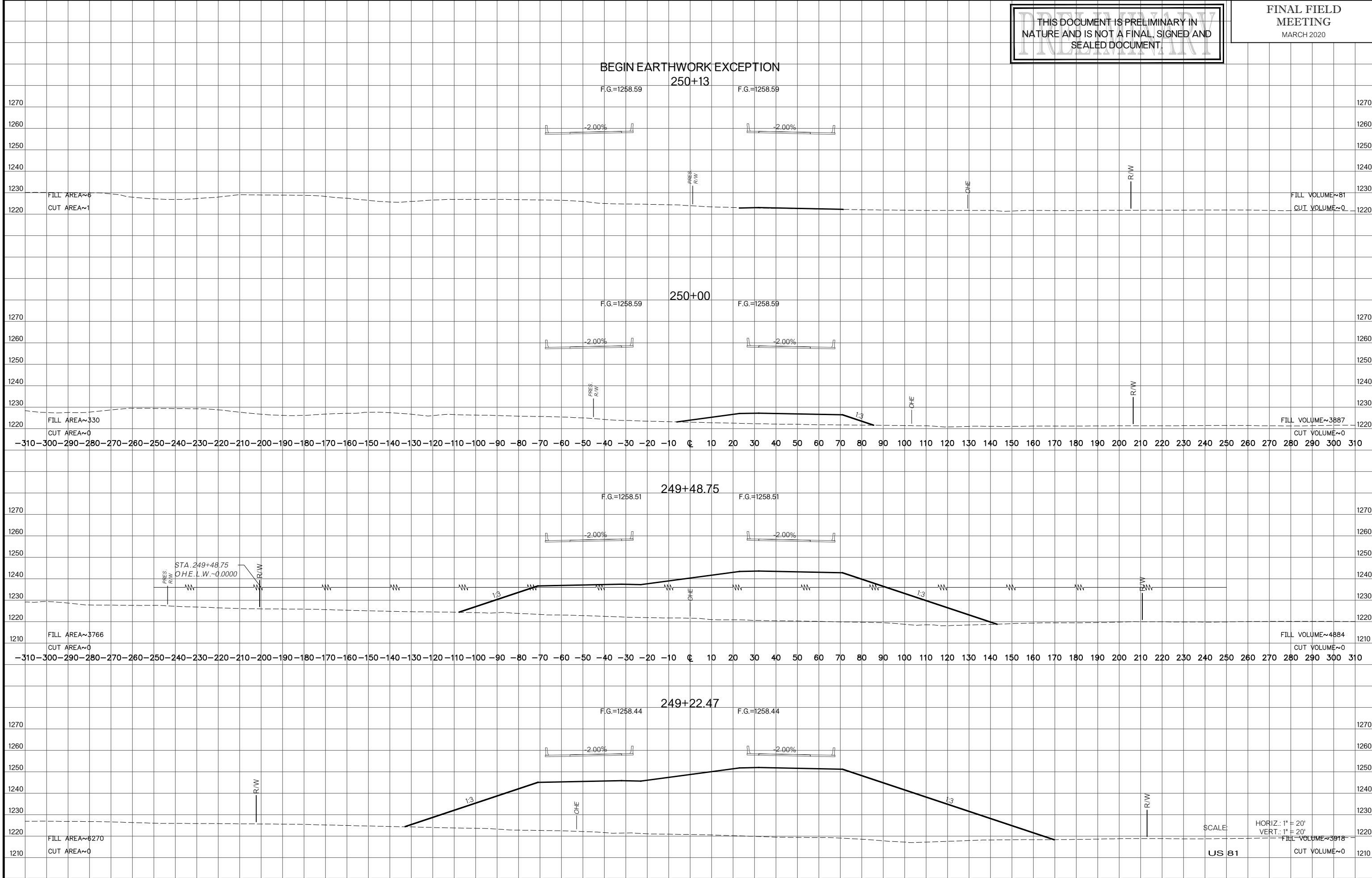


SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

GRADY COUNTY
US 81 REALIGNMENT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020

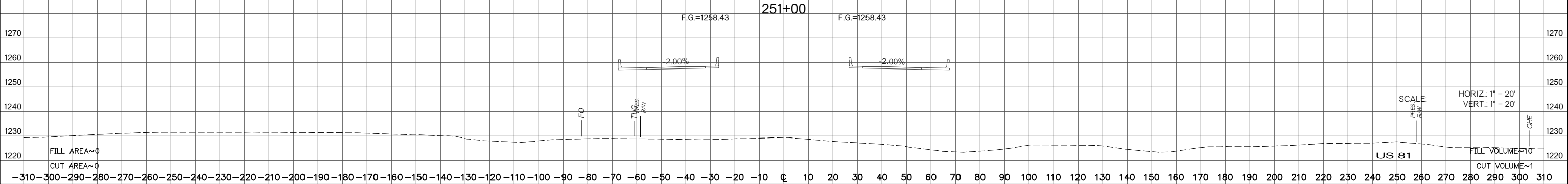
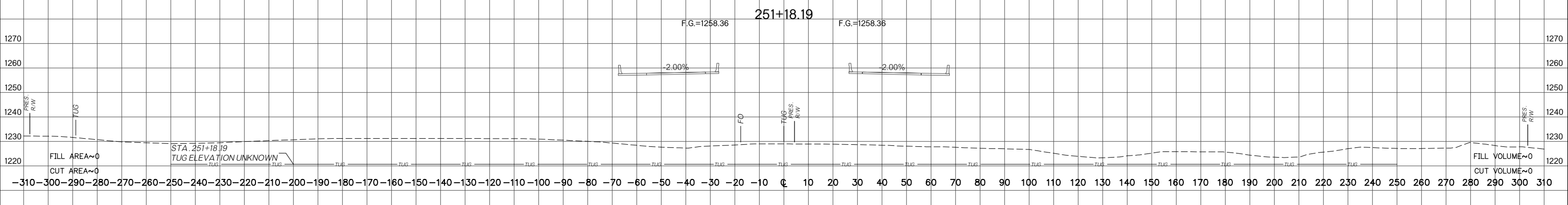
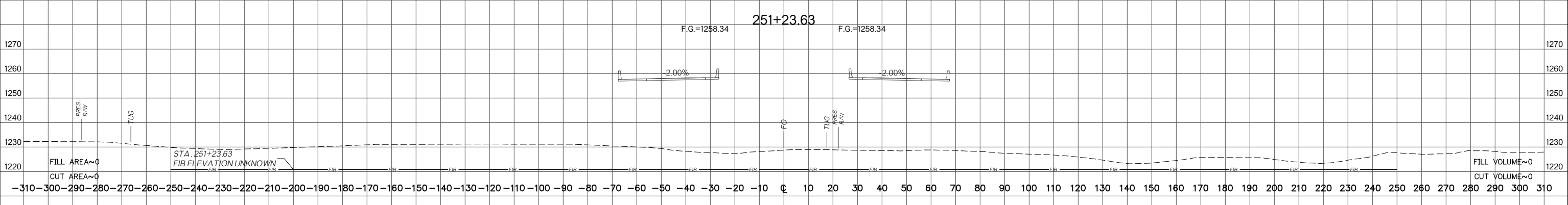
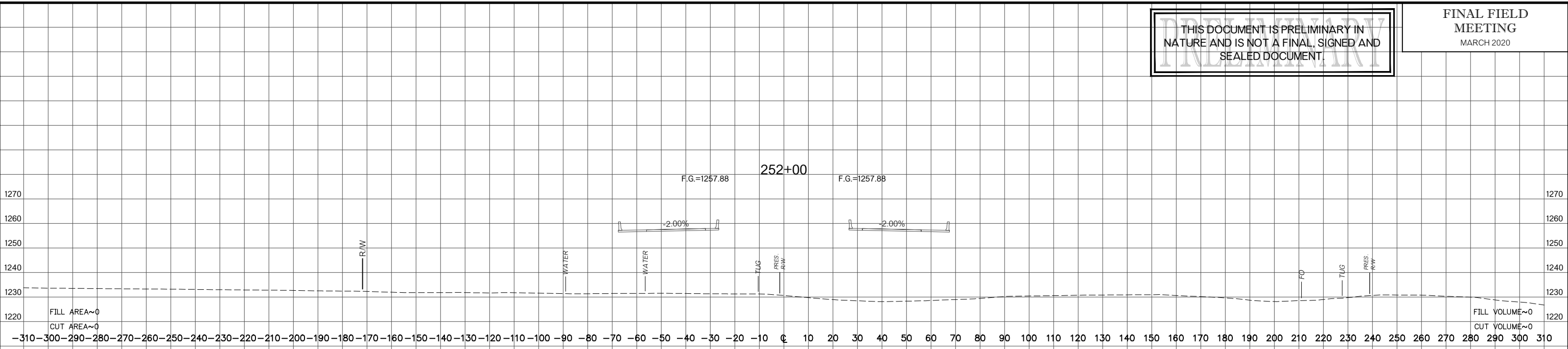


SCALE US 81
HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

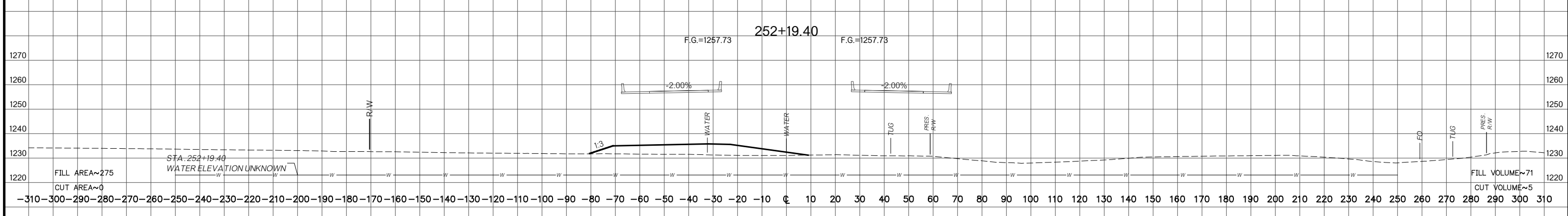
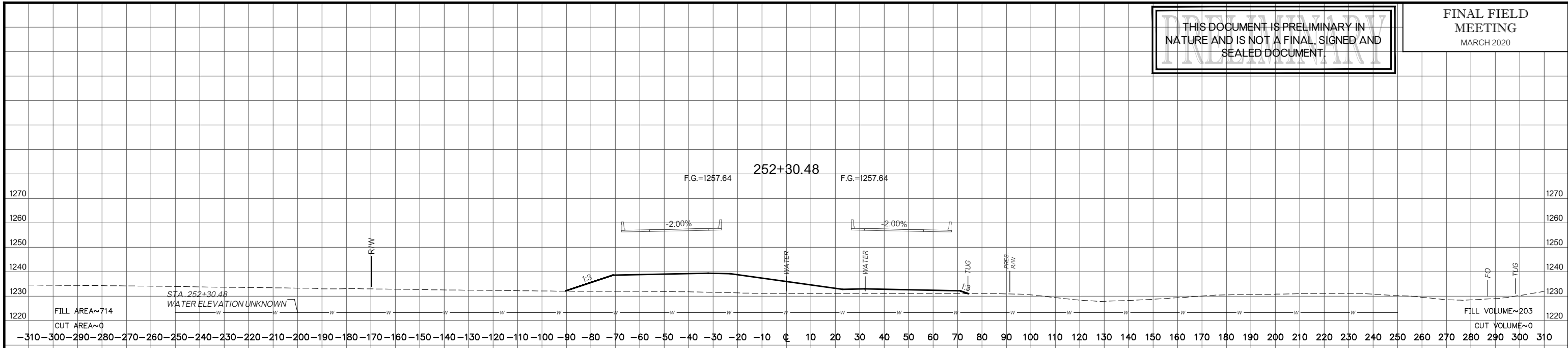
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

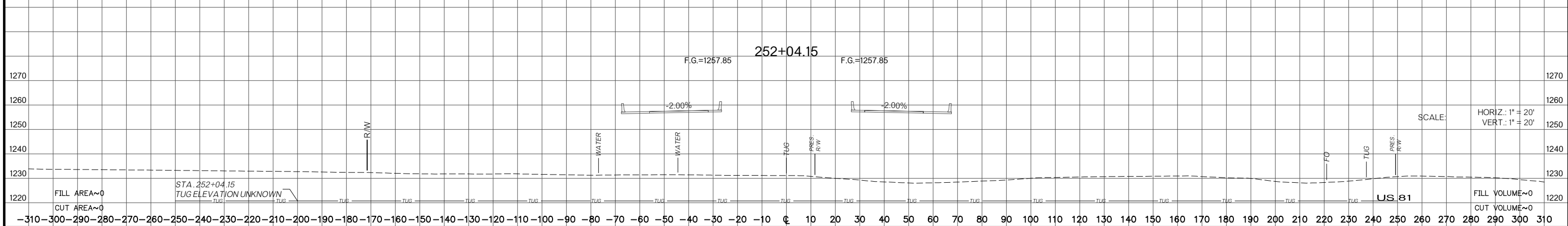
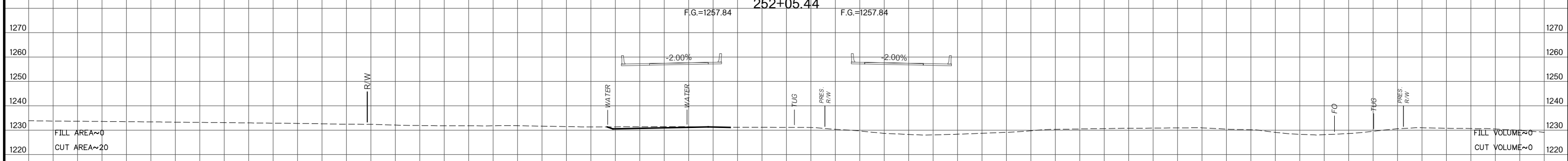


US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



END EARTHWORK EXCEPTION

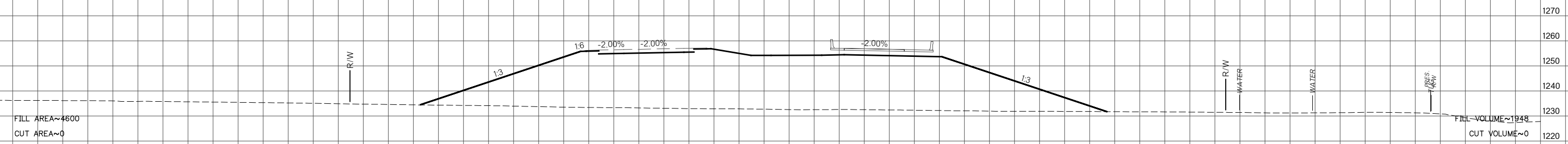


US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

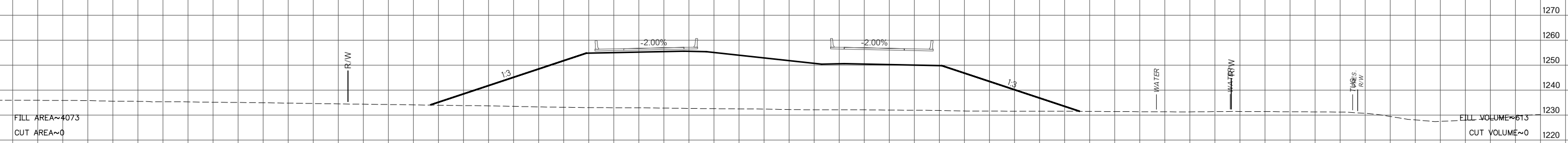
END APPROACH BRIDGE H
252+97.66

F.G.=1256.97 F.G.=1256.97



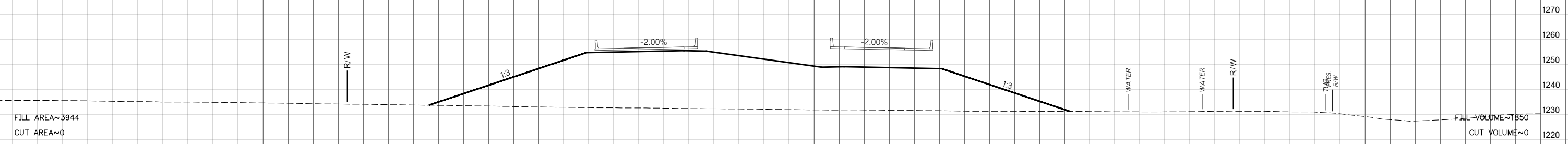
252+85.53

F.G.=1257.10 F.G.=1257.10



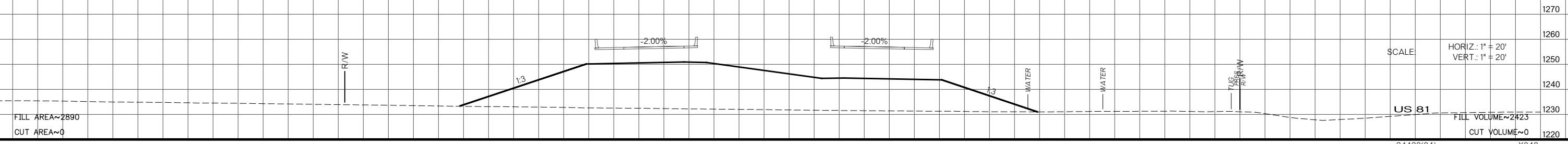
LAST FULL STATION BRIDGE H
252+81.40

F.G.=1257.15 F.G.=1257.15



END BRIDGE H
252+66.78

F.G.=1257.30 F.G.=1257.30

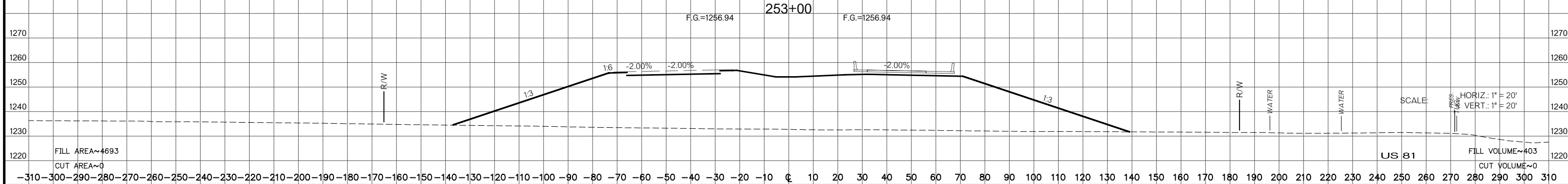
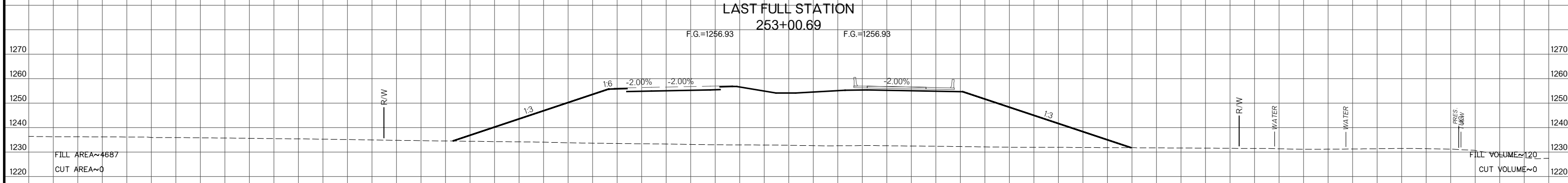
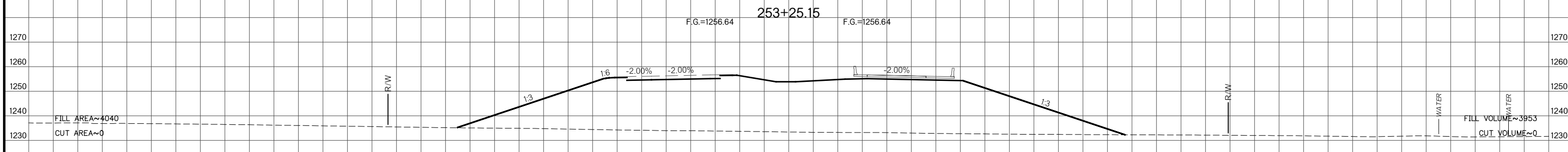
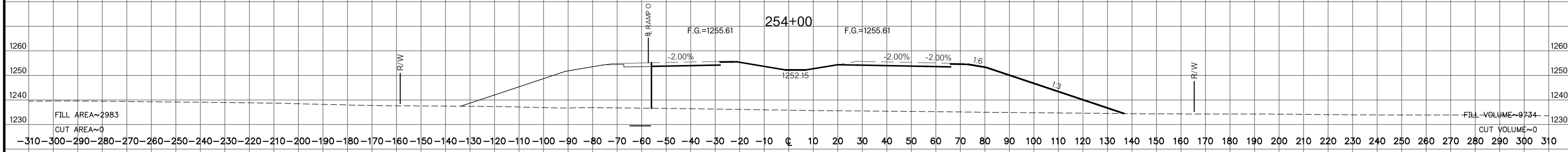


SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81

US 81 REALIGNMENT
GRADY COUNTY

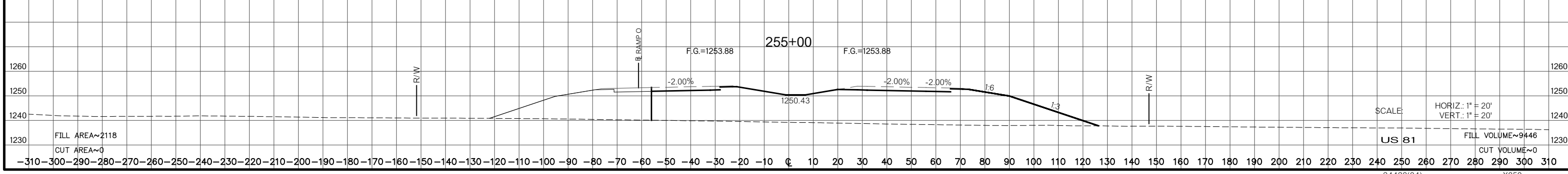
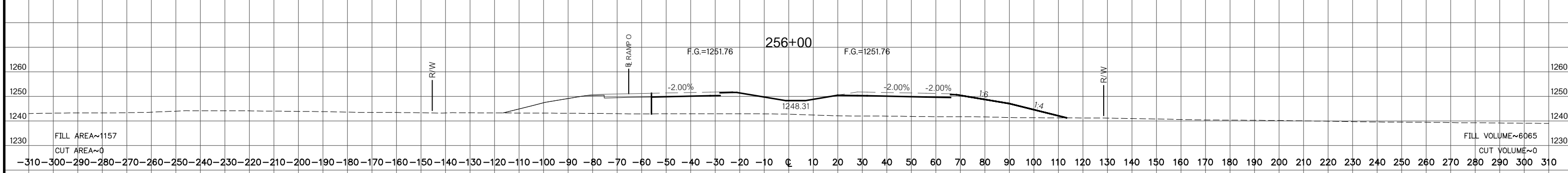
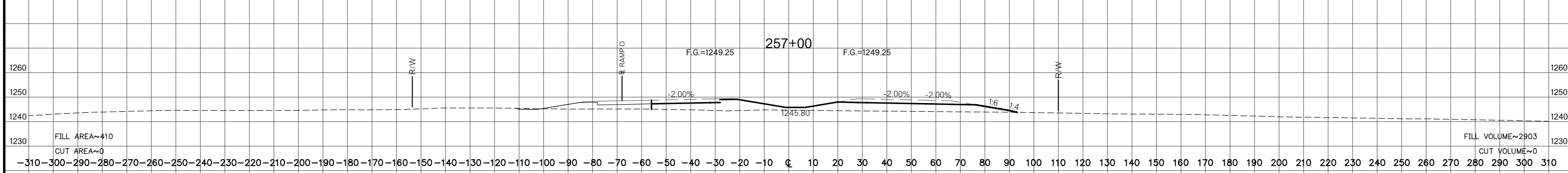
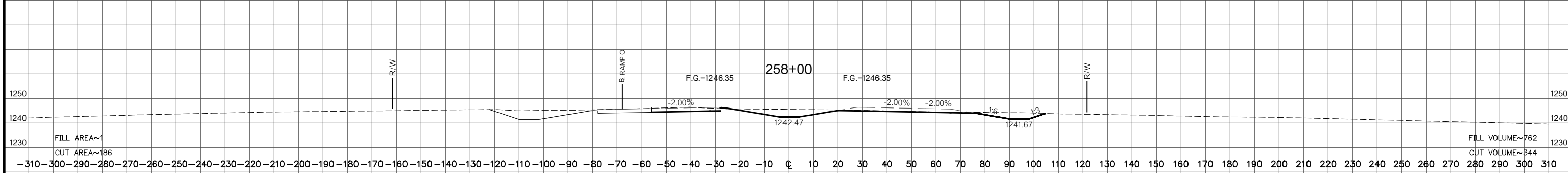
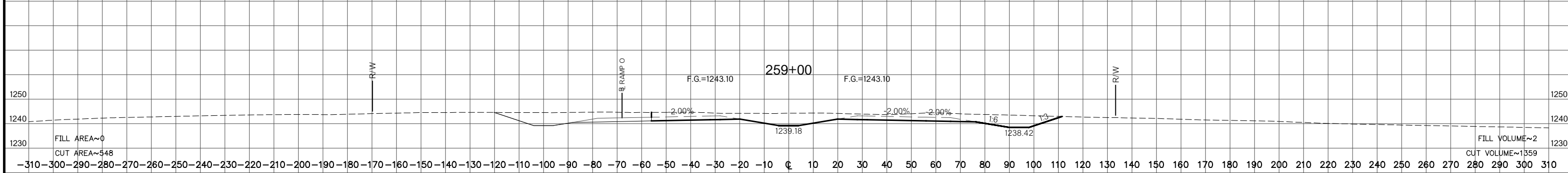
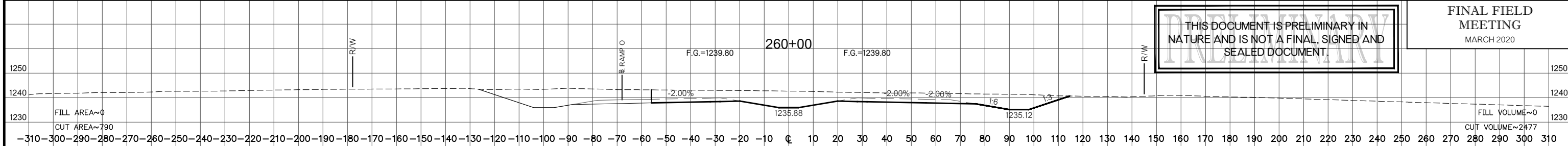
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

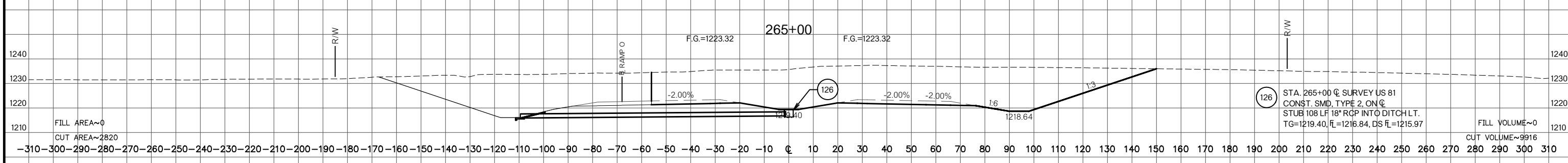
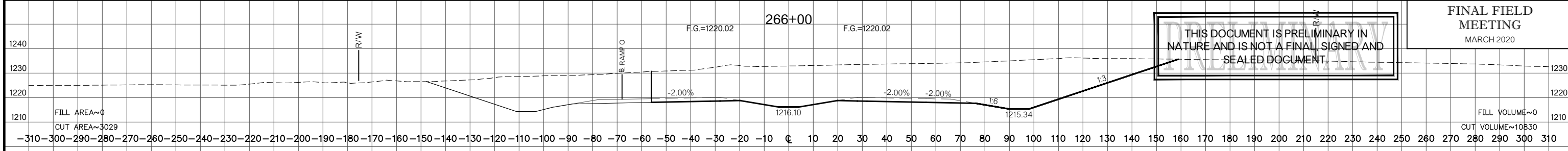


SCALE
US 81
HORIZ.: 1" = 20'
VERT.: 1" = 20'

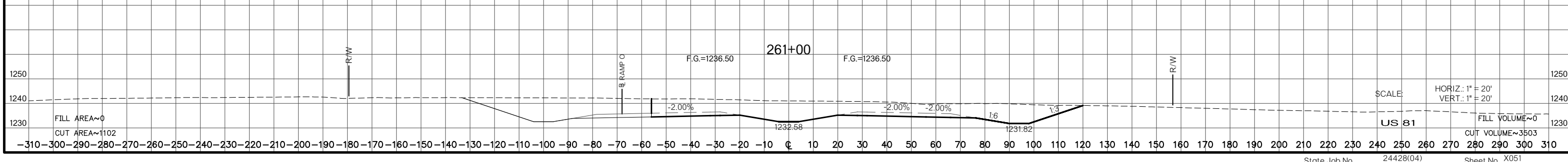
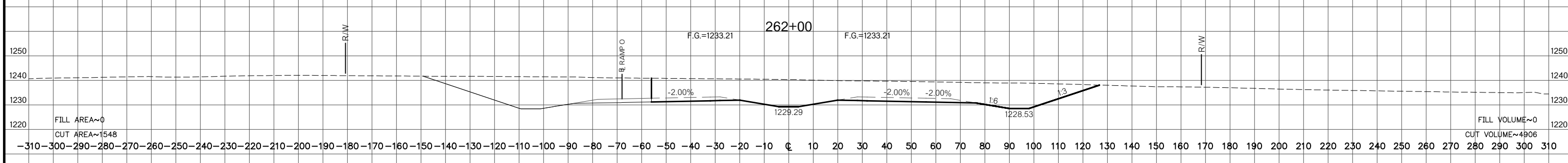
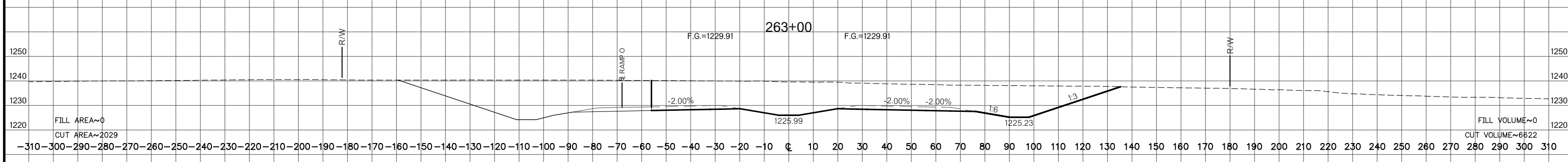
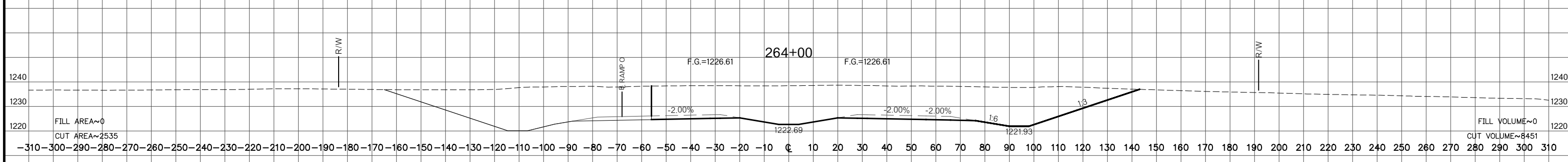
FILL VOLUME~9446
CUT VOLUME~0

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL SIGNED AND SEALED DOCUMENT.



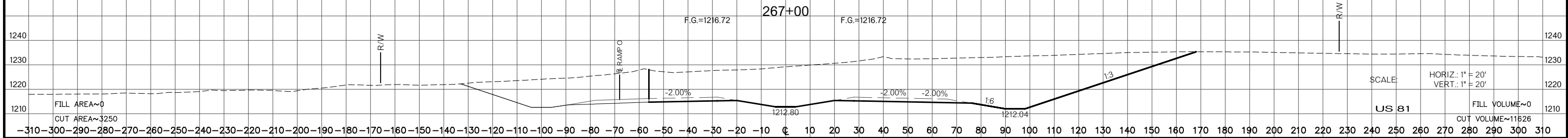
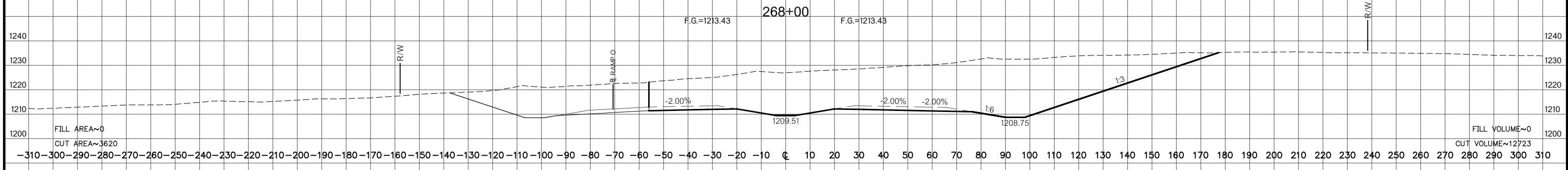
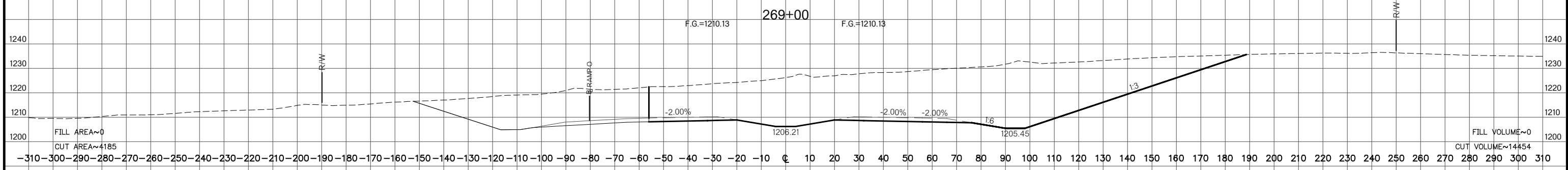
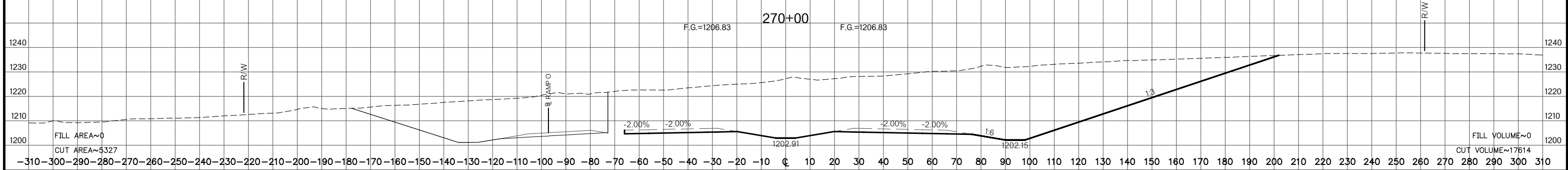
126 STA. 265+00 @ SURVEY US 81
CONST. SMD. TYPE 2 ON @
STUB 108 LF 18" RCP INTO DITCH LT.
TG=1219.40, f_L =1216.84, DS f_L =1215.97



SCALE
US 81
HORIZ.: 1" = 20'
VERT.: 1" = 20'

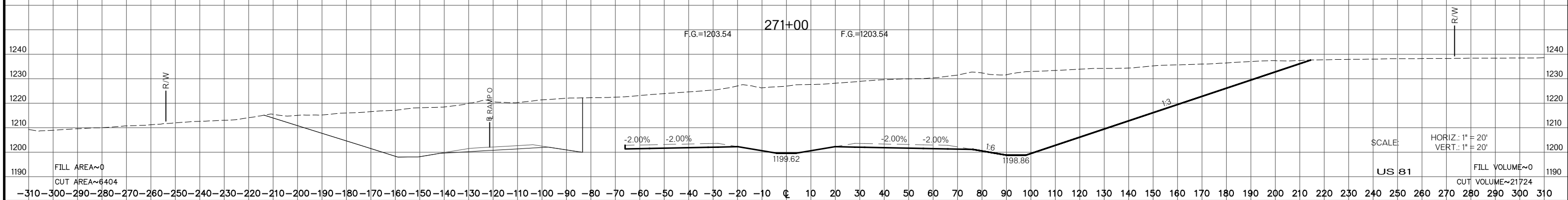
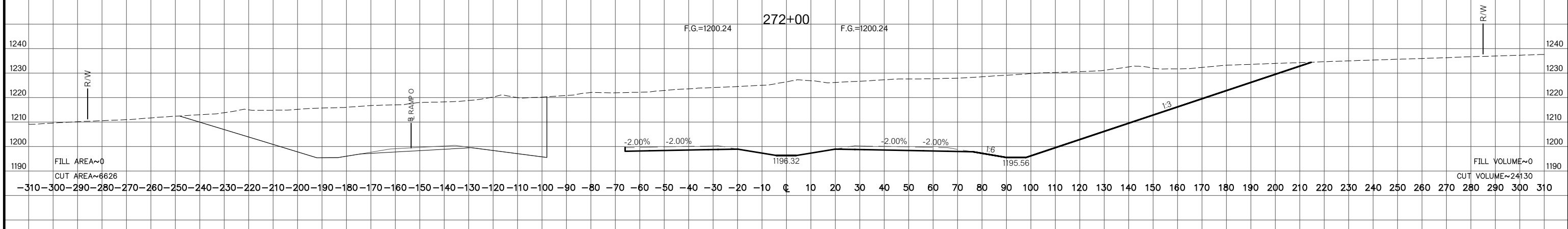
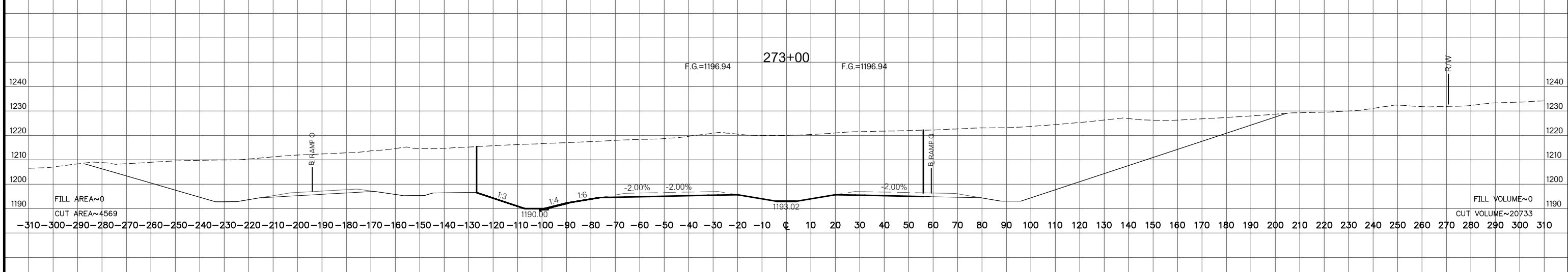
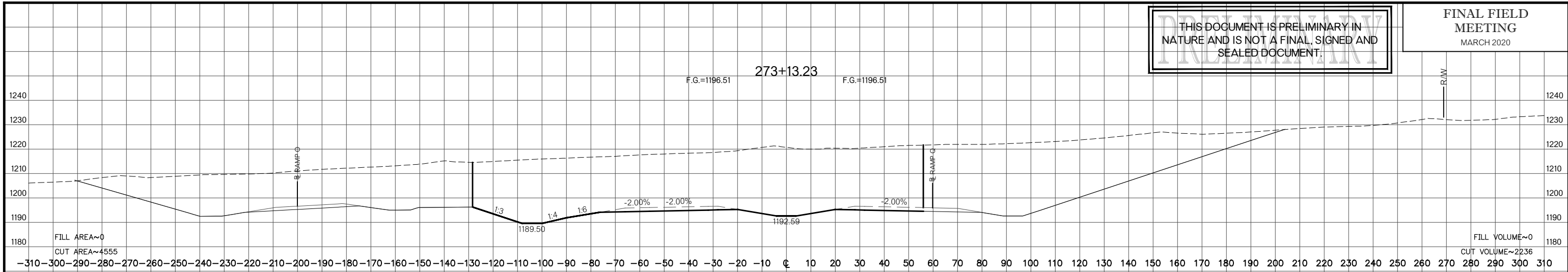
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

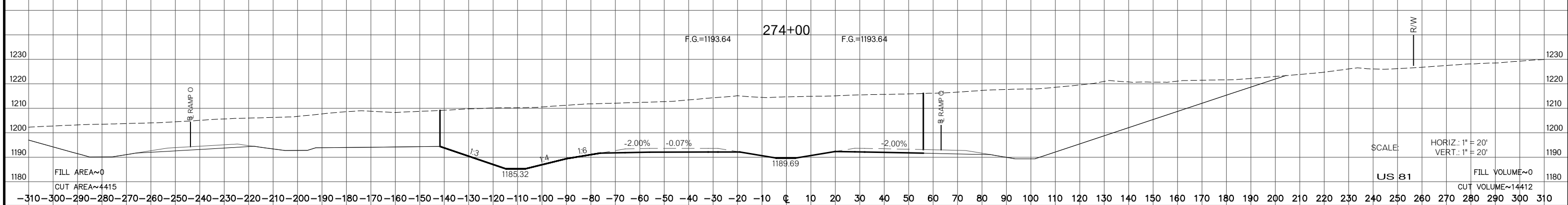
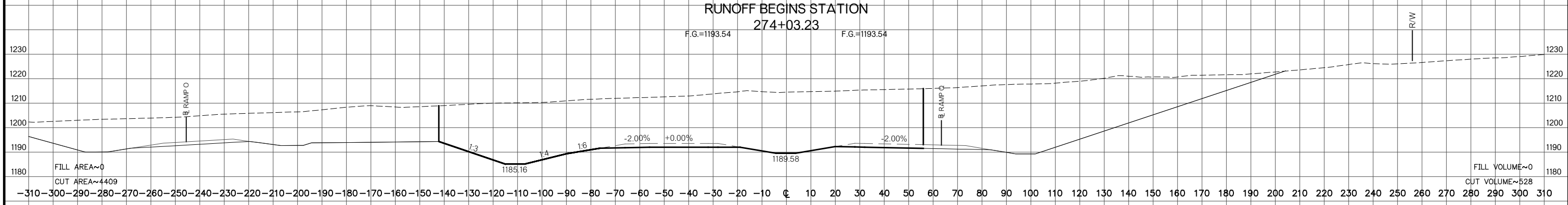
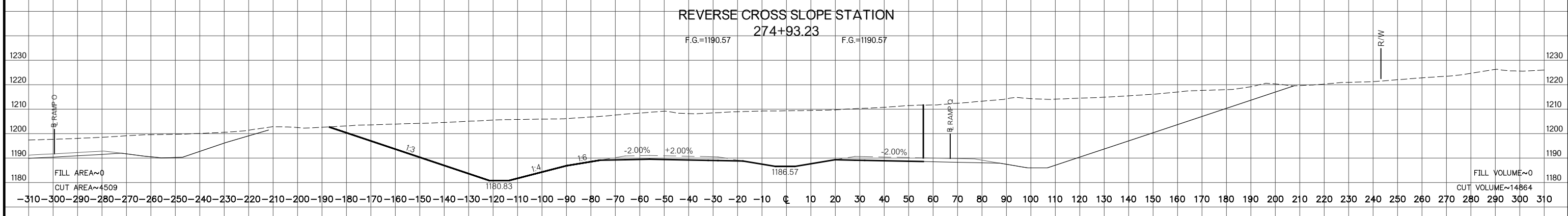
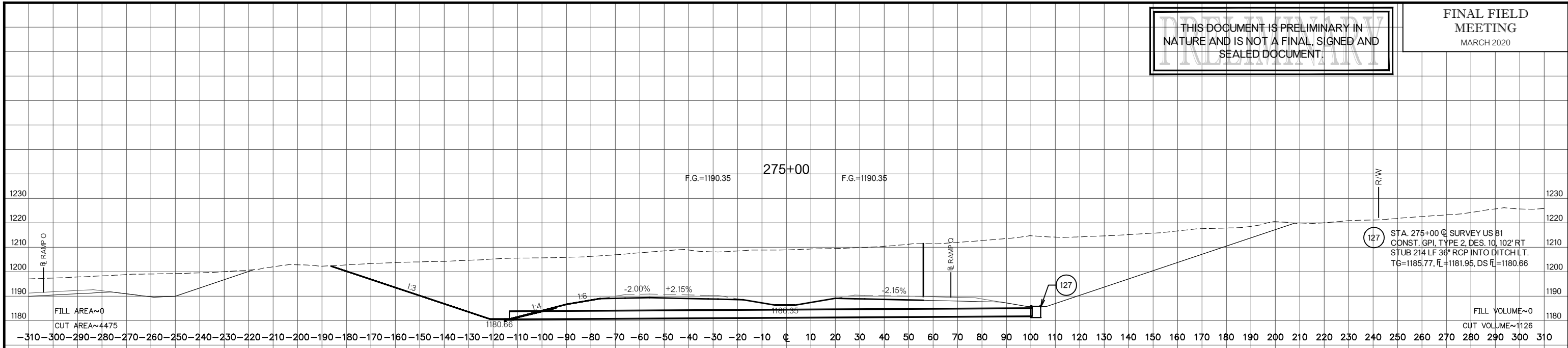
FINAL FIELD MEETING
MARCH 2020



SCALE
US 81
HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

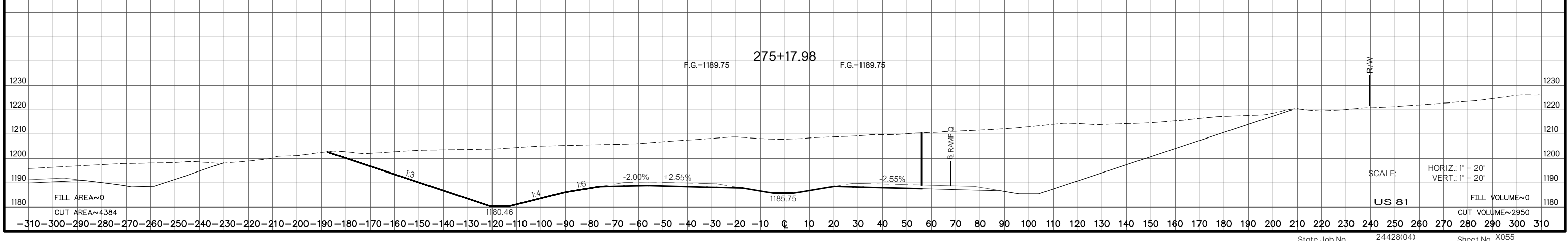
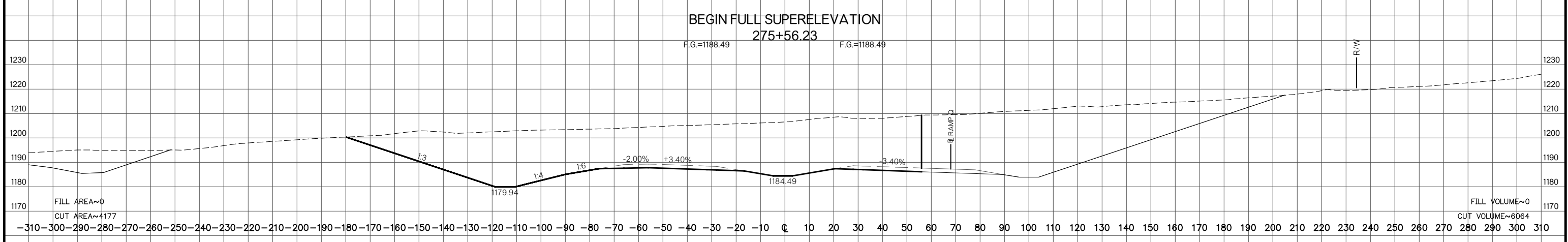
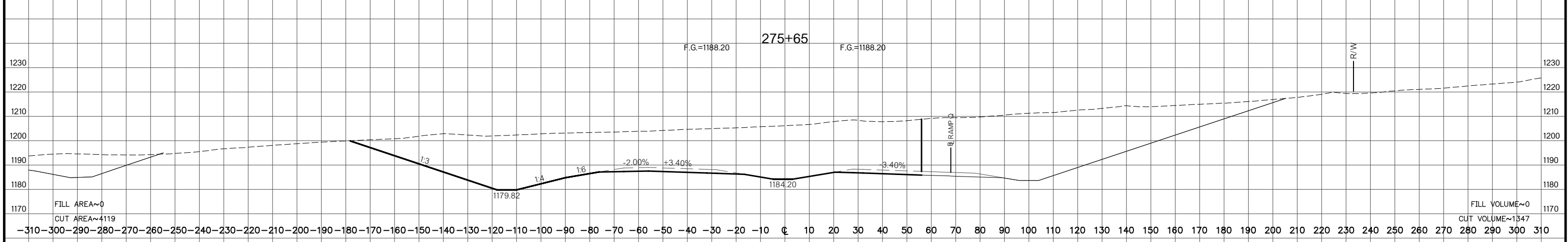
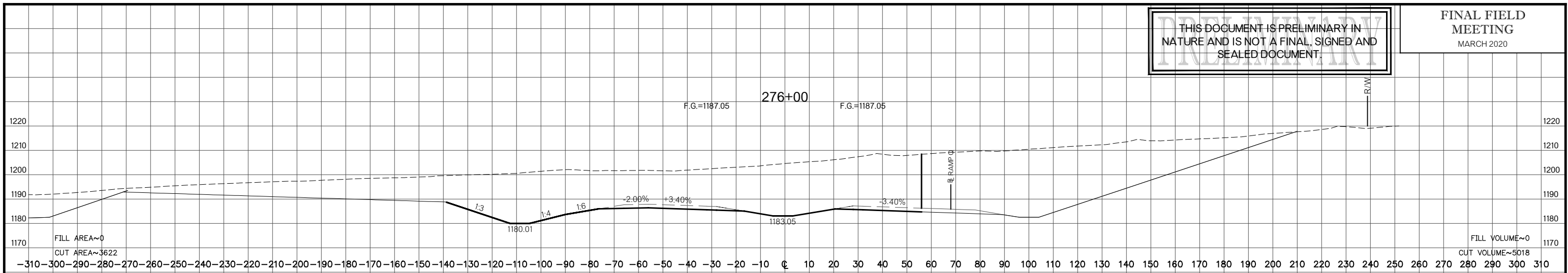


SCALE HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



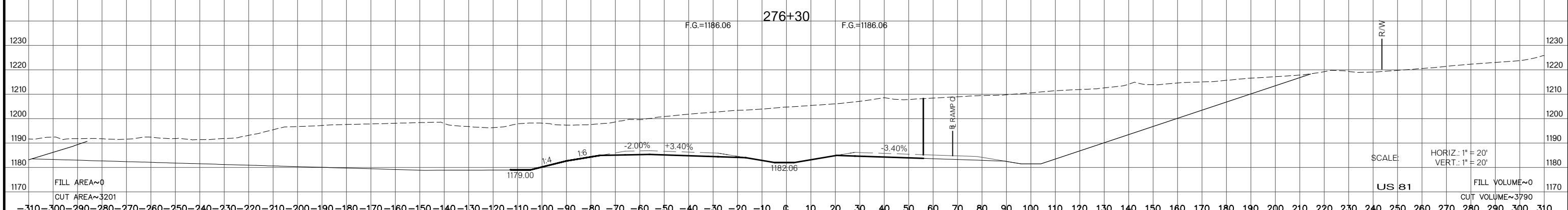
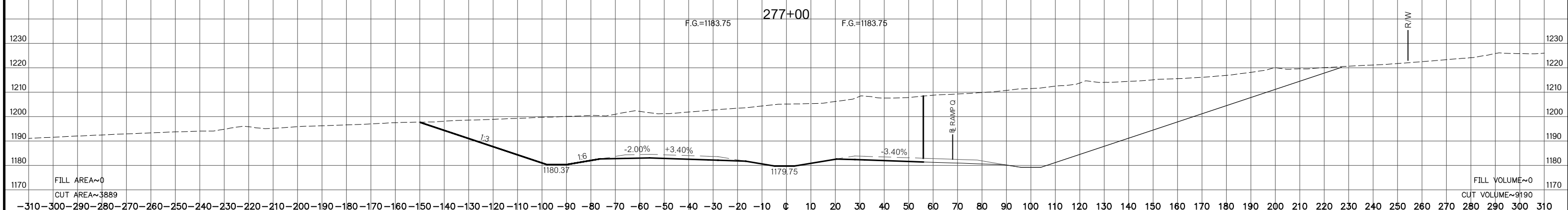
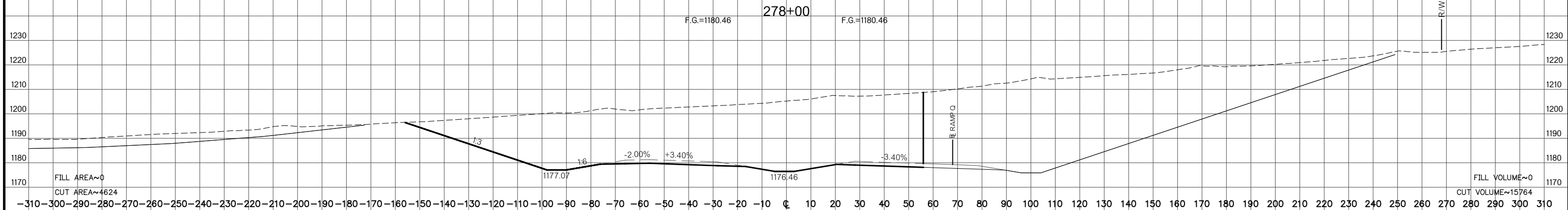
SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

State Job No. 24428(04) Sheet No. X055

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

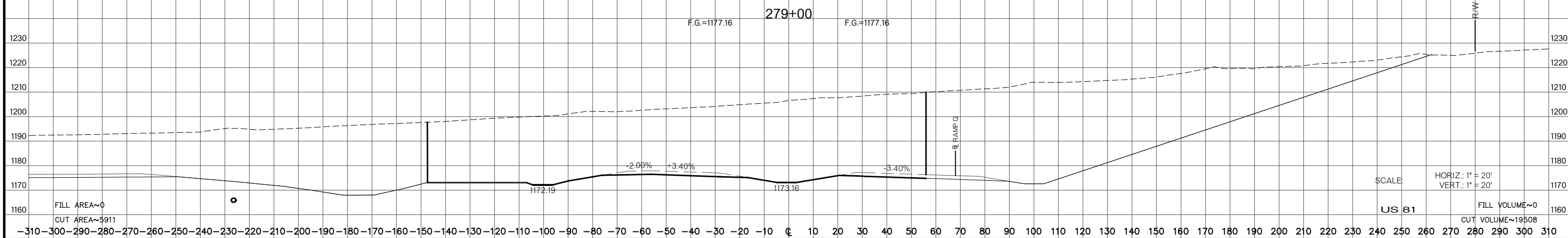
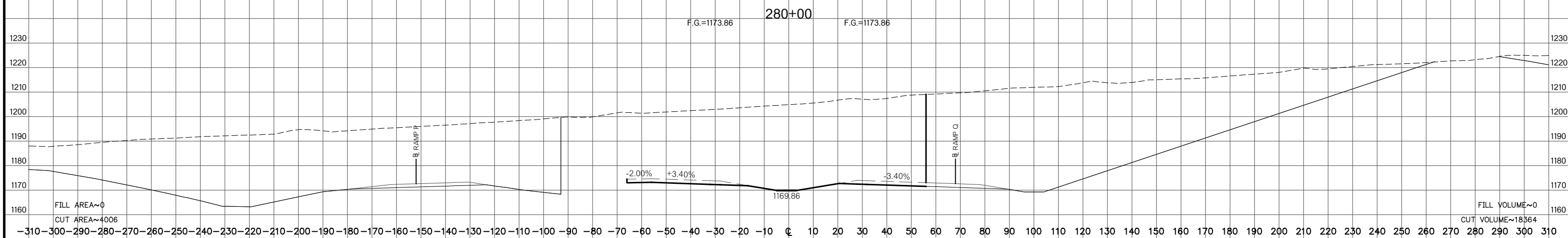
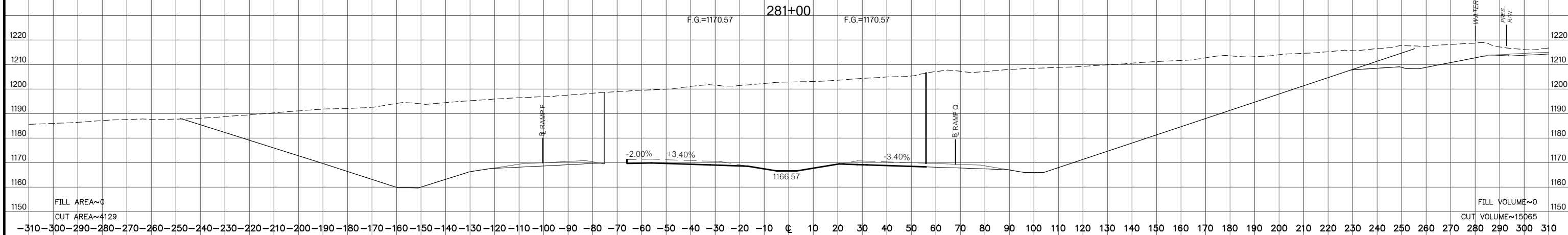


SCALE
US 81
HORIZ.: 1" = 20'
VERT.: 1" = 20'
State Job No. 24428(04) Sheet No. X056

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

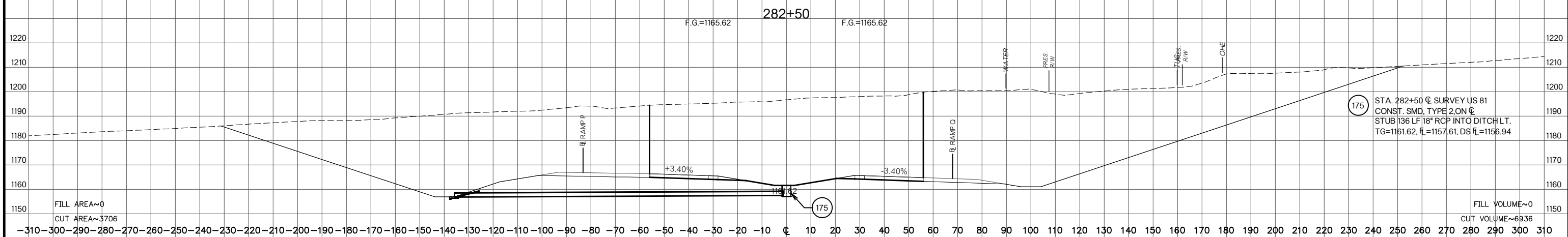
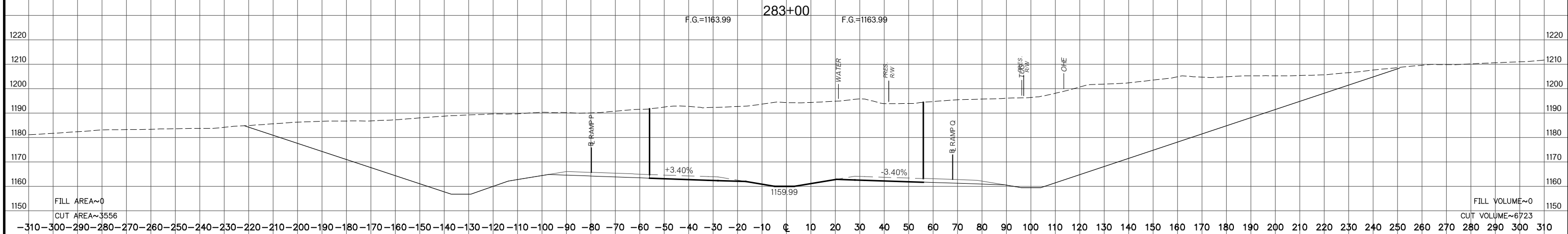


SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

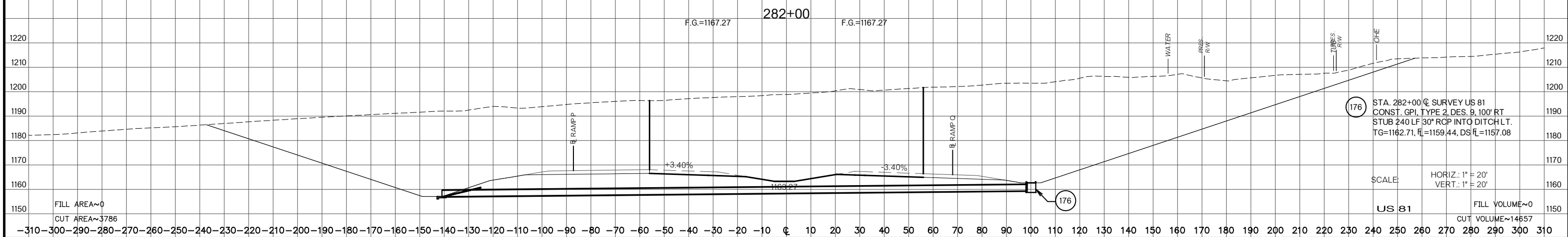
US 81
FILL VOLUME~0
CUT VOLUME~19508

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



(175) STA. 282+50 @ SURVEY US 81
CONST. SMD, TYPE 2, ON @
STUB 136 LF 18" RCP INTO DITCH LT.
TG=1161.62, FL=1157.61, DS FL=1156.94



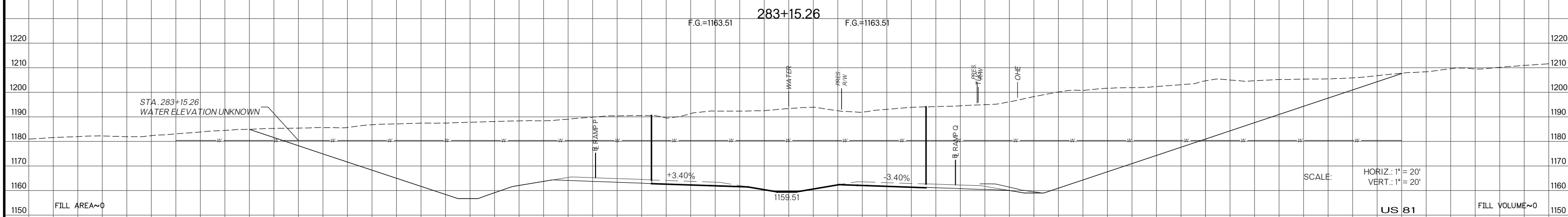
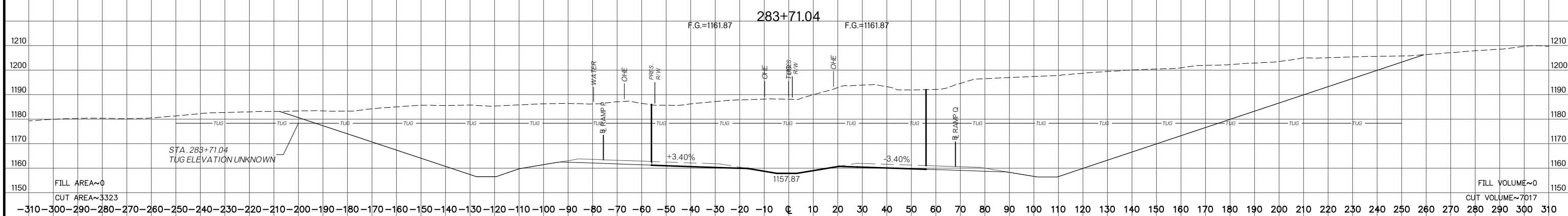
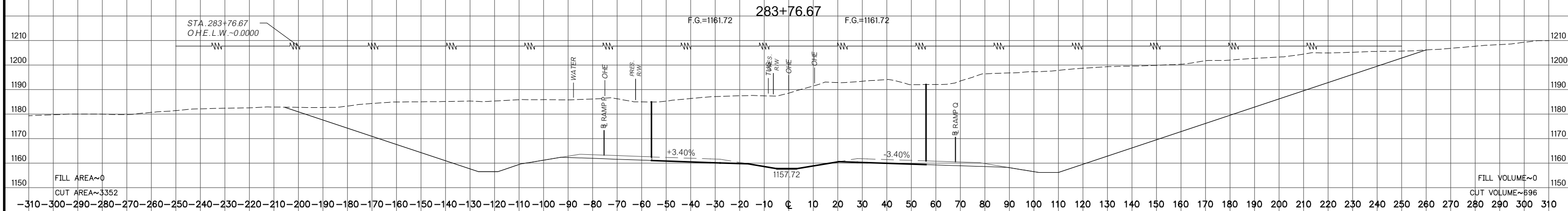
(176) STA. 282+00 @ SURVEY US 81
CONST. GPI, TYPE 2, DES. 9, 100' RT
STUB 240 LF 30" RCP INTO DITCH LT.
TG=1162.71, FL=1159.44, DS FL=1157.08

SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

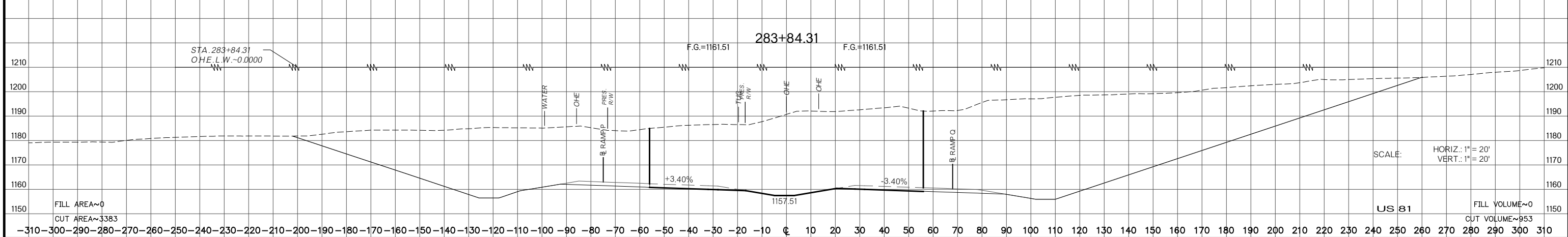
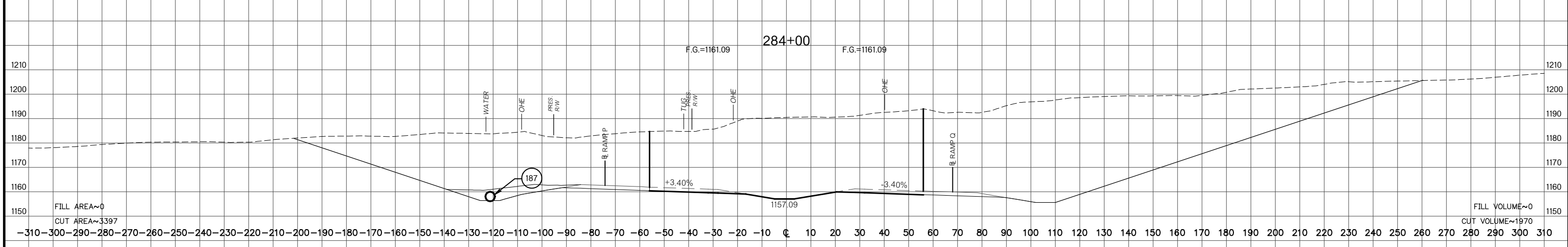
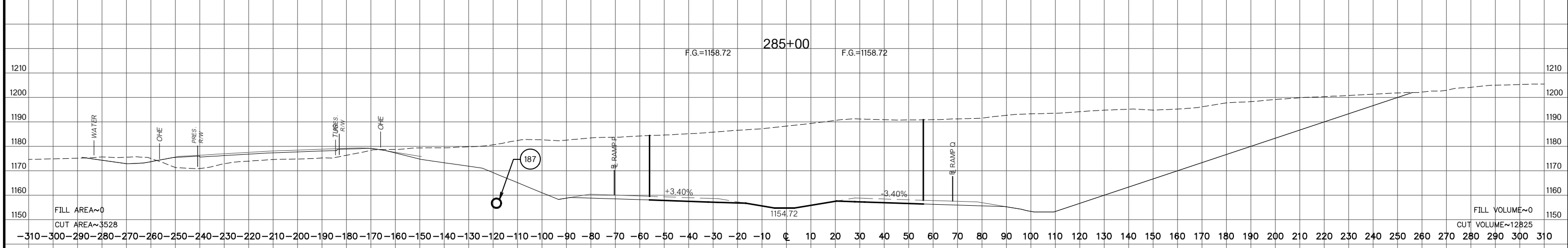
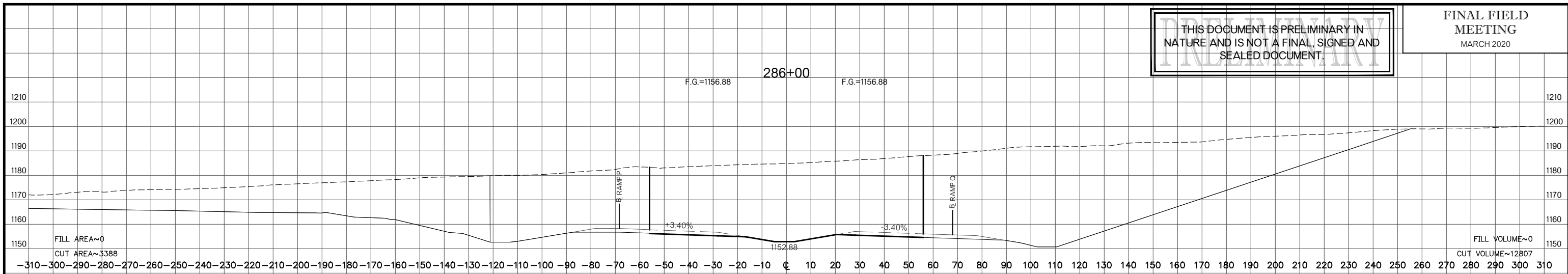
US 81

FILL AREA~0
CUT AREA~3470

FILL VOLUME~0
CUT VOLUME~1985

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



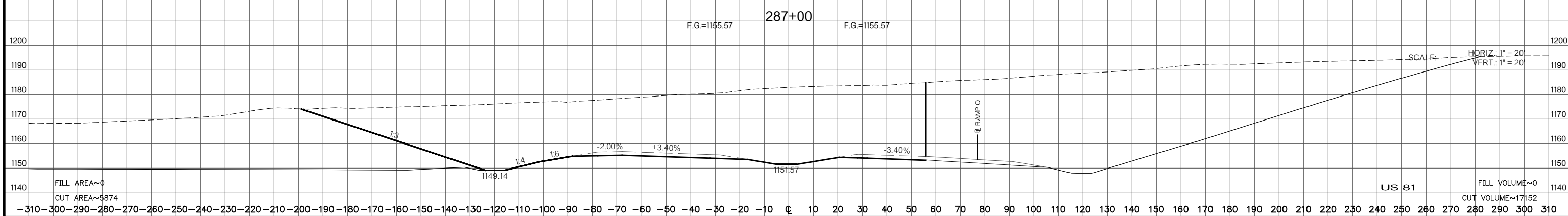
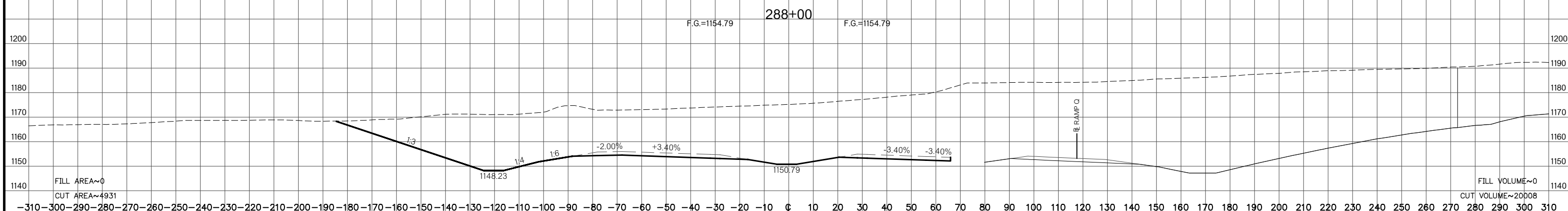
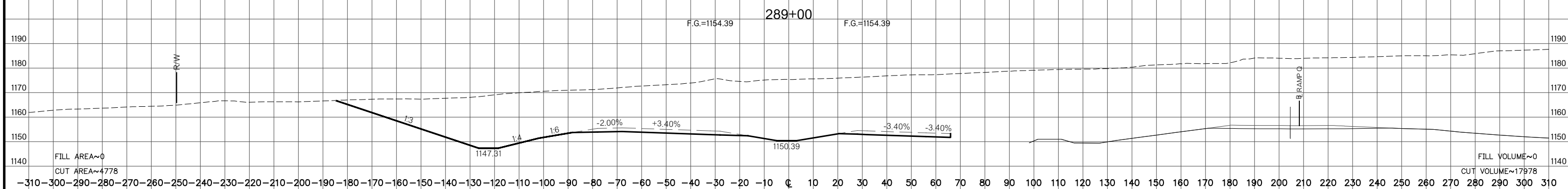
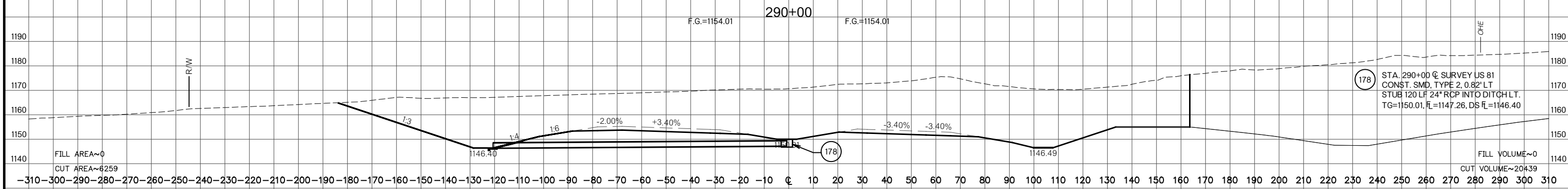
SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

US 81

GRADY COUNTY US 81 REALIGNMENT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

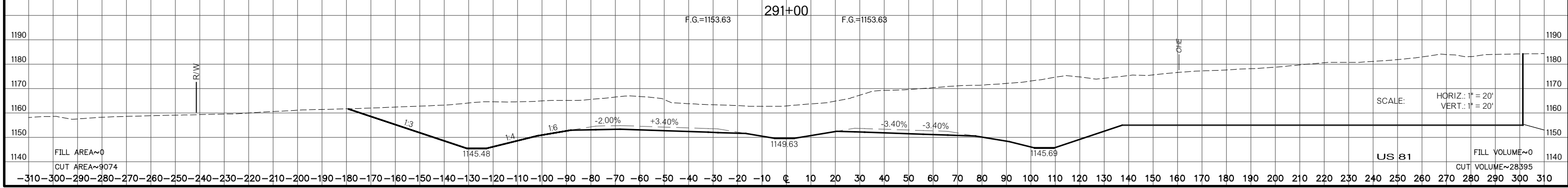
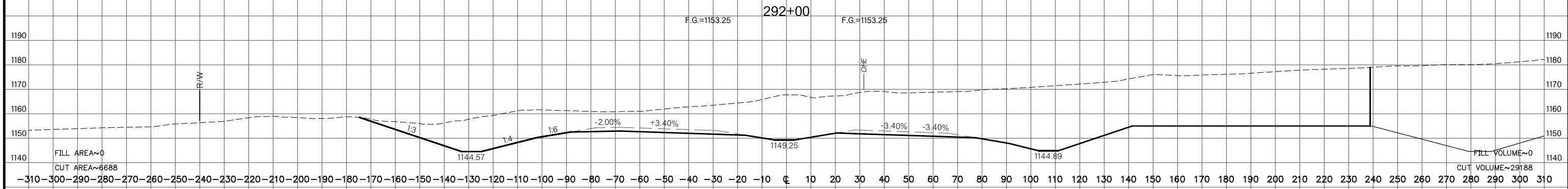
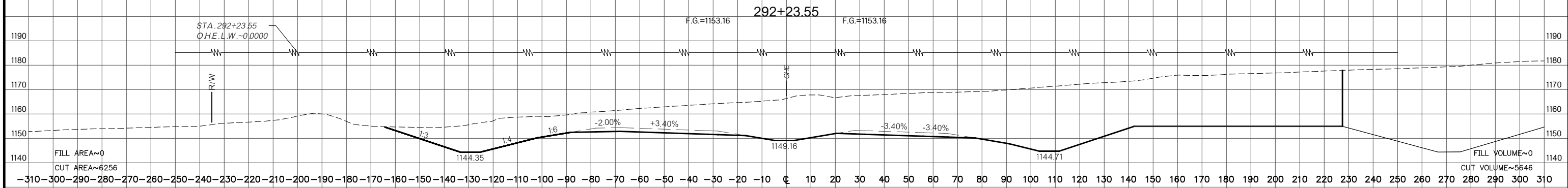
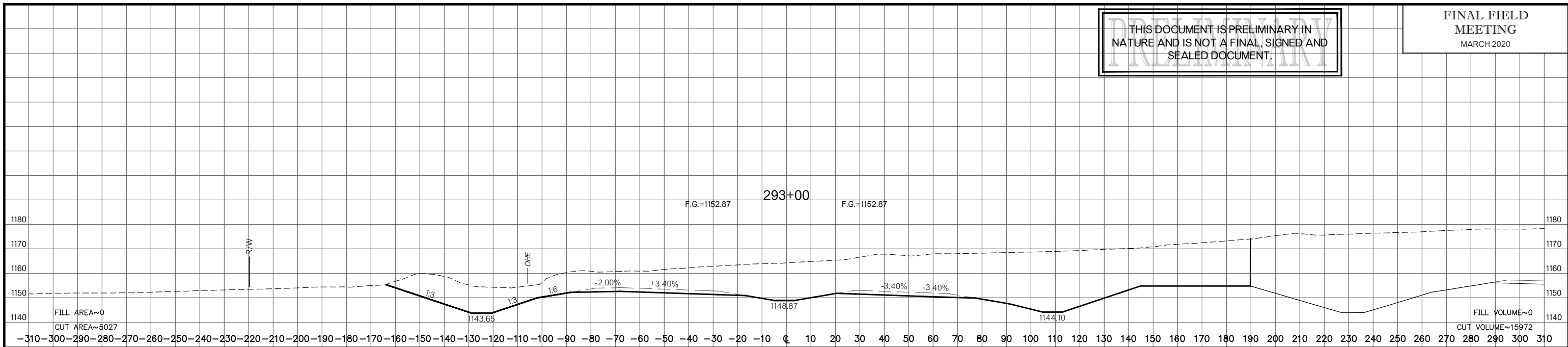
FINAL FIELD MEETING
MARCH 2020



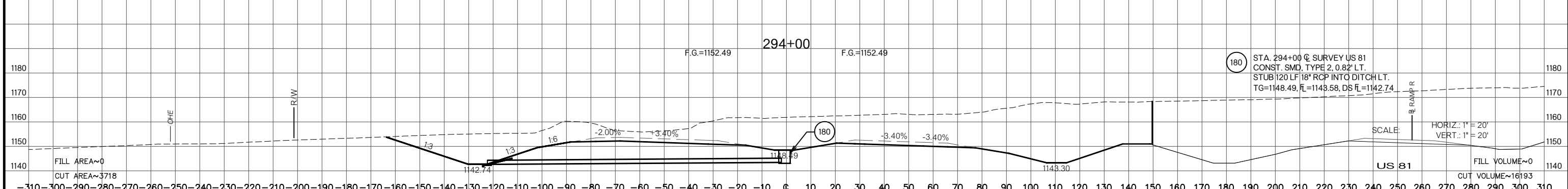
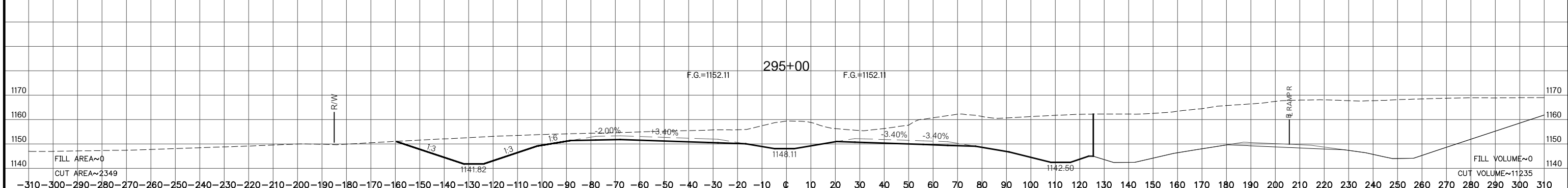
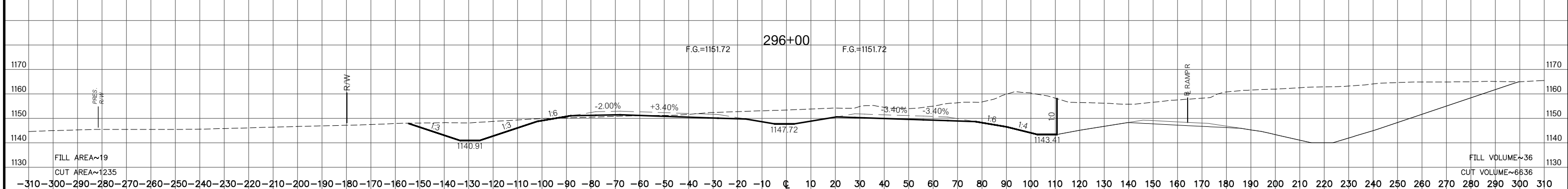
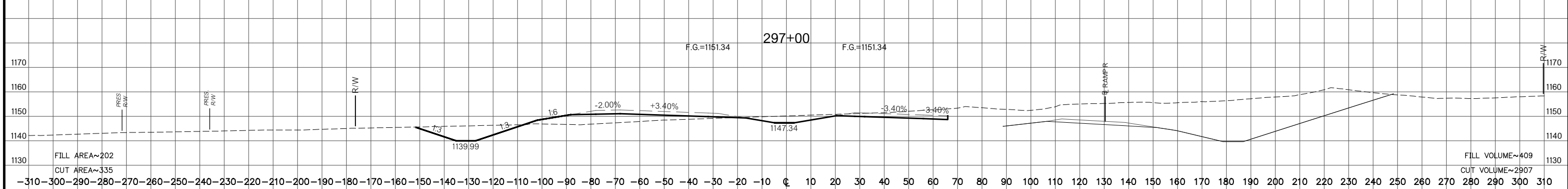
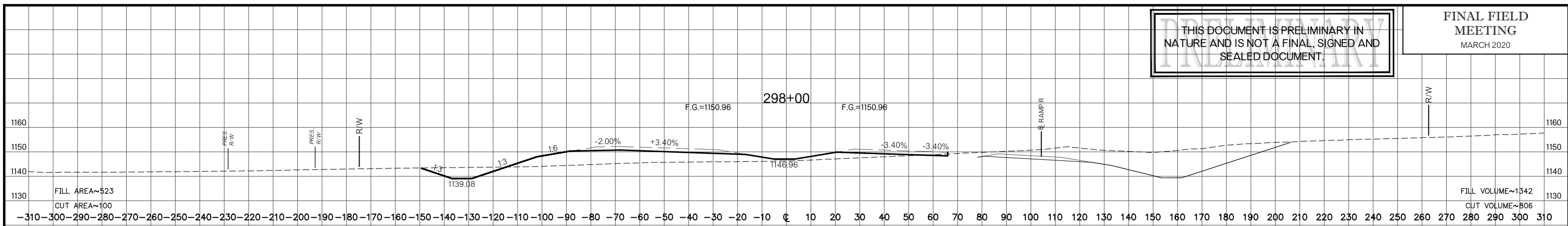
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



(180) STA. 294+00 @ SURVEY US 81
CONST. SMD. TYPE 2, 0.82' LT.
STUB 120 LF 18" RCP INTO DITCH LT.
TG=1148.49, FL=1143.58, DS FL=1142.74

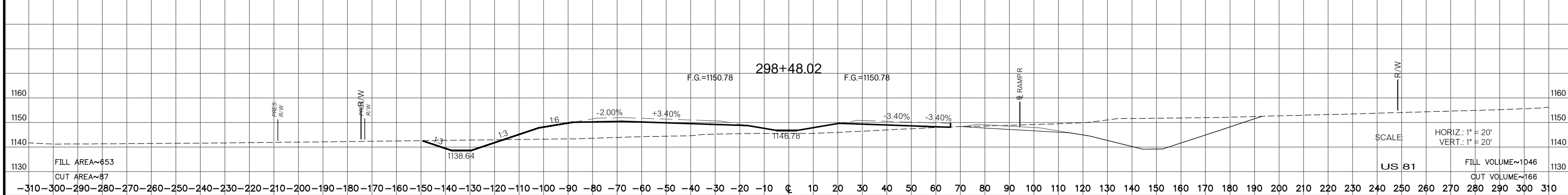
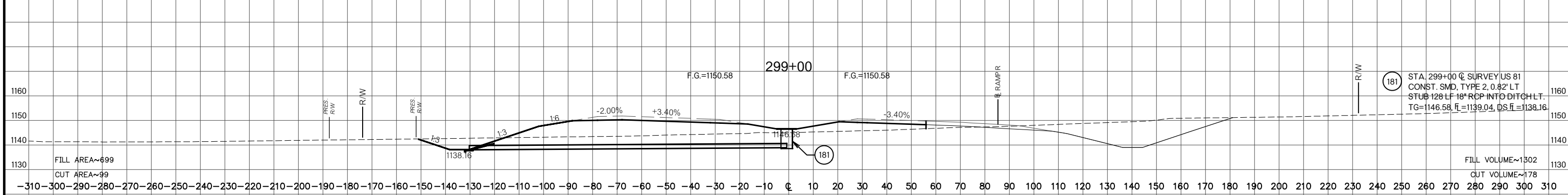
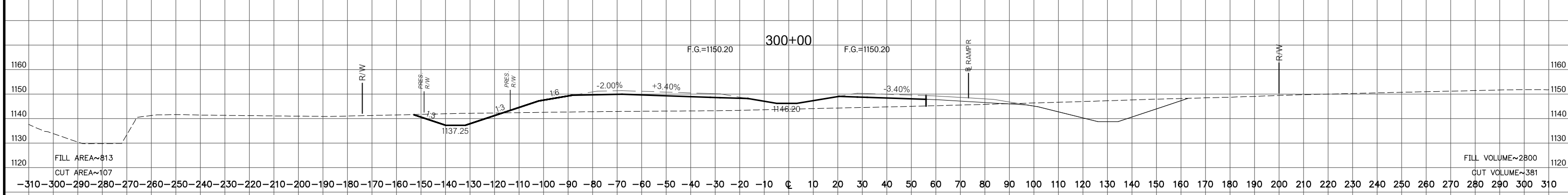
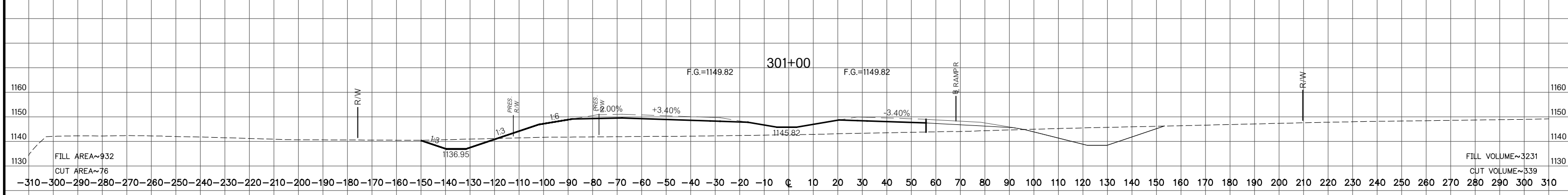
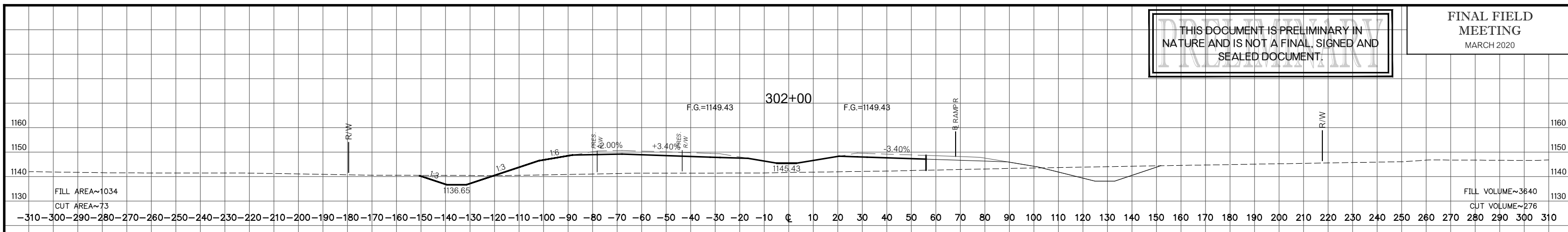
SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



(181) STA. 299+00 @ SURVEY US 81 CONST. SMD, TYPE 2, 0.82' LT STUB 128 LF 18" RCP INTO DITCH LT. TG=1146.58, fi=1139.04, DS fi=1138.16

SCALE HORIZ.: 1" = 20' VERT.: 1" = 20'

US 81 FILL VOLUME~1046 CUT VOLUME~166

US 81 REALIGNMENT GRADY COUNTY

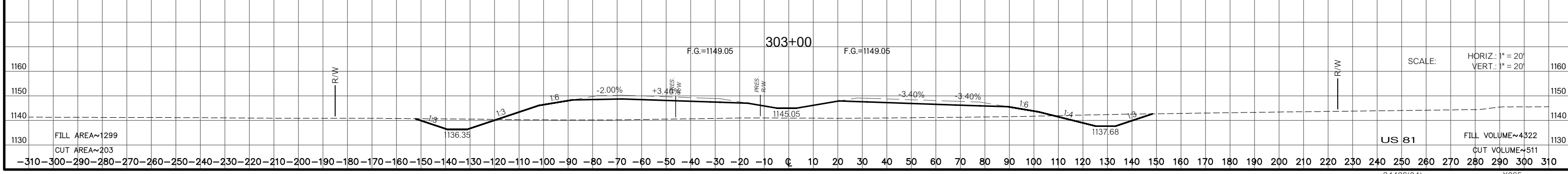
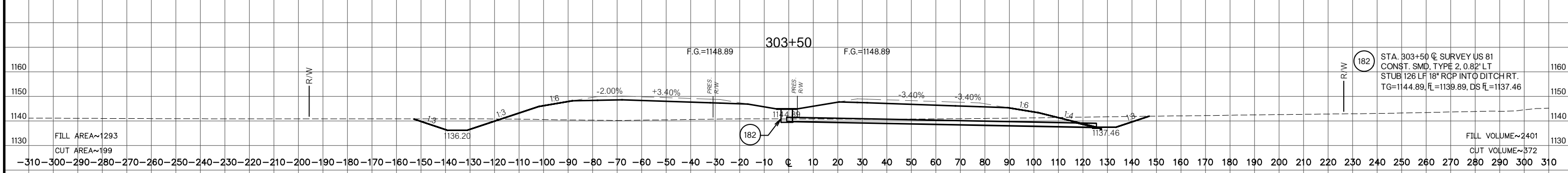
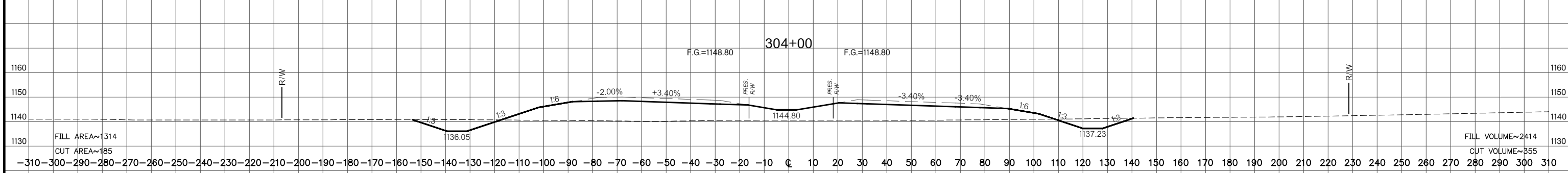
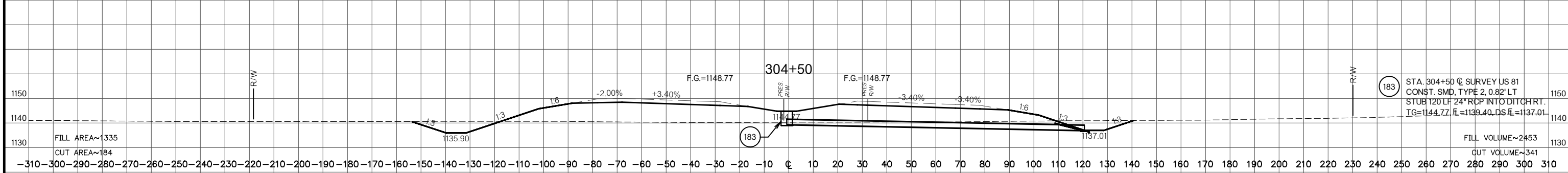
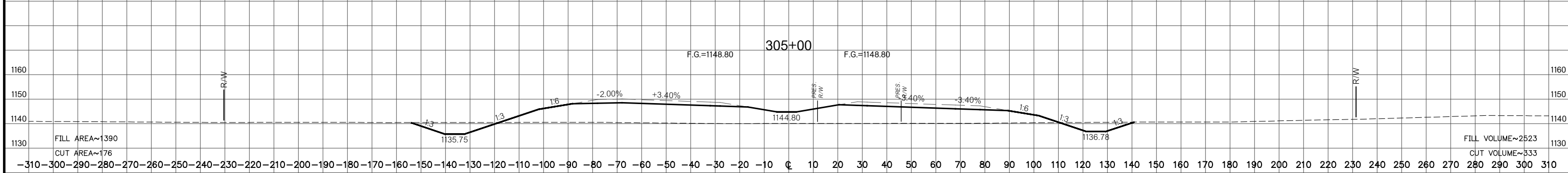
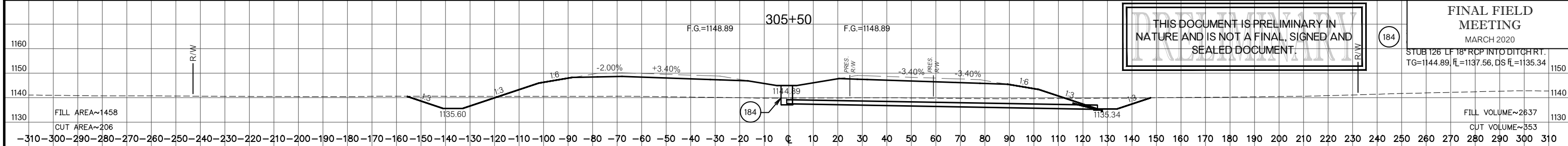
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING

MARCH 2020

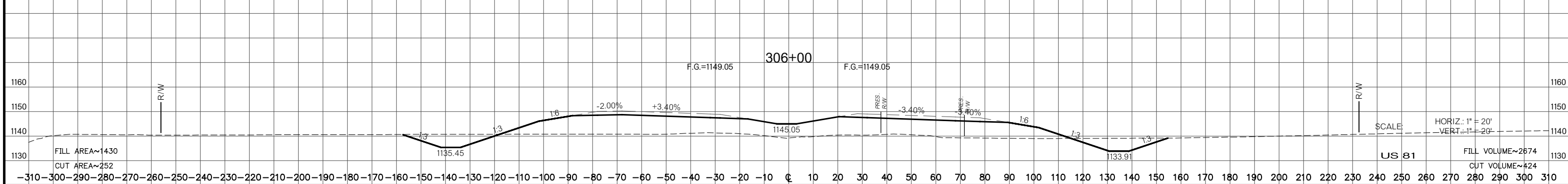
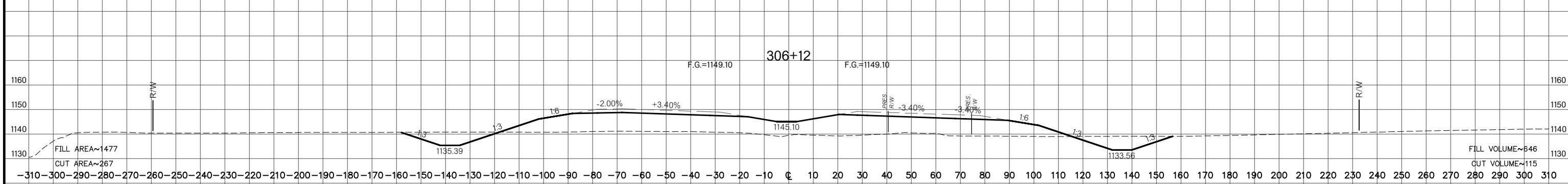
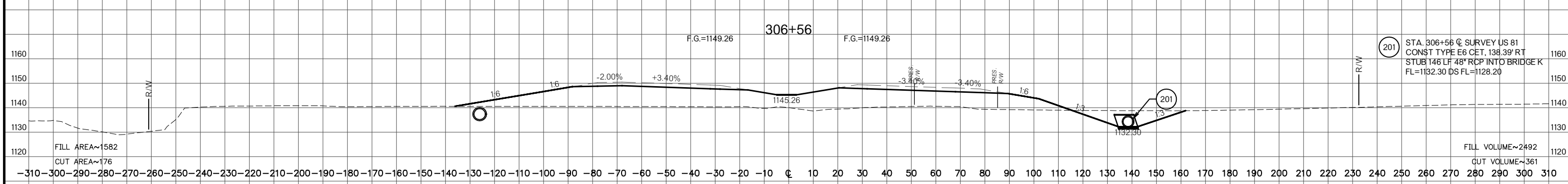
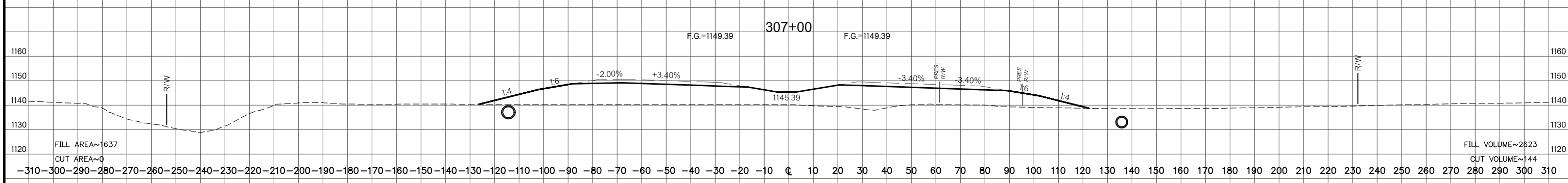
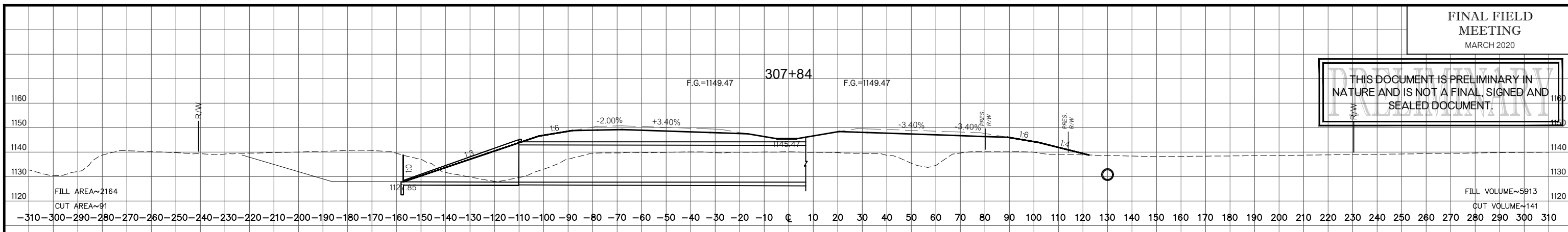
STUB 126 LF 18" RCP INTO DITCH RT.
TG=1144.89, fL=1137.56, DS fL=1135.34

184



US 81 REALIGNMENT
GRADY COUNTY

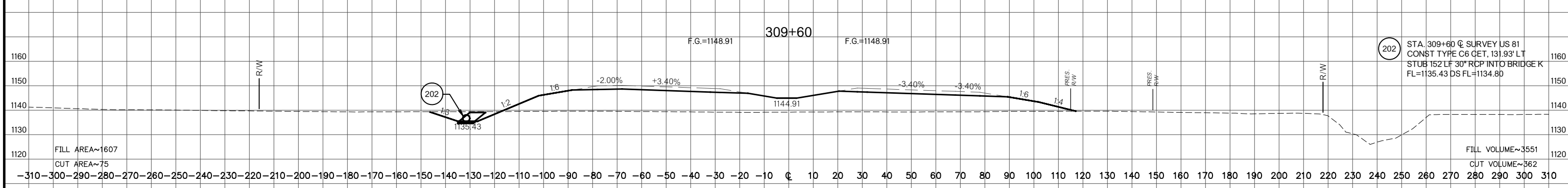
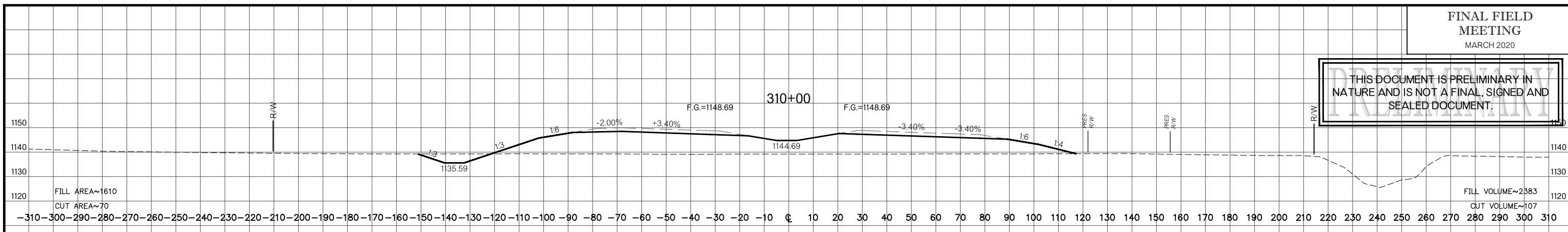
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



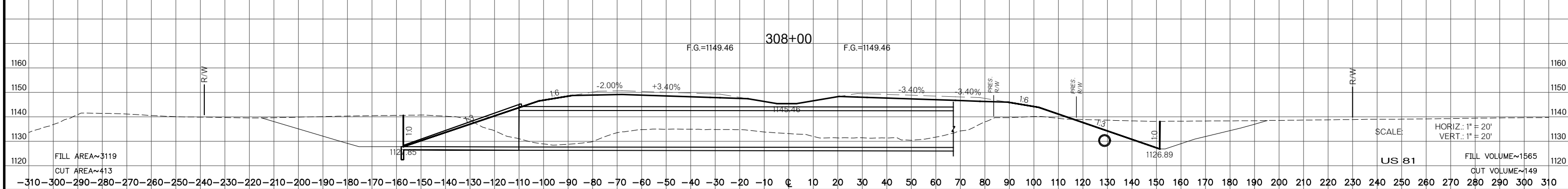
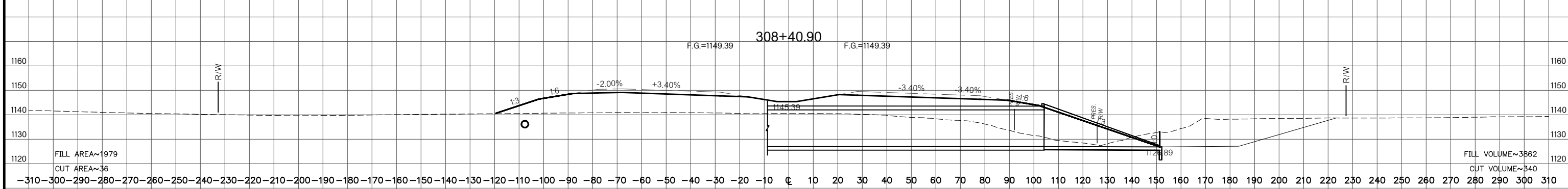
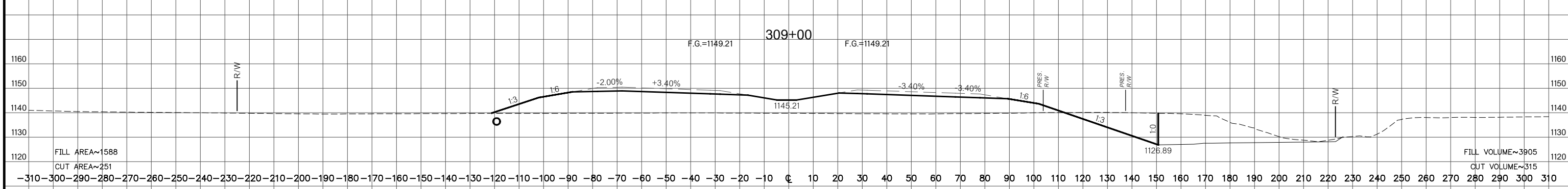
SCALE HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



202 STA. 309+60 C SURVEY US 81
CONST TYPE C6 GET, 131.93' LT
STUB 152 LF 30" RCP INTO BRIDGE K
FL=1135.43 DS FL=1134.80

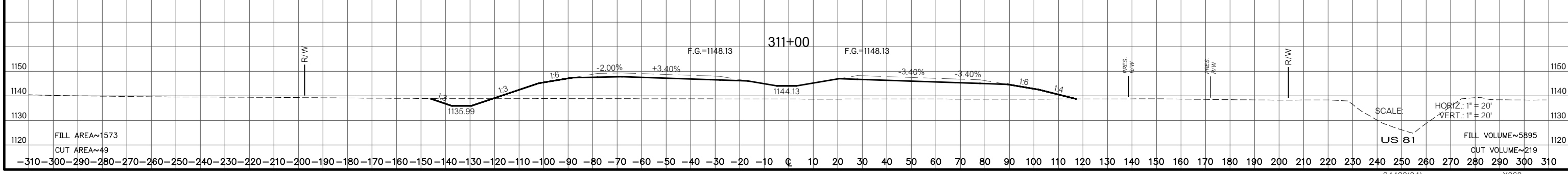
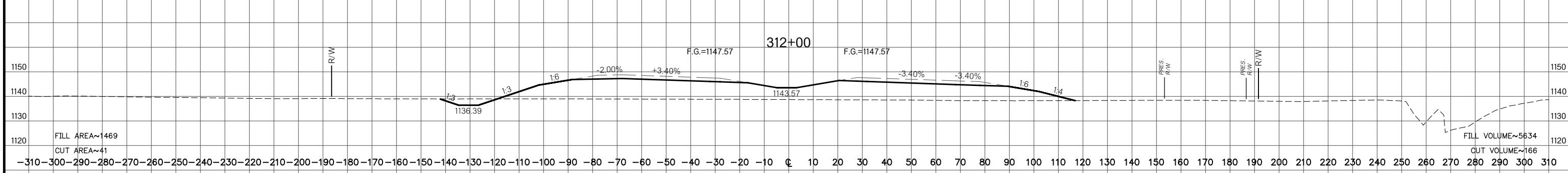
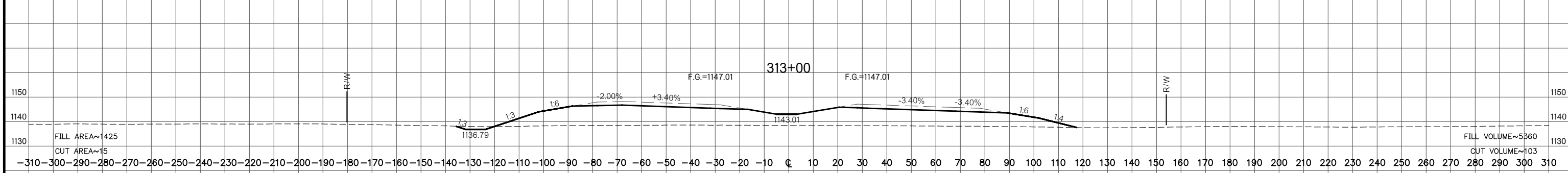
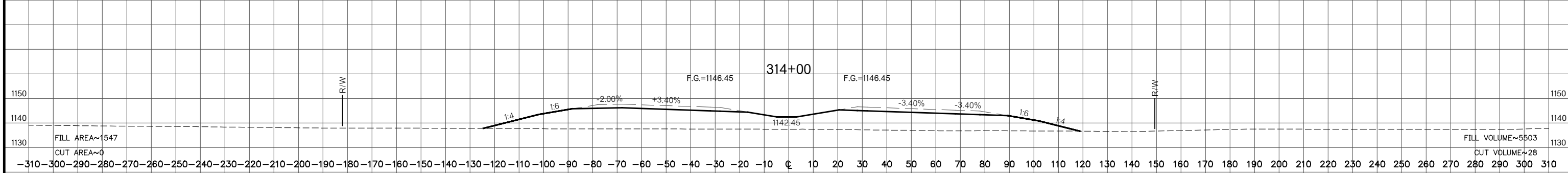
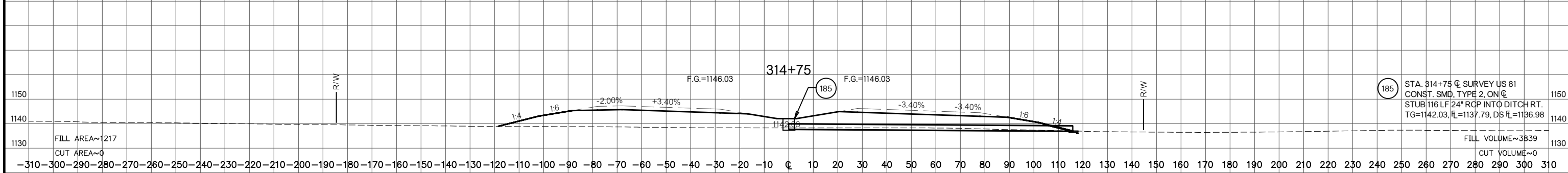
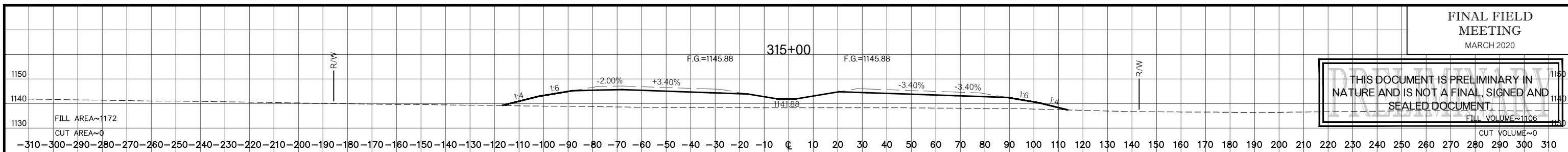


SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81
FILL VOLUME~1565
CUT VOLUME~149

US 81 REALIGNMENT
GRADY COUNTY

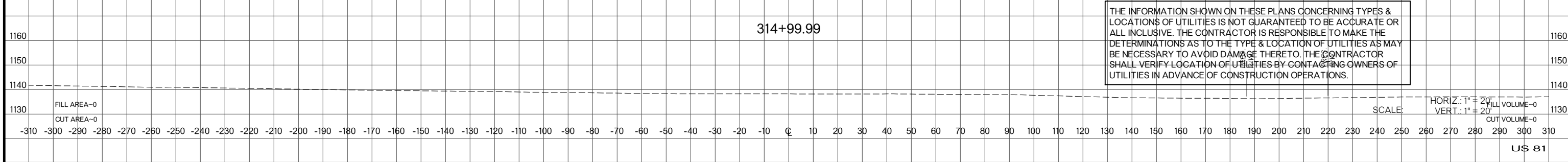
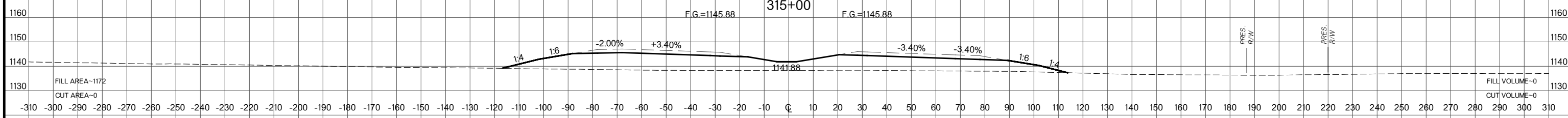
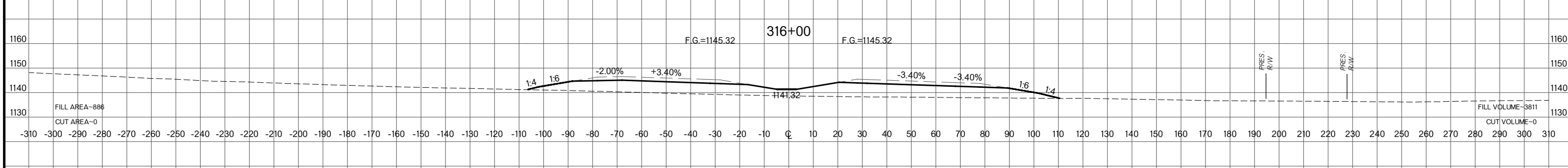
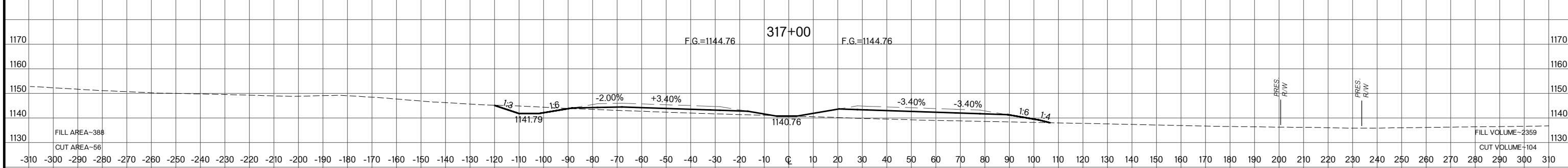
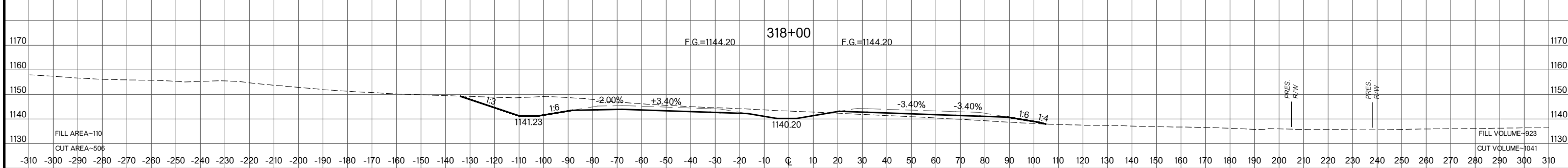
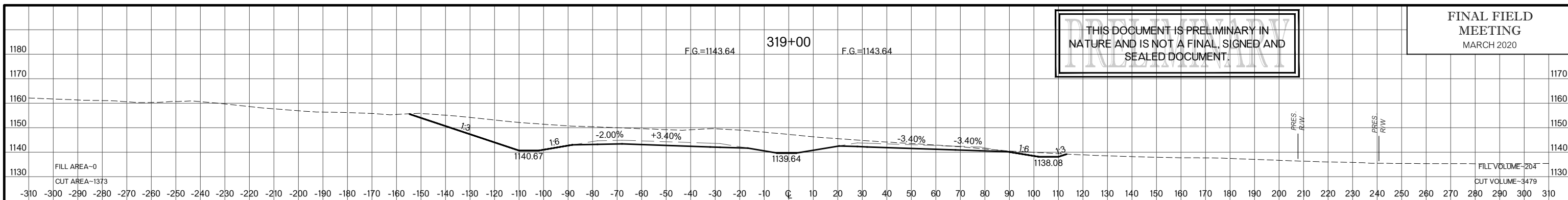
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE
US 81
HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



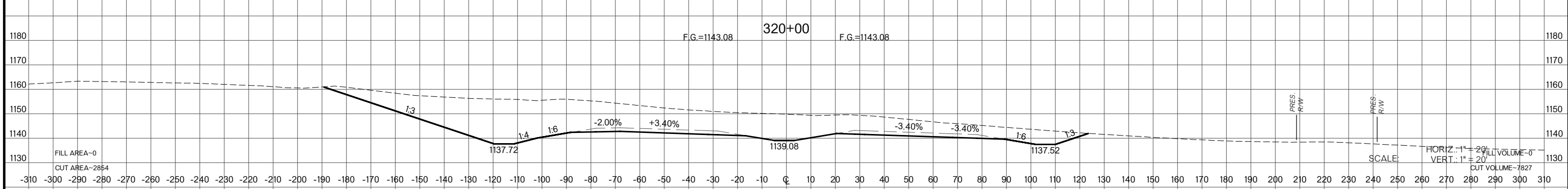
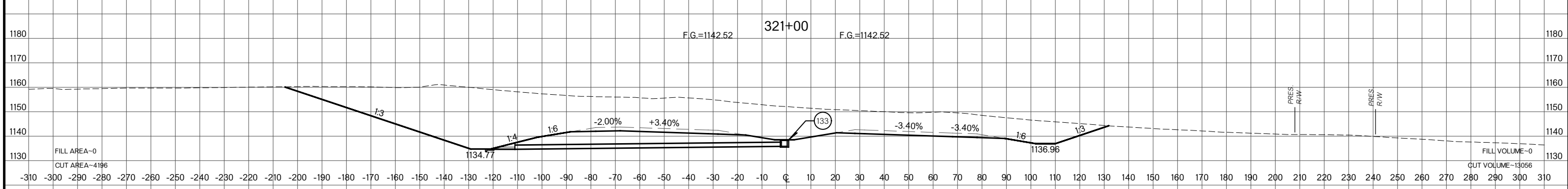
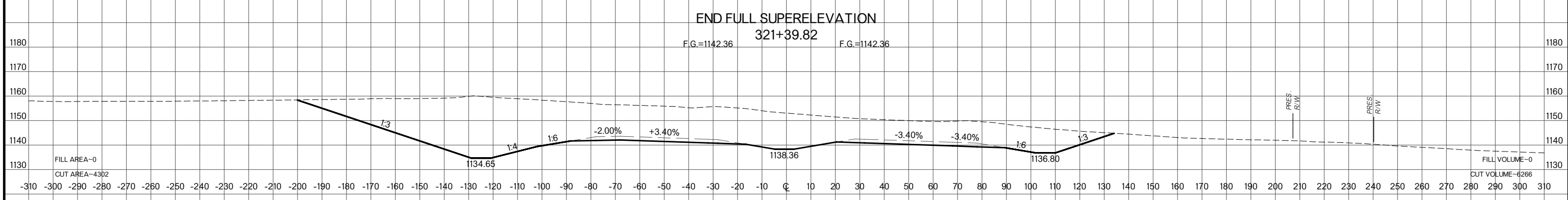
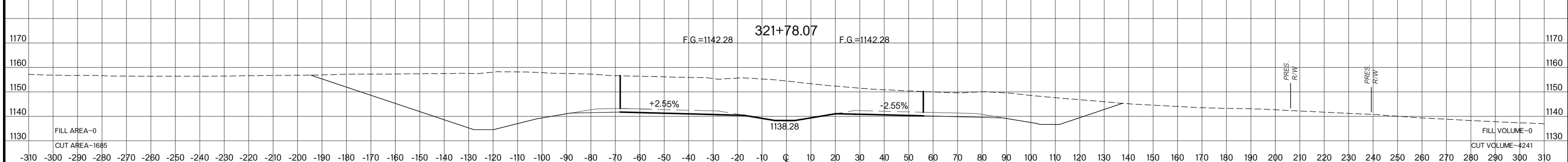
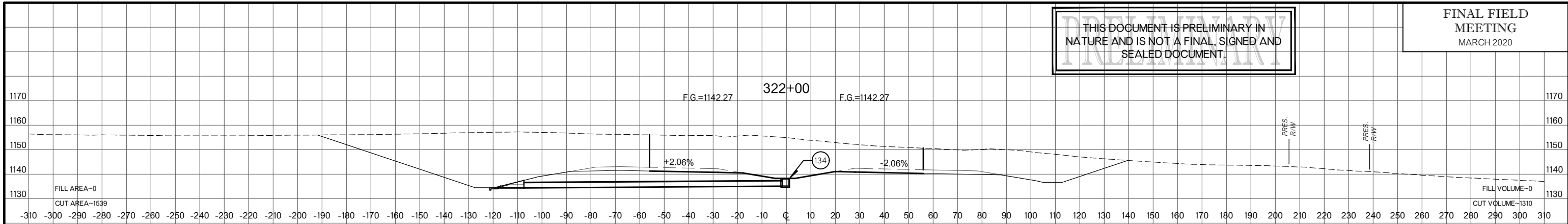
THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

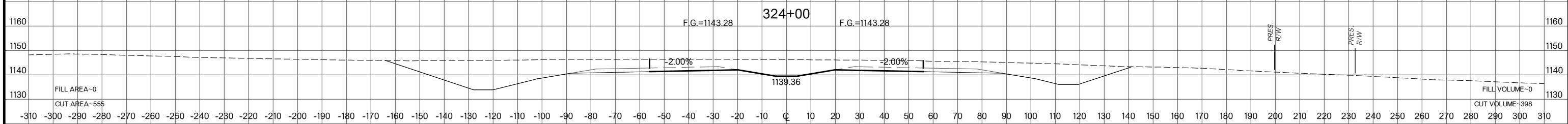
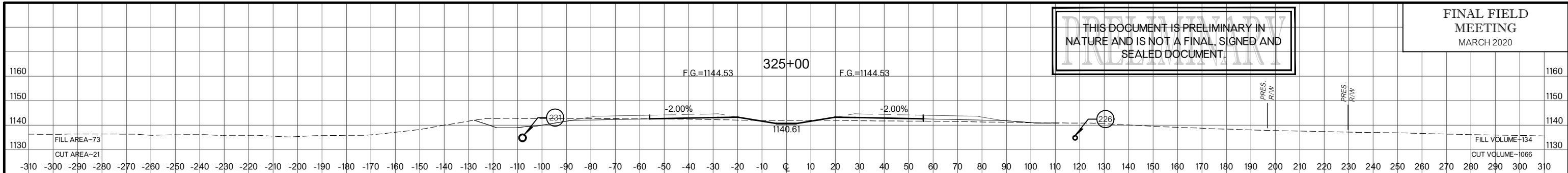
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

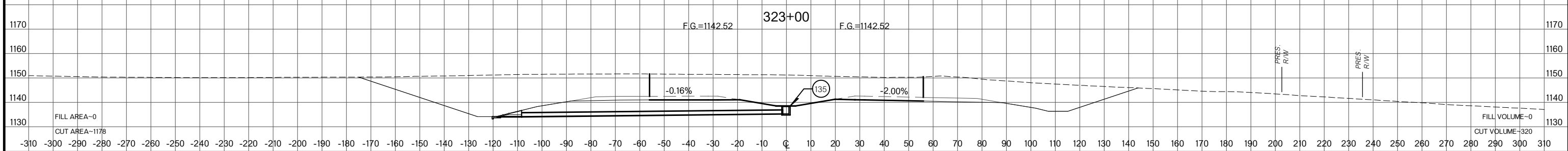
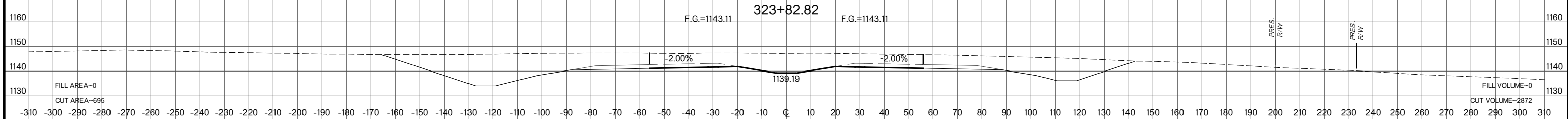


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

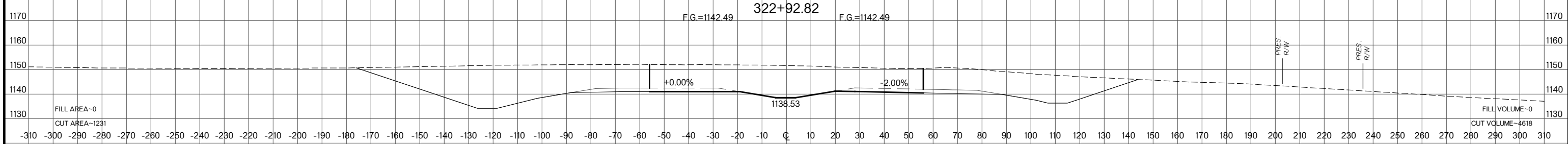
FINAL FIELD MEETING MARCH 2020



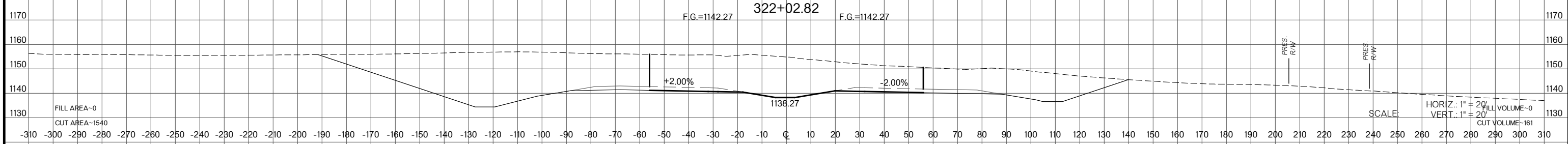
END SUPERELEVATION



RUNOFF ENDS STATION

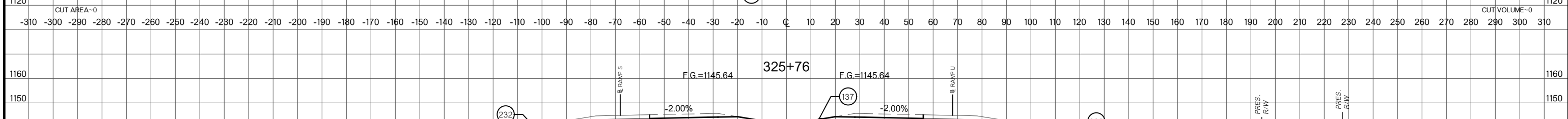
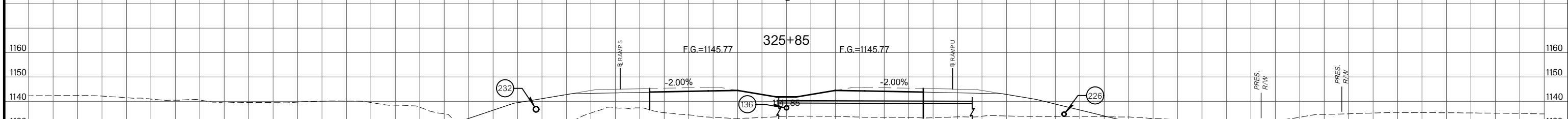
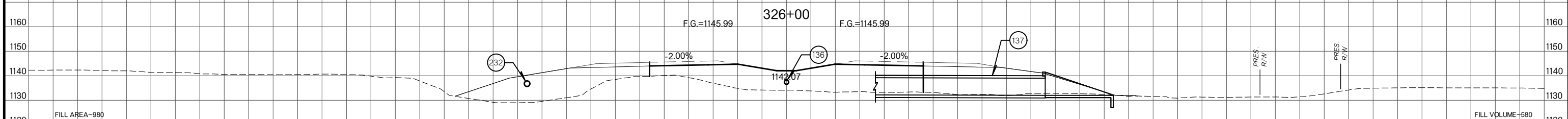
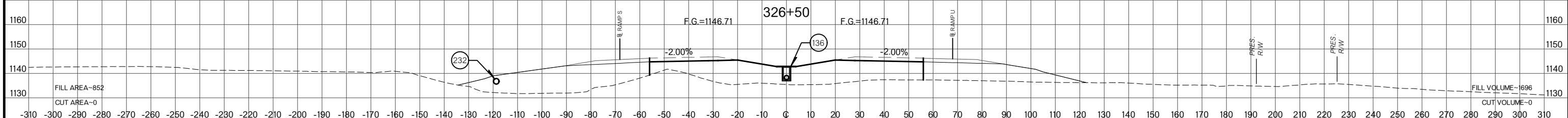
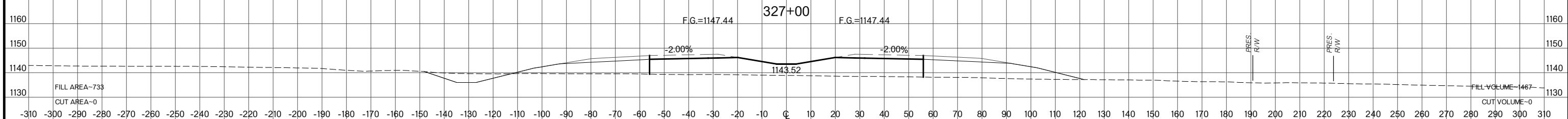
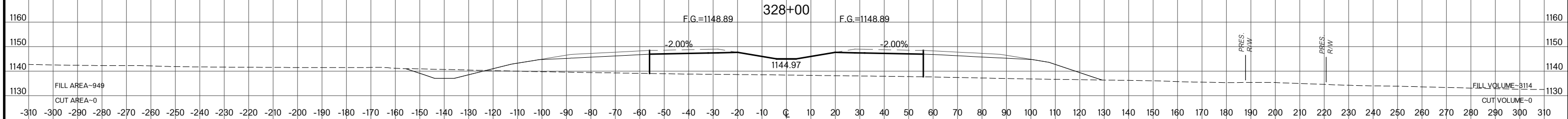


REVERSE CROSS SLOPE STATION



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

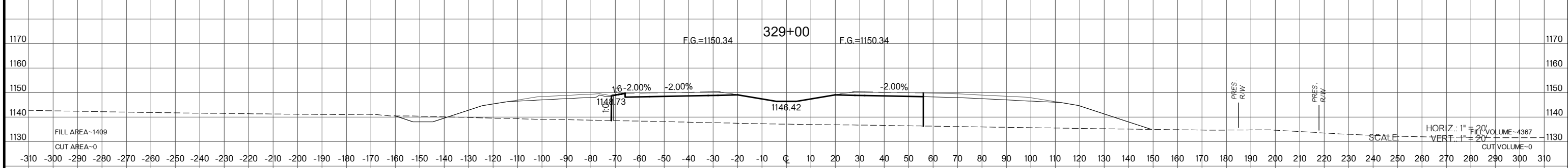
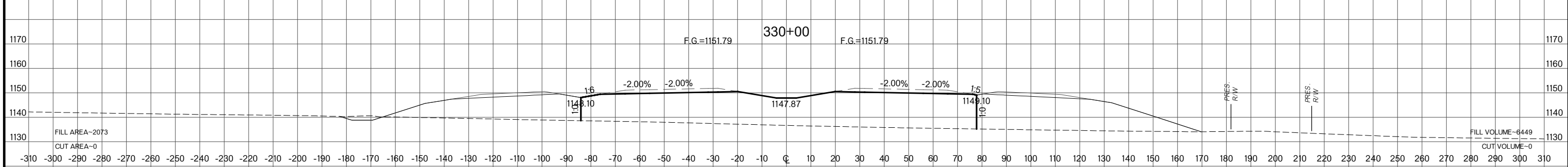
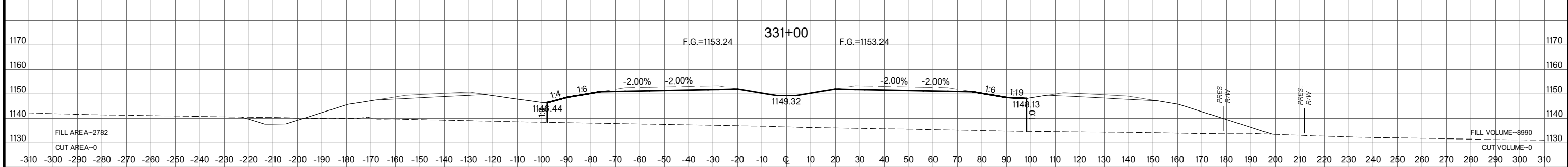
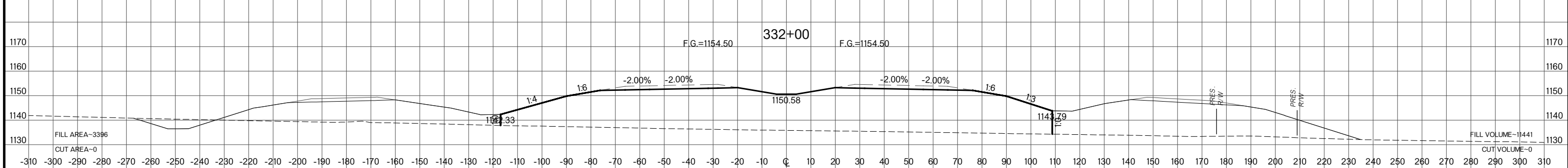
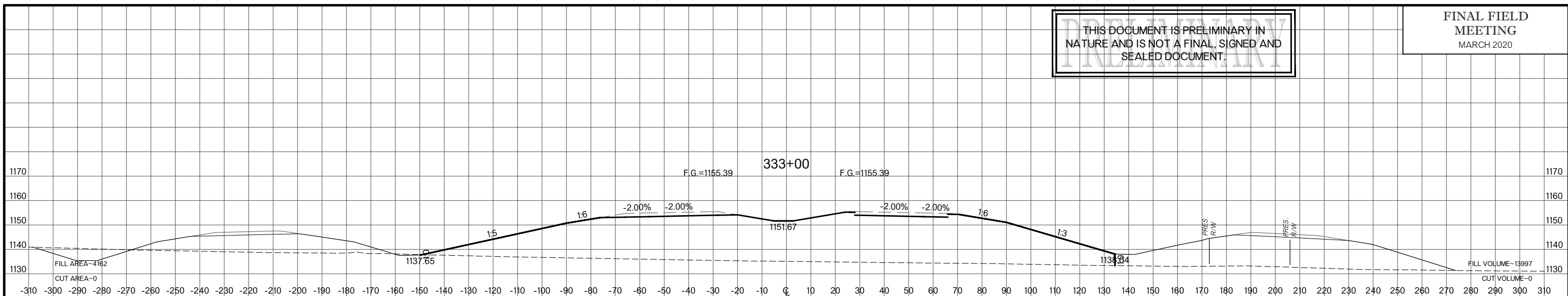
FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

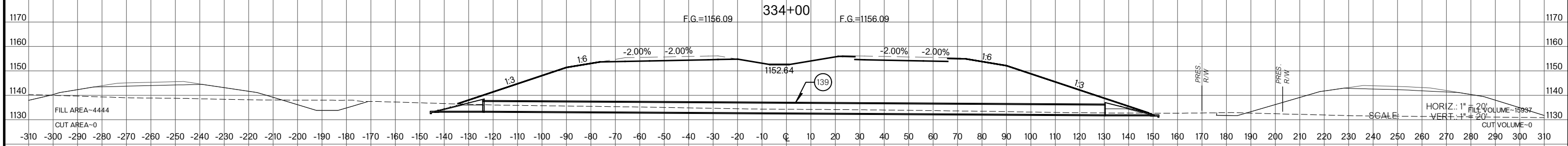
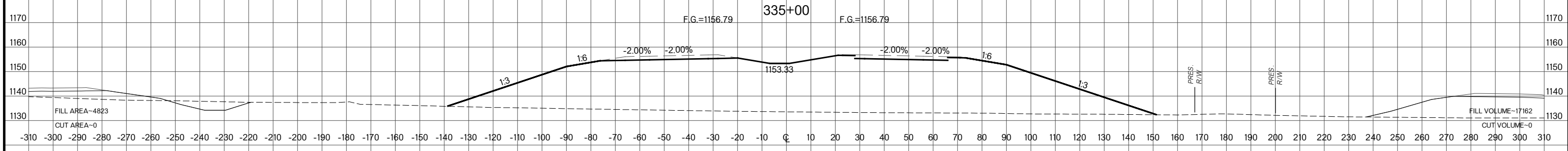
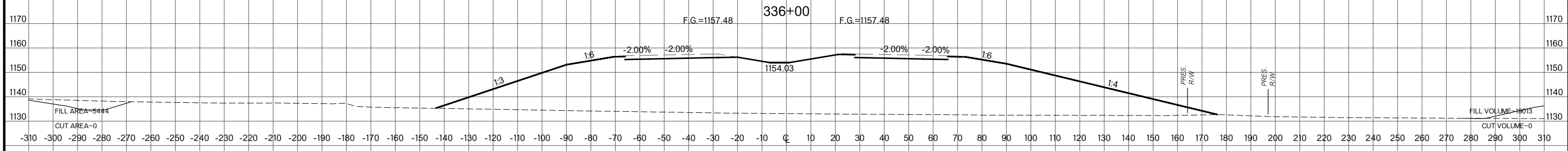
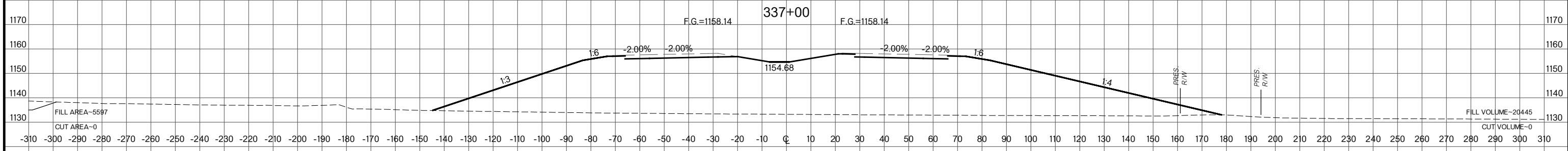
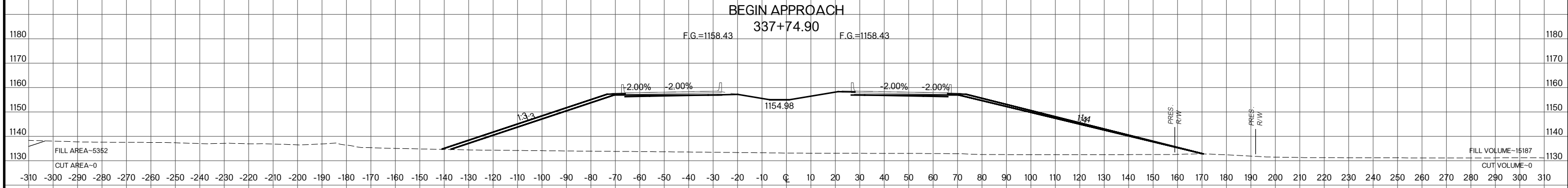


HORIZ.: 1" = 20'
VERT.: 1" = 20'

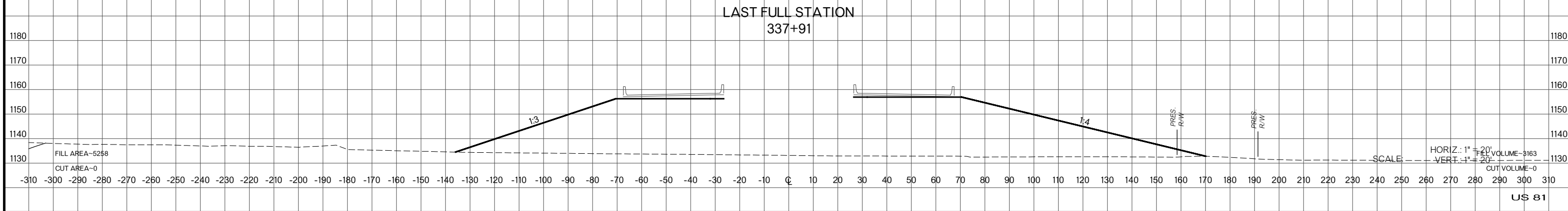
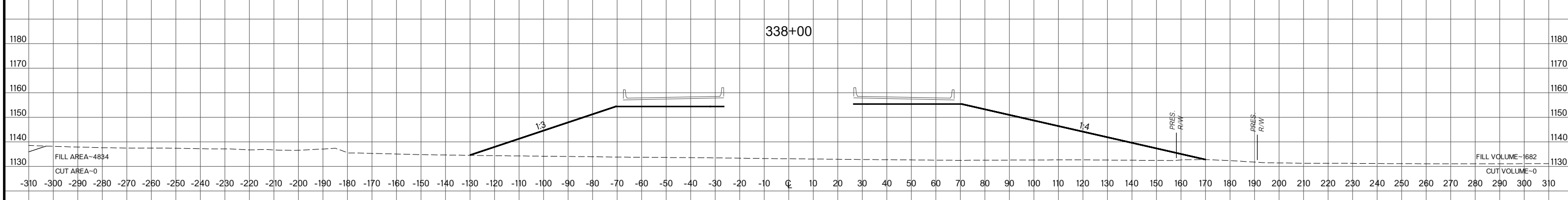
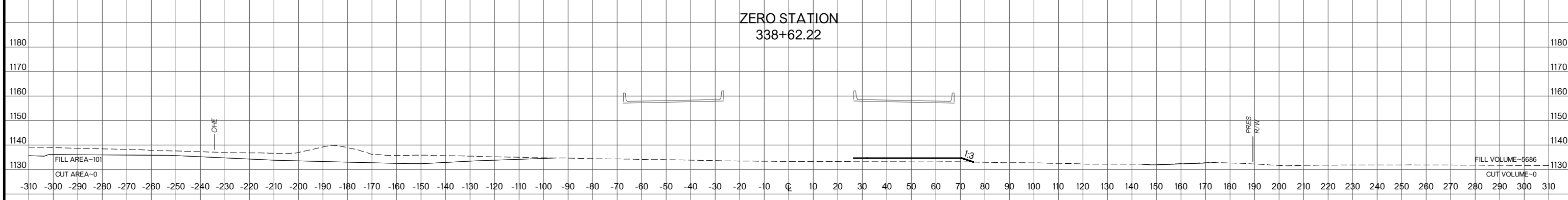
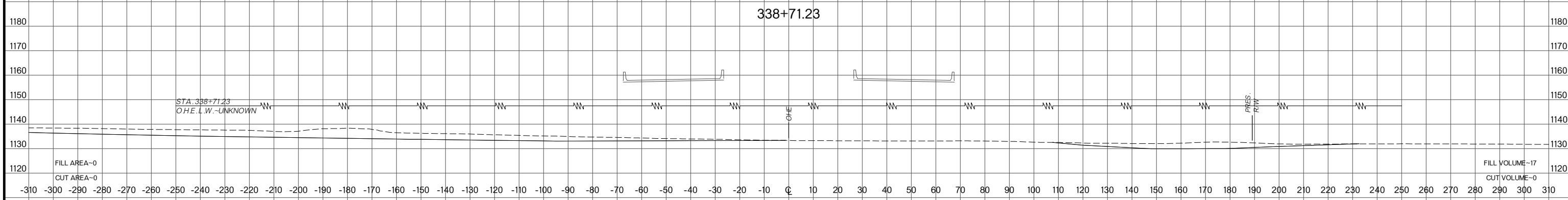
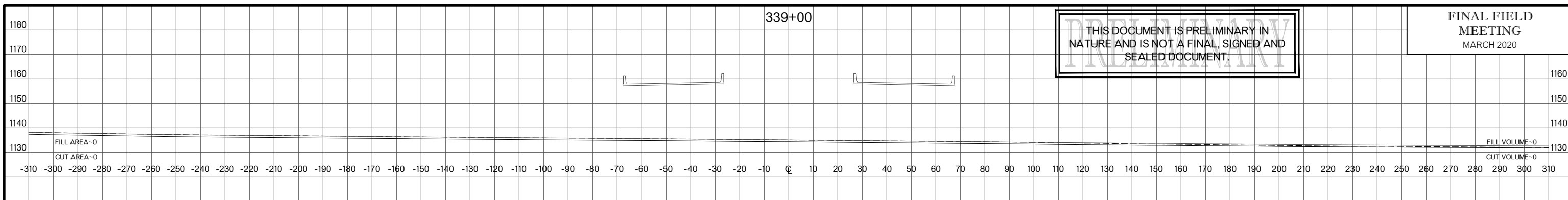
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

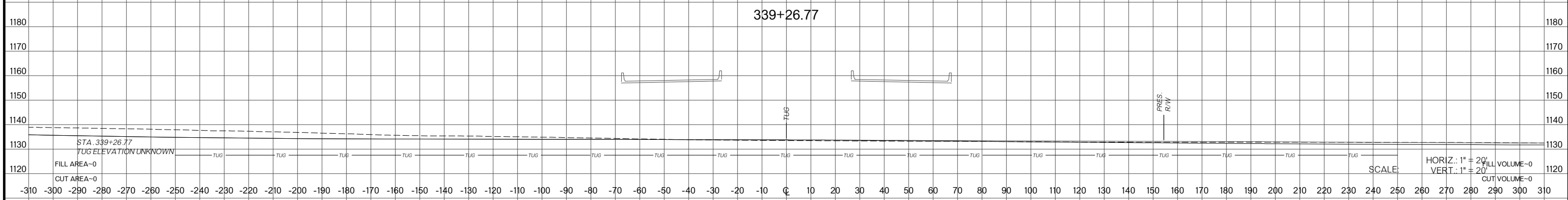
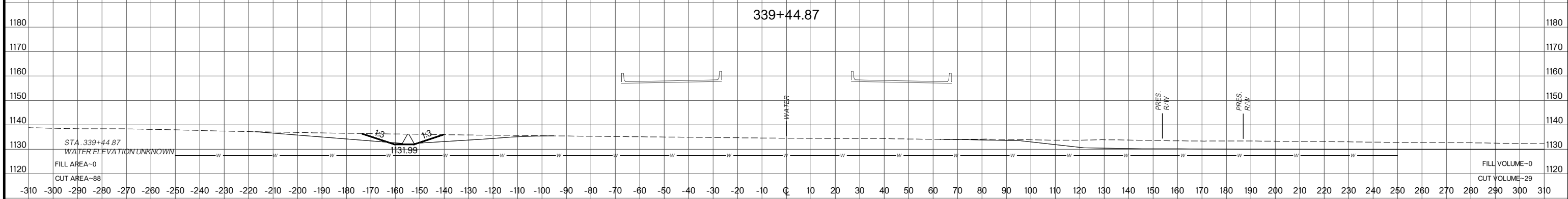
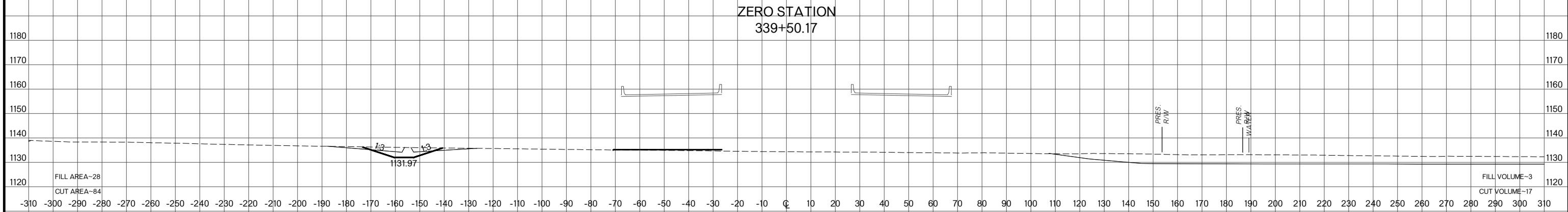
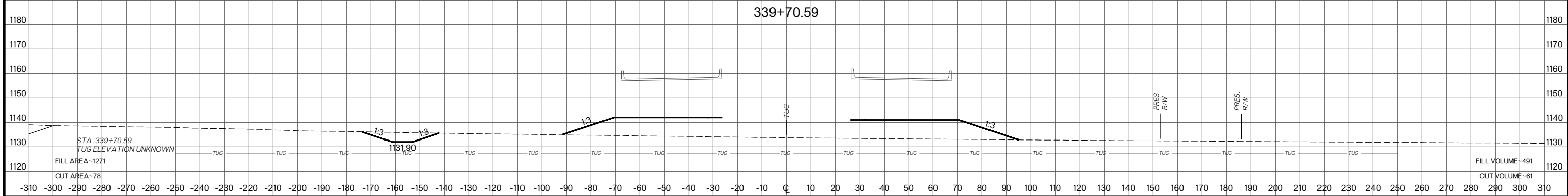


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

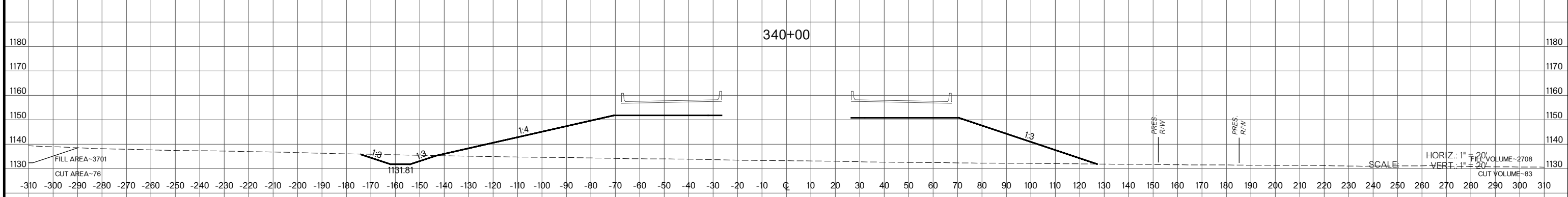
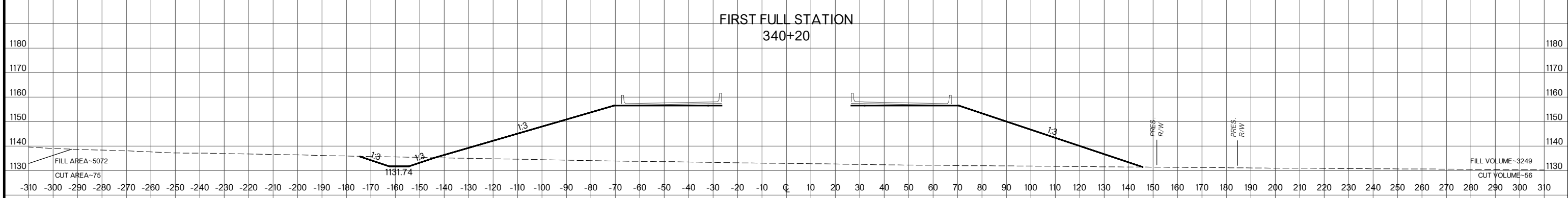
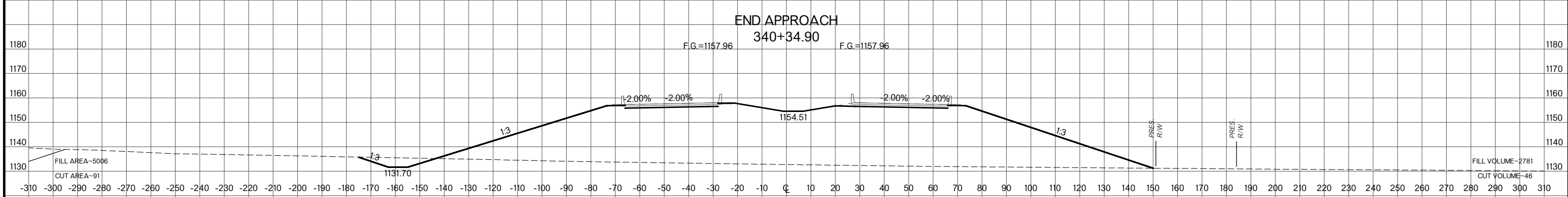
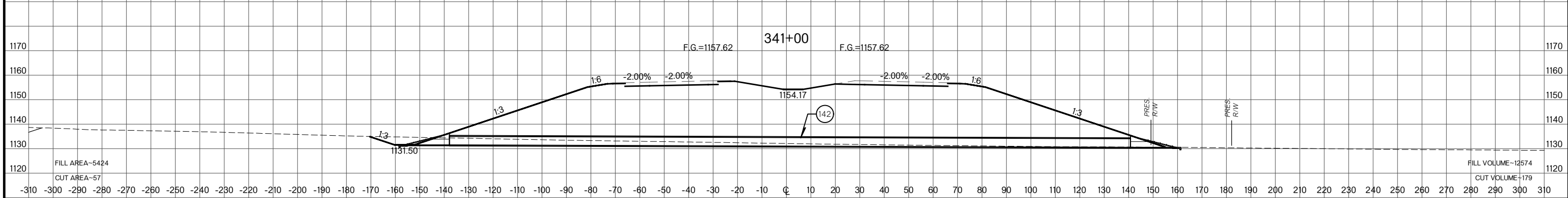
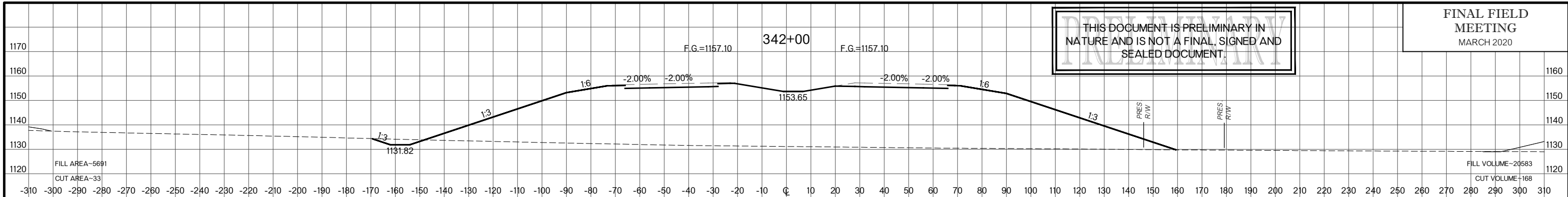
FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

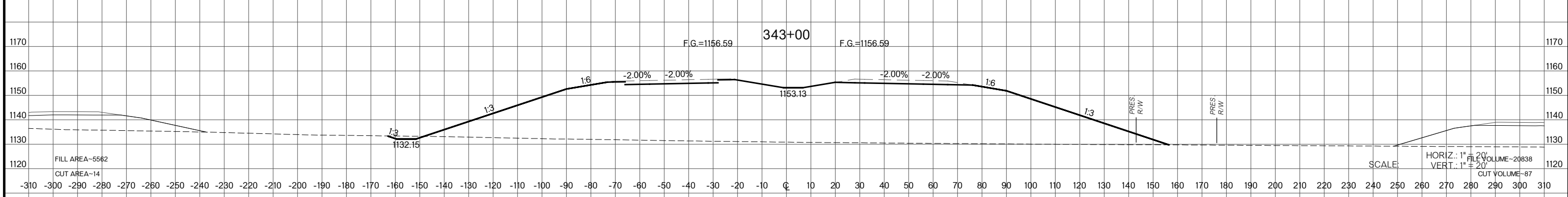
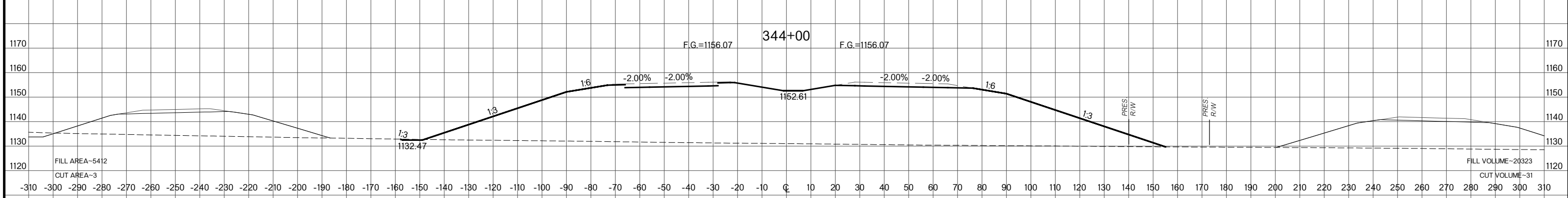
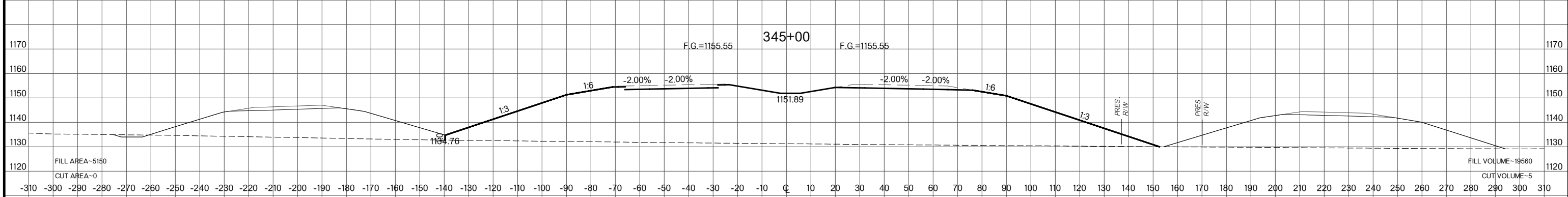
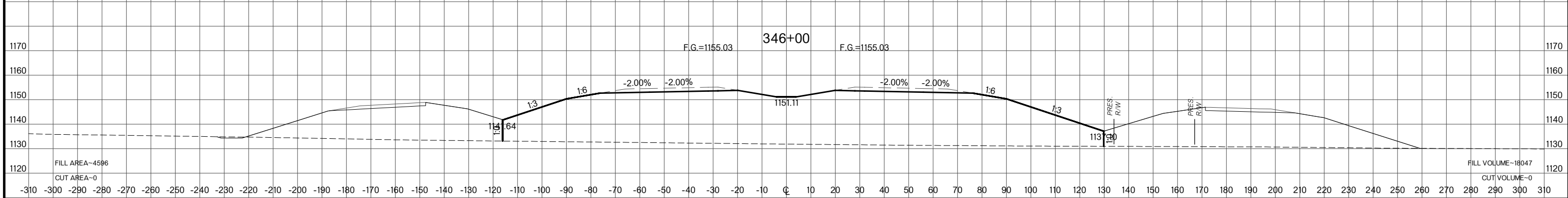
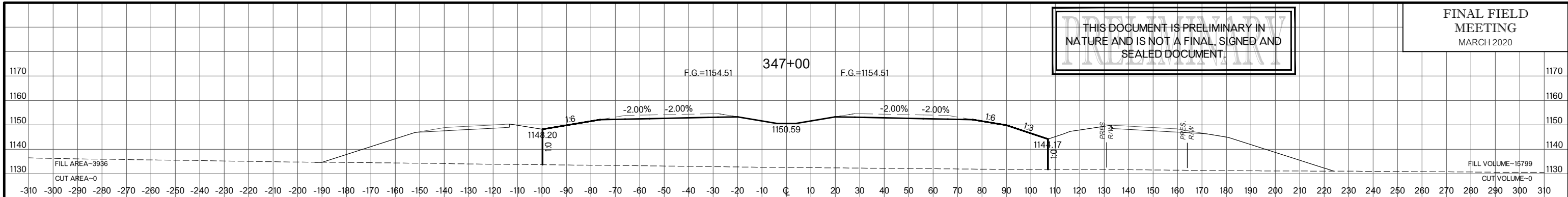
FINAL FIELD MEETING MARCH 2020



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

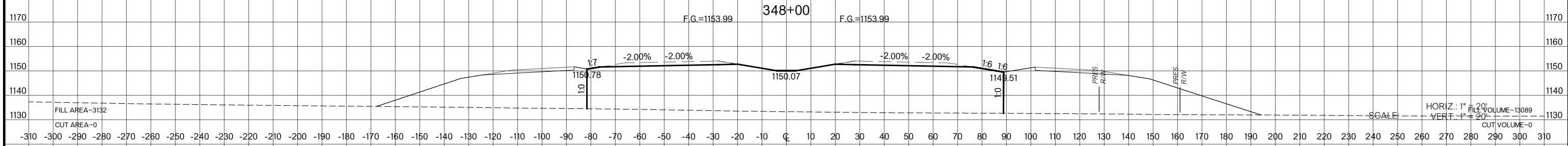
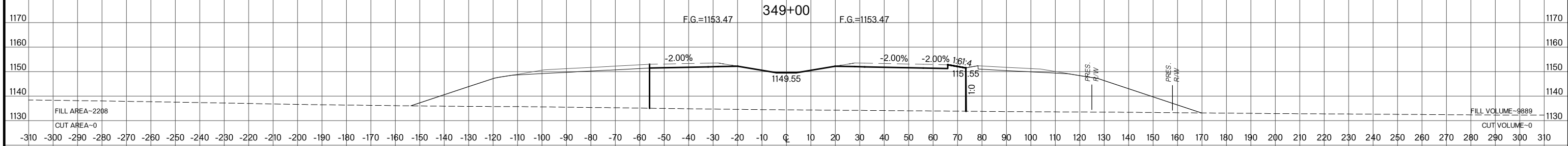
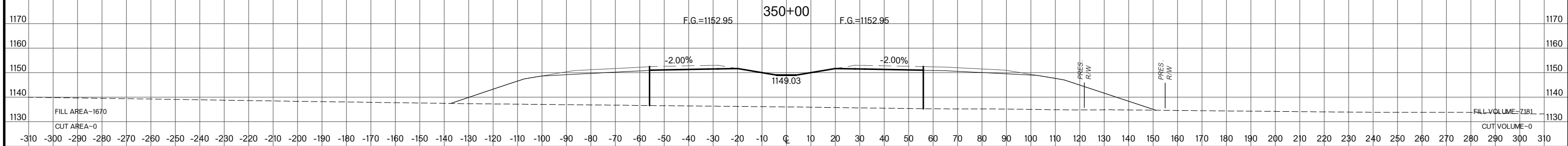
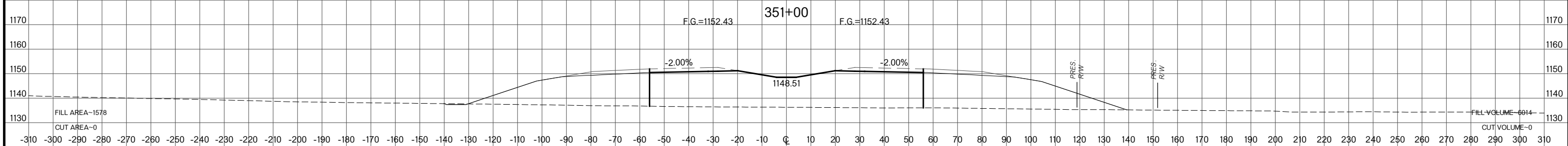
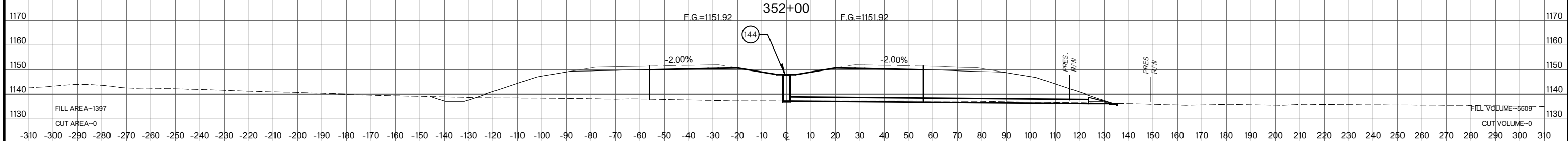
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

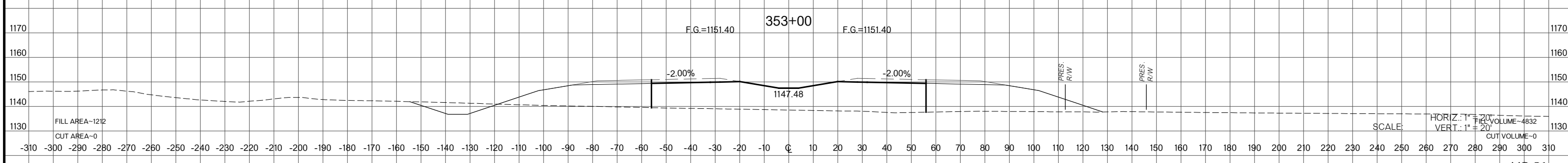
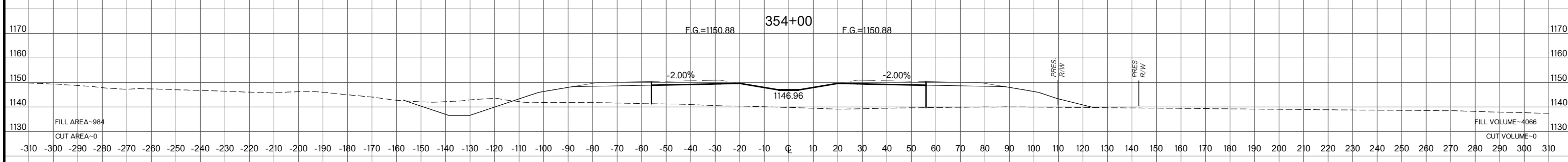
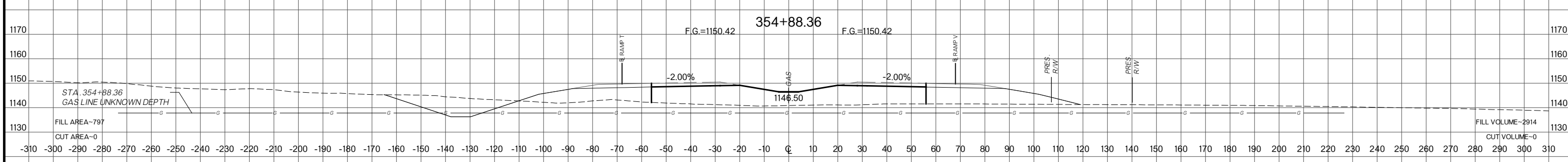
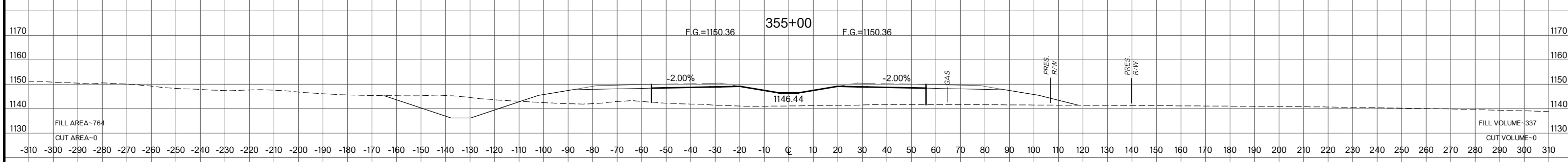
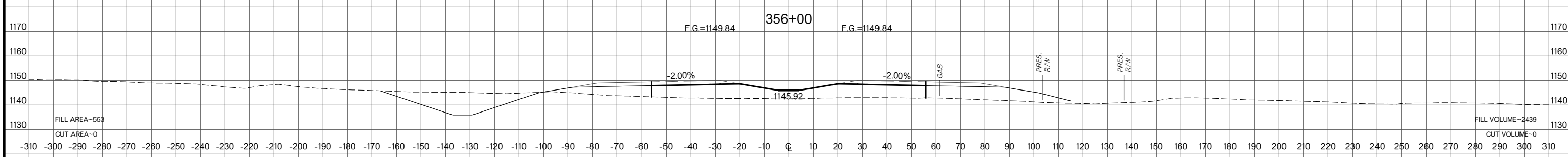
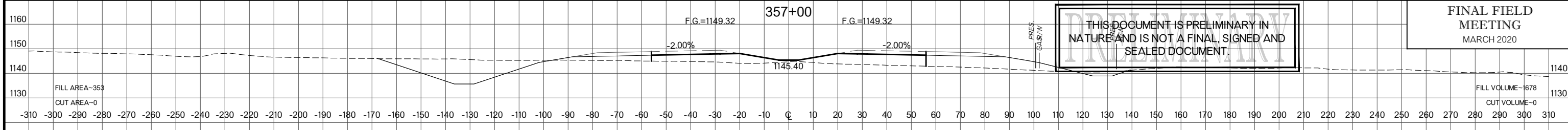
US 81

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

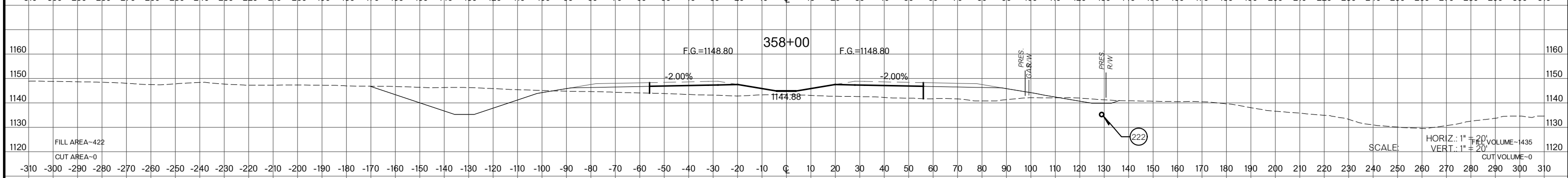
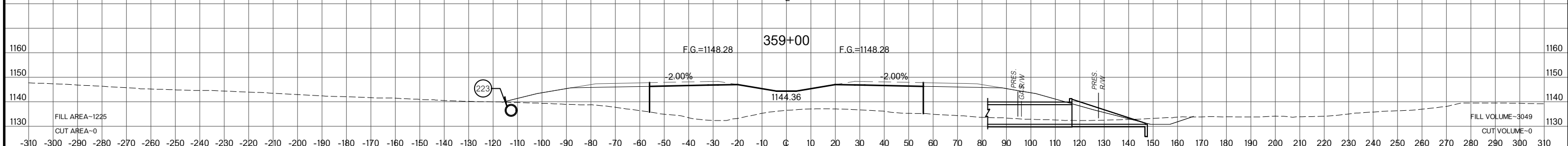
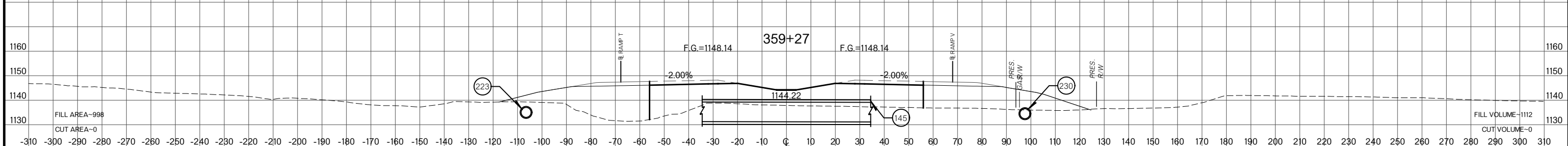
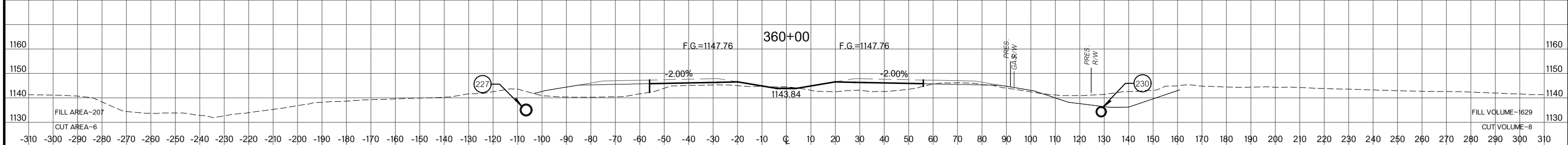
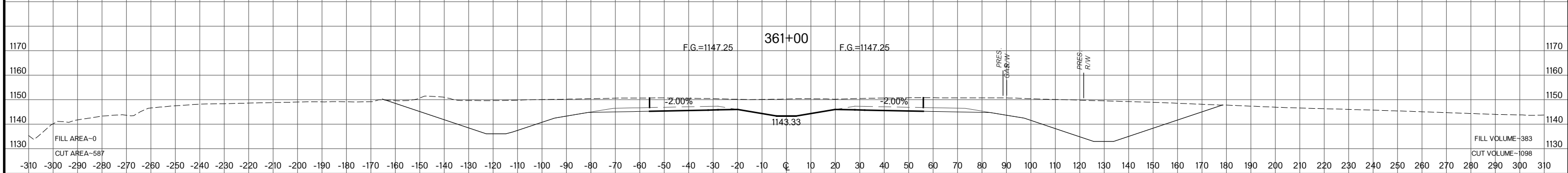
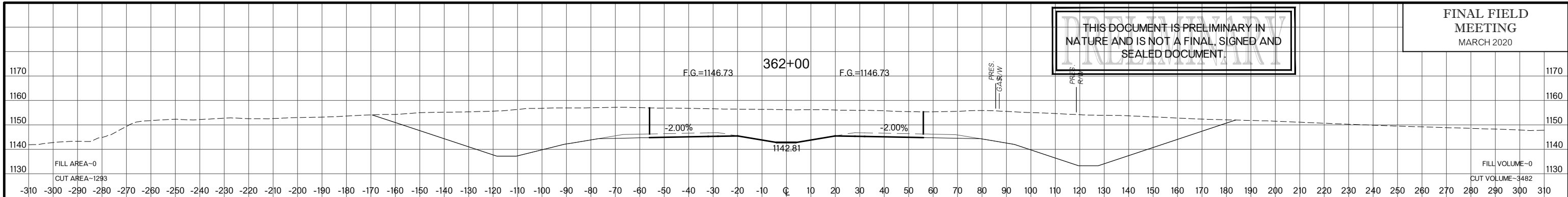
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

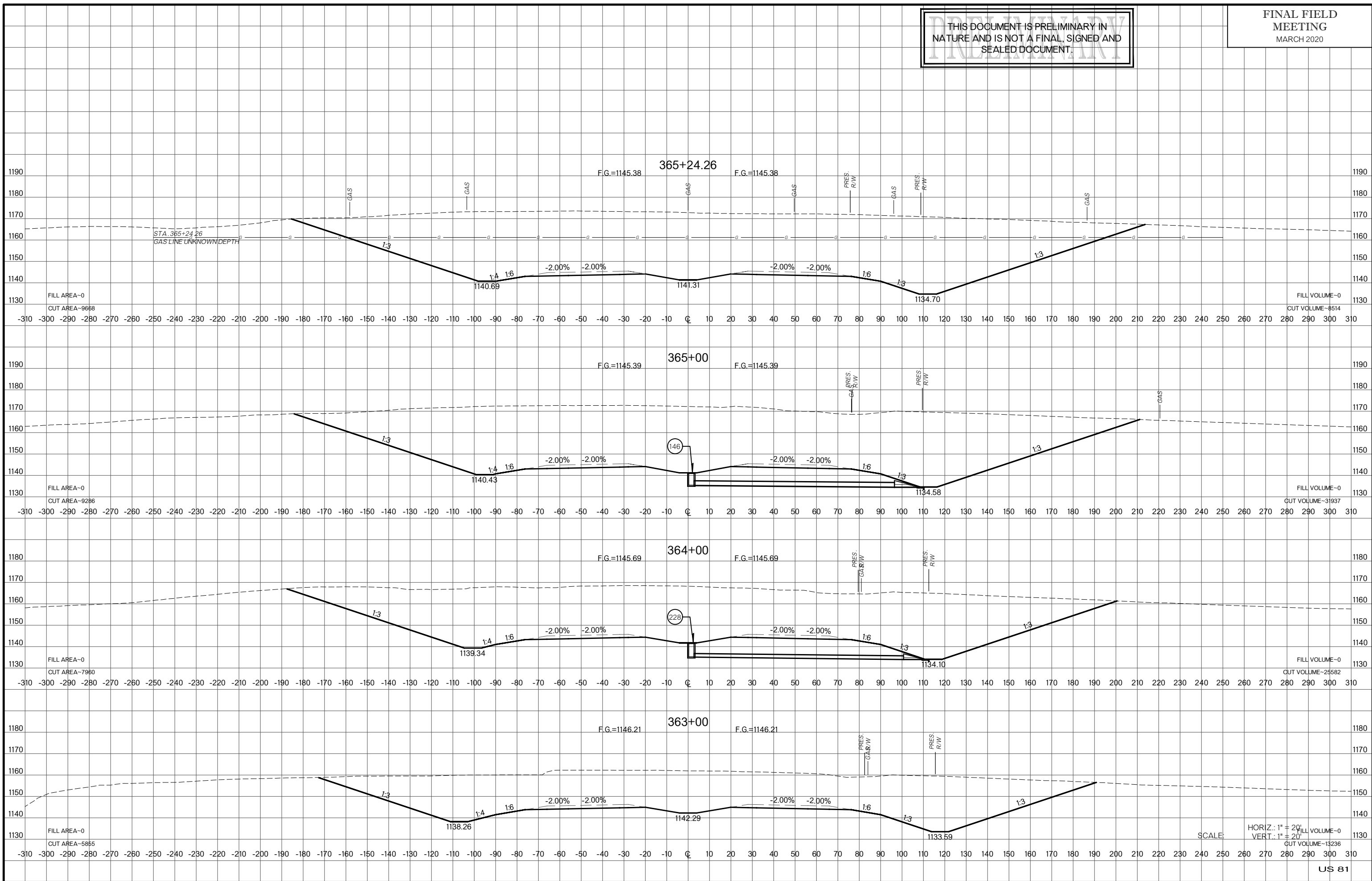
FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

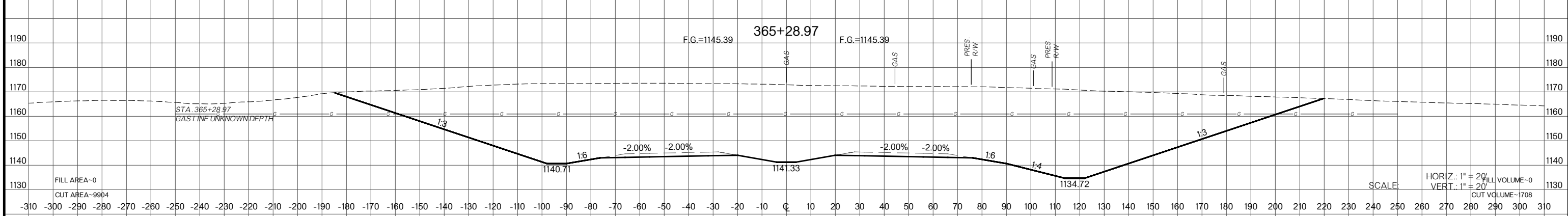
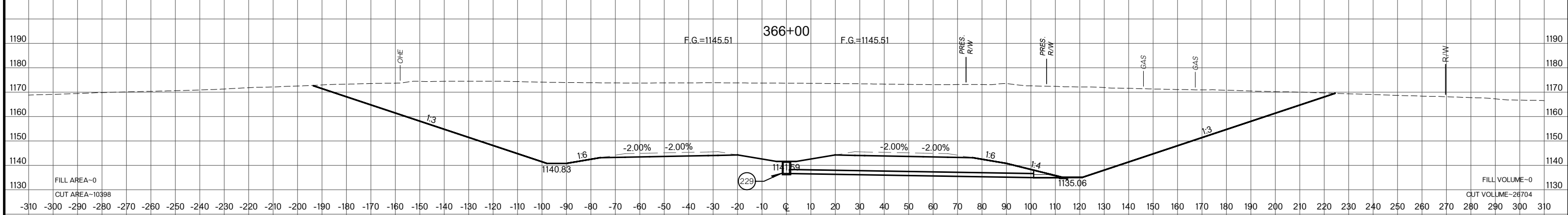
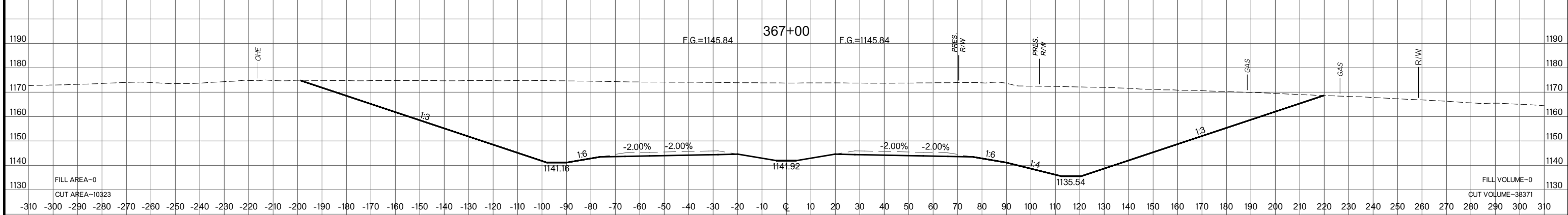
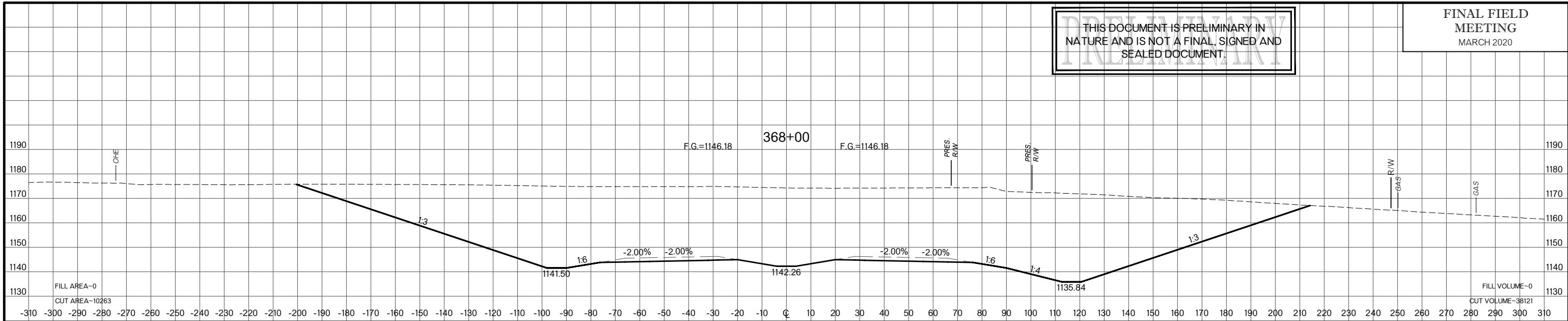
FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

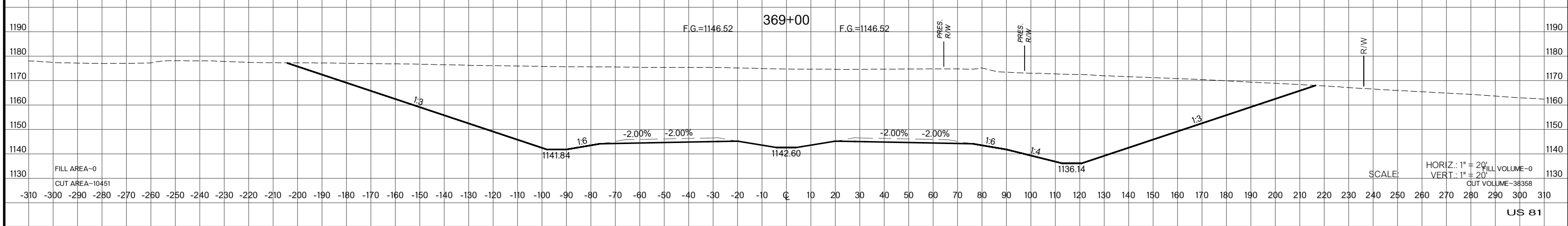
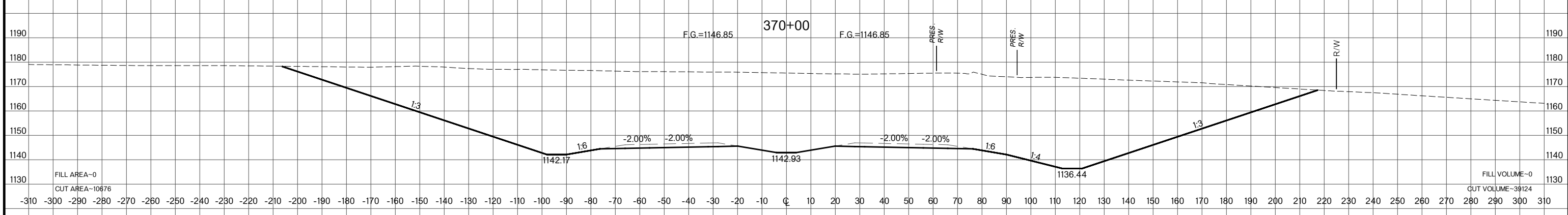
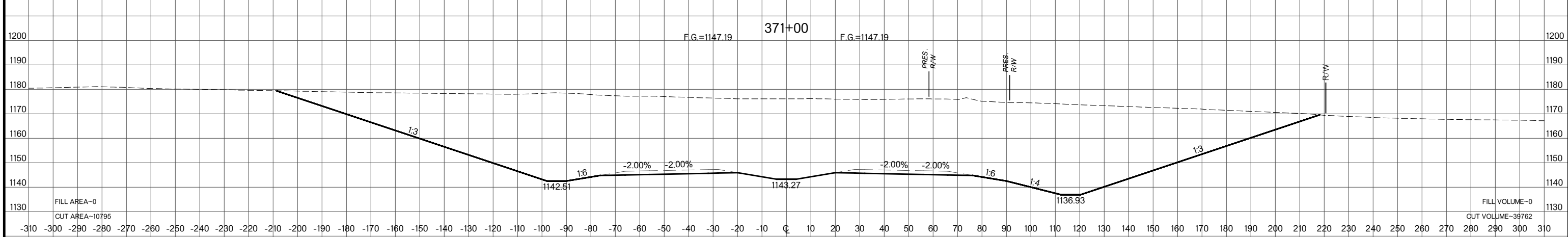
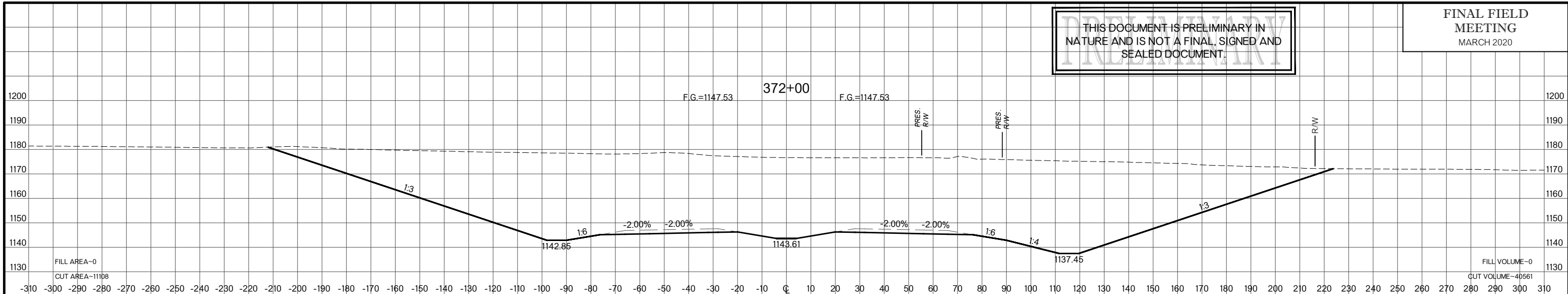
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

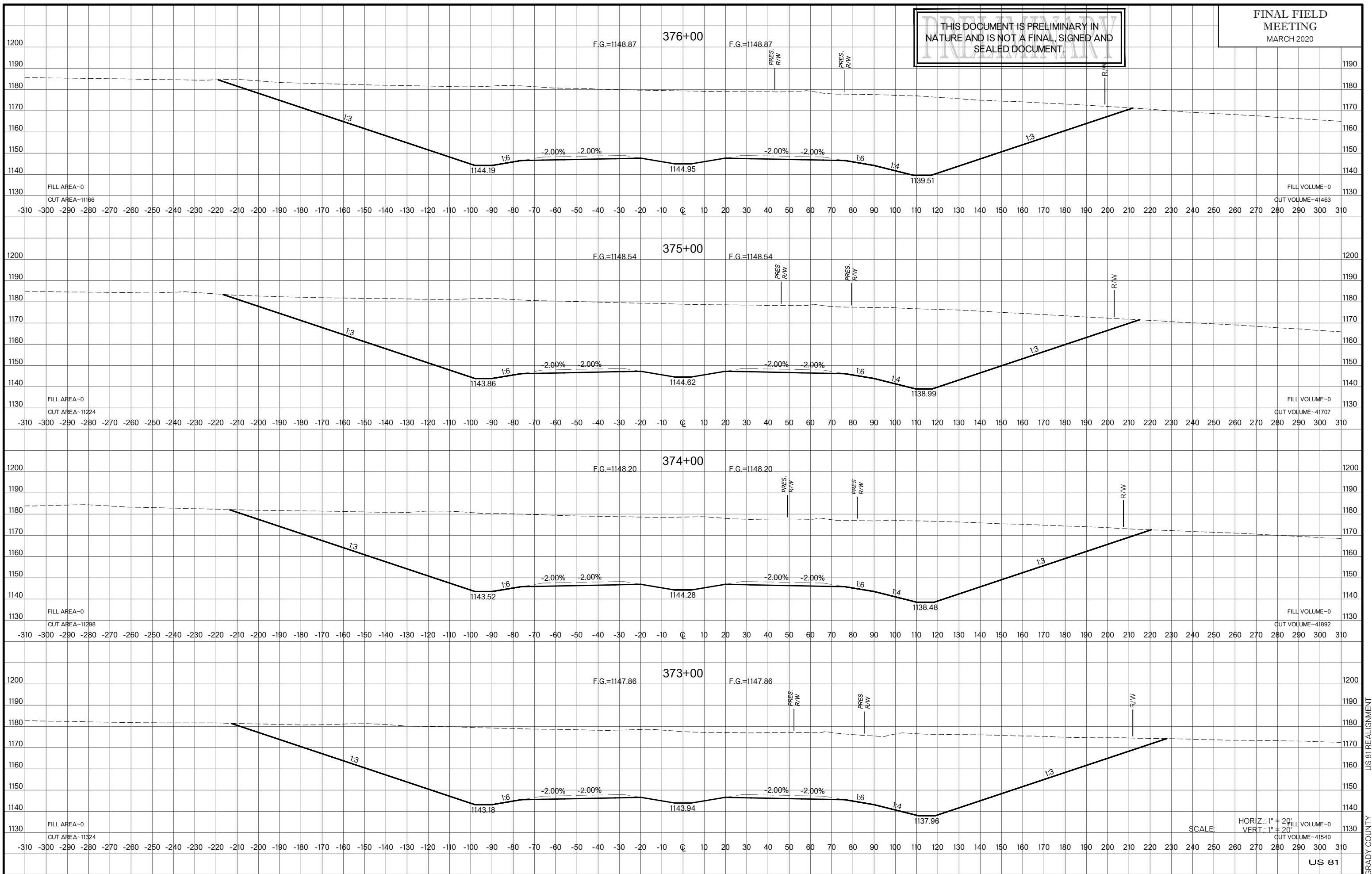
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

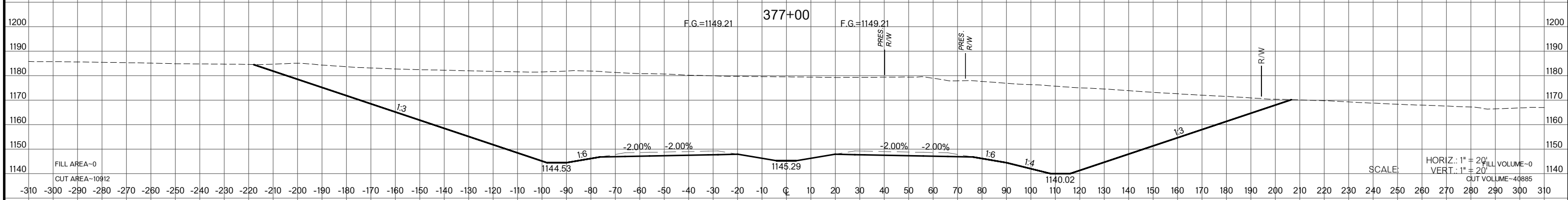
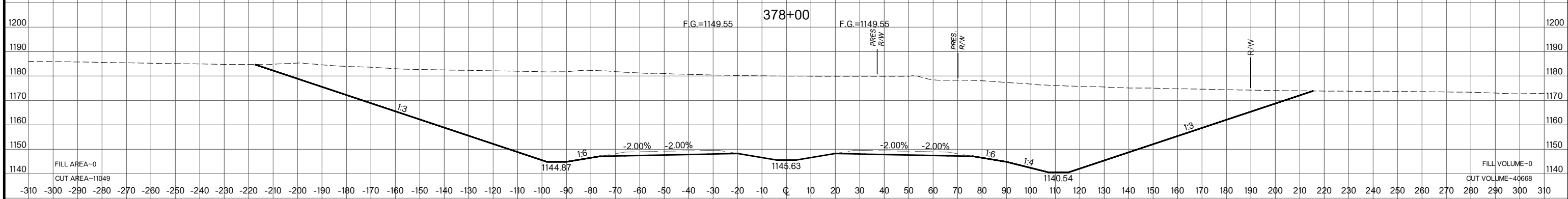
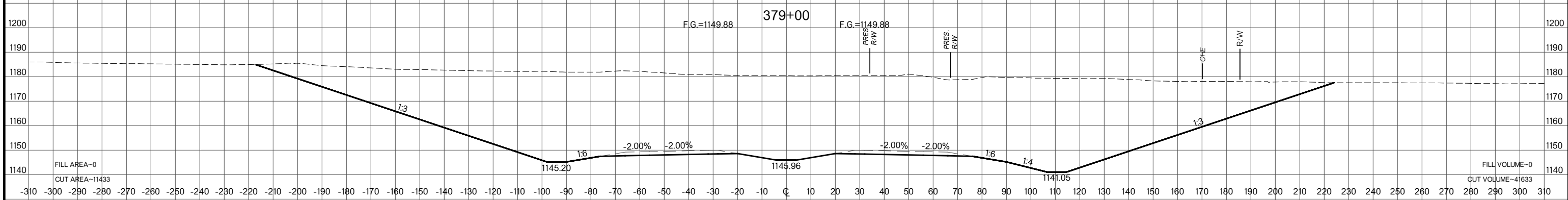
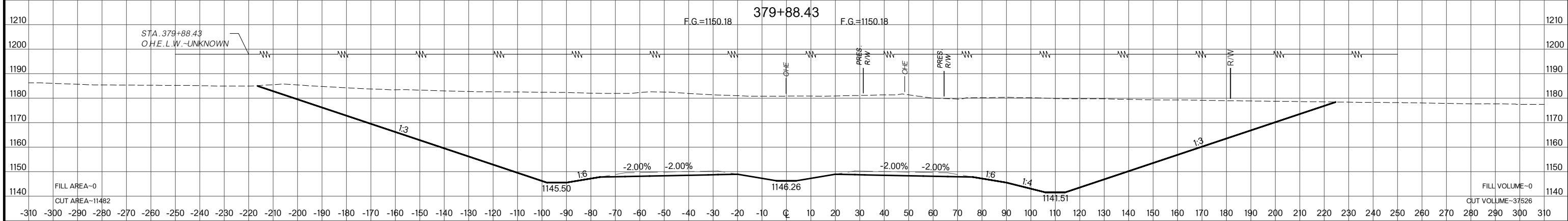
US 81

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

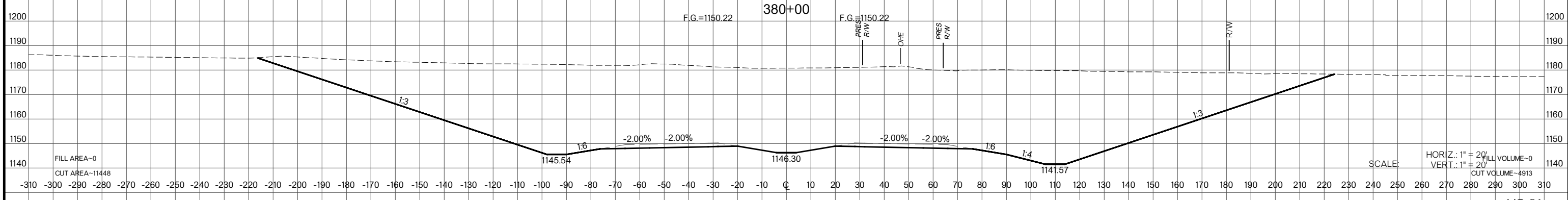
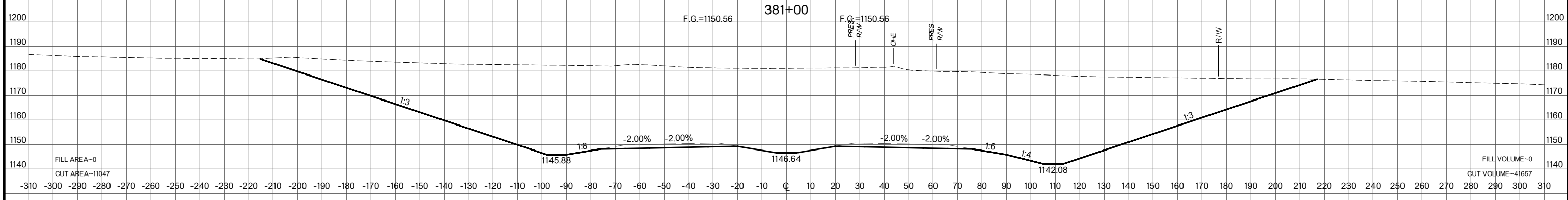
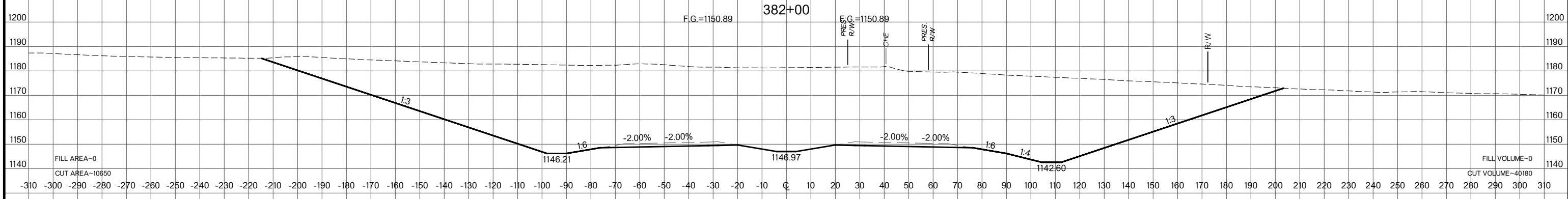
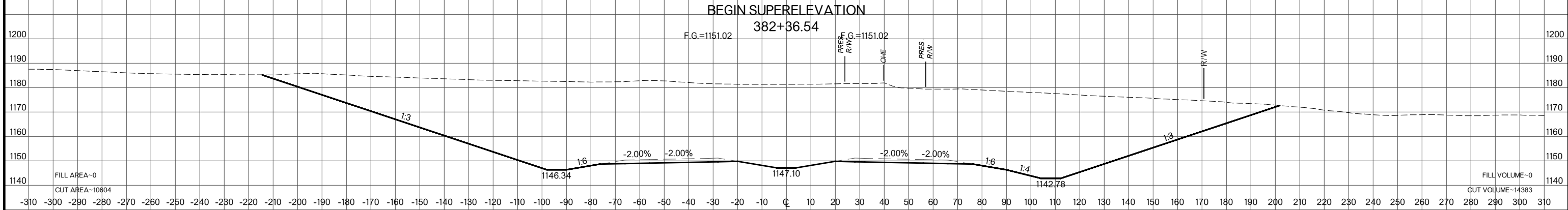
FINAL FIELD MEETING MARCH 2020



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

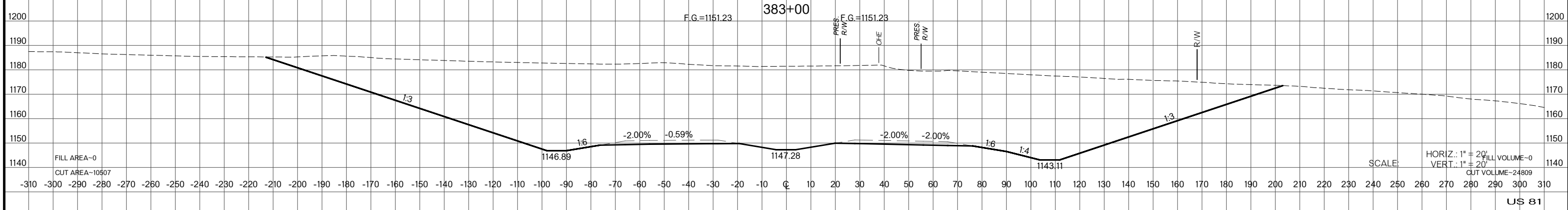
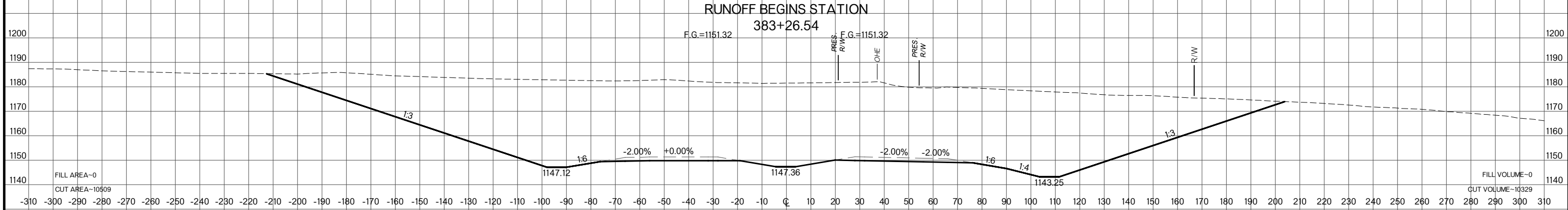
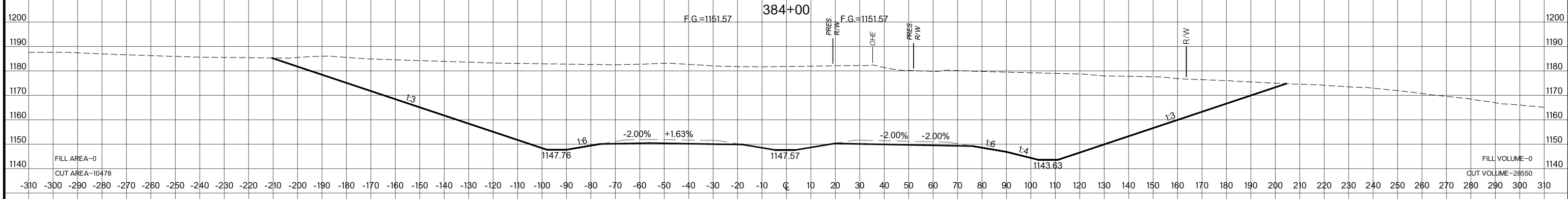
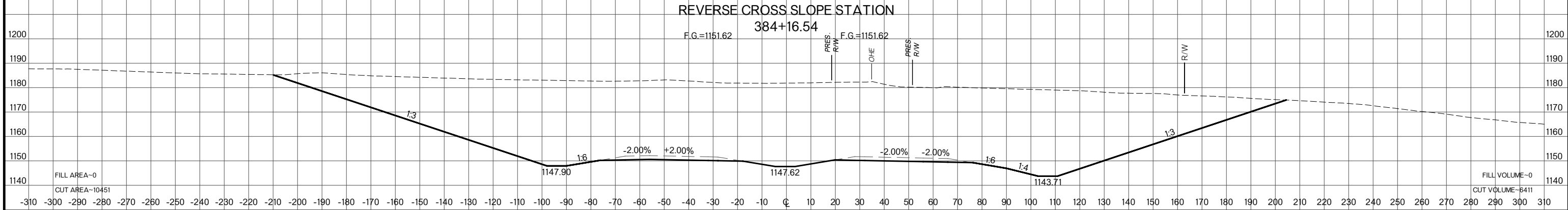
FINAL FIELD MEETING MARCH 2020



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

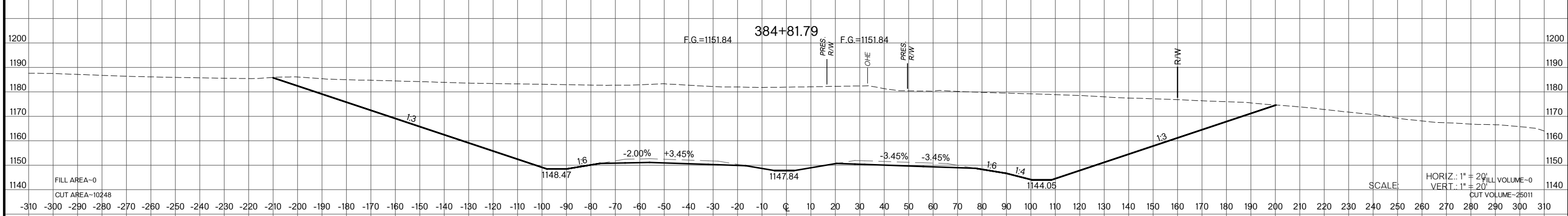
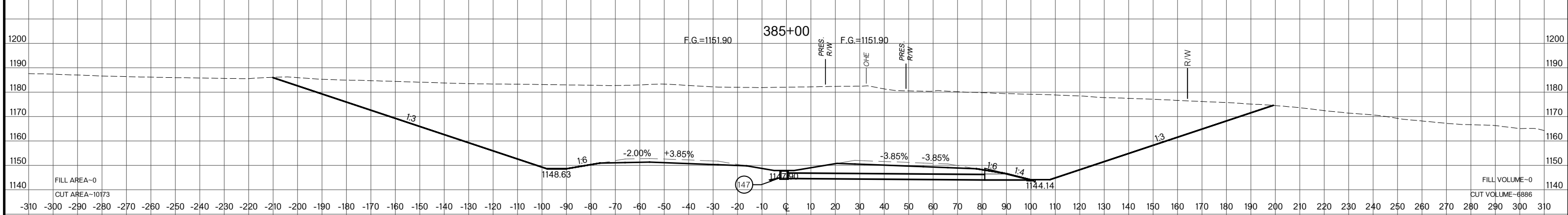
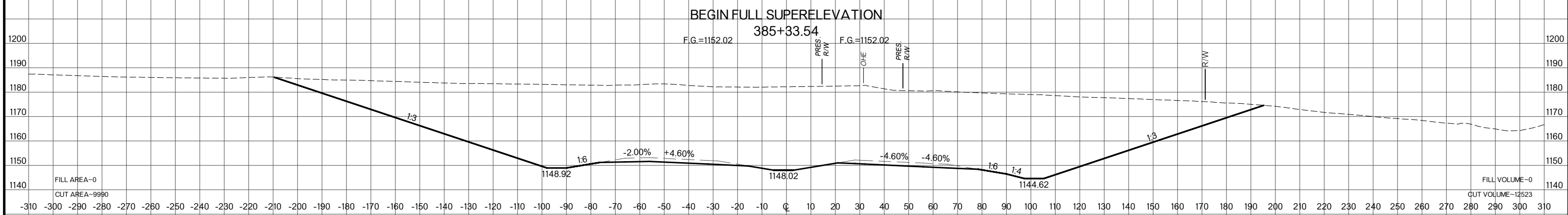
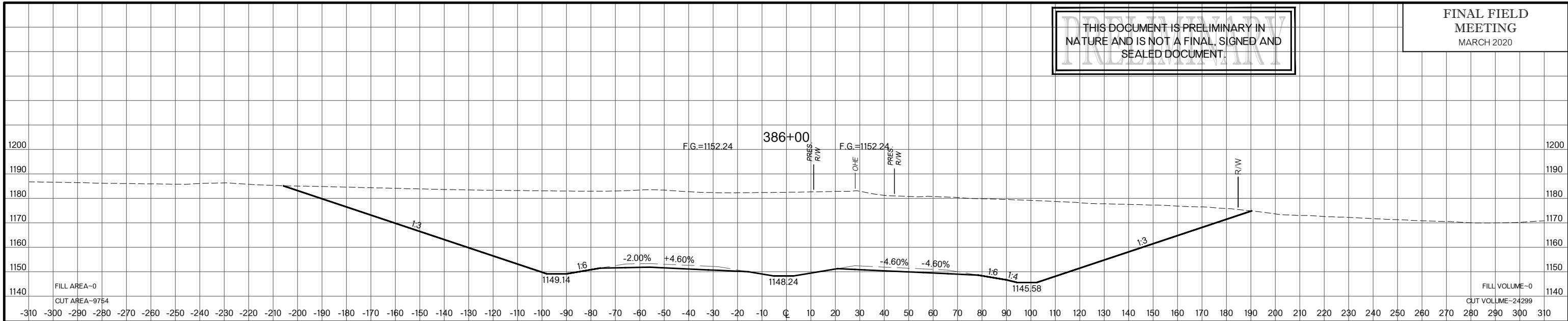
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

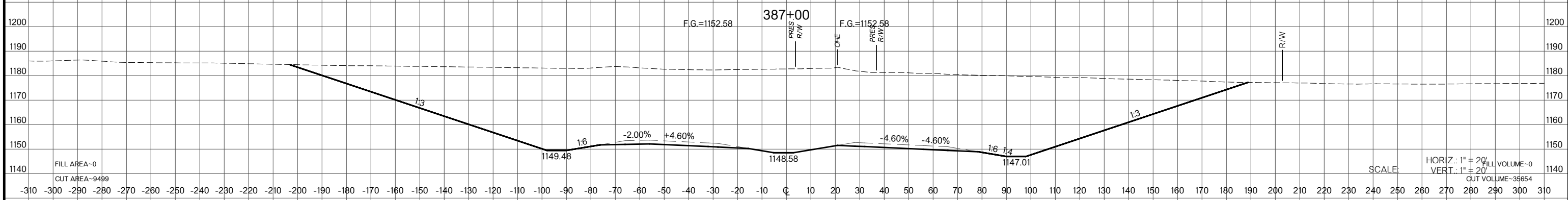
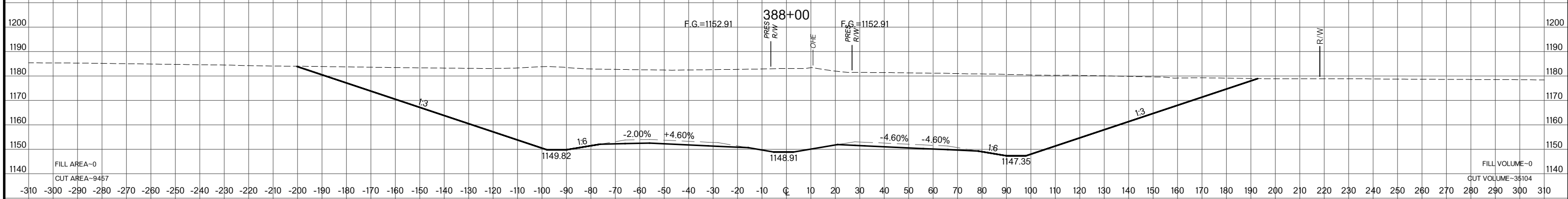
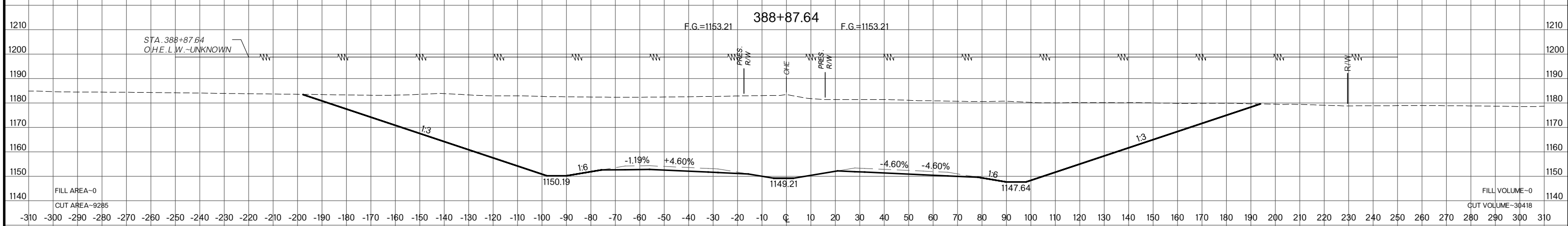
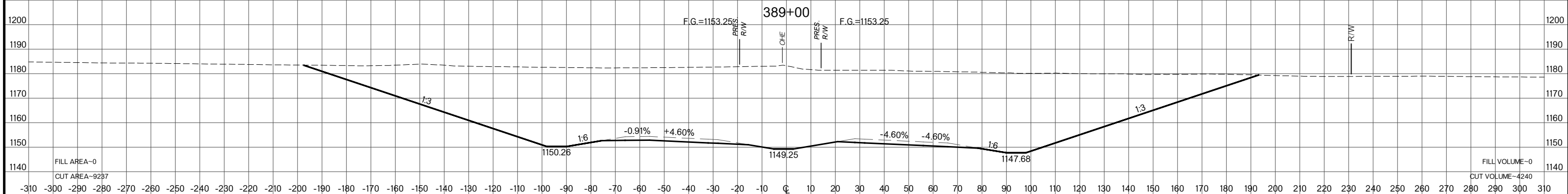
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

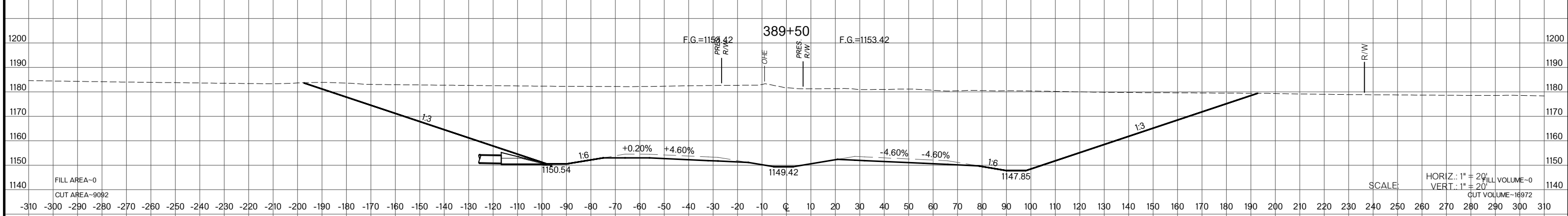
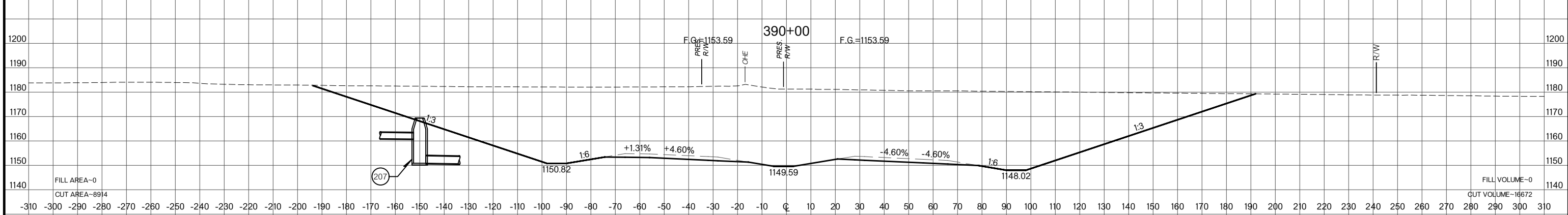
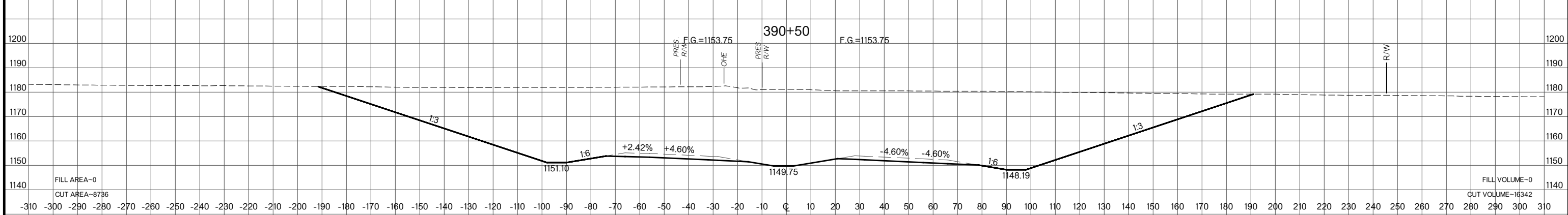
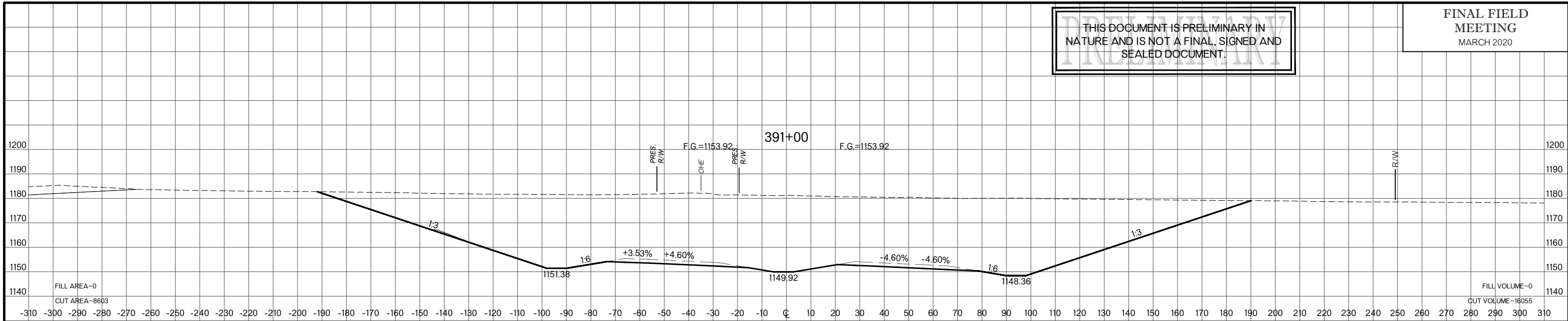
FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



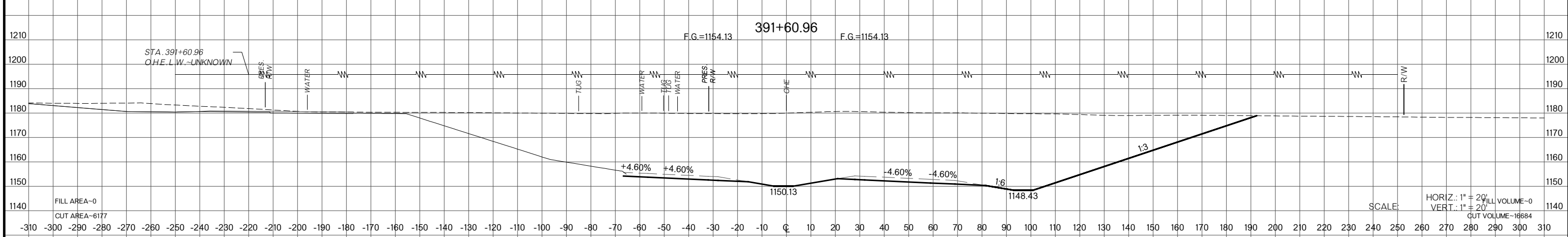
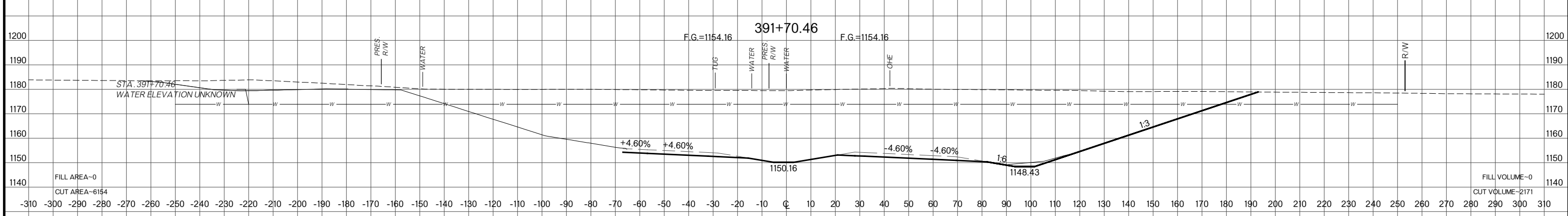
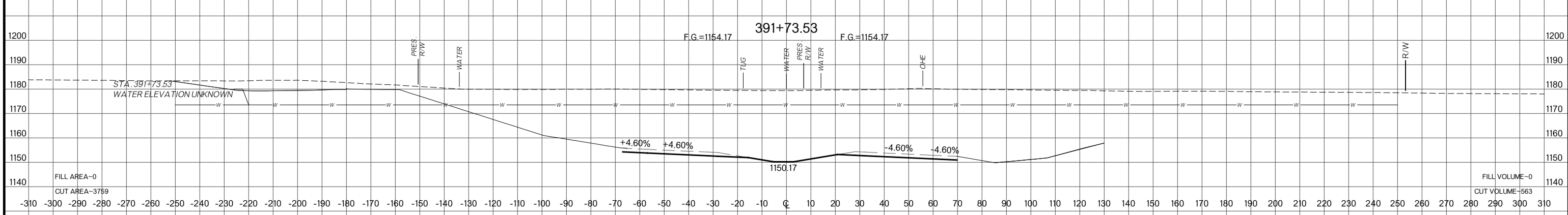
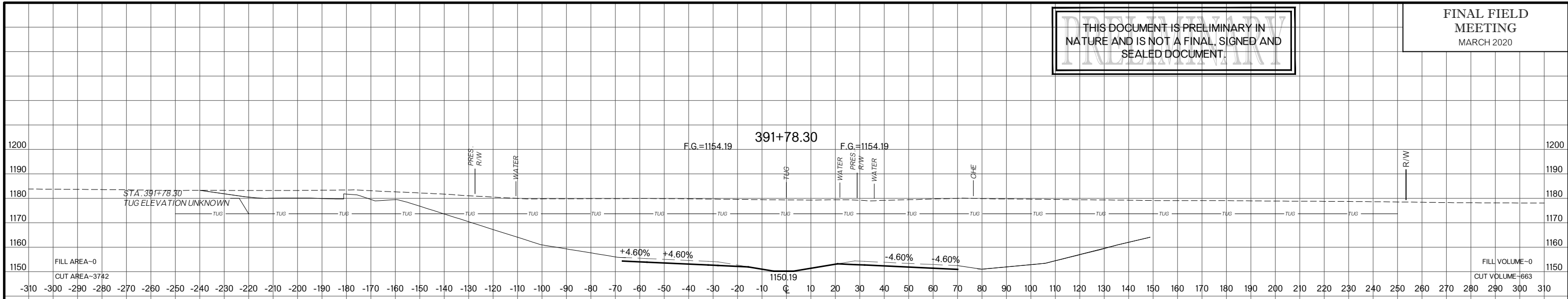
SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

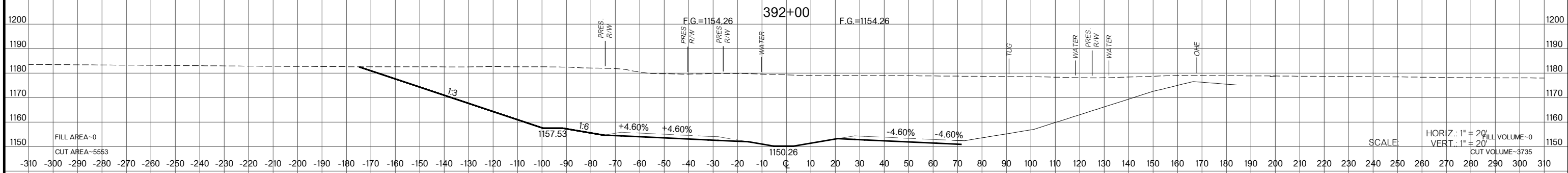
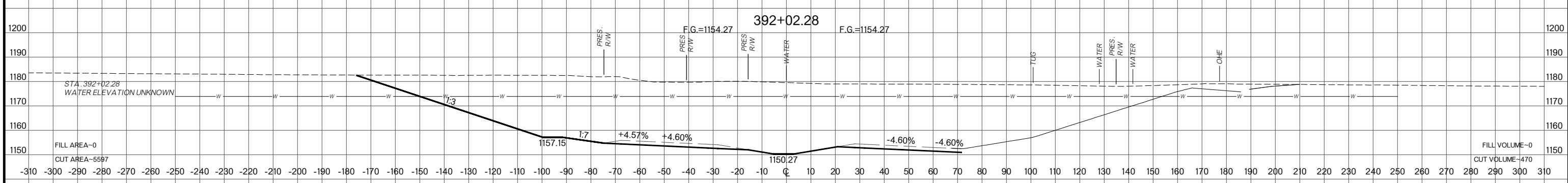
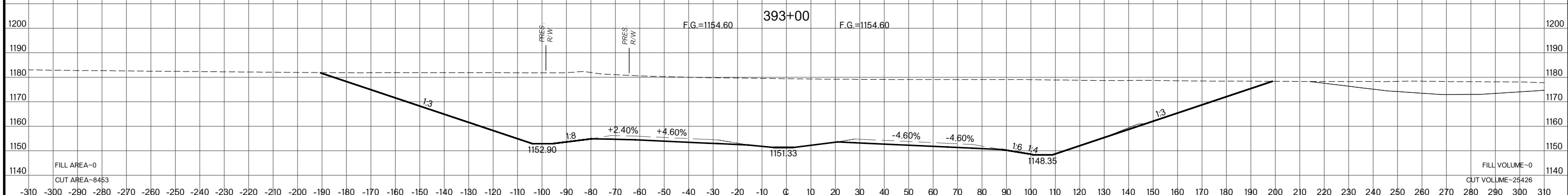
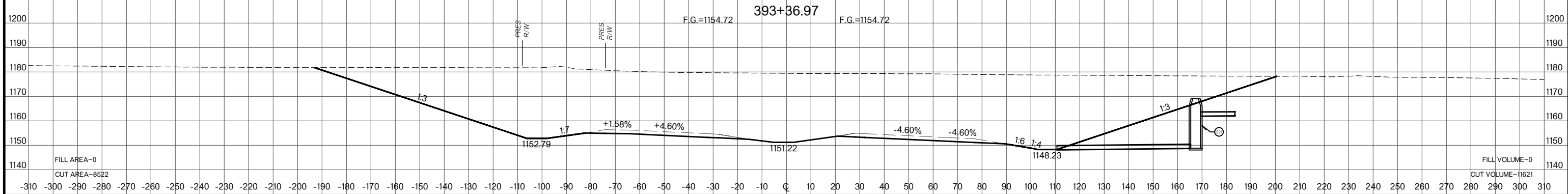


SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

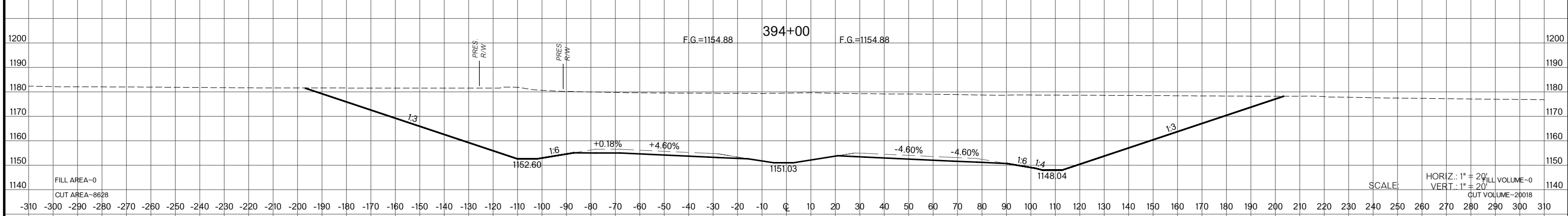
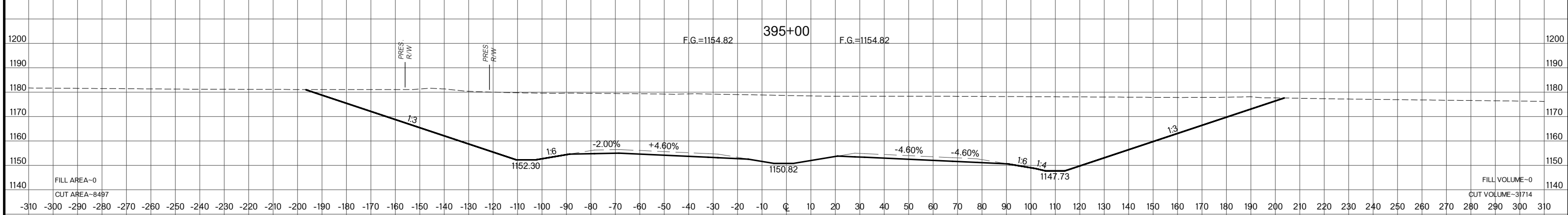
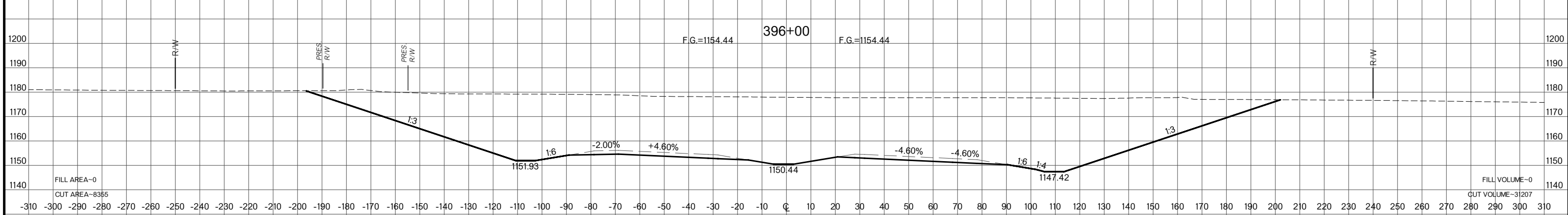
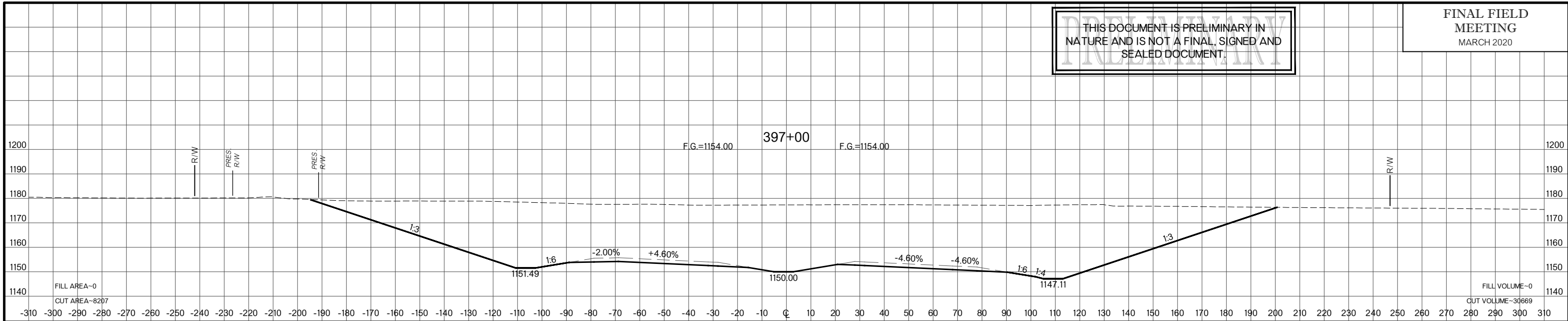
FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

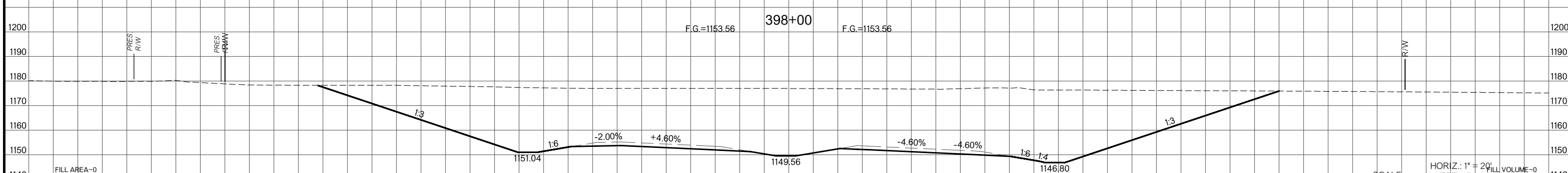
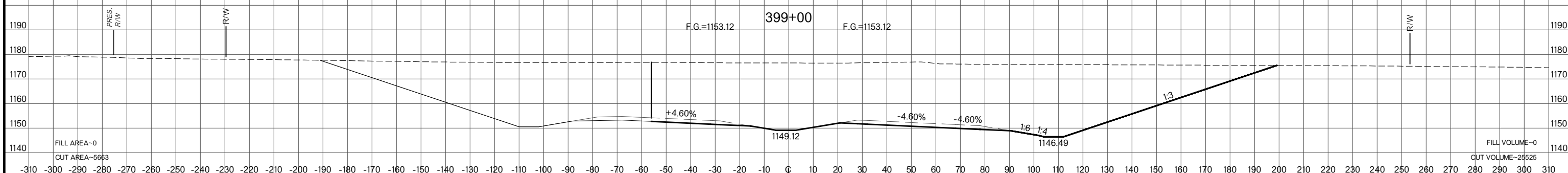
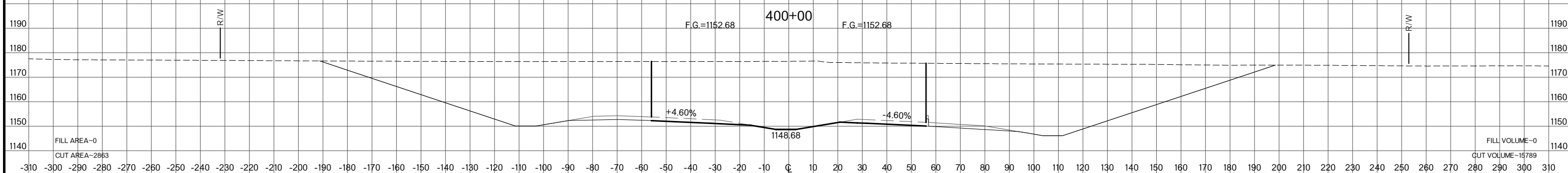
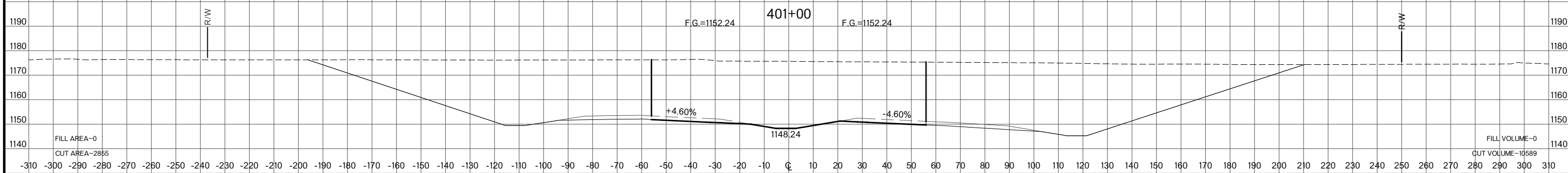
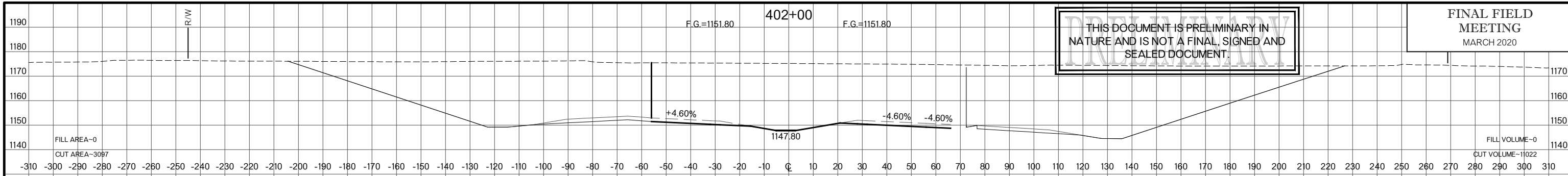
FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

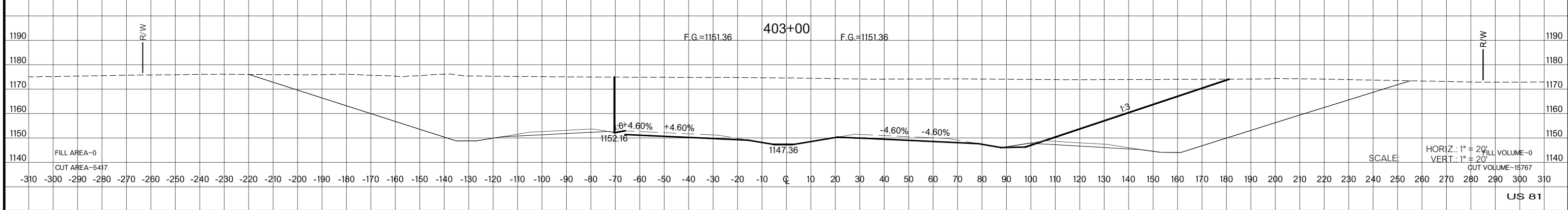
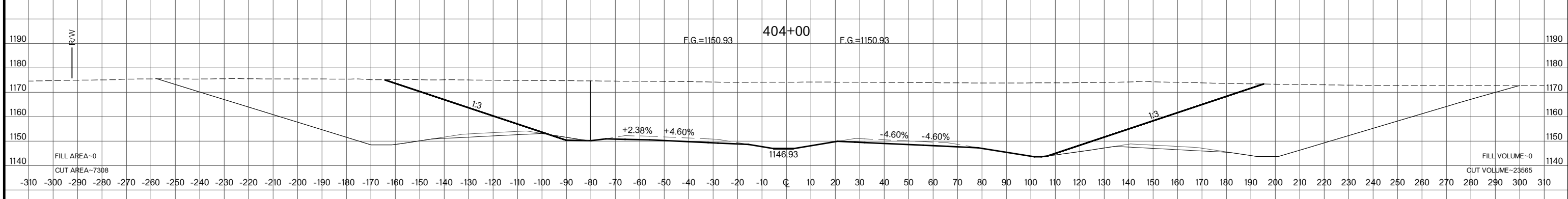
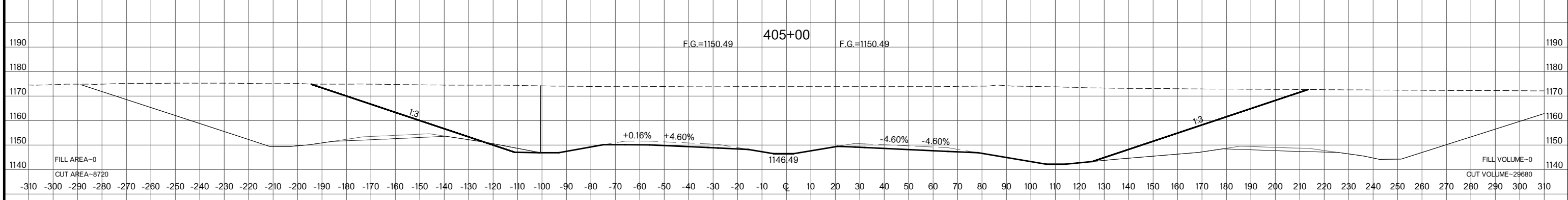
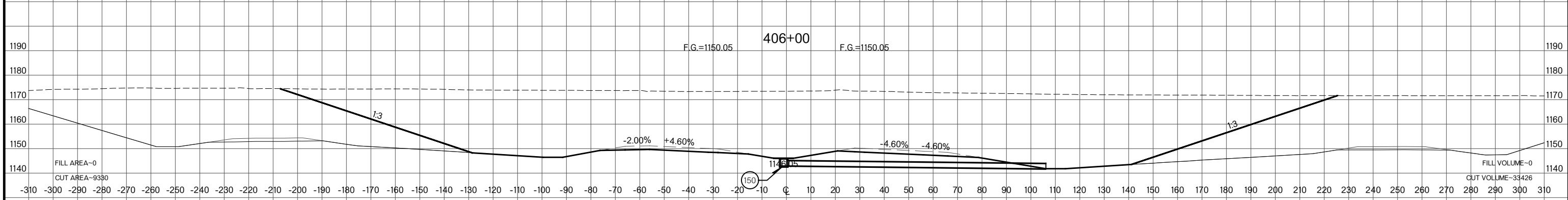
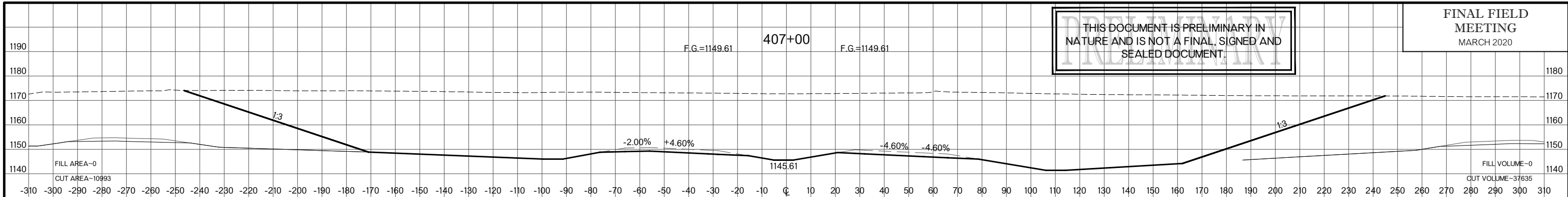
FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

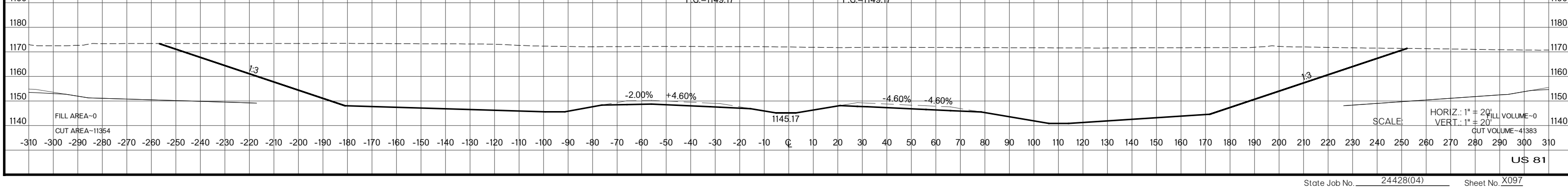
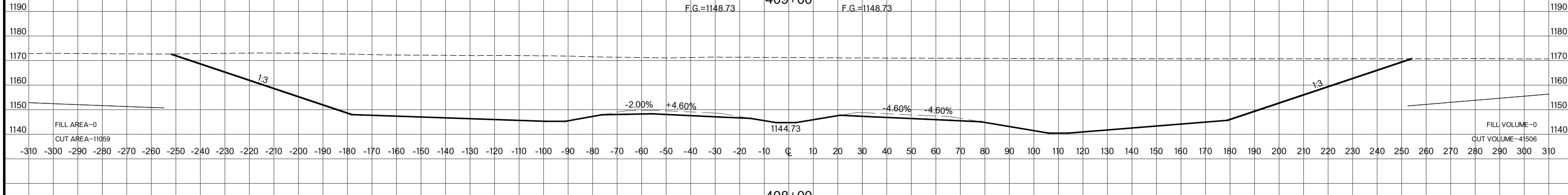
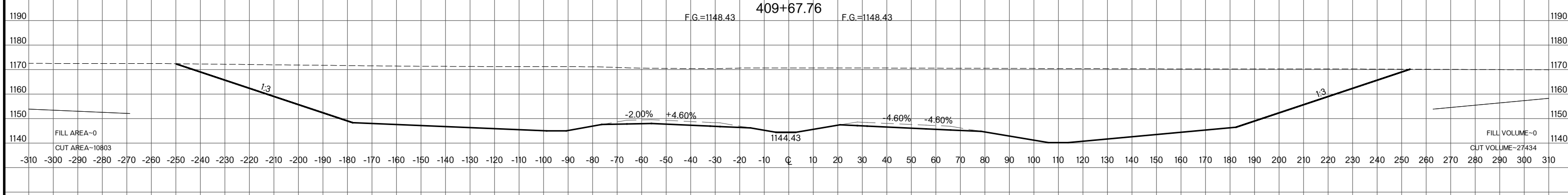
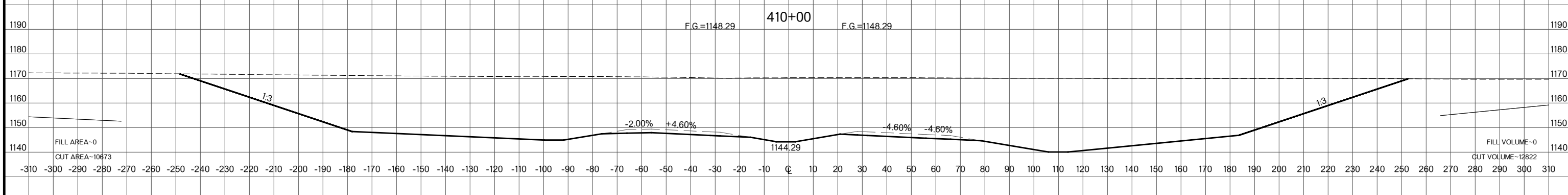
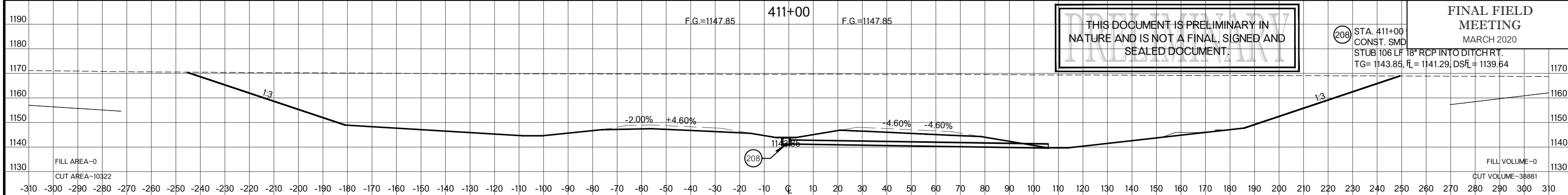
US 81

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

208

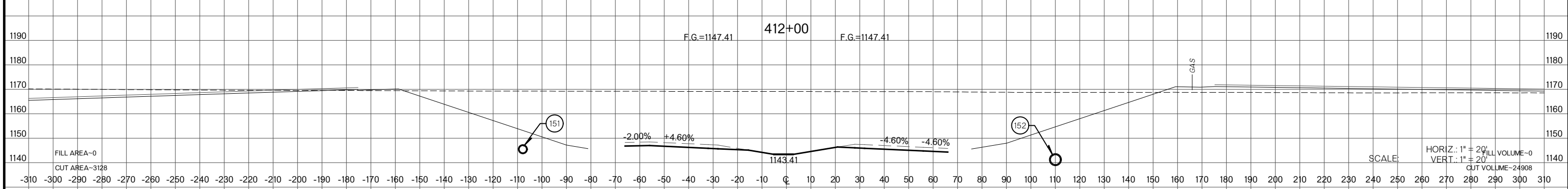
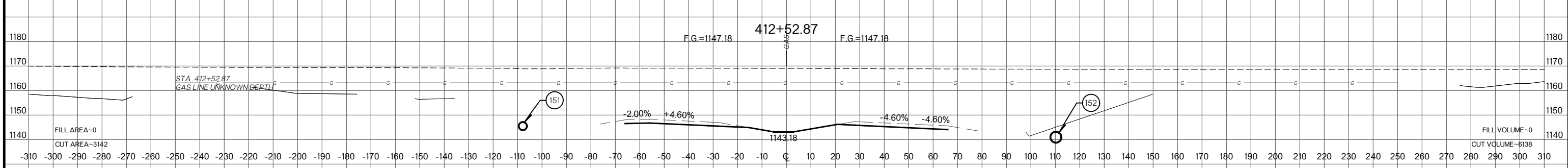
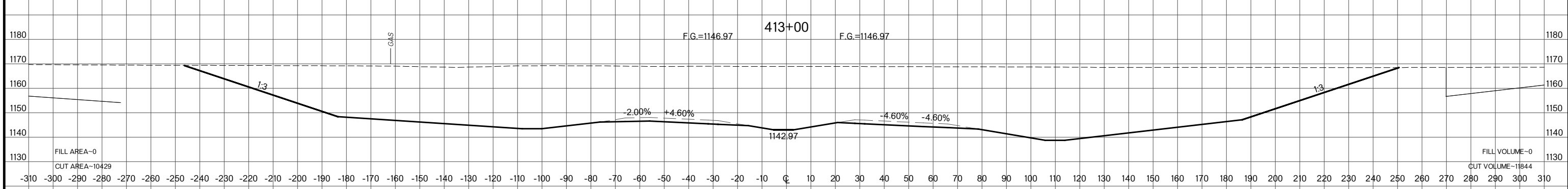
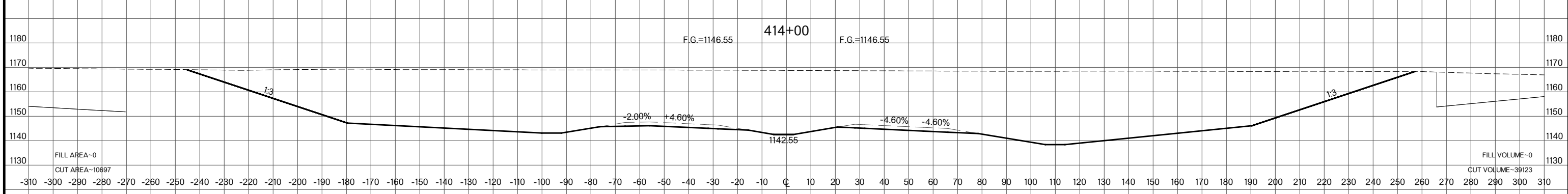
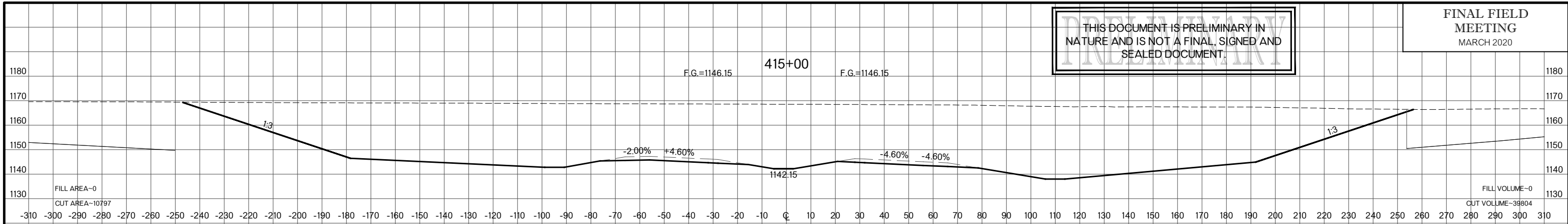
STA. 411+00
CONST. SMD
STUB 106 LF 18" RCP INTO DITCH RT.
TG= 1143.85, FL= 1141.29, DSW= 1139.64

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

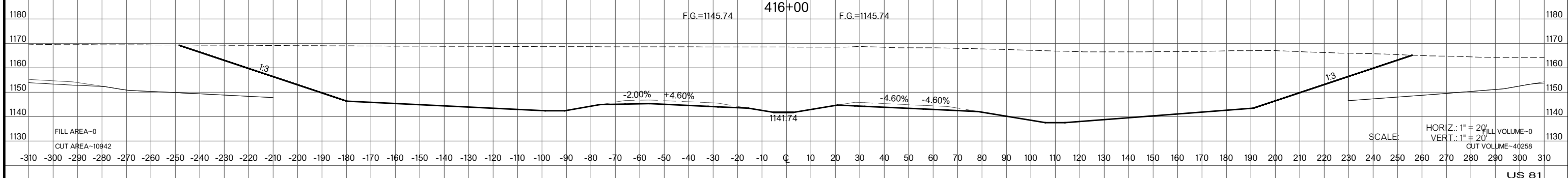
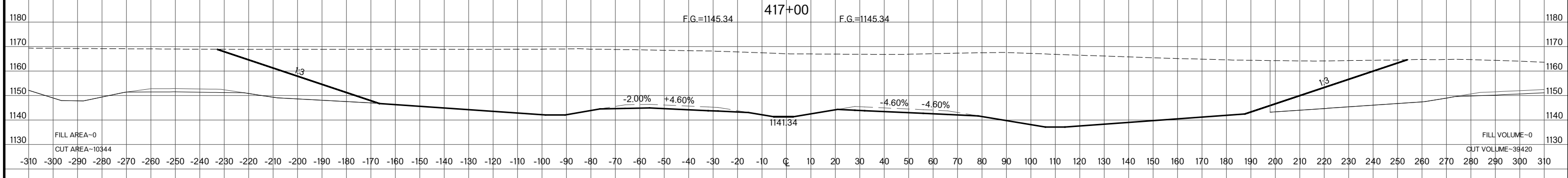
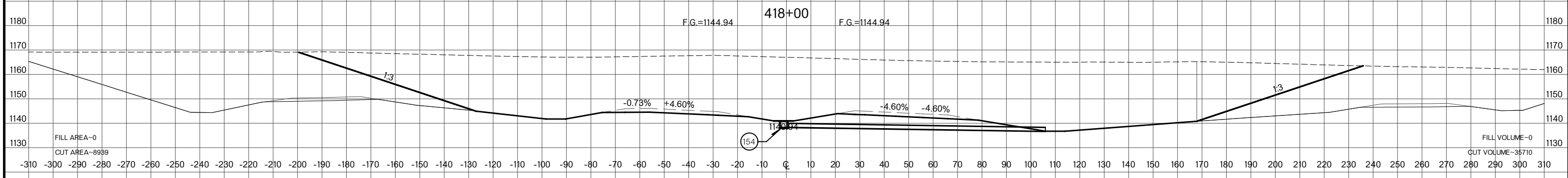
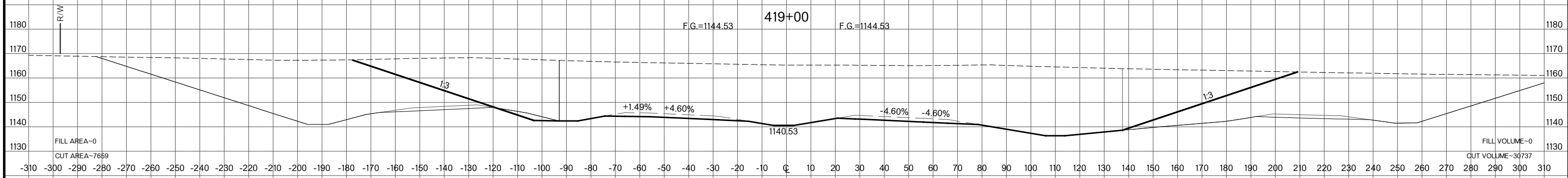
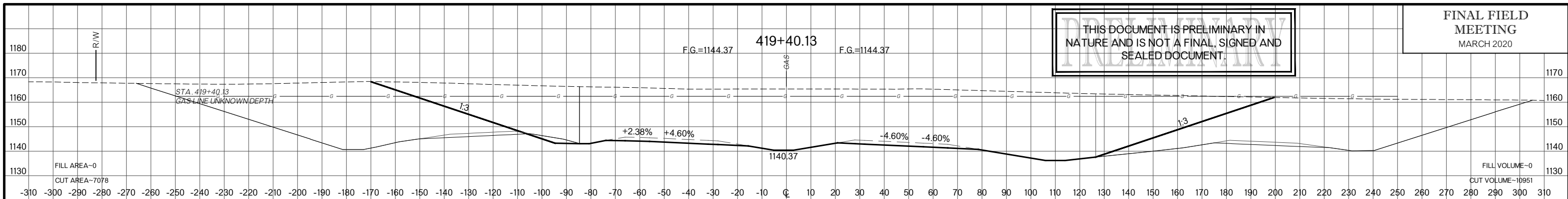
FINAL FIELD MEETING MARCH 2020



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

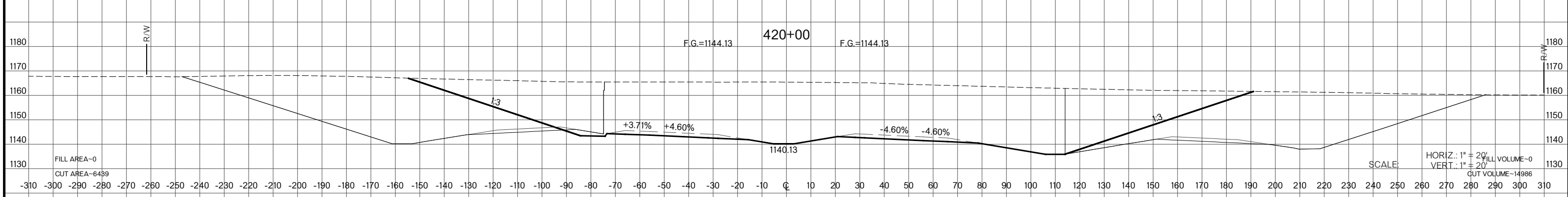
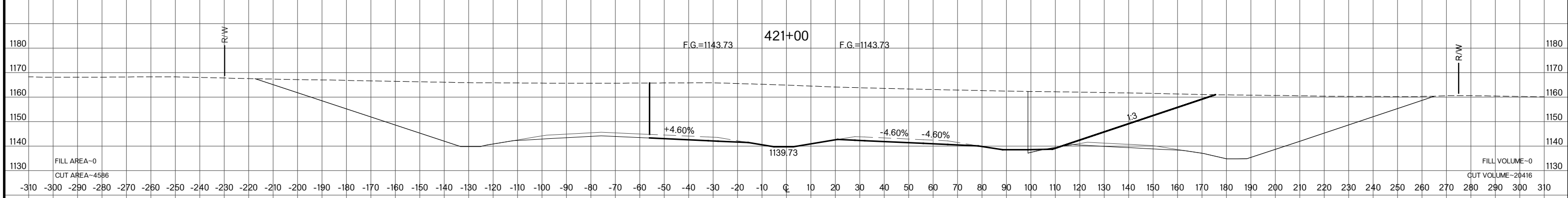
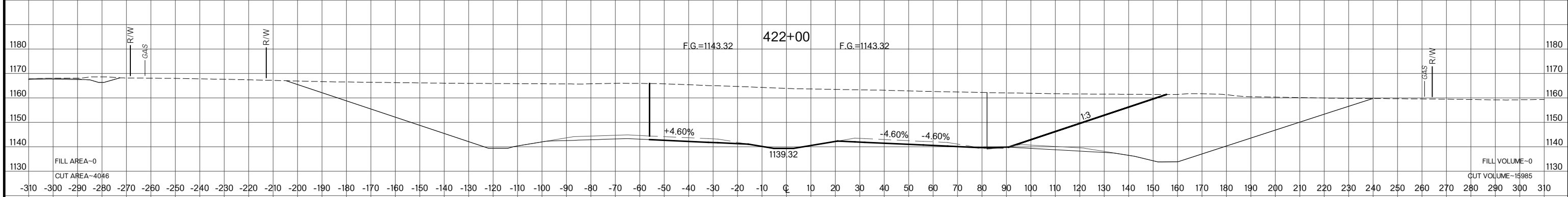
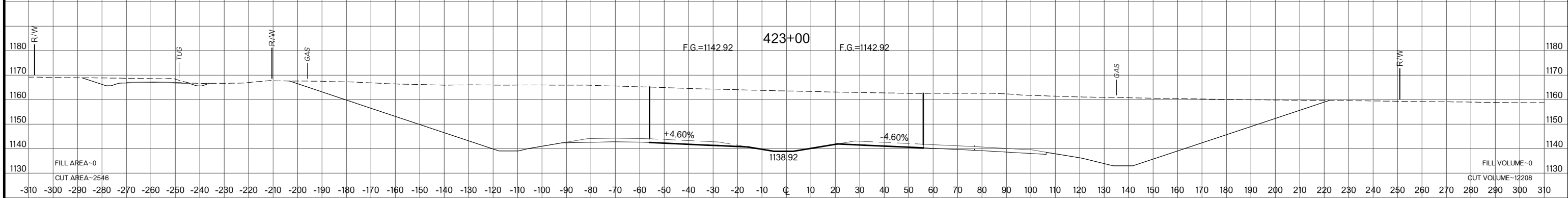
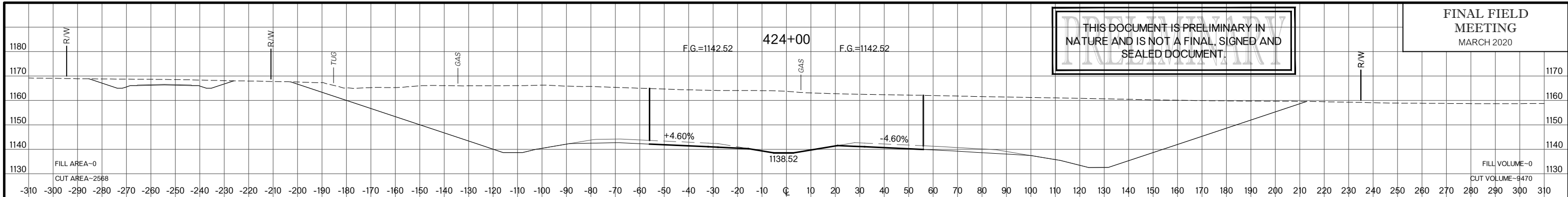
FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

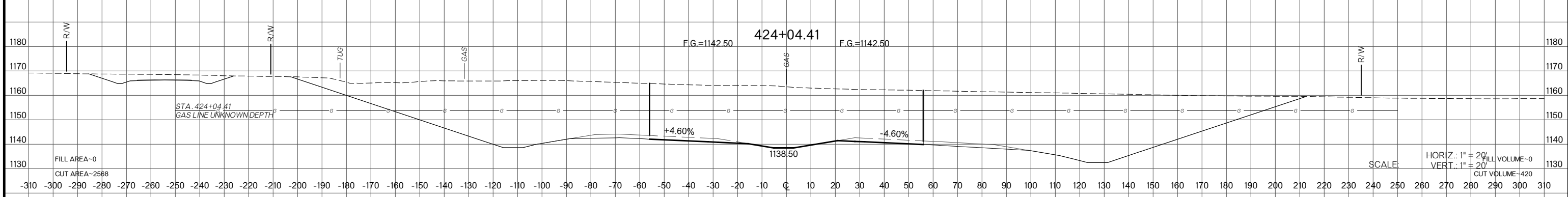
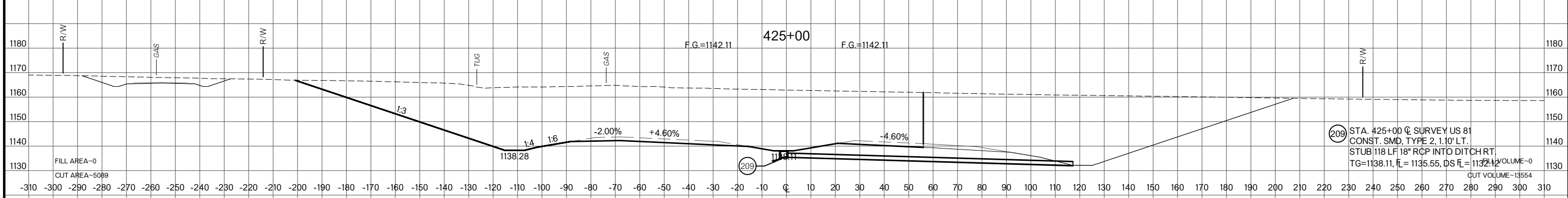
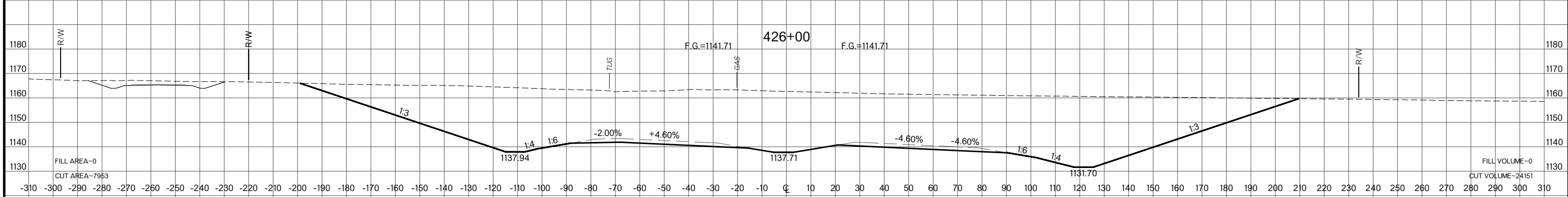
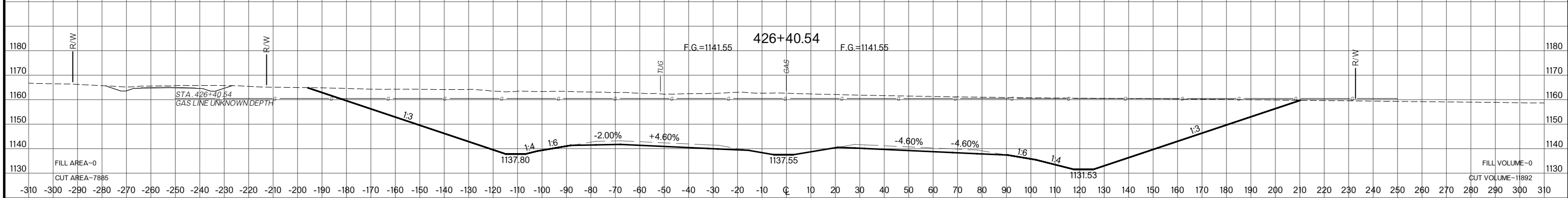
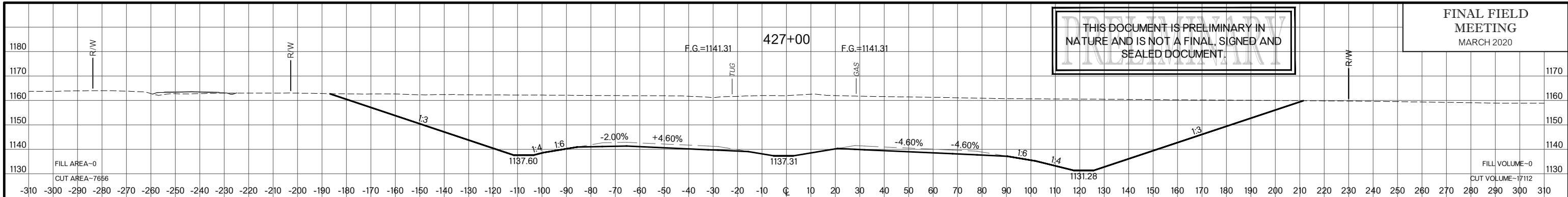
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

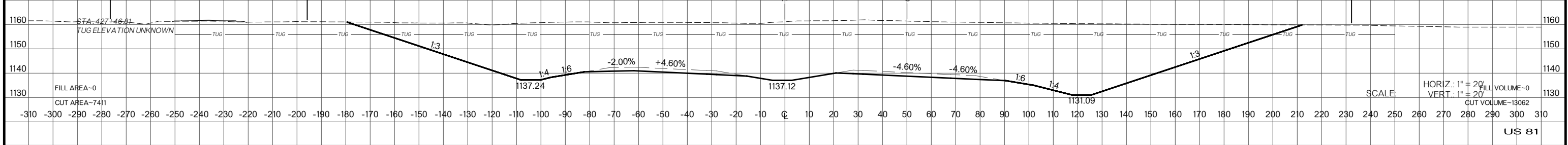
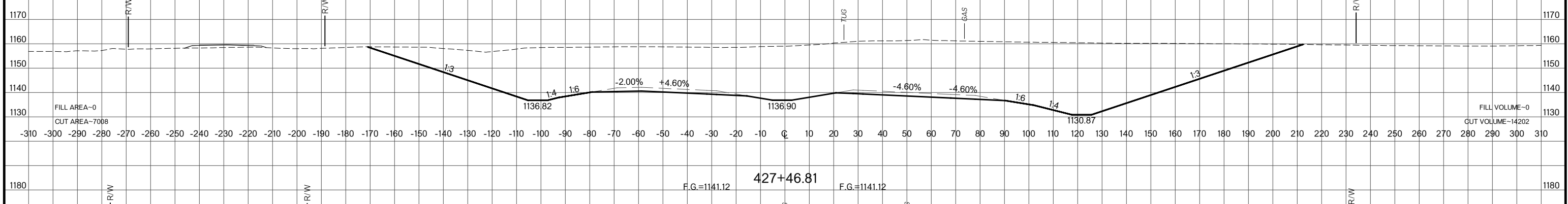
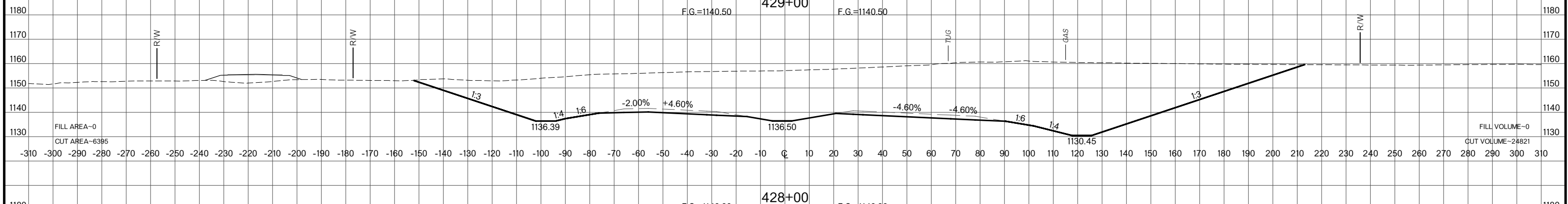
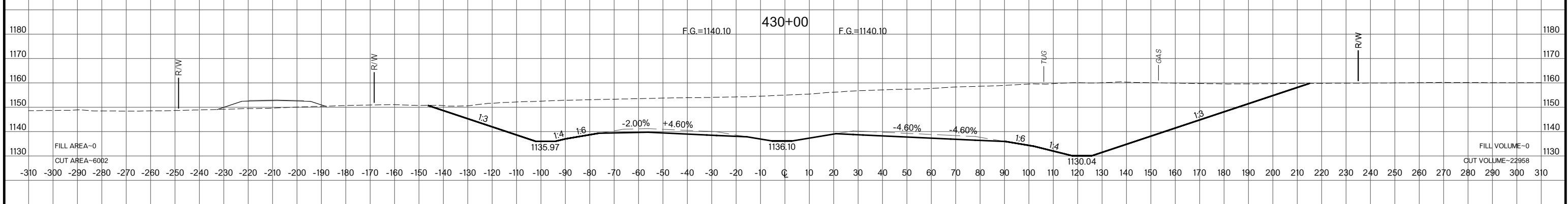
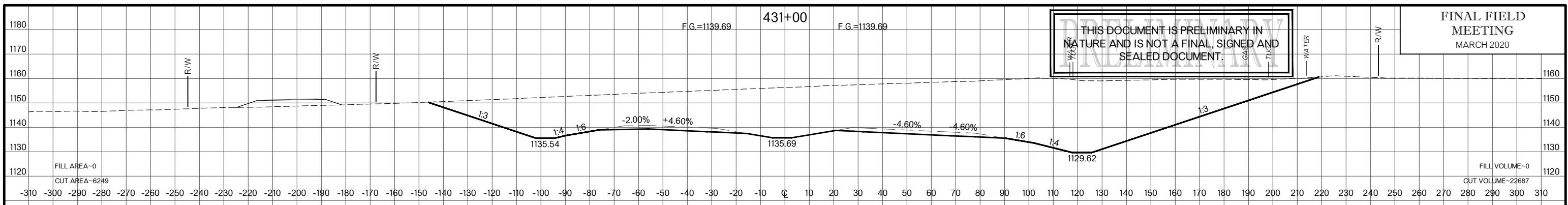
FINAL FIELD MEETING MARCH 2020



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

US 81 REALIGNMENT GRADY COUNTY

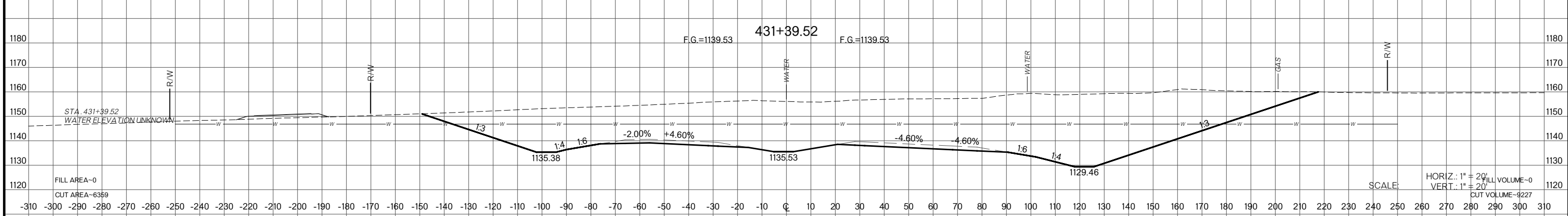
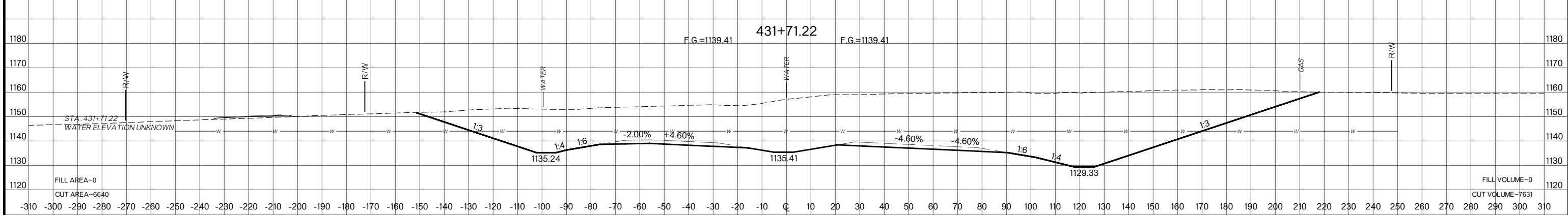
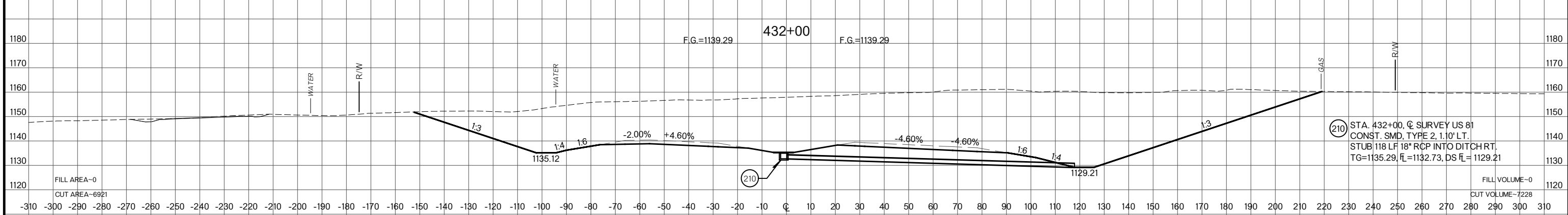
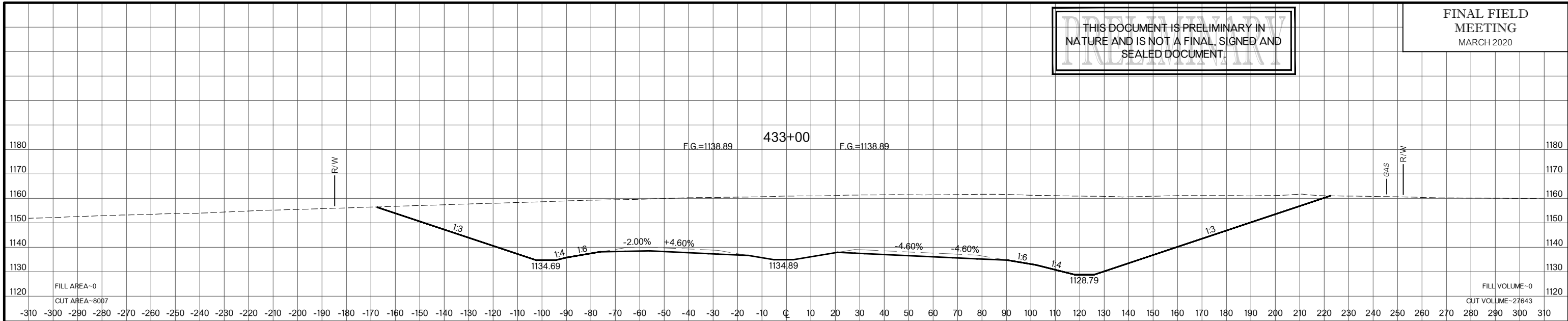
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

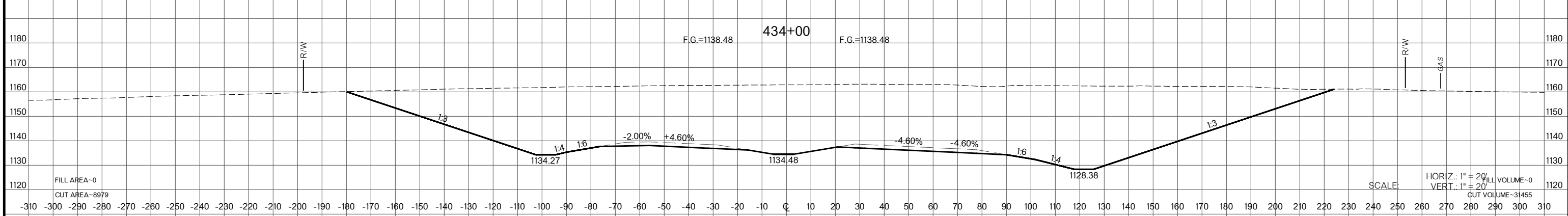
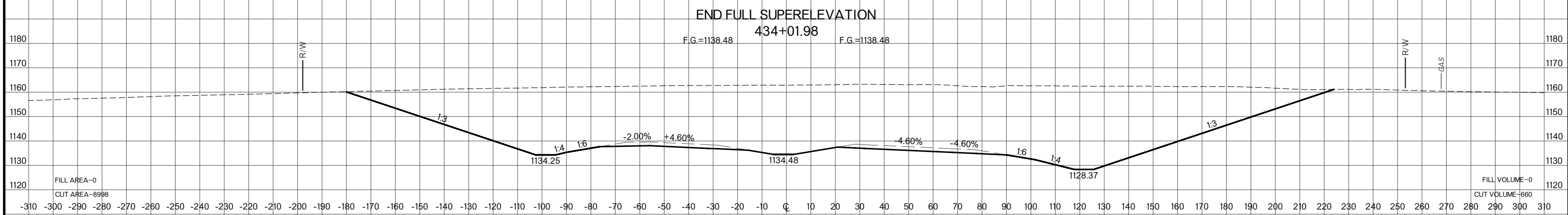
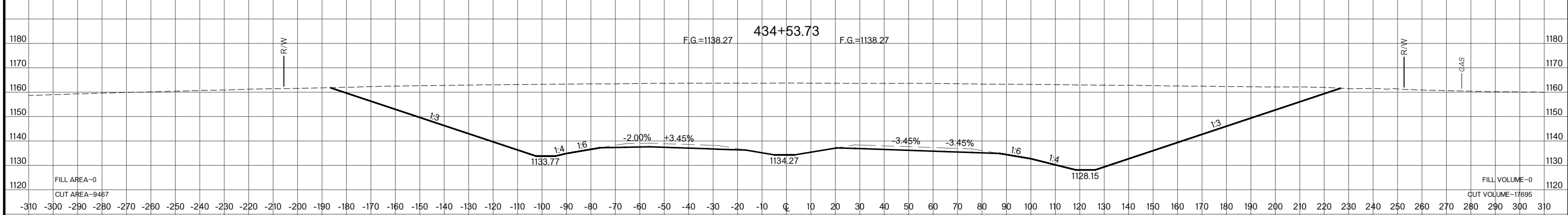
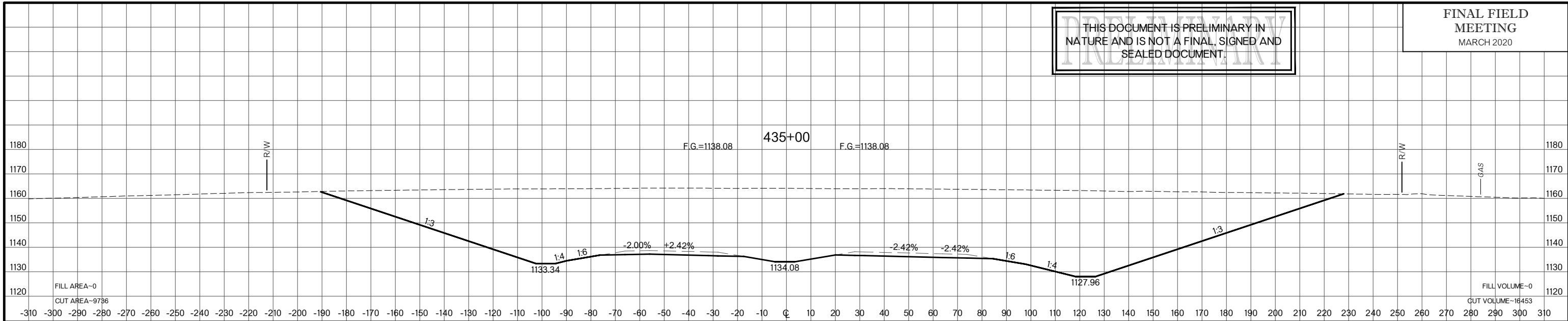
FINAL FIELD MEETING MARCH 2020



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

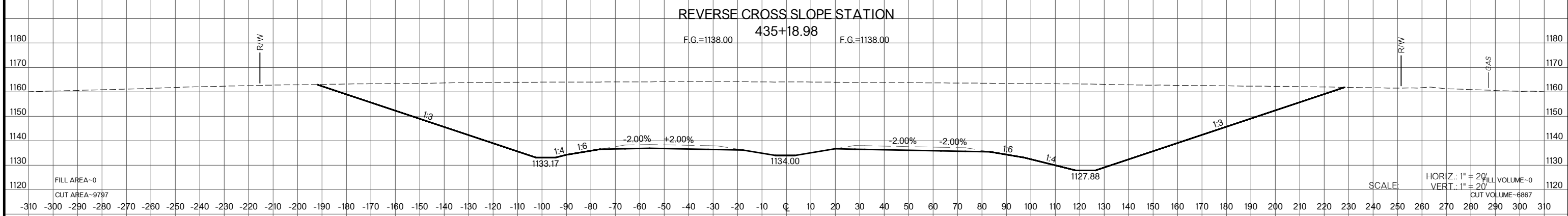
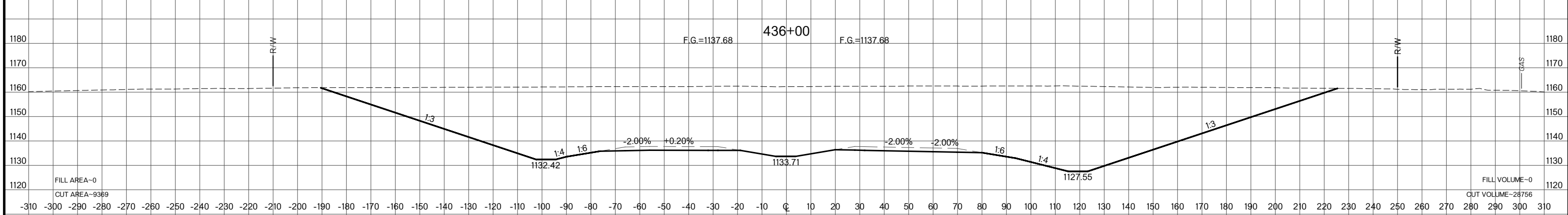
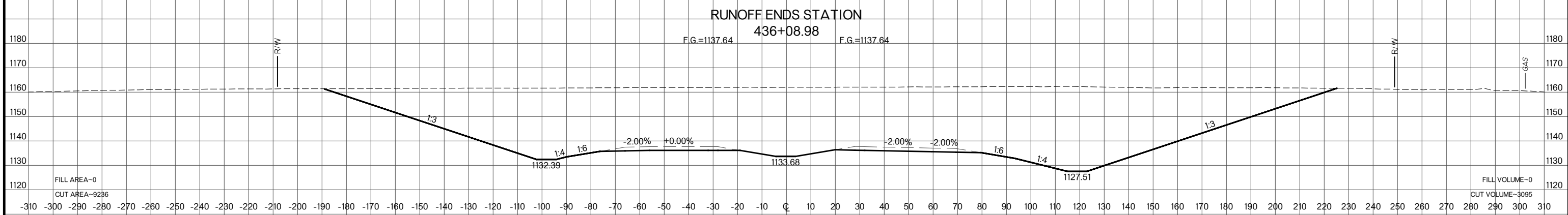
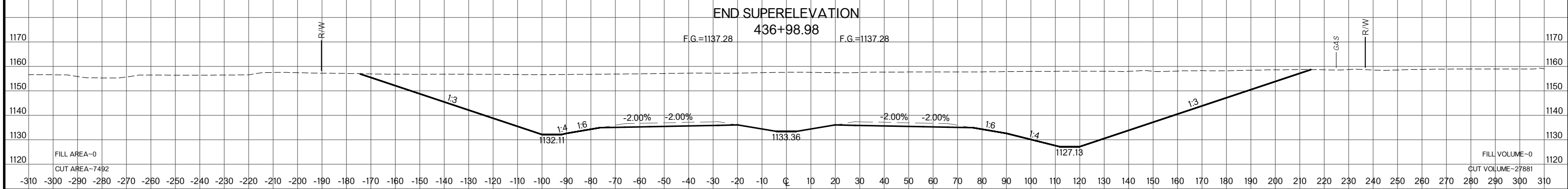
FINAL FIELD MEETING MARCH 2020



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

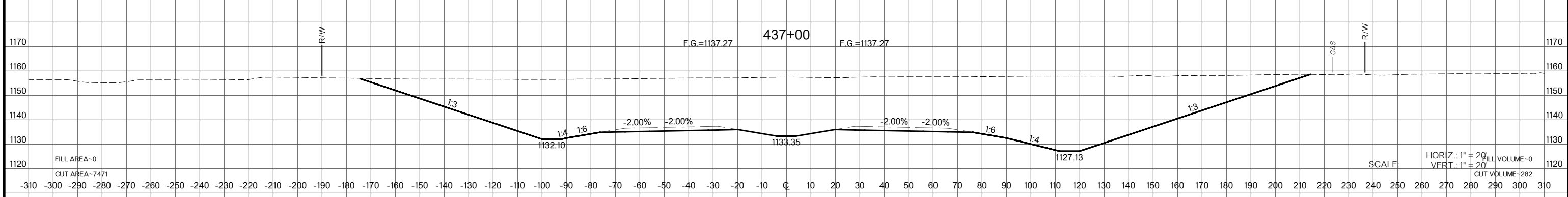
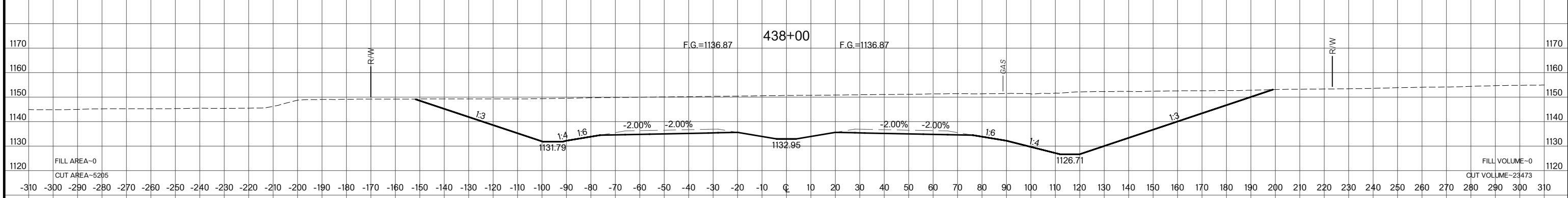
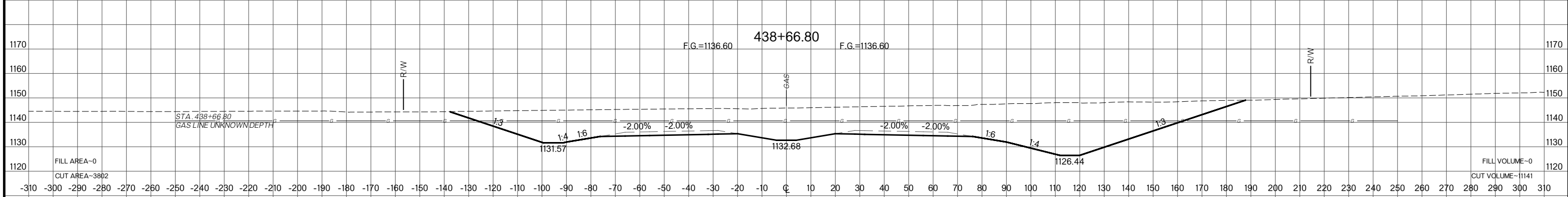
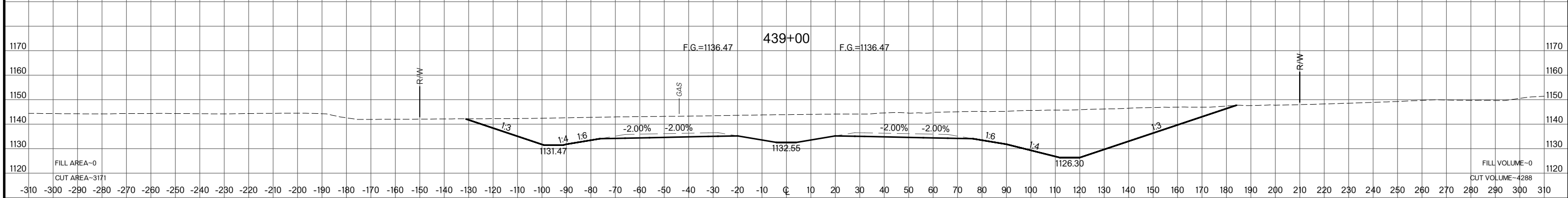
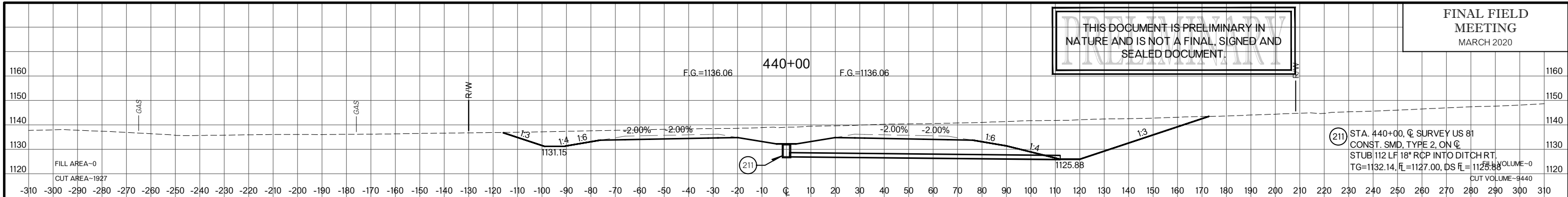
FINAL FIELD MEETING MARCH 2020



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

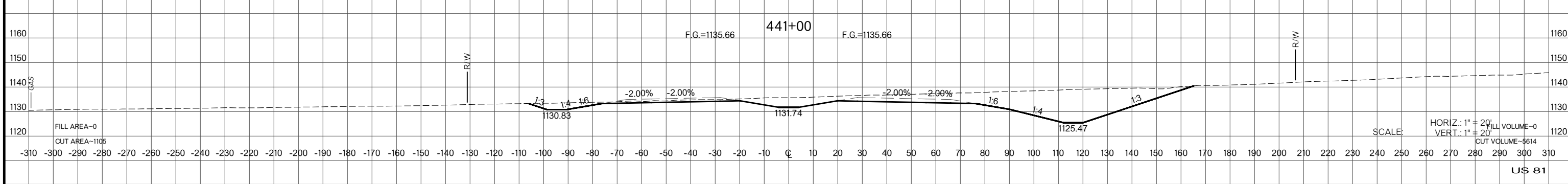
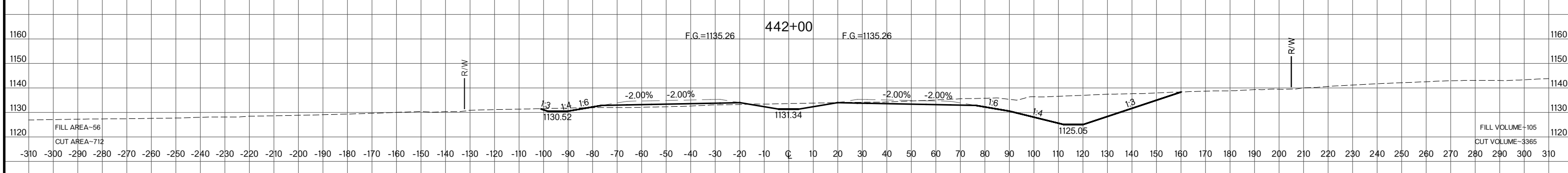
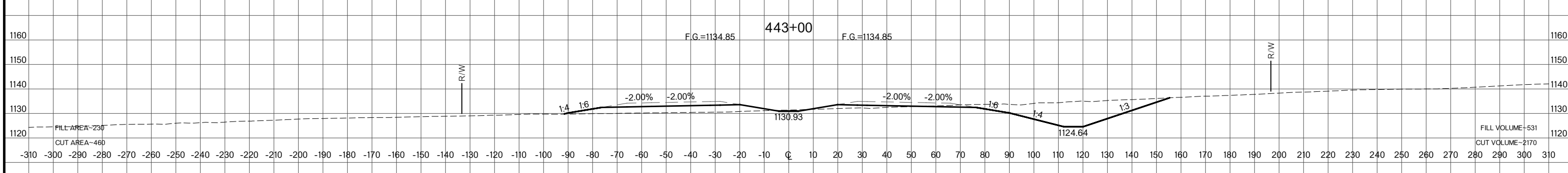
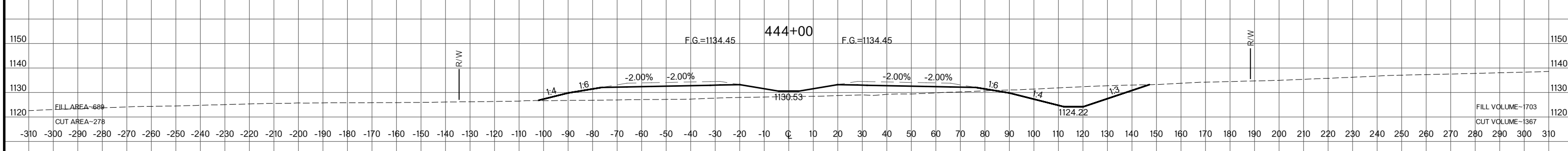
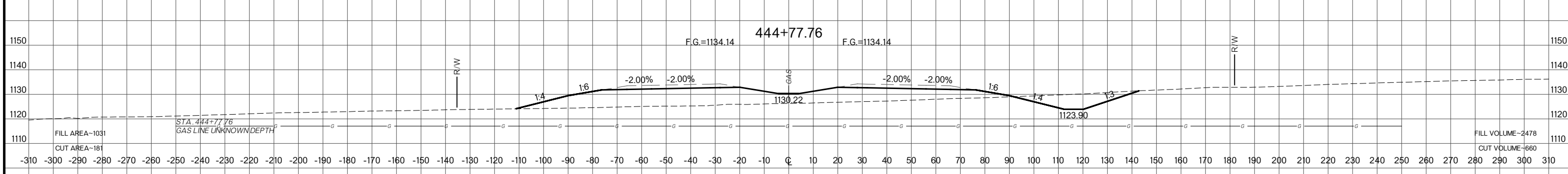
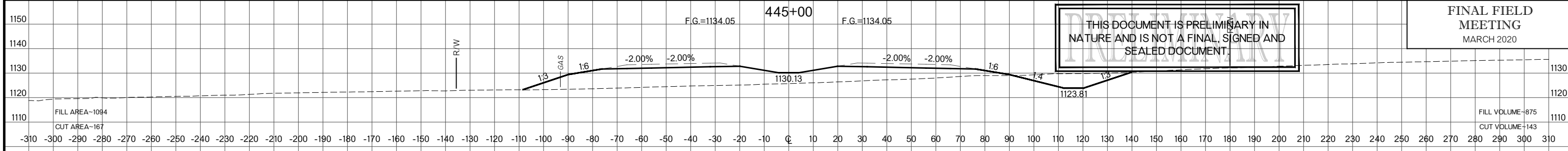
FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

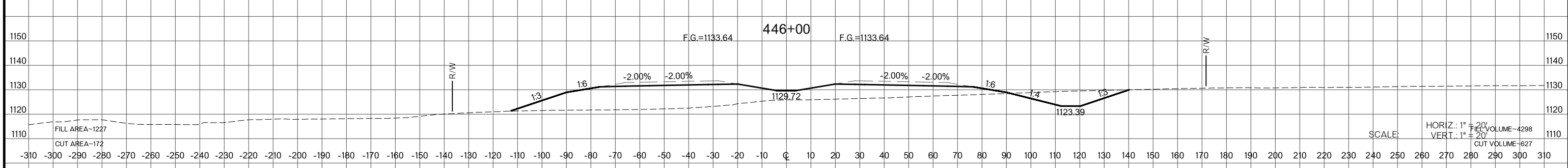
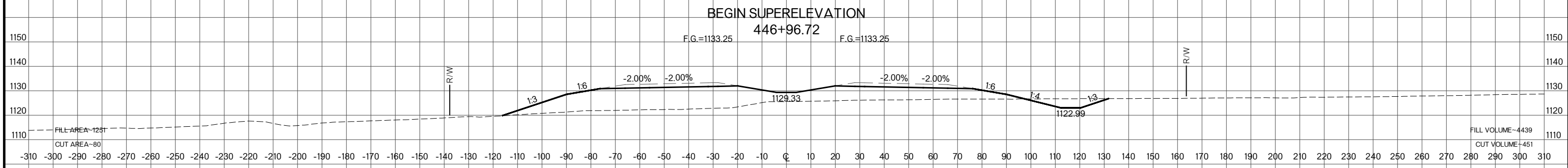
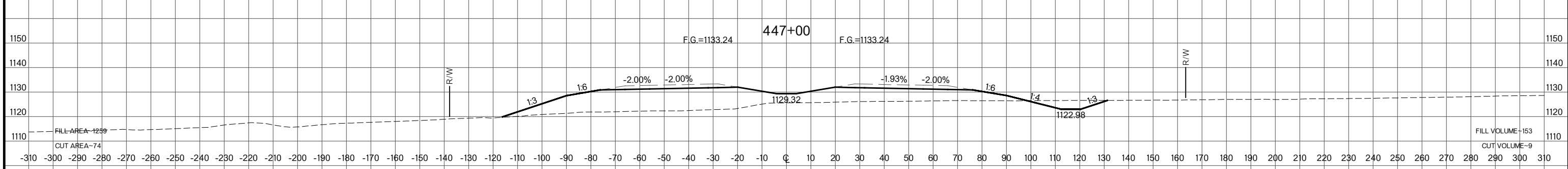
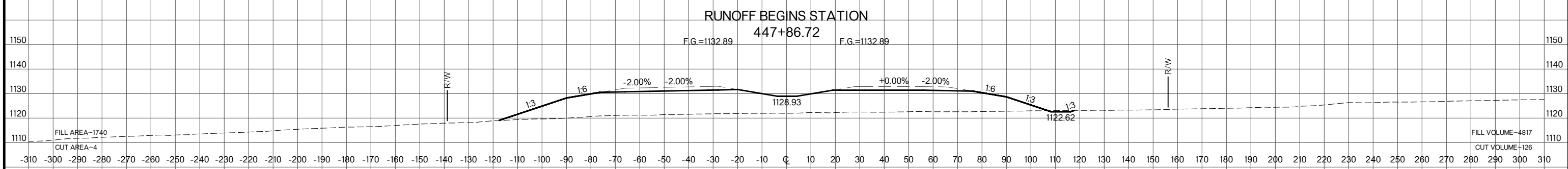
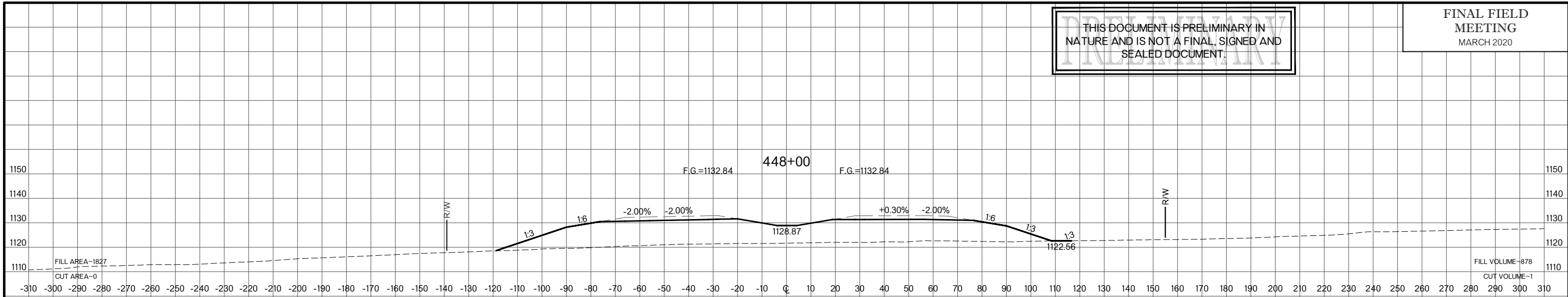


SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

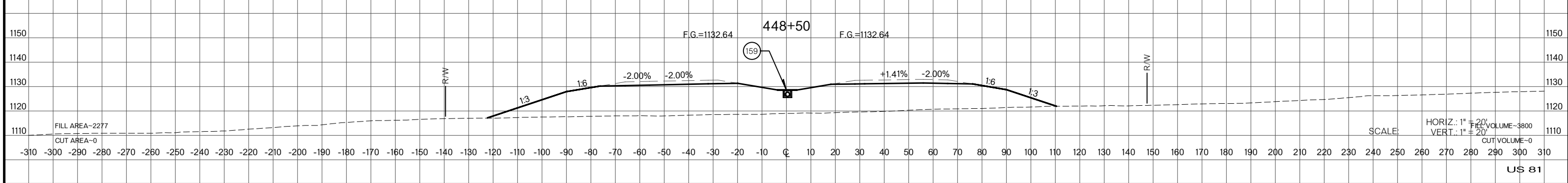
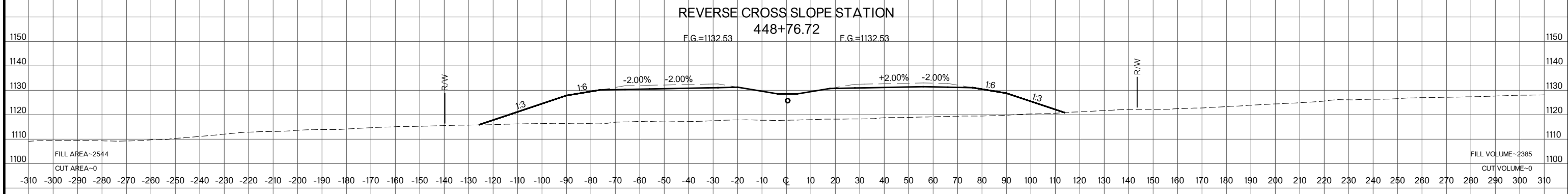
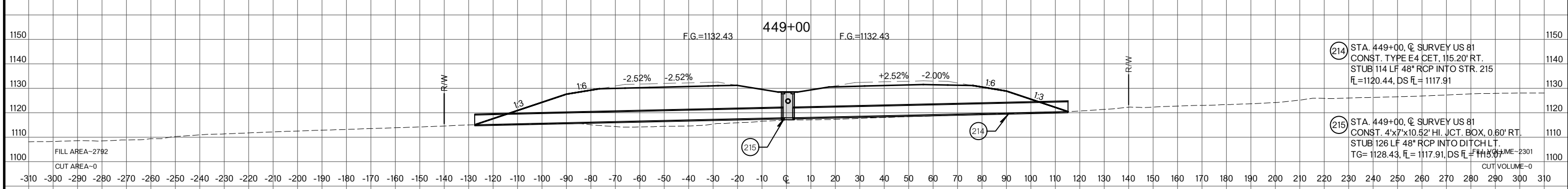
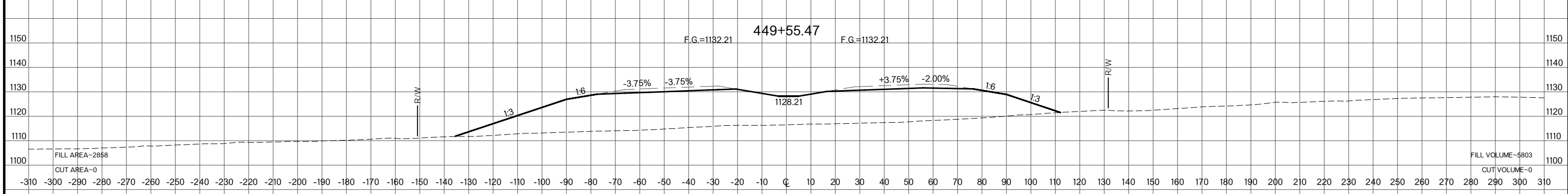
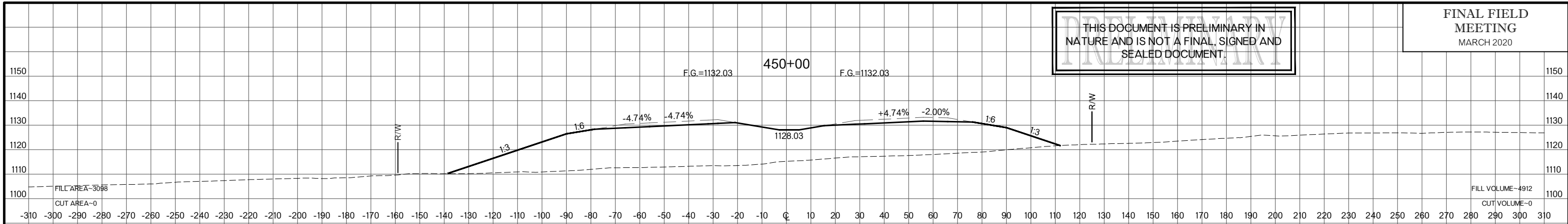


SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

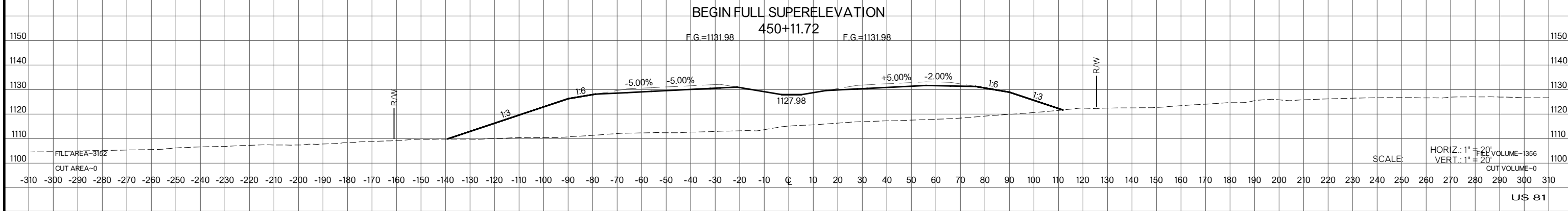
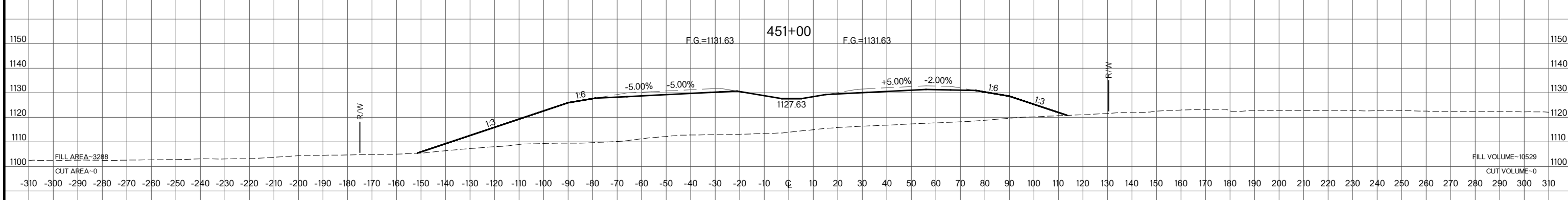
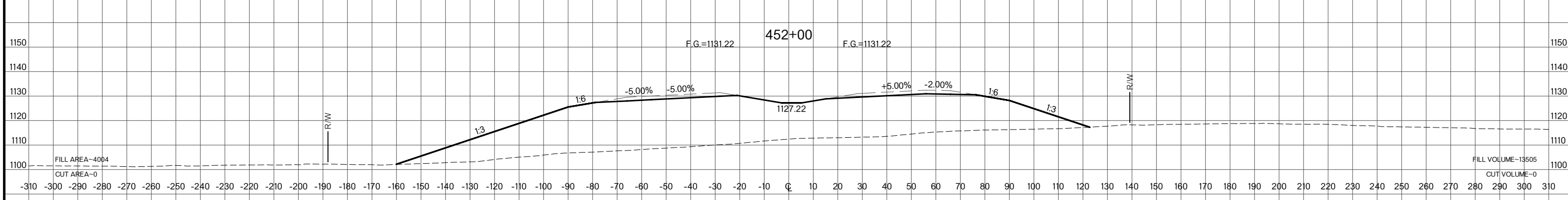
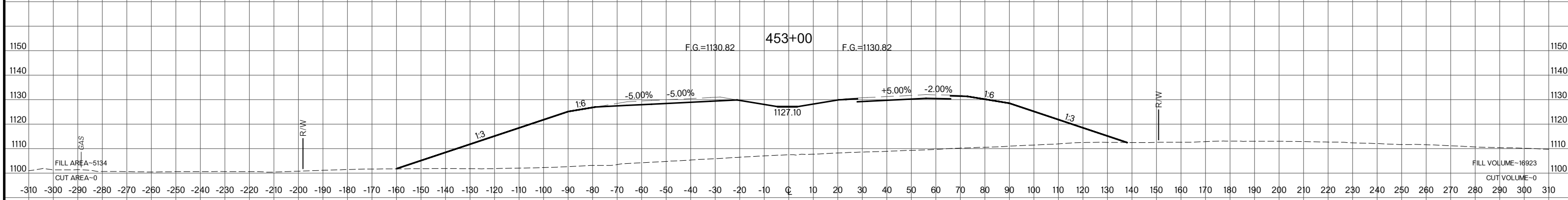
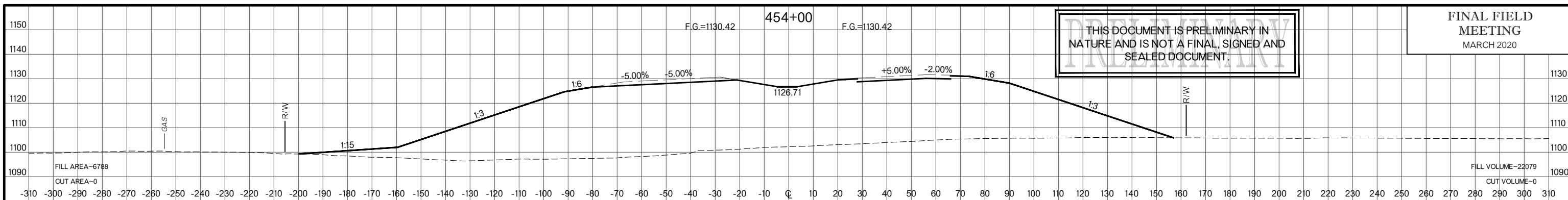
FINAL FIELD MEETING MARCH 2020



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

US 81 REALIGNMENT GRADY COUNTY

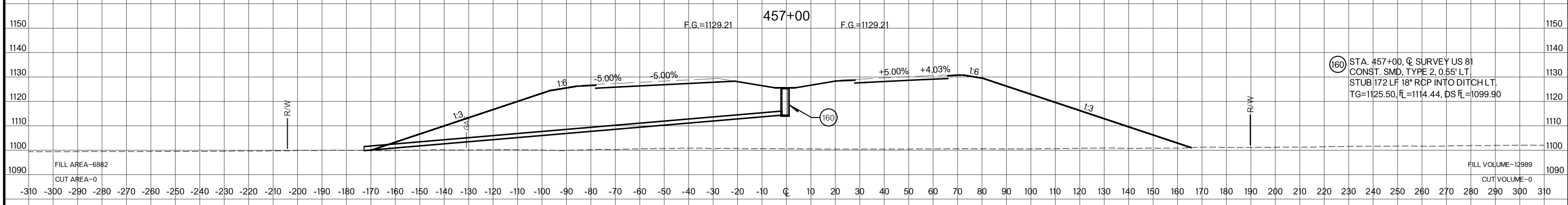
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



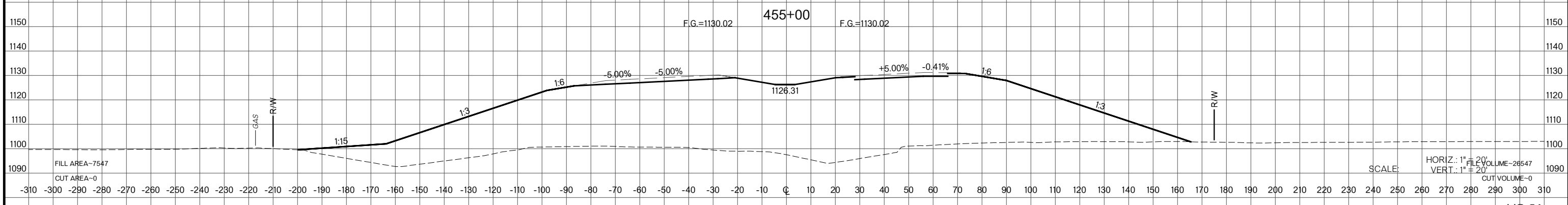
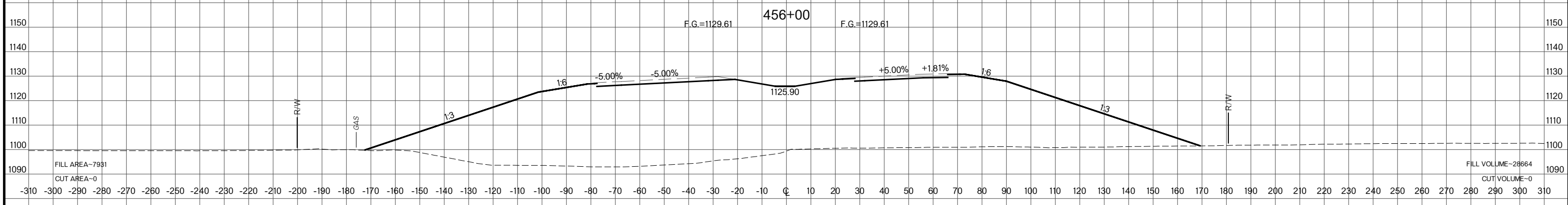
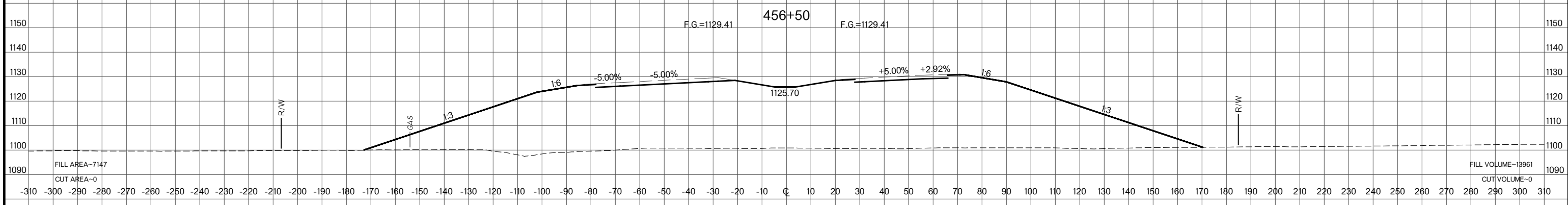
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



160 STA. 457+00, C SURVEY US 81
CONST. SMD, TYPE 2, 0.55' LT.
STUB 172 LF 18" RCP INTO DITCH LT.
TG=1125.50, FL=1114.44, DS FL=1099.90



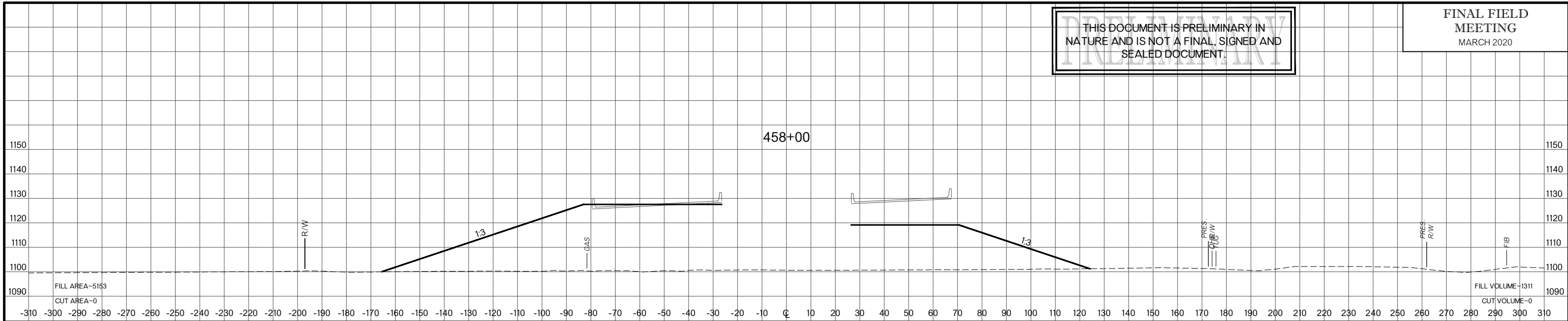
SCALE: HORIZ.: 1"=20'
VERT.: 1"=20'

US 81

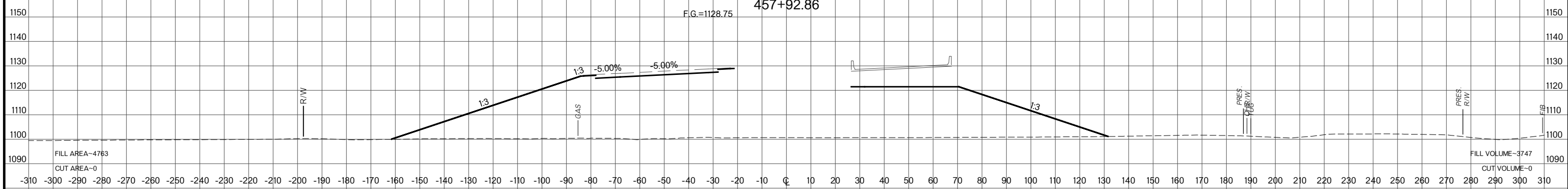
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

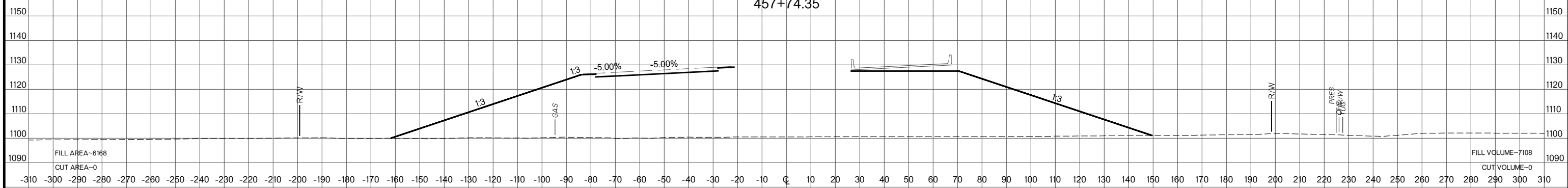
FINAL FIELD MEETING MARCH 2020



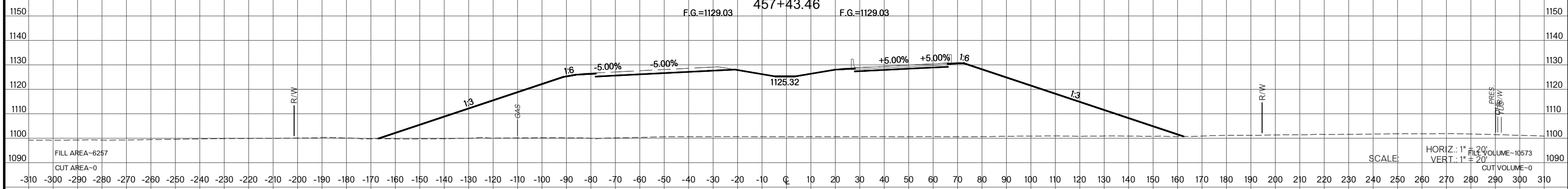
BEGIN APPROACH BRIDGE "Q"
457+92.86
F.G.=1128.75



LAST FULL STATION BRIDGE "R"
457+74.35



BEGIN APPROACH BRIDGE "R"
457+43.46
F.G.=1129.03 F.G.=1129.03



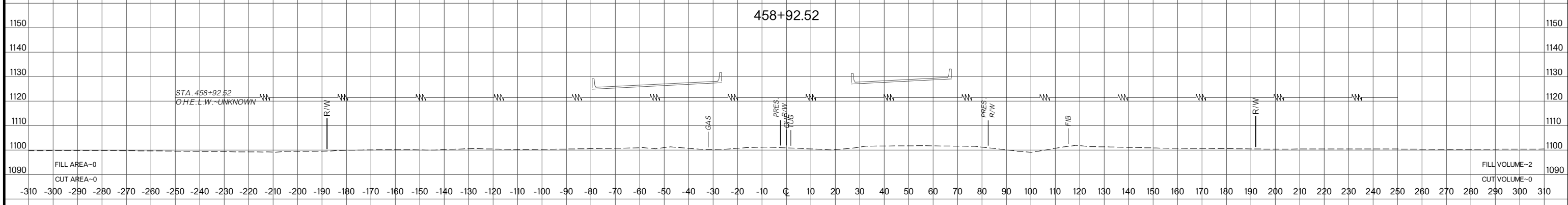
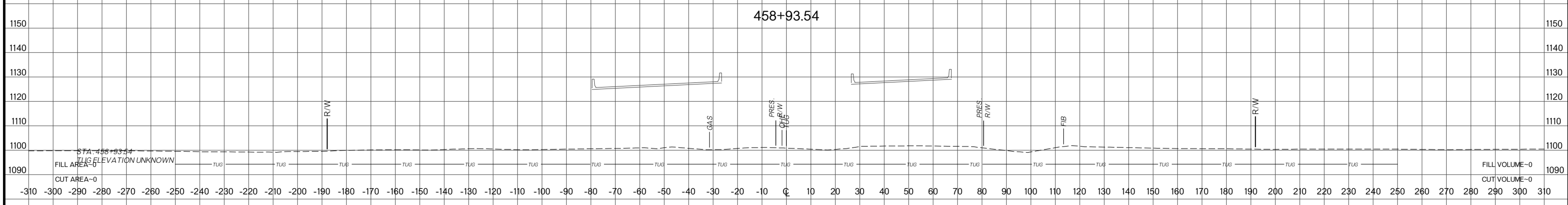
SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

US 81

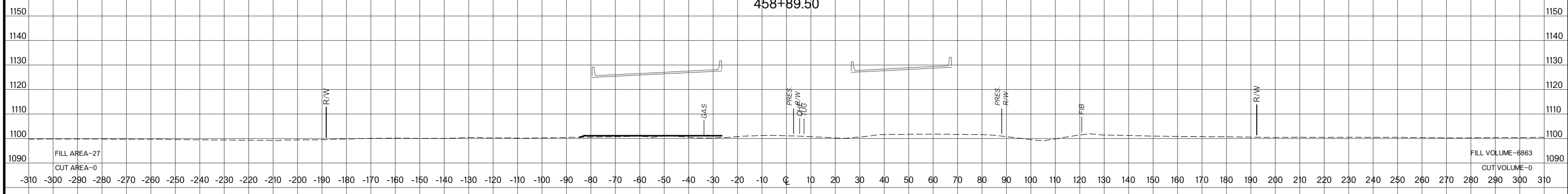
US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

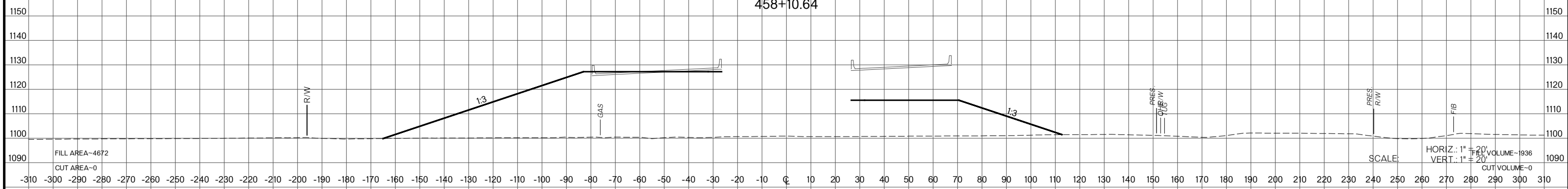
FINAL FIELD MEETING MARCH 2020



BEGIN EARTHWORK EXCEPTION
458+89.50

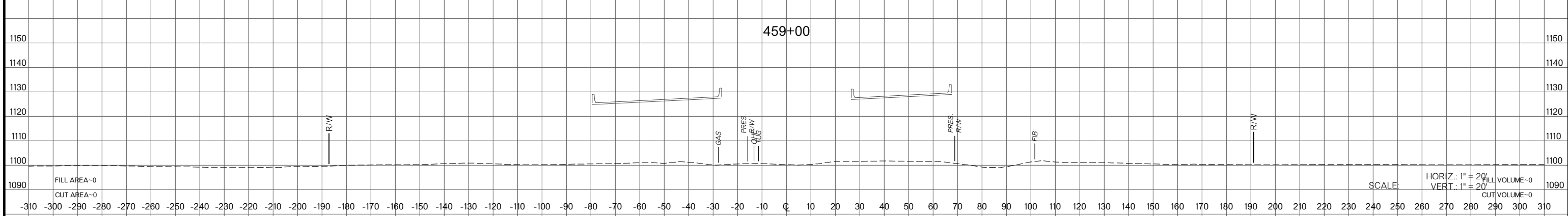
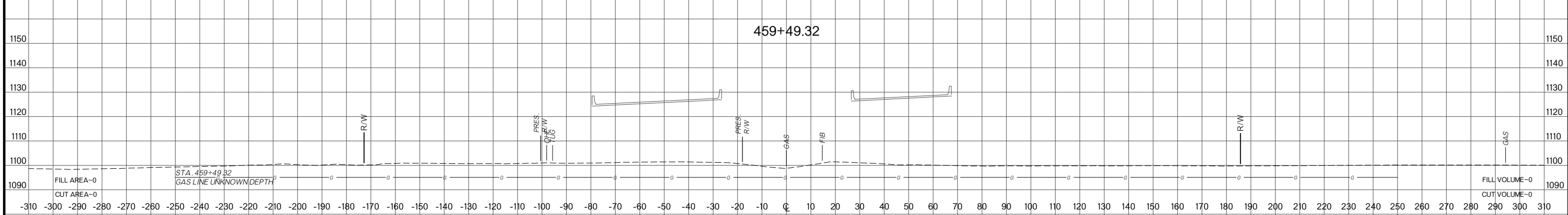
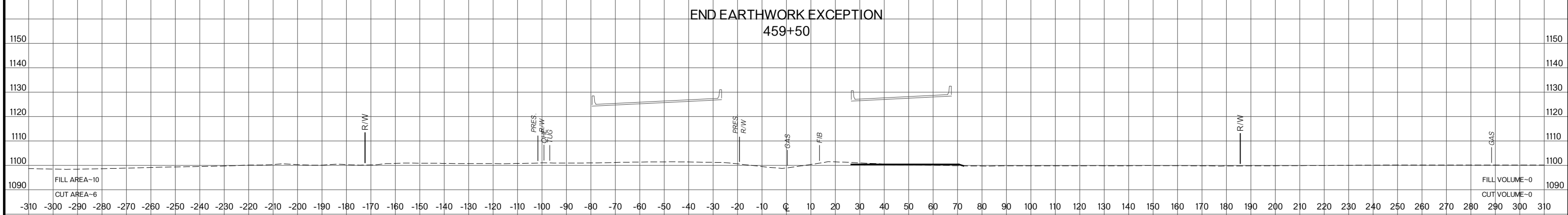
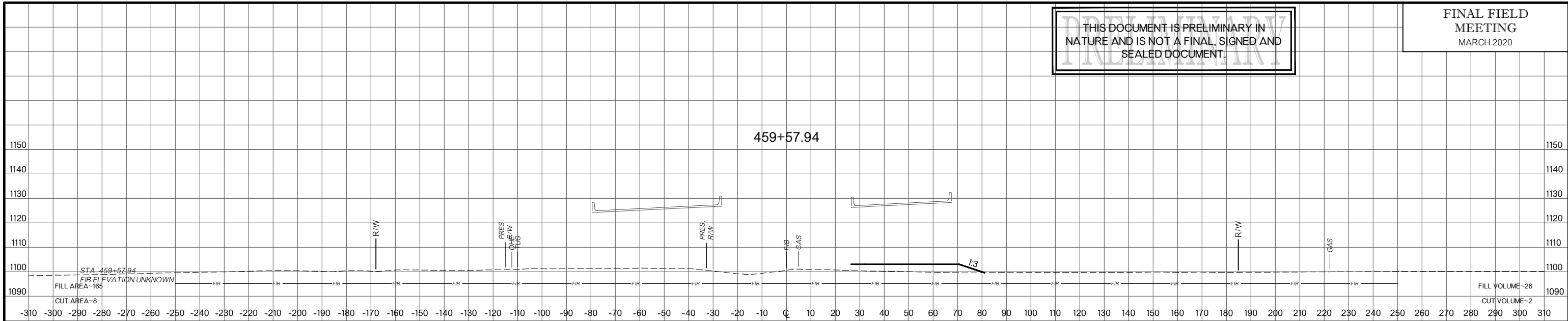


LAST FULL STATION BRIDGE "Q"
458+10.64



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

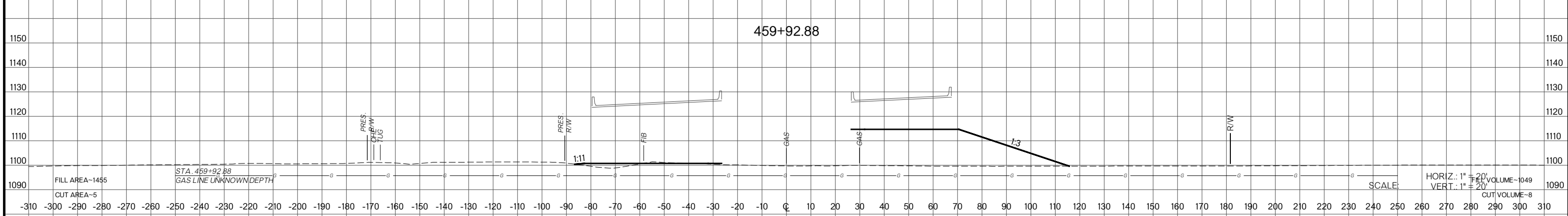
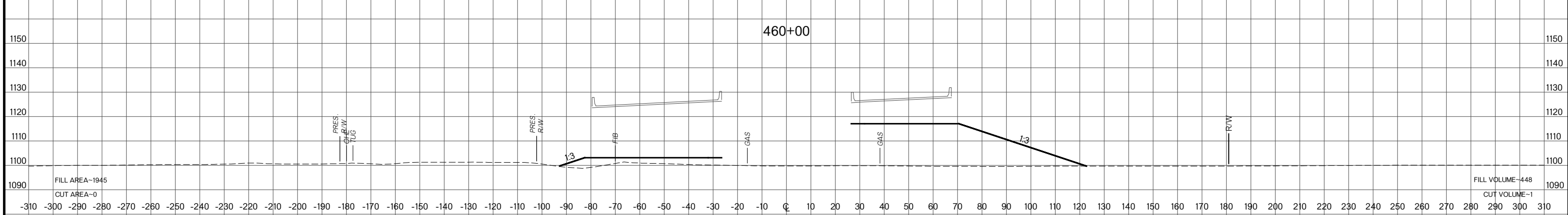
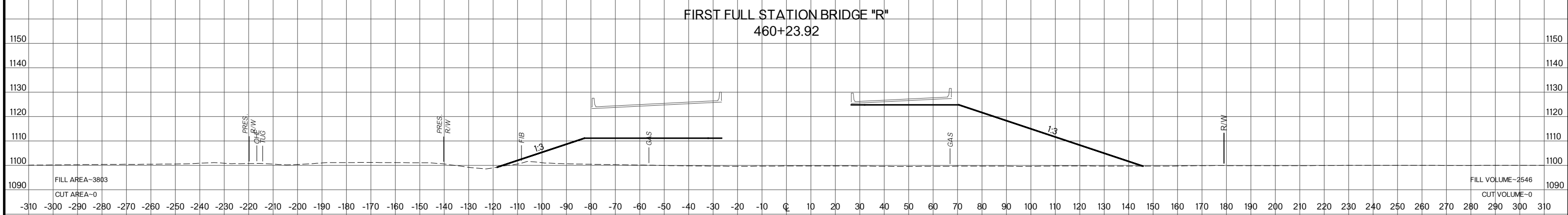
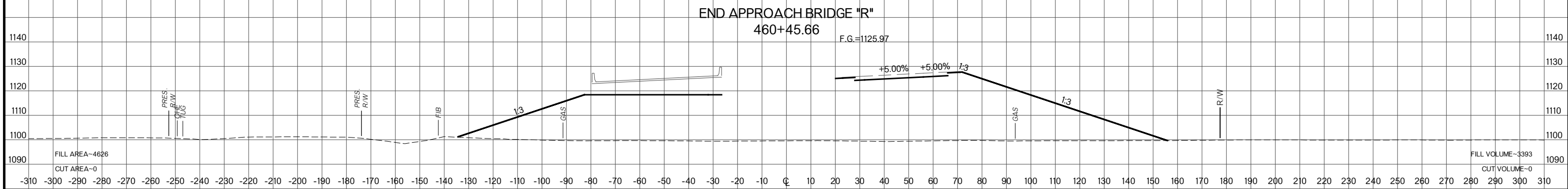
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020

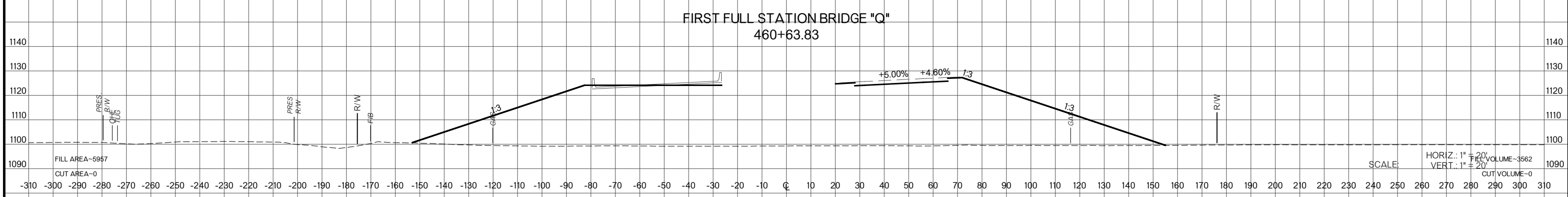
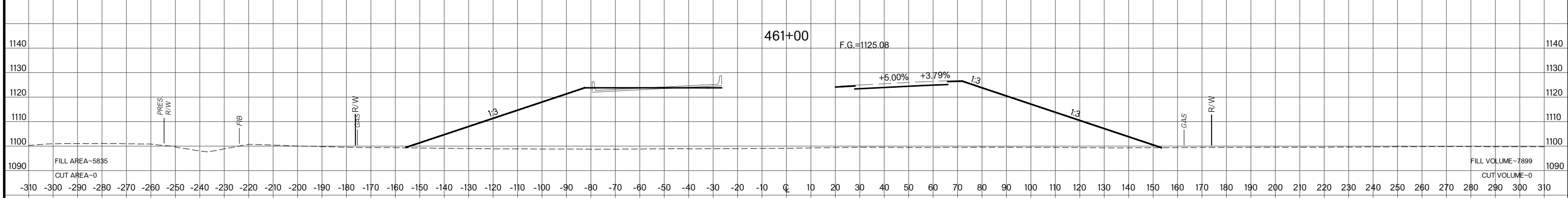
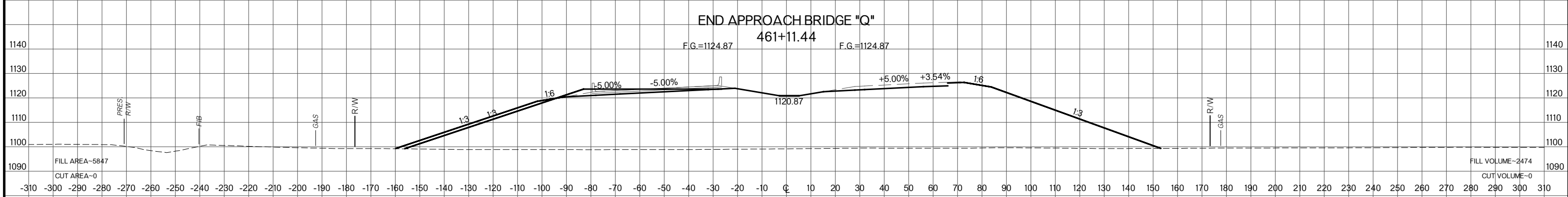
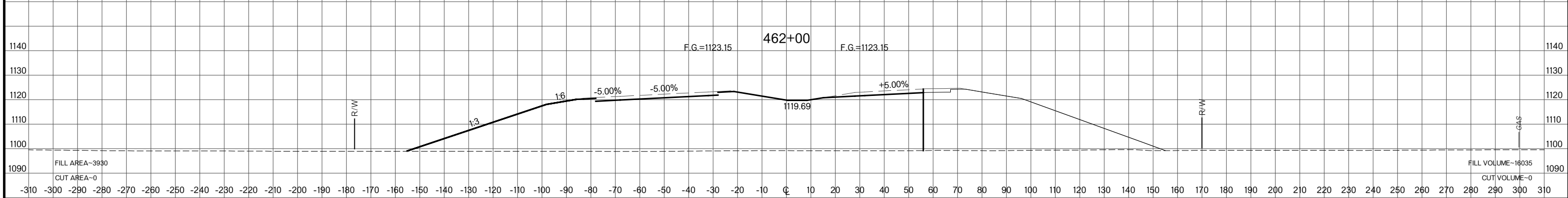
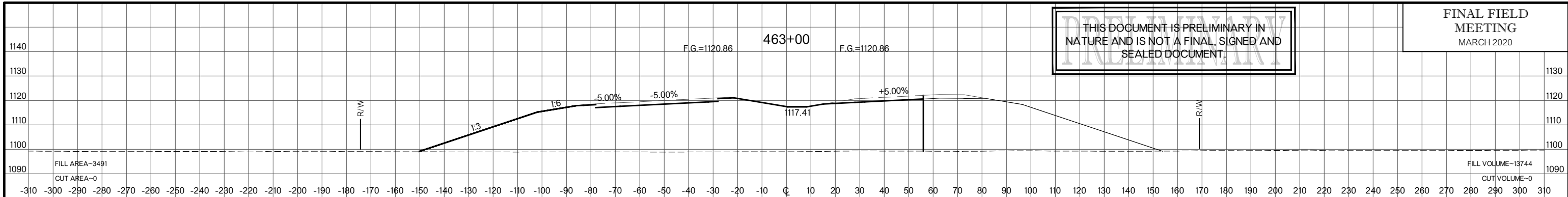


SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020

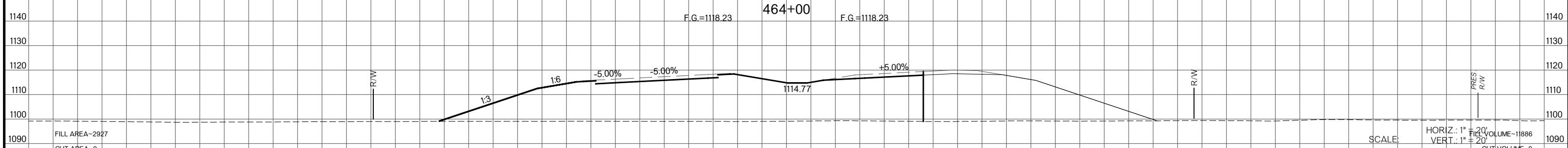
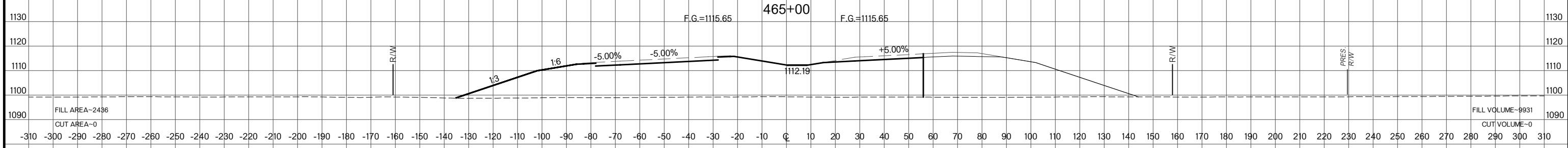
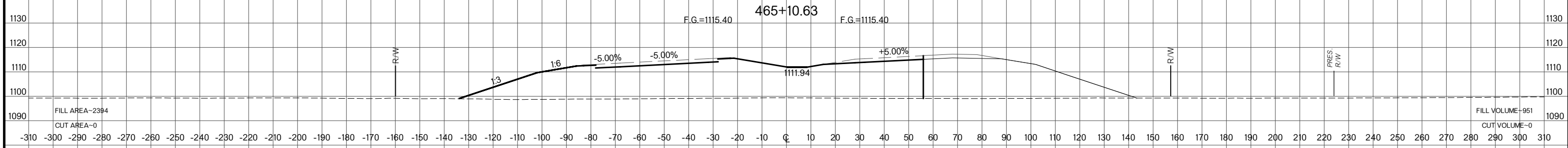
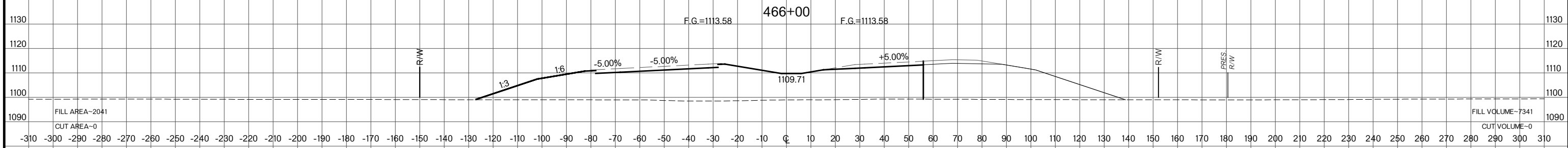
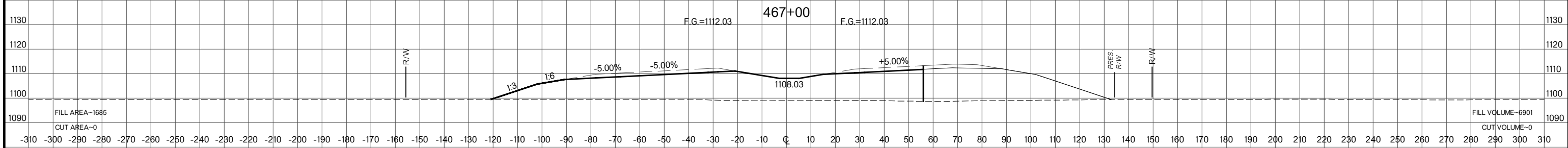


SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



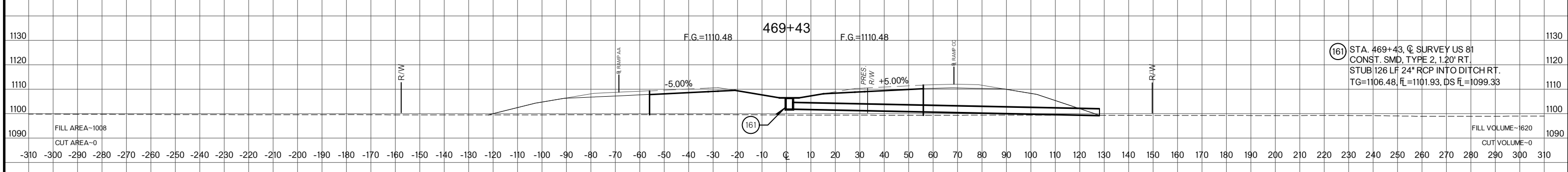
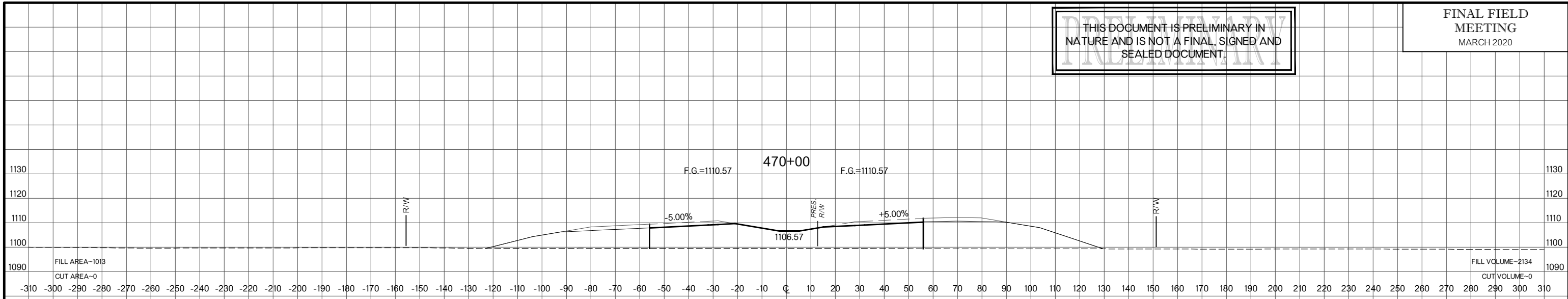
SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

US 81

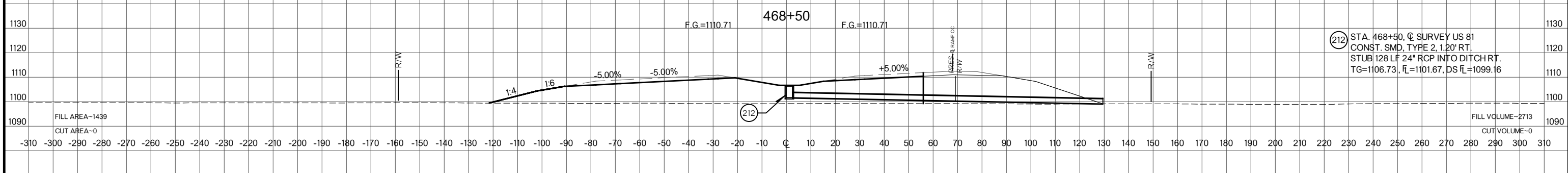
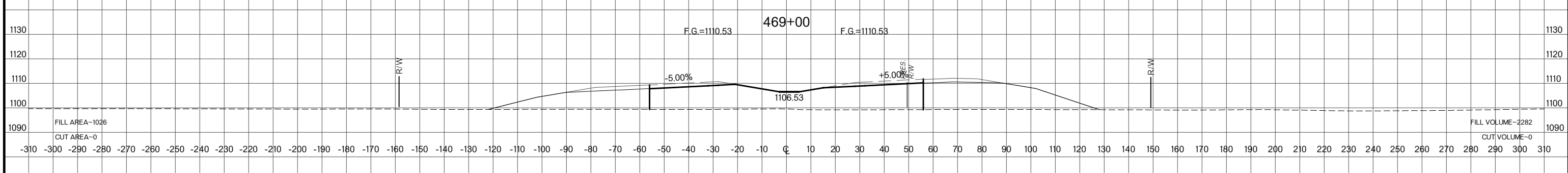
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

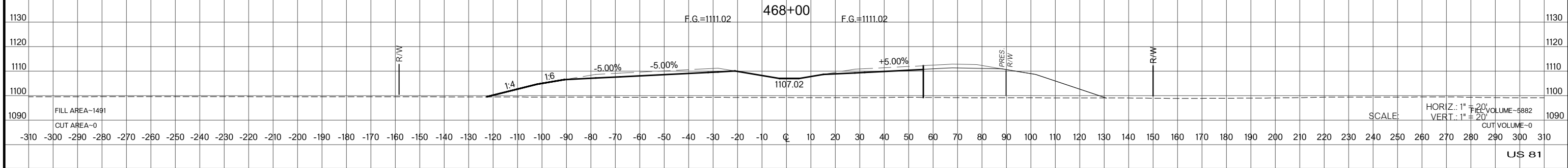
FINAL FIELD MEETING
MARCH 2020



(161) STA. 469+43, C SURVEY US 81
CONST. SMD. TYPE 2, 1.20' RT.
STUB 126 LF 24" RCP INTO DITCH RT.
TG=1106.48, FL=1101.93, DS FL=1099.33



(212) STA. 468+50, C SURVEY US 81
CONST. SMD. TYPE 2, 1.20' RT.
STUB 128 LF 24" RCP INTO DITCH RT.
TG=1106.73, FL=1101.67, DS FL=1099.16



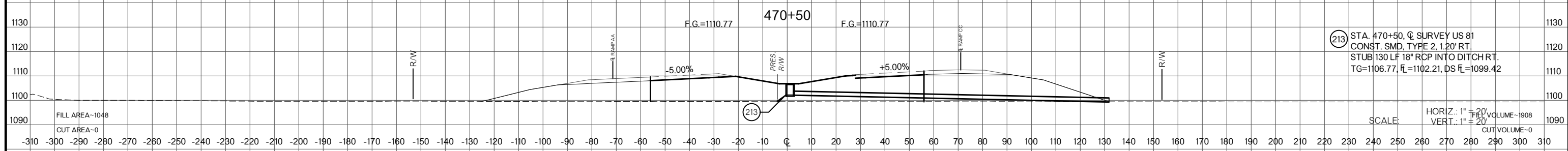
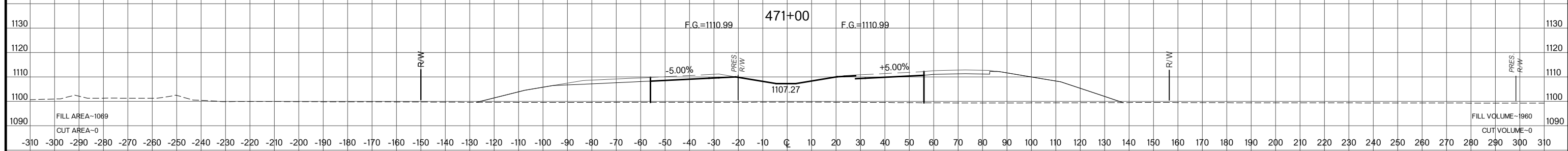
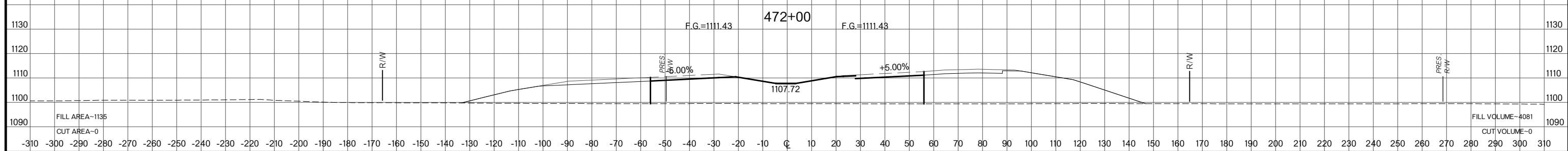
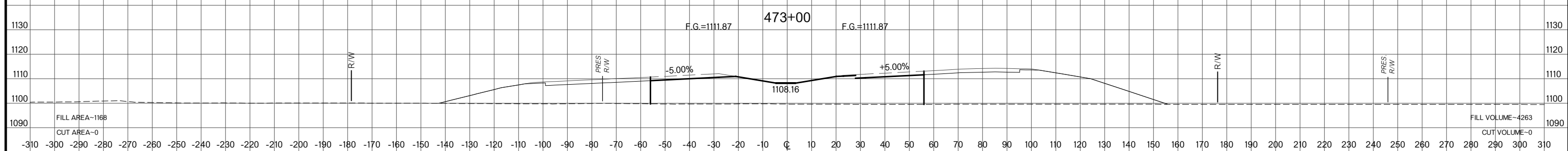
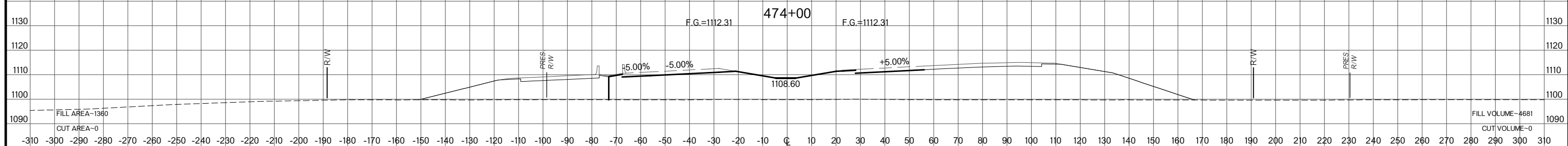
SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



(213) STA. 470+50, C SURVEY US 81
CONST. SMD. TYPE 2, 1.20' RT.
STUB 130 LF 18" RCP INTO DITCH RT.
TG=1106.77, FL=1102.21, DS FL=1099.42

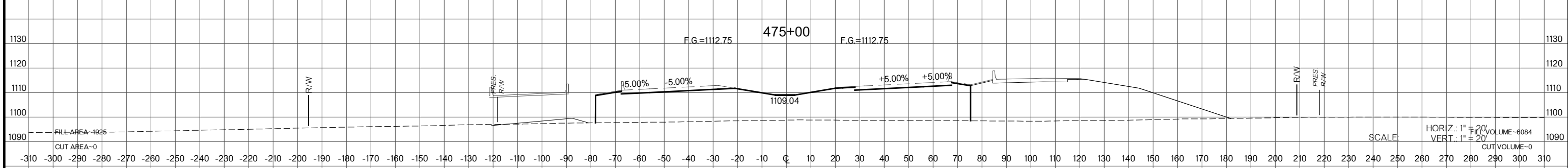
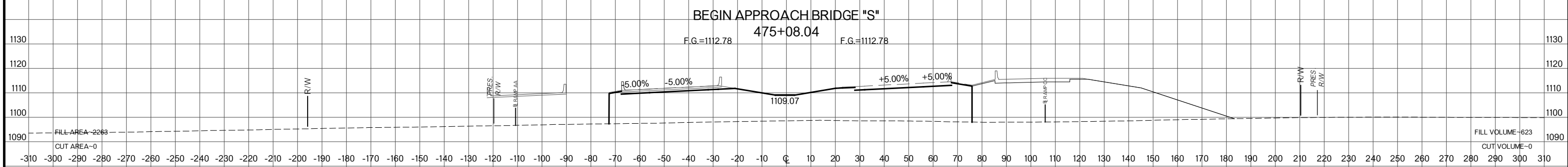
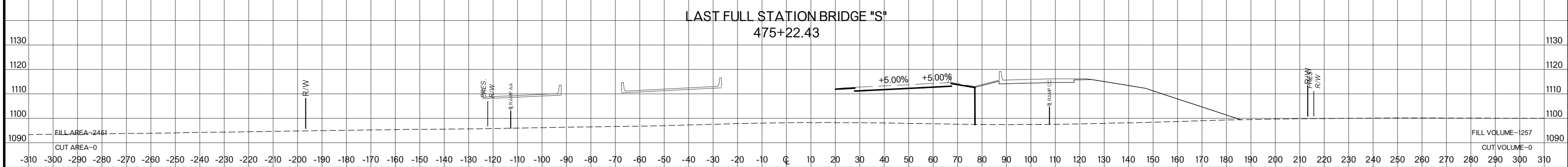
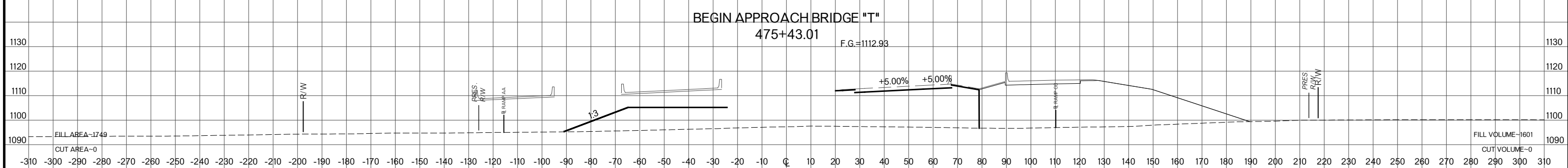
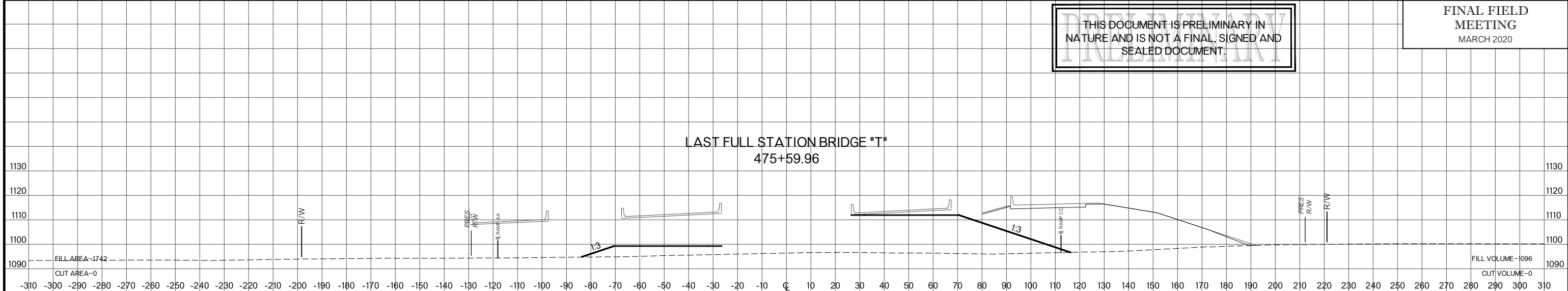
SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

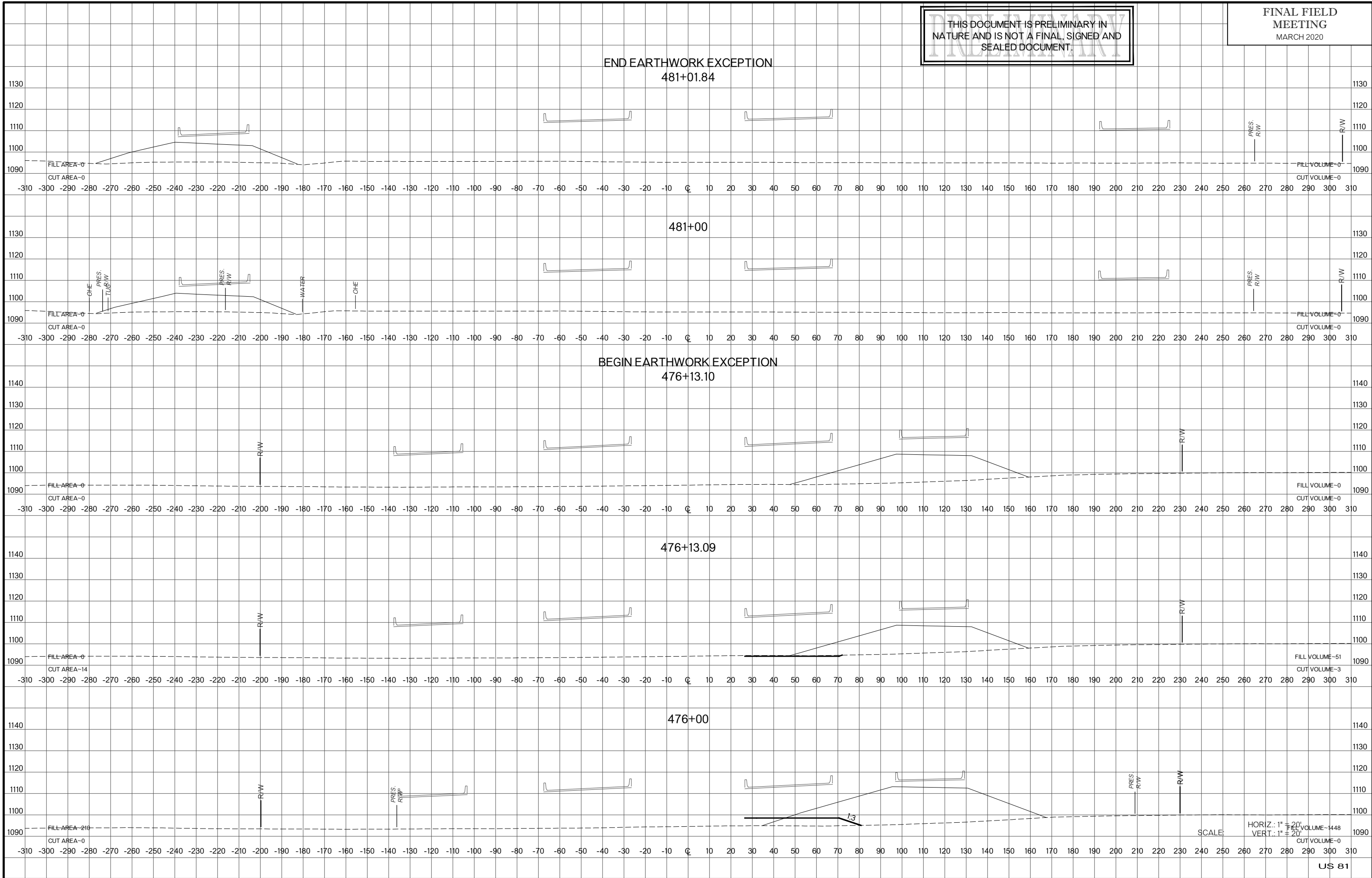
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

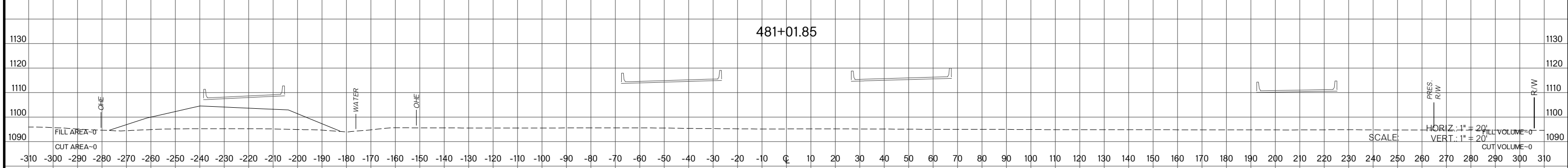
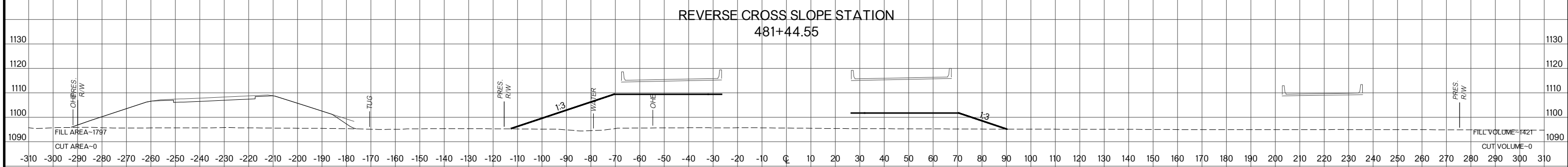
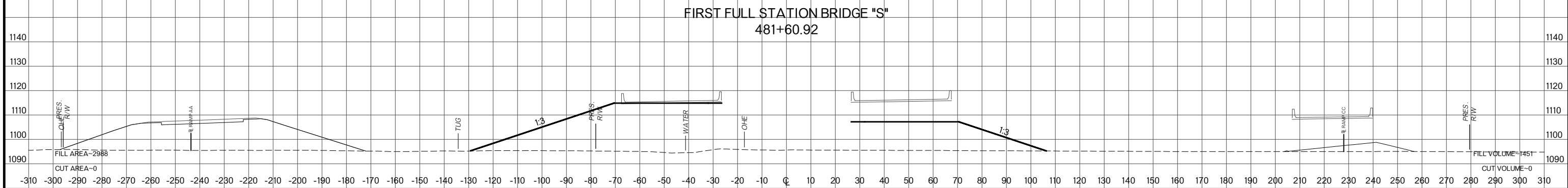
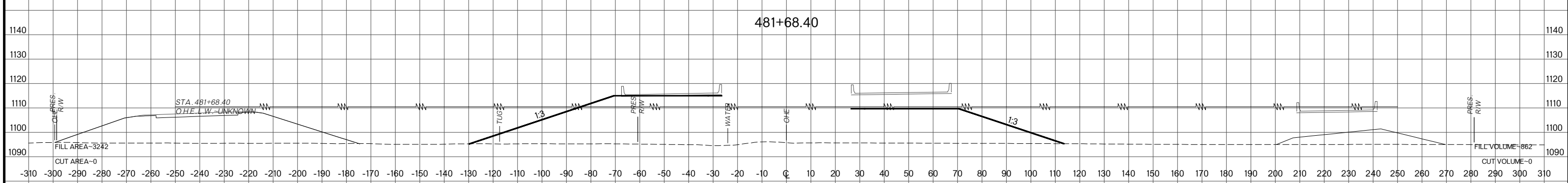
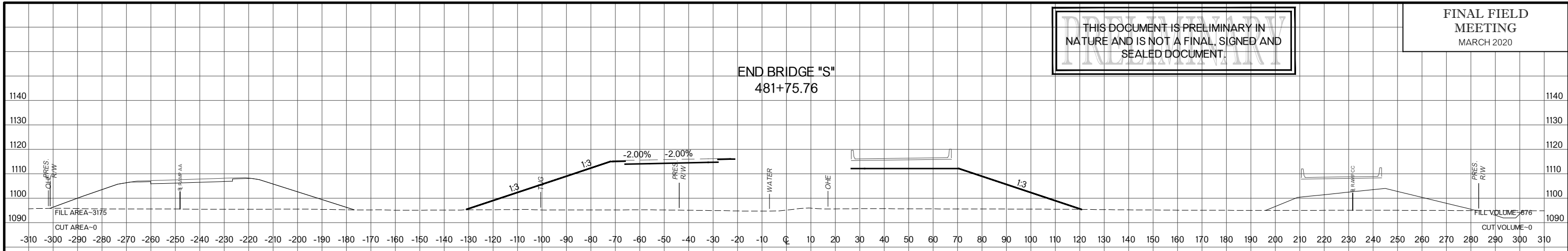
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



US 81 REALIGNMENT GRADY COUNTY

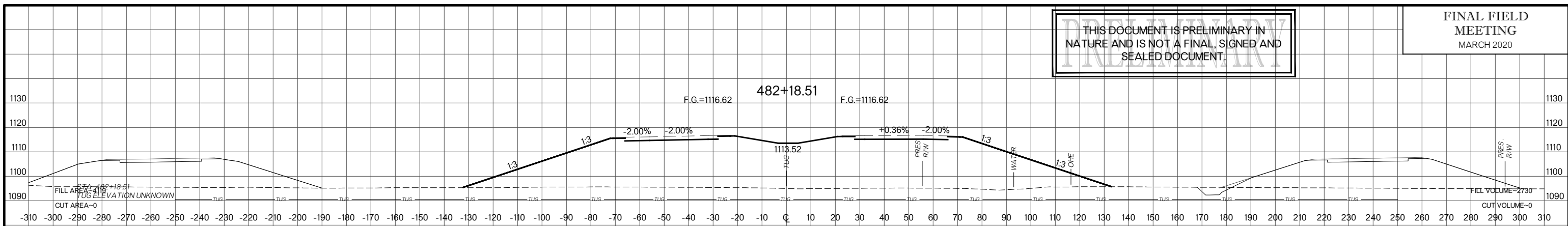
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



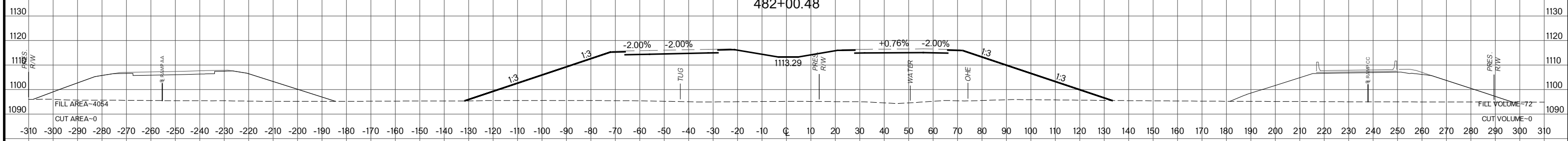
SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

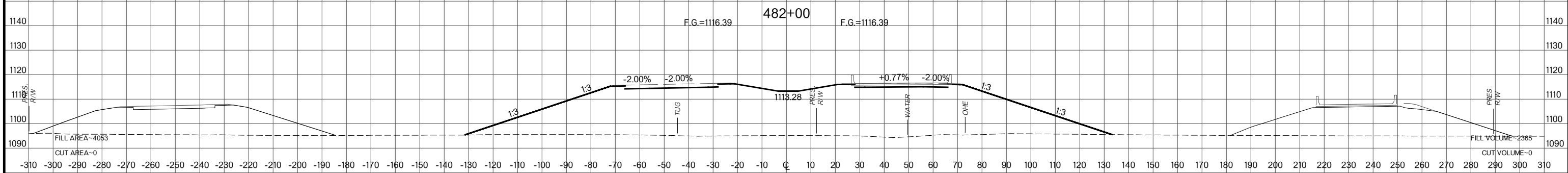
FINAL FIELD MEETING
MARCH 2020



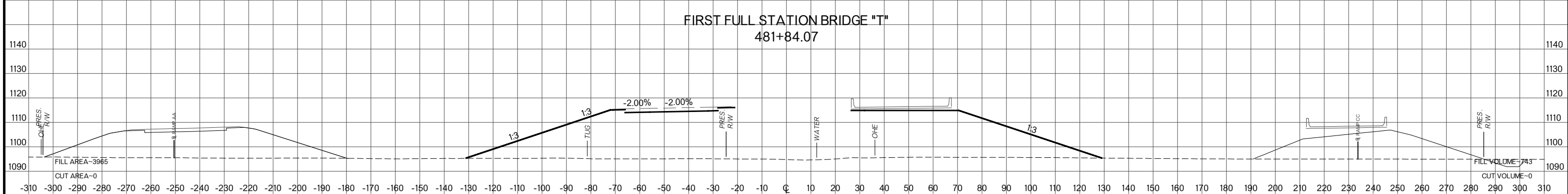
END BRIDGE "T" APPROACH
482+00.48



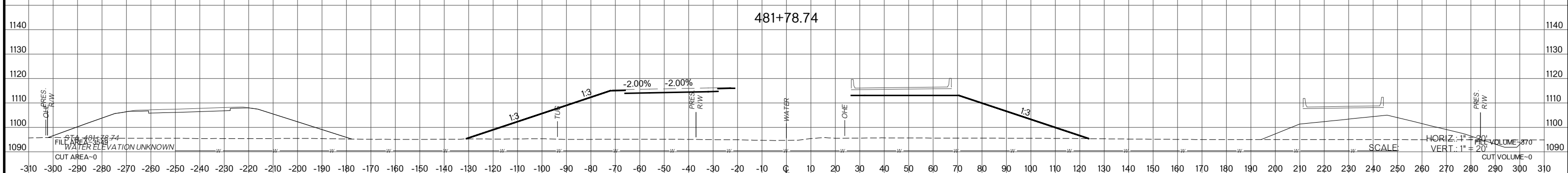
482+00
F.G.=1116.39



FIRST FULL STATION BRIDGE "T"
481+84.07

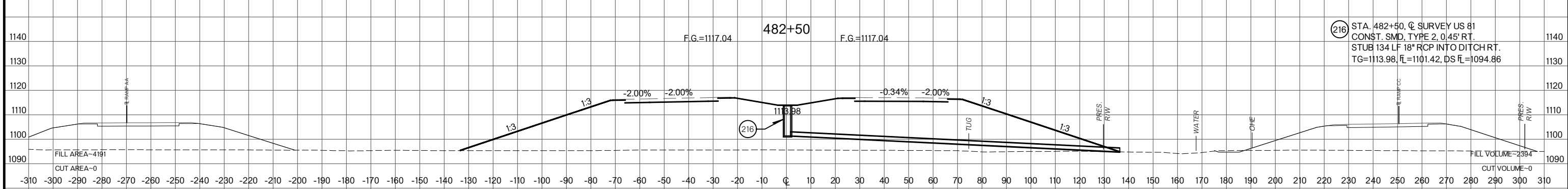
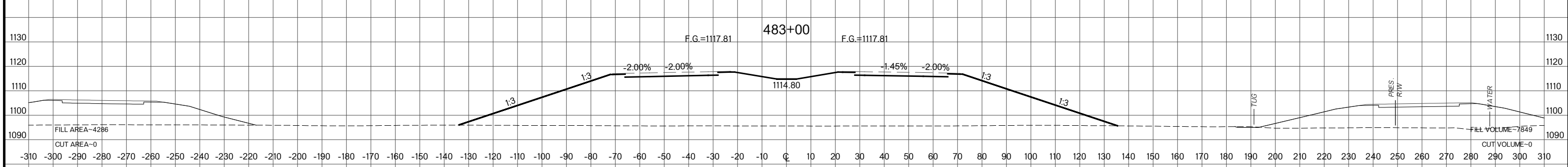
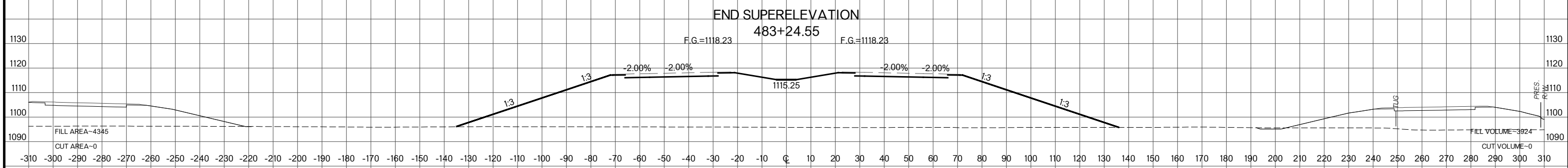
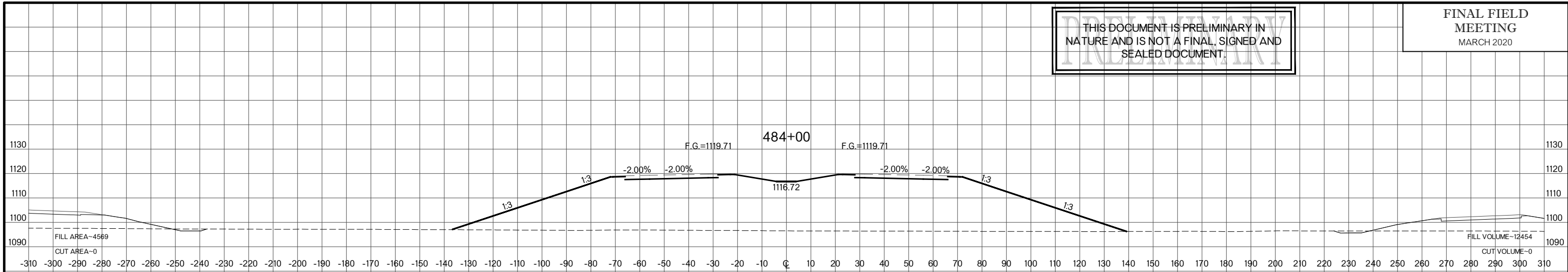


481+78.74

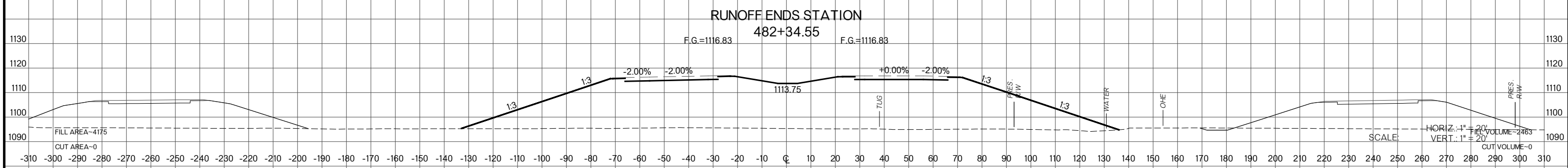


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



(216) STA. 482+50, C. SURVEY US 81 CONST. SMD, TYPE 2, 0.45' RT. STUB 134 LF 18" RCP INTO DITCH RT. TG=1113.98, RL=1101.42, DS RL=1094.86



SCALE: HORIZ.: 1" = 20' VERT.: 1" = 20'

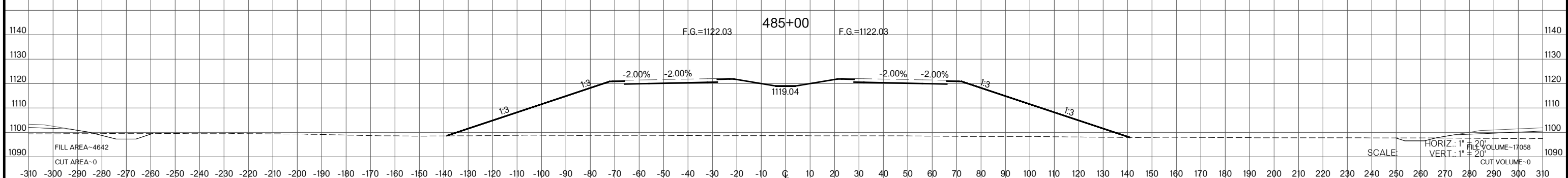
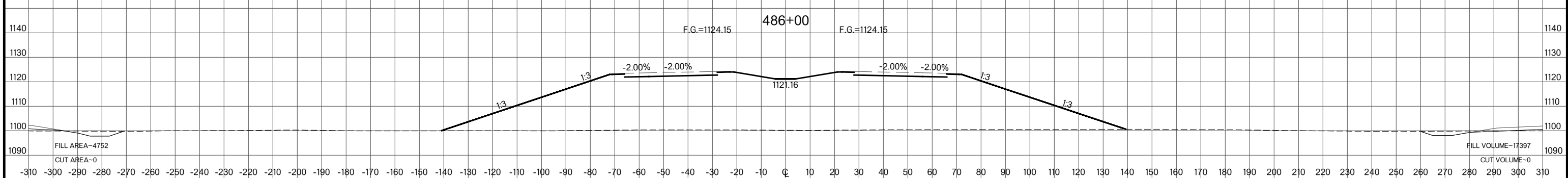
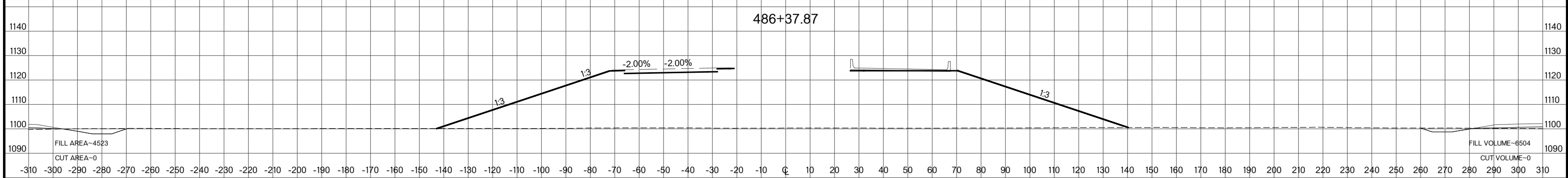
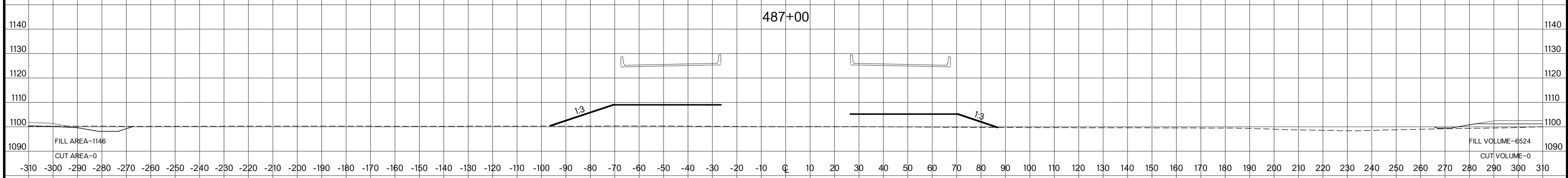
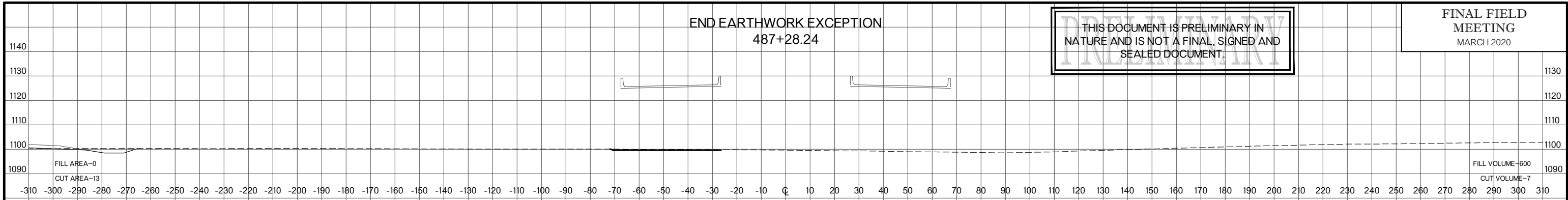
US 81

US 81 REALIGNMENT GRADY COUNTY

END EARTHWORK EXCEPTION
487+28.24

THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

FINAL FIELD
MEETING
MARCH 2020



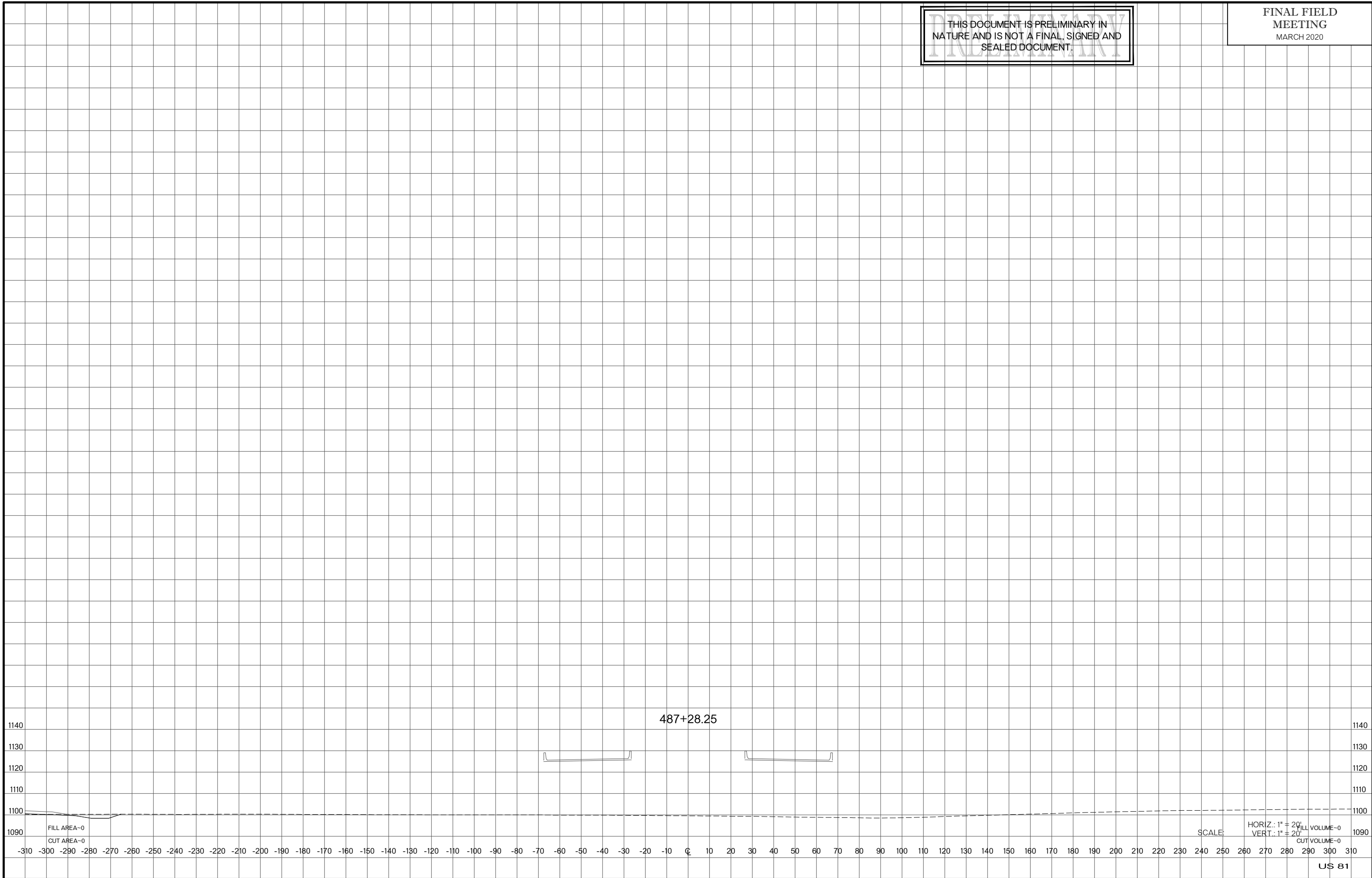
SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

US 81

US 81 REALIGNMENT
GRADY COUNTY

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

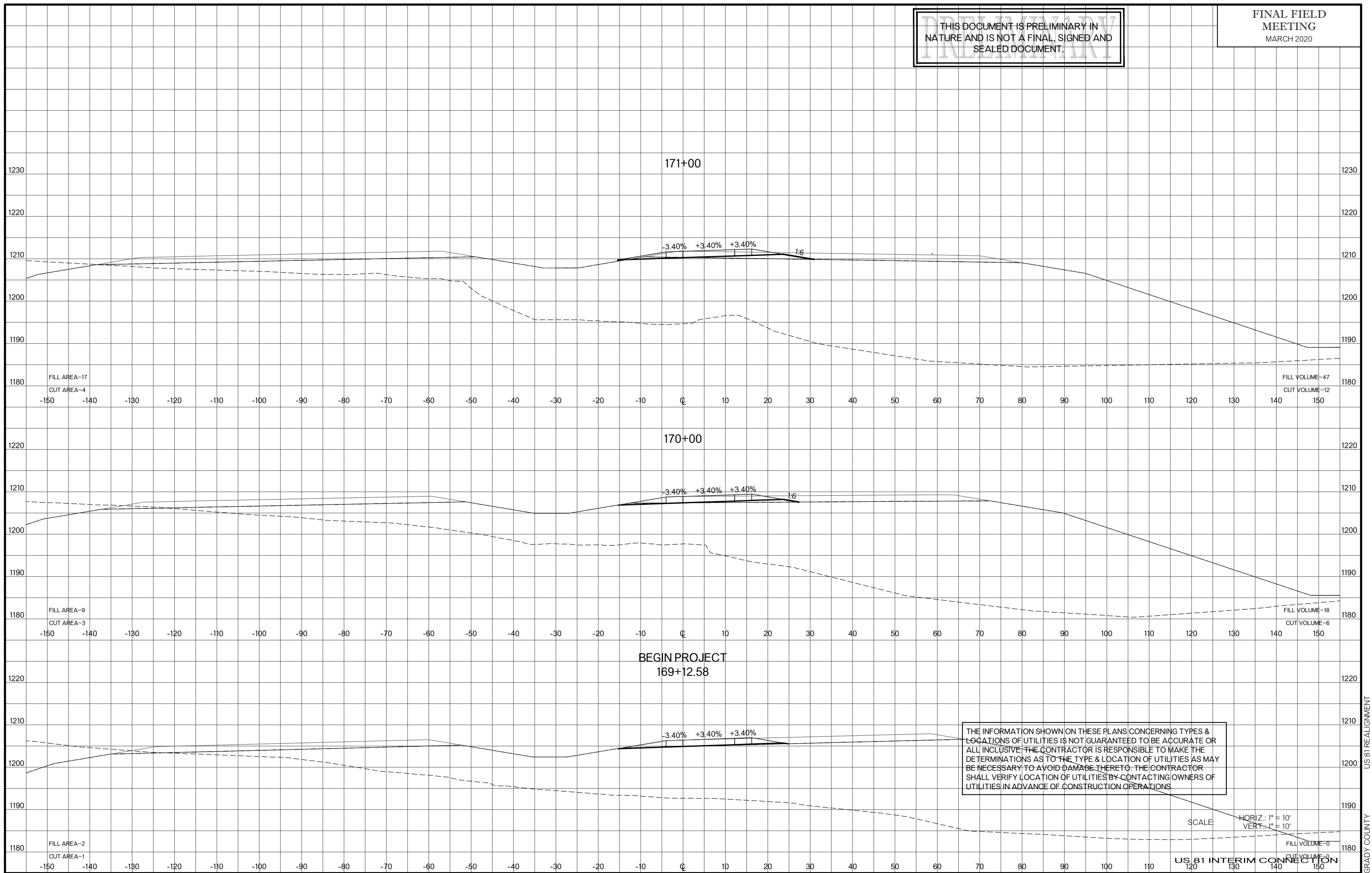


1140 1140
1130 1130
1120 1120
1110 1110
1100 1100
1090 1090
FILL AREA-0
CUT AREA-0
-310 -300 -290 -280 -270 -260 -250 -240 -230 -220 -210 -200 -190 -180 -170 -160 -150 -140 -130 -120 -110 -100 -90 -80 -70 -60 -50 -40 -30 -20 -10 0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150 160 170 180 190 200 210 220 230 240 250 260 270 280 290 300 310
487+28.25
SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'
FILL VOLUME-0
CUT VOLUME-0
US 81

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

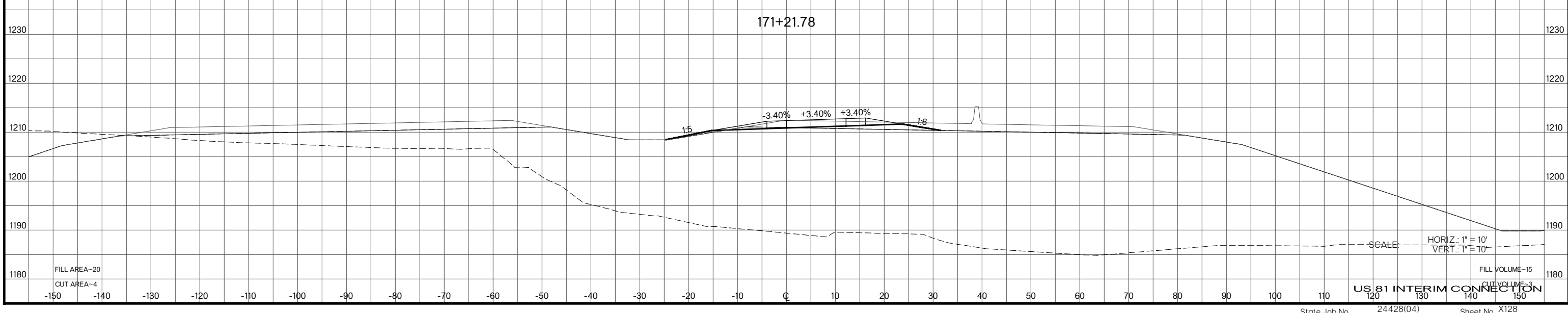
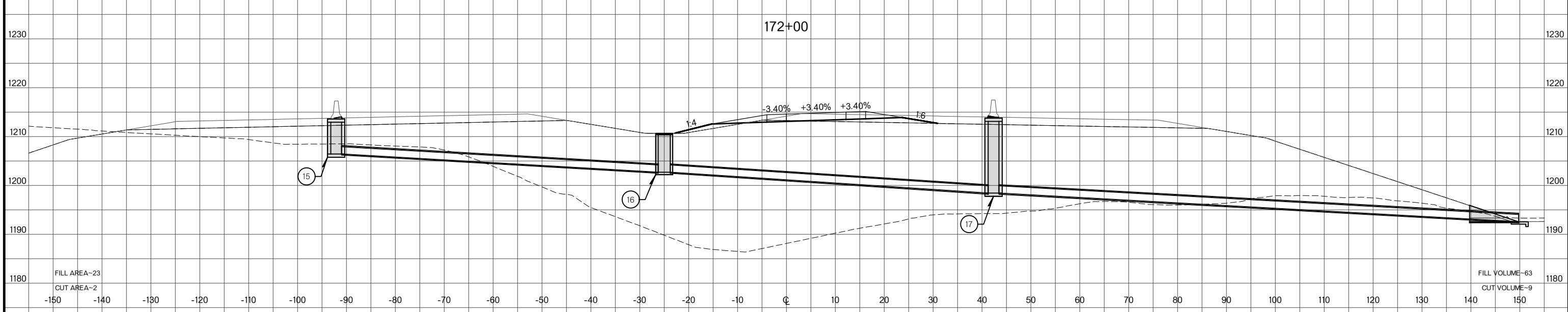
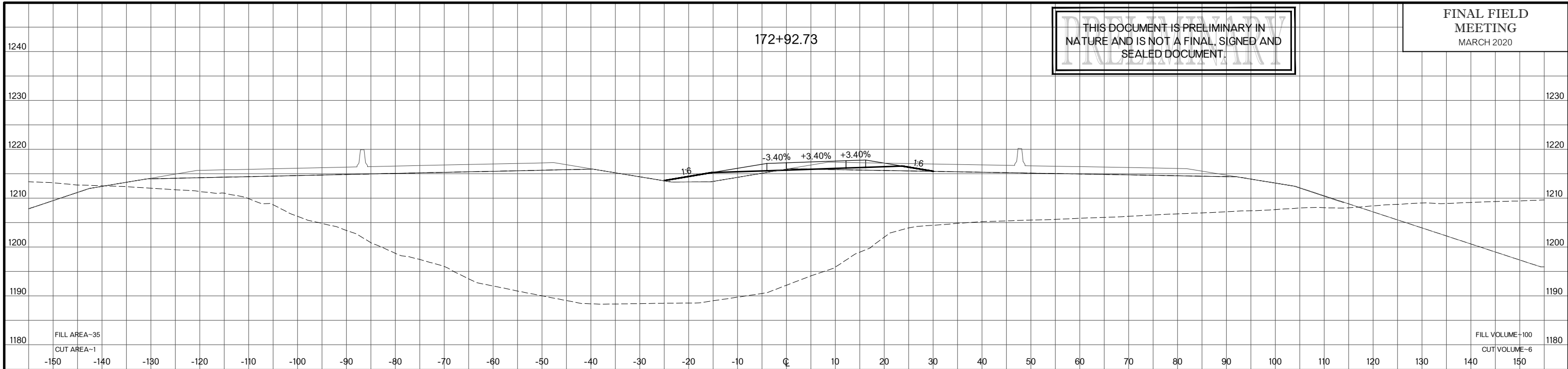
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 INTERIM CONNECTION

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

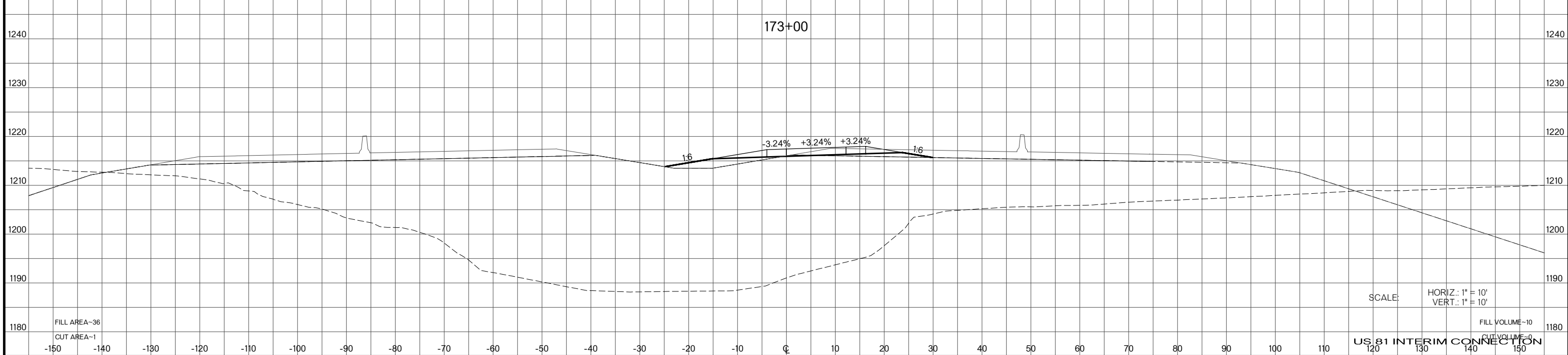
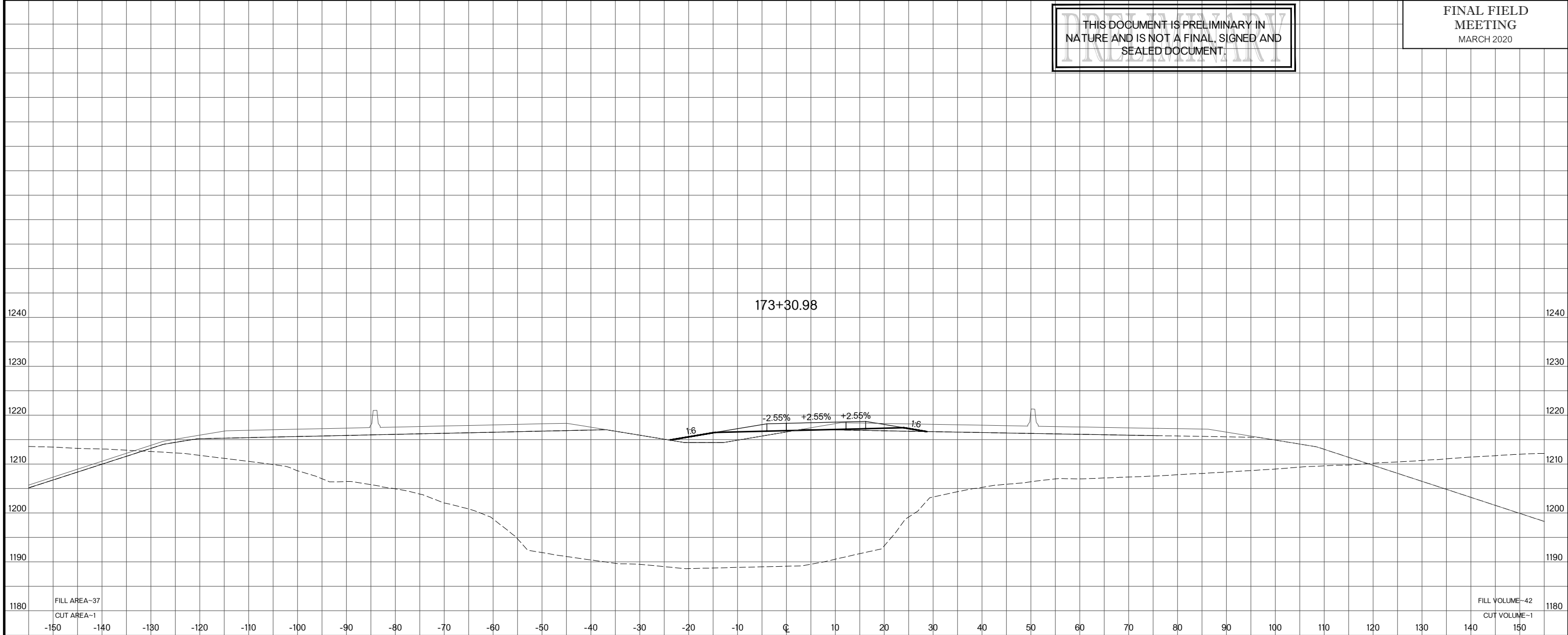
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



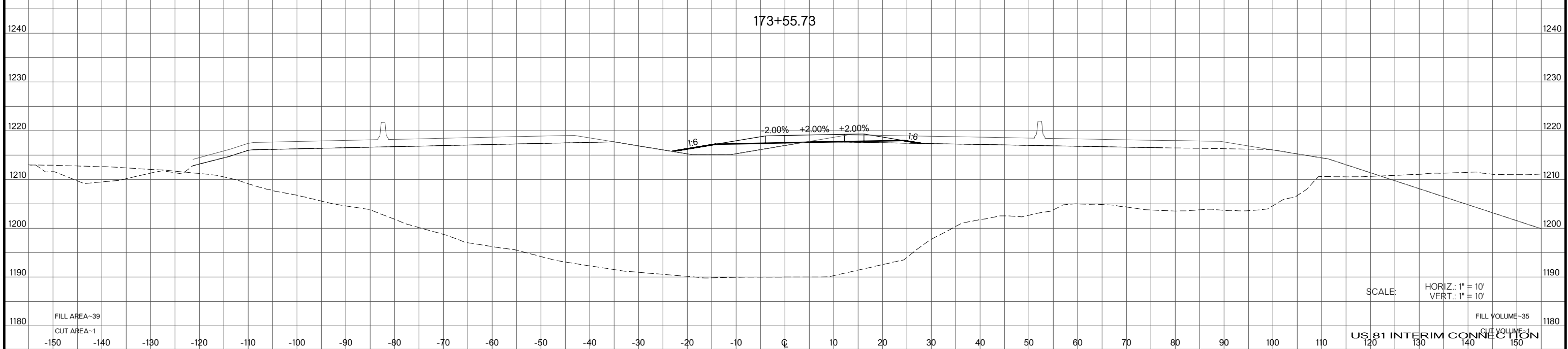
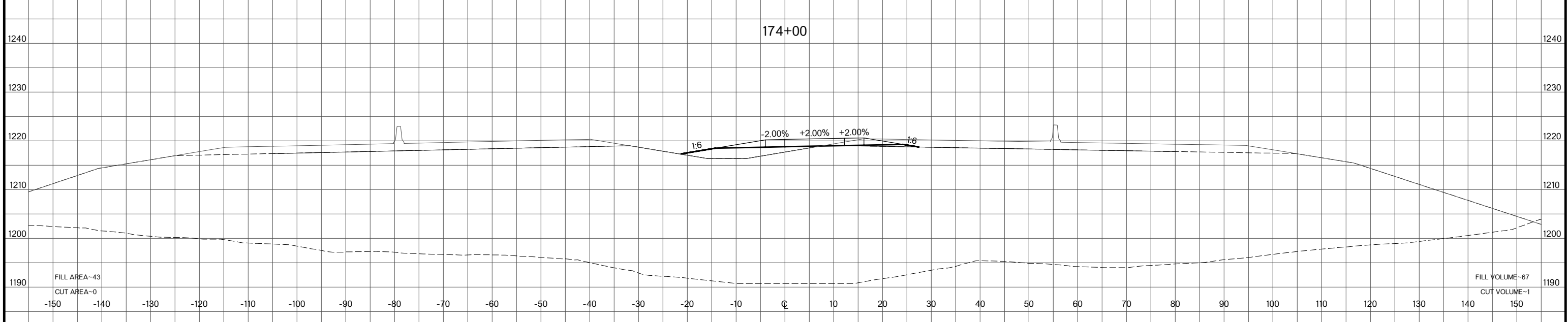
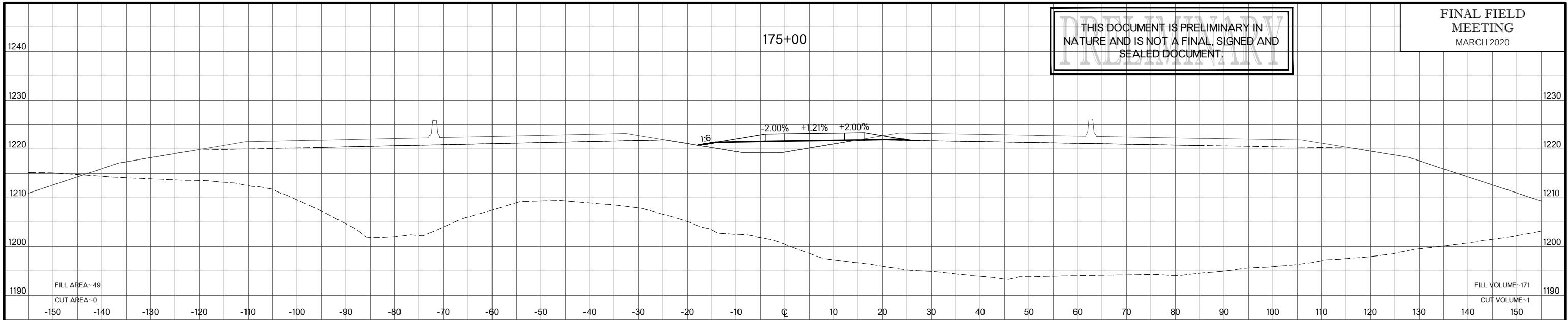
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 INTERIM CONNECTION

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



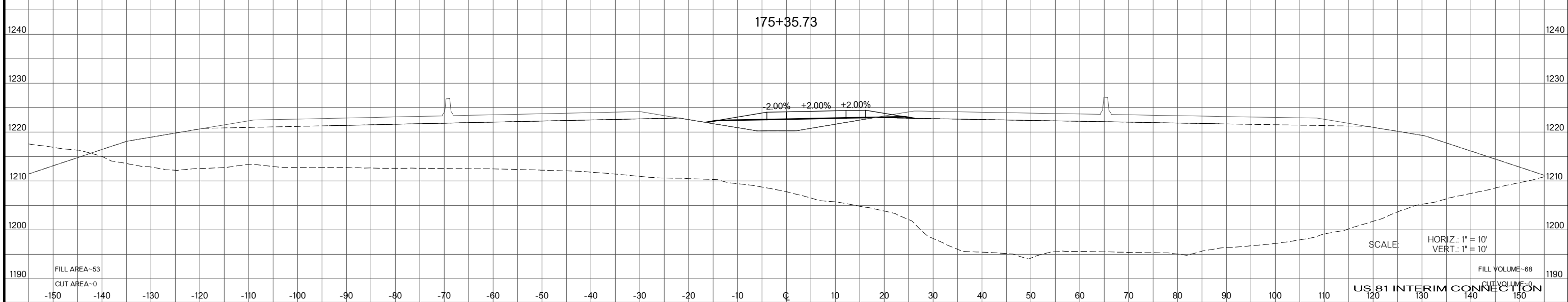
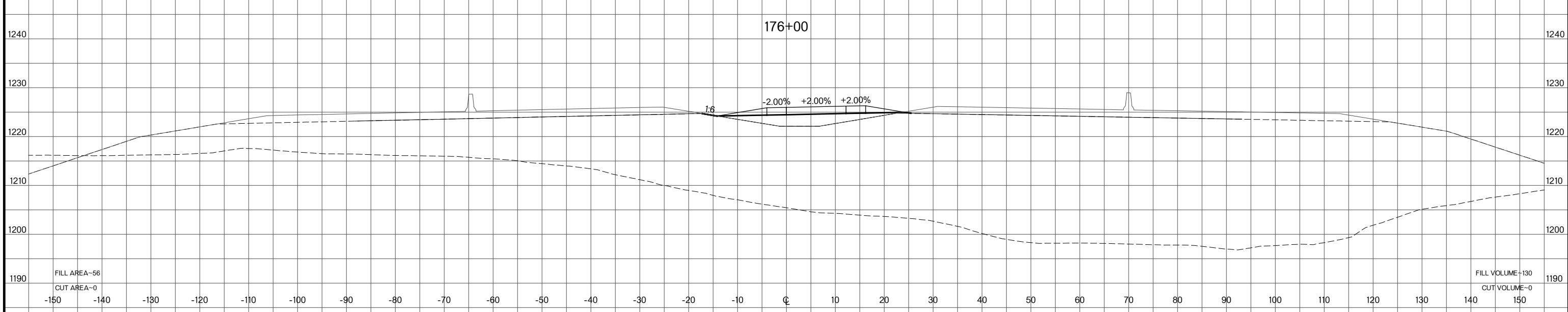
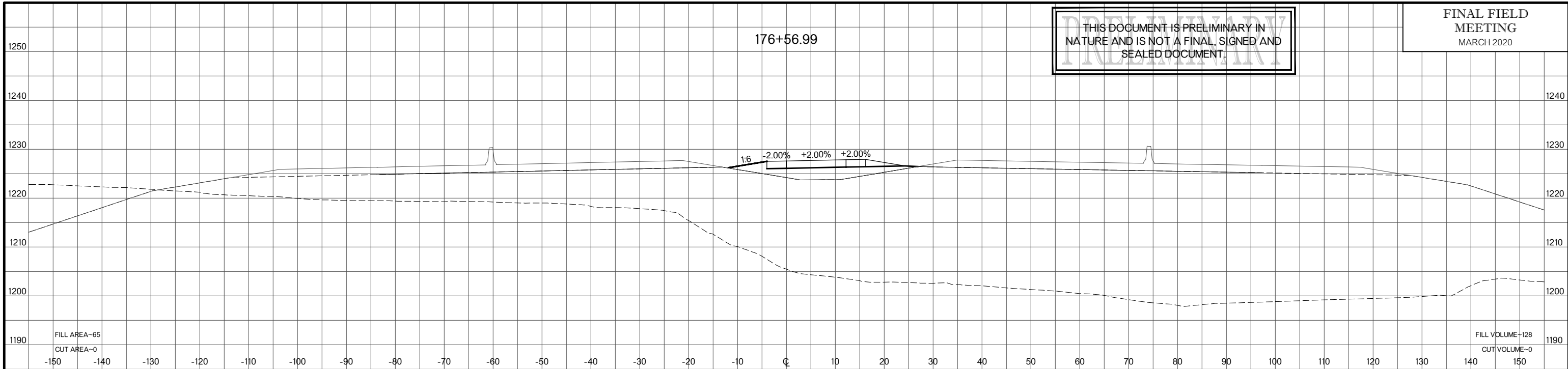
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 INTERIM CONNECTION

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

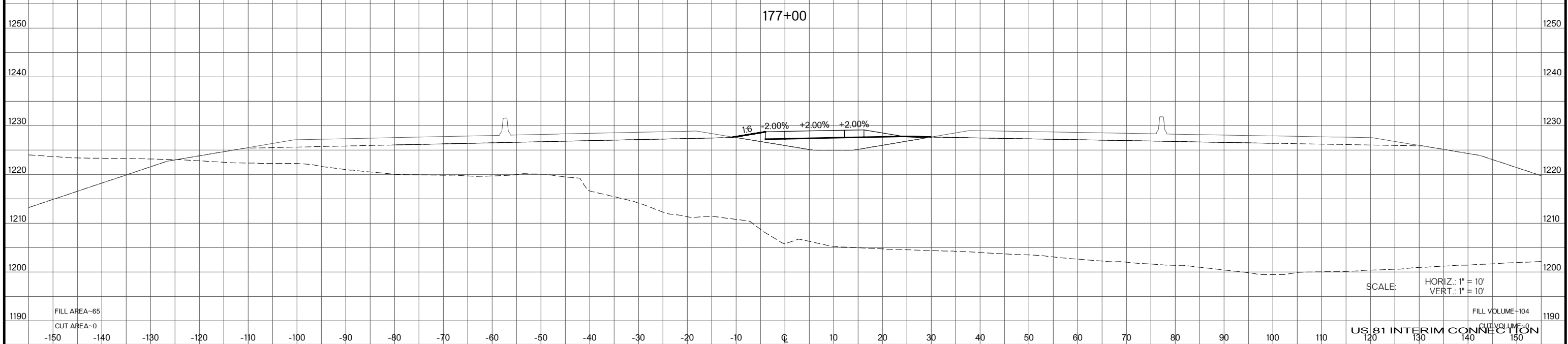
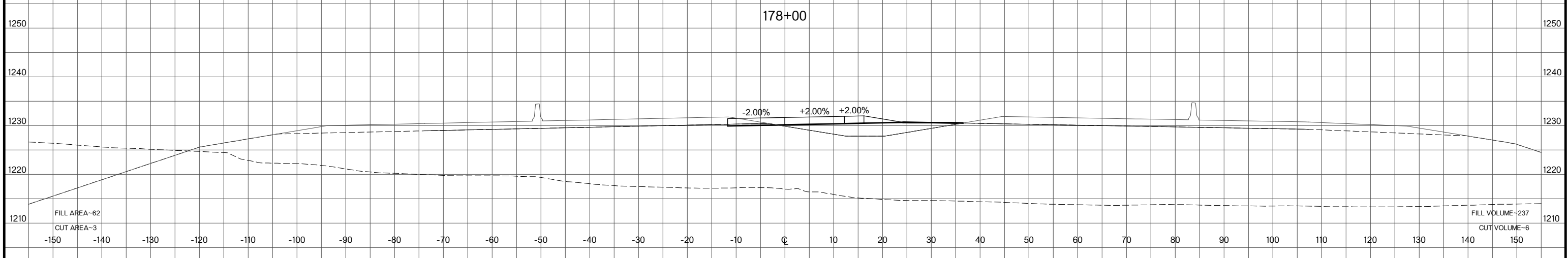
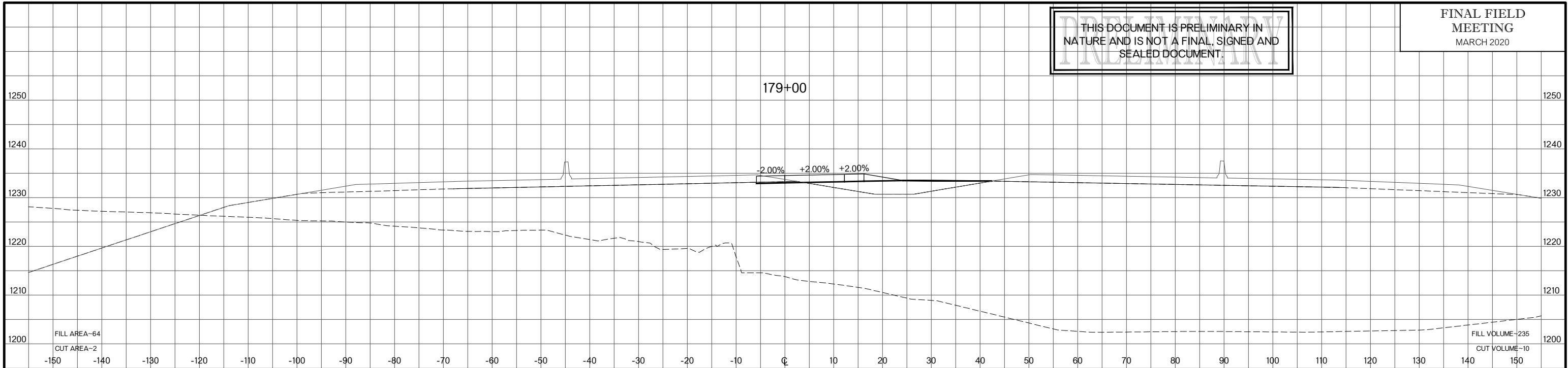
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



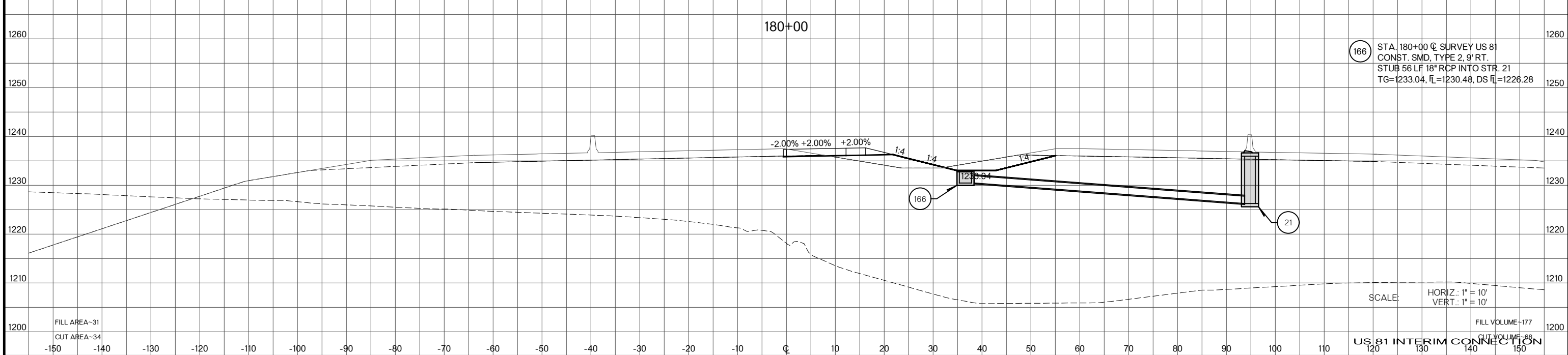
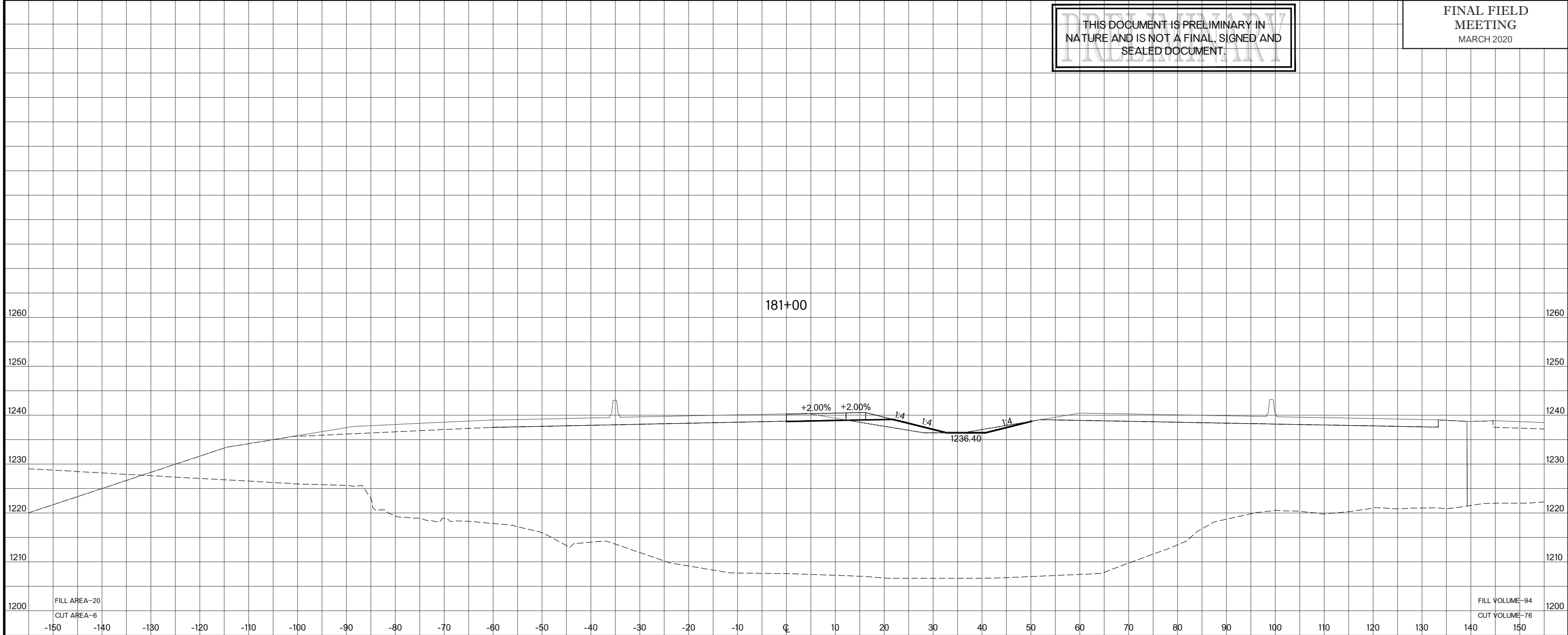
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 INTERIM CONNECTION

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



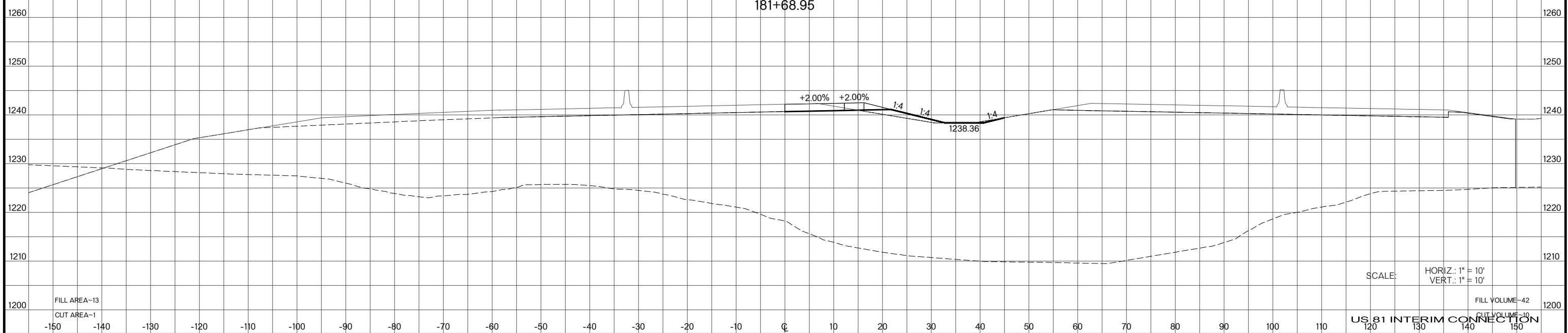
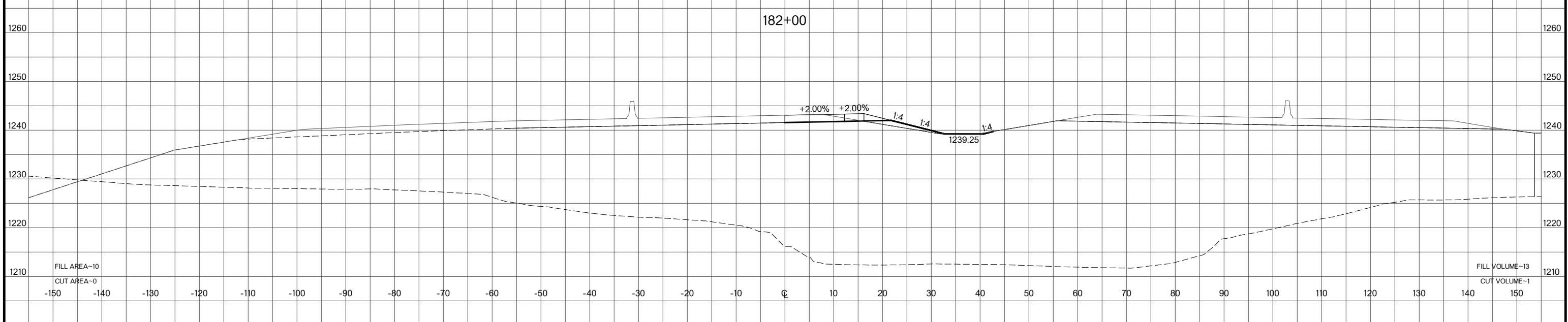
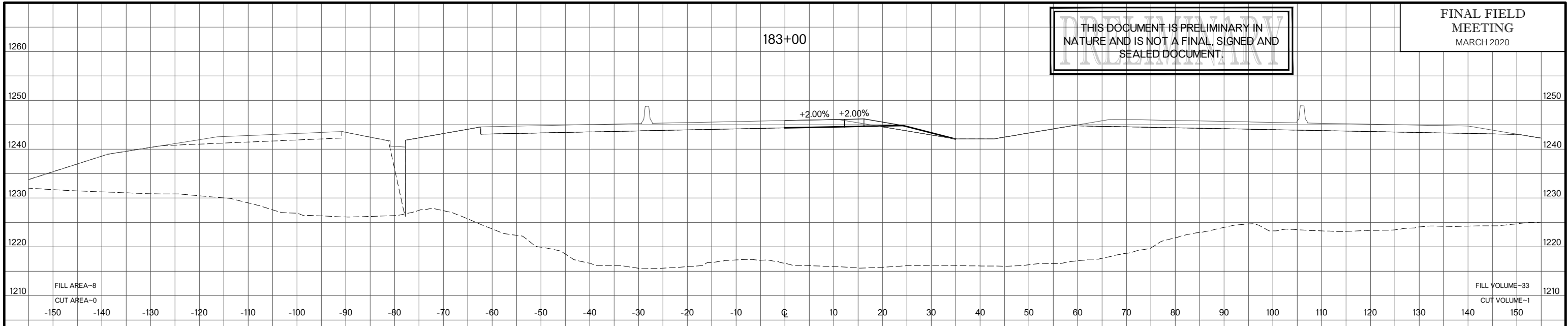
166 STA. 180+00 C SURVEY US 81
CONST. SMD. TYPE 2, 9' RT.
STUB 56 LF 18" RCP INTO STR. 21
TG=1233.04, FL=1230.48, DS FL=1226.28

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 INTERIM CONNECTION

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



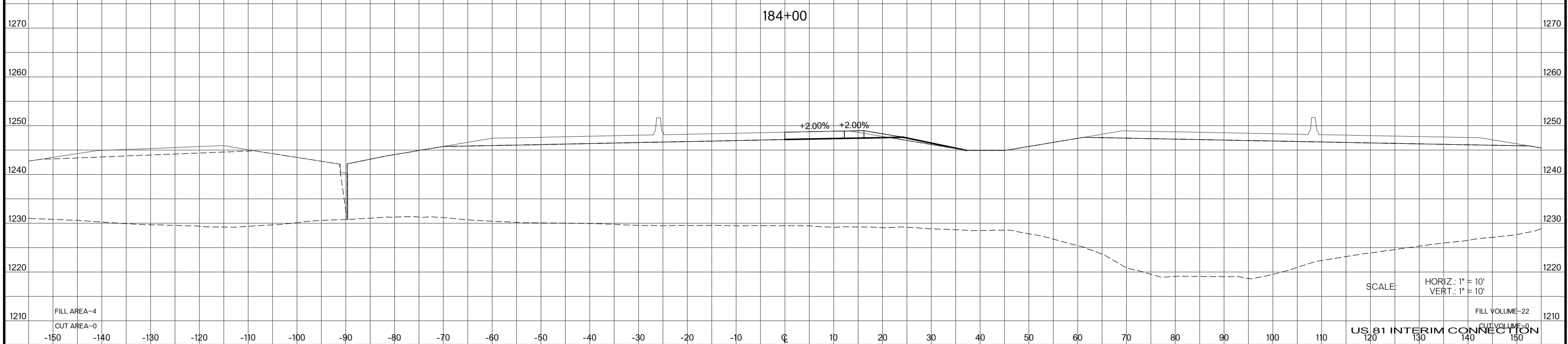
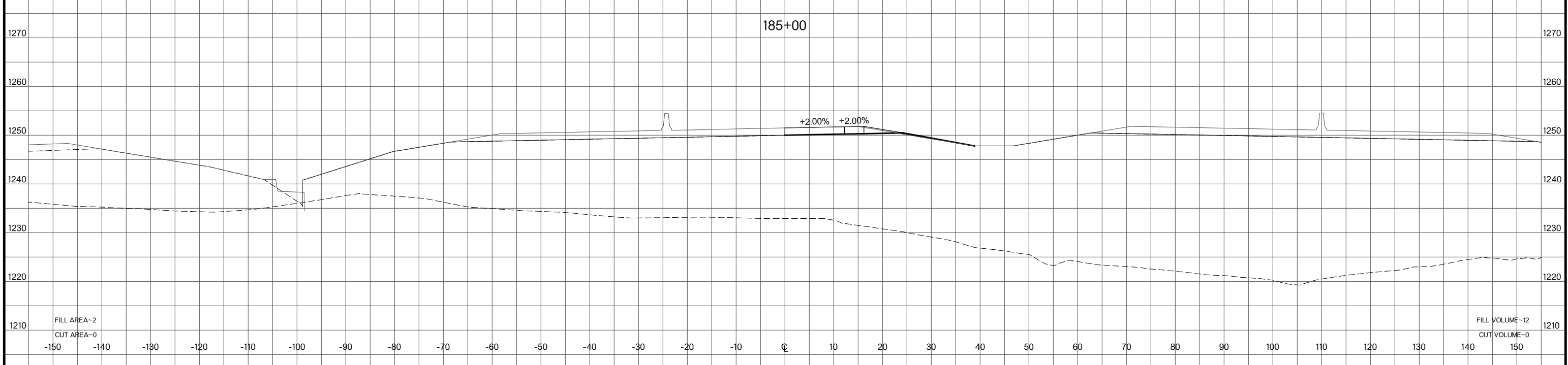
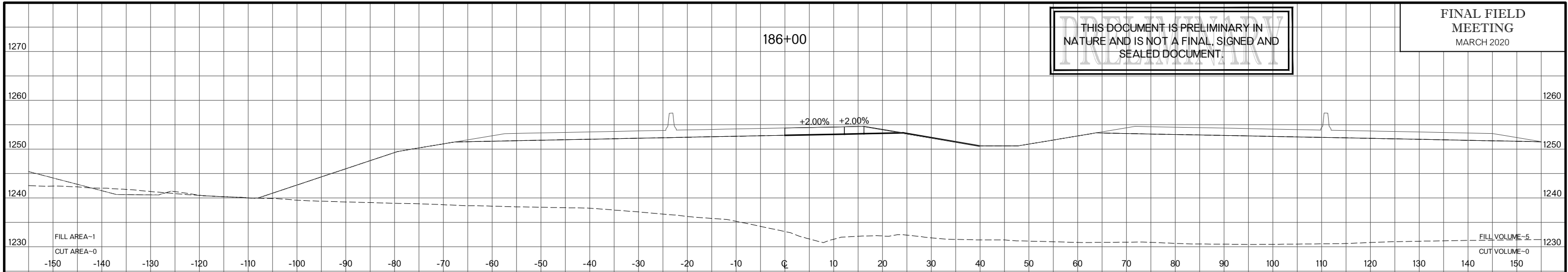
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 INTERIM CONNECTION

US 81 REALIGNMENT
GRADY COUNTY

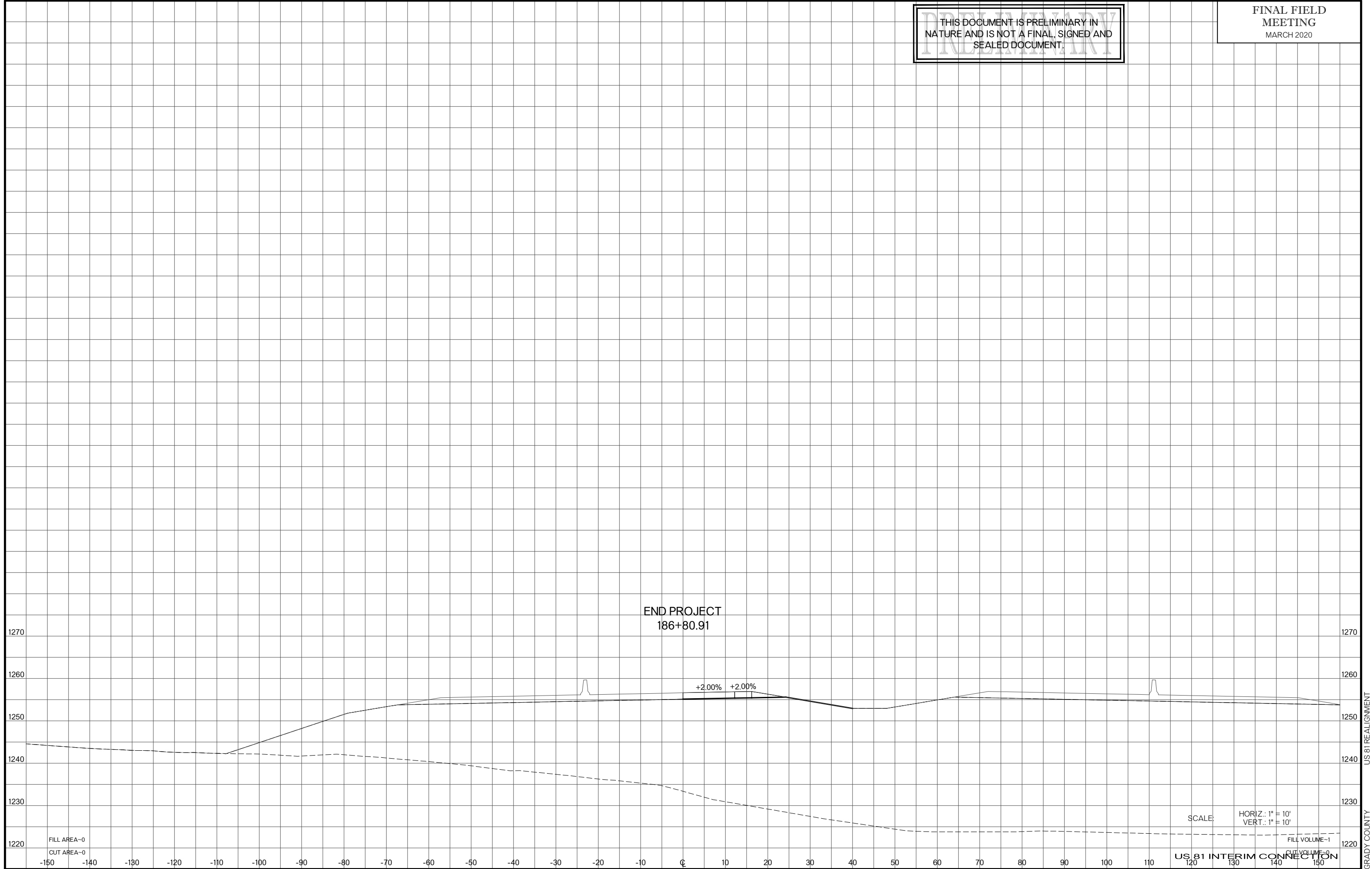
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

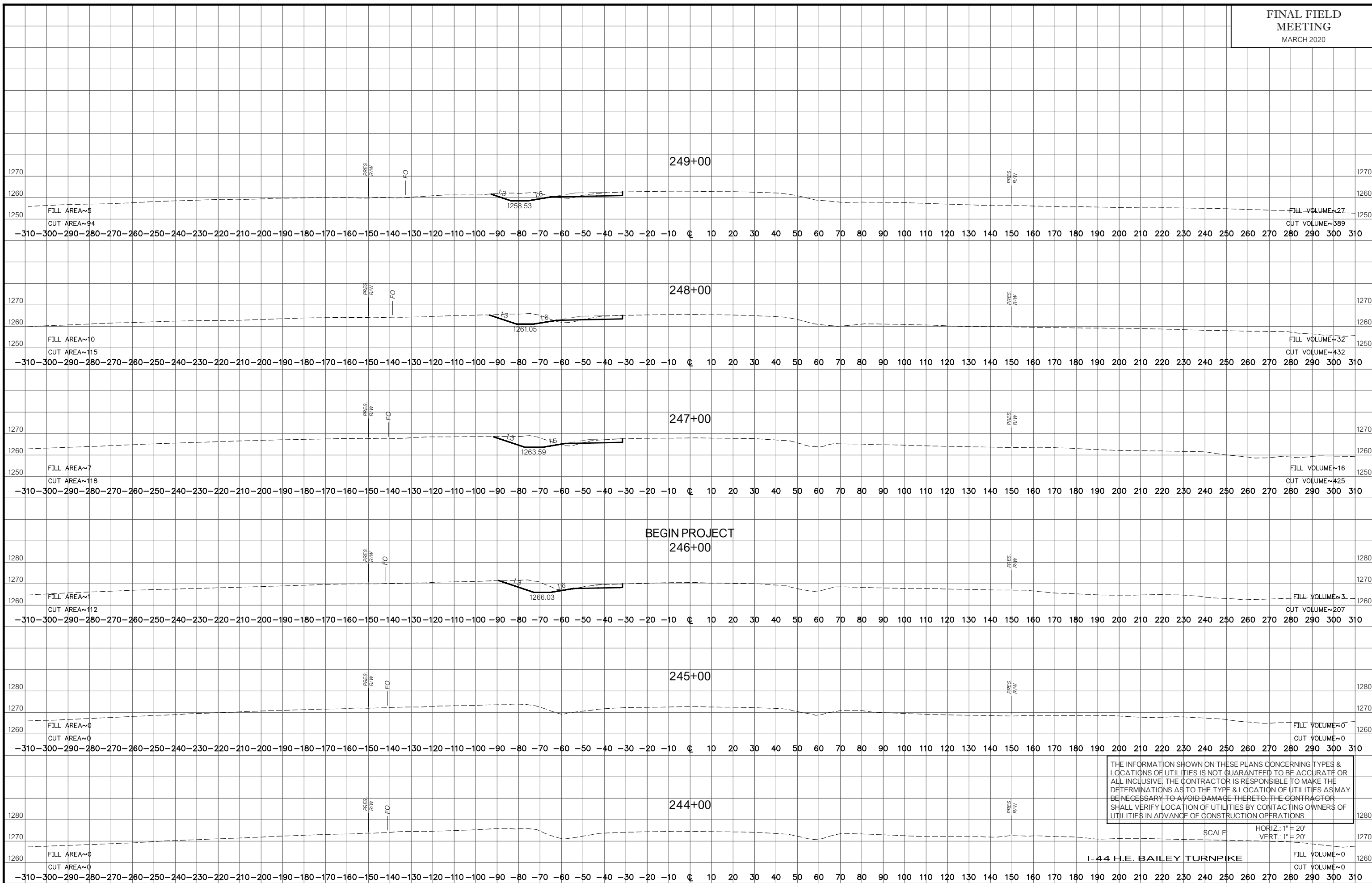
FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



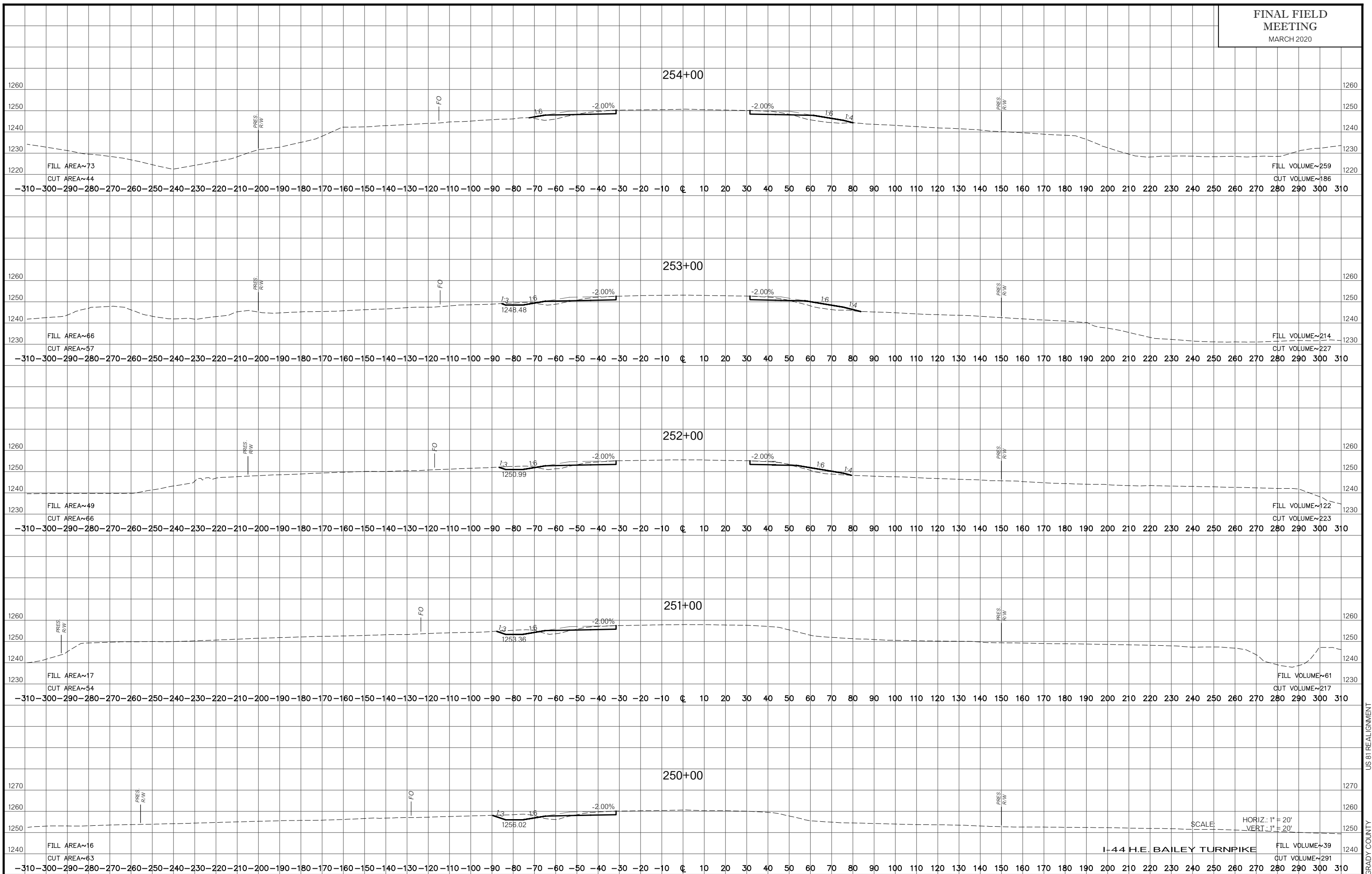


THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

SCALE HORIZ.: 1" = 20'
 VERT.: 1" = 20'

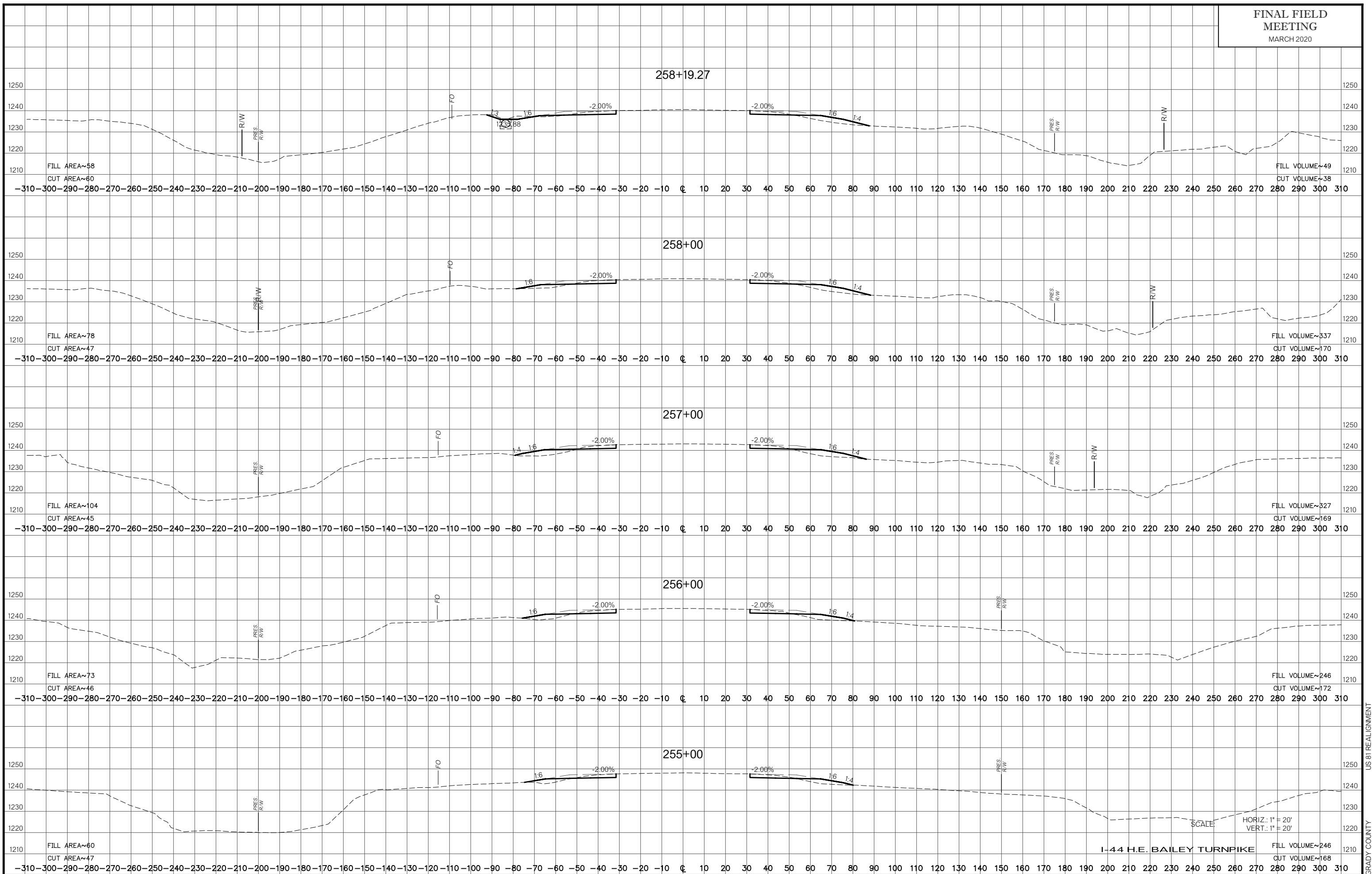
I-44 H.E. BAILEY TURNPIKE

US 81 REALIGNMENT
GRADY COUNTY



SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

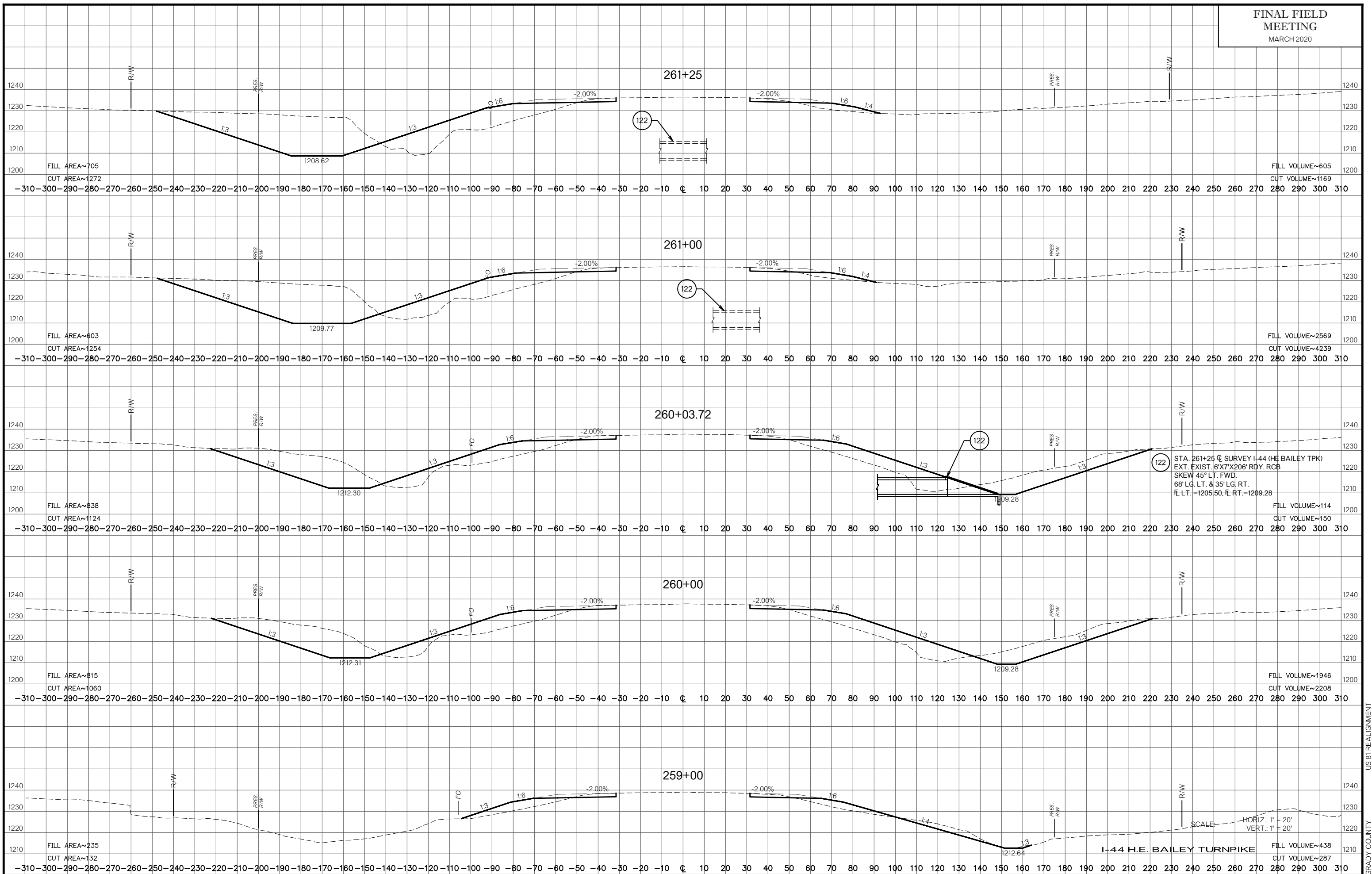
I-44 H.E. BAILEY TURNPIKE



SCALE
 HORIZ.: 1" = 20'
 VERT.: 1" = 20'

I-44 H.E. BAILEY TURNPIKE

US 81 REALIGNMENT
 GRADY COUNTY



FILL AREA~705
CUT AREA~1272

FILL VOLUME~605
CUT VOLUME~1169

FILL AREA~603
CUT AREA~1254

FILL VOLUME~2569
CUT VOLUME~4239

FILL AREA~838
CUT AREA~1124

FILL VOLUME~114
CUT VOLUME~150

FILL AREA~815
CUT AREA~1060

FILL VOLUME~1946
CUT VOLUME~2208

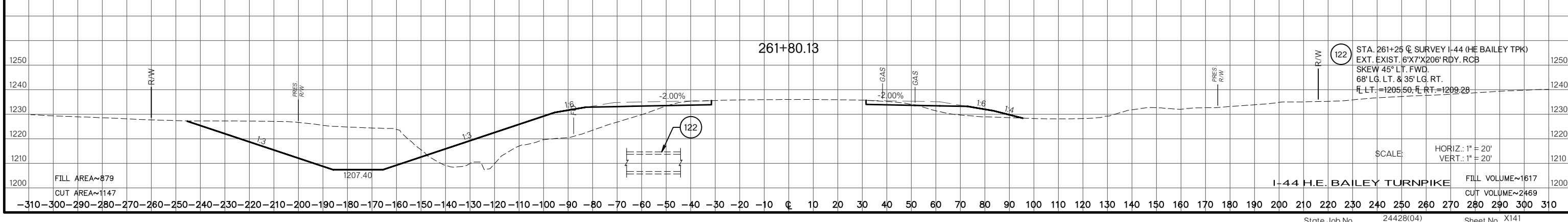
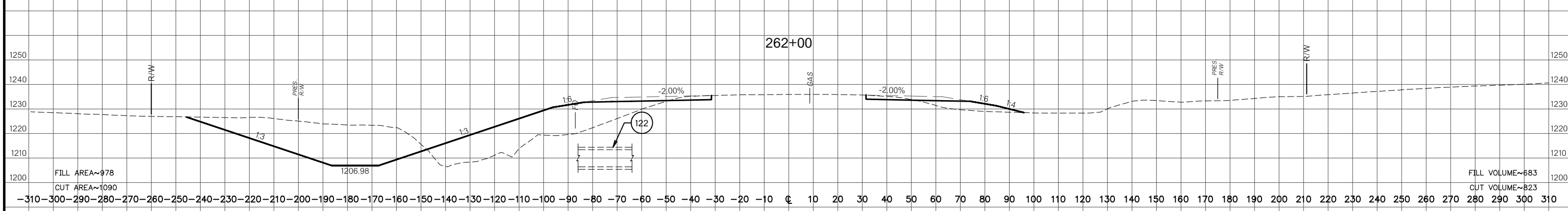
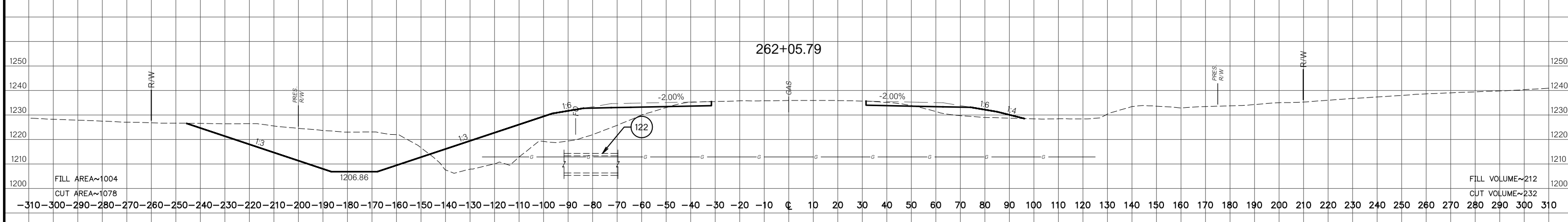
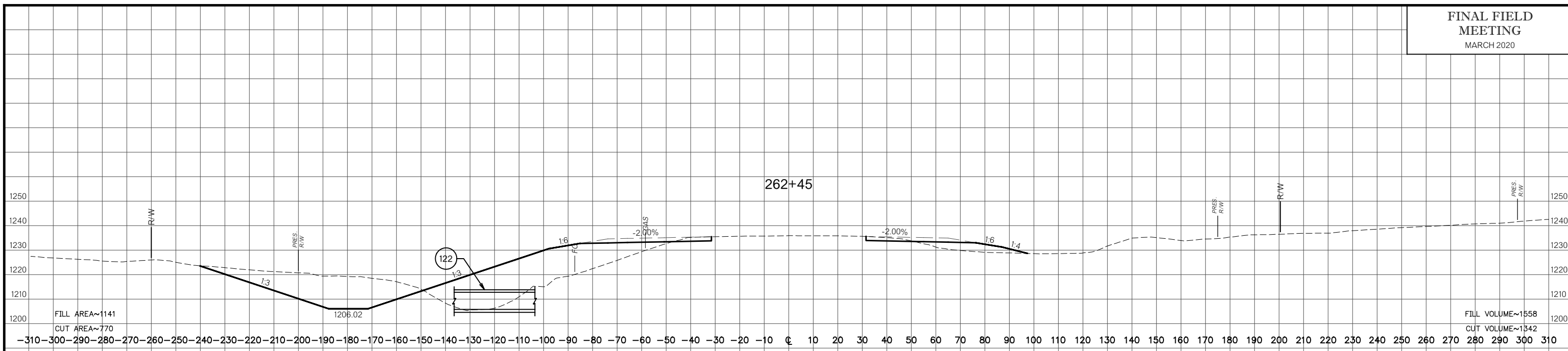
FILL AREA~235
CUT AREA~132

FILL VOLUME~438
CUT VOLUME~287

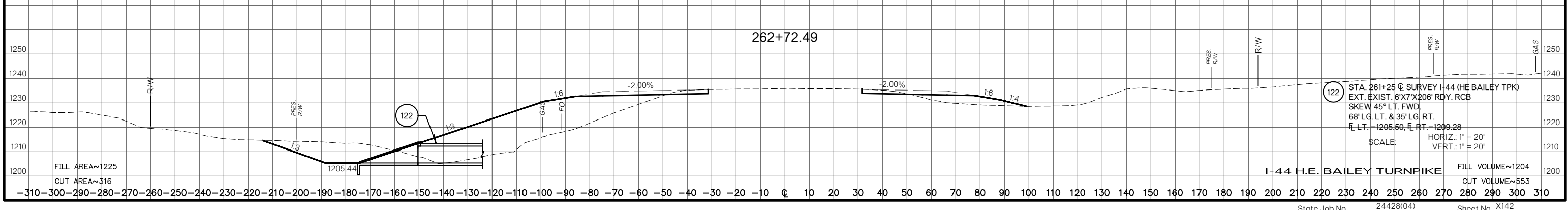
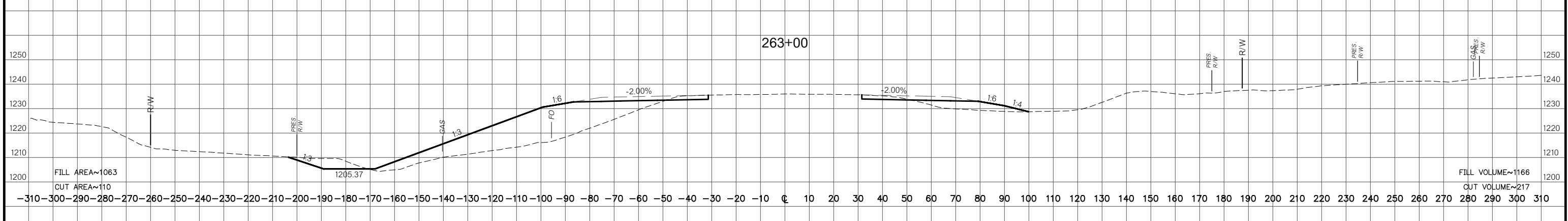
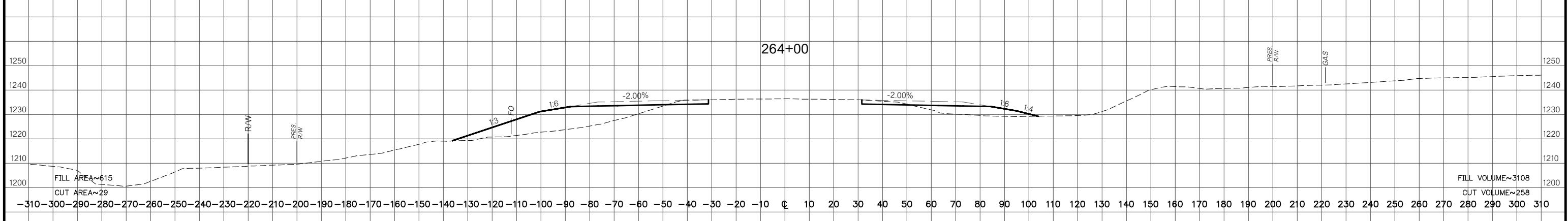
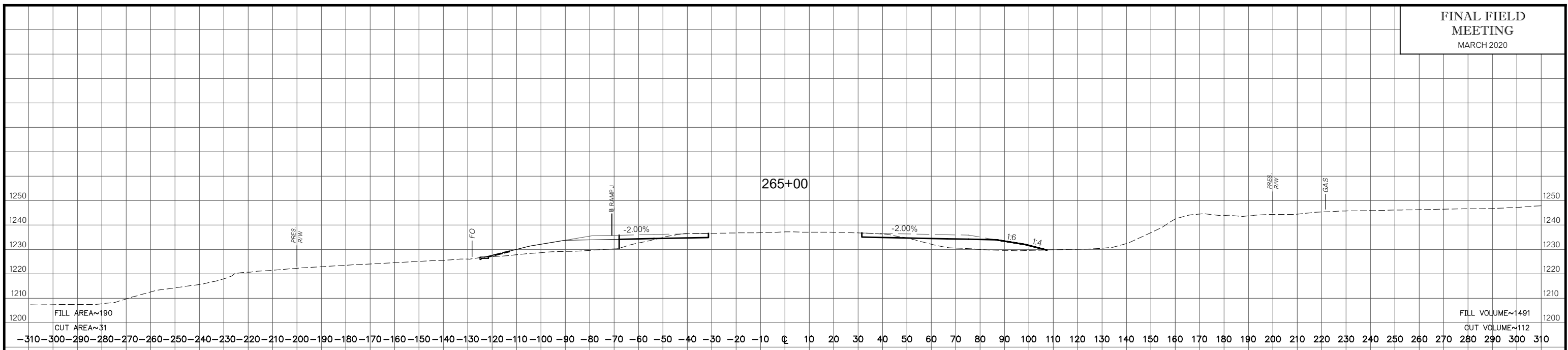
STA. 261+25 CL SURVEY I-44 (HE BAILEY TPK)
EXT. EXIST. 6'X7'X206' RDY. RCB
SKEW 45° LT. FWD.
68' LG. LT. & 35' LG. RT.
FL LT.=1205.50, FL RT.=1209.28

SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

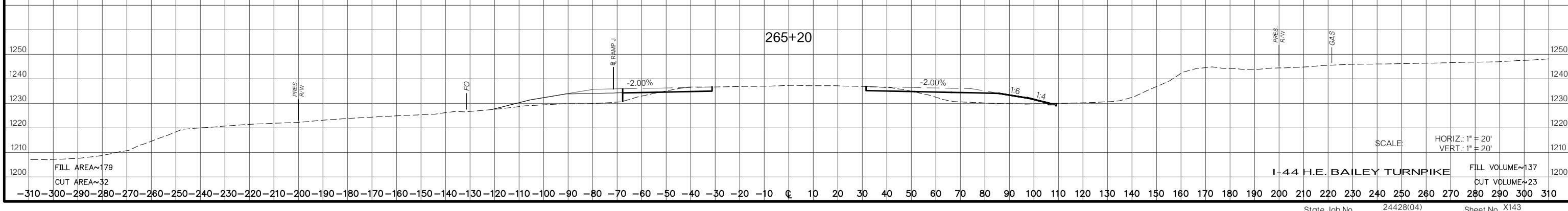
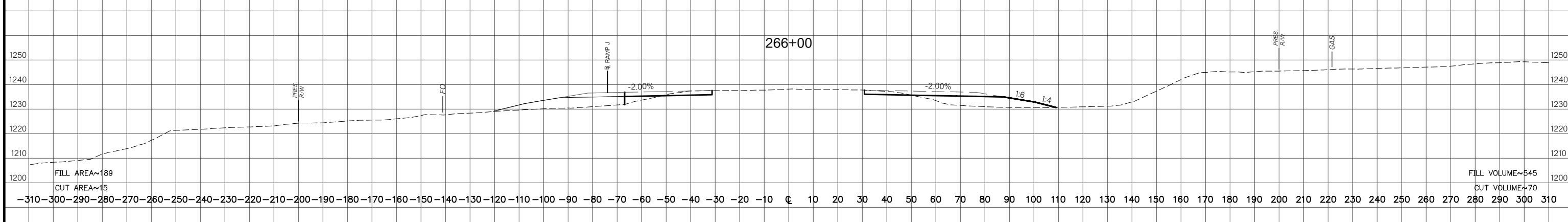
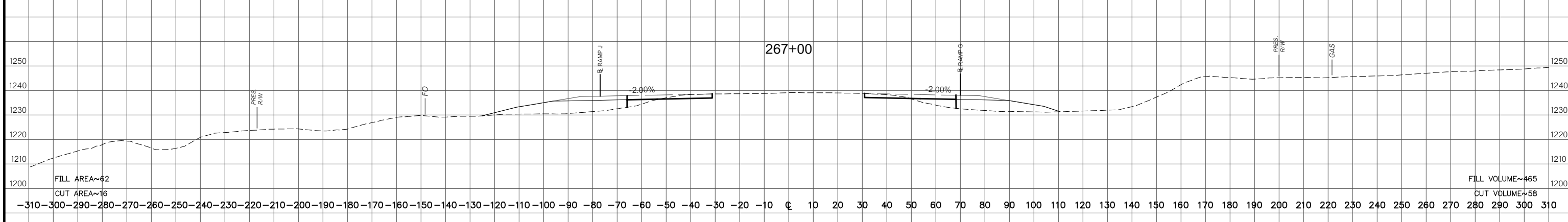
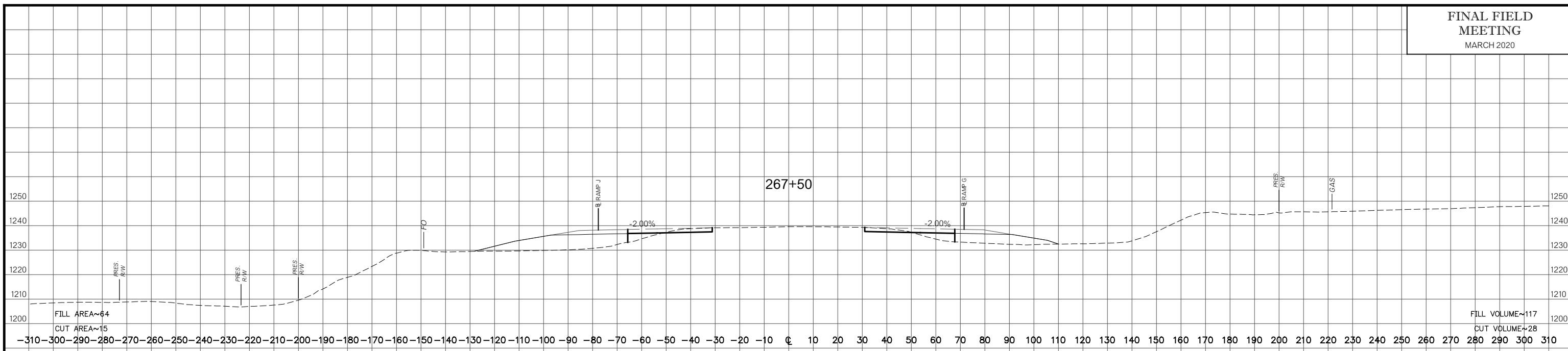
I-44 H.E. BAILEY TURNPIKE



US 81 REALIGNMENT
GRADY COUNTY



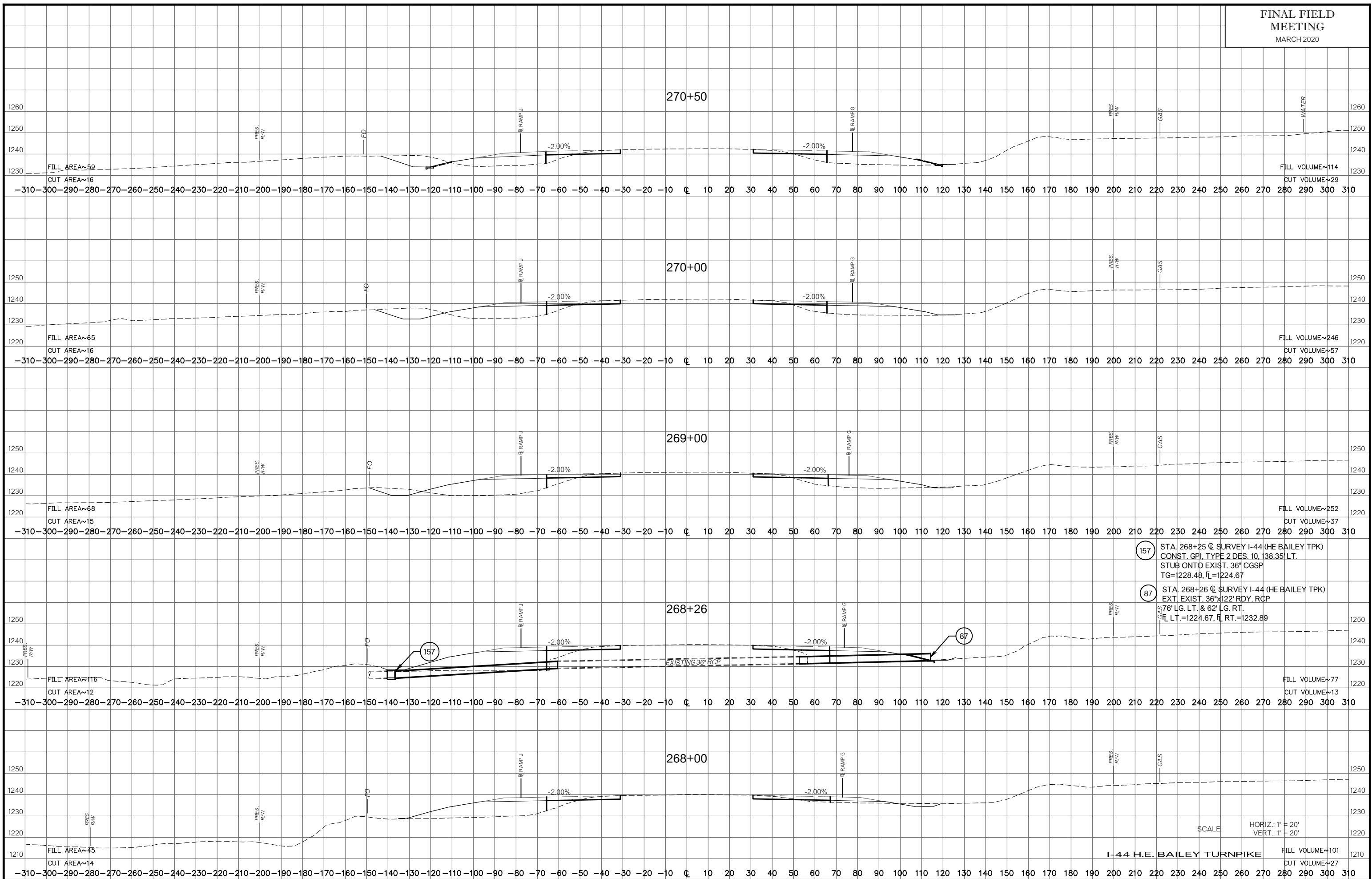
US 81 REALIGNMENT
GRADY COUNTY



SCALE: HORIZ.: 1" = 20'
VERT.: 1" = 20'

I-44 H.E. BAILEY TURNPIKE

US 81 REALIGNMENT
GRADY COUNTY



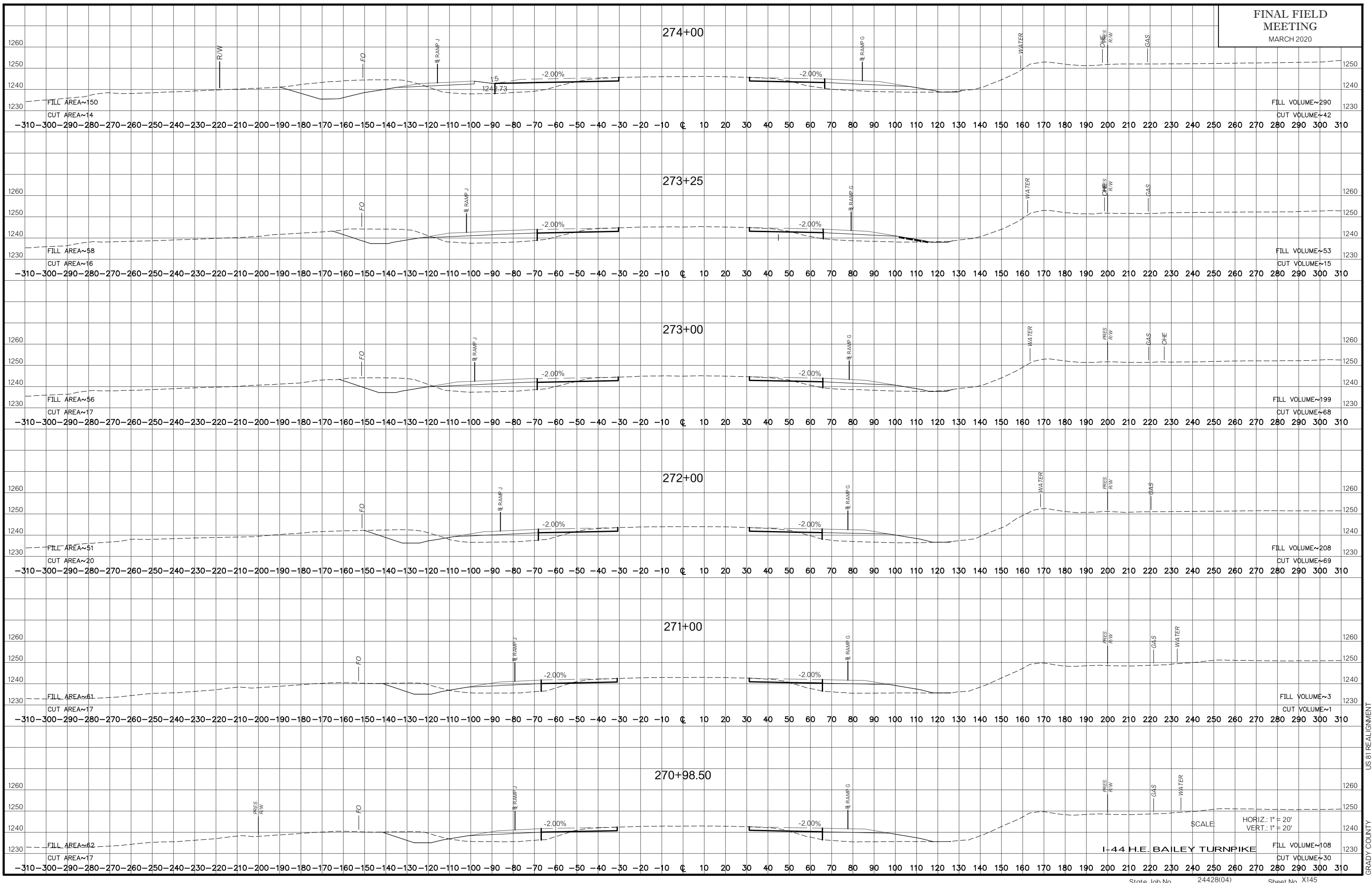
(157) STA. 268+25 @ SURVEY I-44 (HE BAILEY TPK)
CONST. GPI, TYPE 2 DES. 10, 138.35' LT.
STUB ONTO EXIST. 36" CGSP
TG=1228.48, FL=1224.67

(87) STA. 268+26 @ SURVEY I-44 (HE BAILEY TPK)
EXT. EXIST. 36"x122' RDY. RCP
76' LG. LT. & 62' LG. RT.
FL LT.=1224.67, FL RT.=1232.89

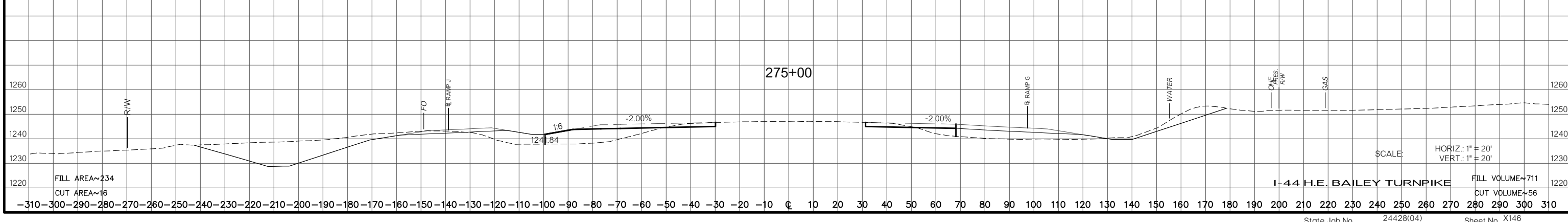
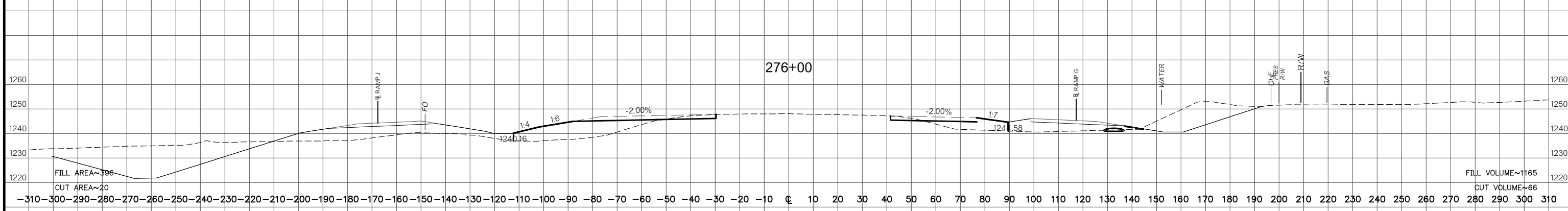
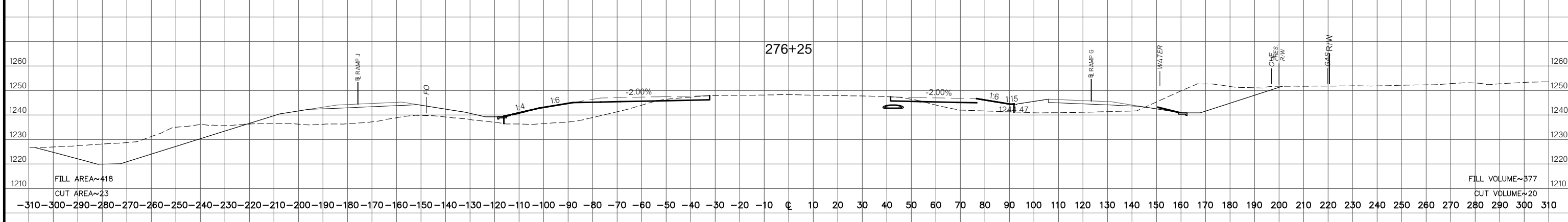
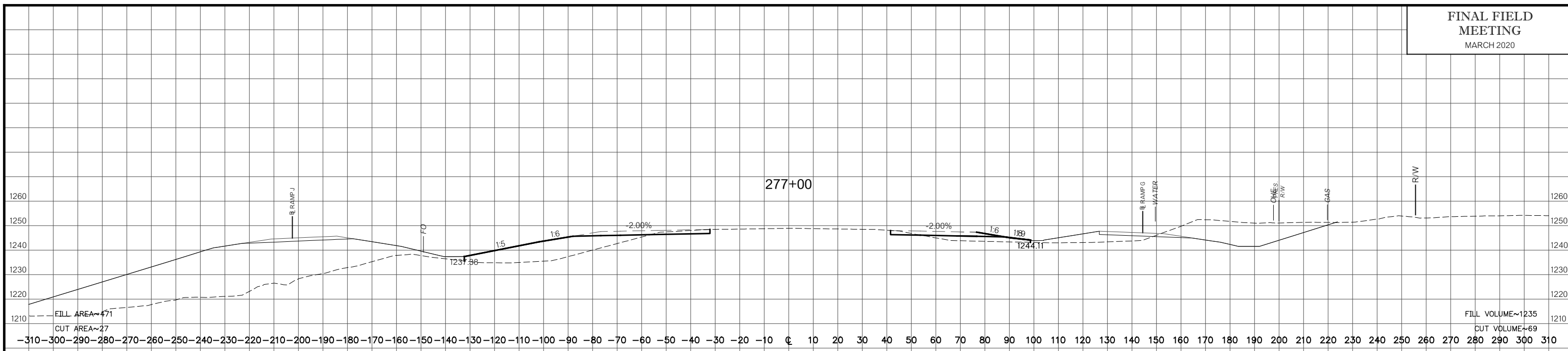
SCALE HORIZ.: 1" = 20'
VERT.: 1" = 20'

I-44 H.E. BAILEY TURNPIKE

US 81 REALIGNMENT
GRADY COUNTY



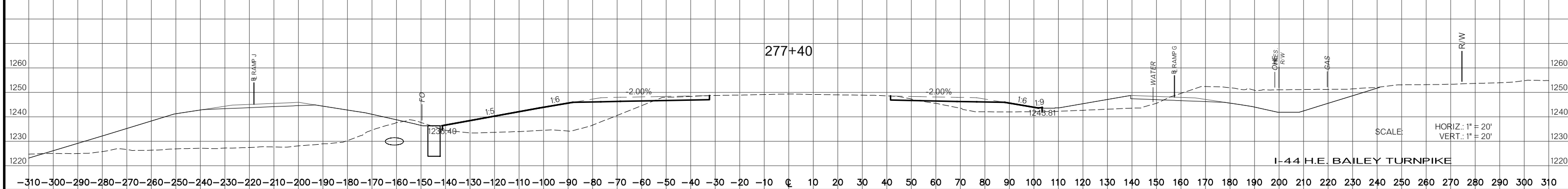
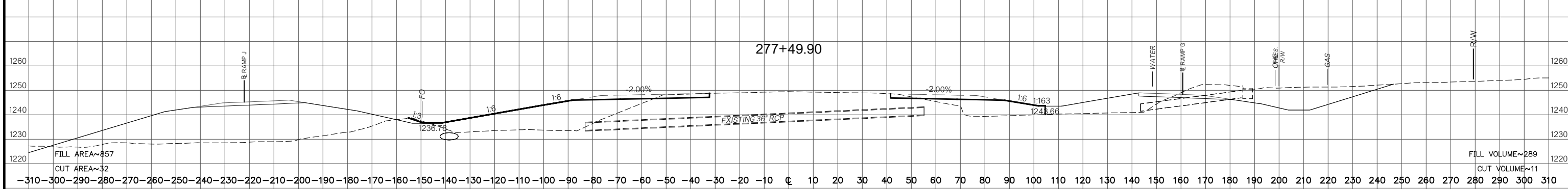
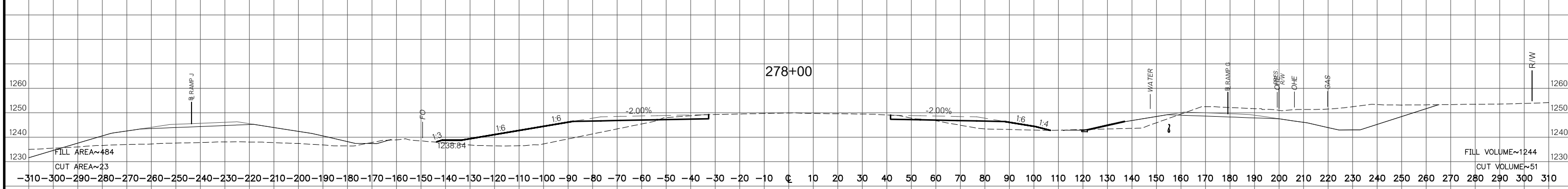
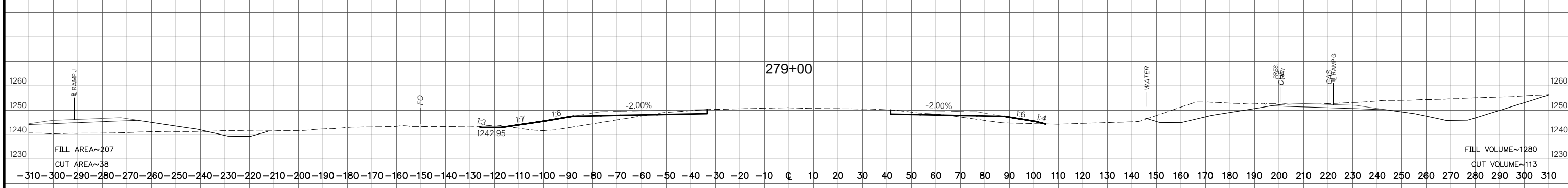
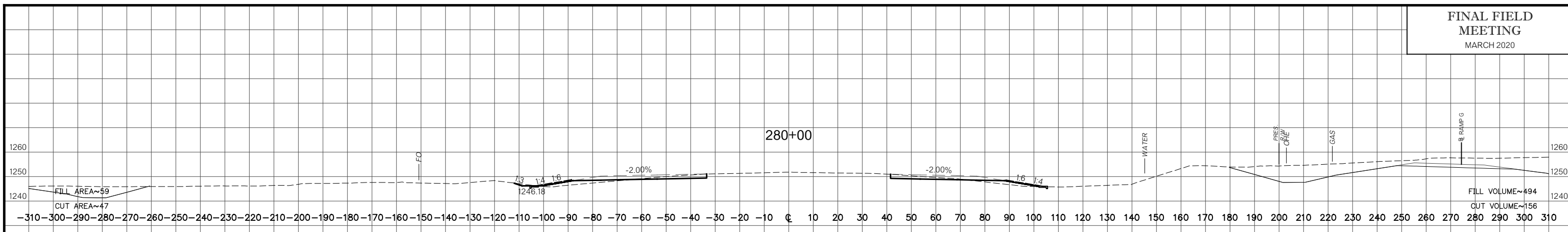
US 81 REALIGNMENT
GRADY COUNTY



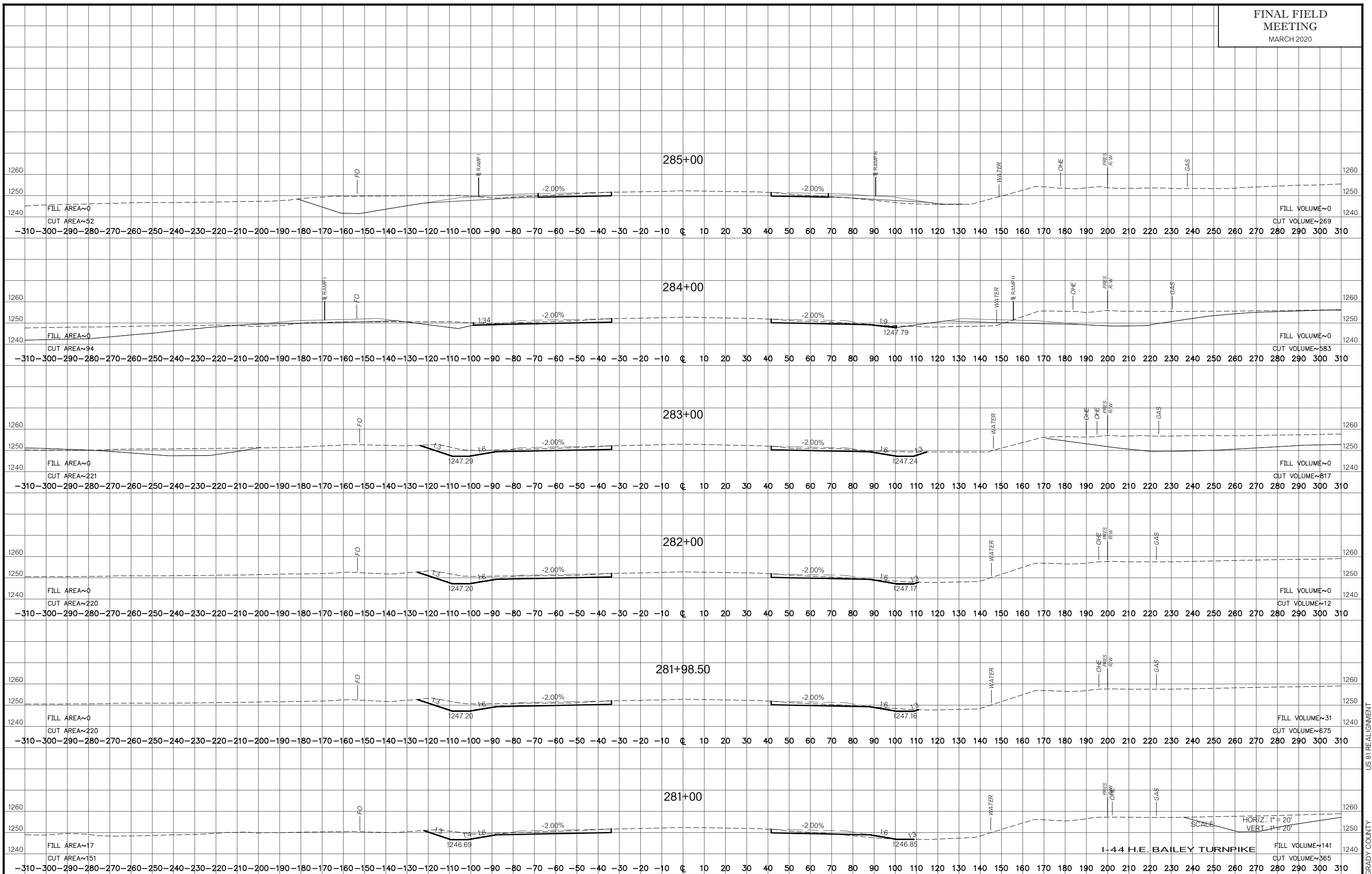
SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

I-44 H.E. BAILEY TURNPIKE

US 81 REALIGNMENT
GRADY COUNTY



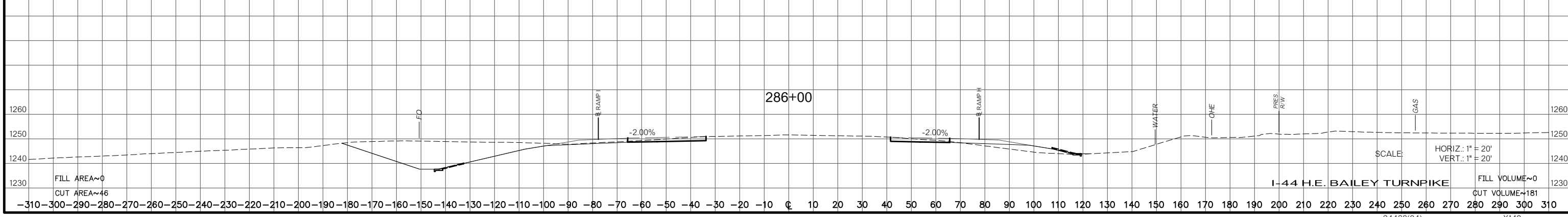
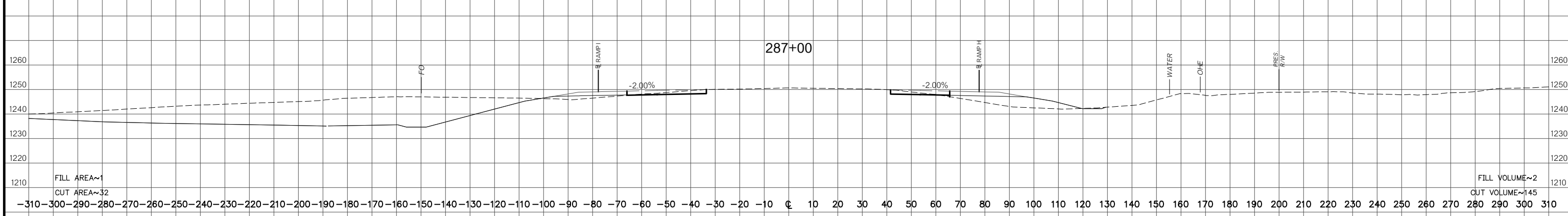
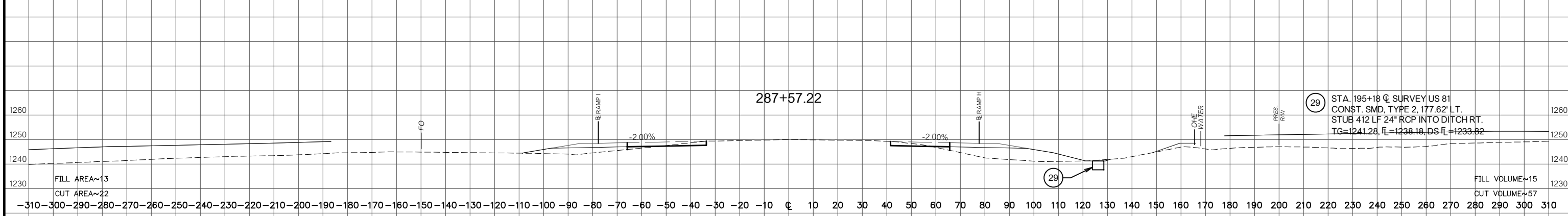
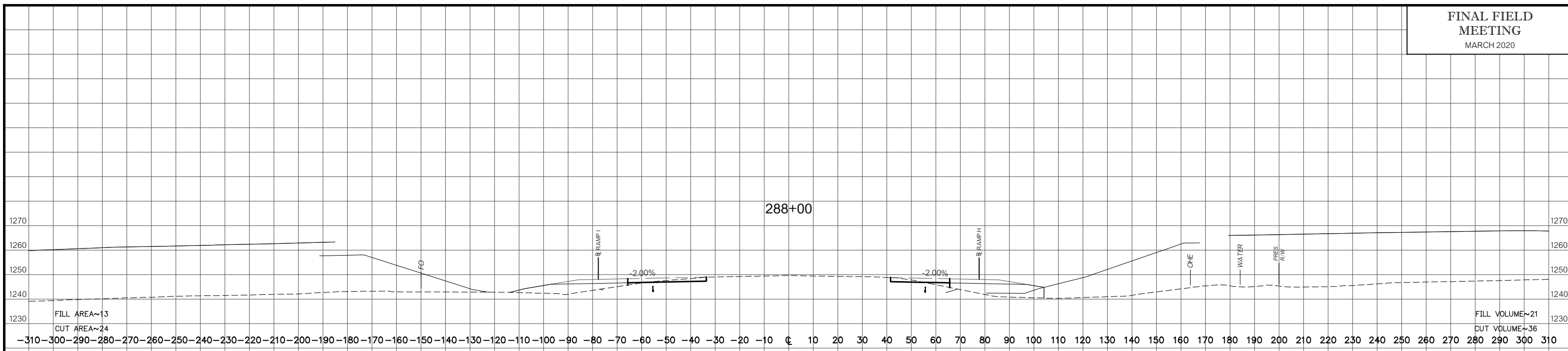
US 81 REALIGNMENT
GRADY COUNTY



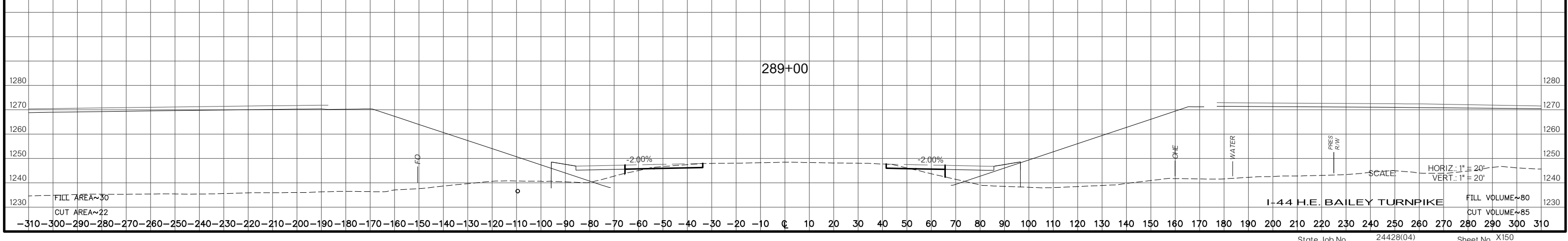
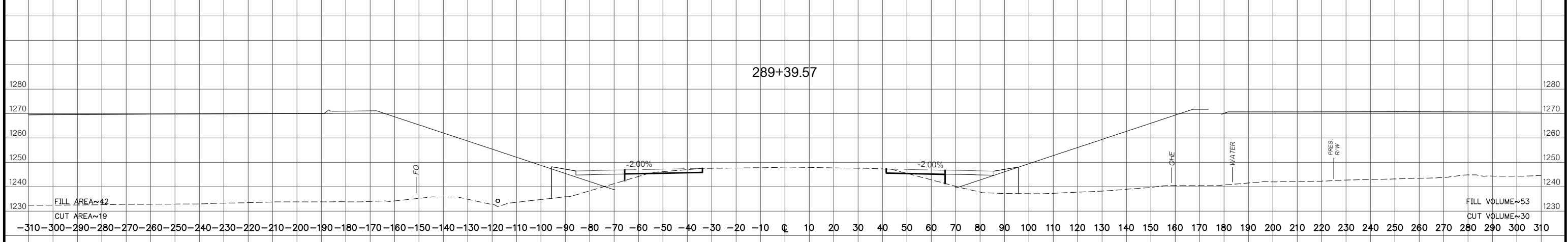
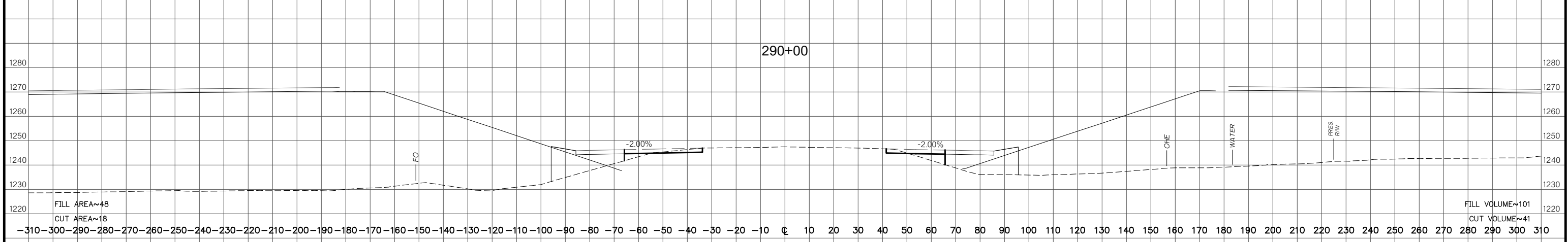
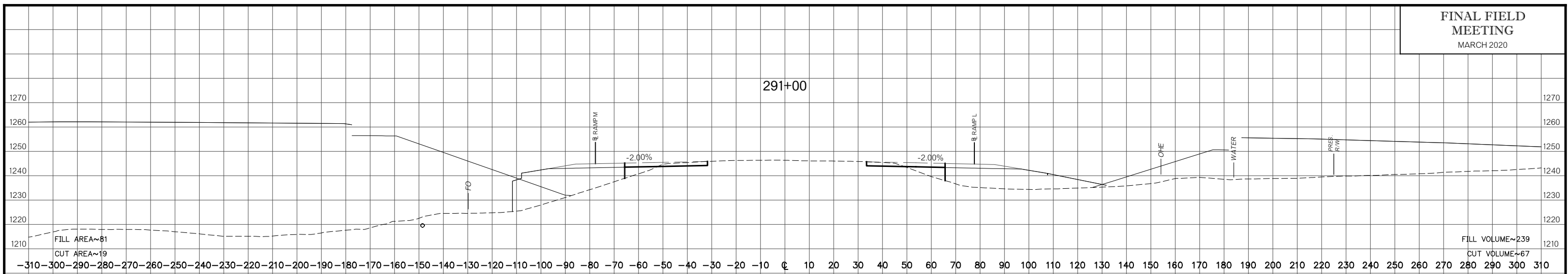
I-44 H.E. BAILEY TURNPIKE

SCALE
 HORIZ.: 1" = 20'
 VERT.: 1" = 20'

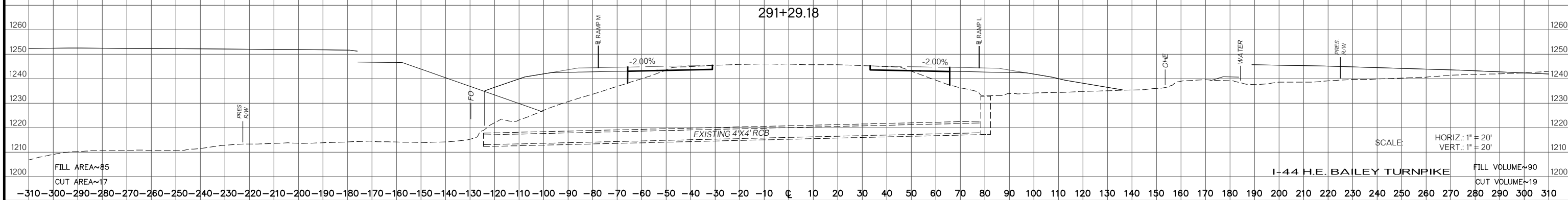
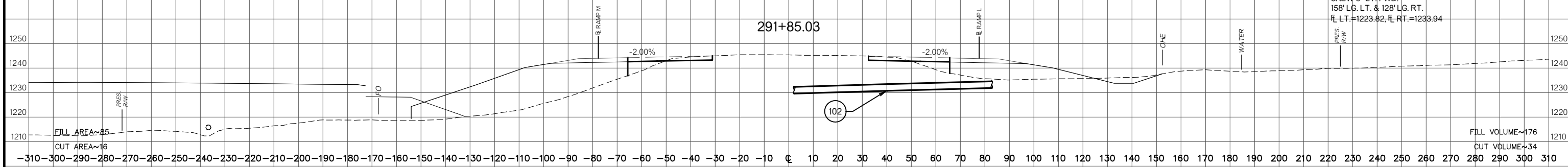
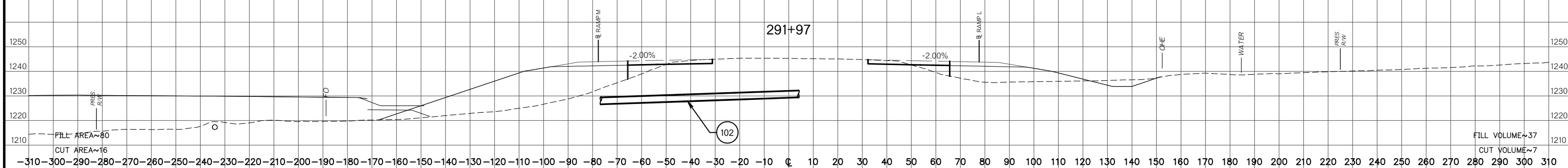
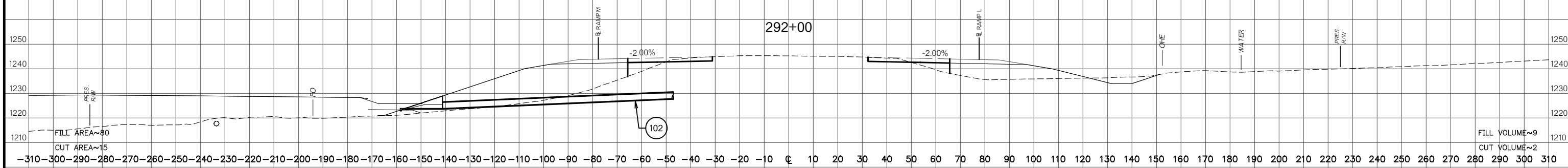
US 81 REALIGNMENT
 GRADY COUNTY



US 81 REALIGNMENT
GRADY COUNTY



GRADY COUNTY
US 81 REALIGNMENT

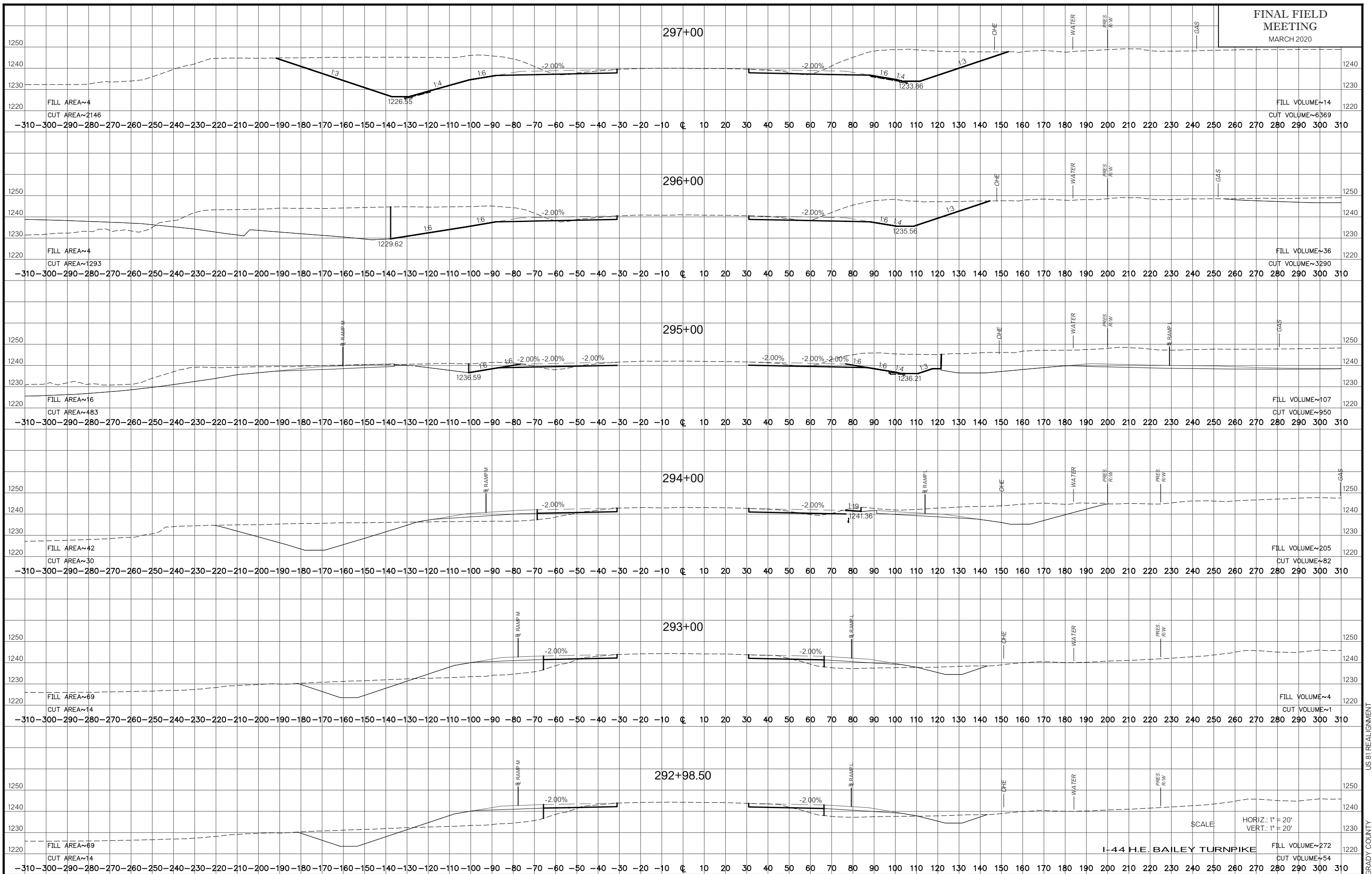


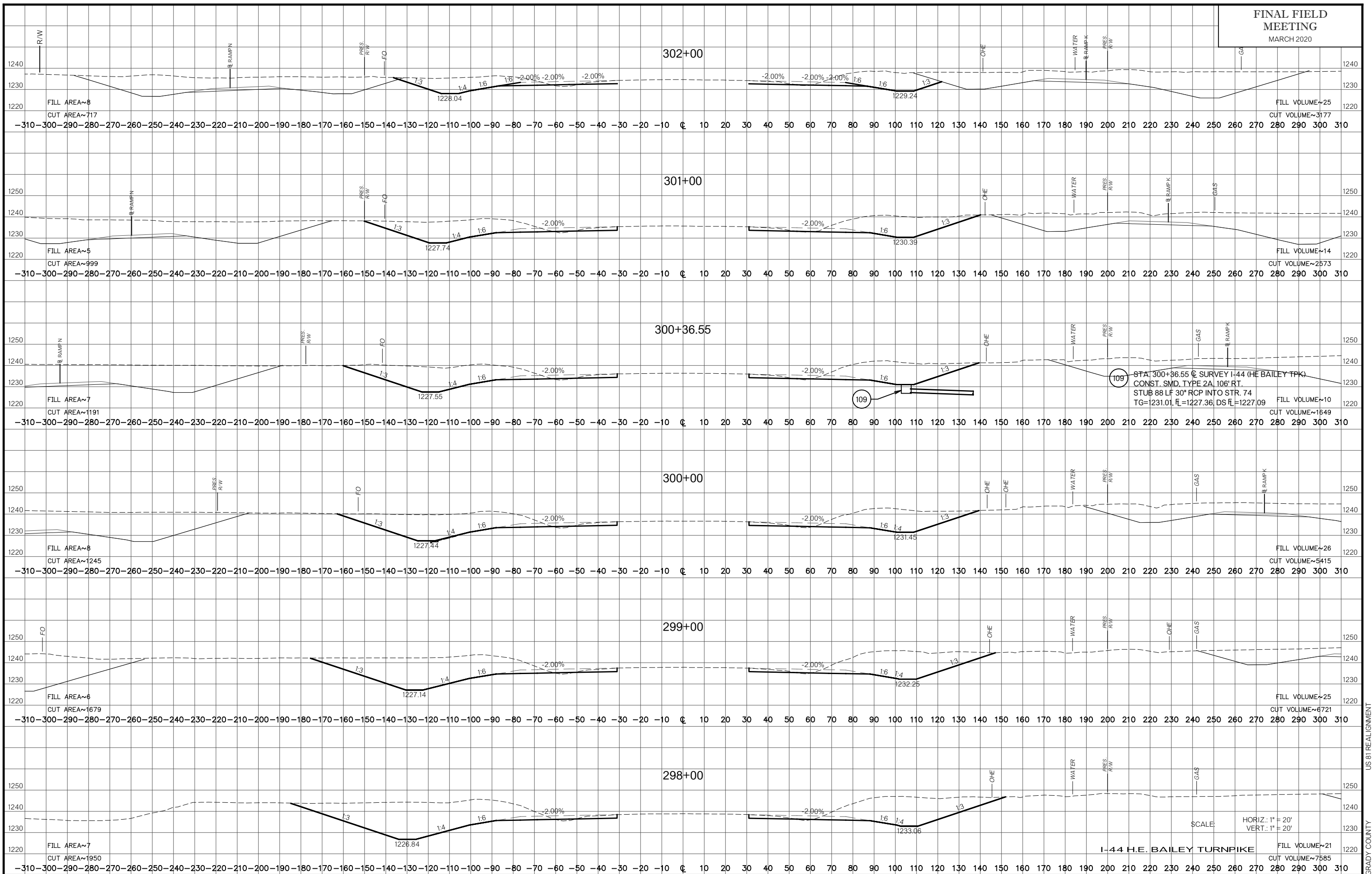
102 STA. 291+91 C SURVEY I-44 (HE BAILEY TPK)
CONST. 30"X286' LG. RCP
SKEW 8° LT. FWD.
158' LG. LT. & 128' LG. RT.
FL LT.=1223.82, FL RT.=1233.94

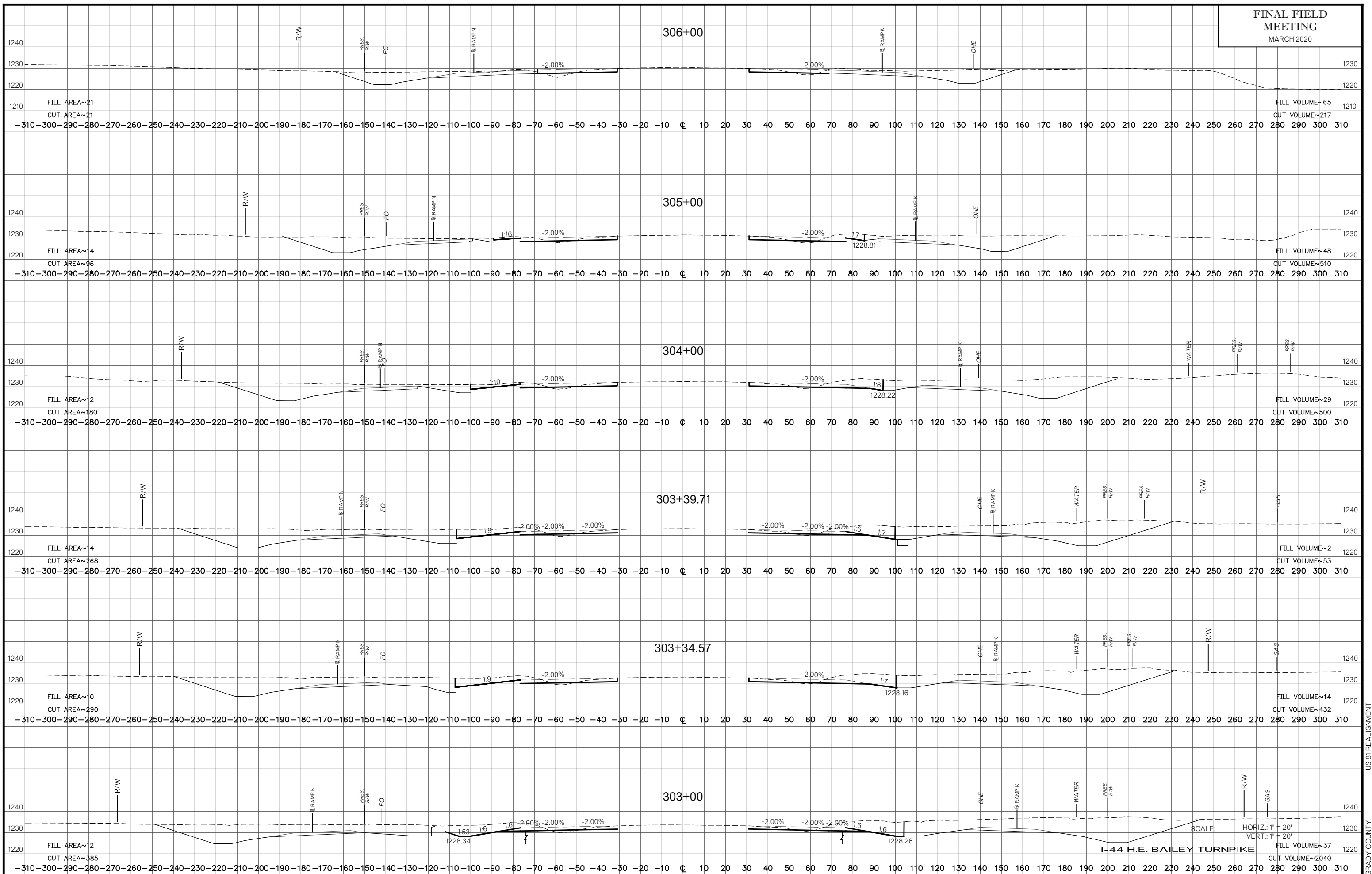
SCALE HORIZ.: 1" = 20'
VERT.: 1" = 20'

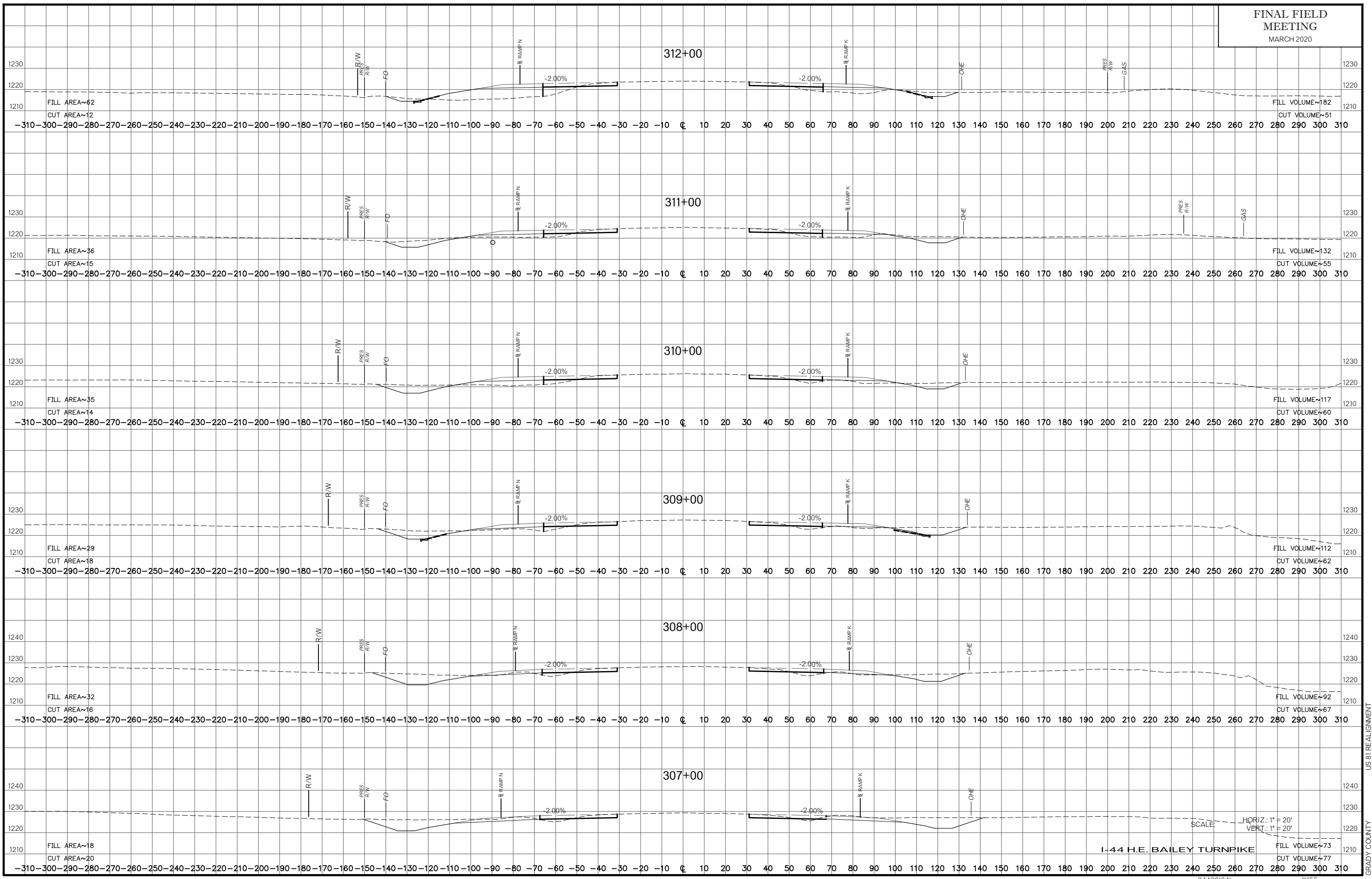
I-44 H.E. BAILEY TURNPIKE

US 81 REALIGNMENT
GRADY COUNTY





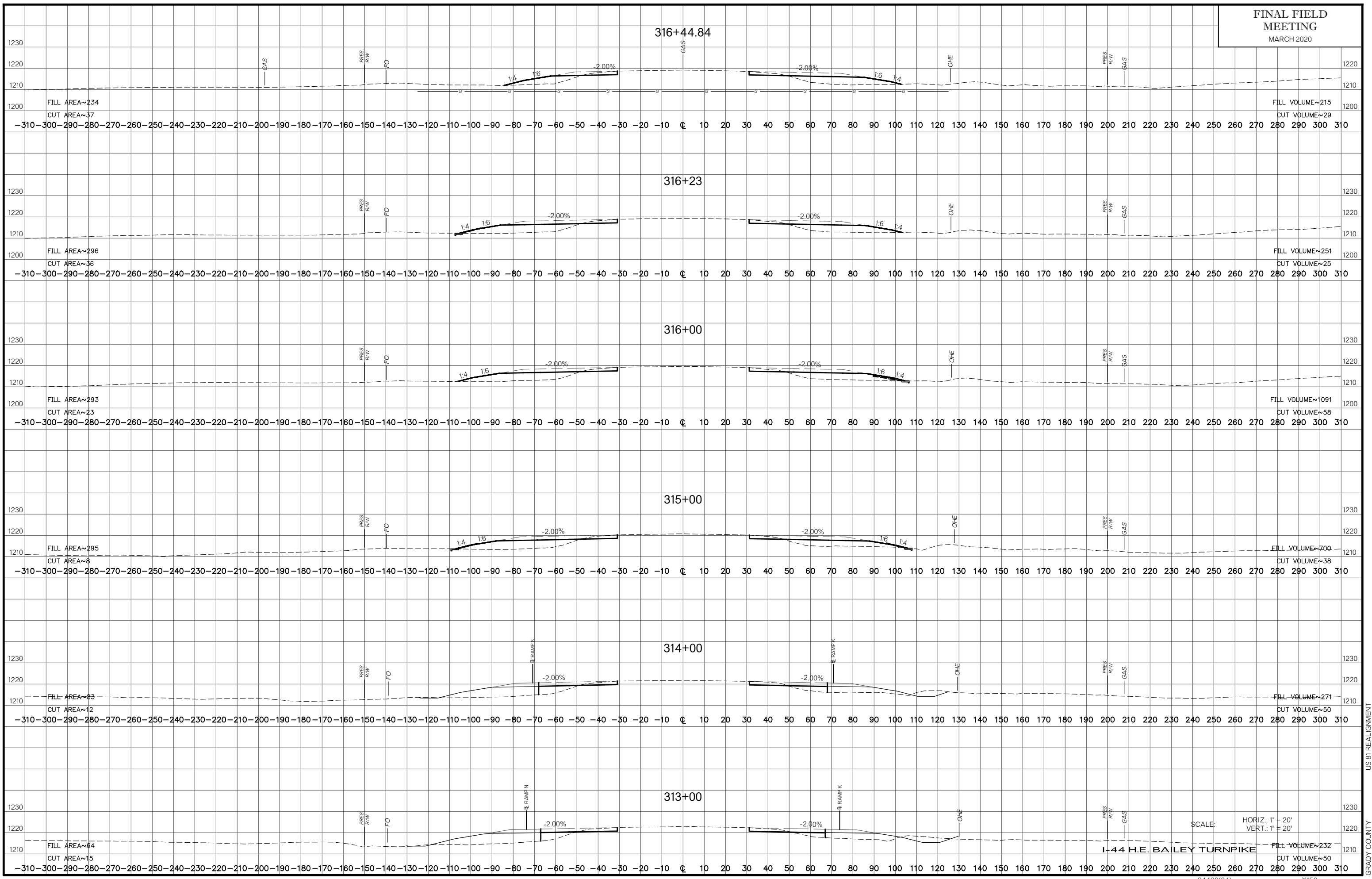




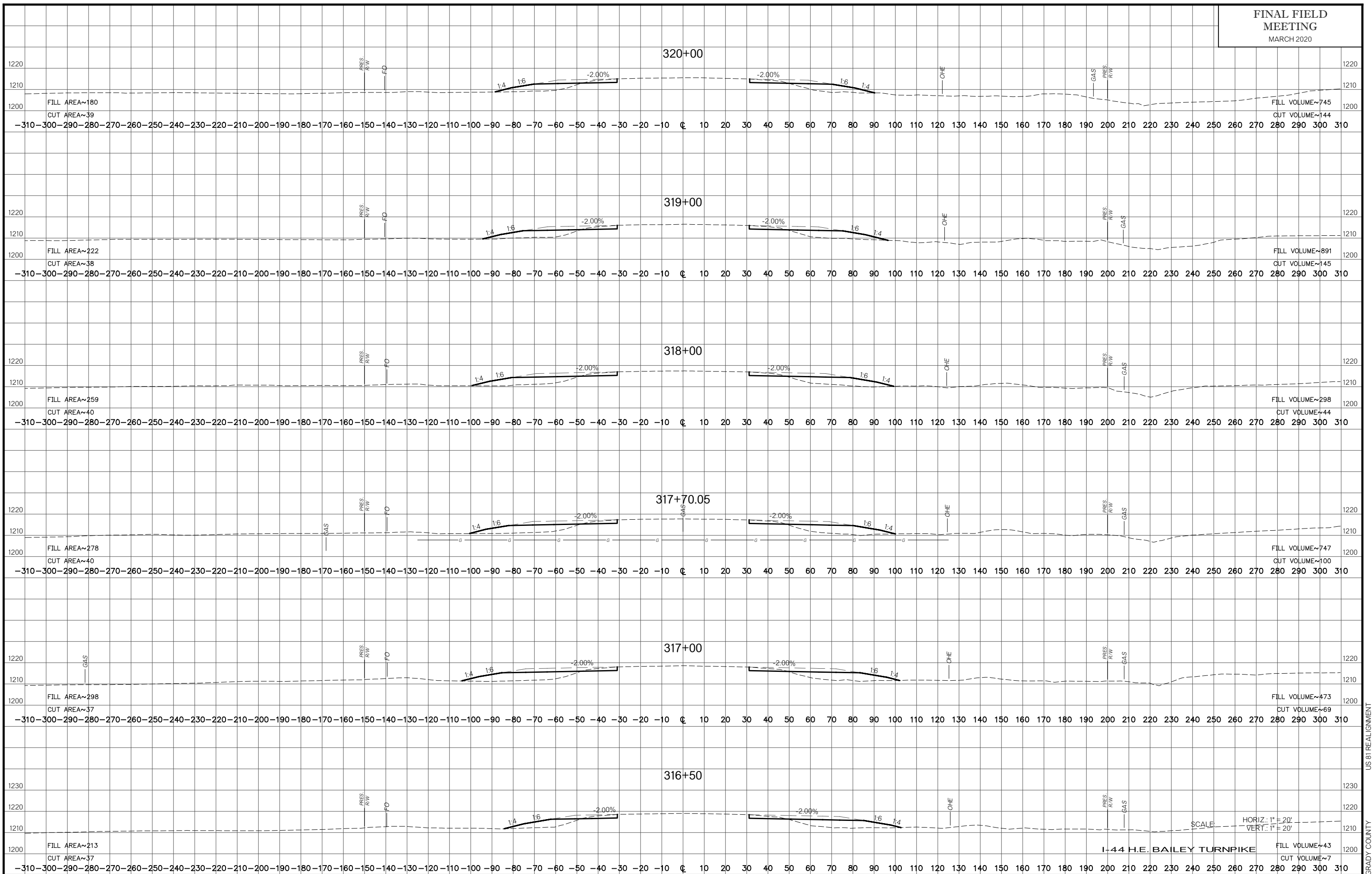
SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

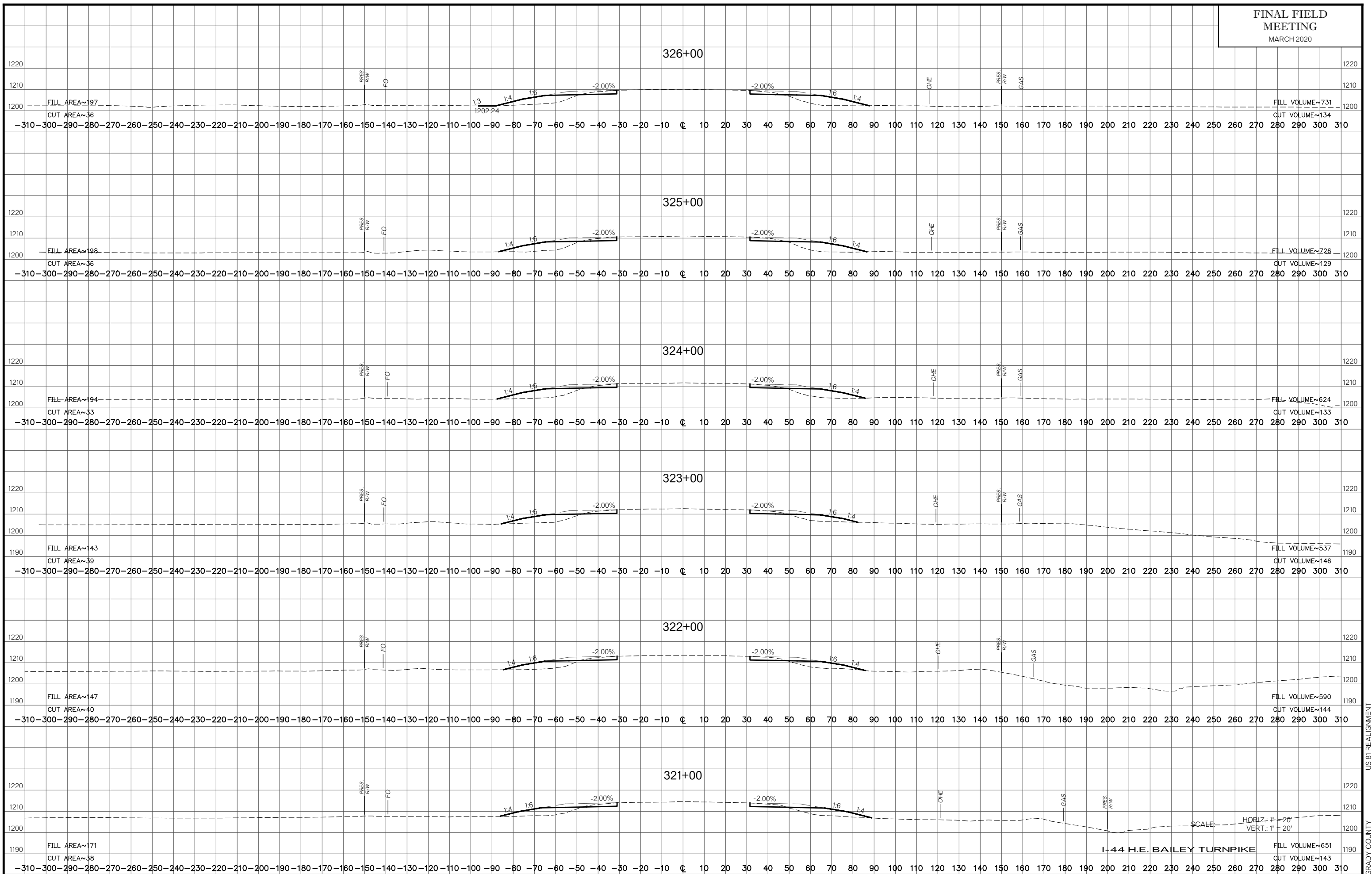
I-44 H.E. BAILEY TURNPIKE

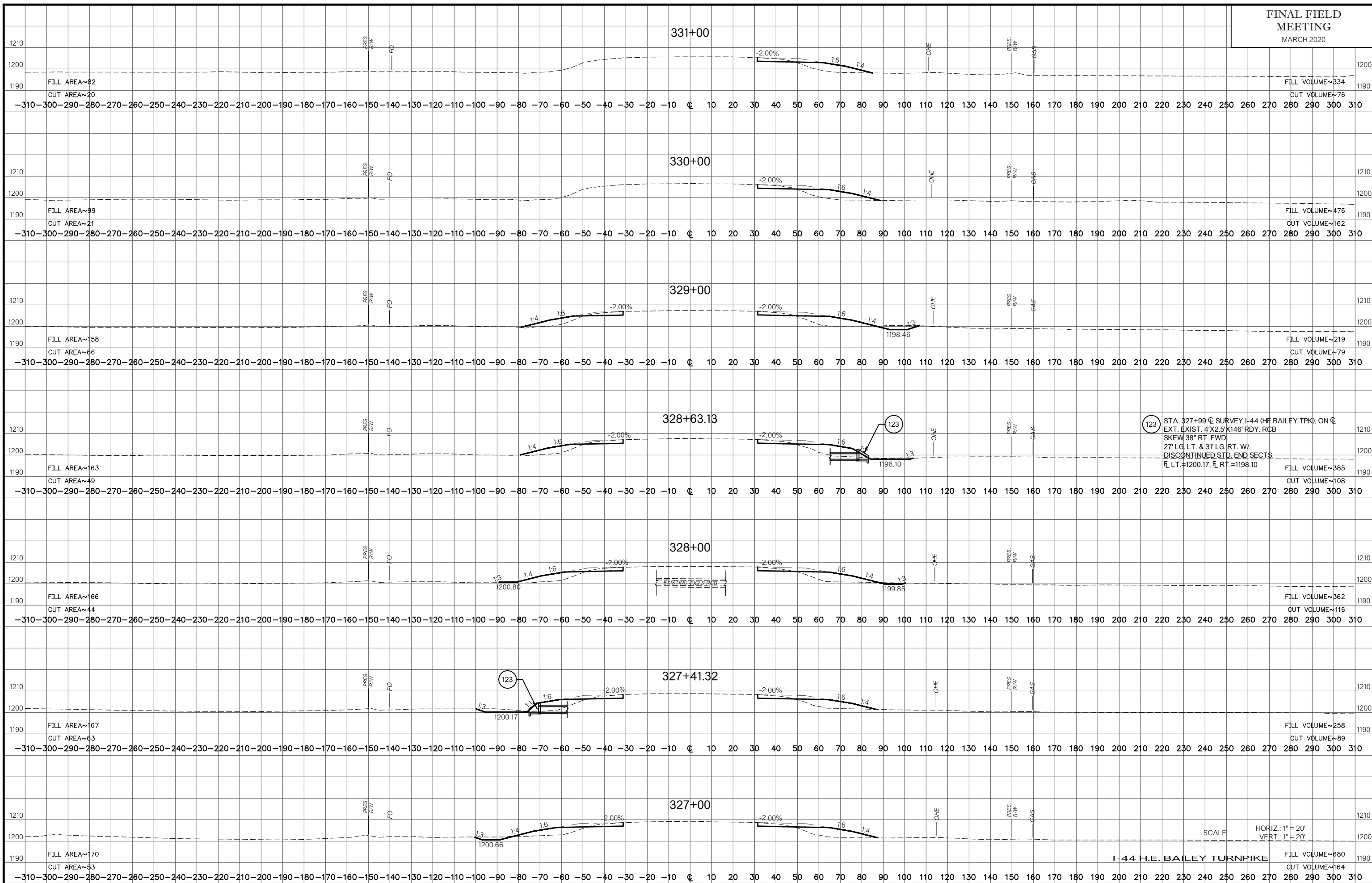
US 81 REALIGNMENT
GRADY COUNTY

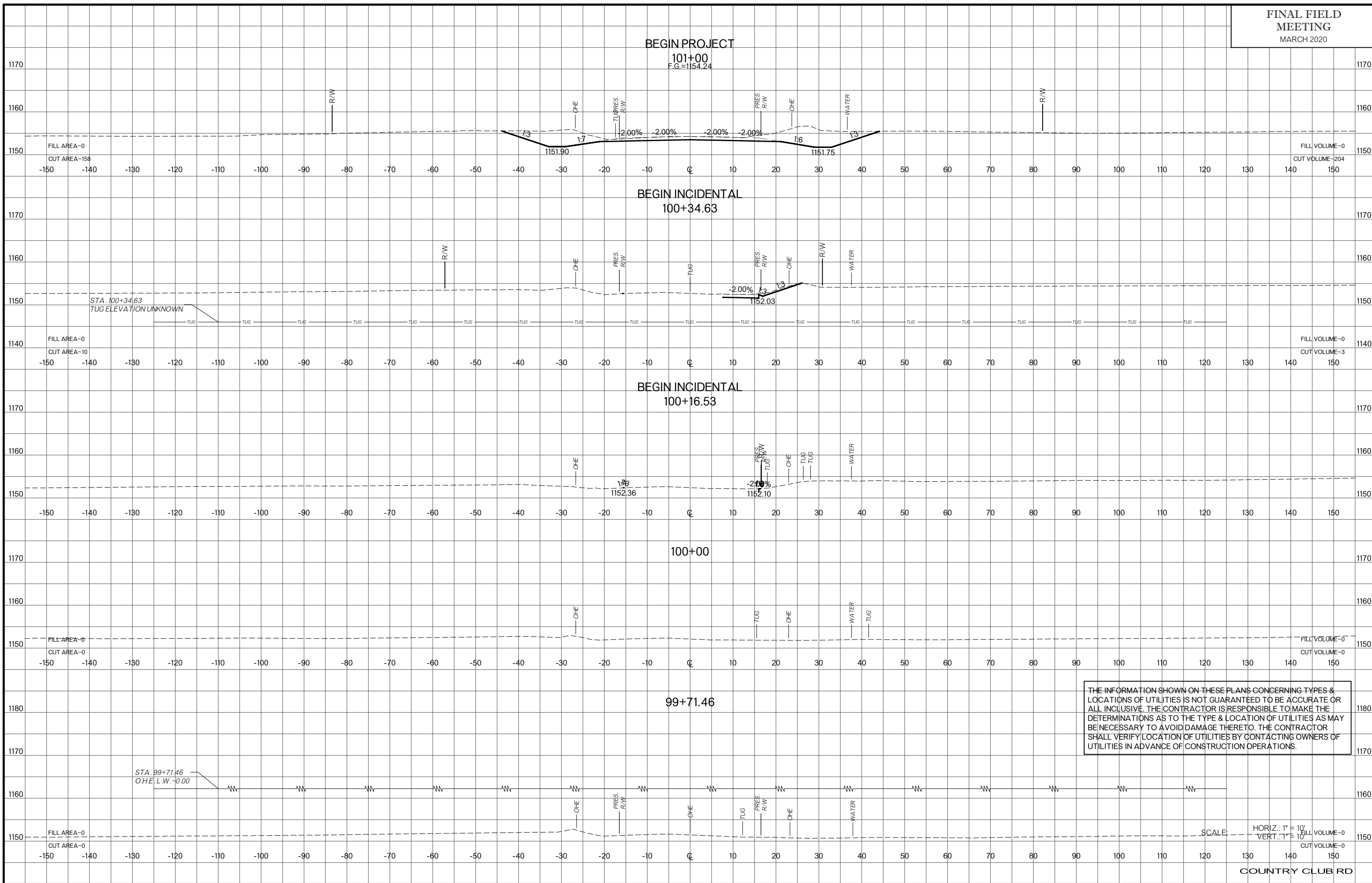


US 81 REALIGNMENT
GRADY COUNTY







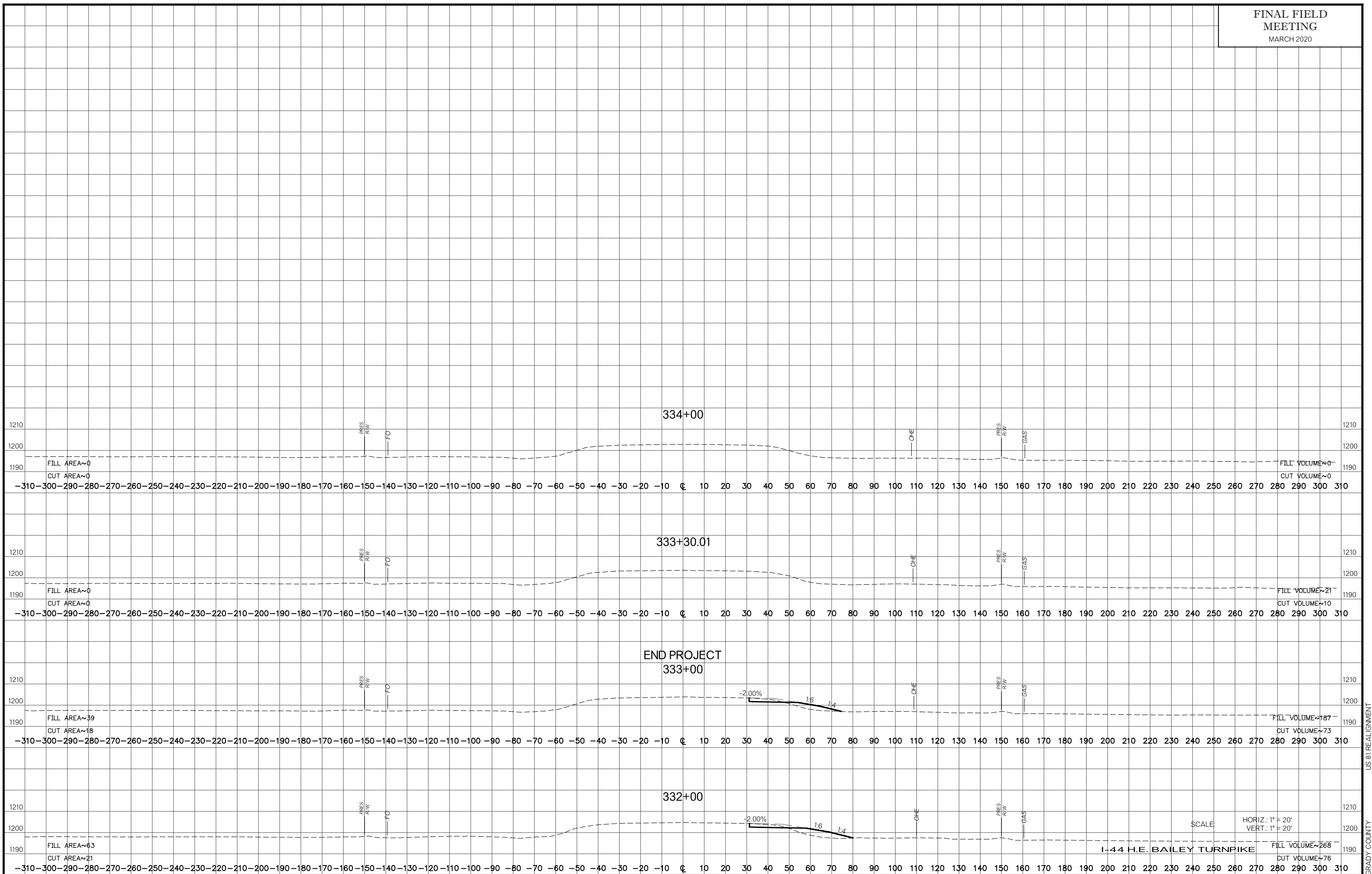


THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTRY CLUB RD

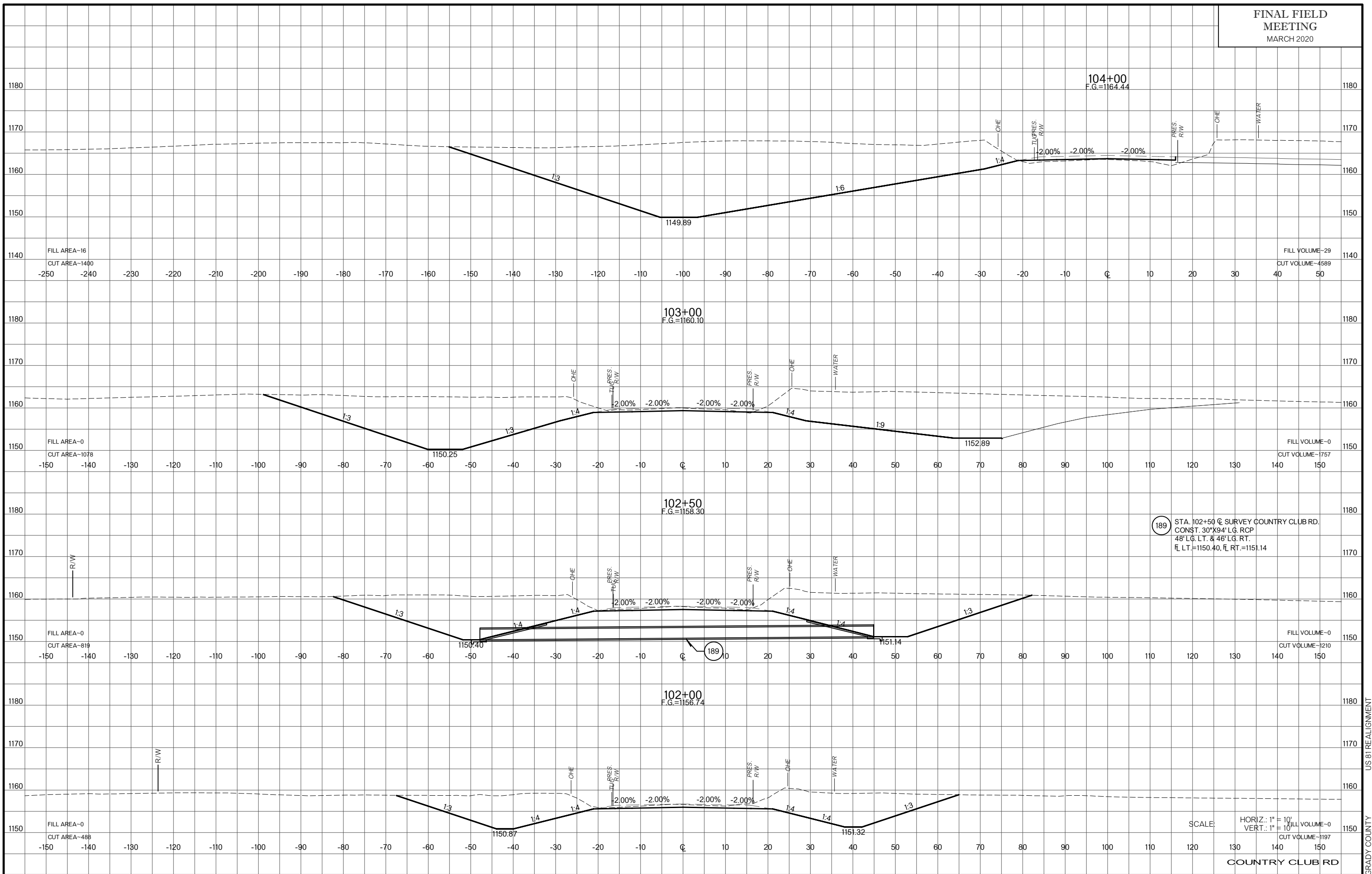
US 81 REALIGNMENT
GRADY COUNTY



SCALE
HORIZ.: 1" = 20'
VERT.: 1" = 20'

I-44 H.E. BAILEY TURNPIKE

US 81 REALIGNMENT
GRADY COUNTY



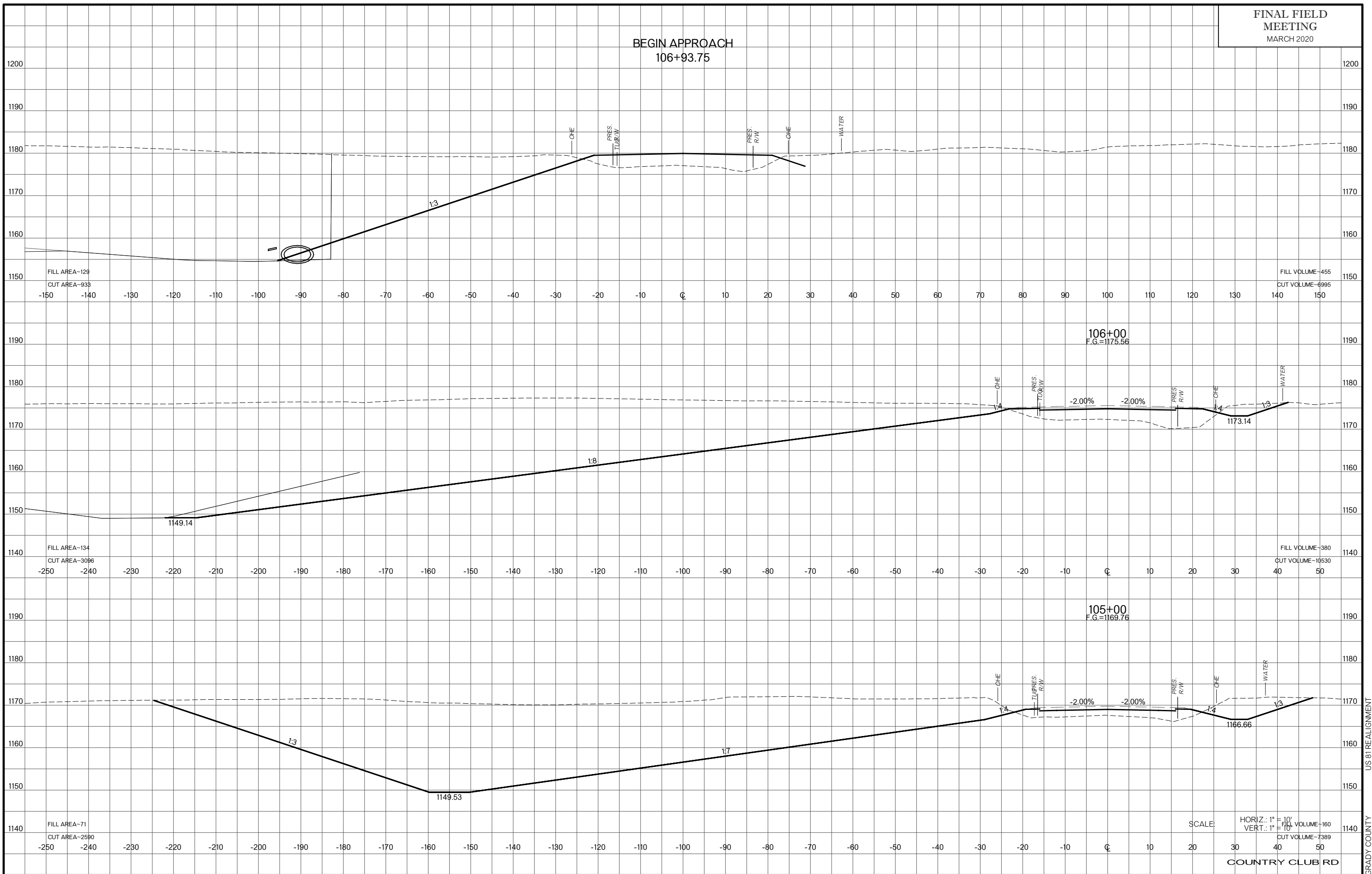
189 STA. 102+50 C SURVEY COUNTRY CLUB RD.
CONST. 30"X94' LG. RCP
48' LG. LT. & 46' LG. RT.
FL LT.=1150.40, FL RT.=1151.14

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTRY CLUB RD

US 81 REALIGNMENT
GRADY COUNTY

BEGIN APPROACH
106+93.75

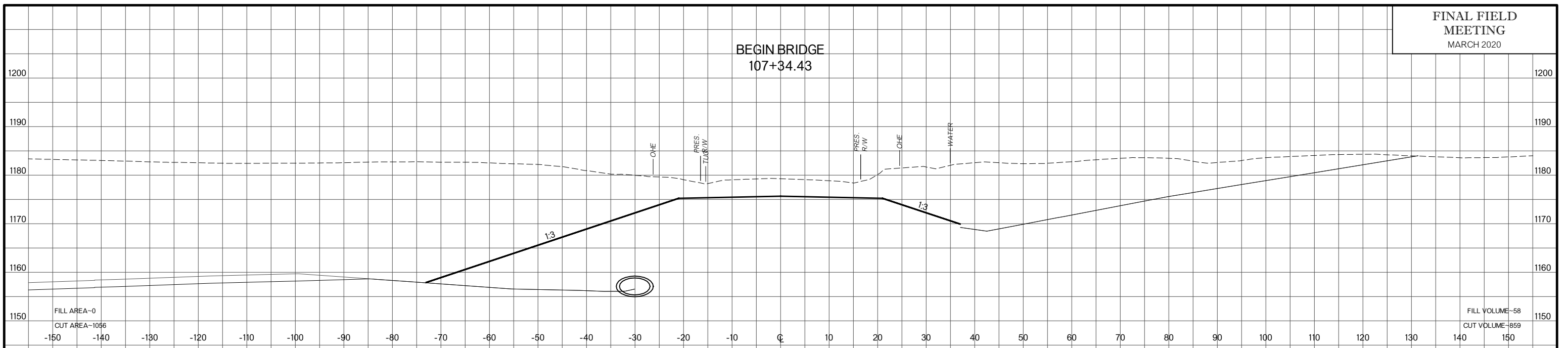


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

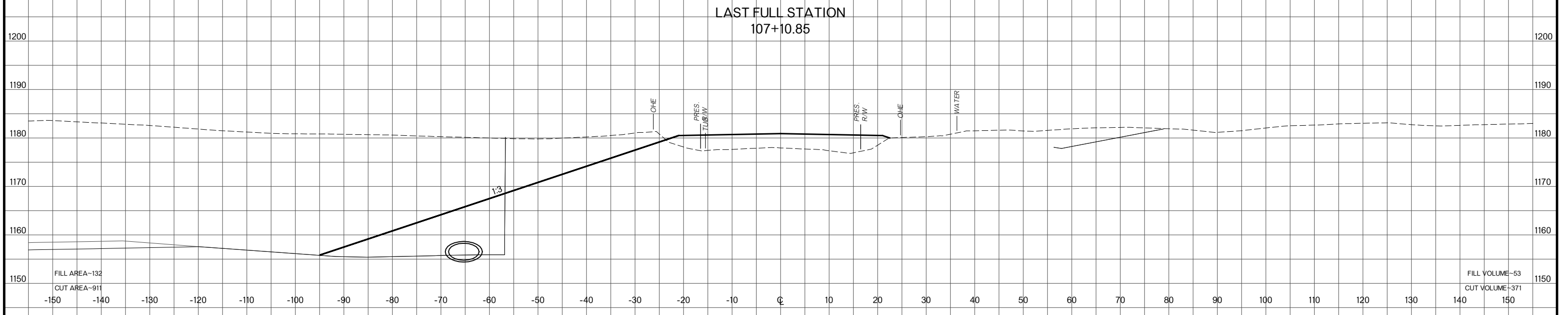
COUNTRY CLUB RD

US 81 REALIGNMENT
GRADY COUNTY

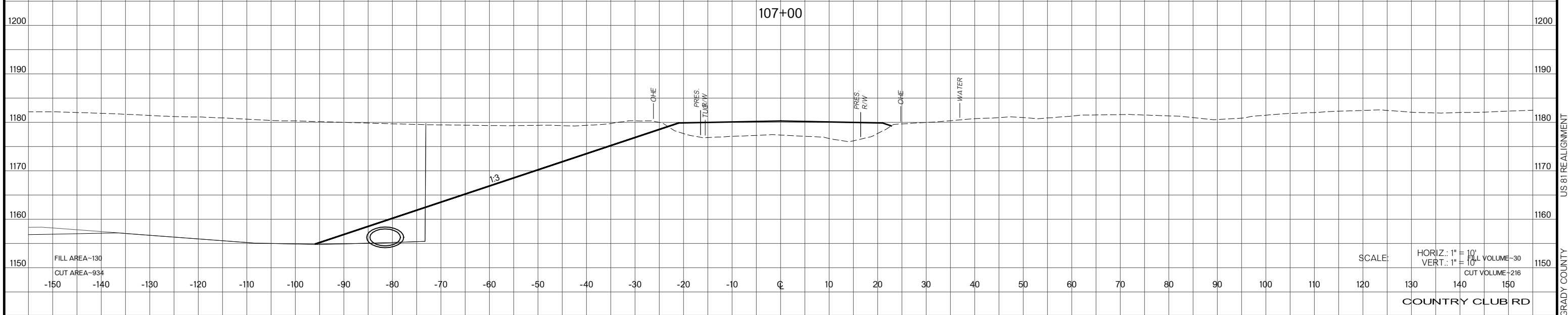
BEGIN BRIDGE
107+34.43



LAST FULL STATION
107+10.85



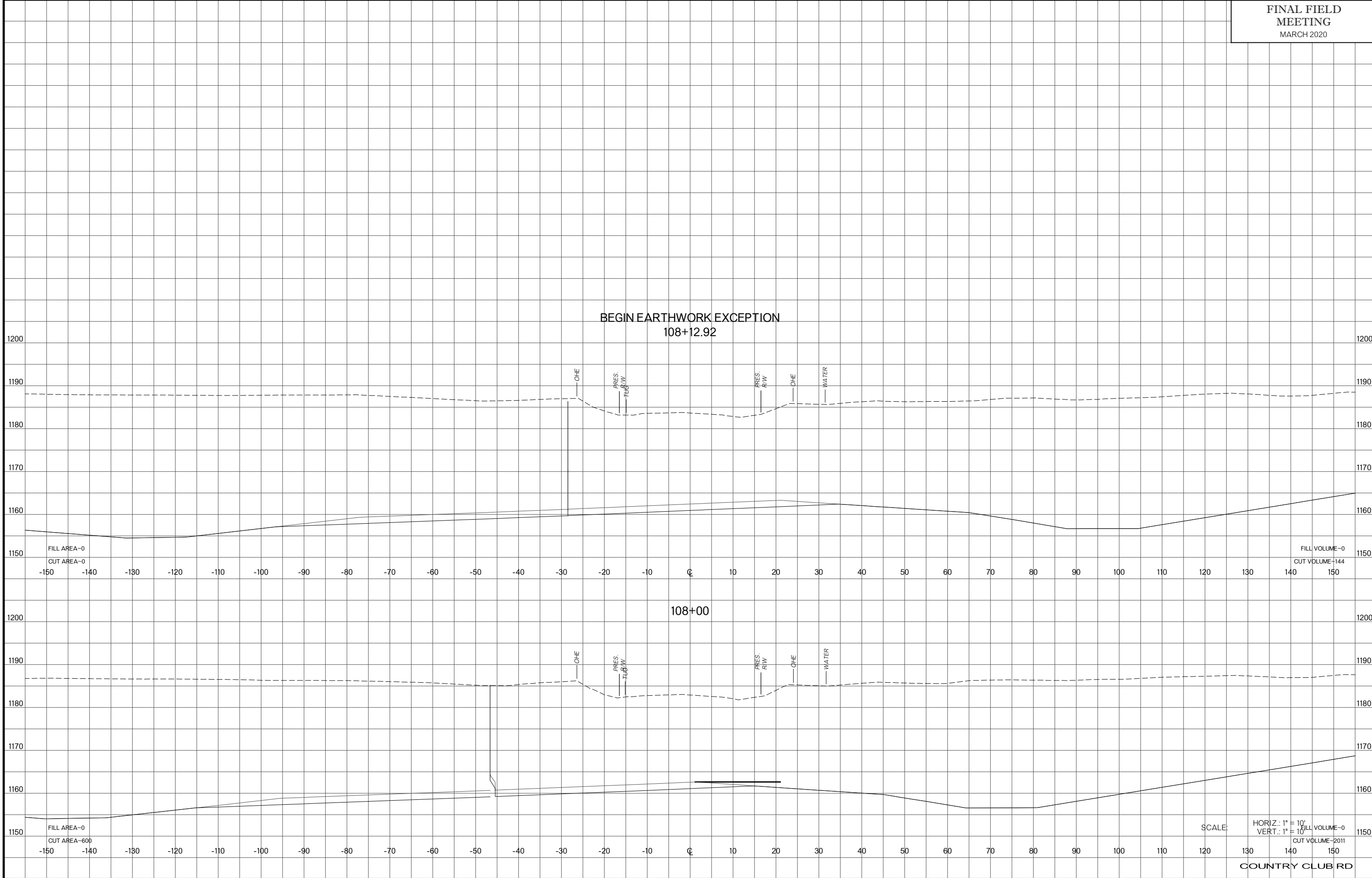
107+00

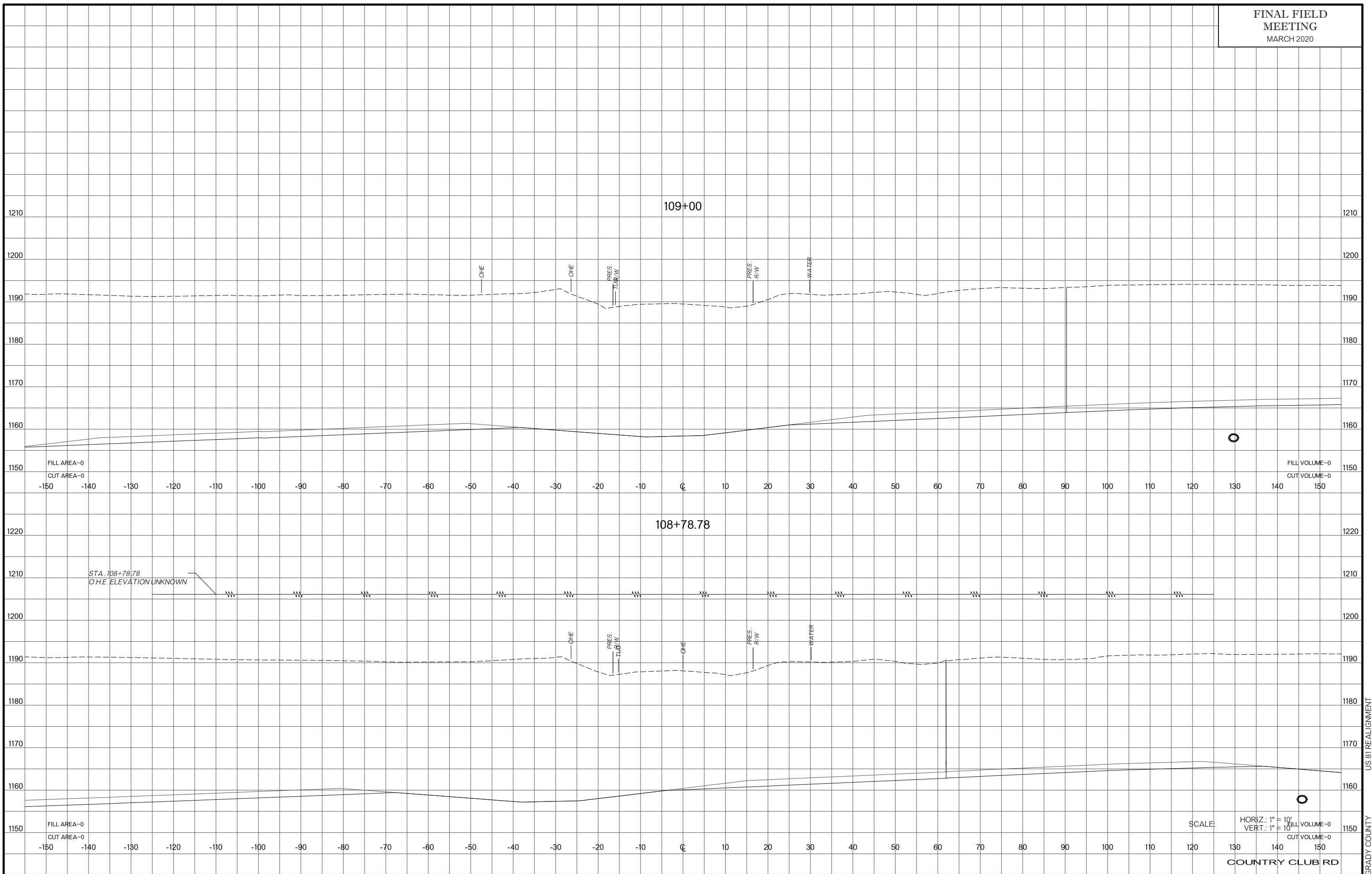


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTRY CLUB RD

US 81 REALIGNMENT
GRADY COUNTY



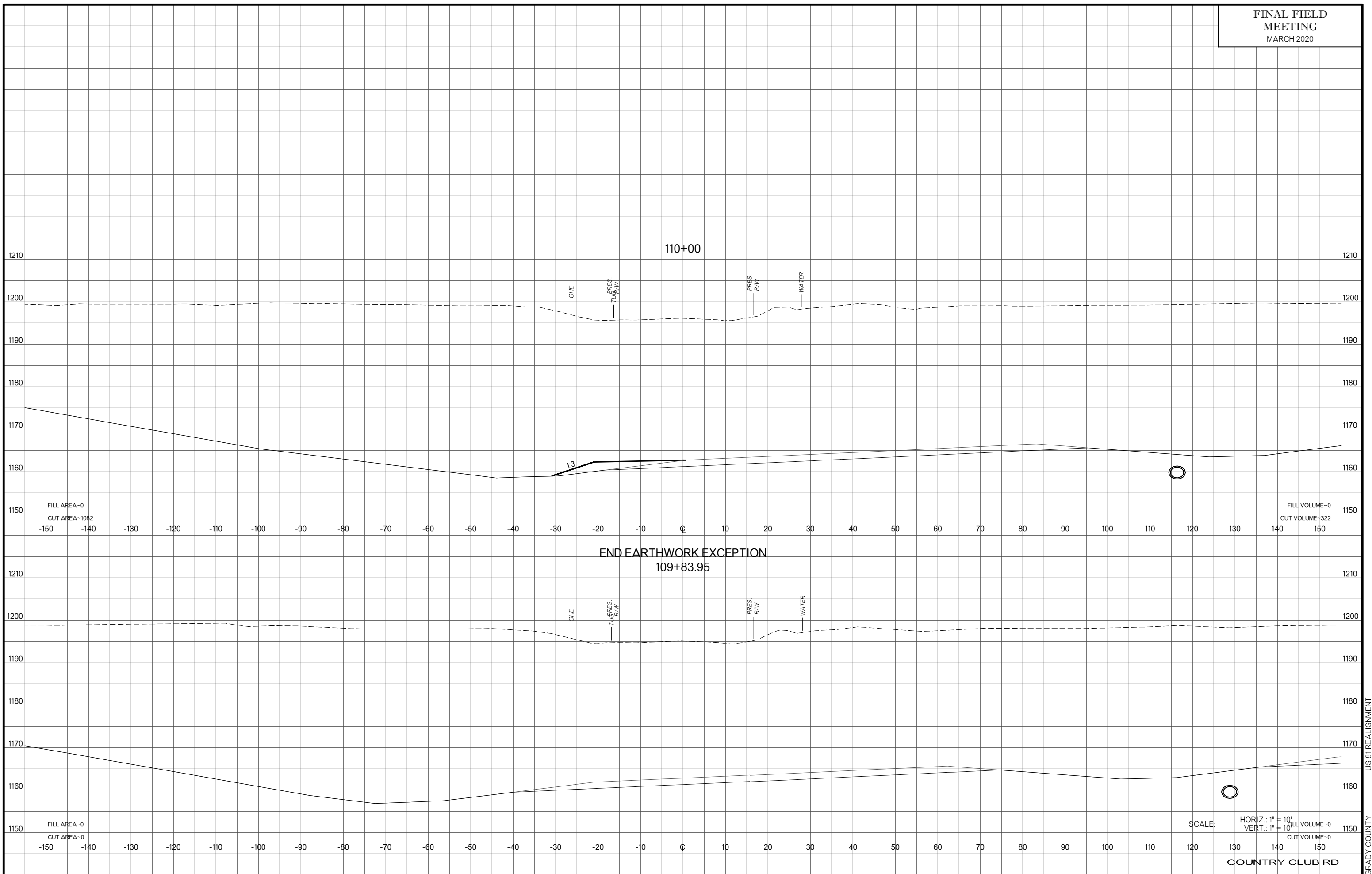


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

FILL VOLUME=0
CUT VOLUME=0

COUNTRY CLUB RD

US 81 REALIGNMENT
GRADY COUNTY

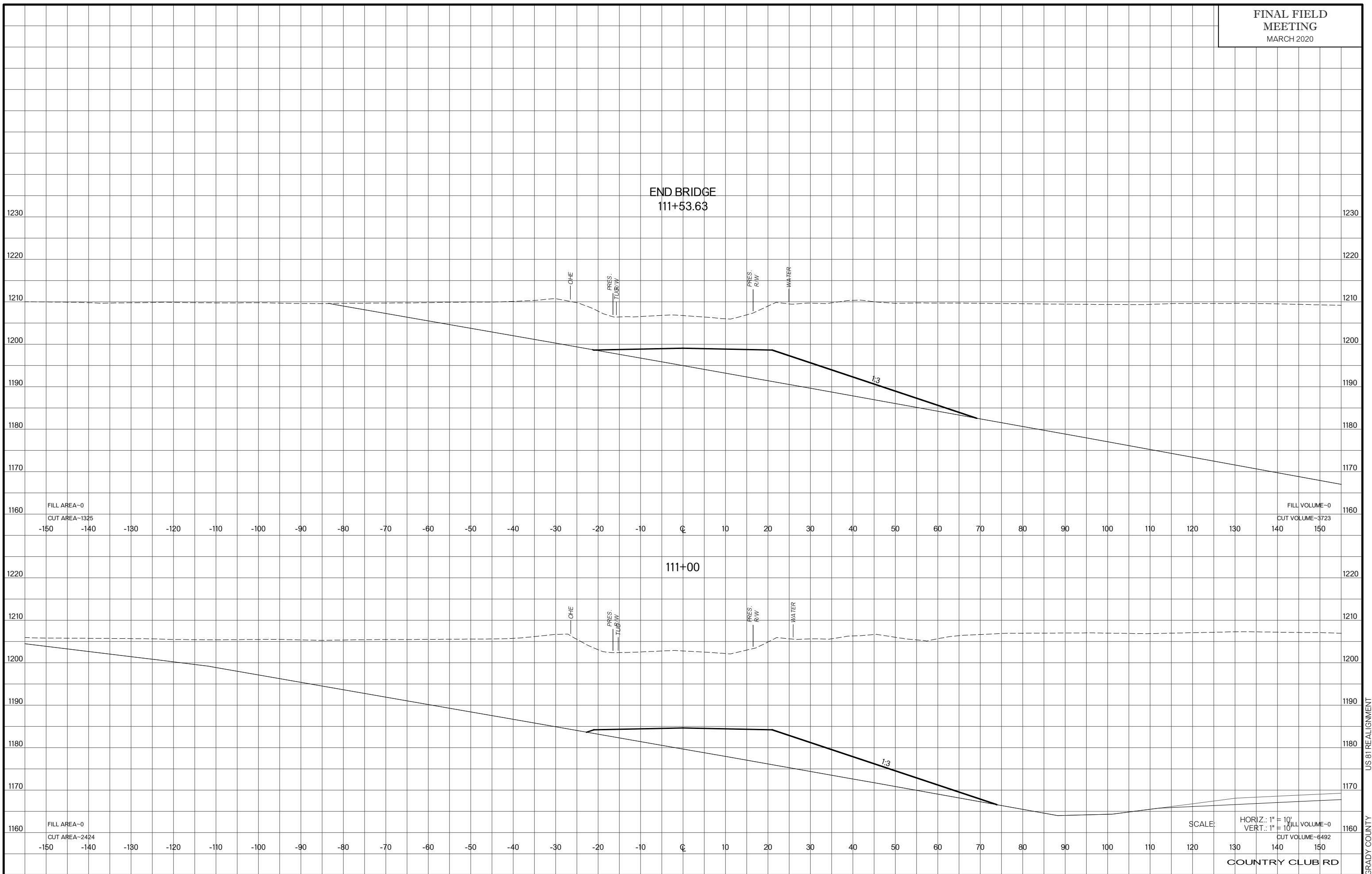


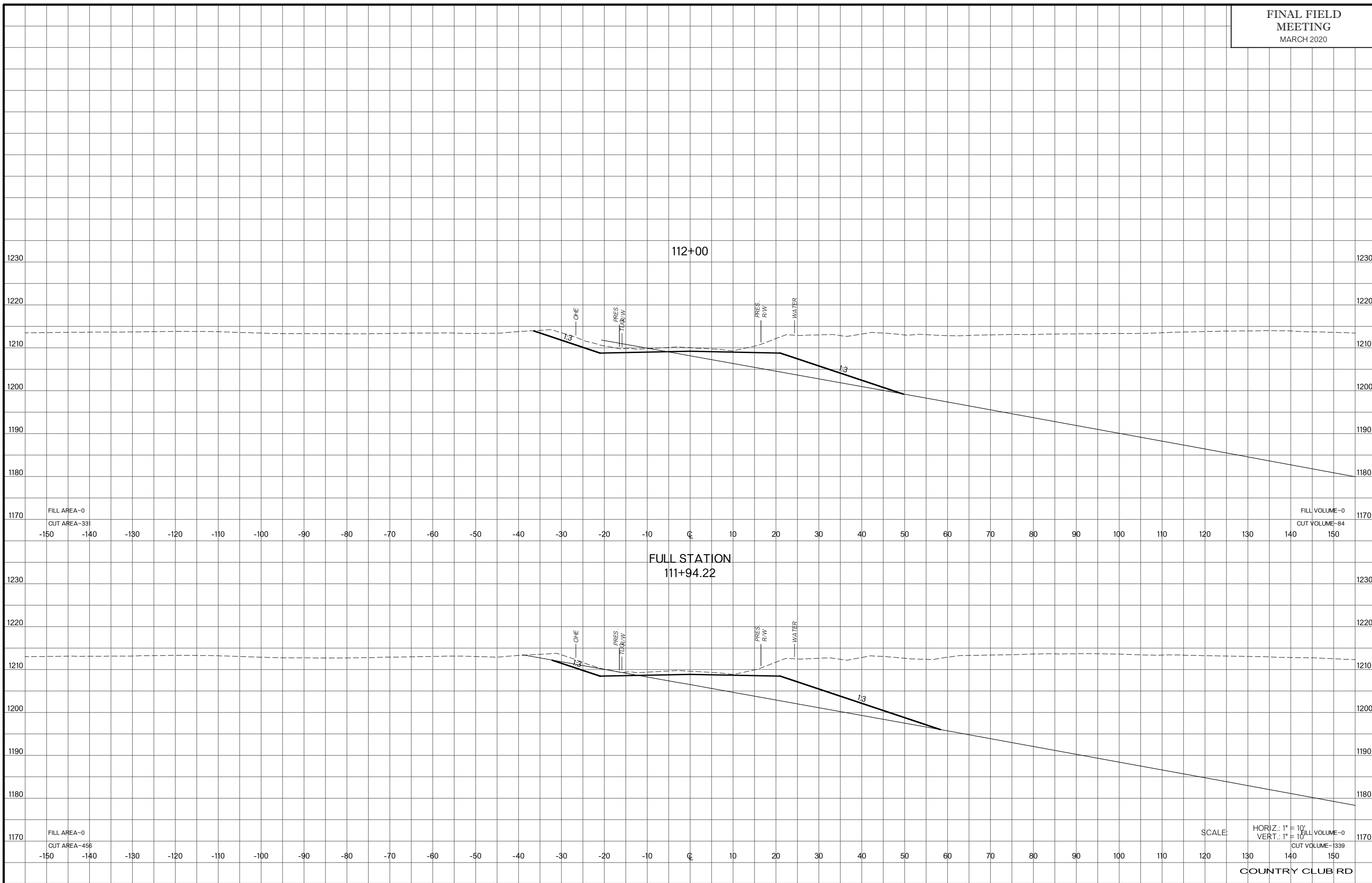
END EARTHWORK EXCEPTION
109+83.95

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTRY CLUB RD

US 81 REALIGNMENT
GRADY COUNTY

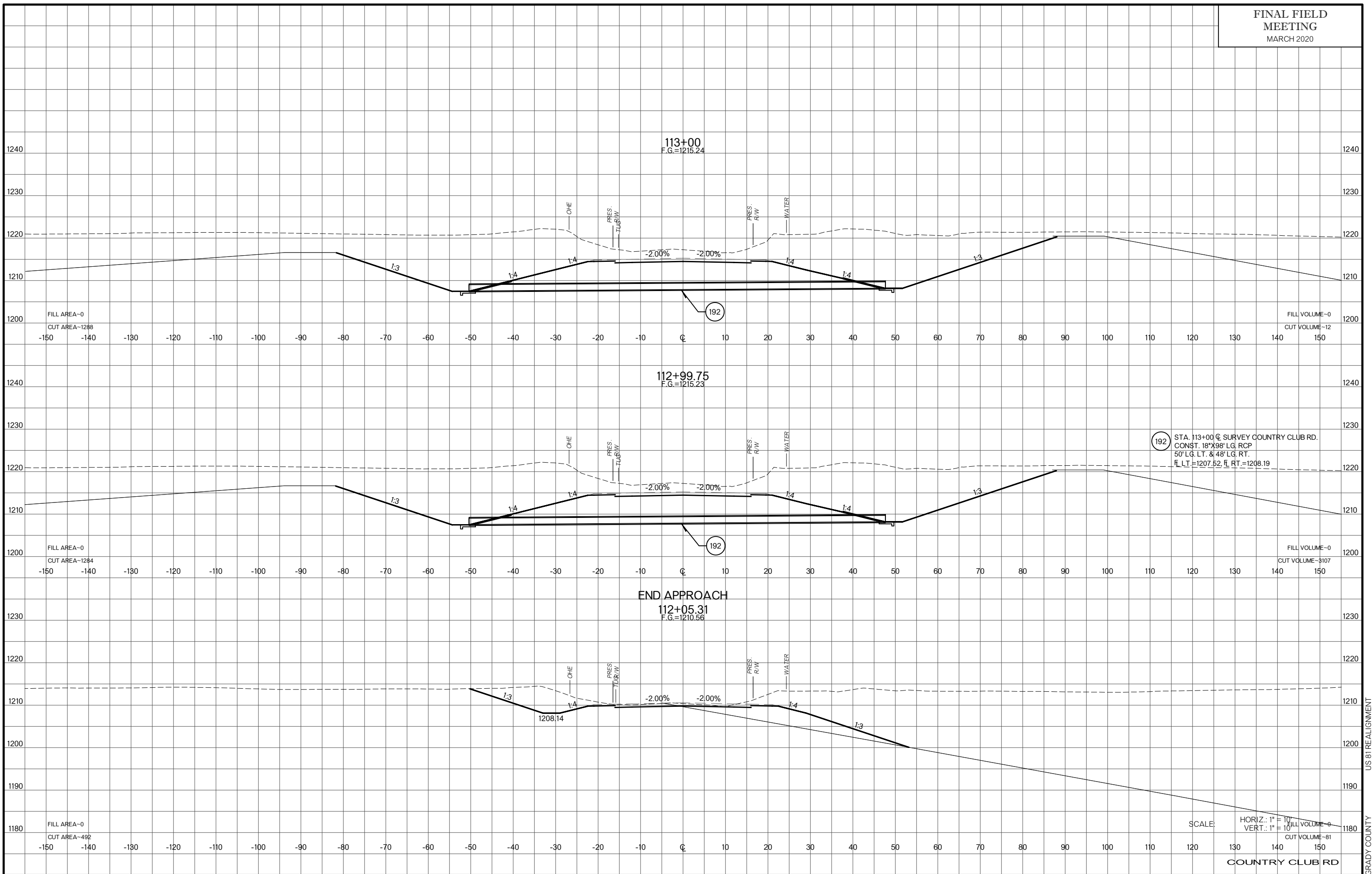




SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTRY CLUB RD

US 81 REALIGNMENT
GRADY COUNTY



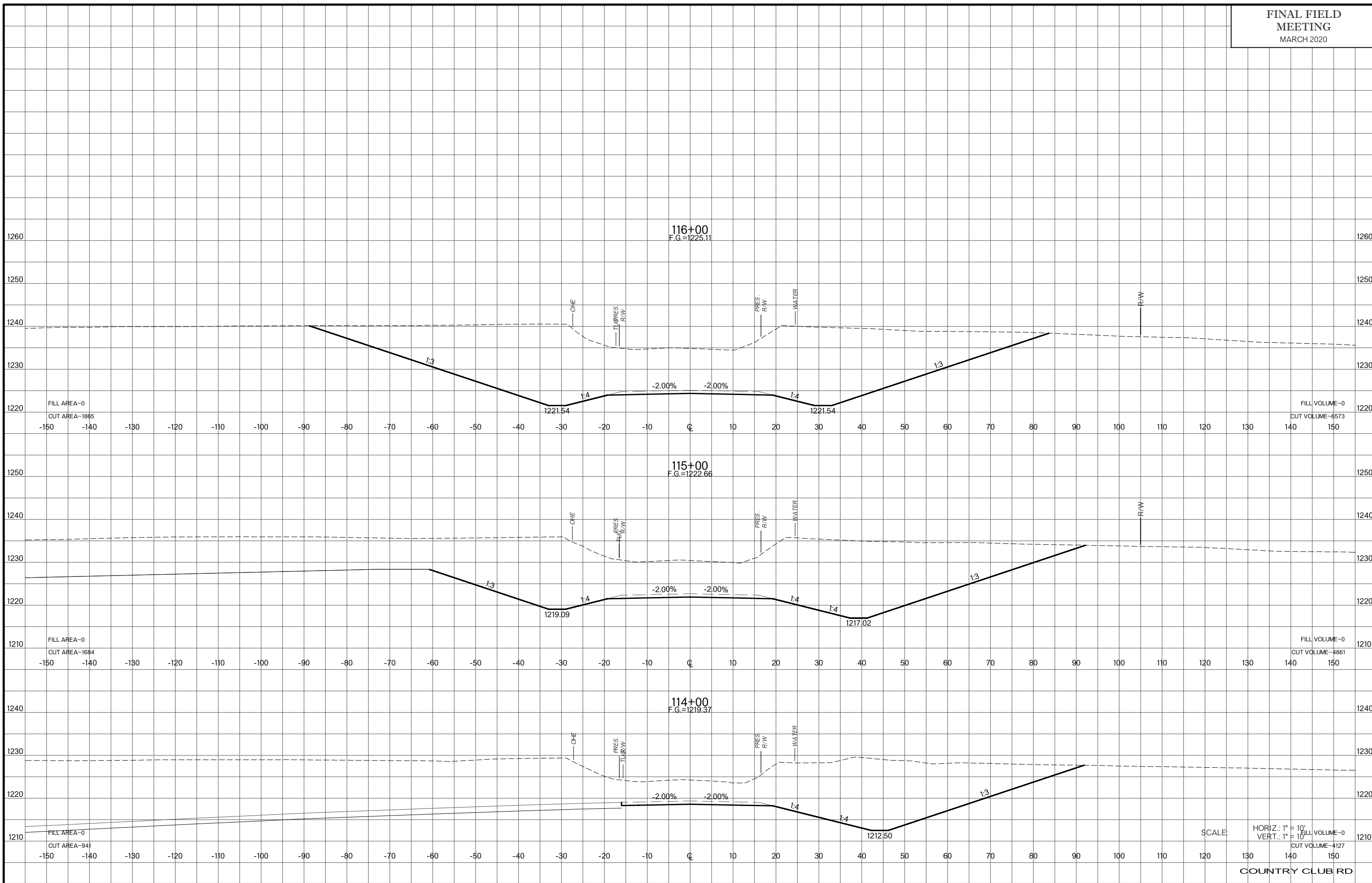
192 STA. 113+00 @ SURVEY COUNTRY CLUB RD.
CONST. 18'X98' LG. RCP
50' LG. LT. & 48' LG. RT.
FL LT.=1207.52, FL RT.=1208.19

END APPROACH

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTRY CLUB RD

US 81 REALIGNMENT
GRADY COUNTY



FILL AREA-0
CUT AREA-1865

FILL VOLUME-0
CUT VOLUME-6573

FILL AREA-0
CUT AREA-1684

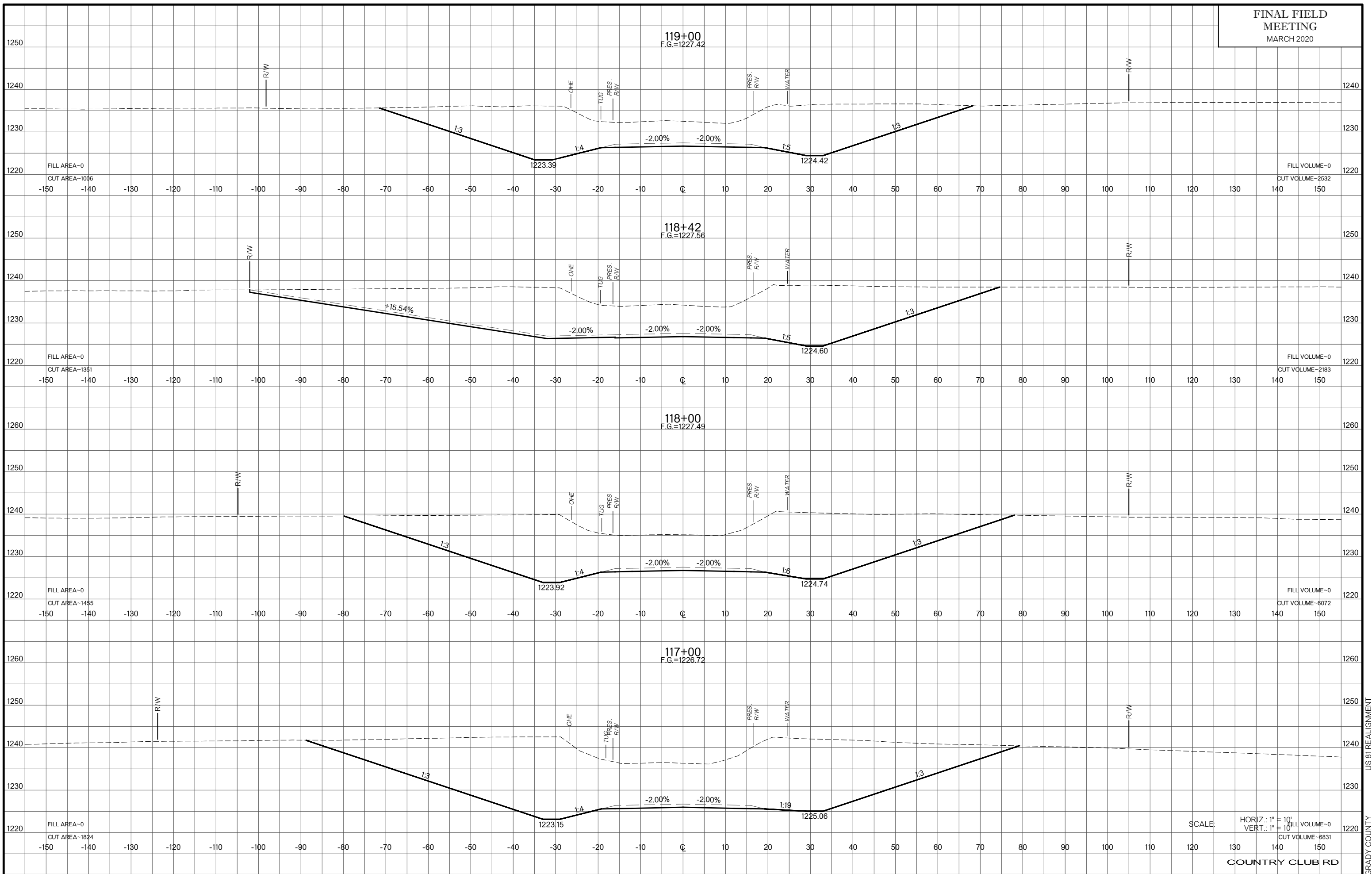
FILL VOLUME-0
CUT VOLUME-4861

FILL AREA-0
CUT AREA-941

FILL VOLUME-0
CUT VOLUME-4127

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

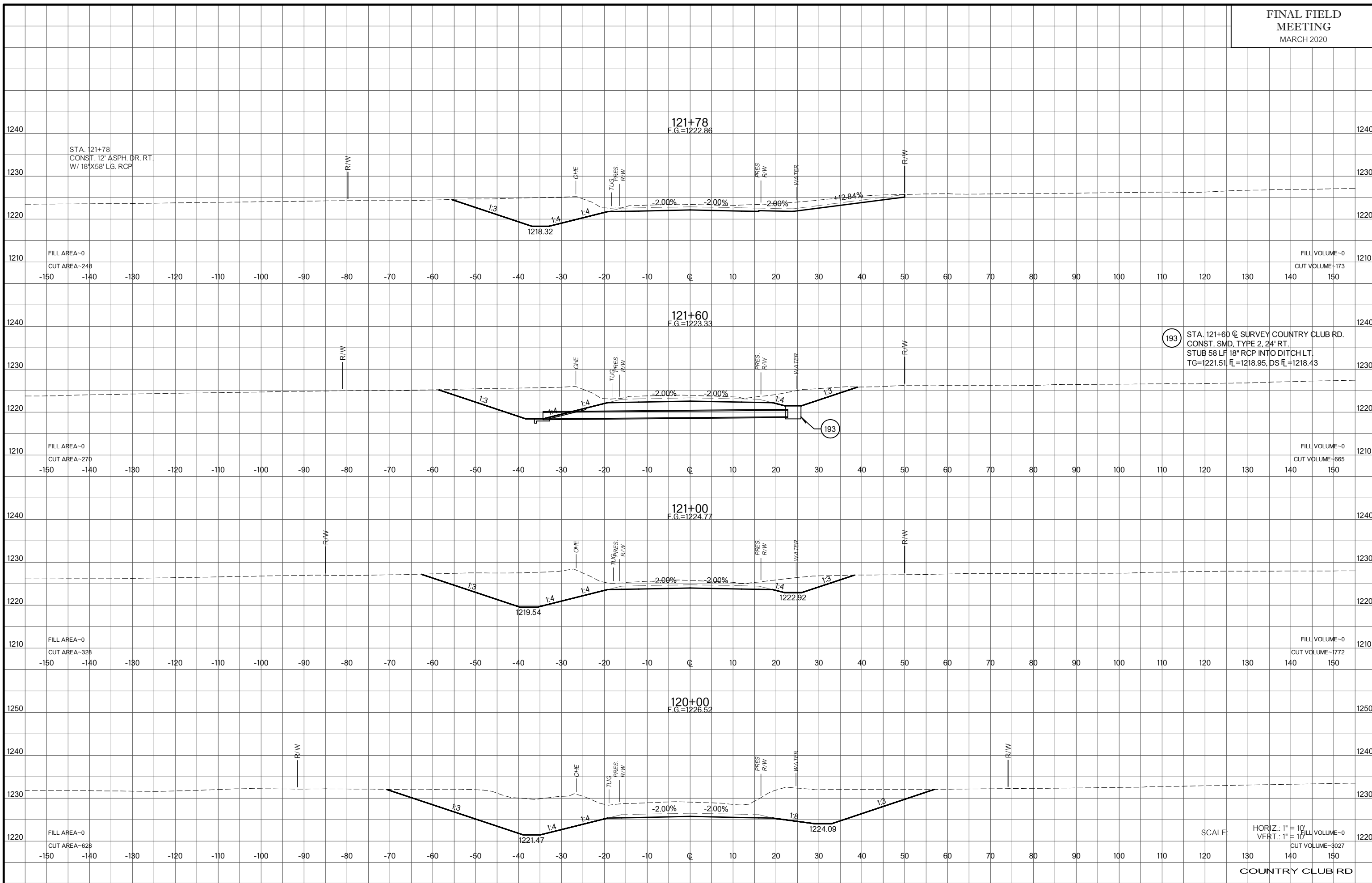
COUNTRY CLUB RD



SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTRY CLUB RD

US 81 REALIGNMENT
GRADY COUNTY

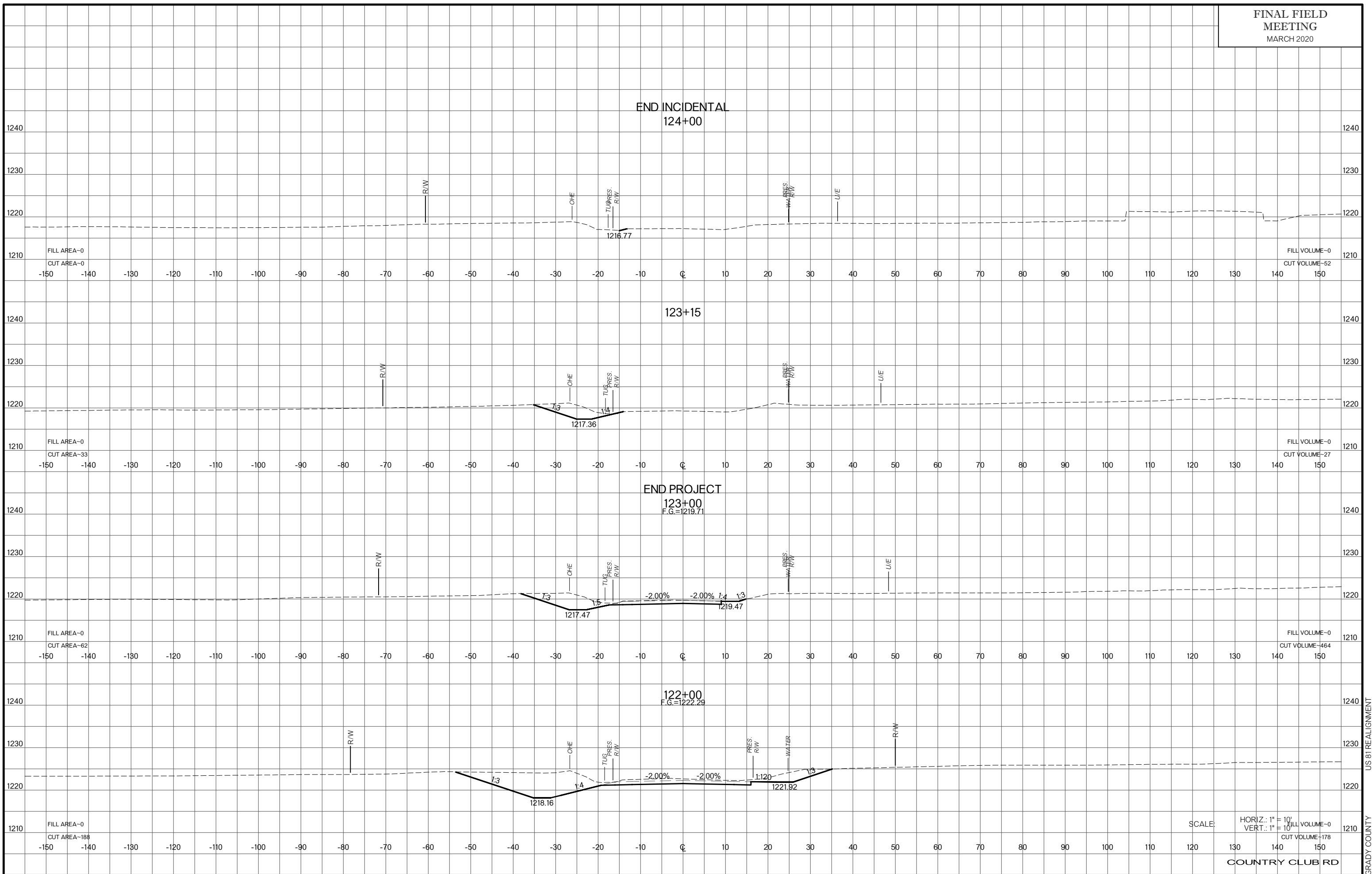


193 STA. 121+60 @ SURVEY COUNTRY CLUB RD.
CONST. SMD. TYPE 2, 24' RT.
STUB 58 LF 18" RCP INTO DITCH LT.
TG=1221.51, FL=1218.95, DS FL=1218.43

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTRY CLUB RD

US 81 REALIGNMENT
GRADY COUNTY



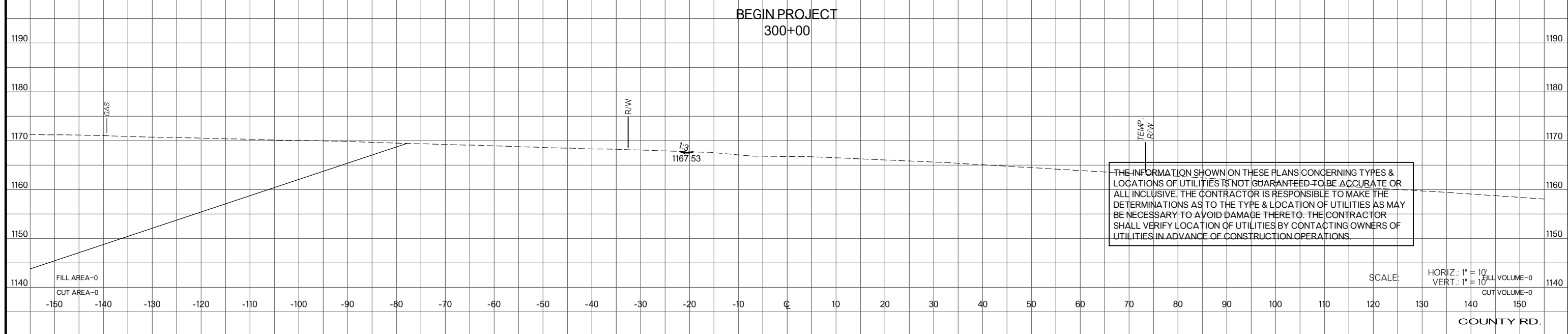
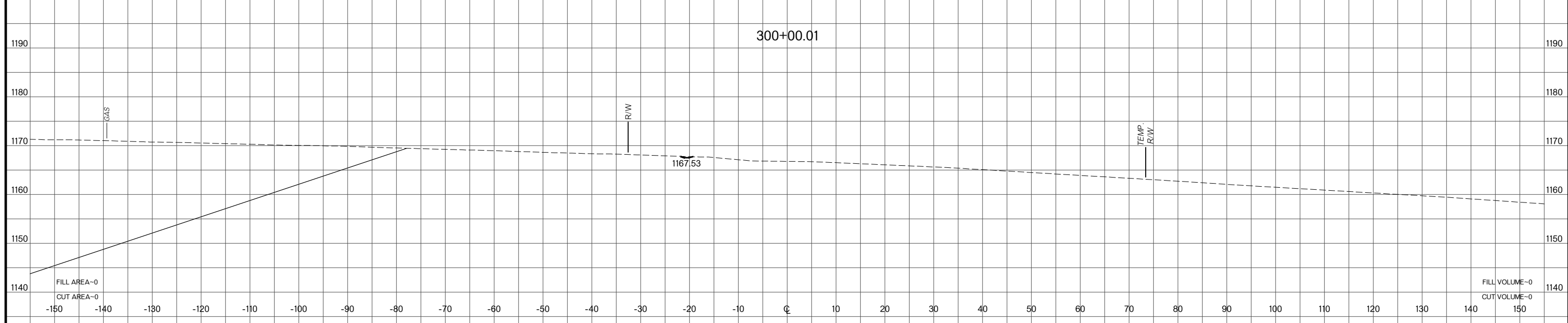
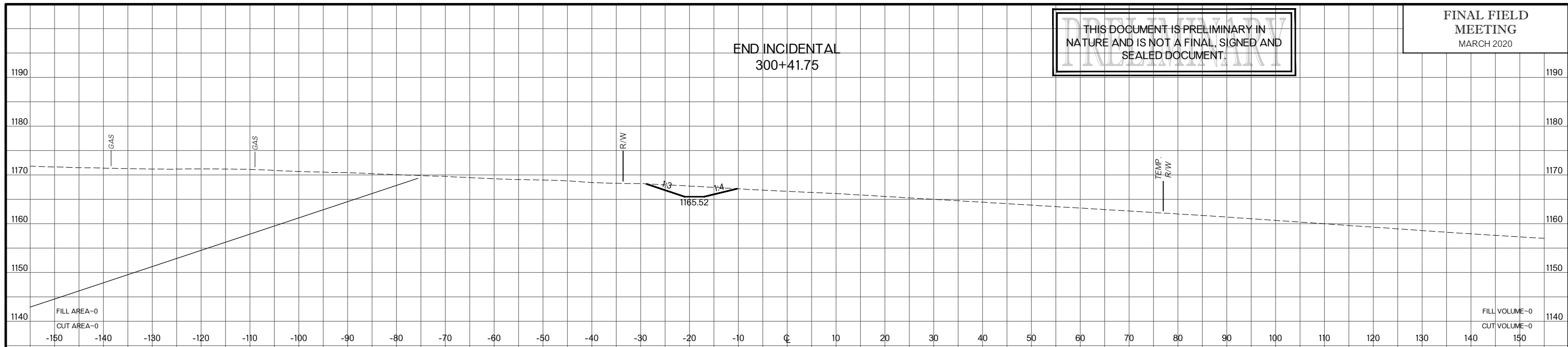
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTRY CLUB RD

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

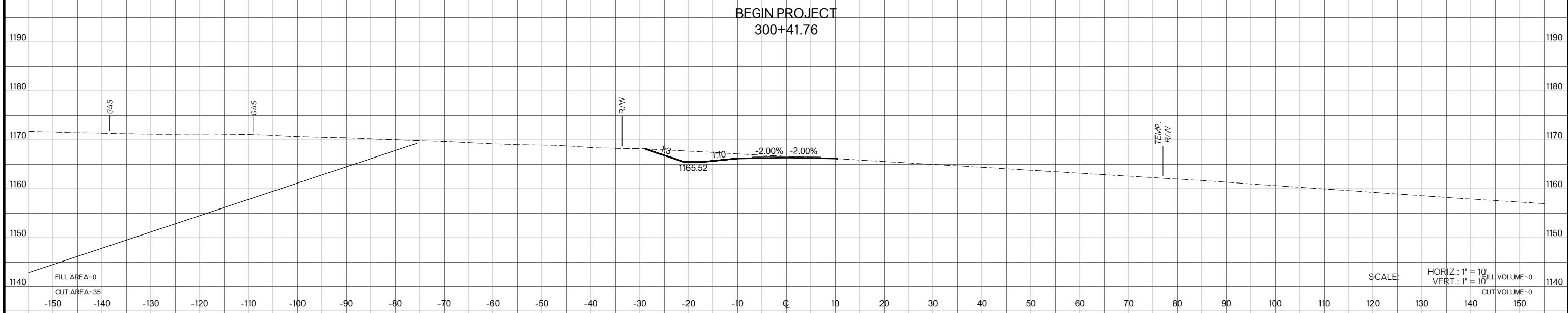
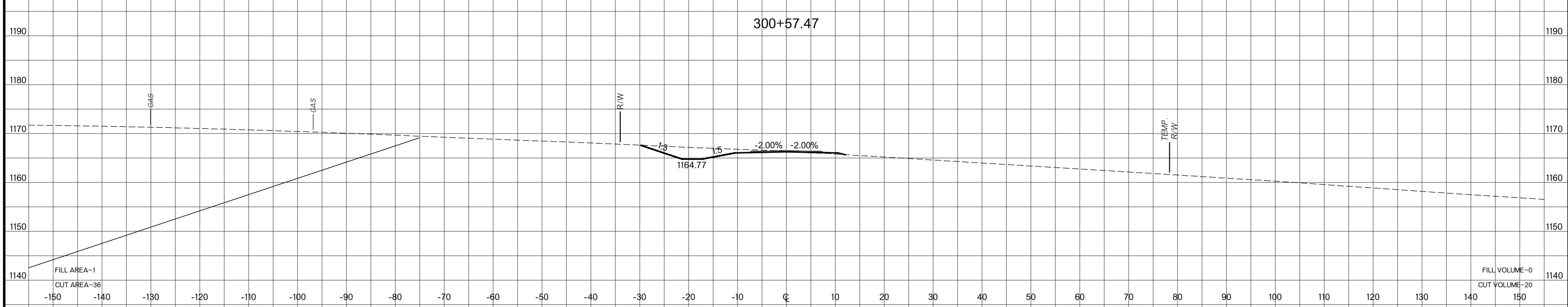
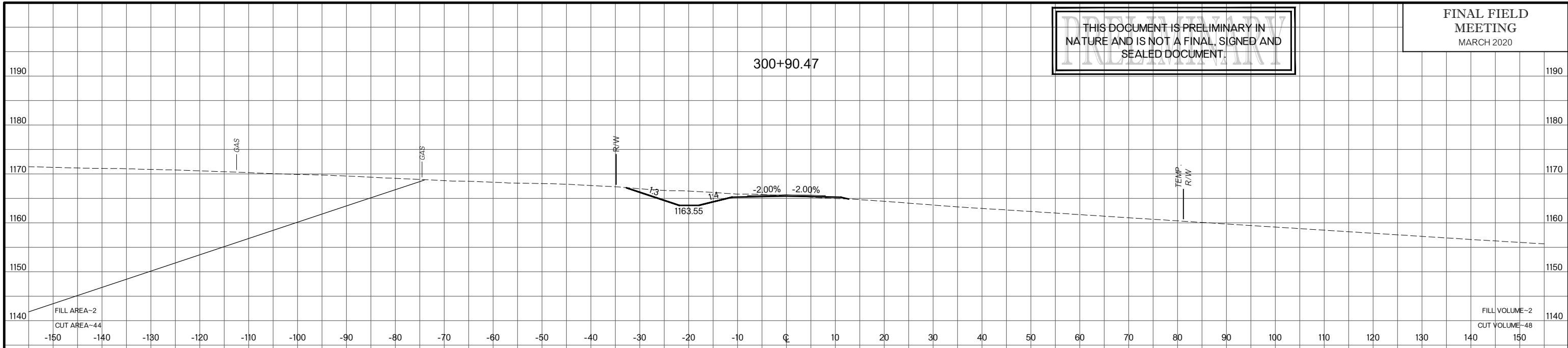
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTY RD.

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



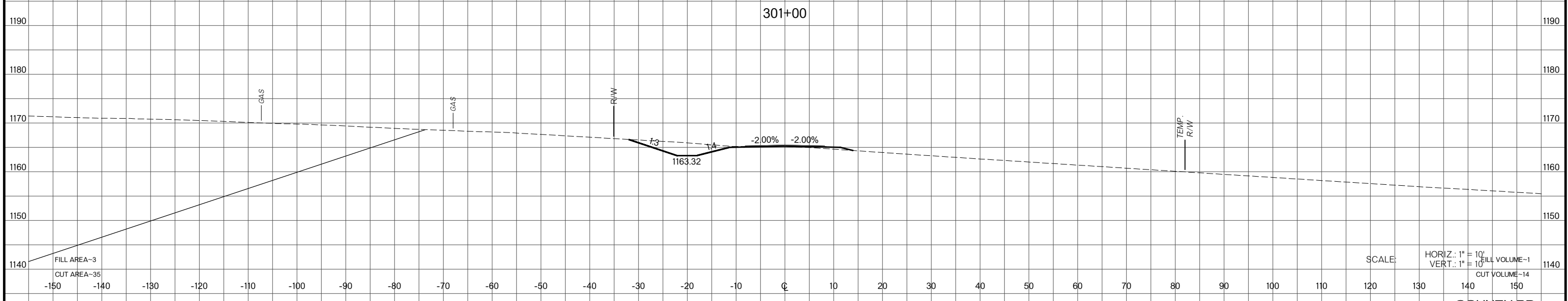
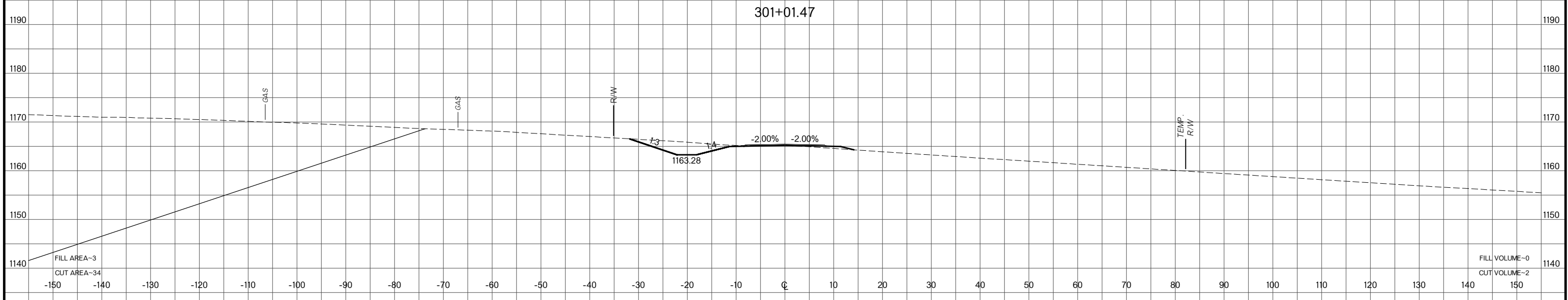
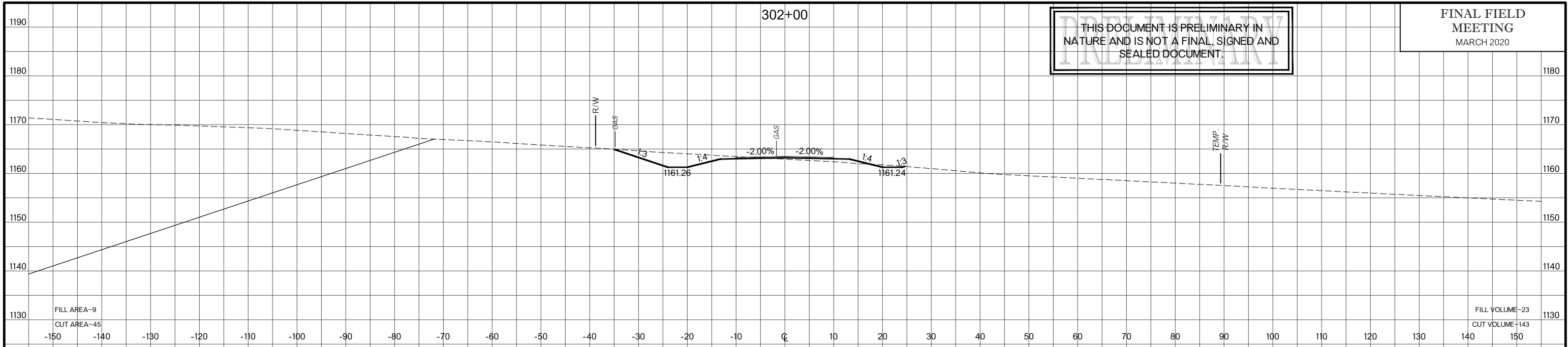
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

COUNTY RD.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

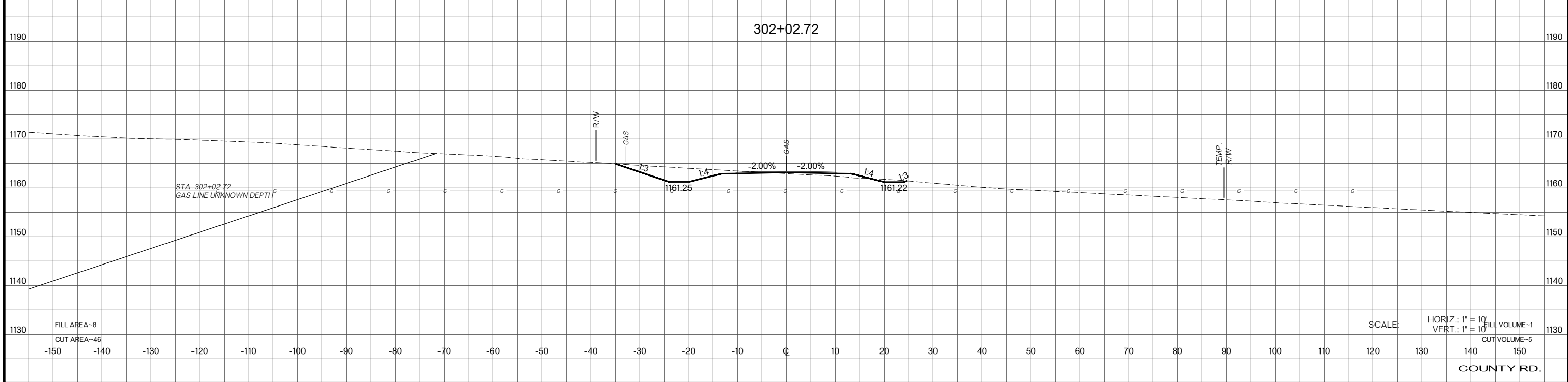


US 81 REALIGNMENT
GRADY COUNTY

COUNTY RD.

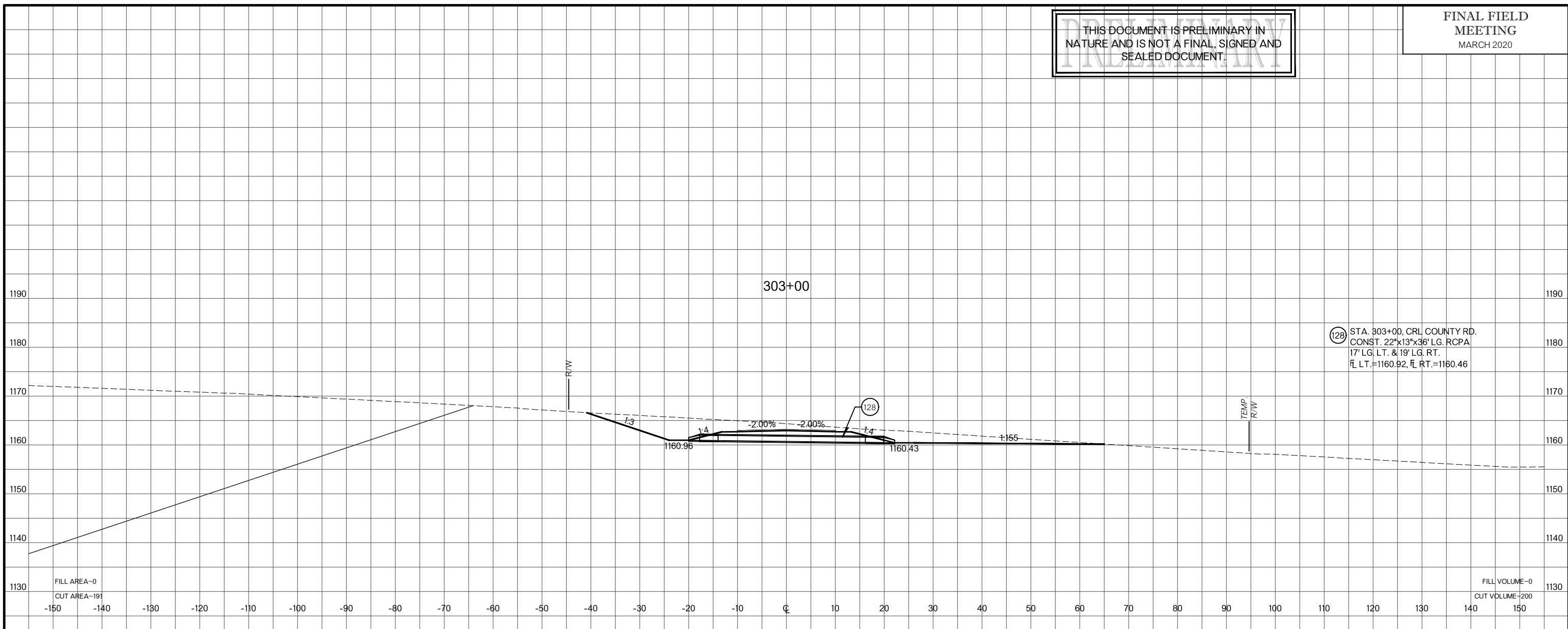
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

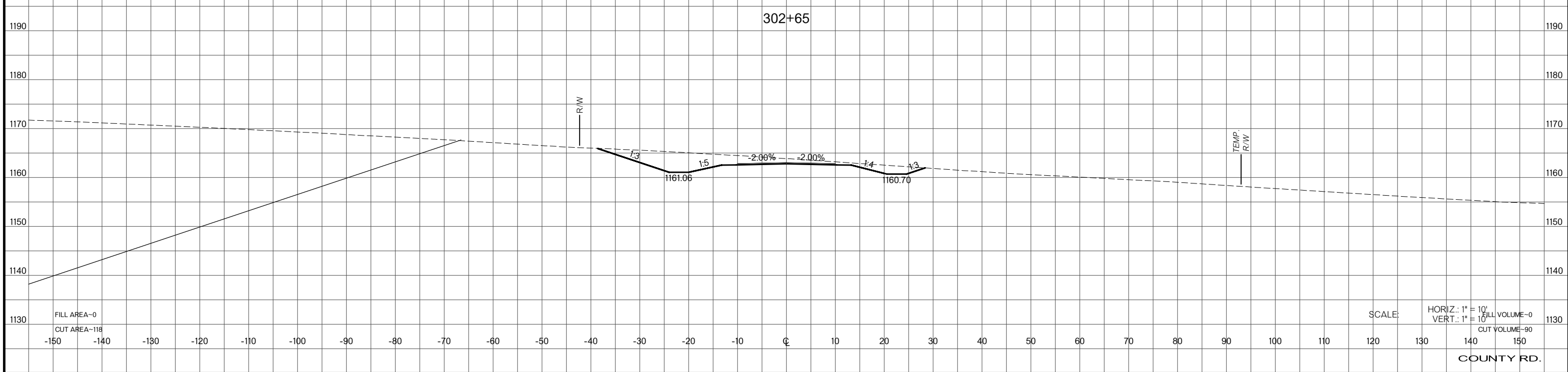


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



128 STA. 303+00, CRL COUNTY RD.
CONST. 22'x13'x36' LG. RCPA
17' LG. LT. & 19' LG. RT.
ℓ LT.=1160.92, ℓ RT.=1160.46



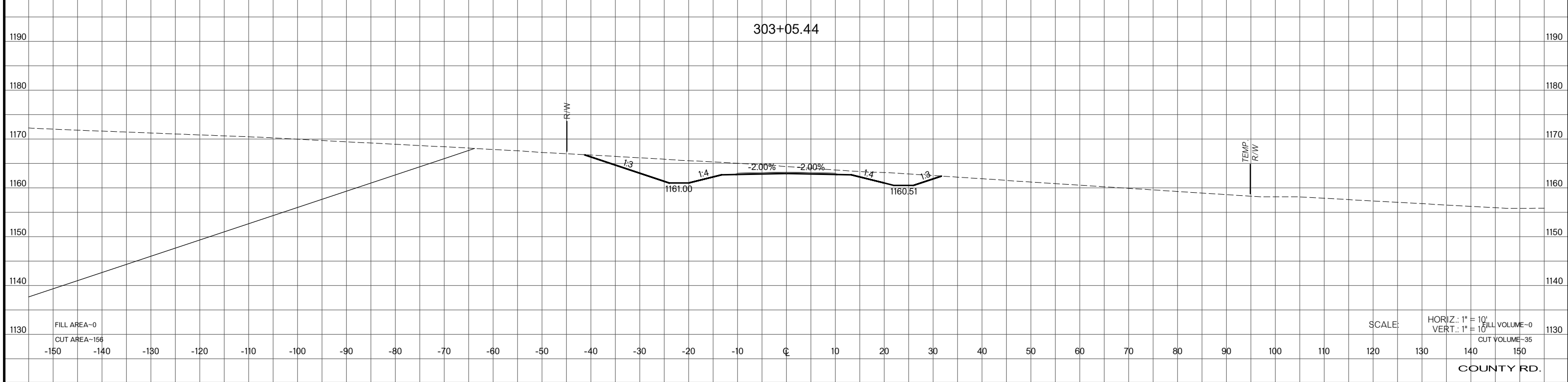
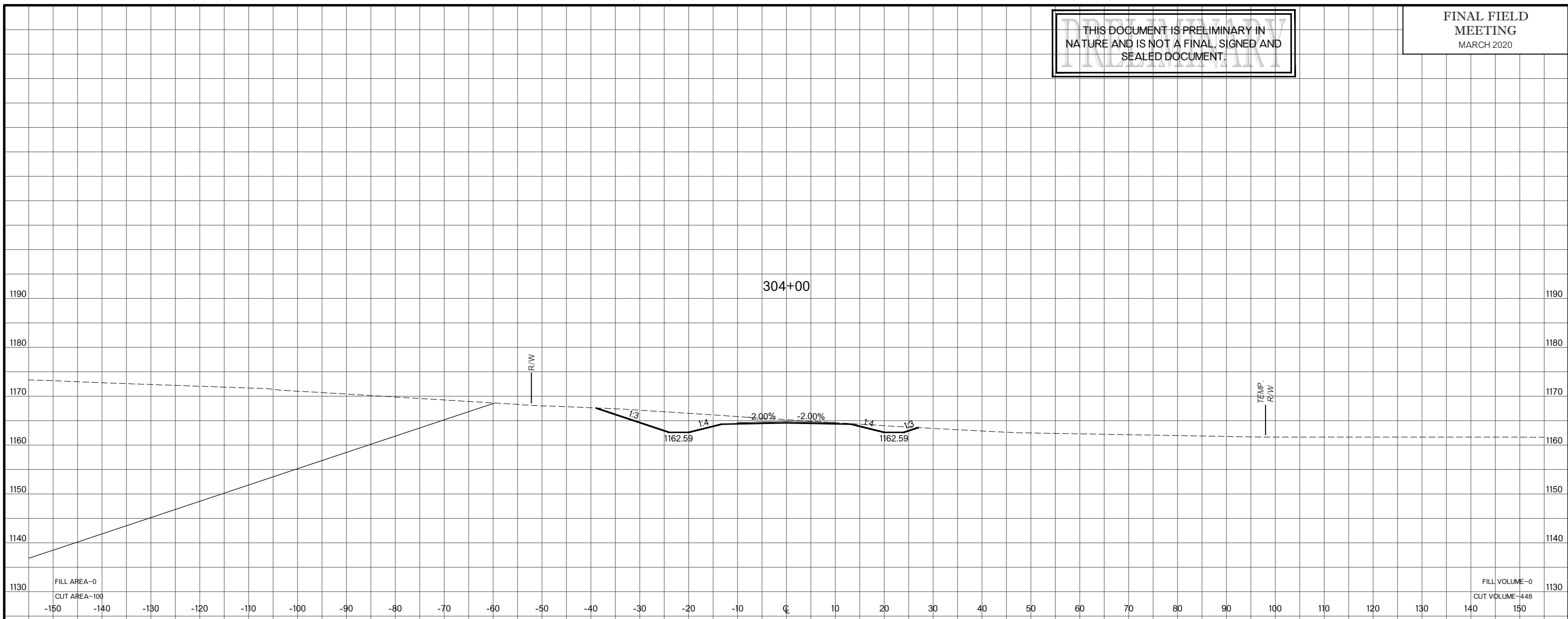
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTY RD.

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



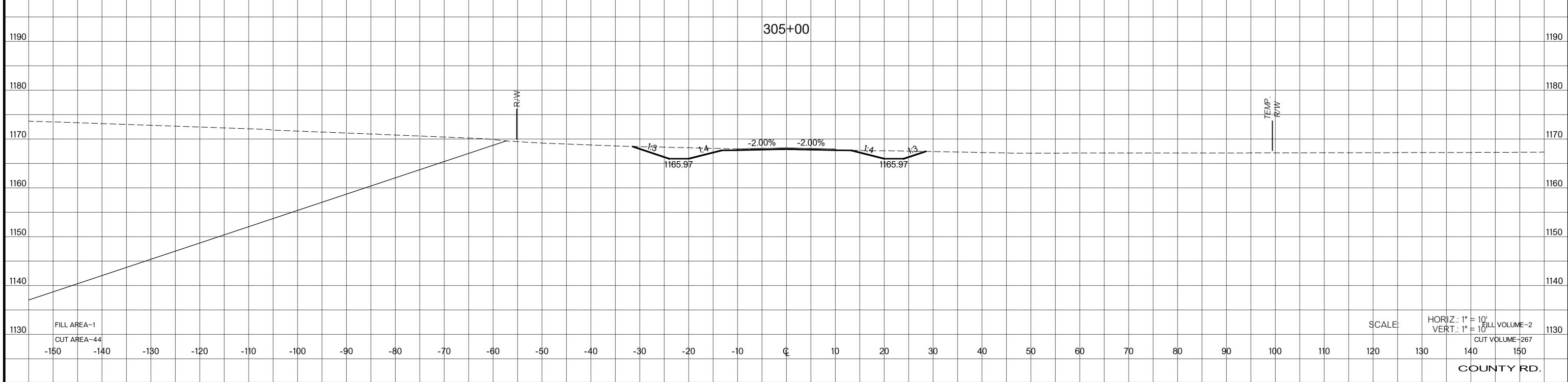
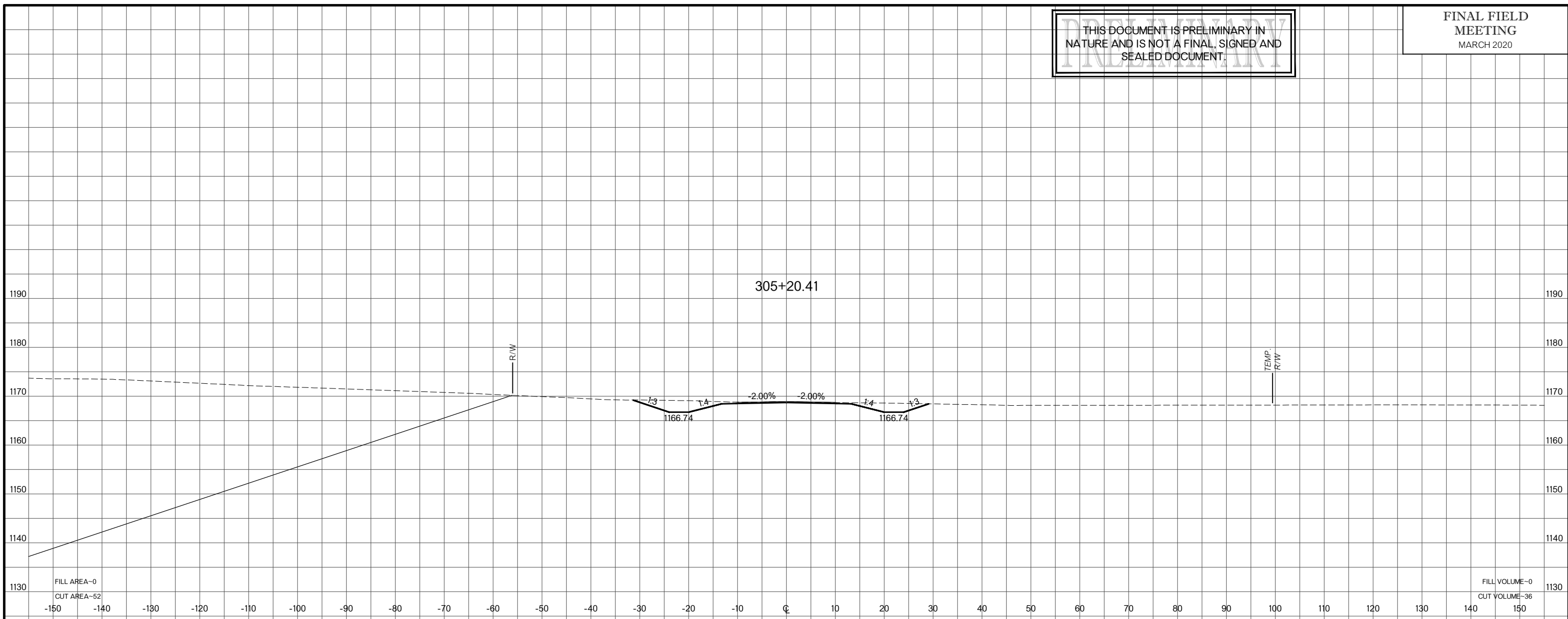
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTY RD.

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



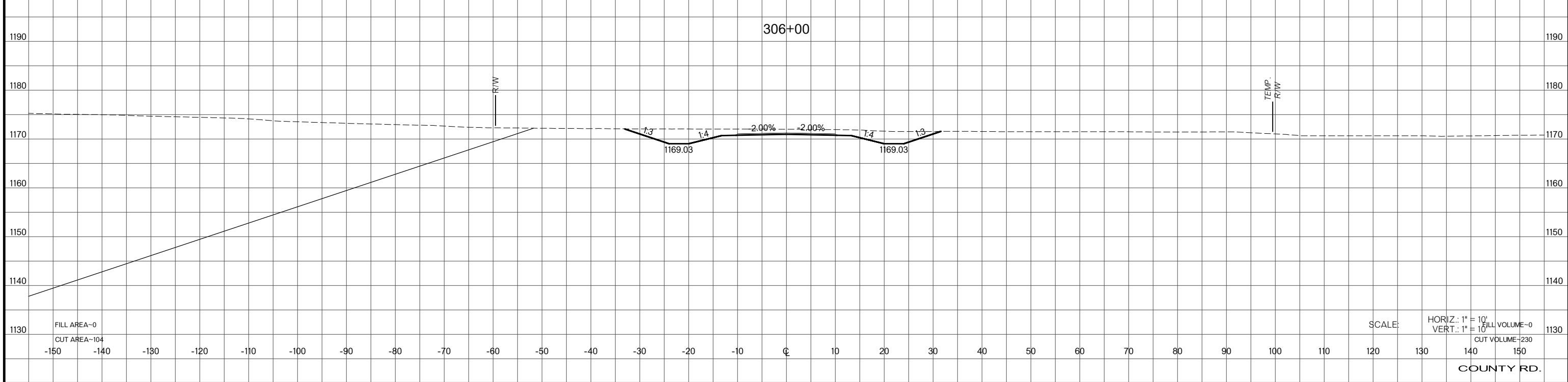
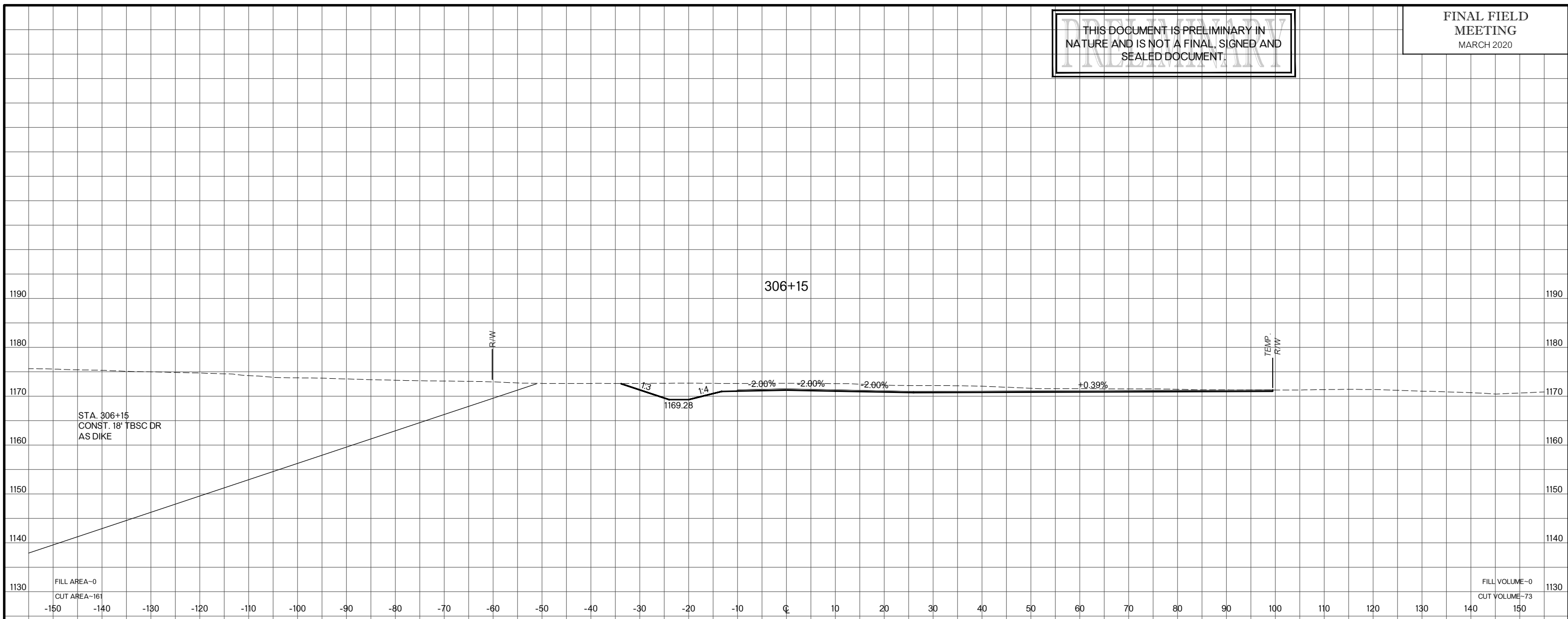
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTY RD.

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



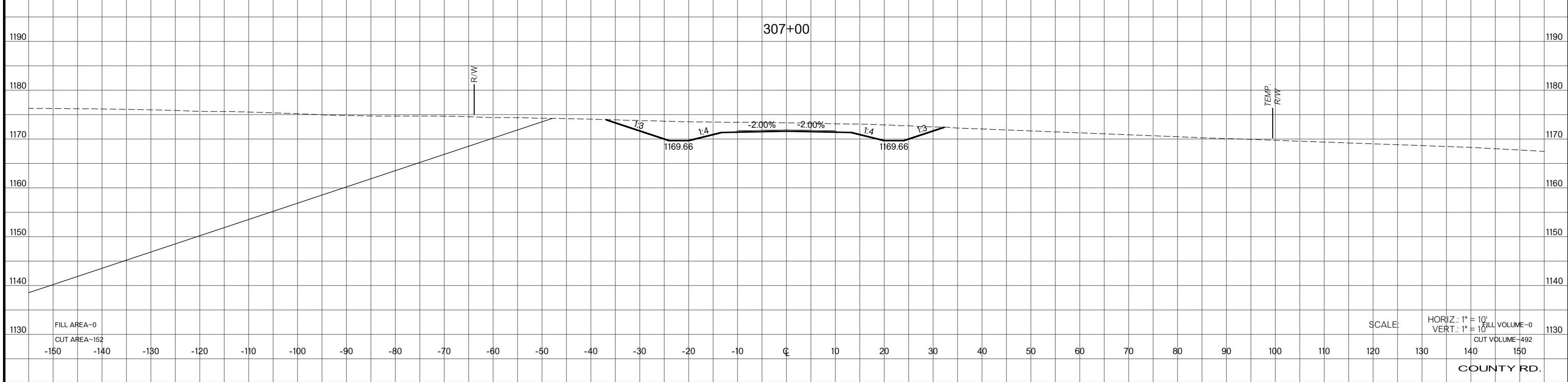
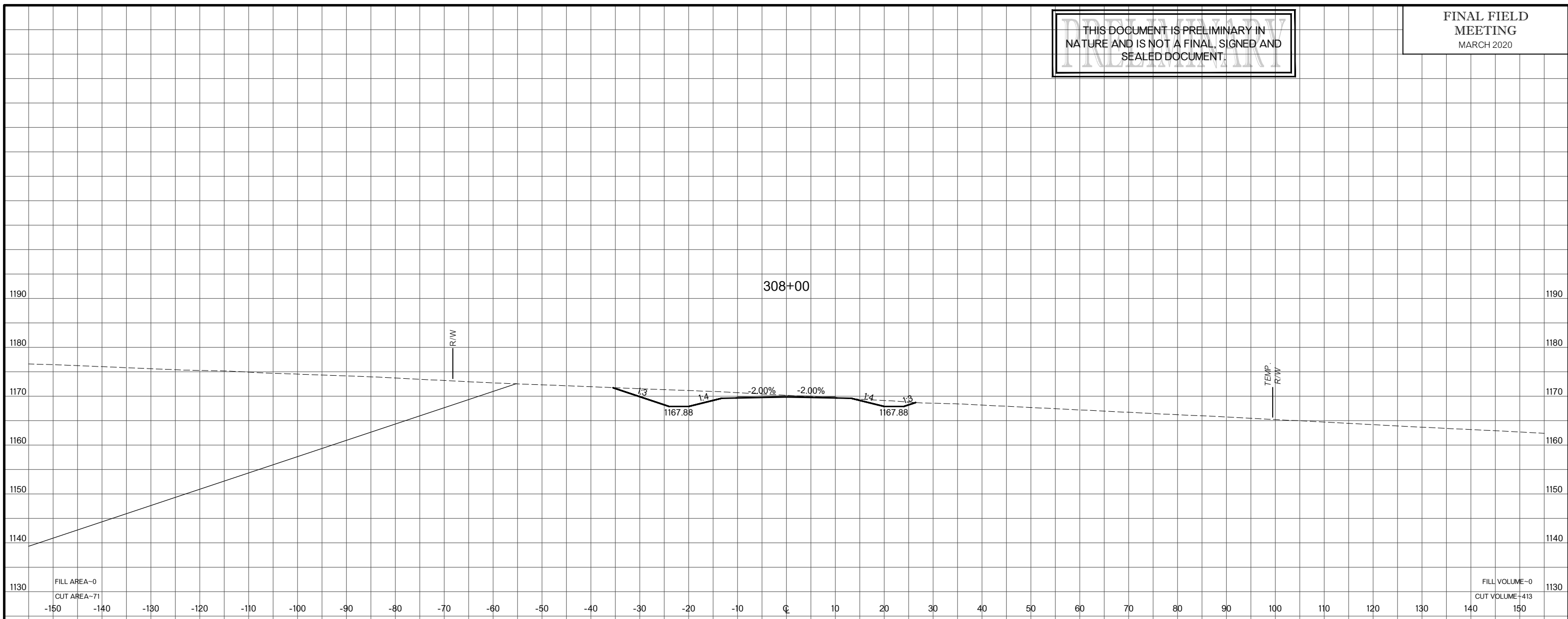
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTY RD.

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

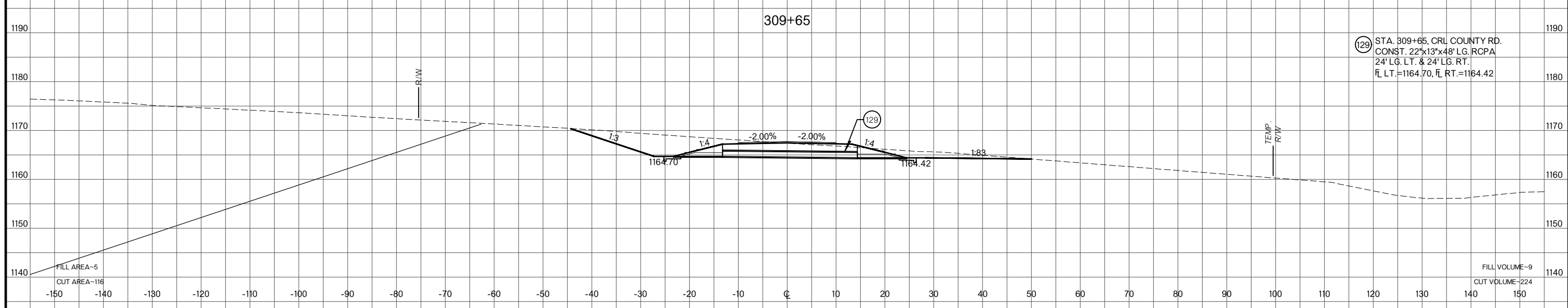
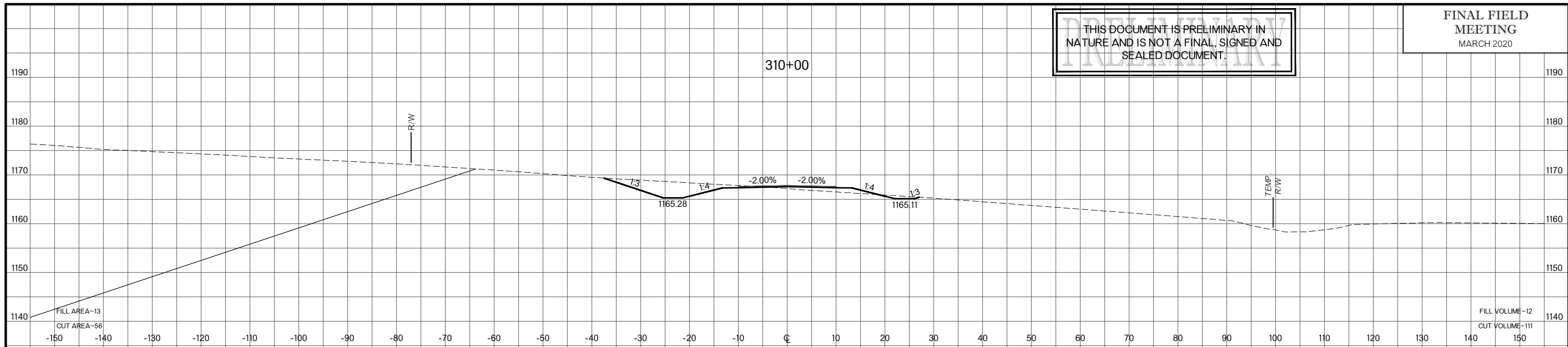


US 81 REALIGNMENT
GRADY COUNTY

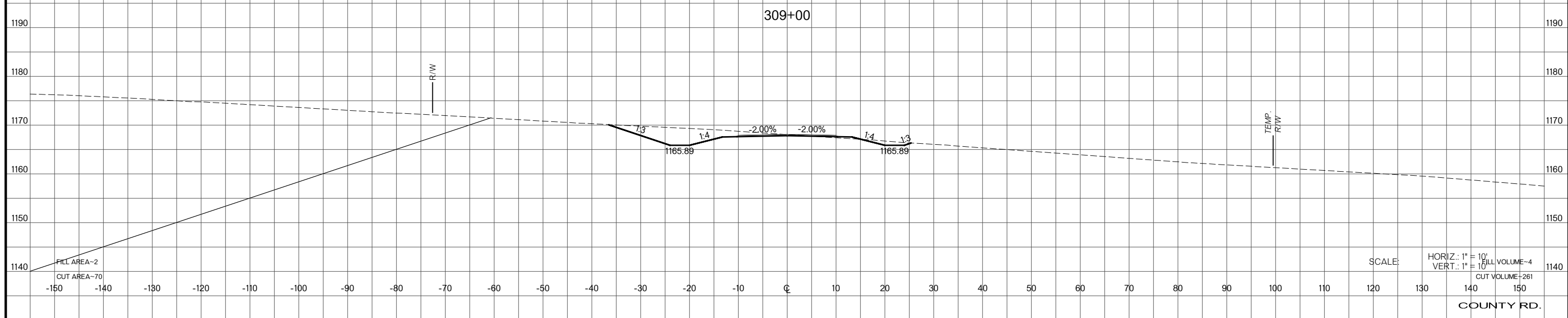
COUNTY RD.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



(129) STA. 309+65, CRL COUNTY RD.
CONST. 22'x13'x48' LG. RCPA
24' LG. LT. & 24' LG. RT.
FL LT.=1164.70, FL RT.=1164.42



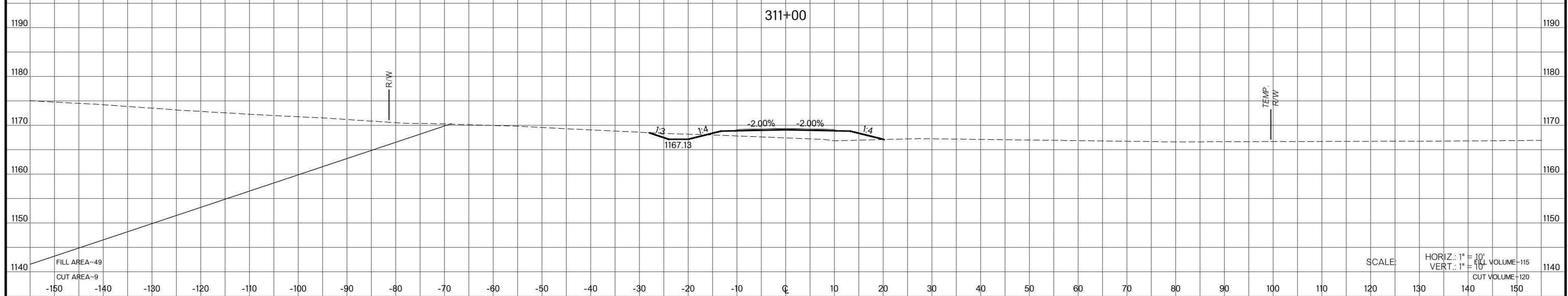
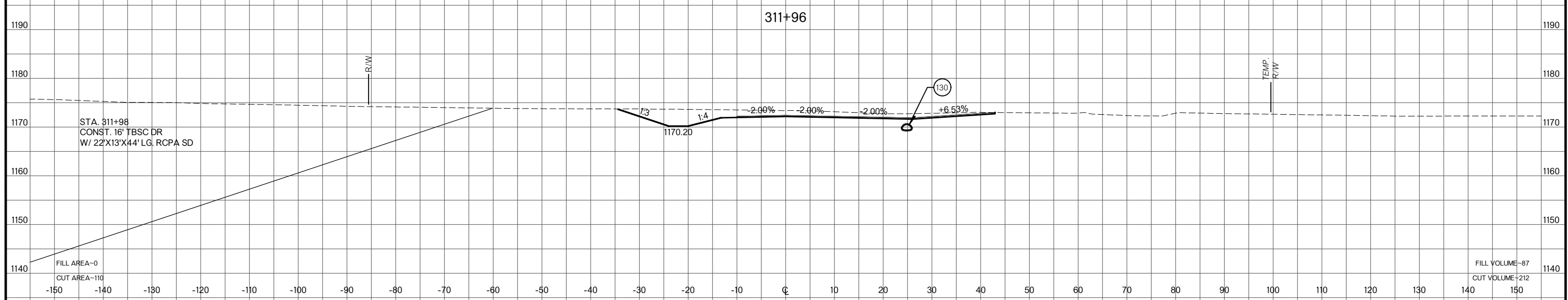
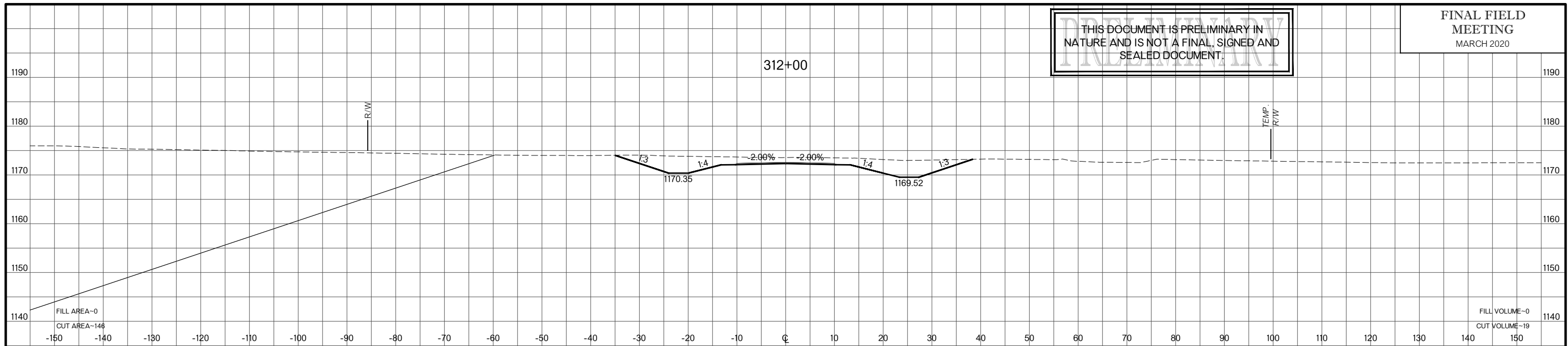
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTY RD.

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

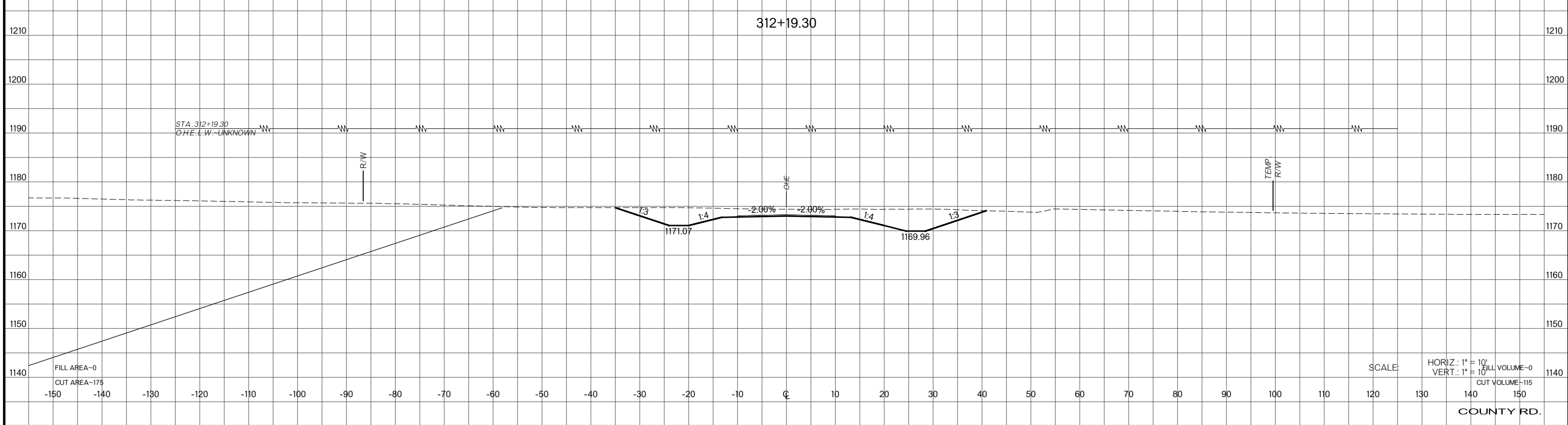
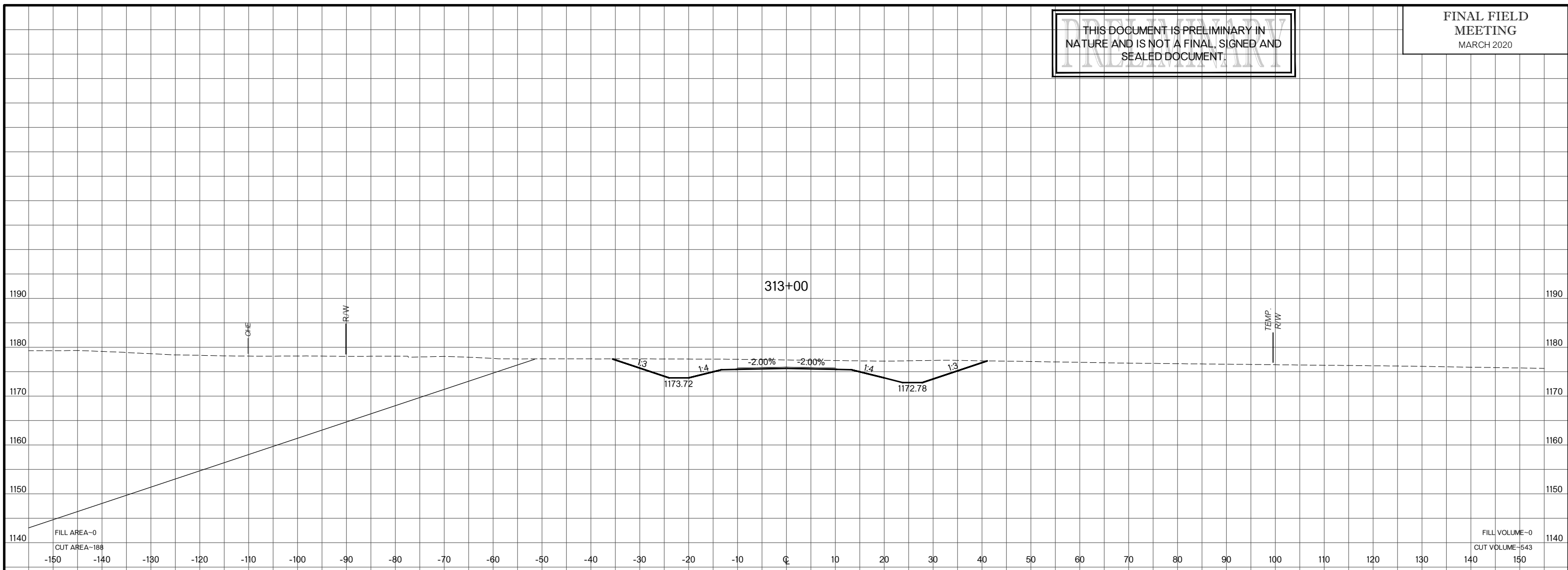


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

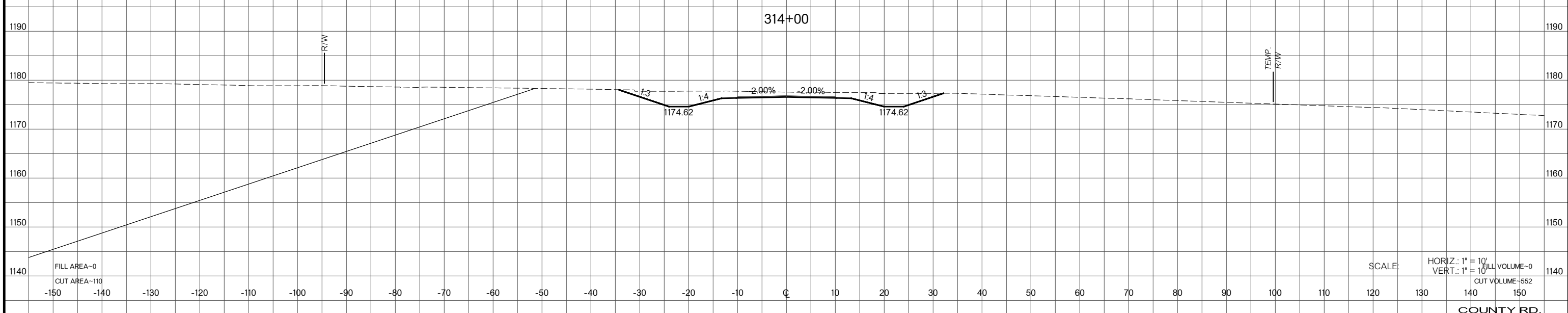
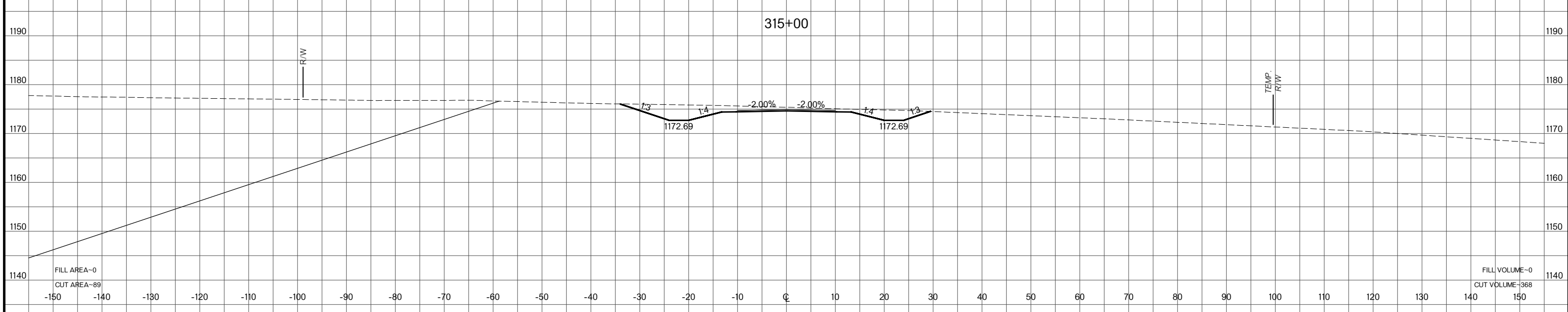
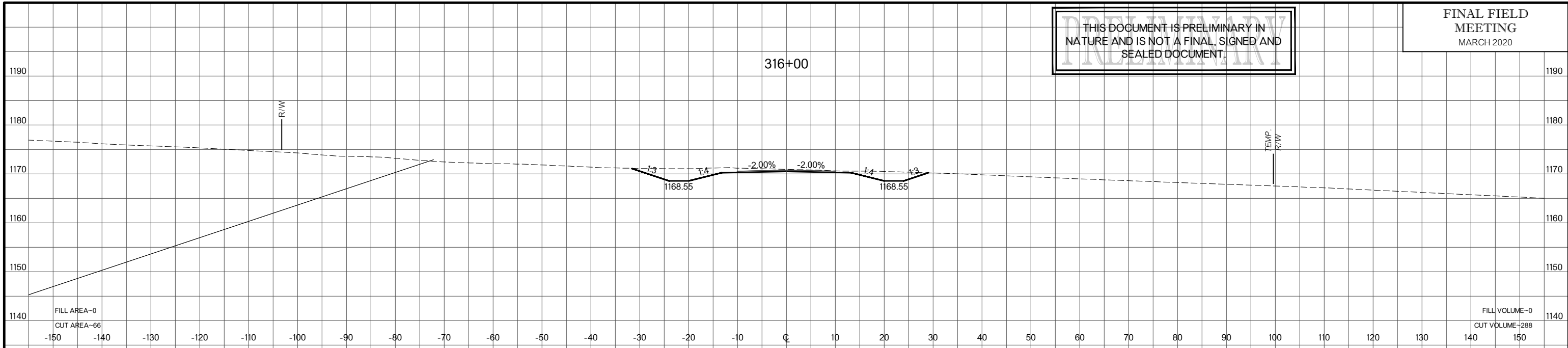


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



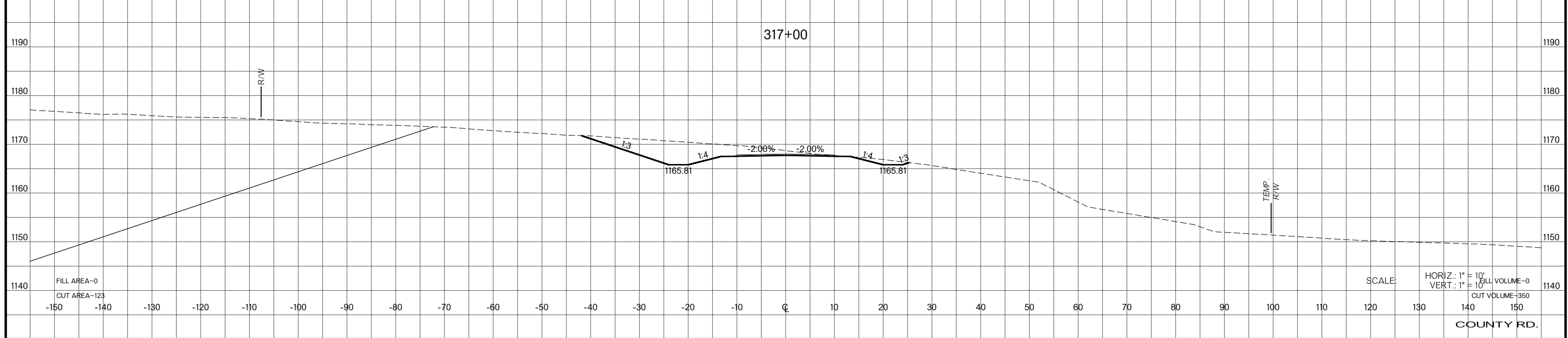
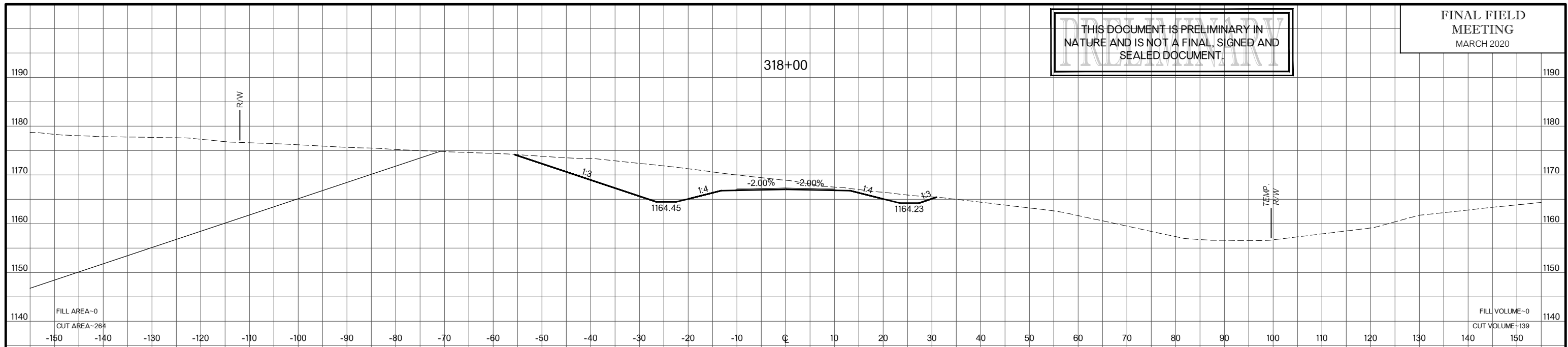
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTY RD.

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

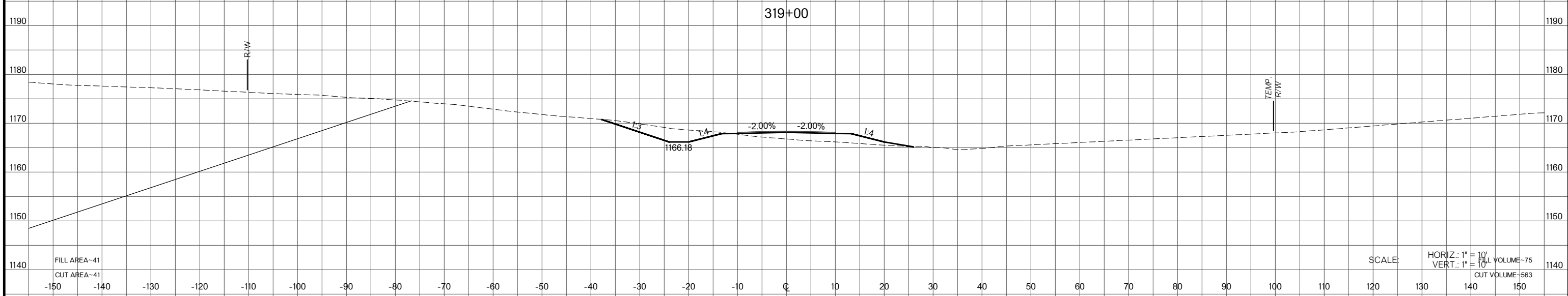
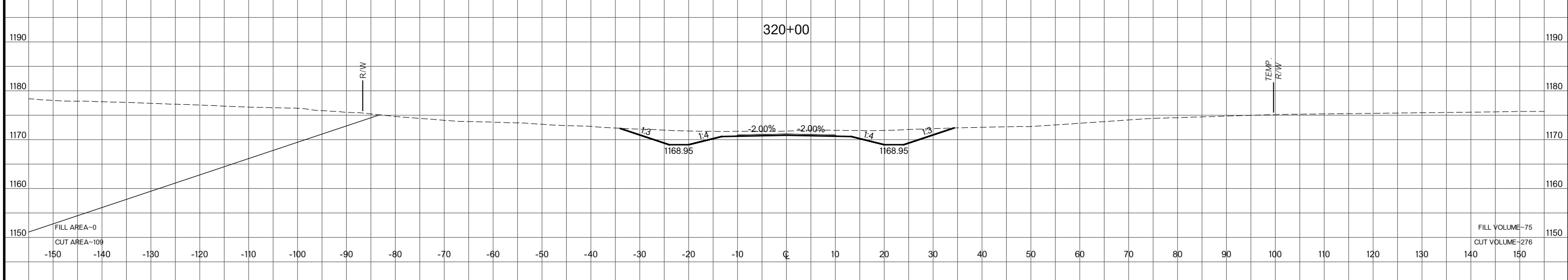
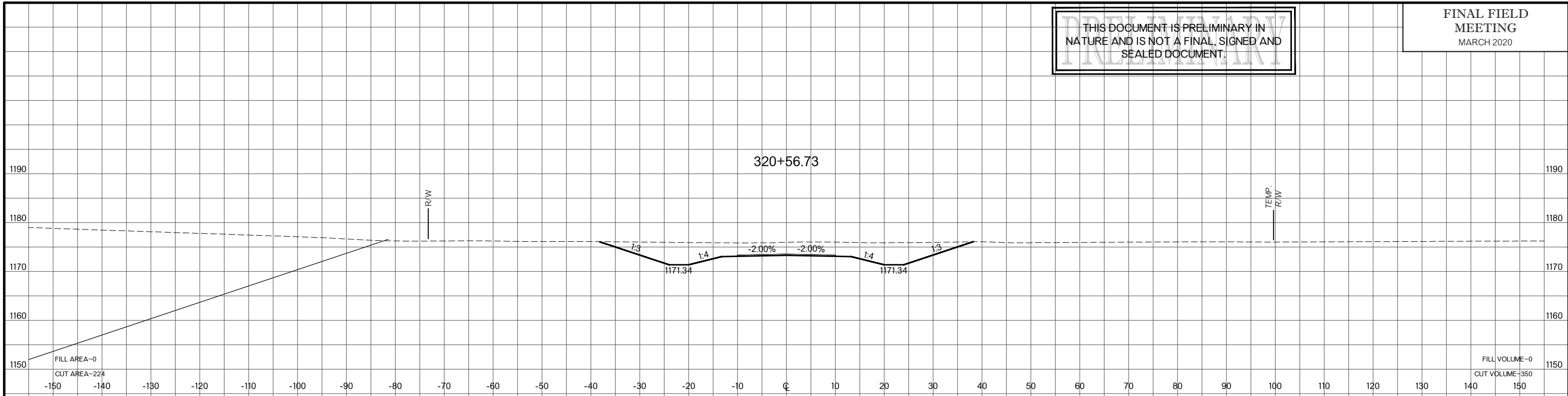


US 81 REALIGNMENT
GRADY COUNTY

COUNTY RD.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



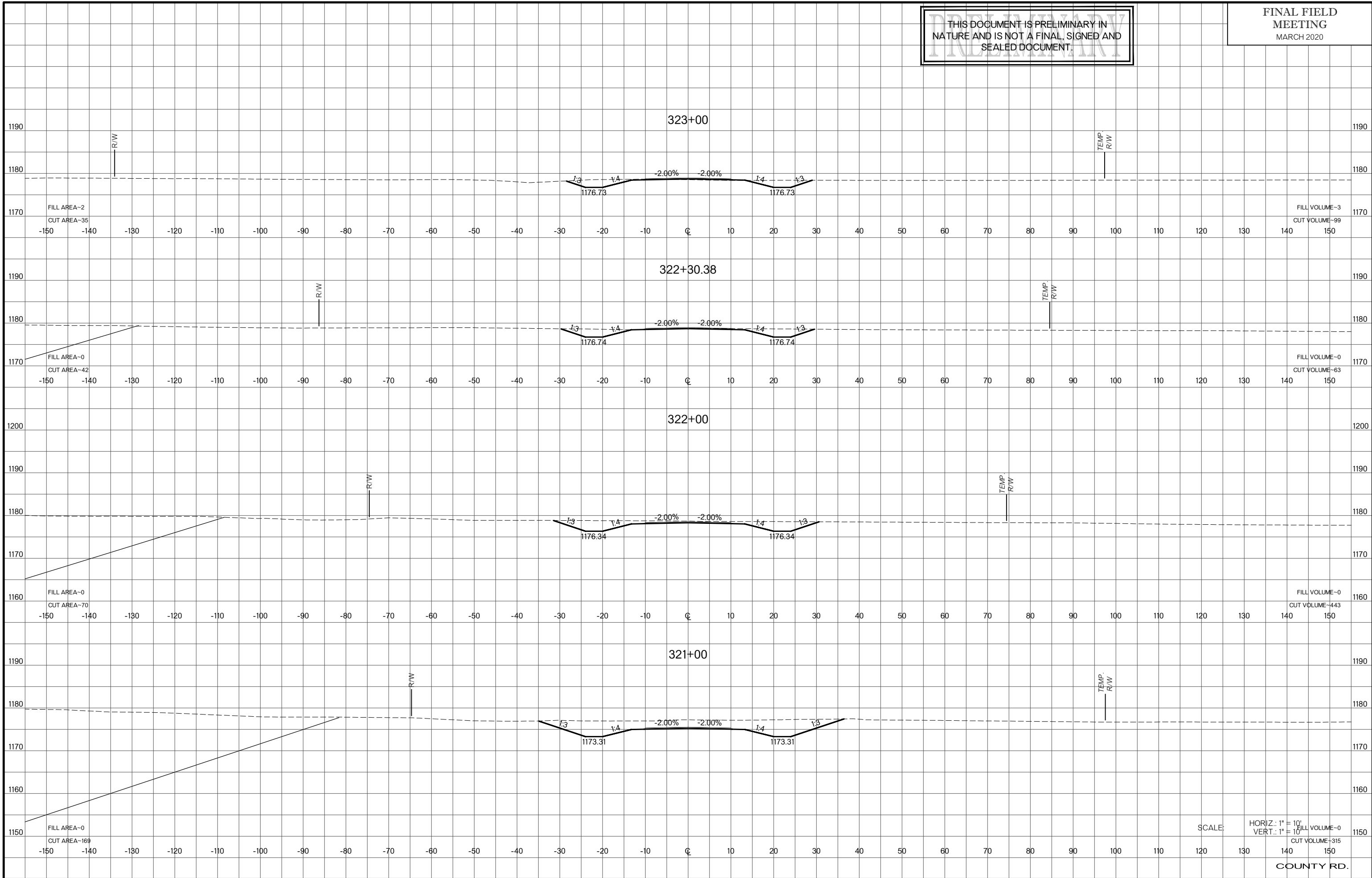
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

COUNTY RD.

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



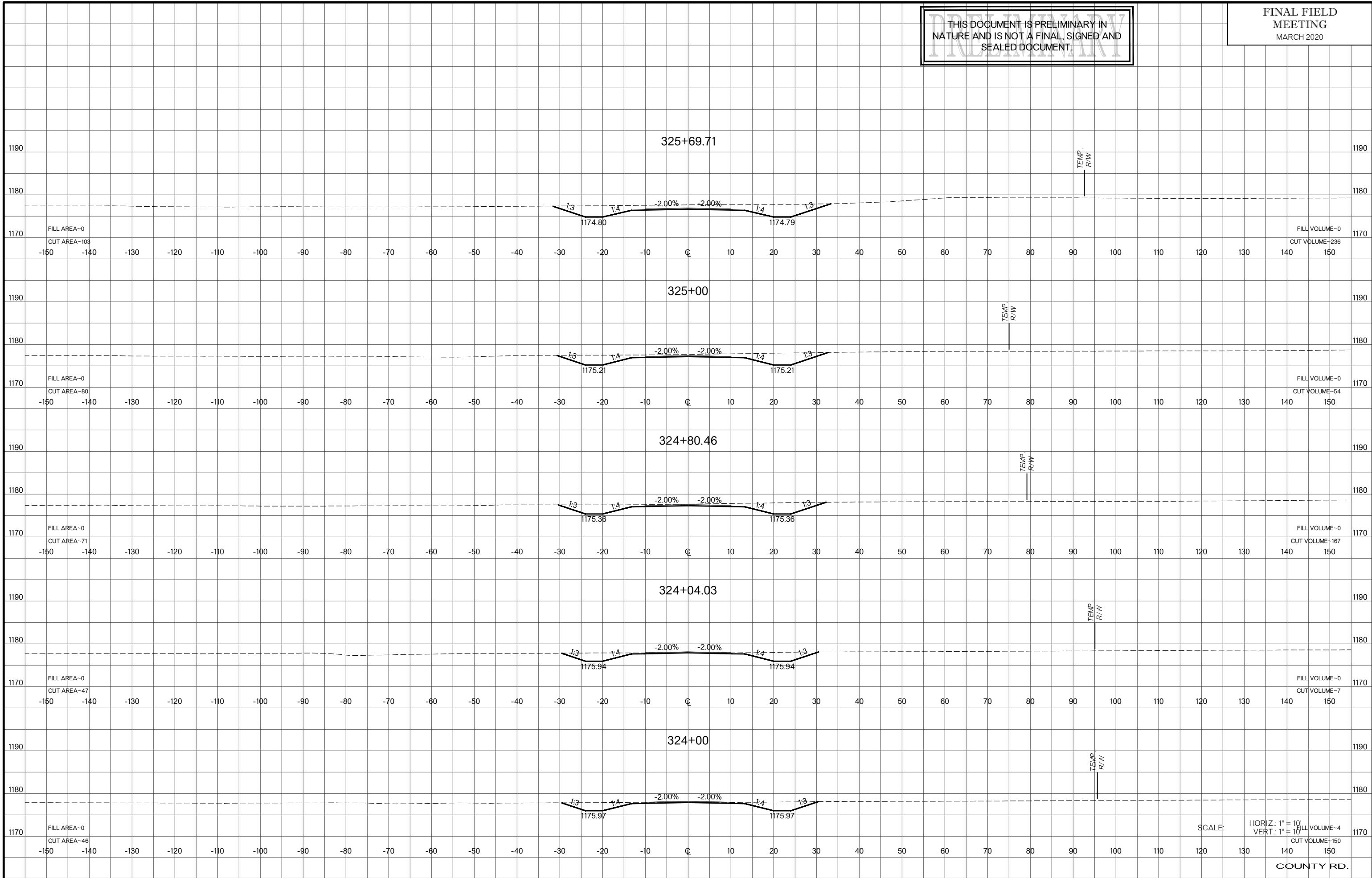
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

COUNTY RD.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

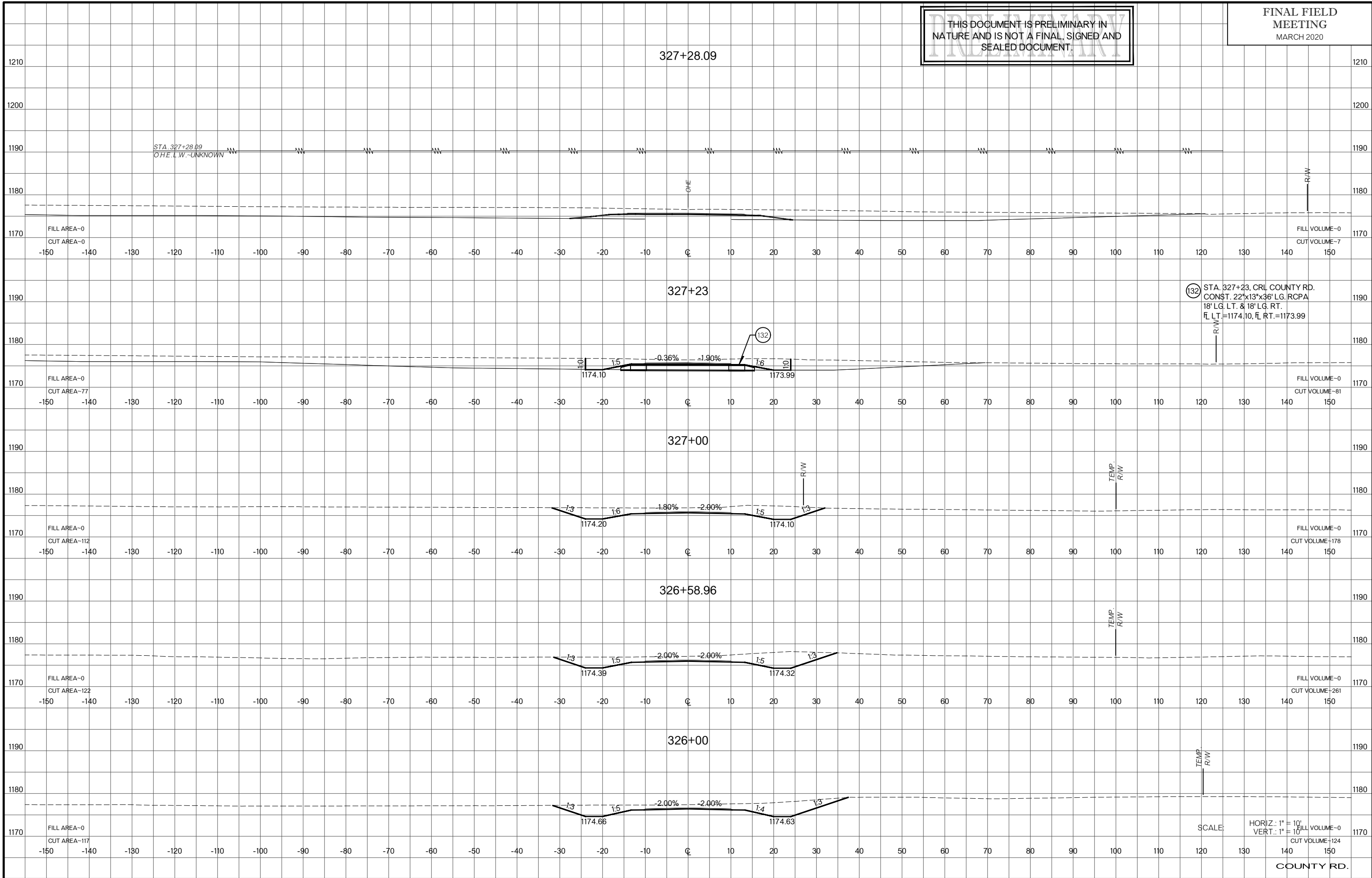


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



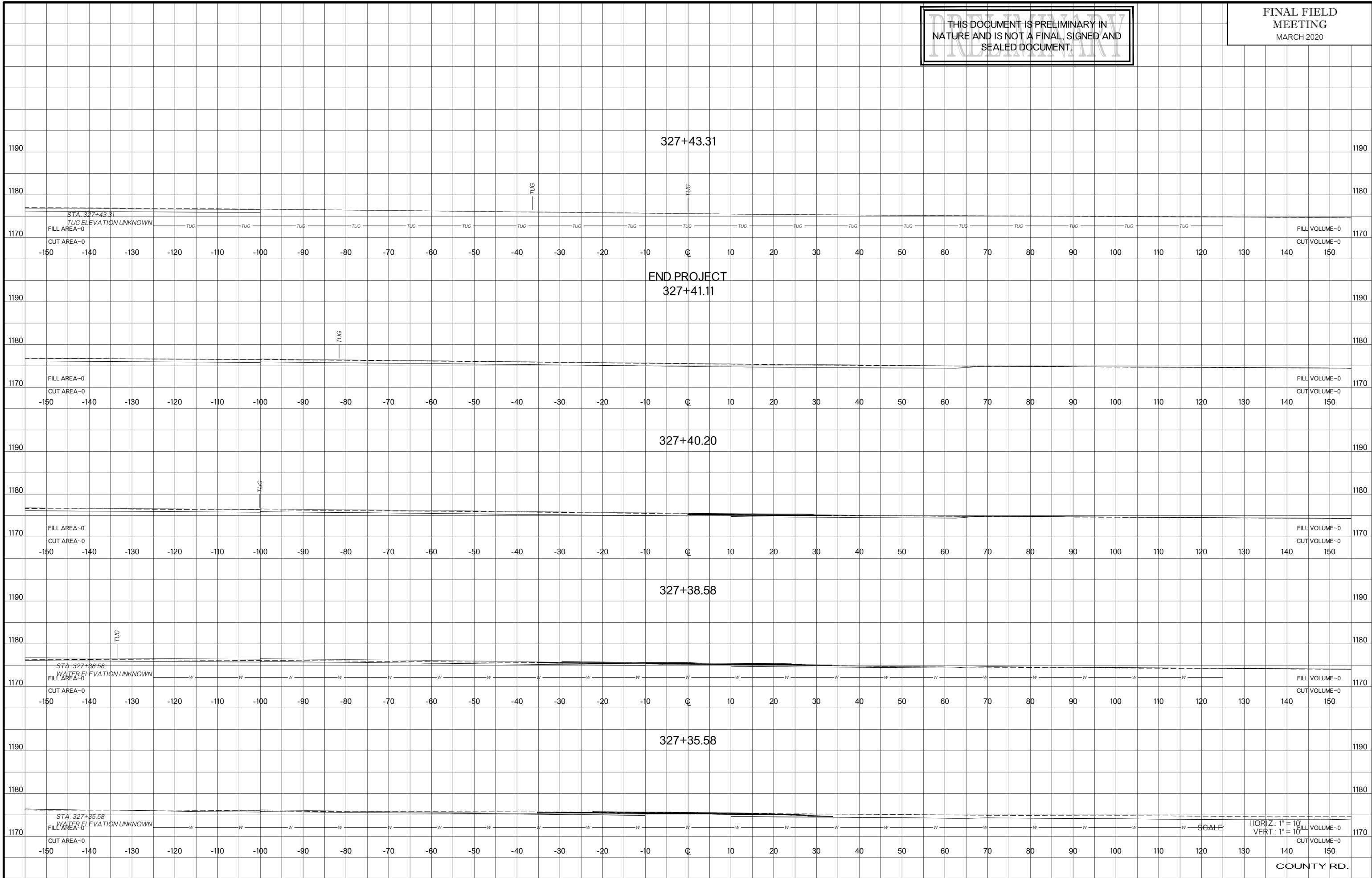
(132) STA. 327+23, CRL COUNTY RD.
CONST. 22'x13'x36' LG. RCPA
18' LG. LT. & 18' LG. RT.
FL LT.=1174.10, FL RT.=1173.99

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

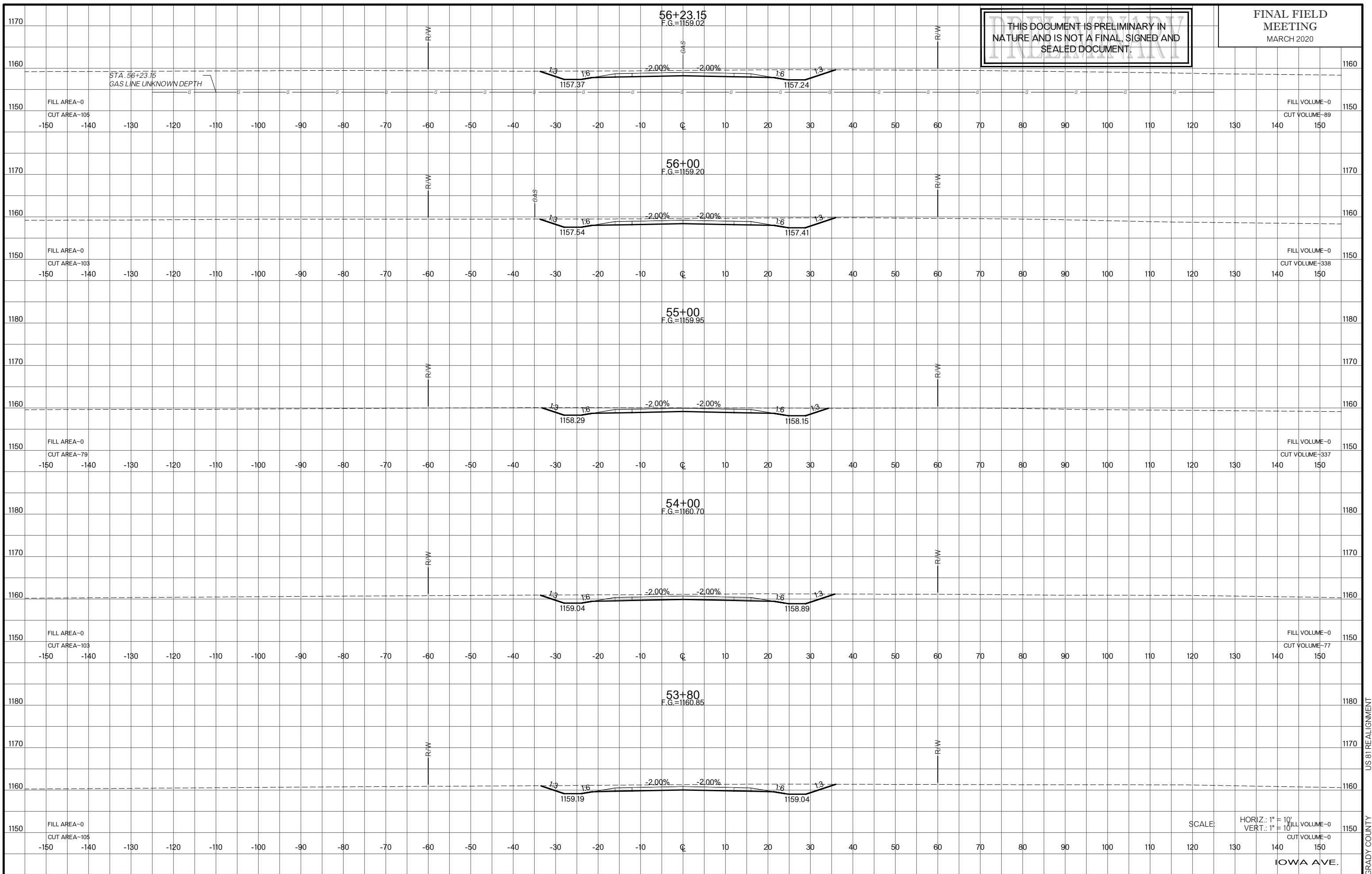
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

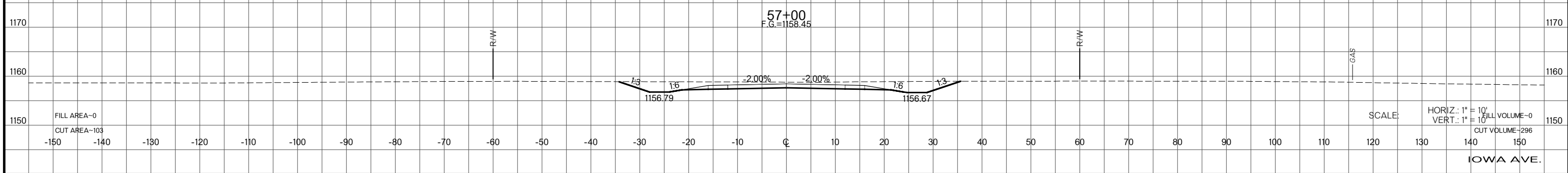
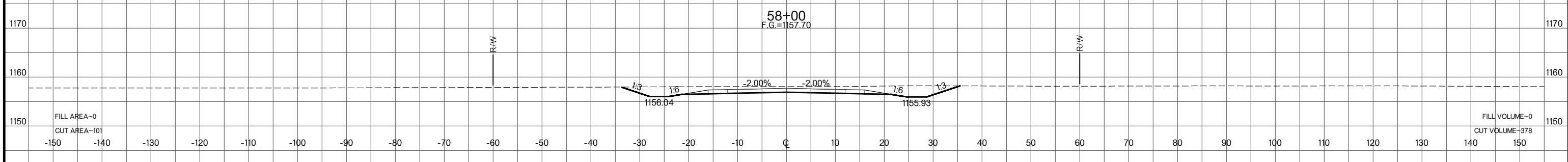
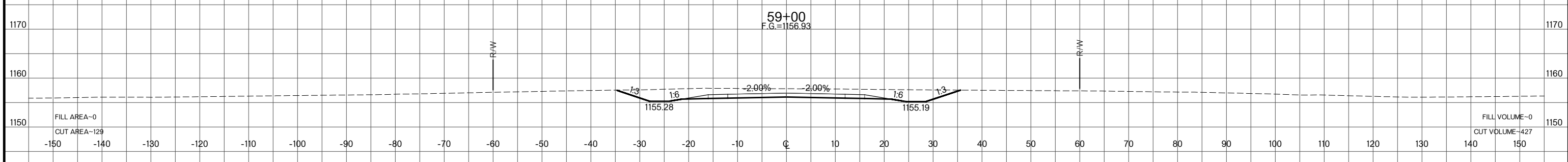
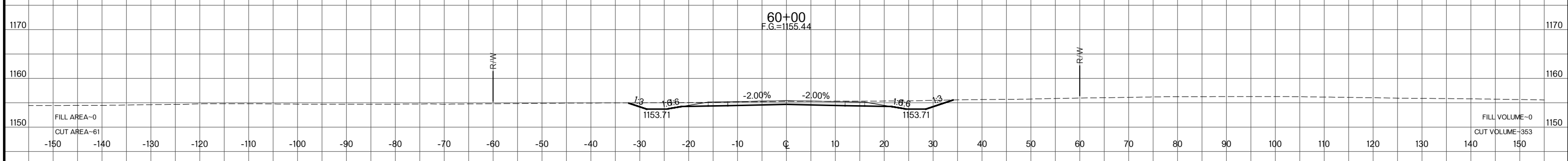
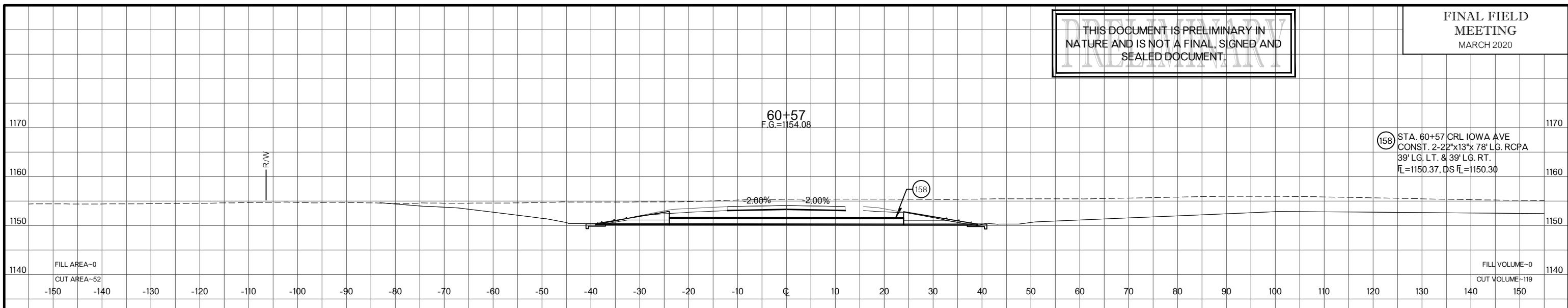
IOWA AVE.

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

158 STA. 60+57 CRL IOWA AVE
CONST. 2-22"x13"x 78' LG. RCPA
39' LG. LT. & 39' LG. RT.
FL=1150.37, DS FL=1150.30



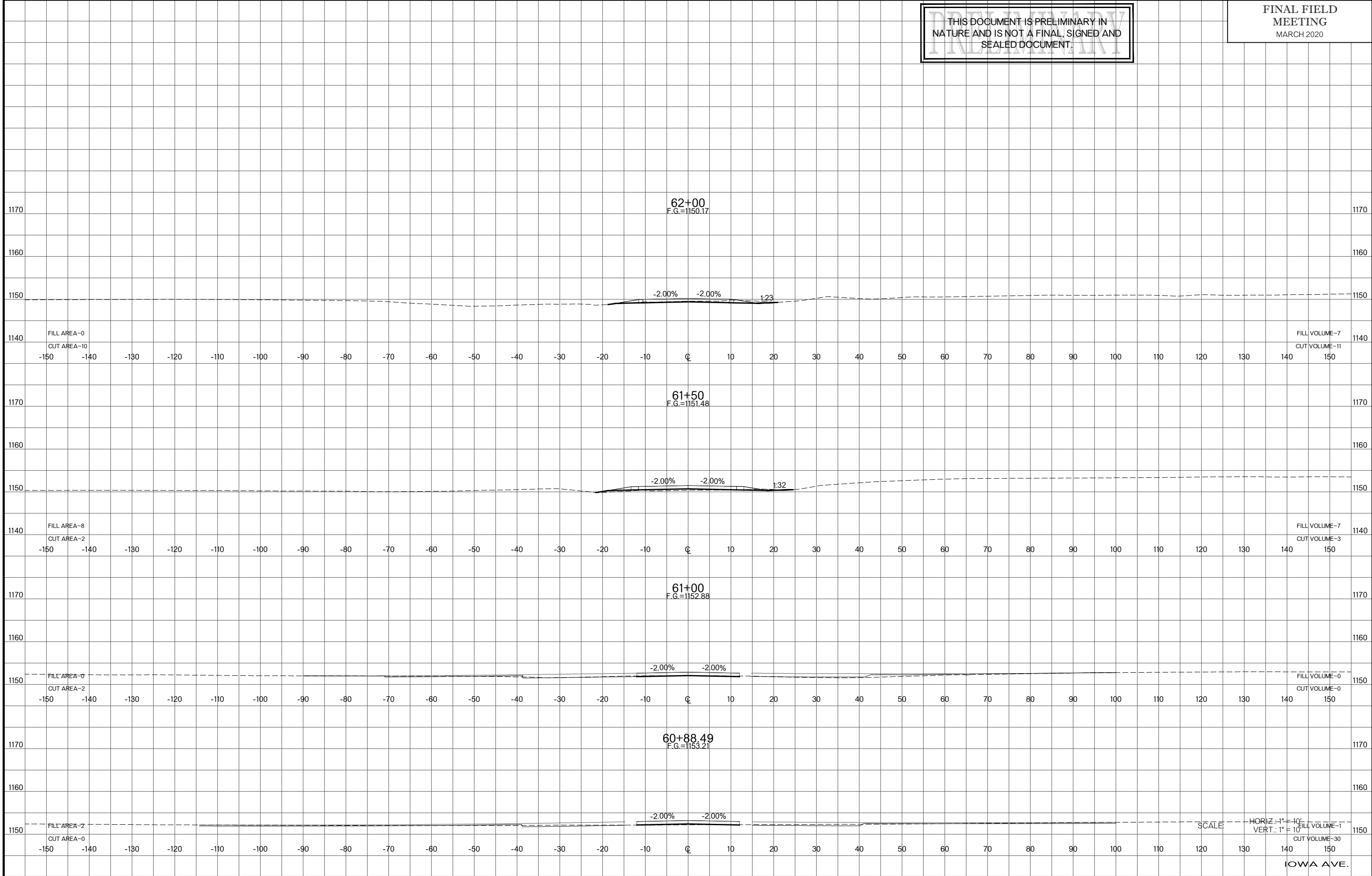
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

IOWA AVE.

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



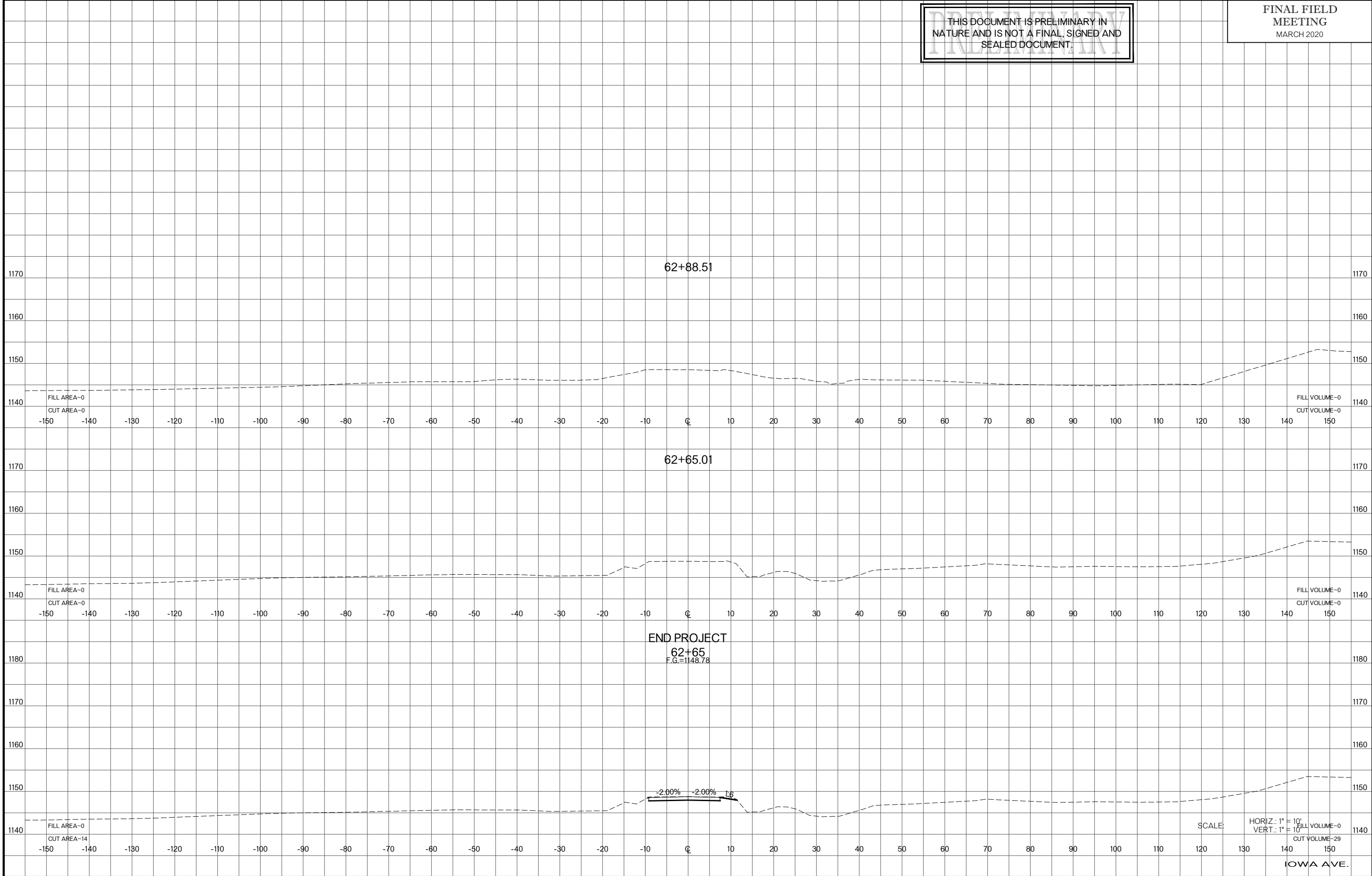
US 81 REALIGNMENT
GRADY COUNTY

IOWA AVE.

SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

IOWA AVE.

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

FILL VOLUME-0
CUT VOLUME-29

FILL AREA-0
CUT AREA-14

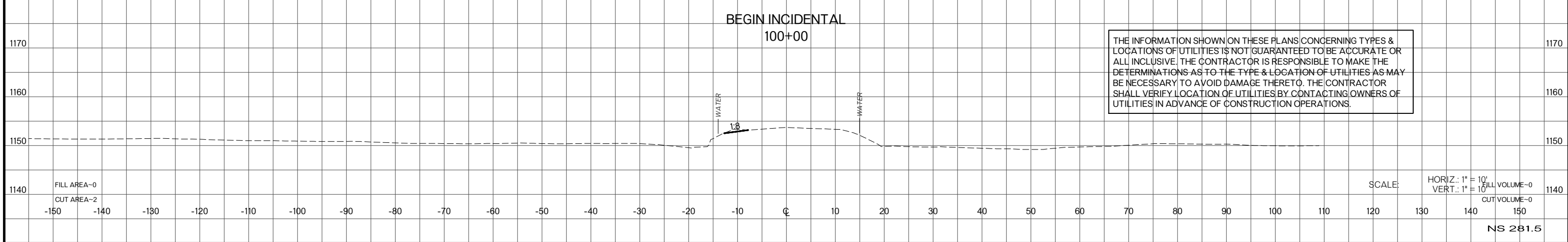
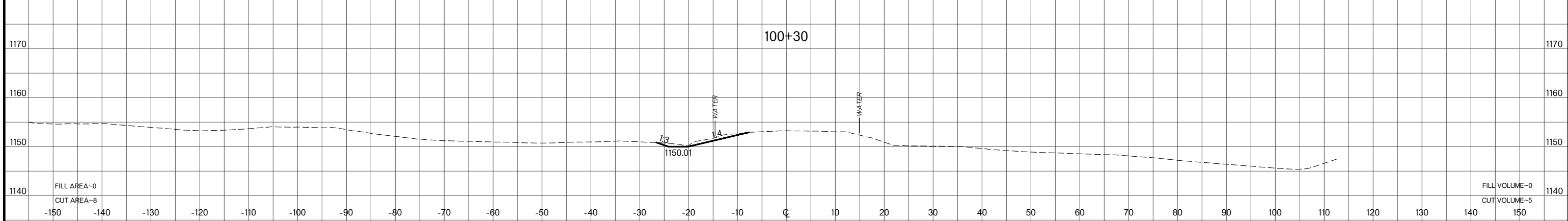
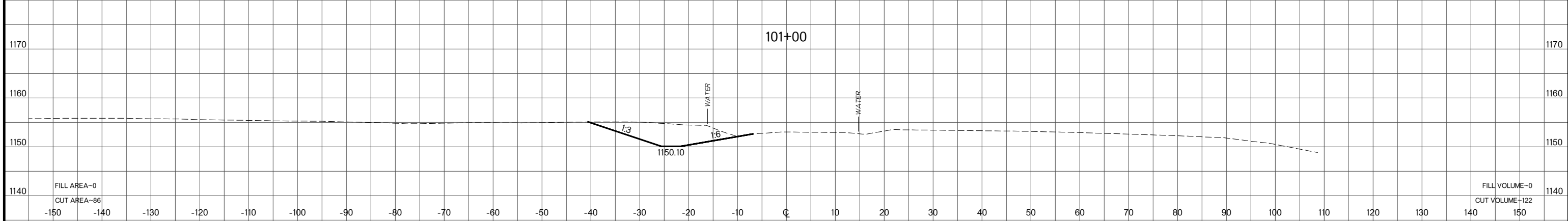
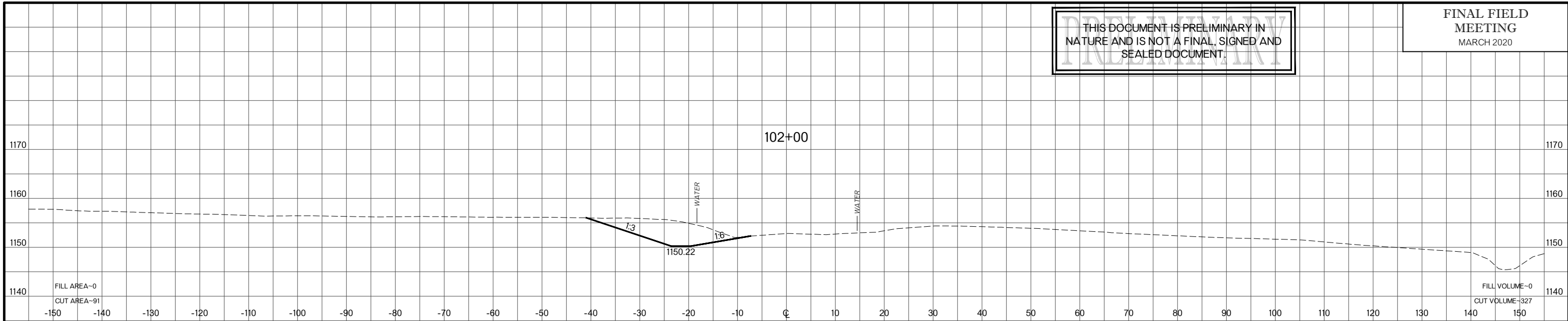
FILL AREA-0
CUT AREA-0

FILL VOLUME-0
CUT VOLUME-0

FILL VOLUME-0
CUT VOLUME-0

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



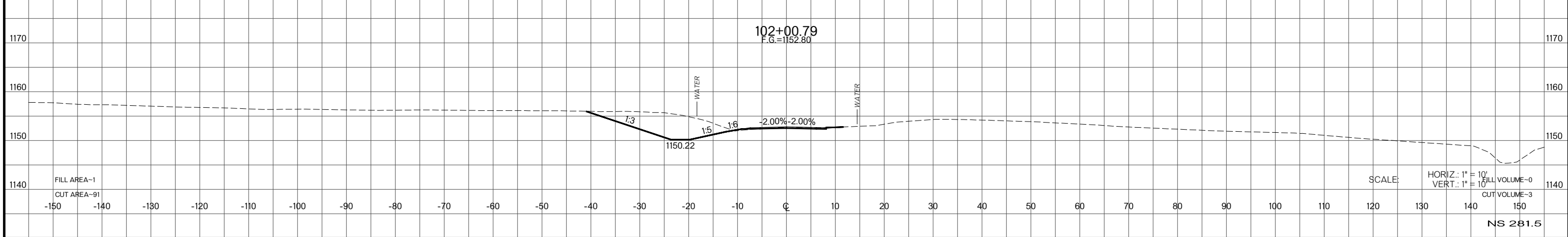
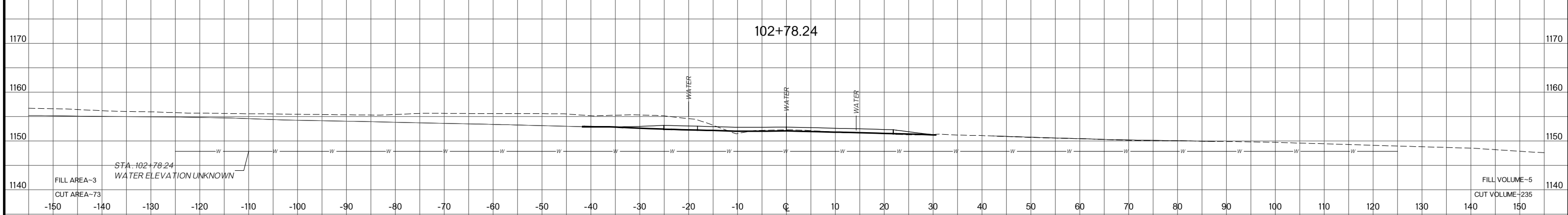
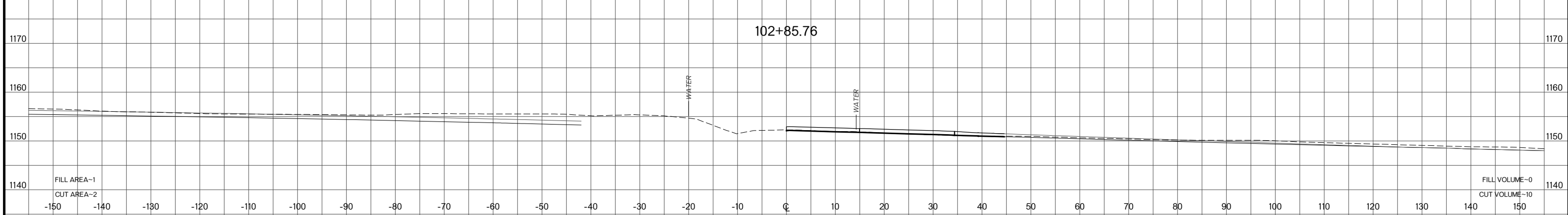
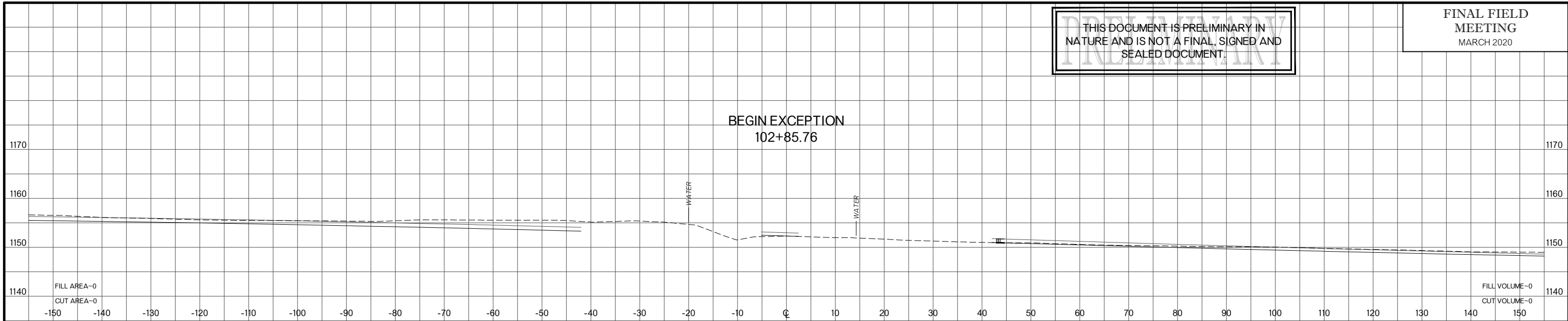
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

NS 281.5

US 81 REALIGNMENT
GRADY COUNTY

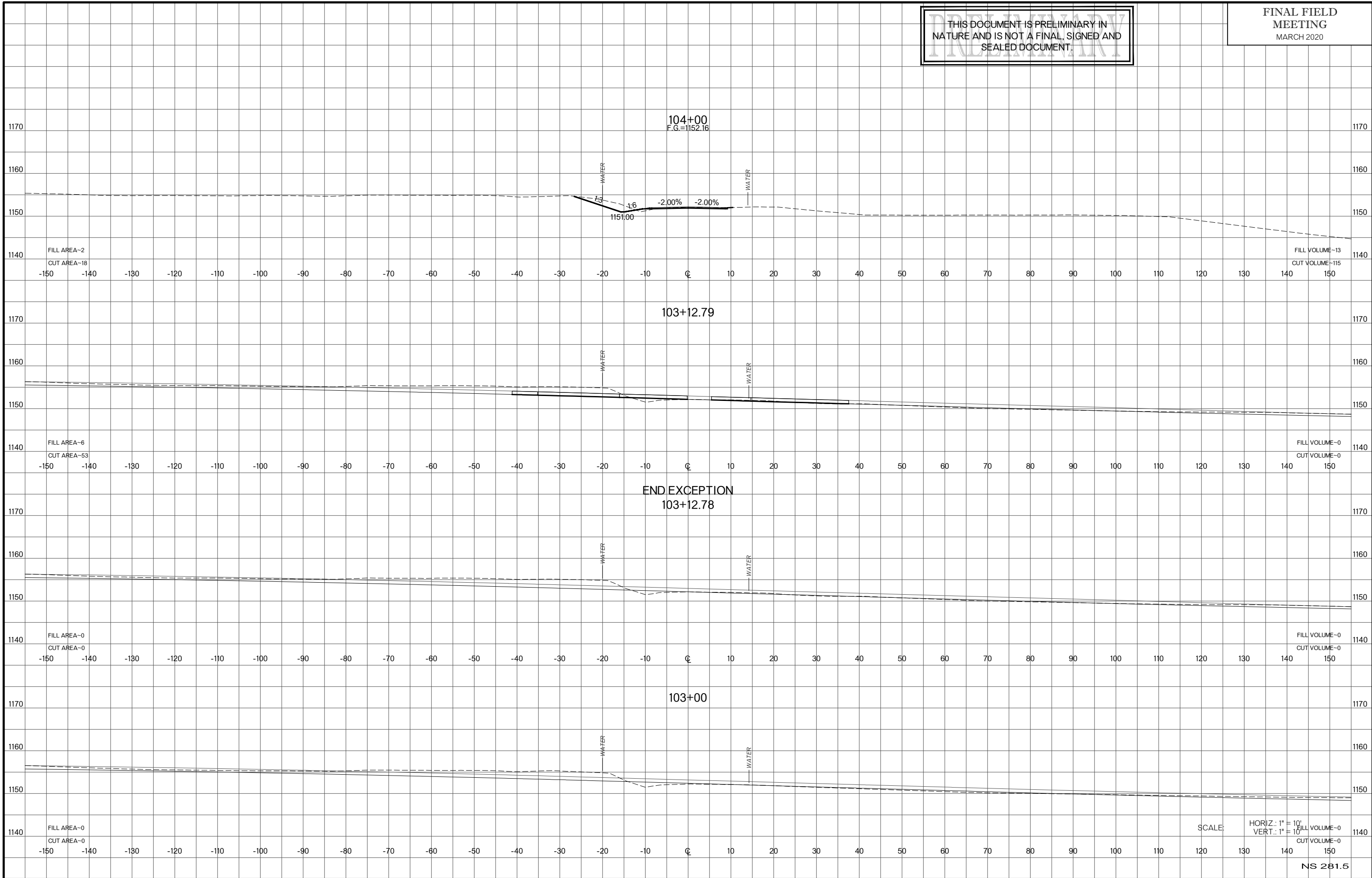
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



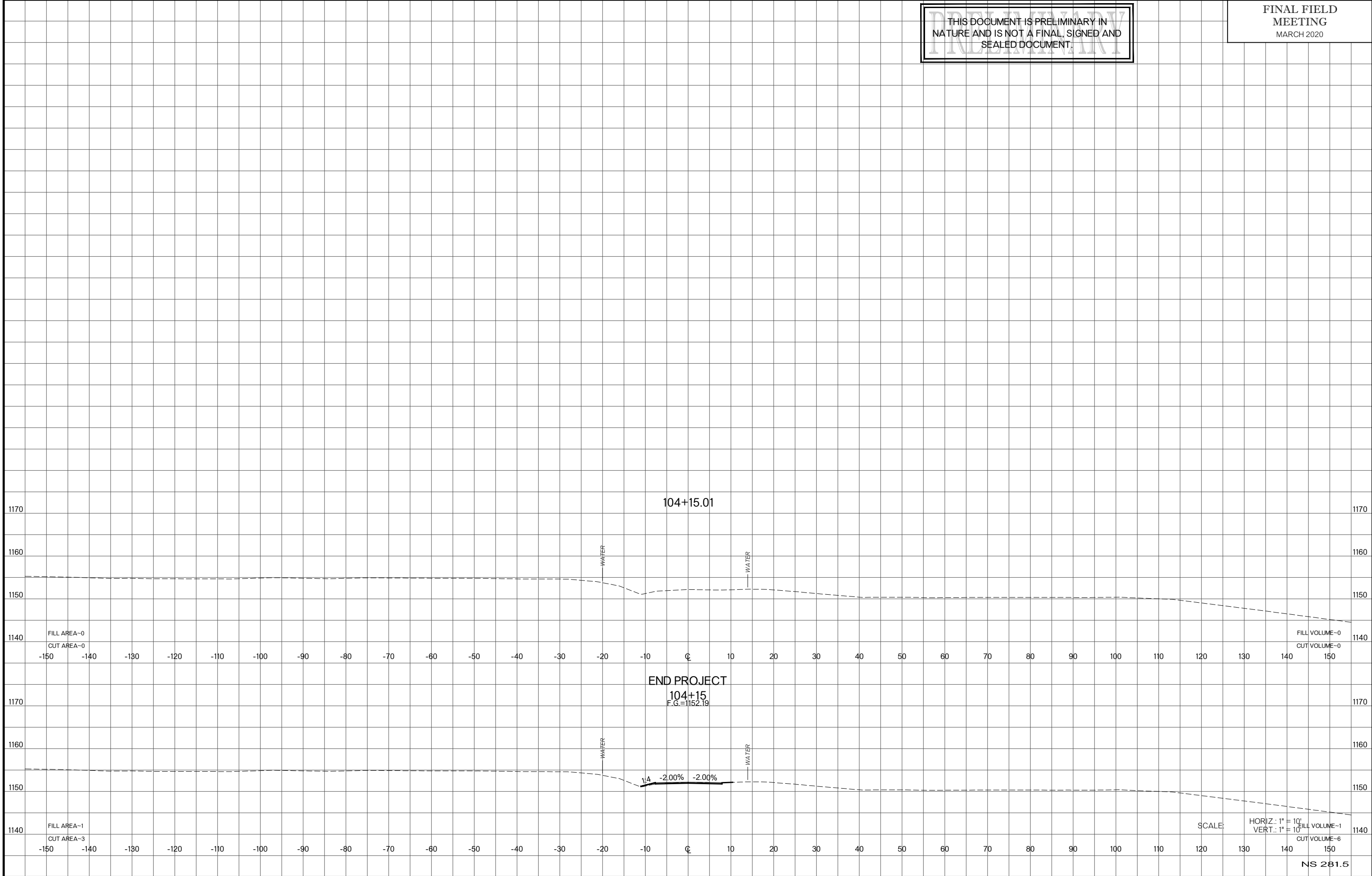
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

NS 281.5

US 81 REALIGNMENT
GRADY COUNTY

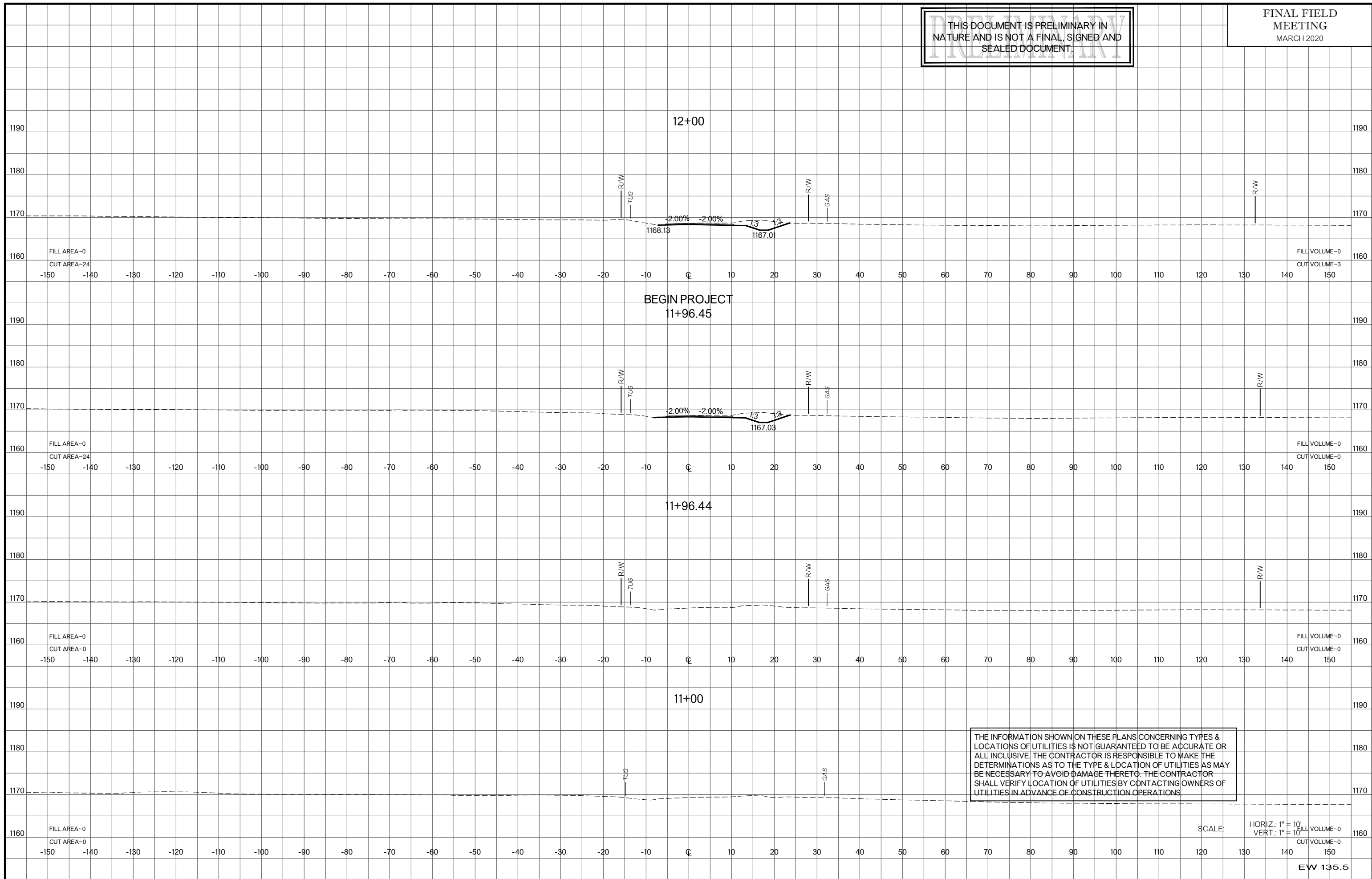
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

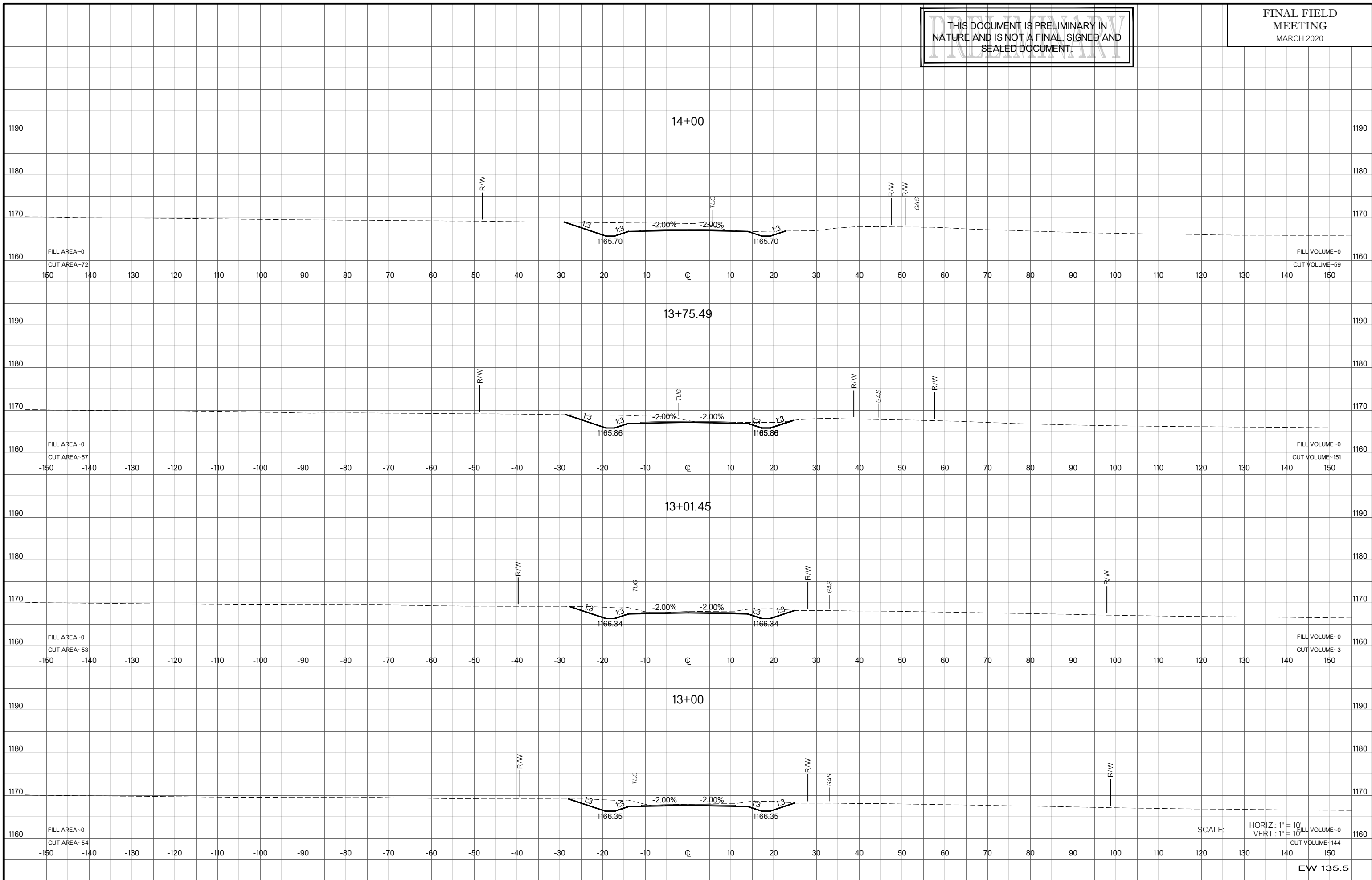


THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



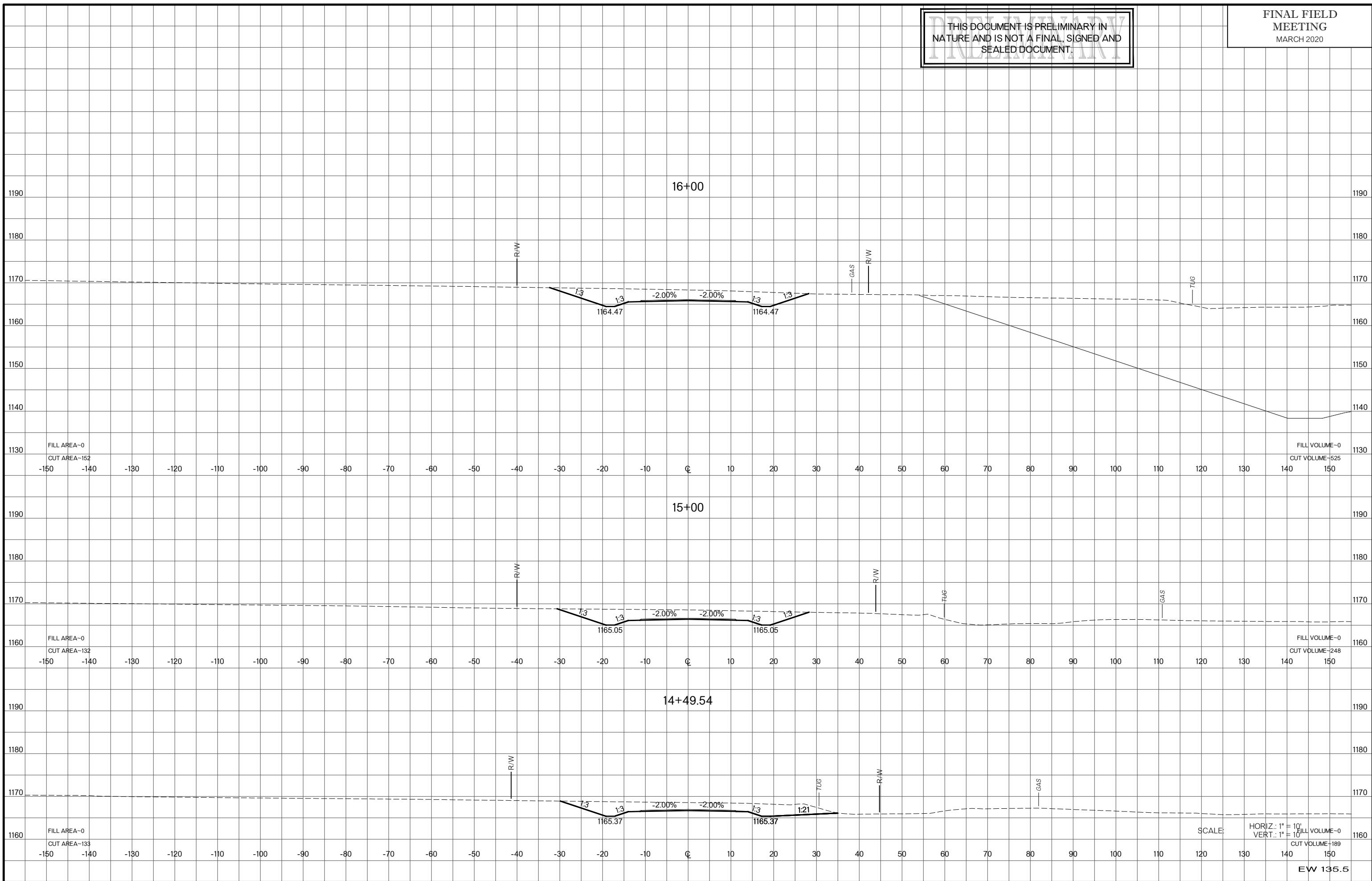
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

EW 135.5

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



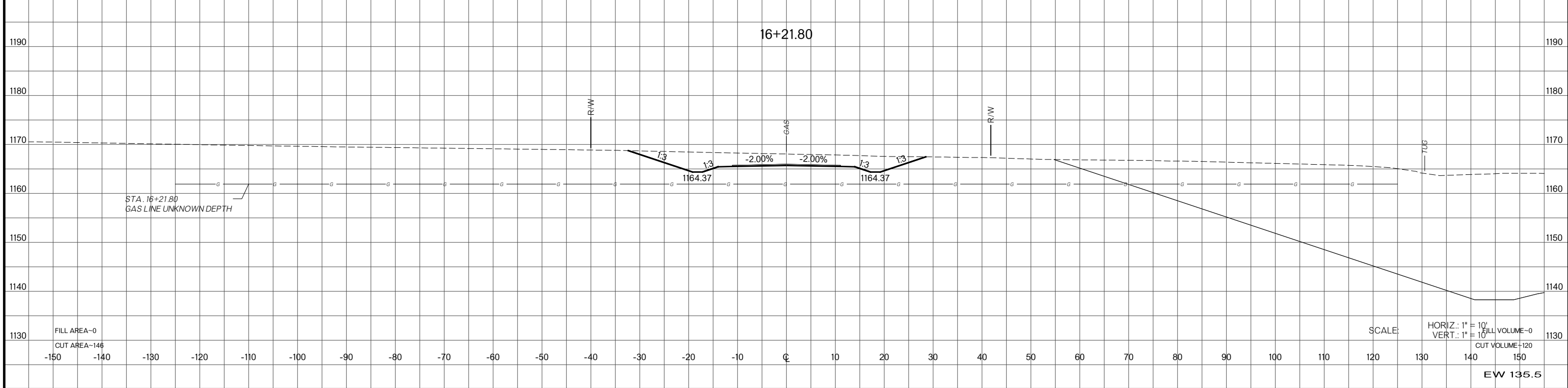
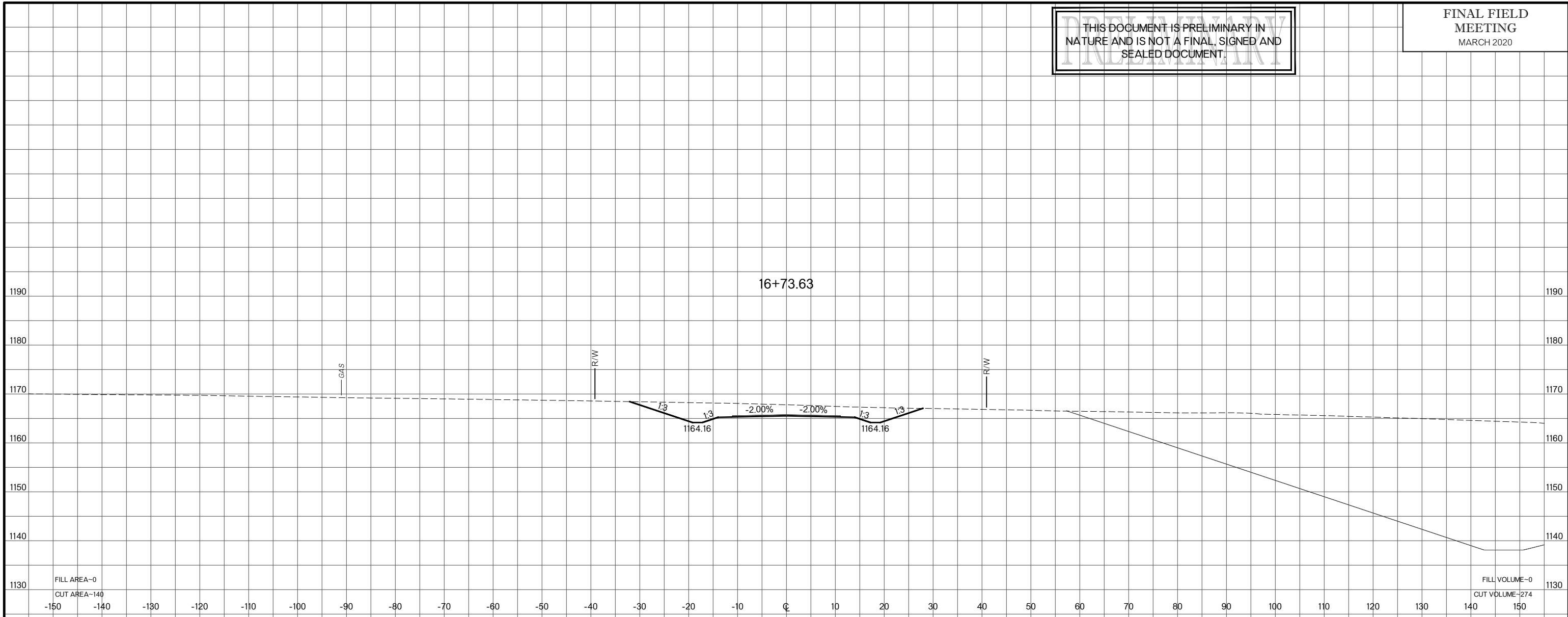
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

EW 135.5

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



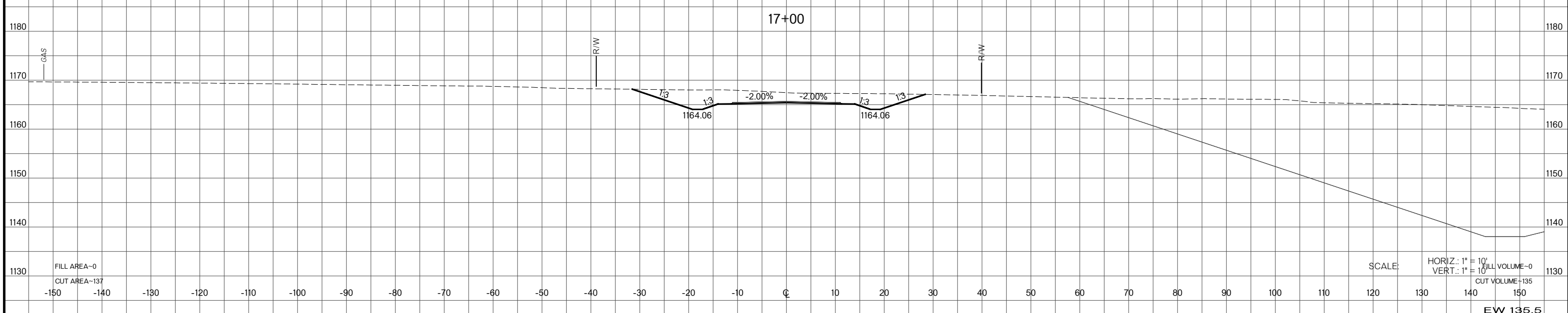
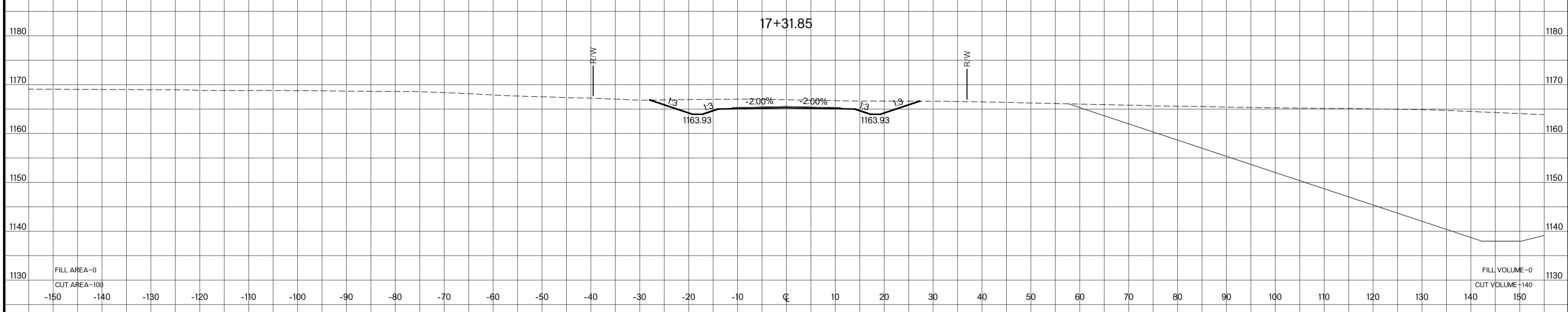
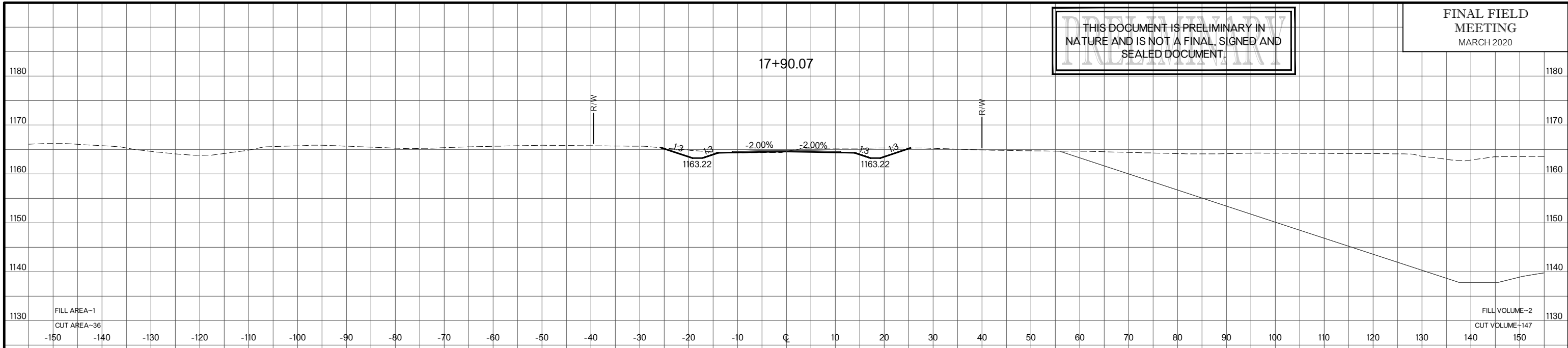
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

EW 135.5

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



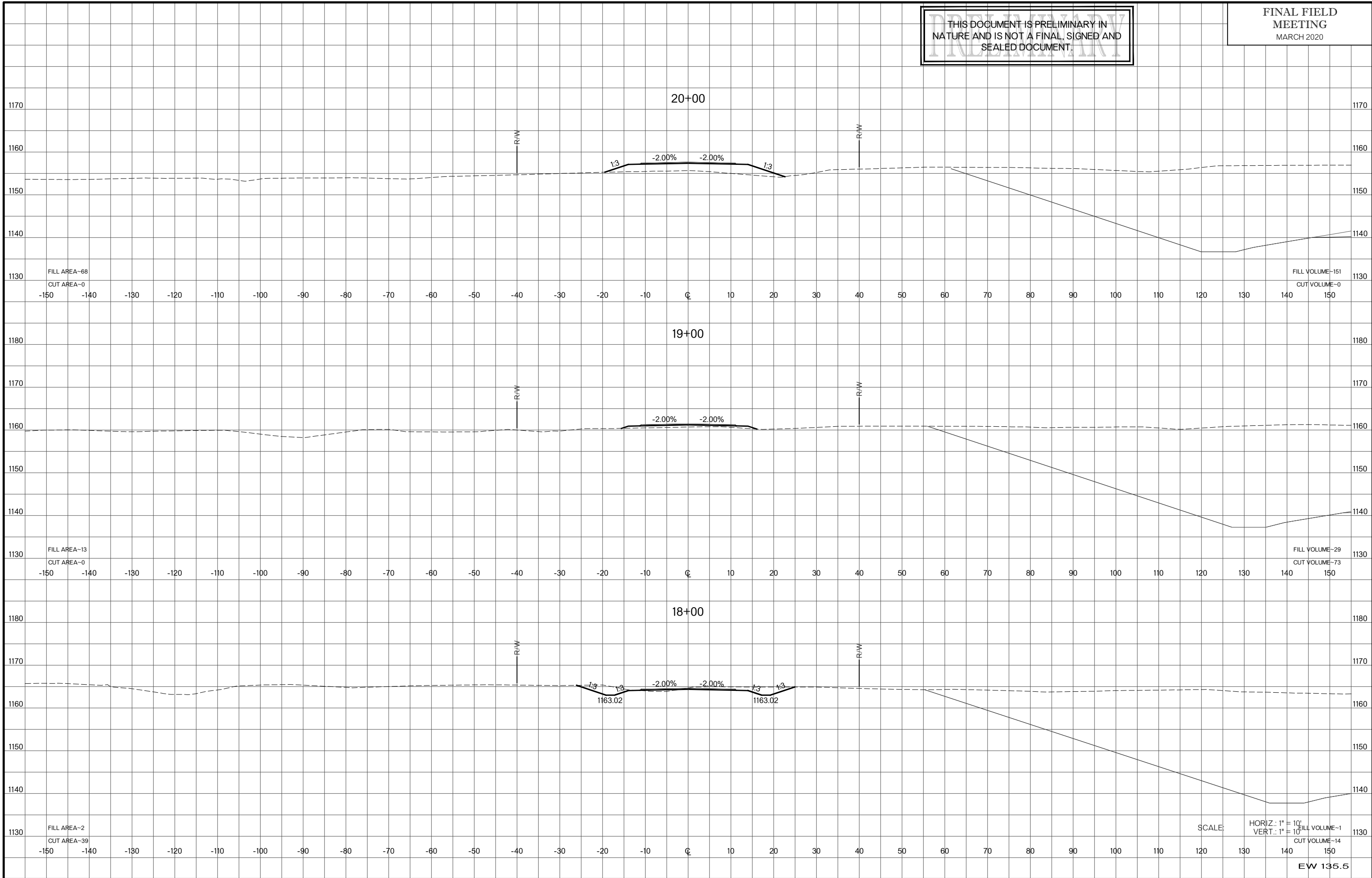
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

EW 135.5

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



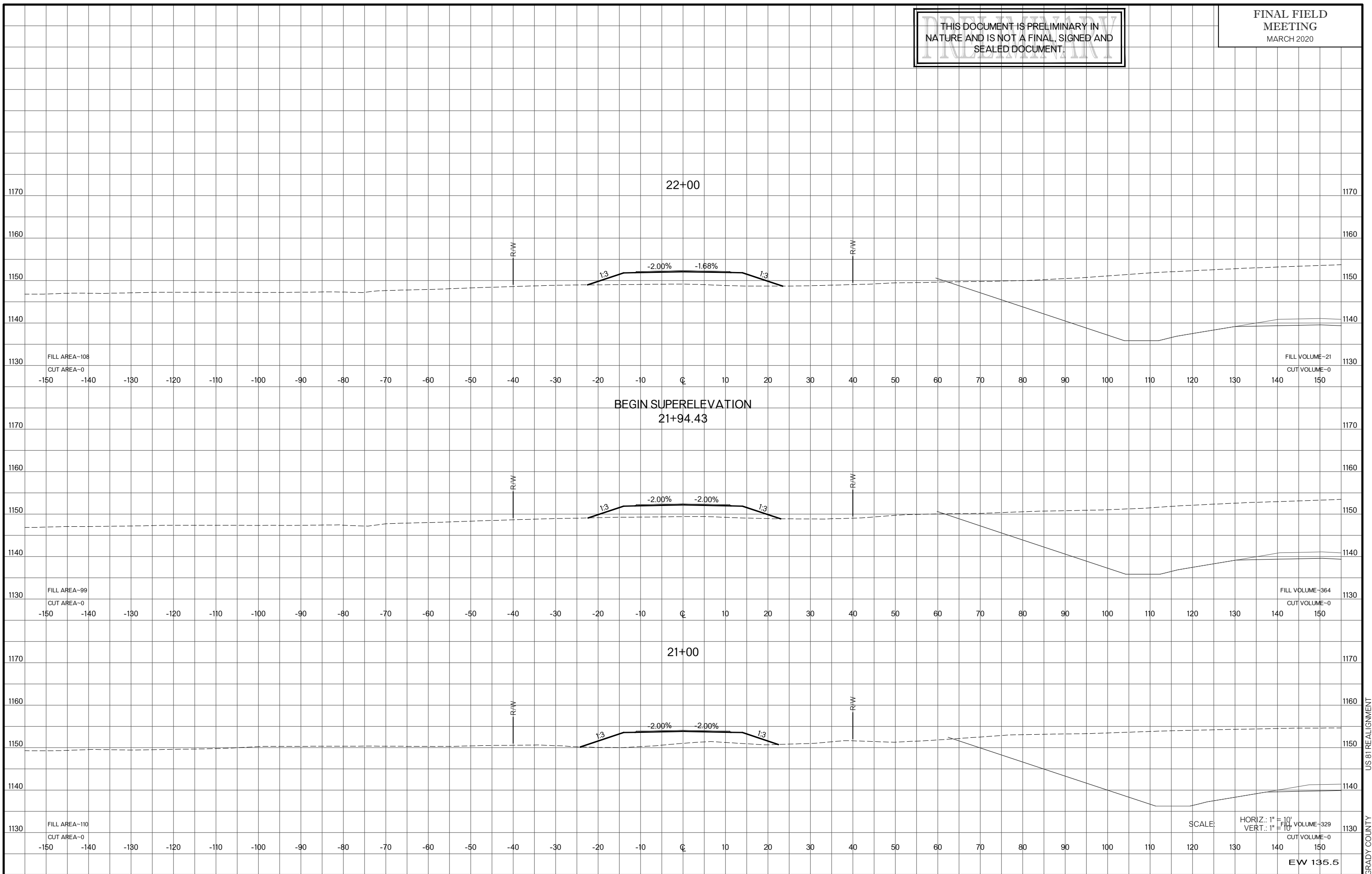
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

EW 135.5

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



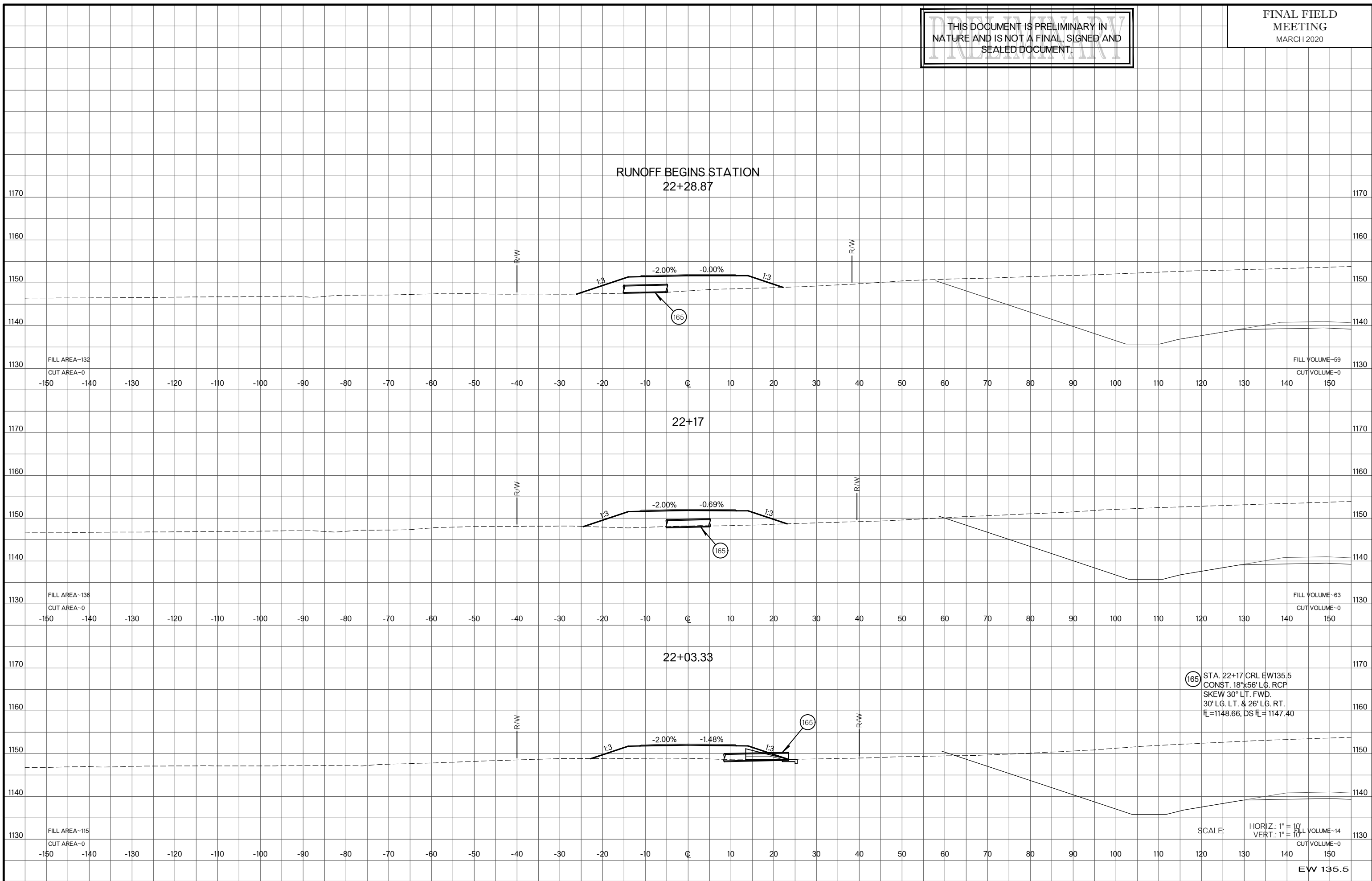
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

EW 135.5

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



RUNOFF BEGINS STATION
22+28.87

22+17

22+03.33

165 STA. 22+17 CRL EW135.5
CONST. 18"x56" LG. RCP
SKEW 30° LT. FWD.
30' LG. LT. & 26' LG. RT.
TL=1148.66, DS TL=1147.40

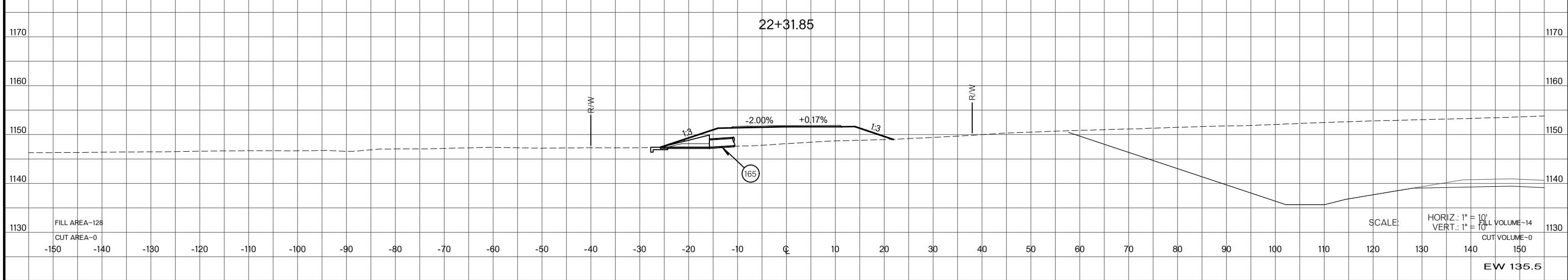
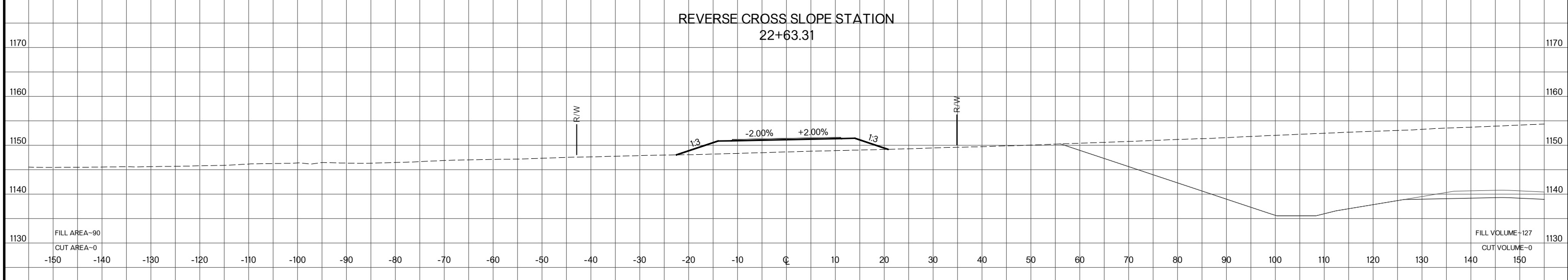
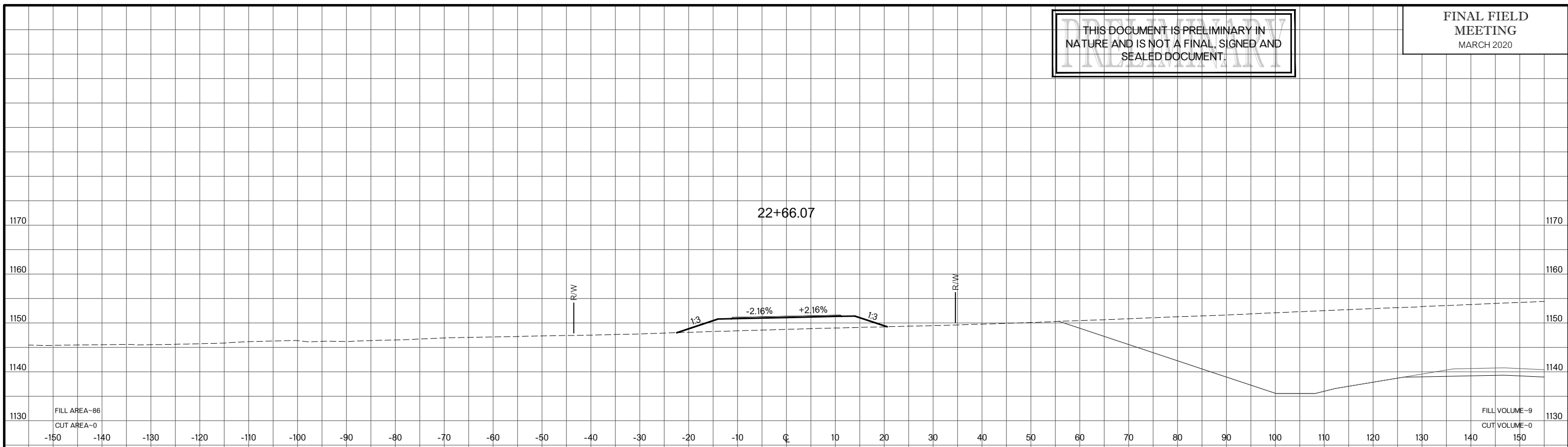
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

EW 135.5

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



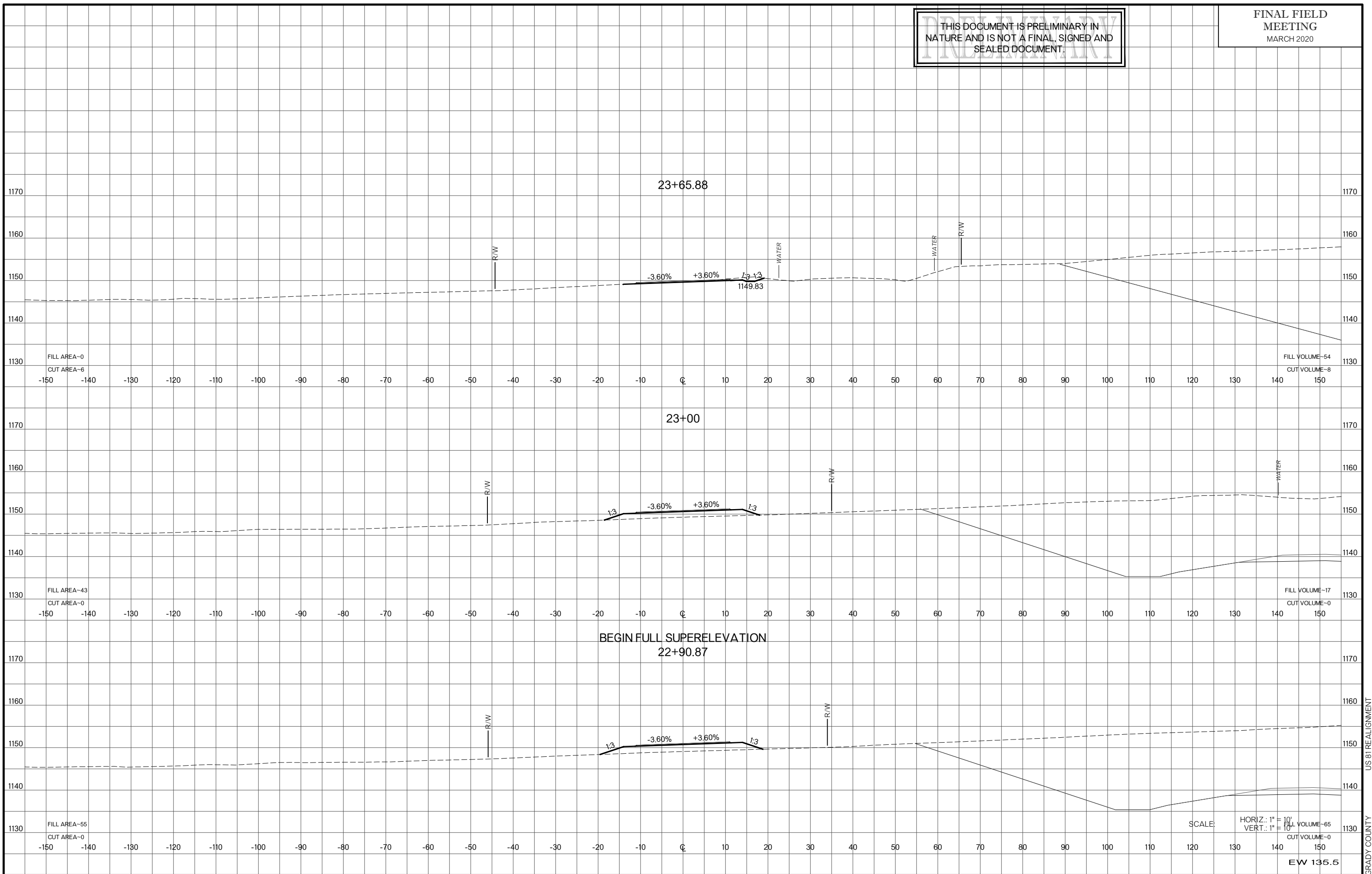
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

EW 135.5

US 81 REALIGNMENT
GRADY COUNTY

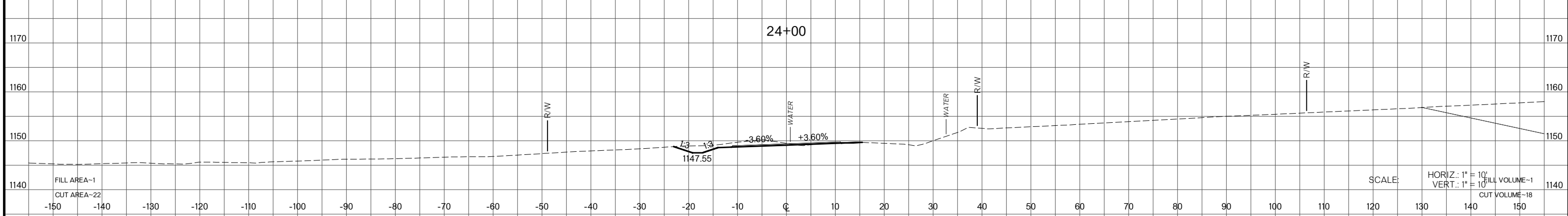
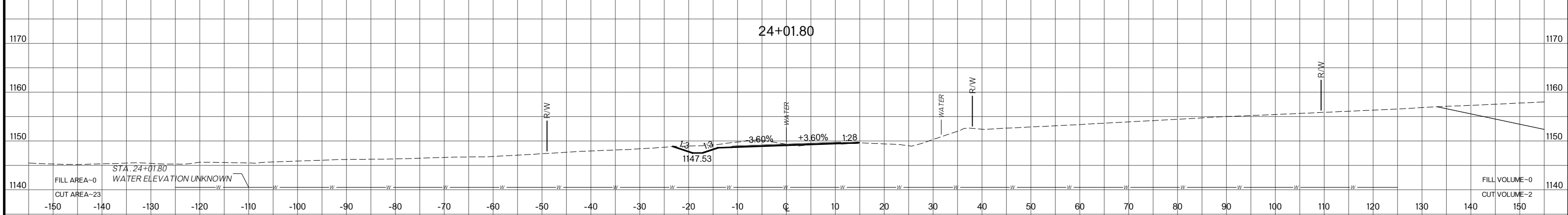
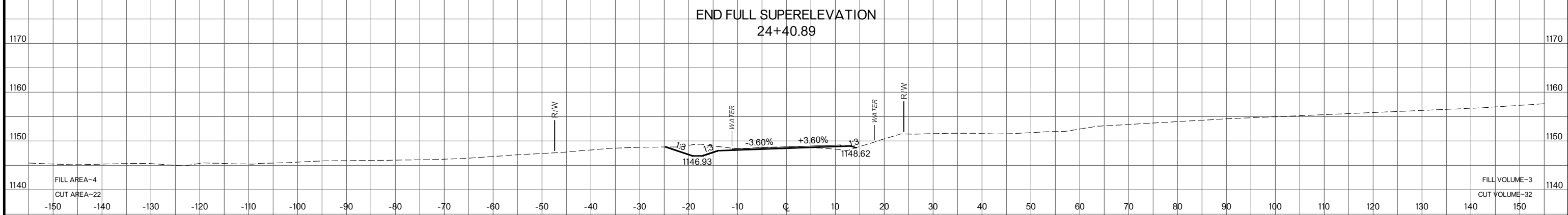
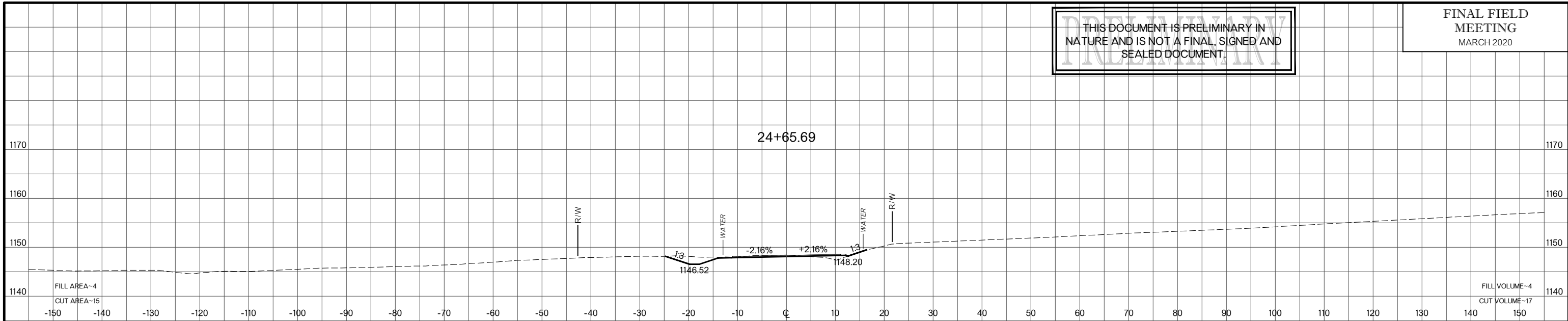
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



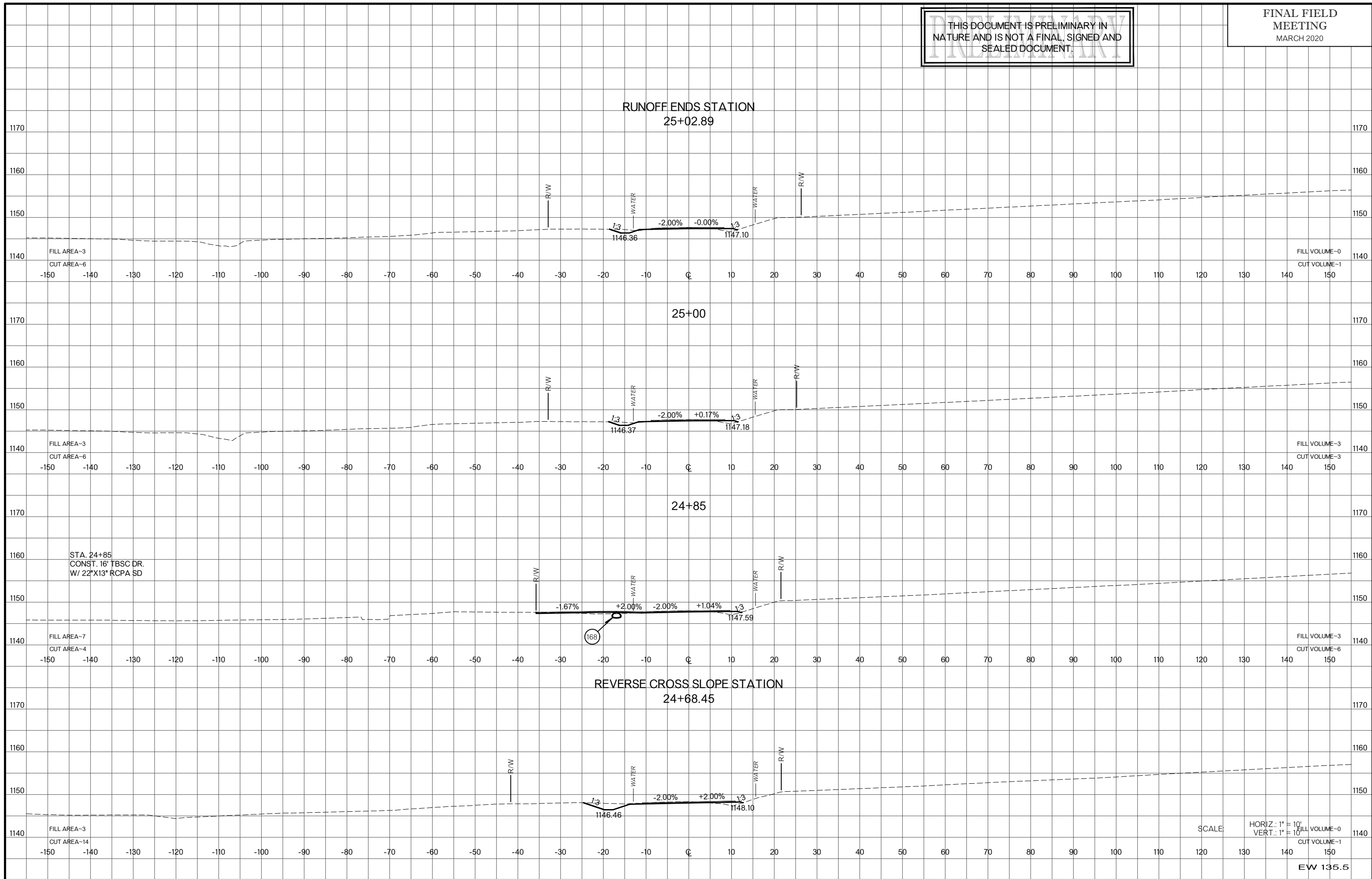
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

EW 135.5

US 81 REALIGNMENT
GRADY COUNTY

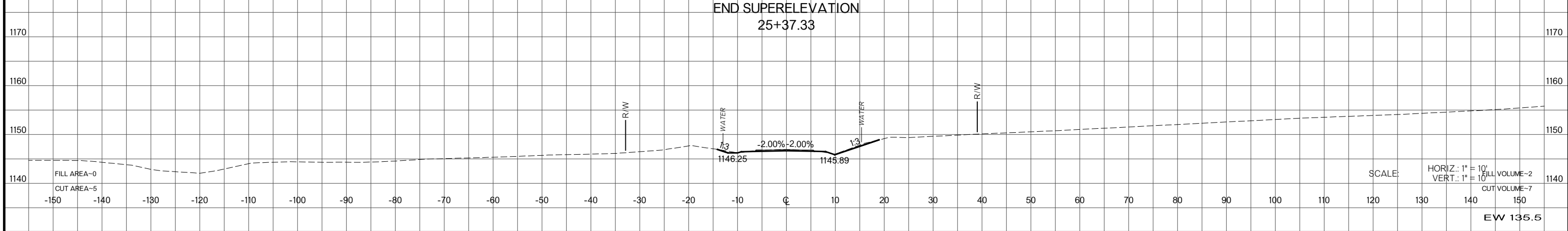
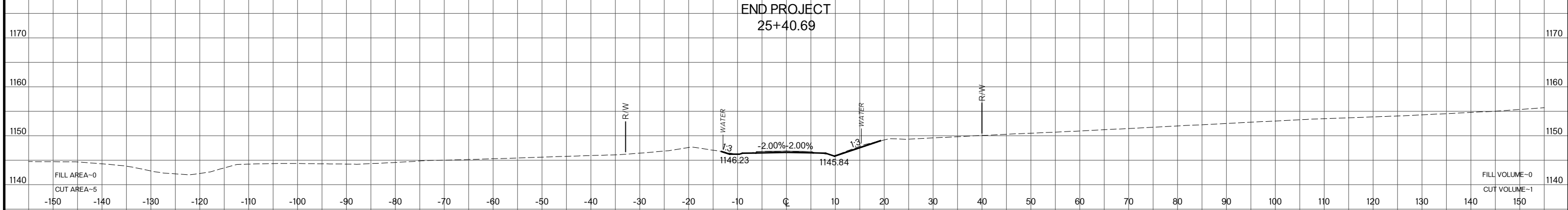
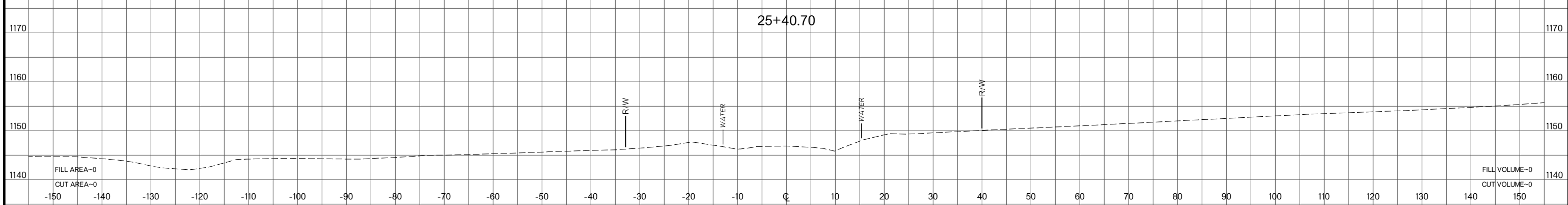
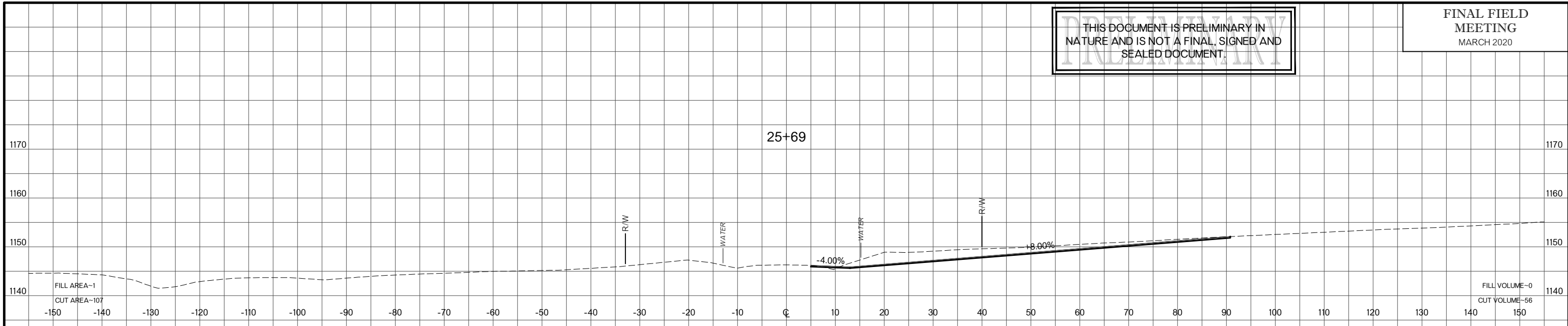
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020

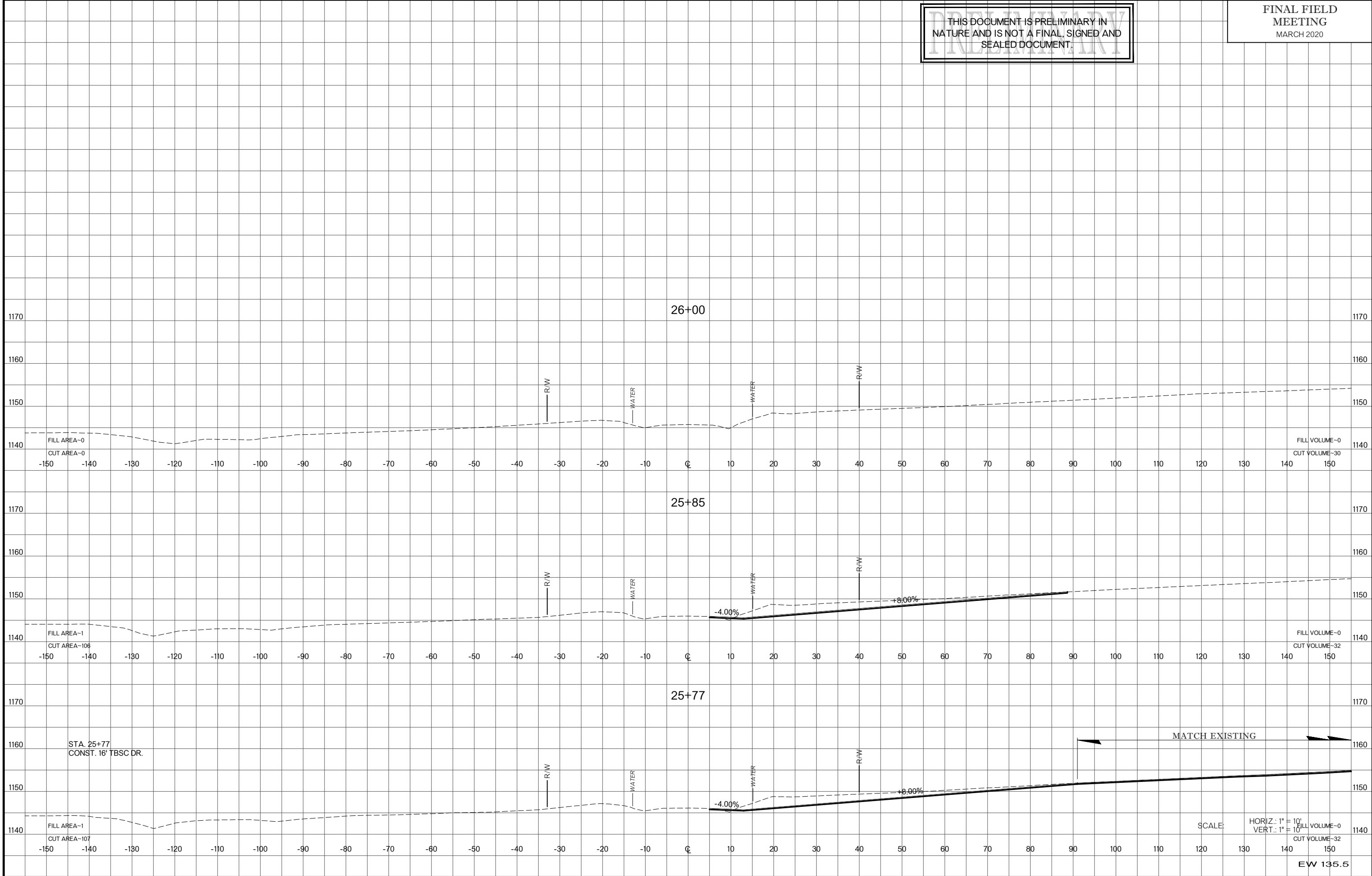


SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

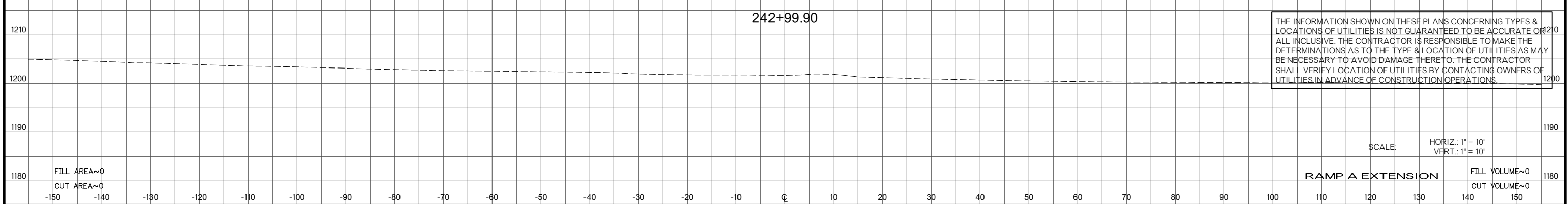
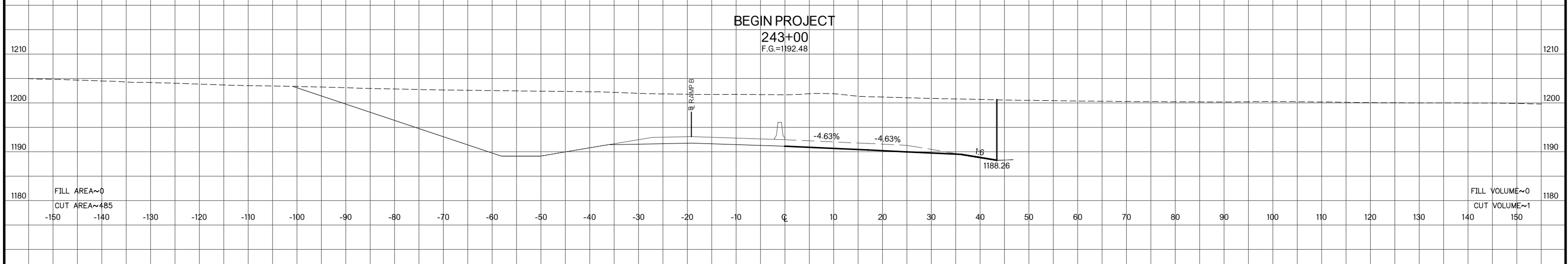
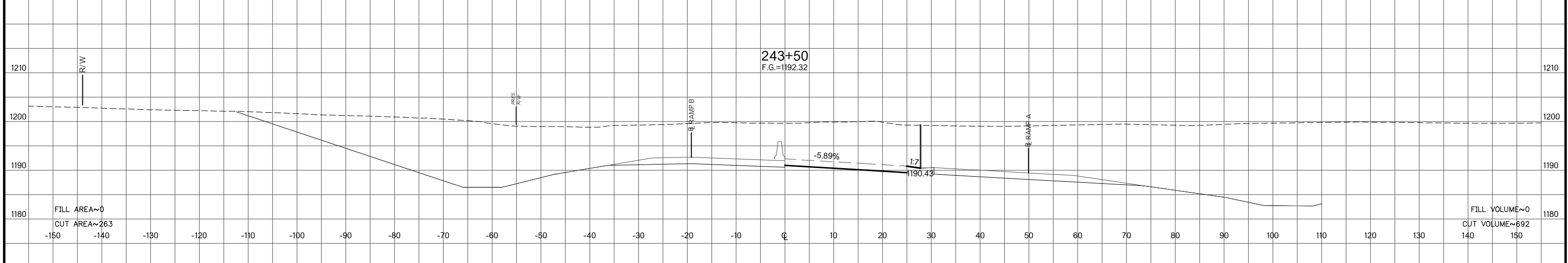
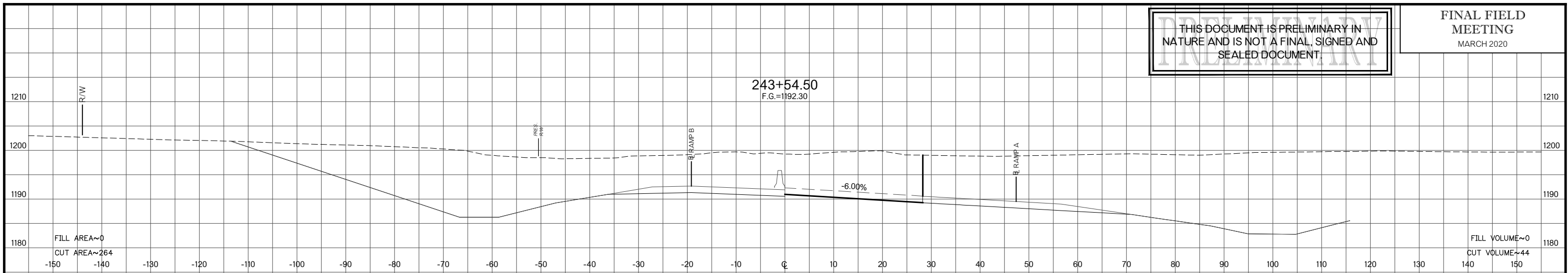


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

EW 135.5

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



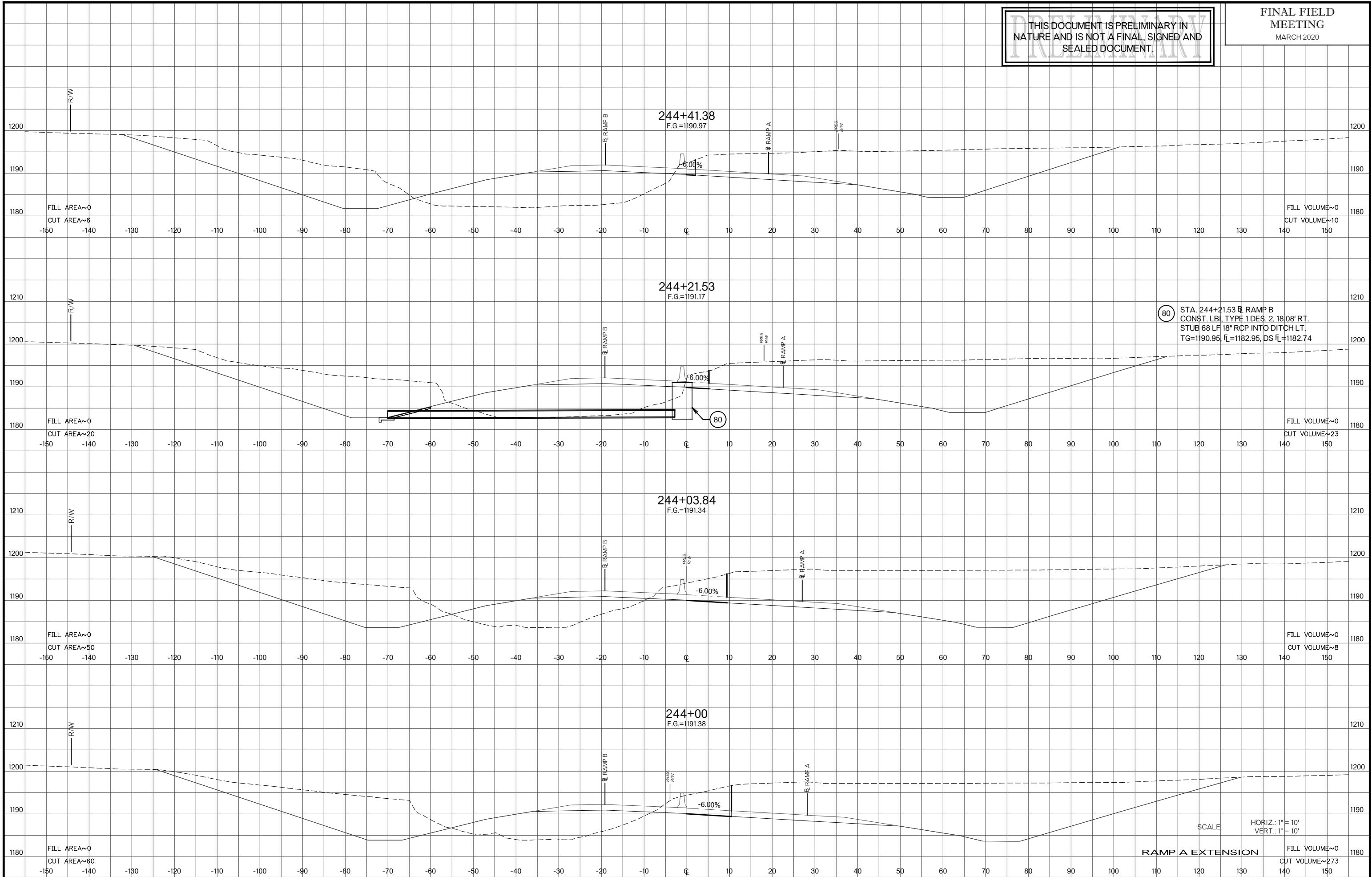
THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

SCALE HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP A EXTENSION

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

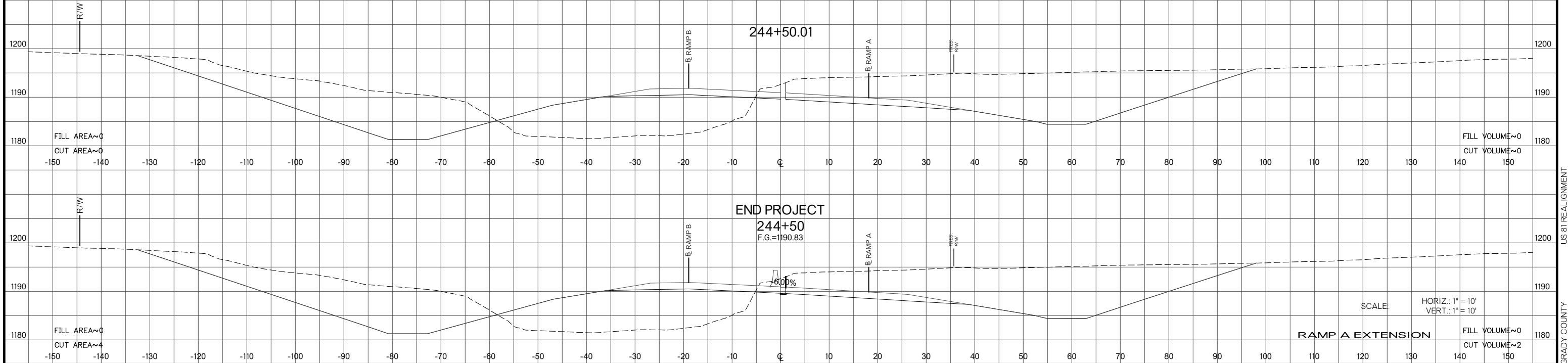
RAMP A EXTENSION

FILL VOLUME~0
CUT VOLUME~273

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

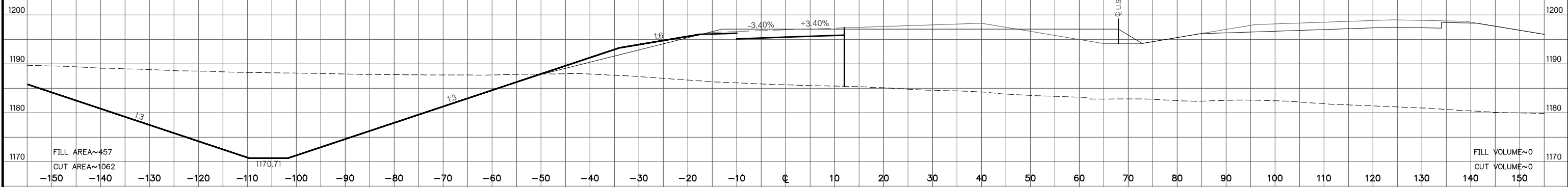
FINAL FIELD MEETING
MARCH 2020



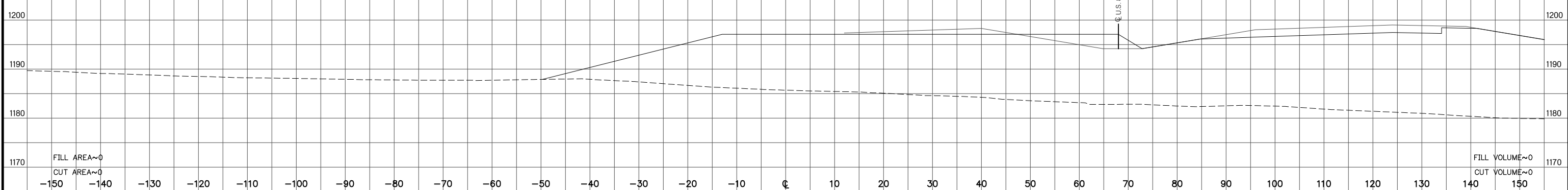
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

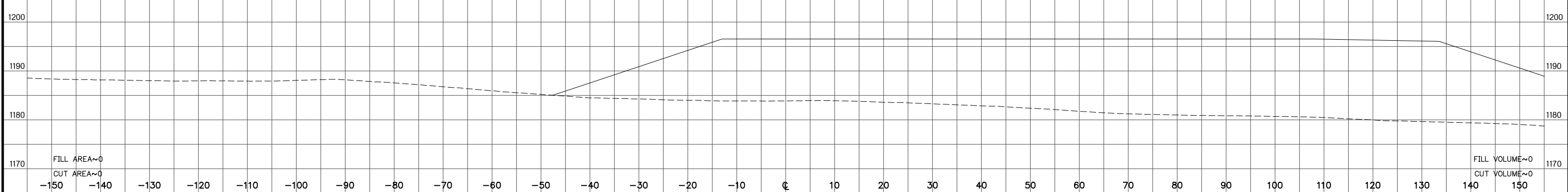
BEGIN PROJECT 142+15.57 F.G.=1196.94



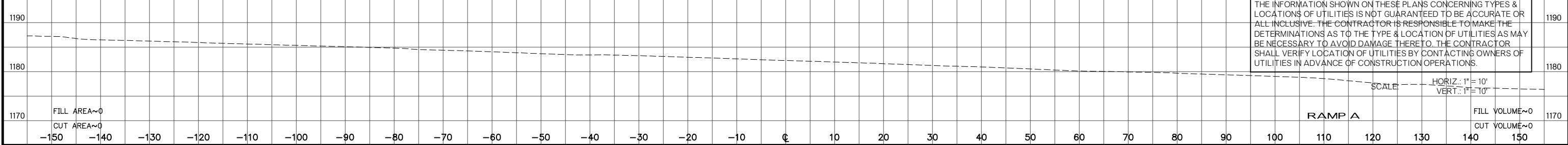
END INCIDENTAL CONSTRUCTION 142+15.56



141+84.17



BEGIN INCIDENTAL CONSTRUCTION 141+35.10



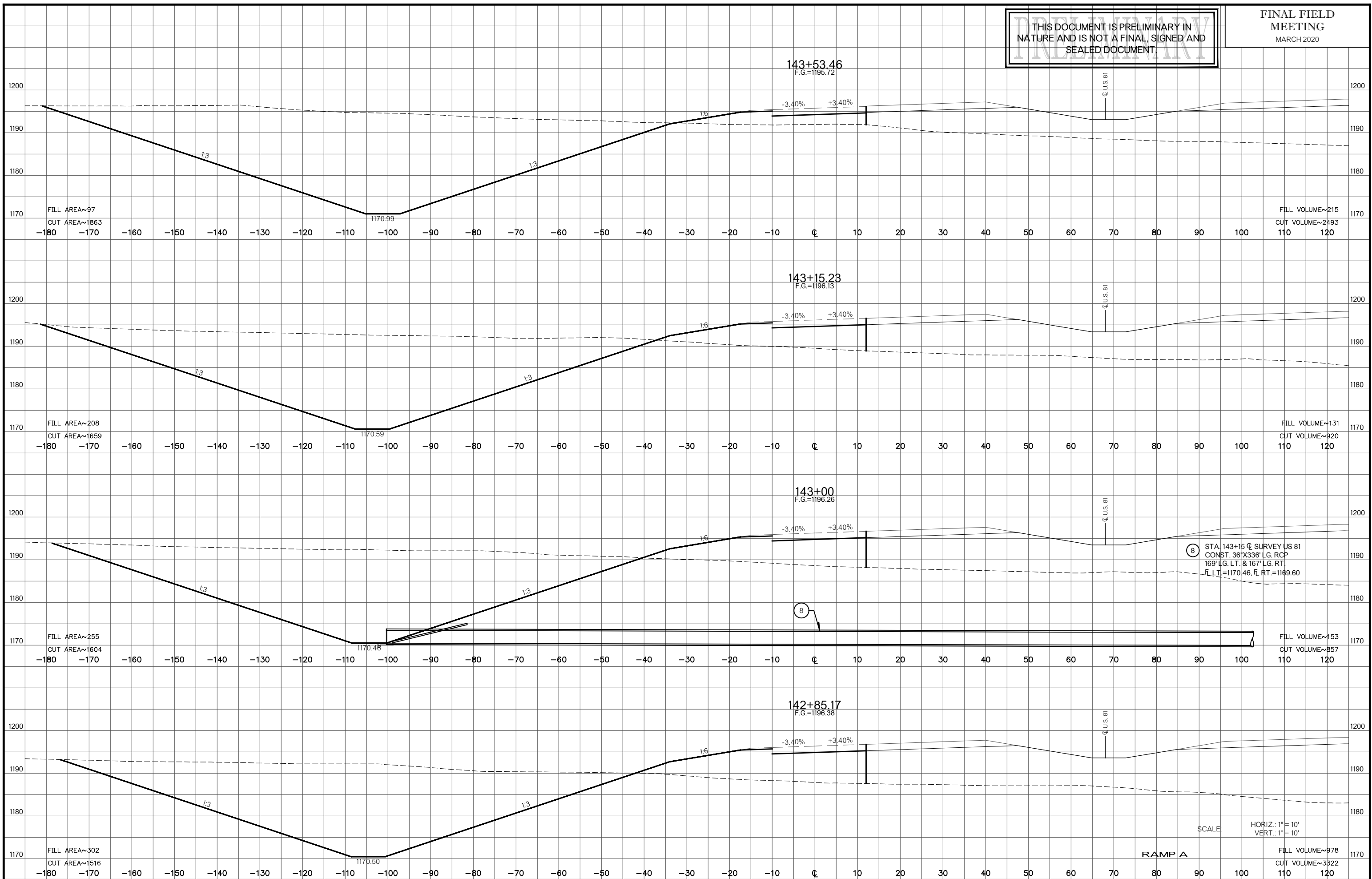
THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERE TO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

SCALE: HORIZ: 1"=10' VERT: 1"=10'

RAMP A

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



8 STA. 143+15 Q SURVEY US 81
CONST. 36'x336' LG. ROP
169' LG. LT. & 167' LG. RT.
FL LT.=1170.46, FL RT.=1169.60

8

SCALE HORIZ.: 1" = 10'
VERT.: 1" = 10'

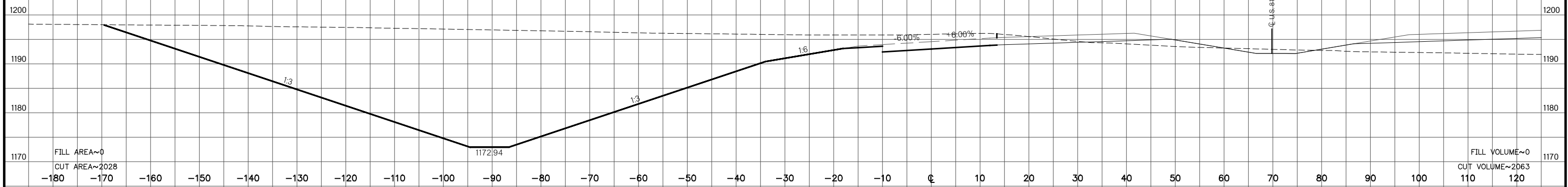
RAMP A

US 81 REALIGNMENT
GRADY COUNTY

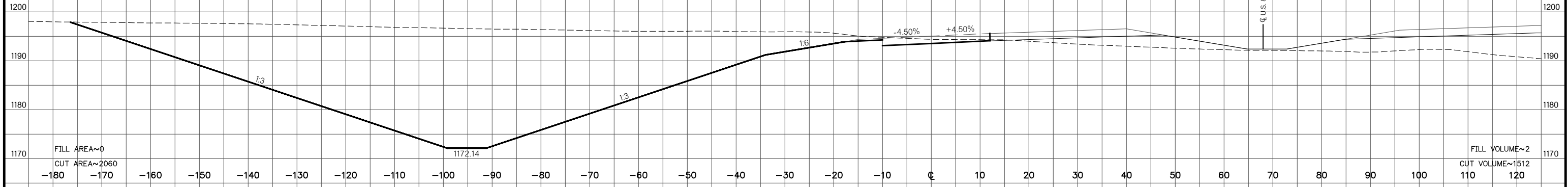
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

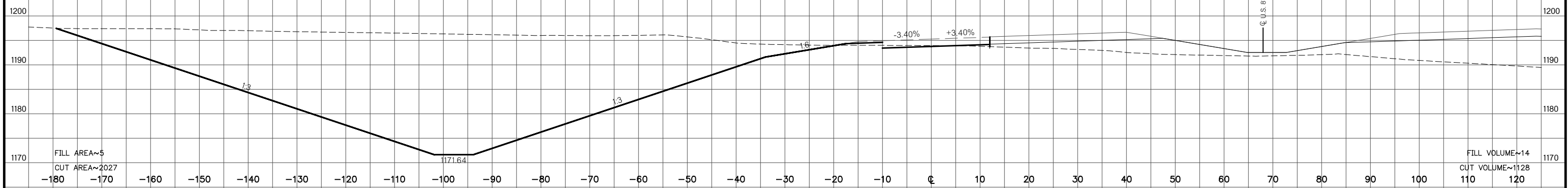
BEGIN FULL SUPERELEVATION
144+62.46
F.G.=1194.53



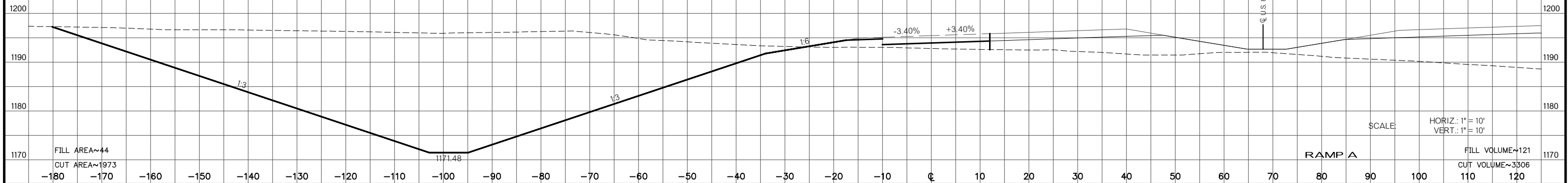
144+35.21
F.G.=1195.04



BEGIN SUPERELEVATION
144+15.23
F.G.=1195.27



144+00
F.G.=1195.45

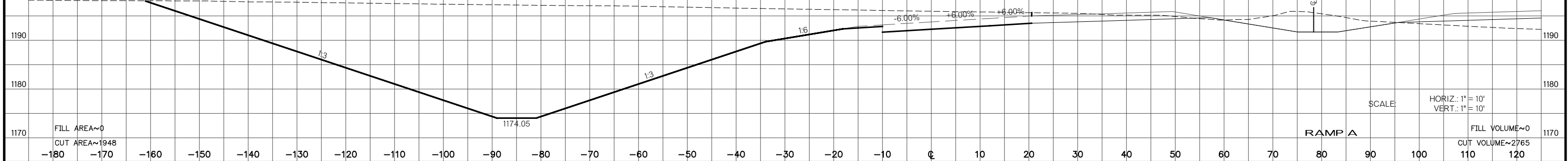
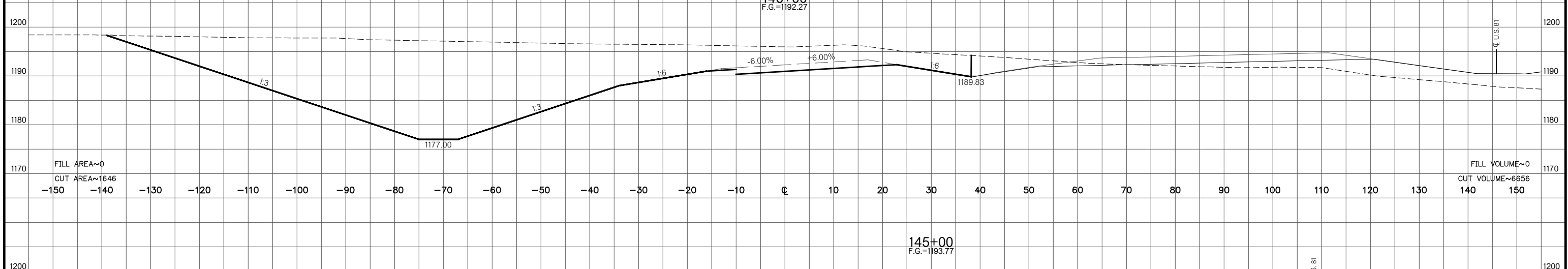
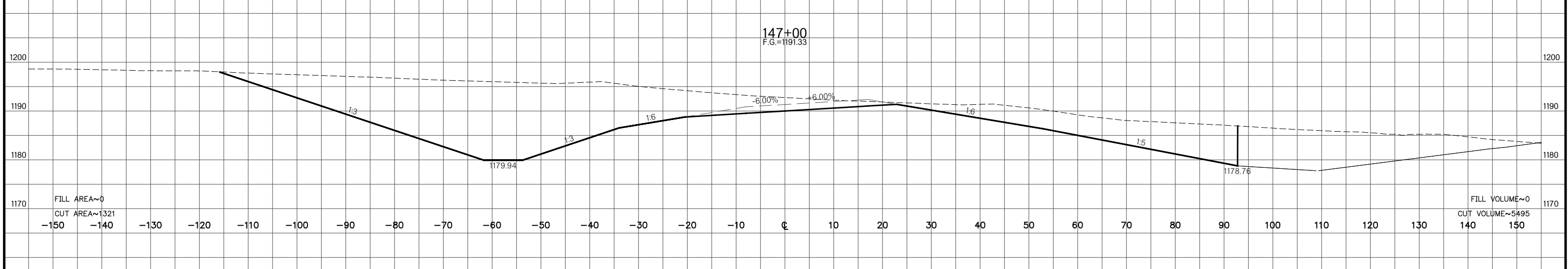
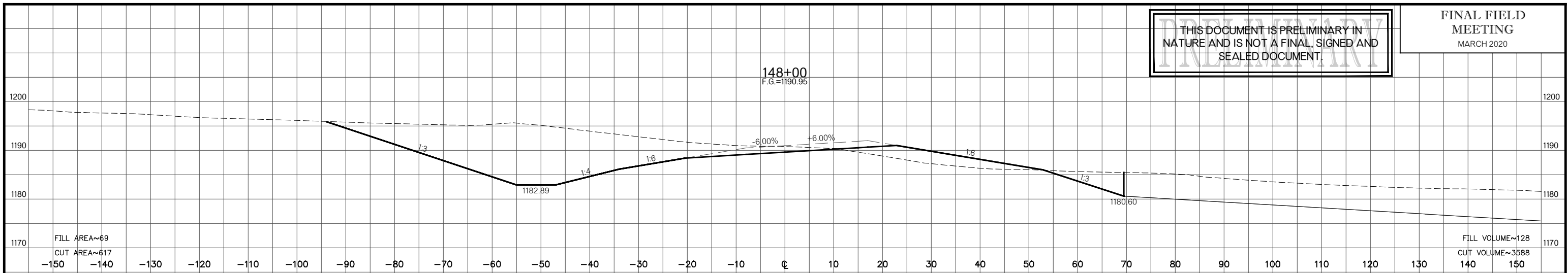


SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP A

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



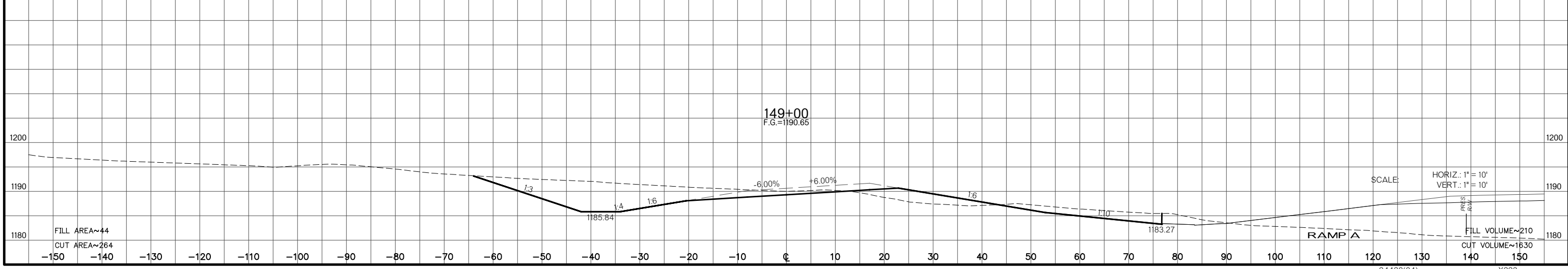
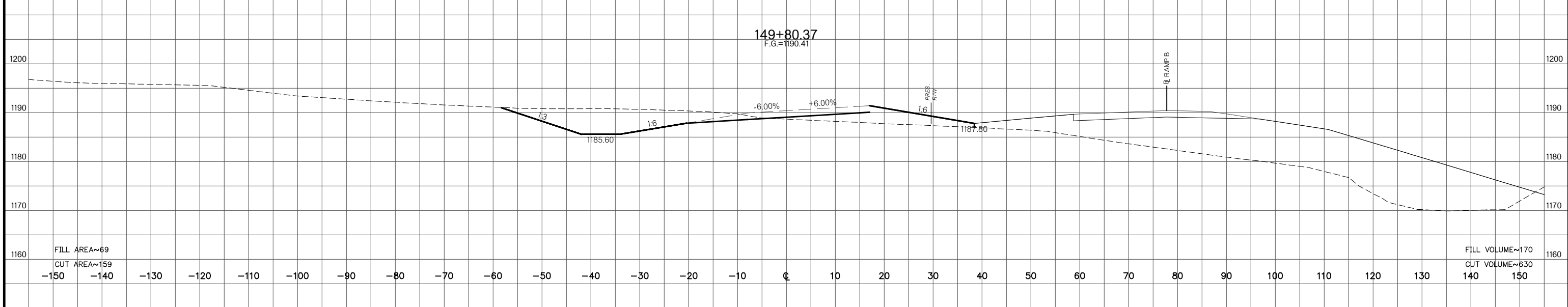
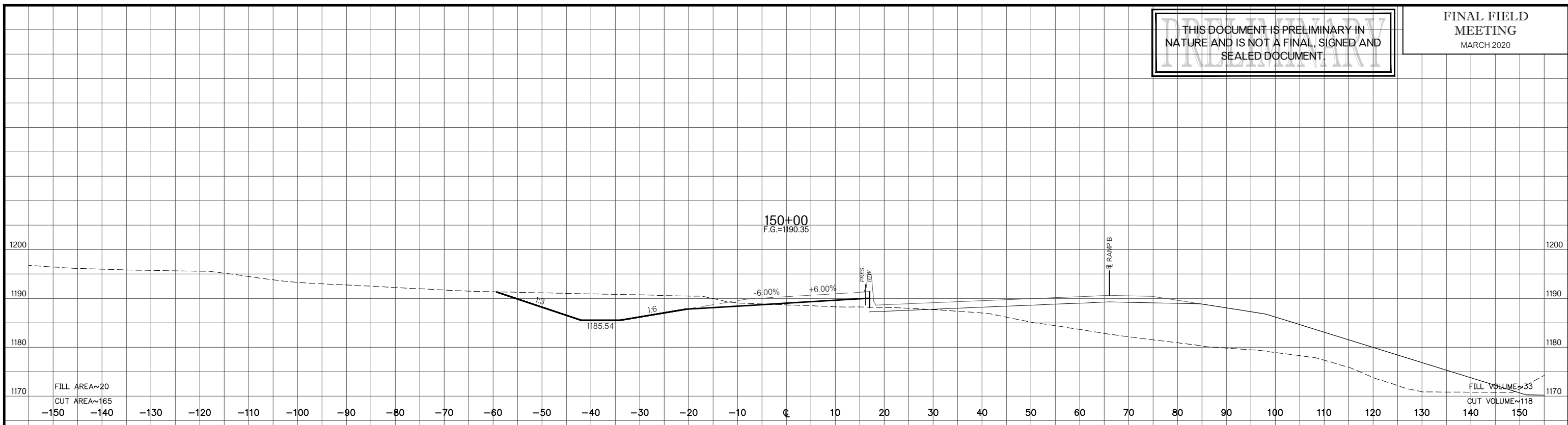
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP A

State Job No. 24428(04) Sheet No. X222

US 81 REALIGNMENT
GRADY COUNTY

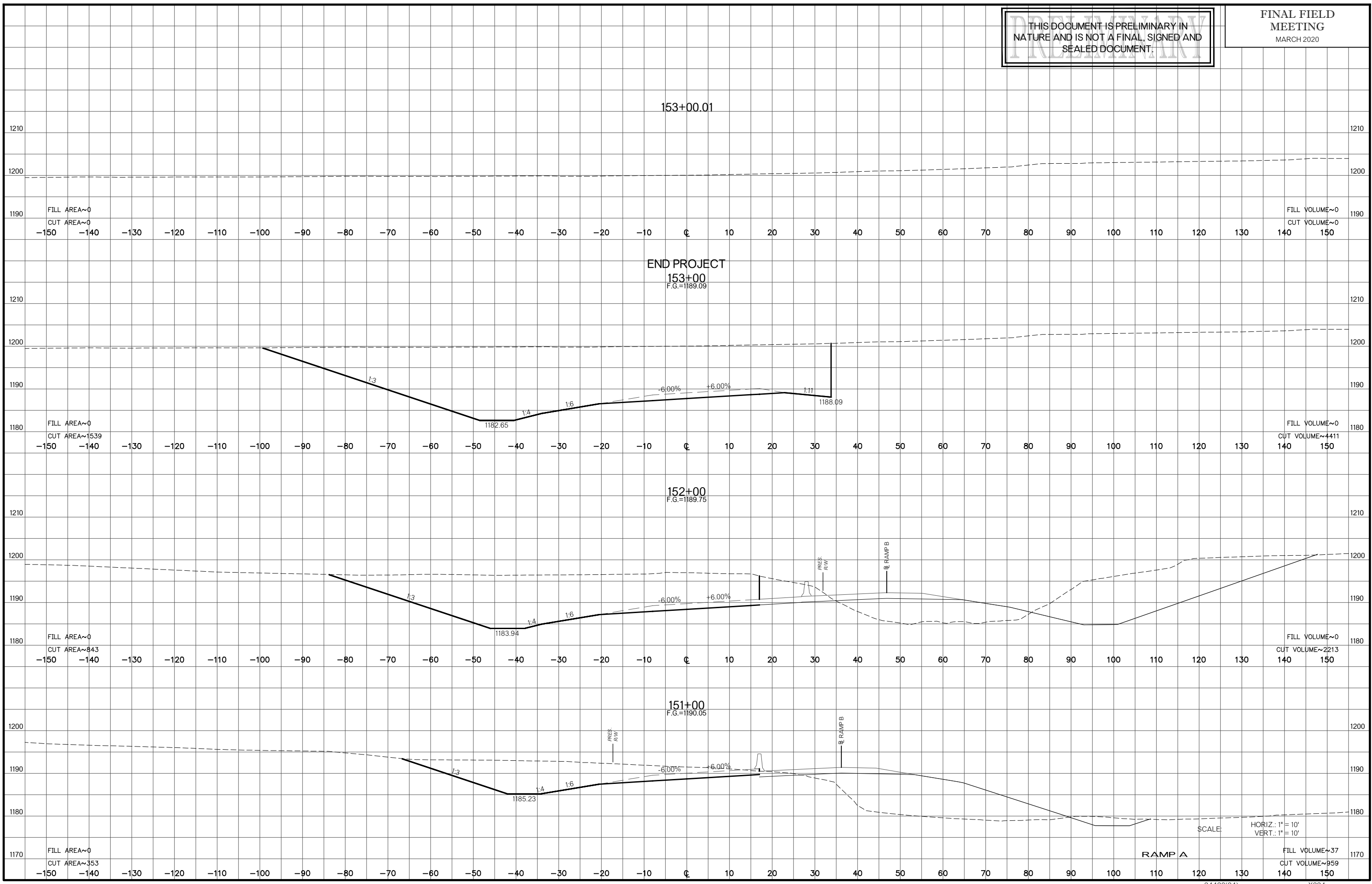
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

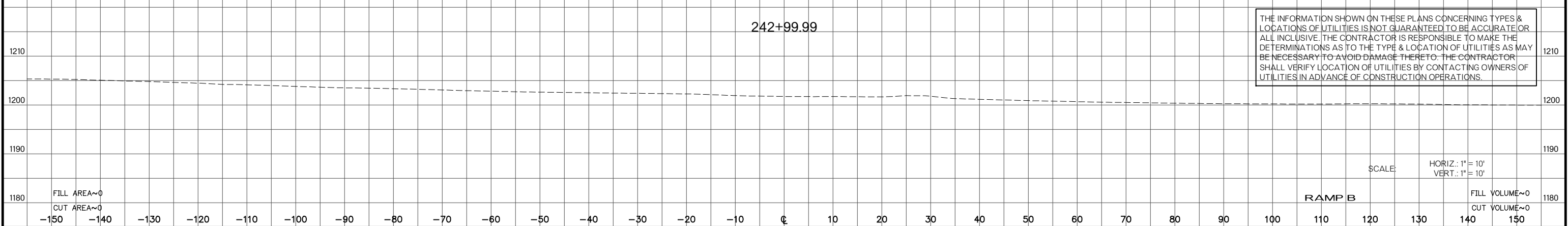
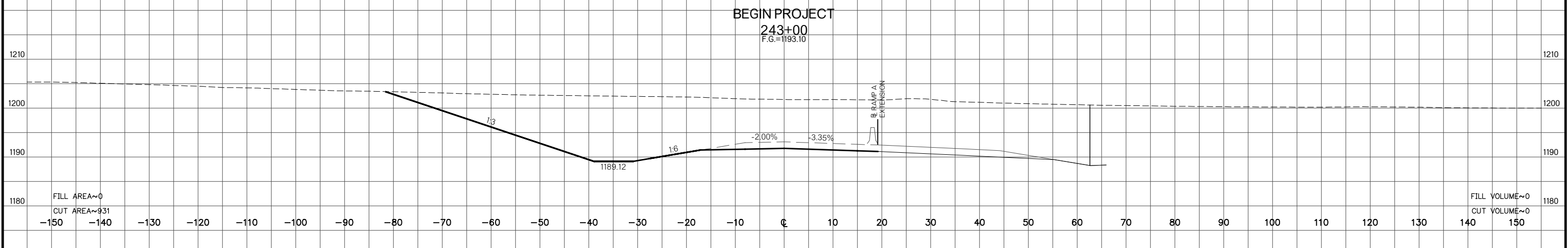
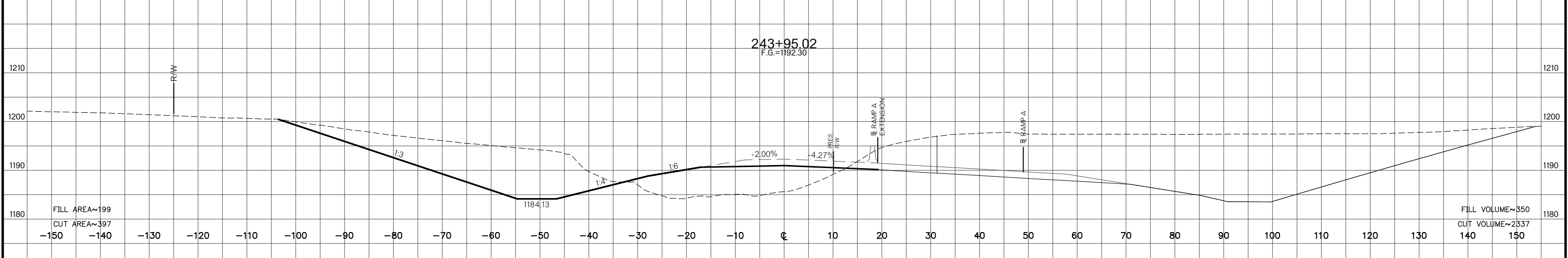
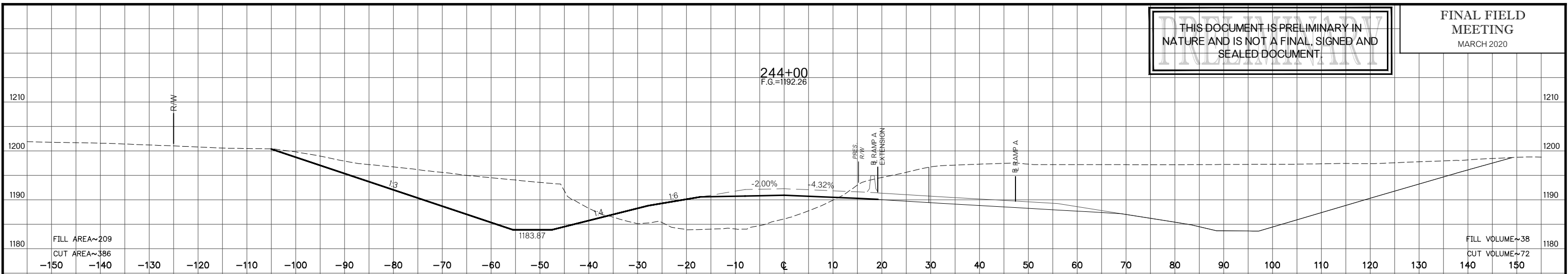
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

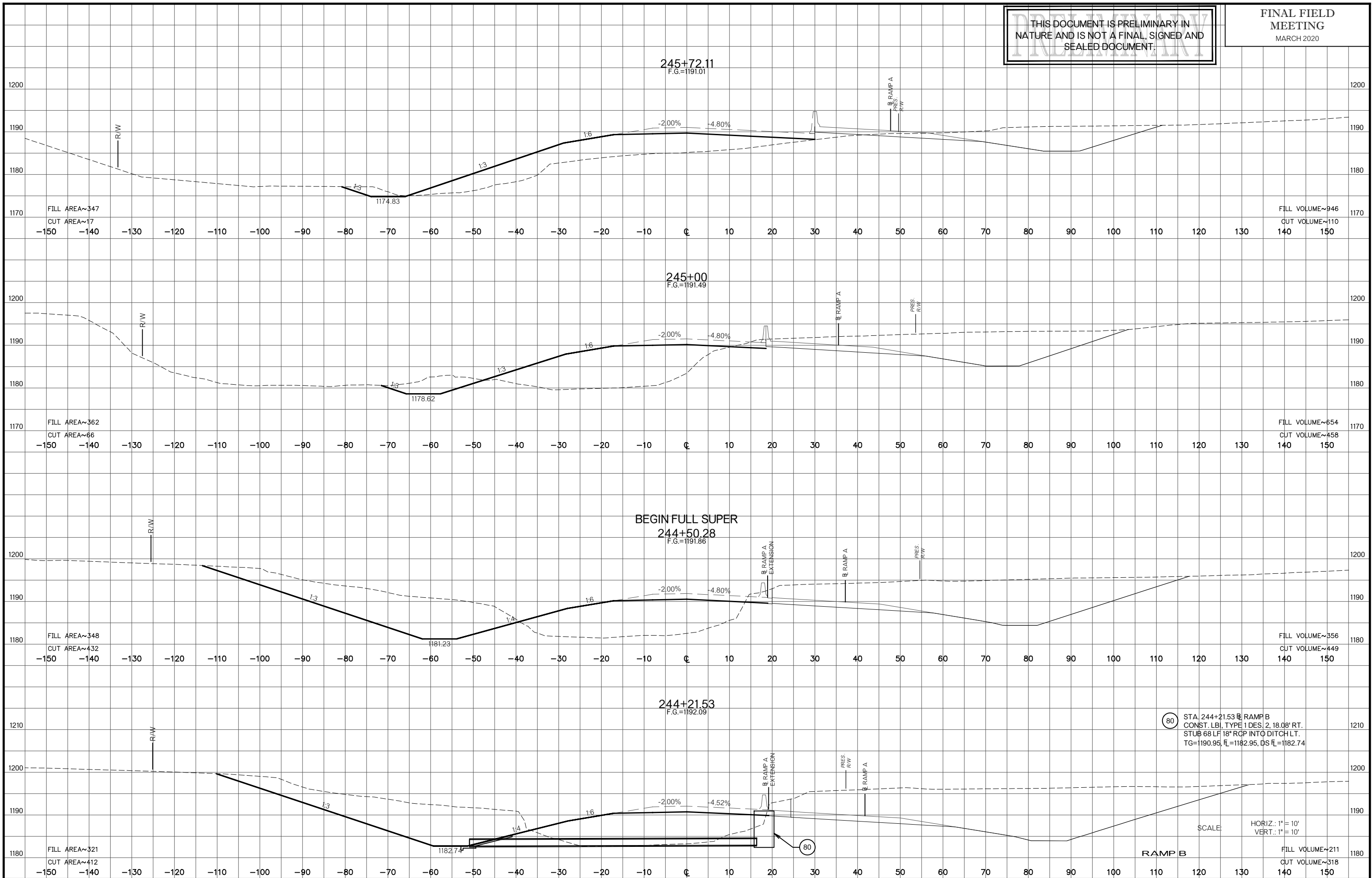
RAMP B

FILL VOLUME~0
CUT VOLUME~0

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



80 STA. 244+21.53 B RAMP B
CONST. LBI, TYPE 1 DES. 2, 18.08' RT.
STUB 68 LF 18" RCP INTO DITCH LT.
TG=1190.95, f_L=1182.95, DS f_L=1182.74

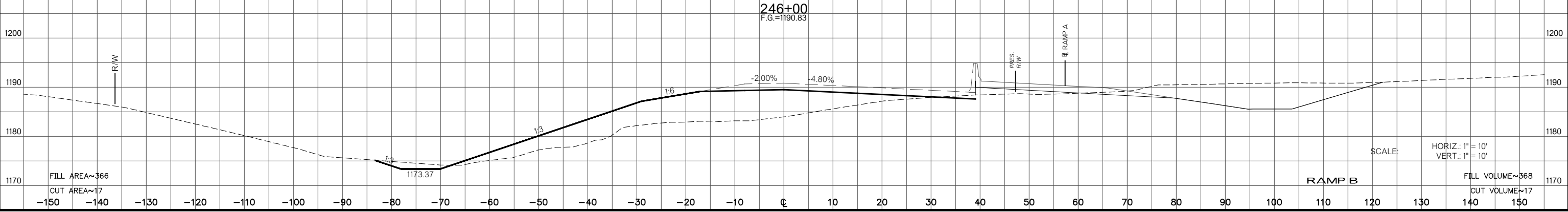
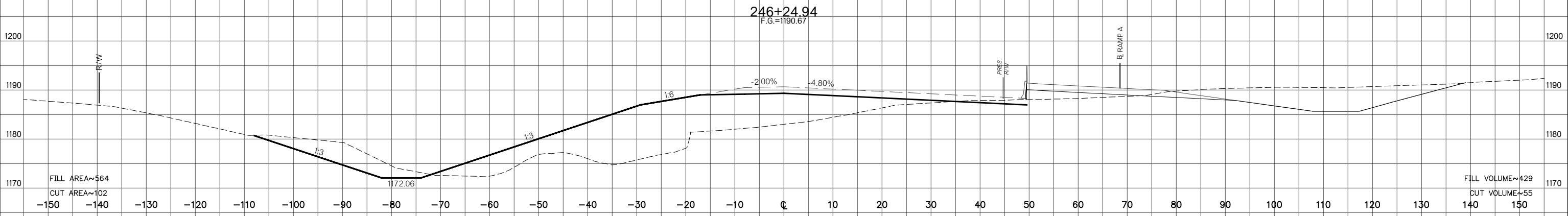
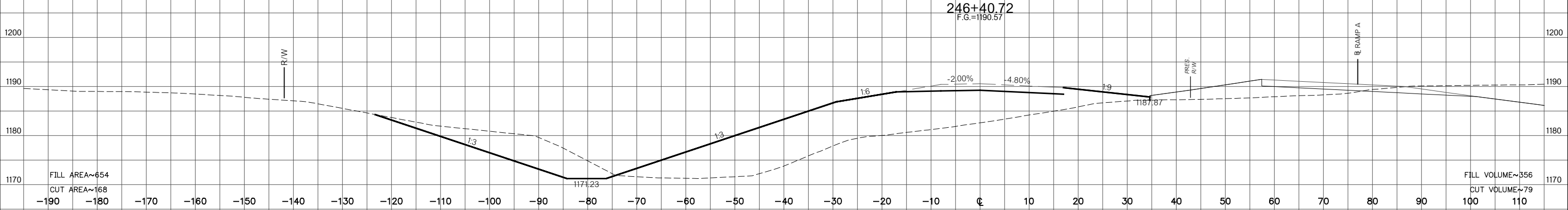
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP B
FILL VOLUME~211
CUT VOLUME~318

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

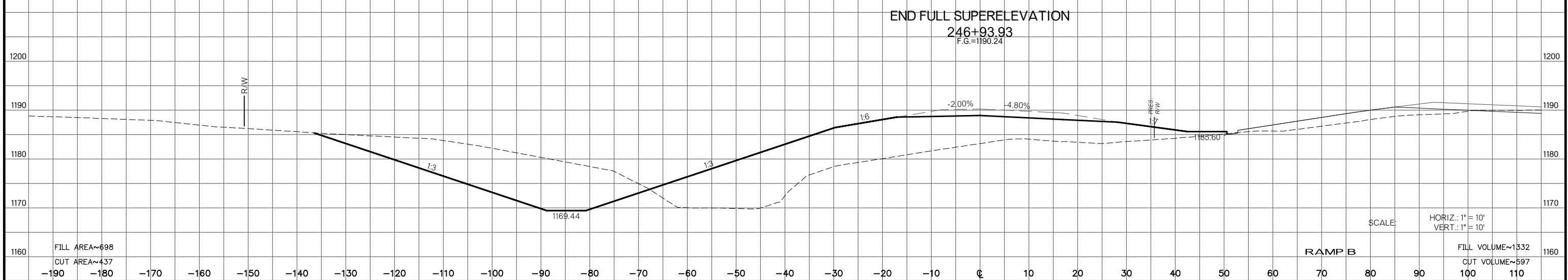
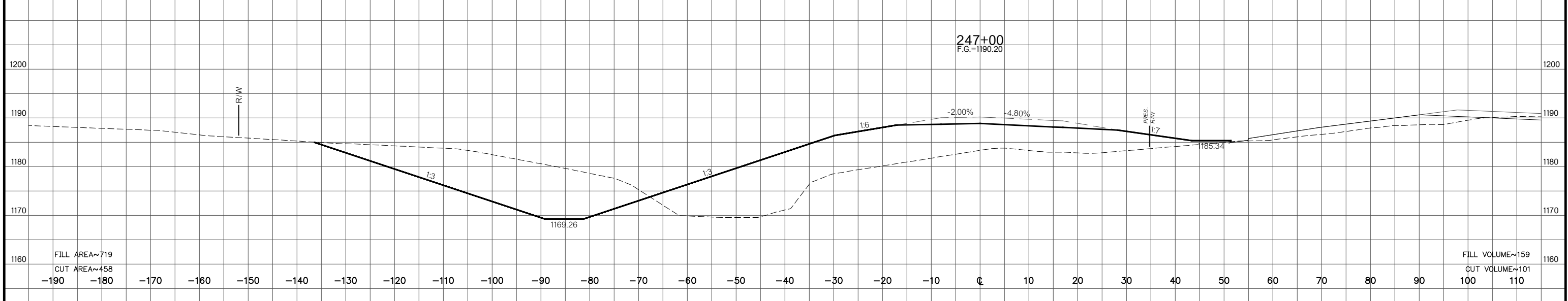
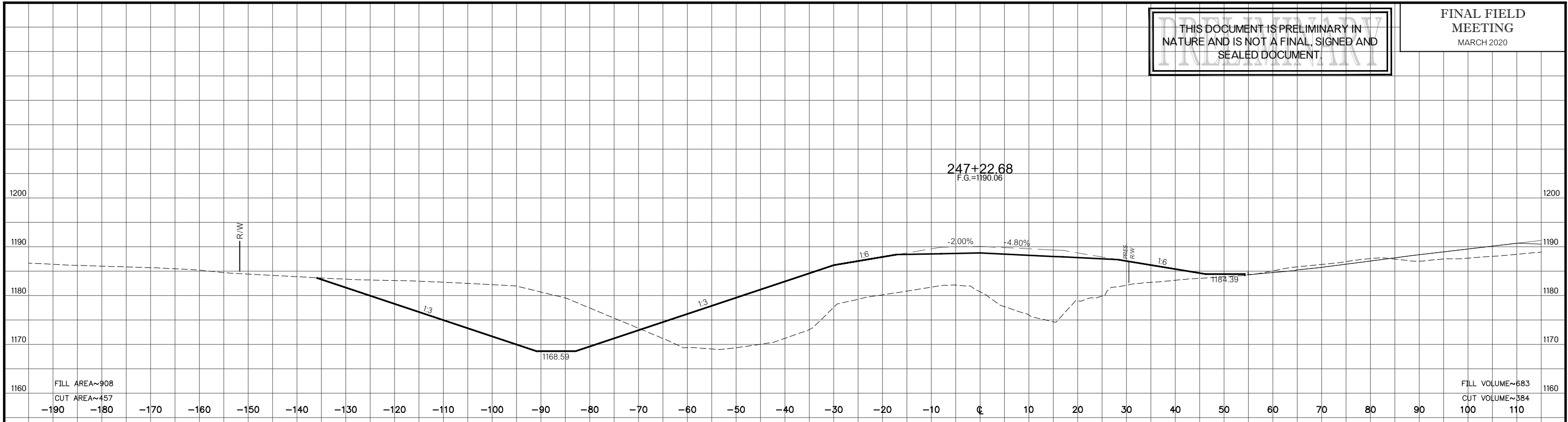
FINAL FIELD
MEETING
MARCH 2020



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'
RAMP B
FILL VOLUME~368
CUT VOLUME~17
State Job No. 24428(04) Sheet No. X227

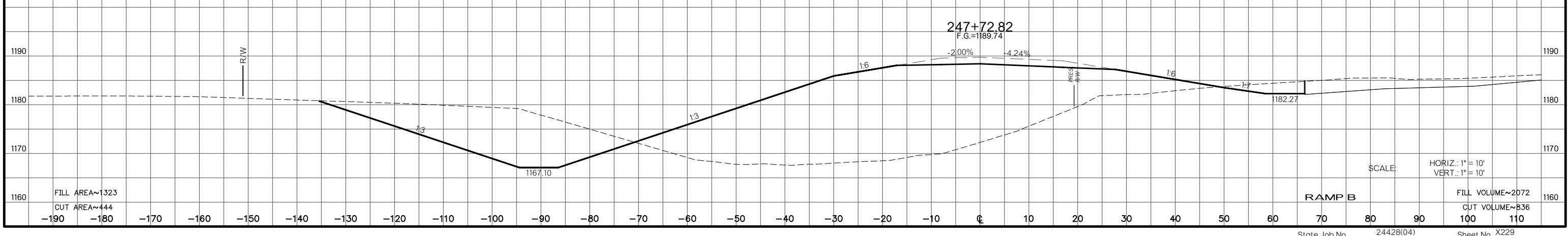
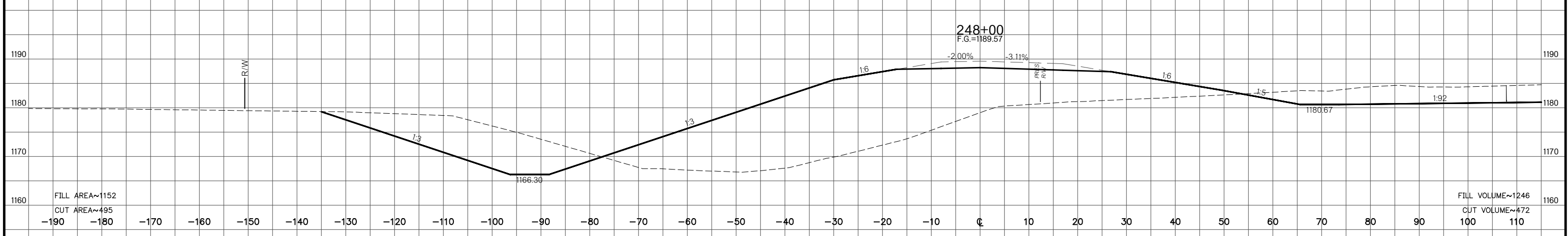
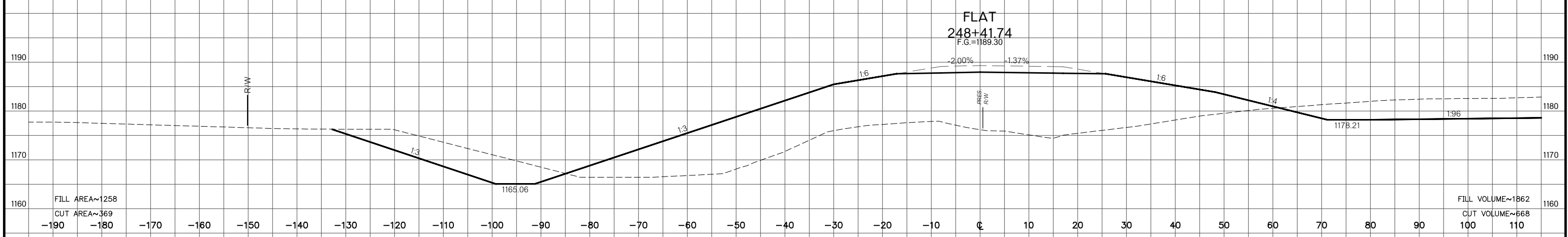
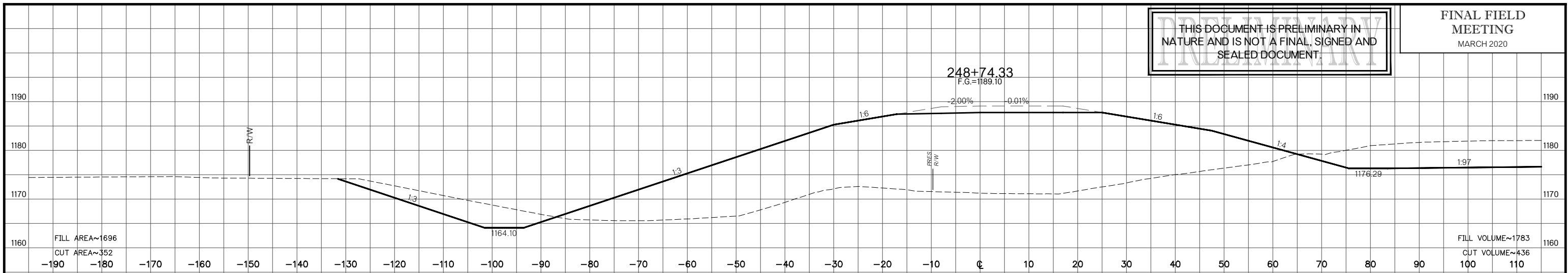
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



SCALE

HORIZ.: 1" = 10'

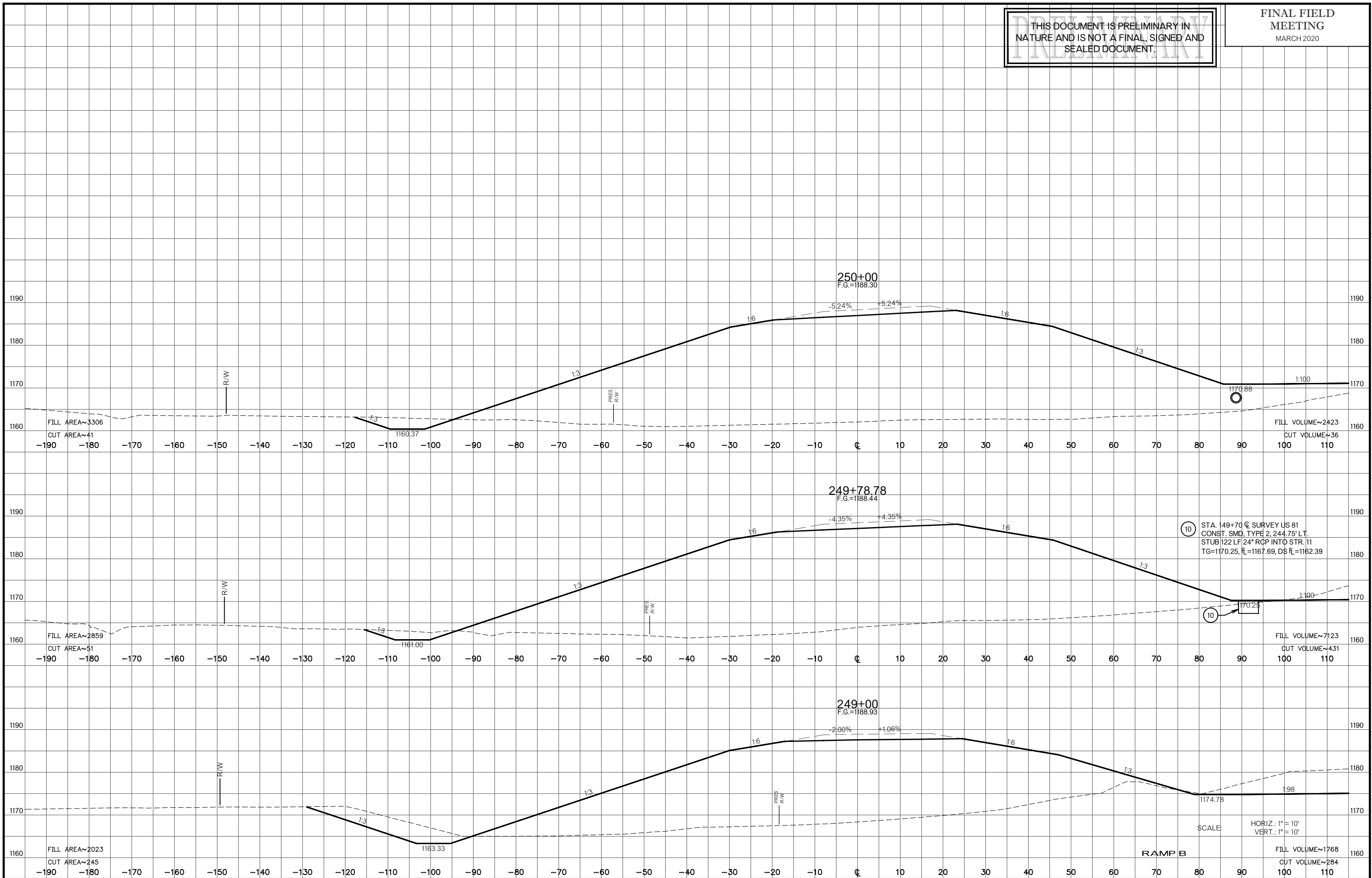
VERT.: 1" = 10'

RAMP B

State Job No. 24428(04) Sheet No. X229

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



10 STA. 149+70 Q SURVEY US 81
CONST. SMD, TYPE 2, 244.75' LT.
STUB I22 LF 24" RCP INTO STR. 11
TG=1170.25, FL=1167.69, DS FL=1162.39

10

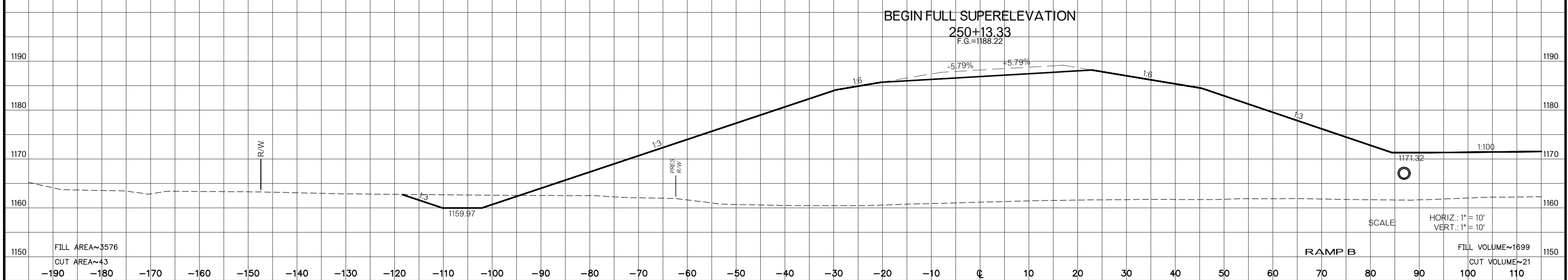
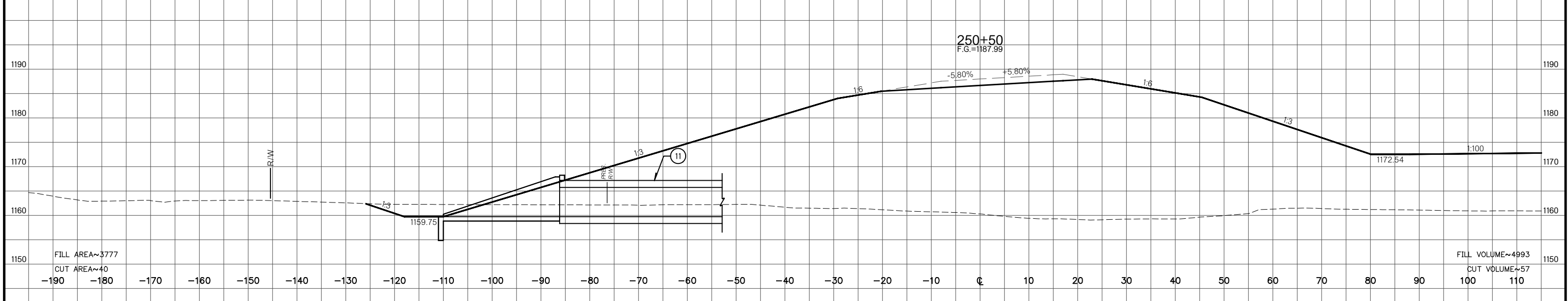
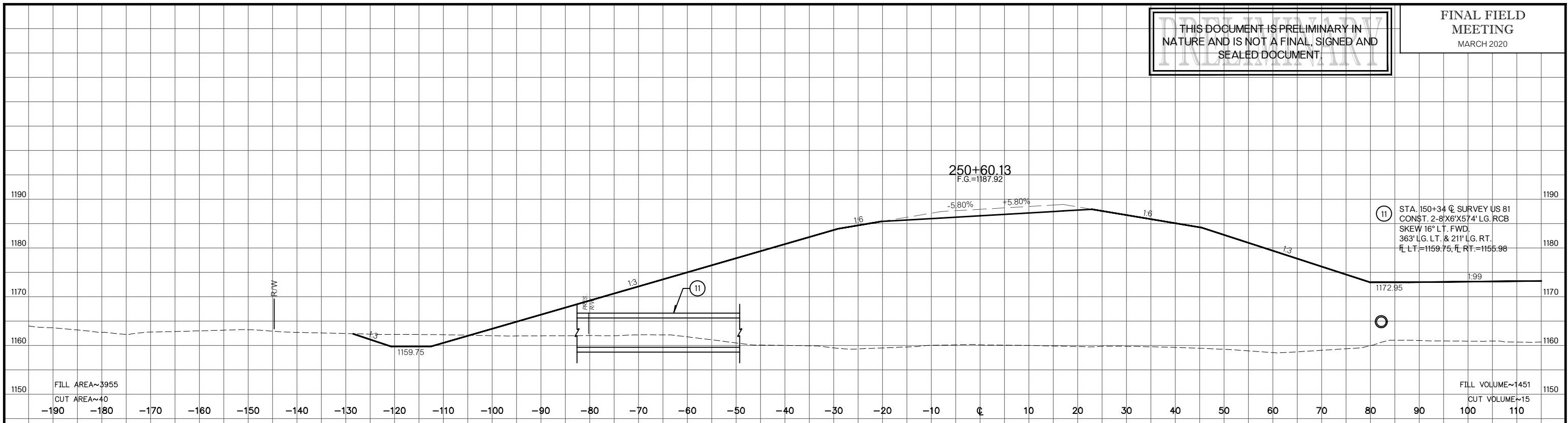
10

SCALE HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP B

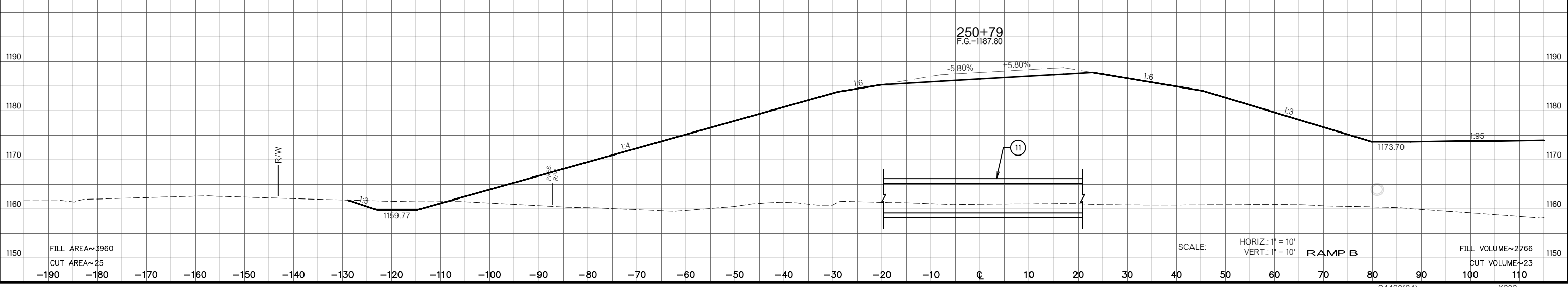
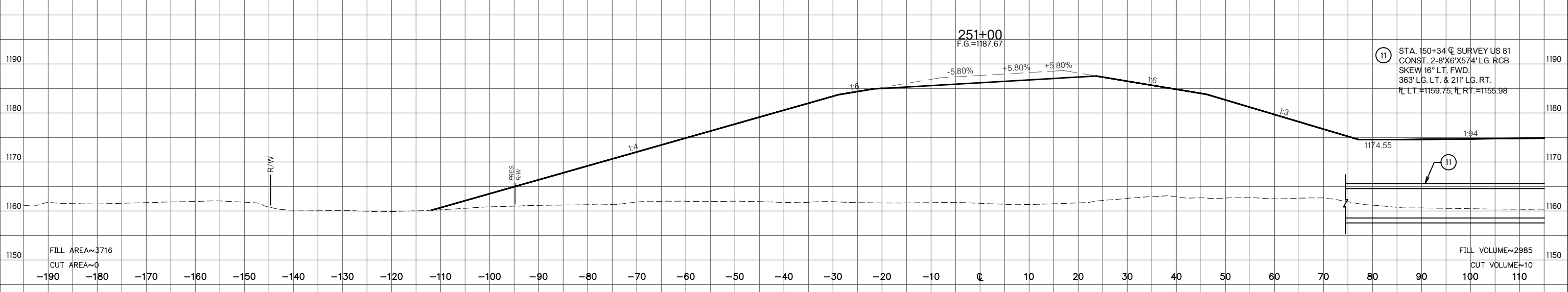
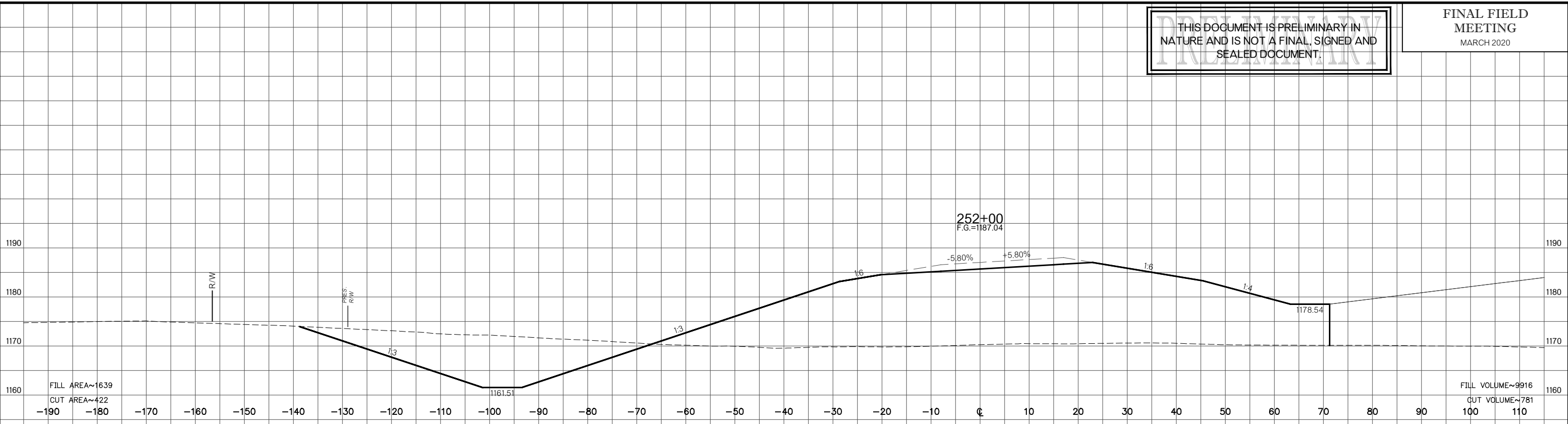
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

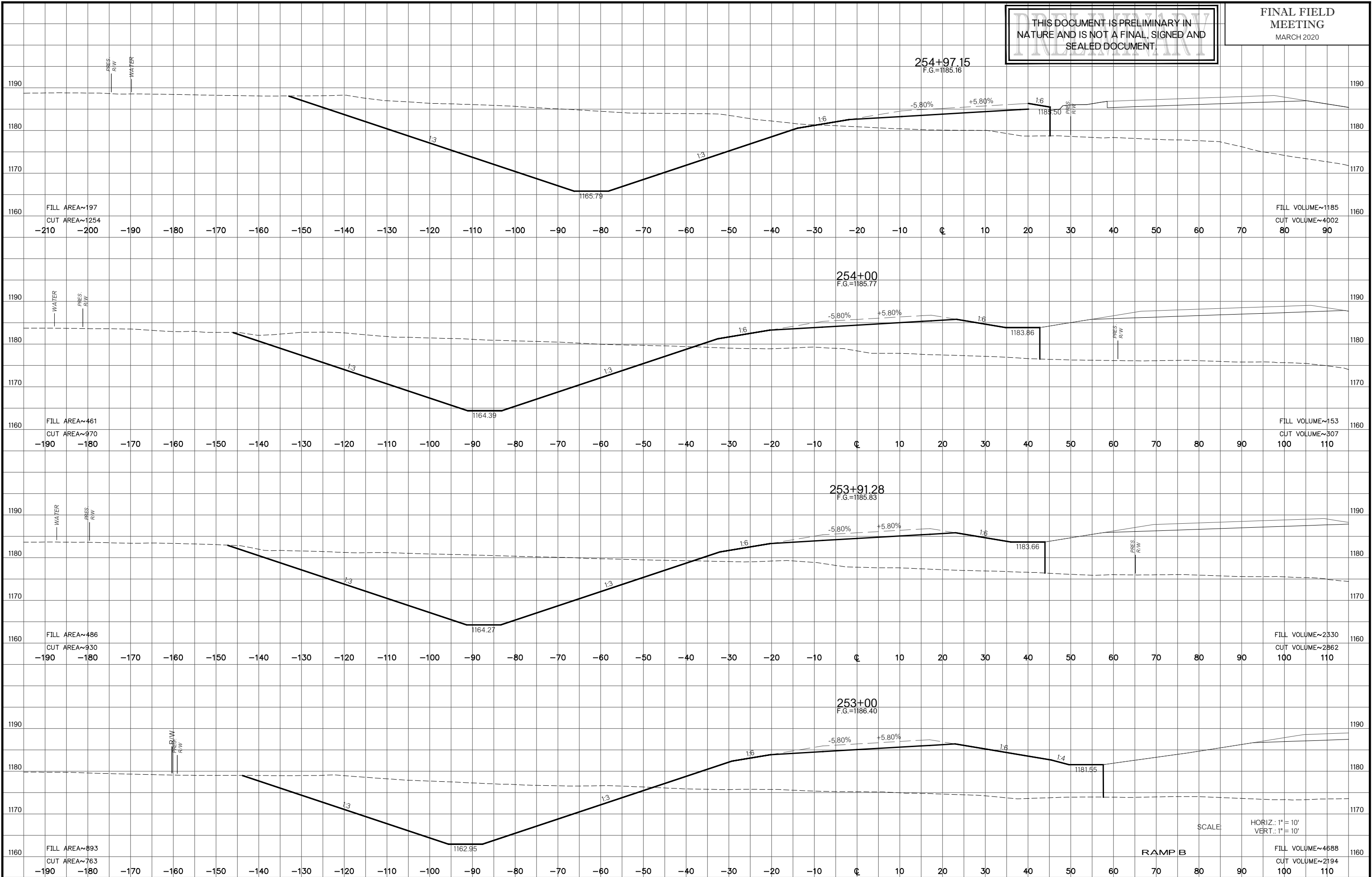


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



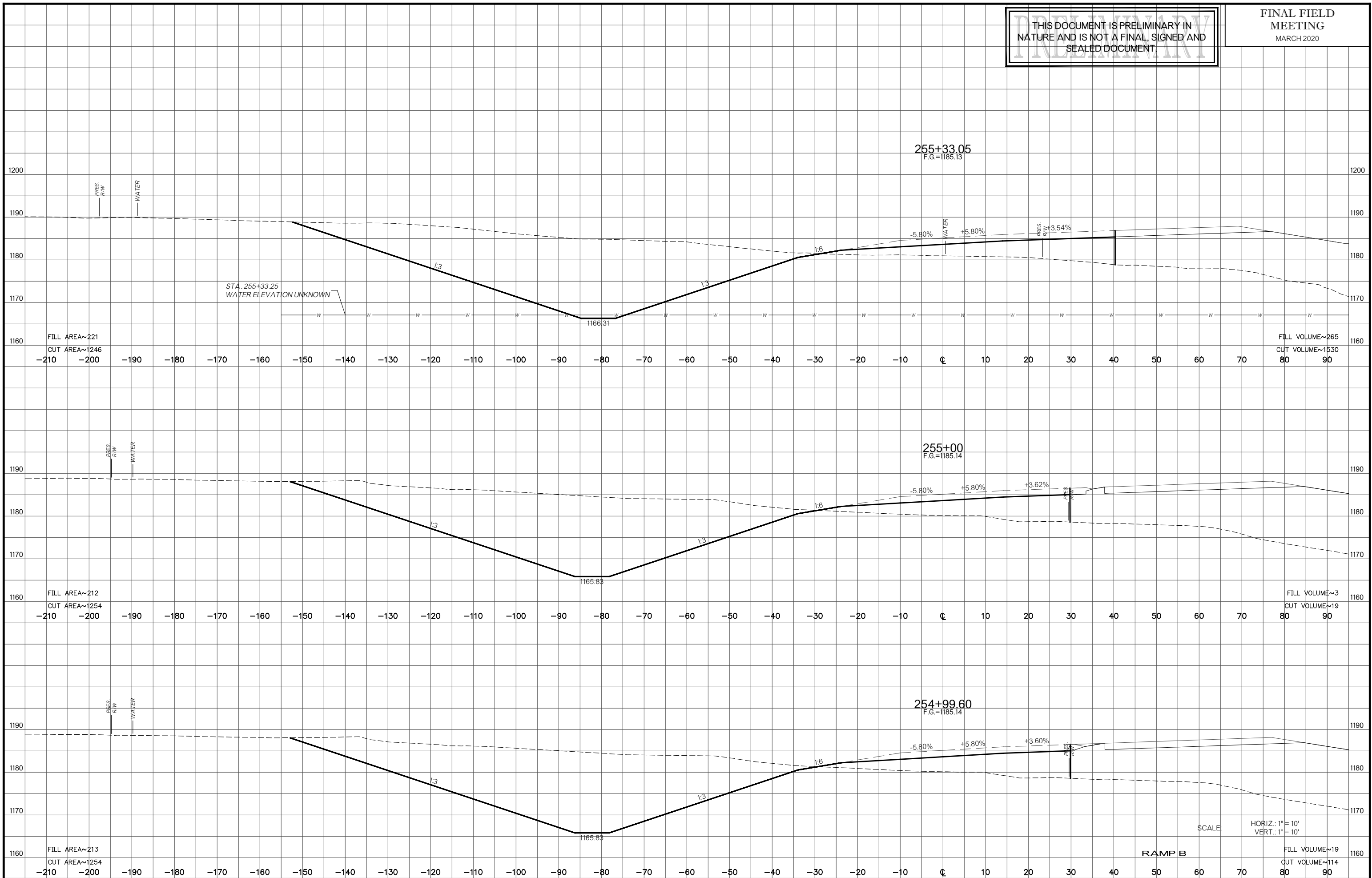
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP B
FILL VOLUME~4688
CUT VOLUME~2194

State Job No. 24428(04) Sheet No. X233

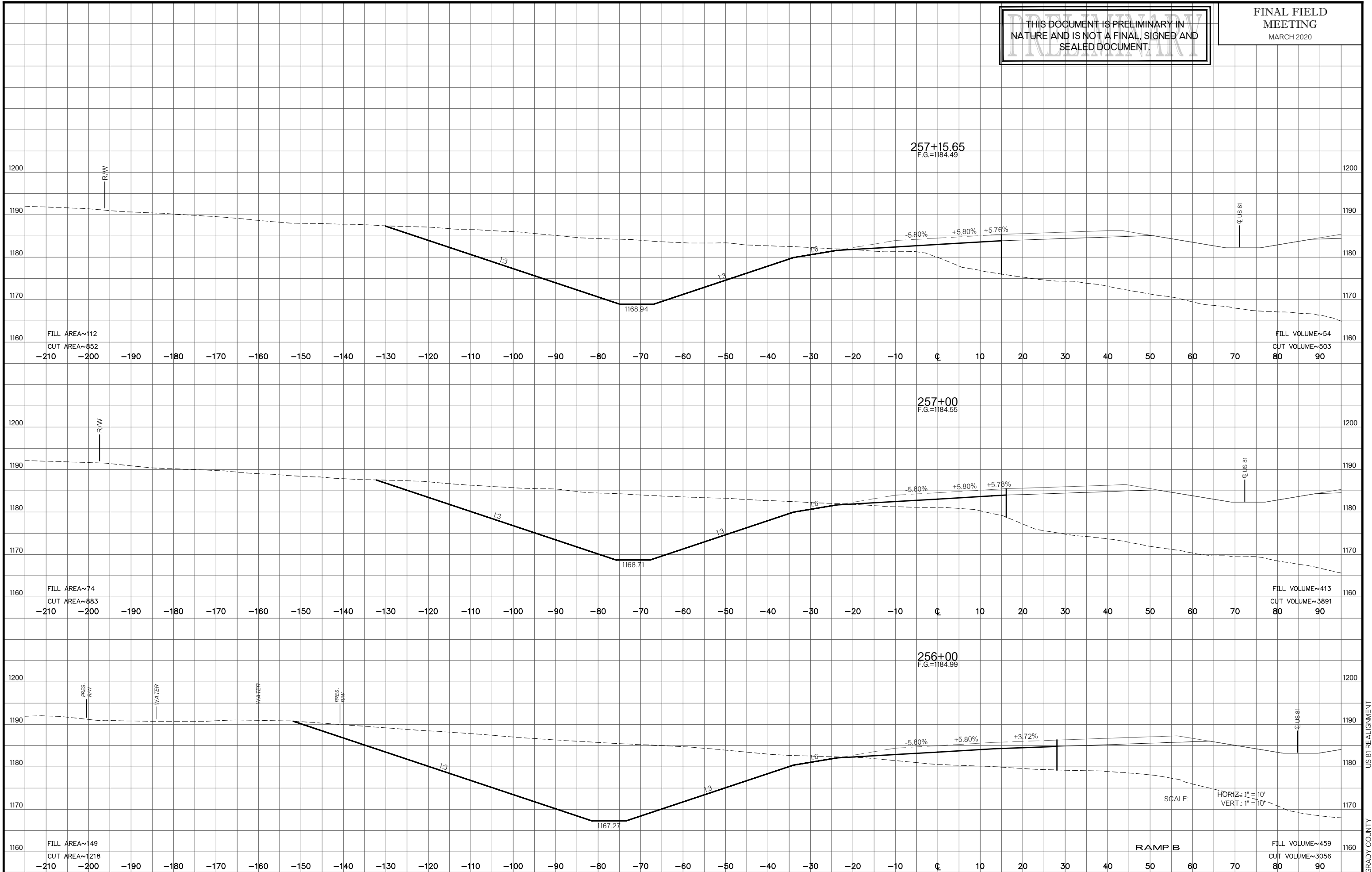
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



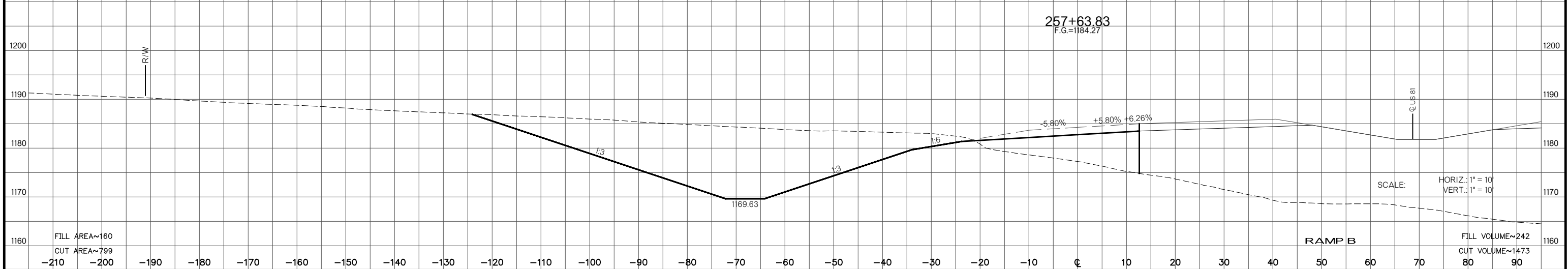
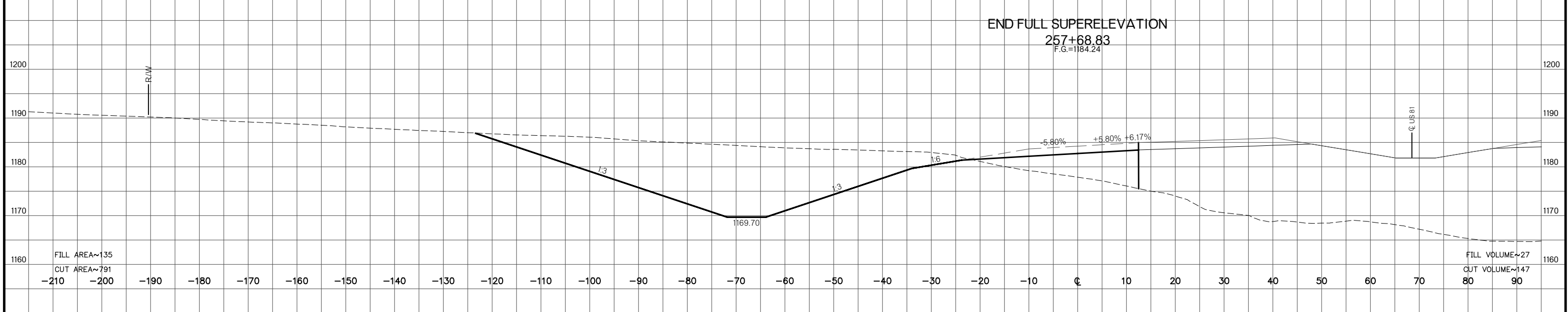
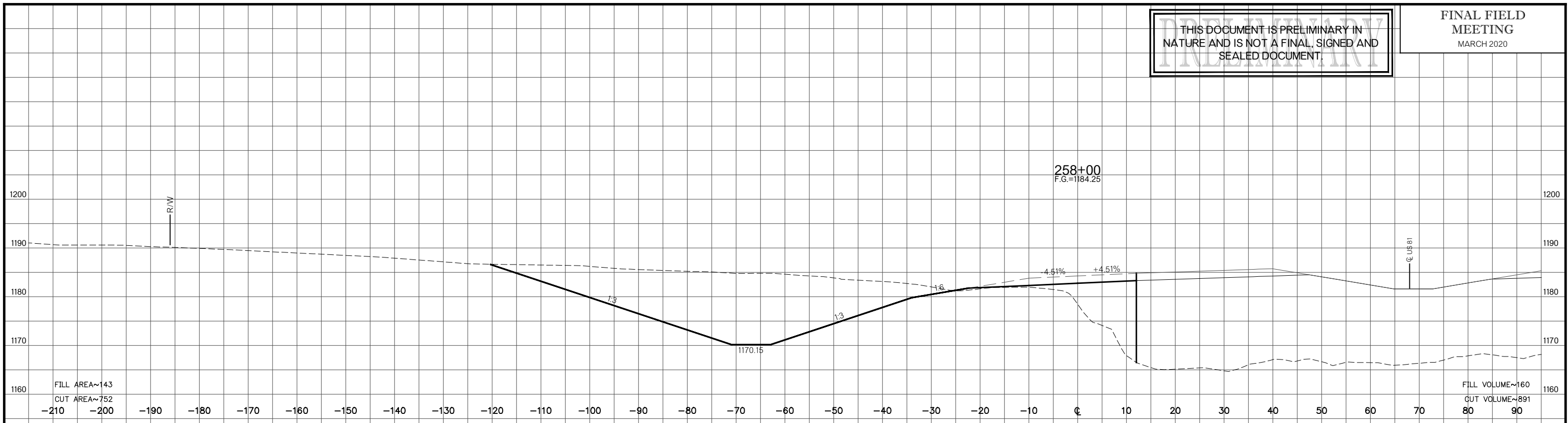
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



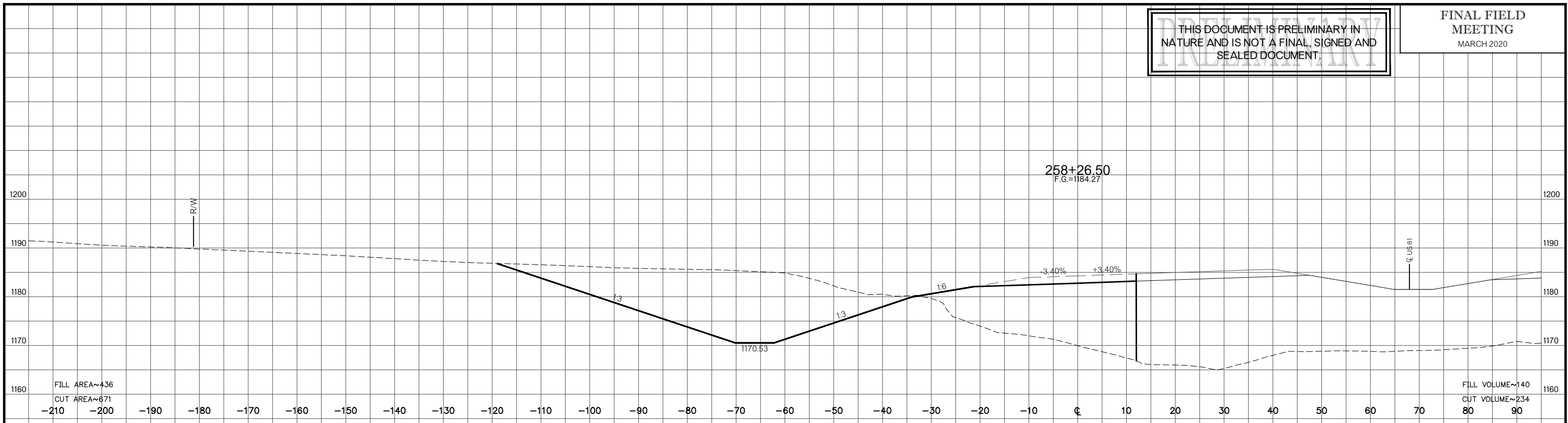
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



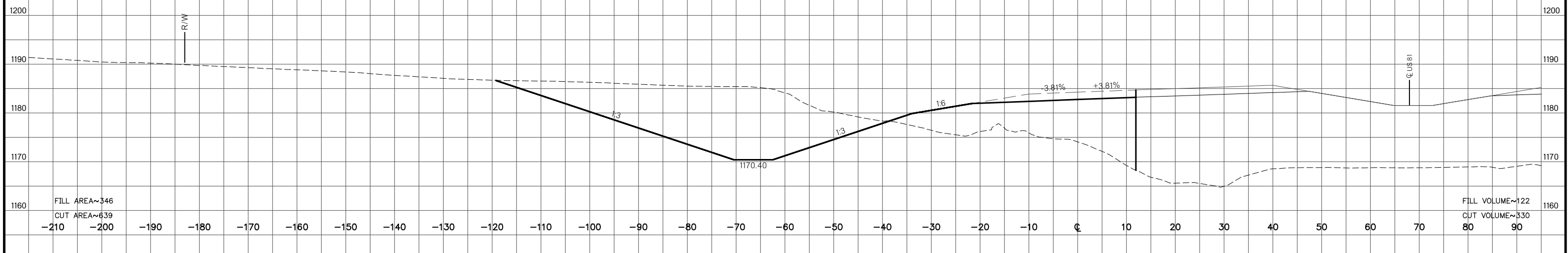
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

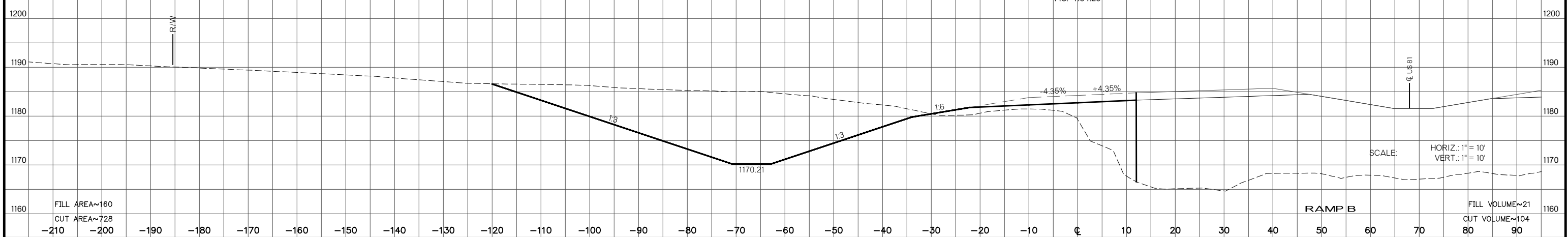


END LANE ROTATION
END SHOULDER ROTATION

258+16.83
F.G.=1184.26



258+03.78
F.G.=1184.25

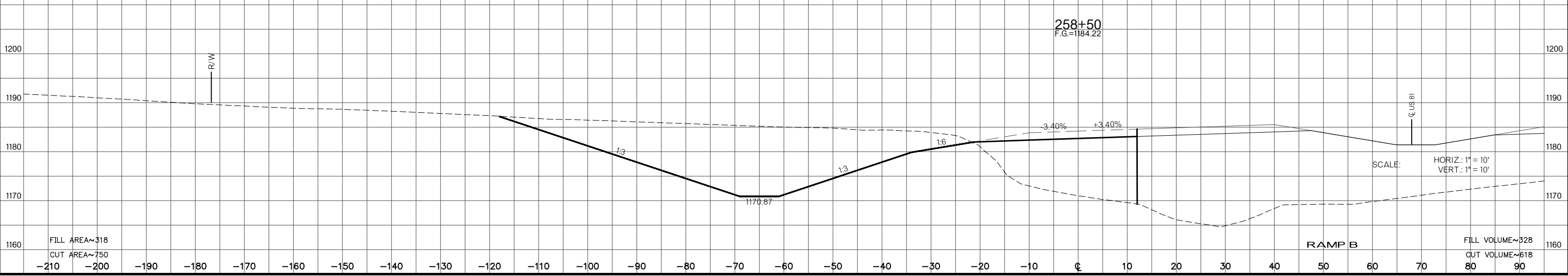
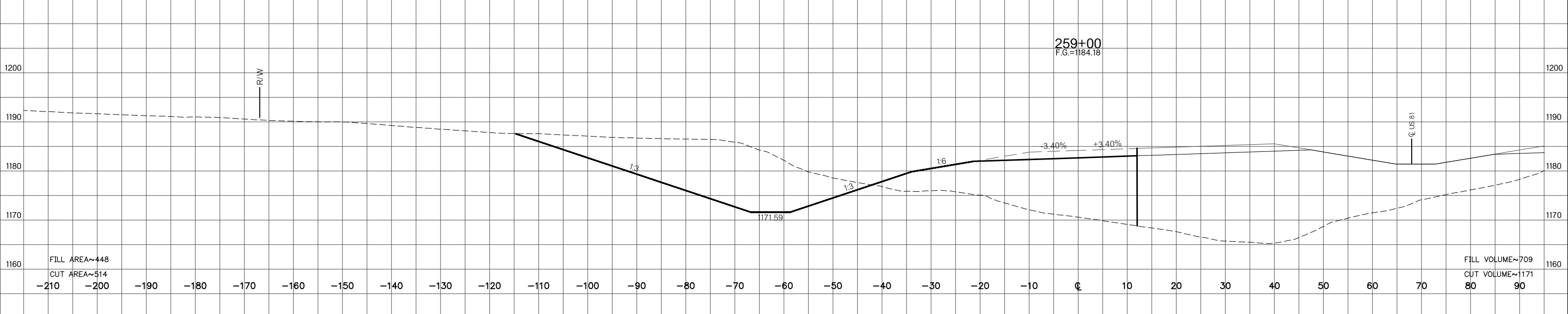
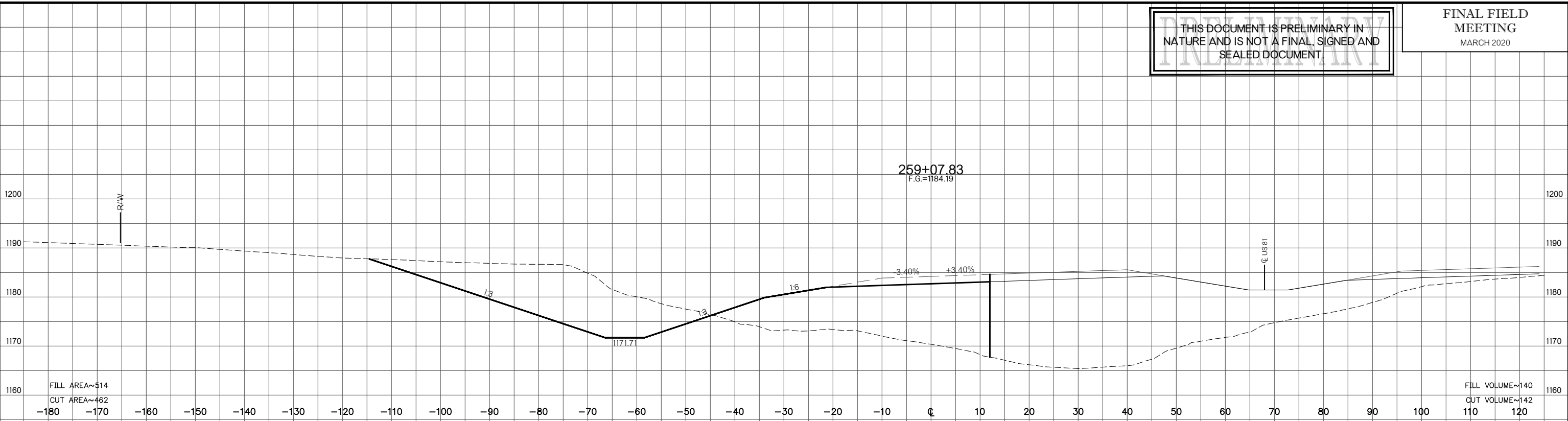


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

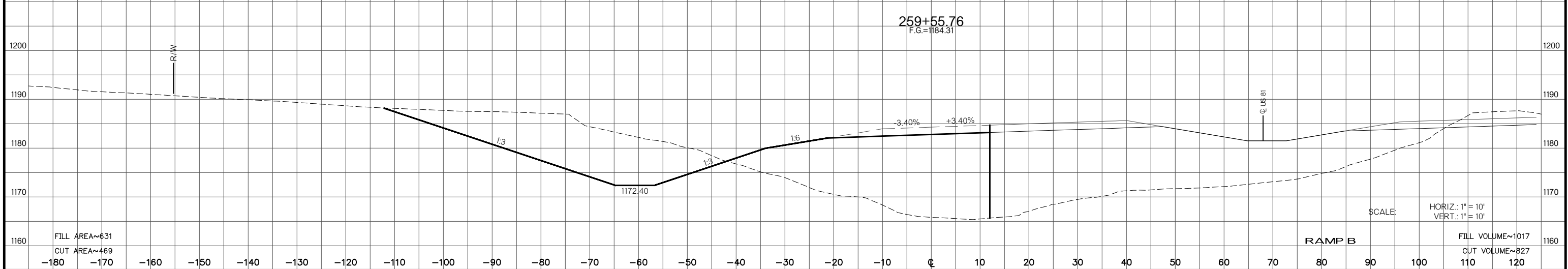
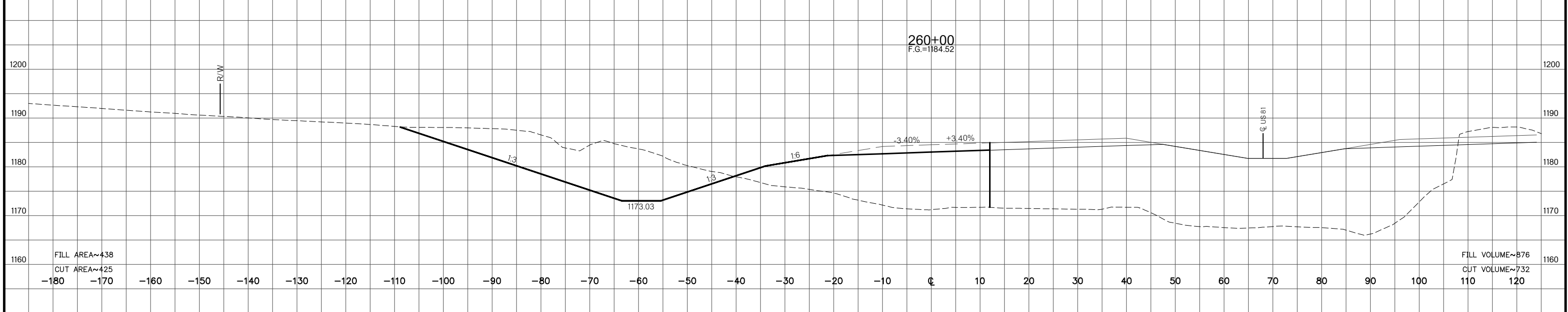
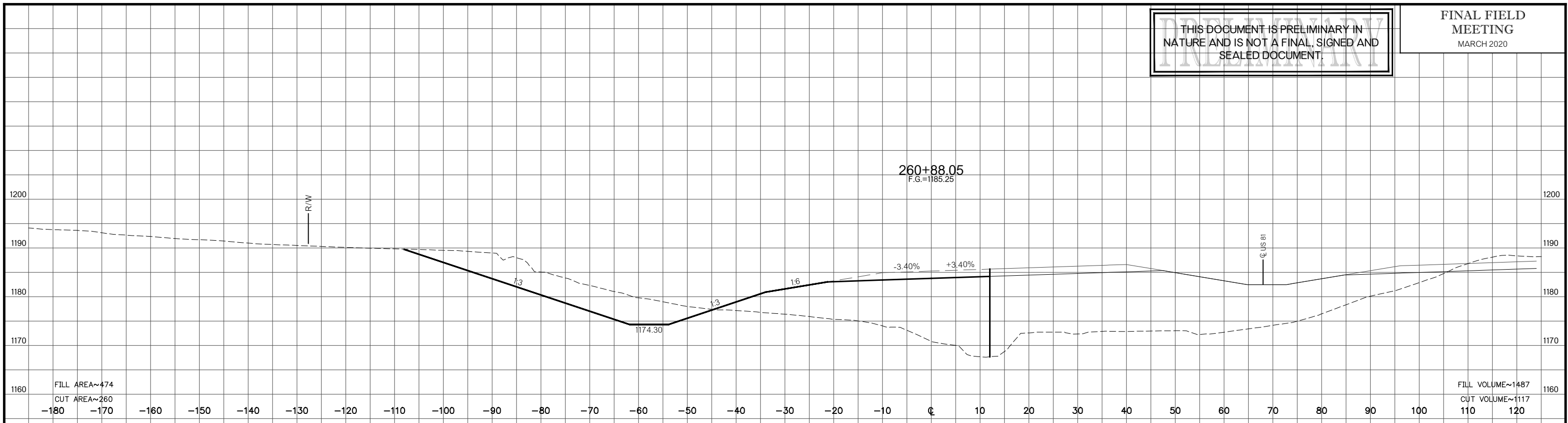
RAMP B

US 81 REALIGNMENT
GRADY COUNTY

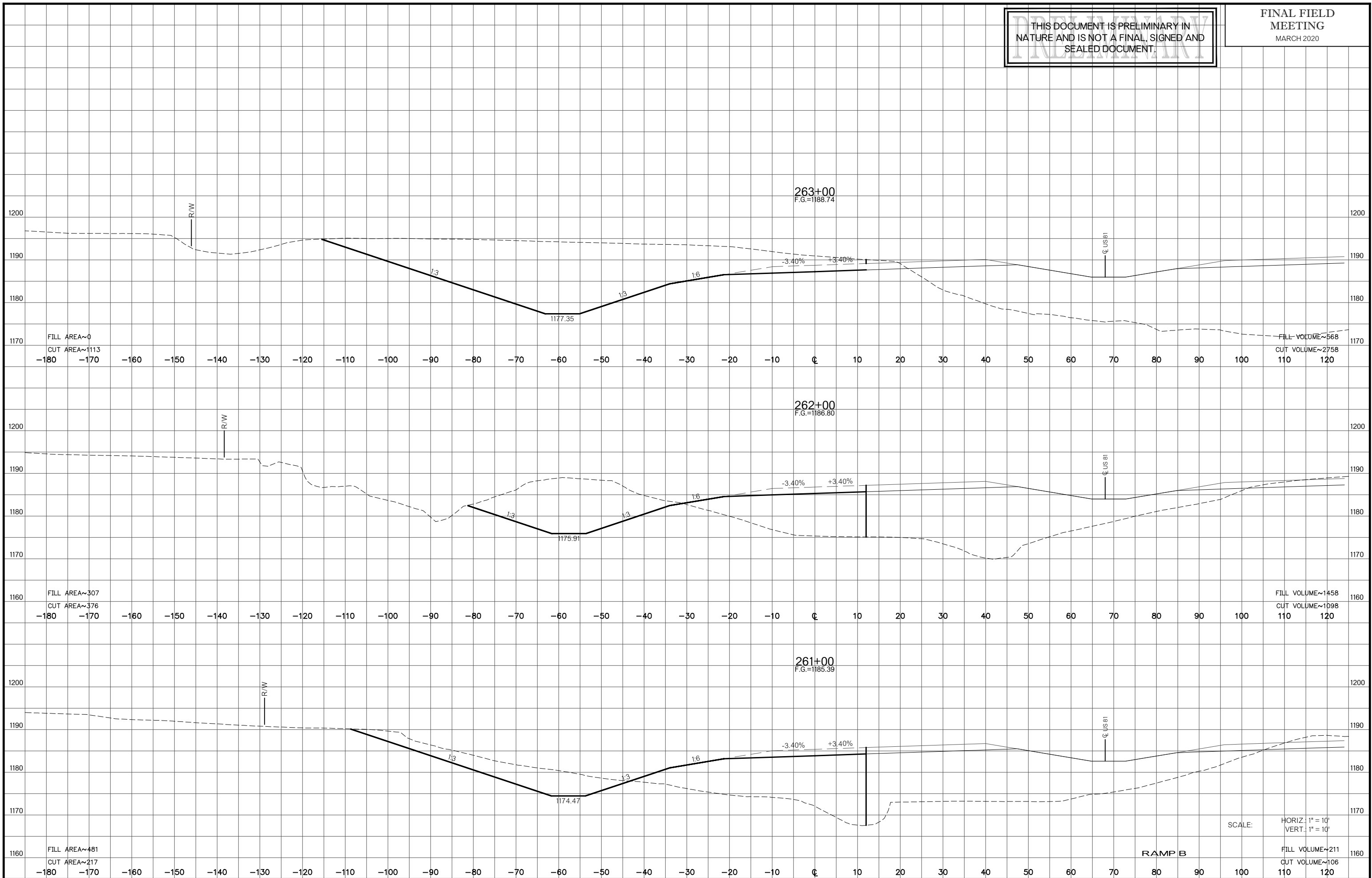
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



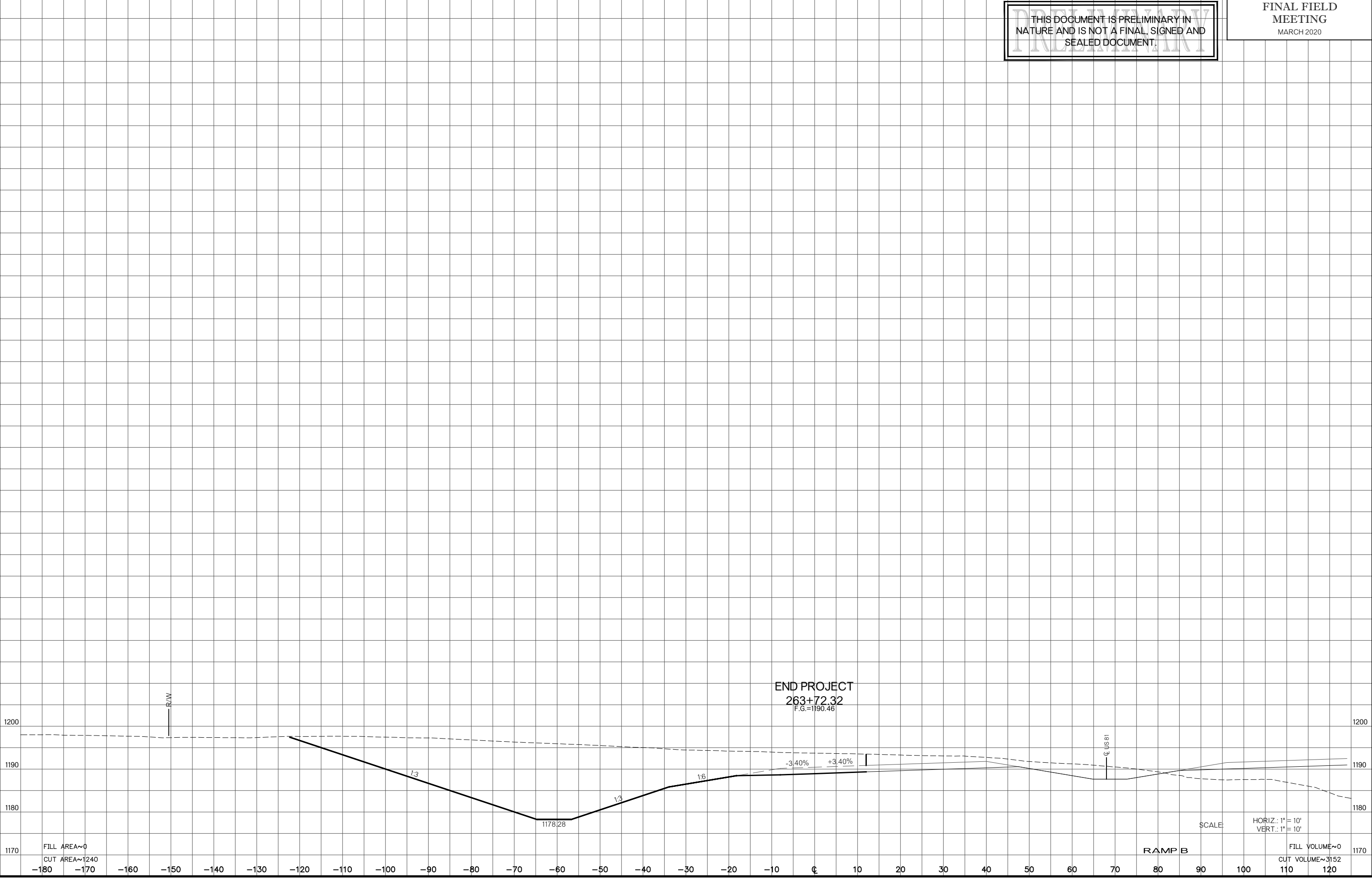
SCALE: HORIZ. 1" = 10'
VERT. 1" = 10'

RAMP B

US 81 REALIGNMENT
GRADY COUNTY

PRELIMINARY
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



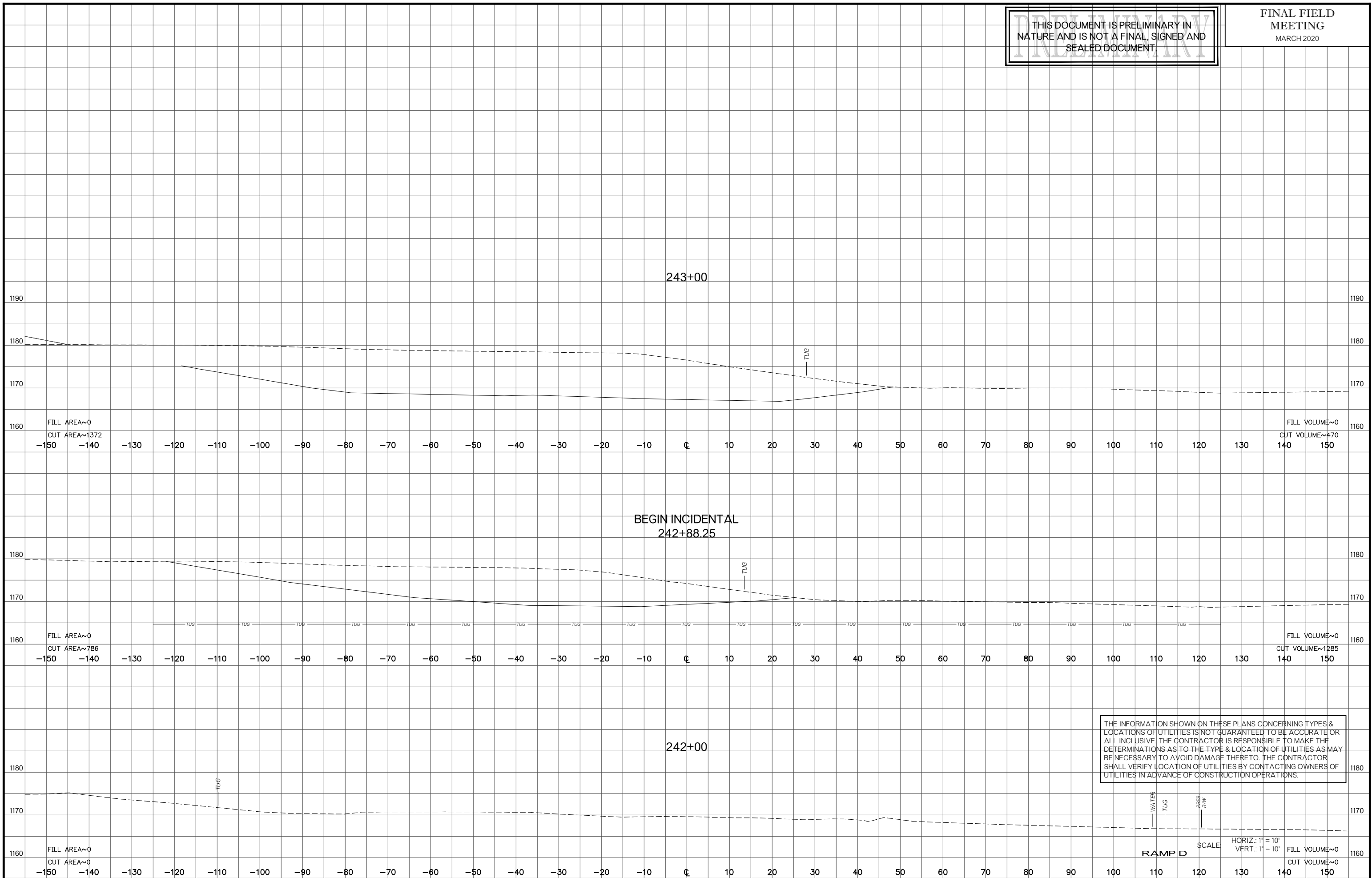
END PROJECT
263+72.32
F.G.=1190.46

1200
1190
1180
1170
FILL AREA~0
CUT AREA~1240
-180 -170 -160 -150 -140 -130 -120 -110 -100 -90 -80 -70 -60 -50 -40 -30 -20 -10 C 10 20 30 40 50 60 70 80 90 100 110 120

SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'
RAMP B
FILL VOLUME~0
CUT VOLUME~3152
110 120

GRADY COUNTY
US 81 REALIGNMENT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

WATER TUG
RAMP D
SCALE

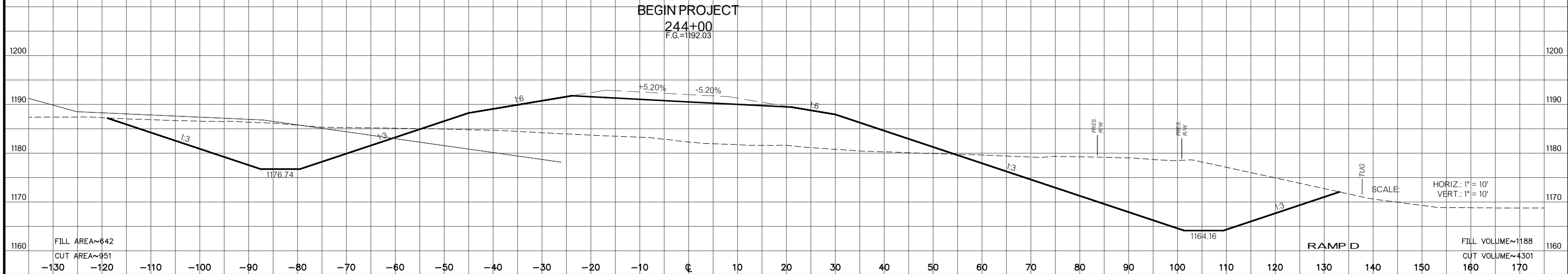
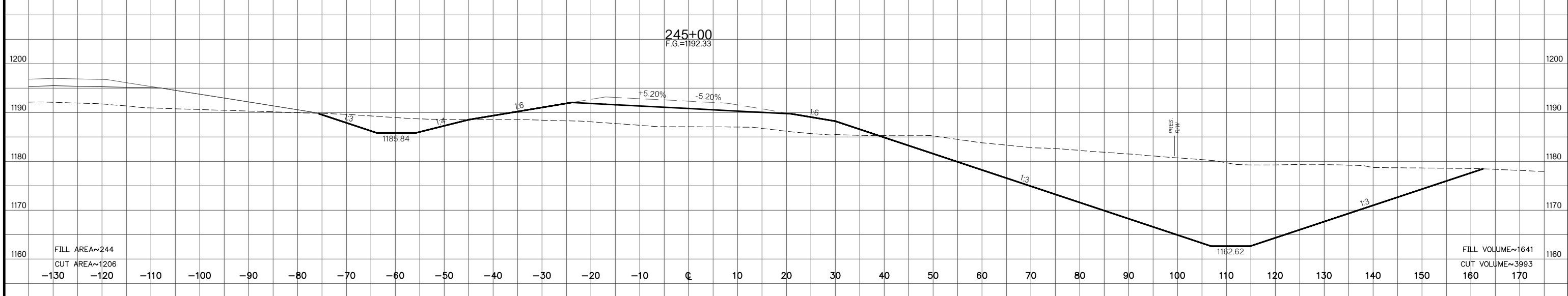
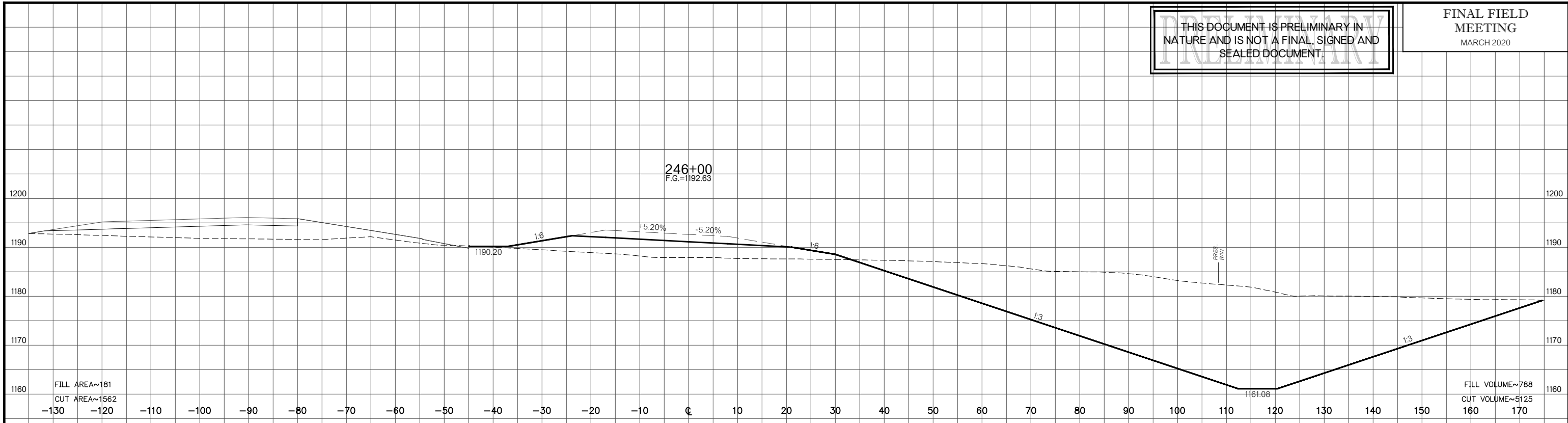
HORIZ.: 1" = 10'
VERT.: 1" = 10'

FILL VOLUME~0
CUT VOLUME~0

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

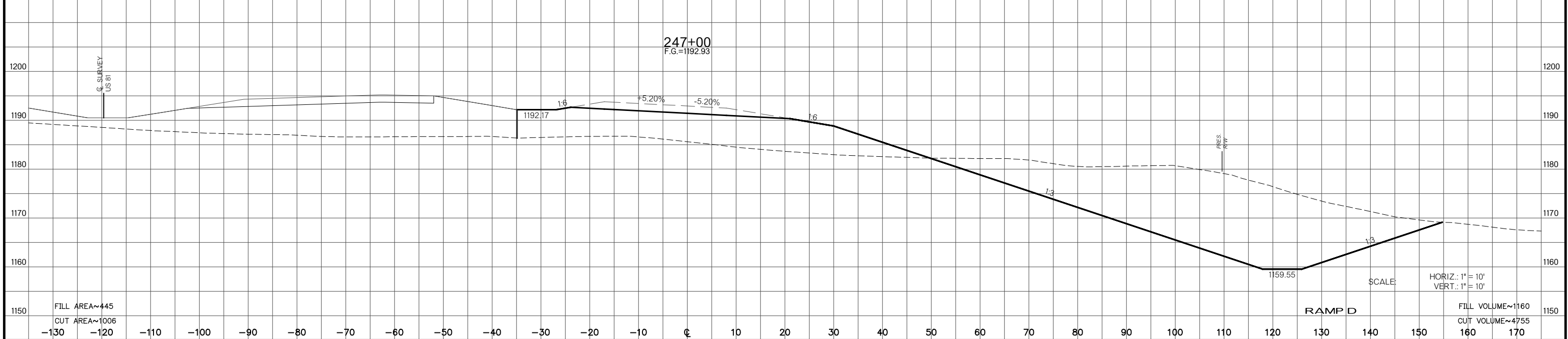
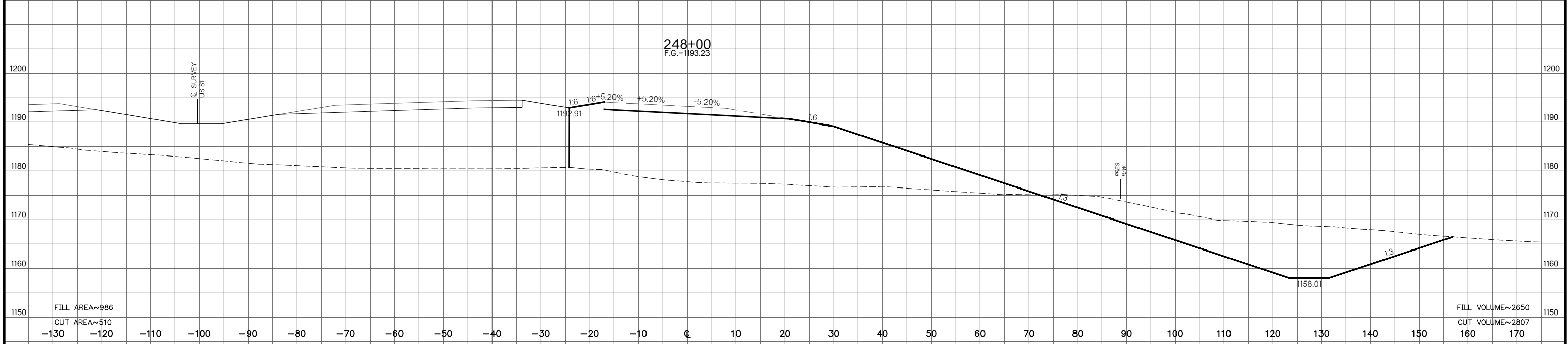
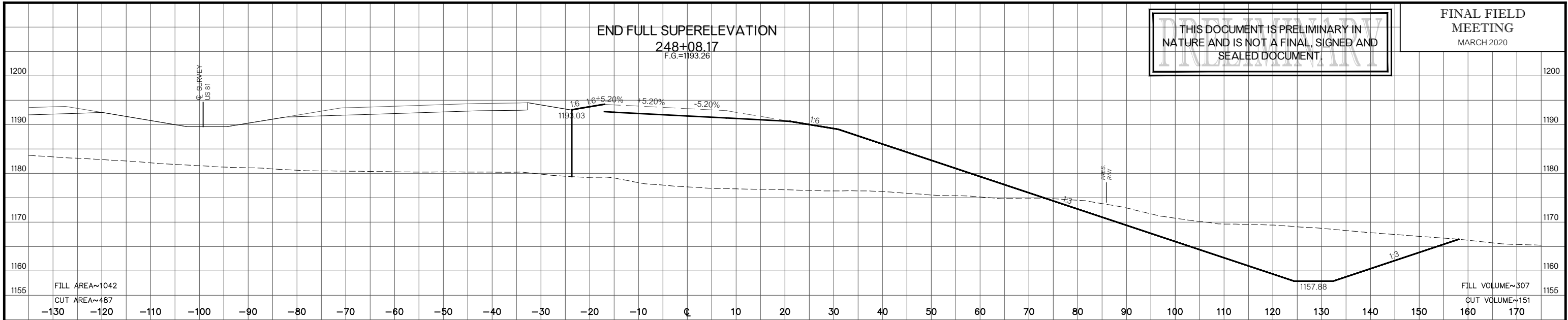


GRADY COUNTY

END FULL SUPERELEVATION
248+08.17
F.G.=1193.26

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



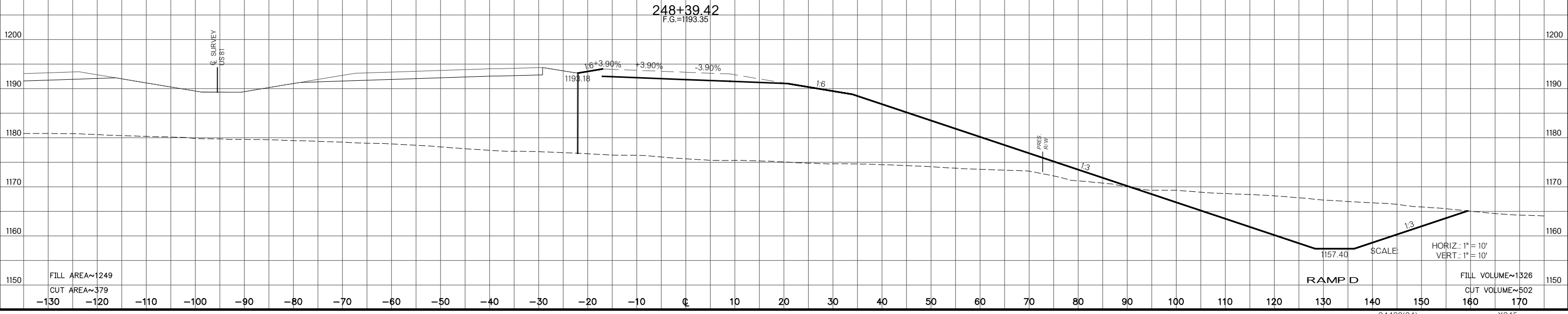
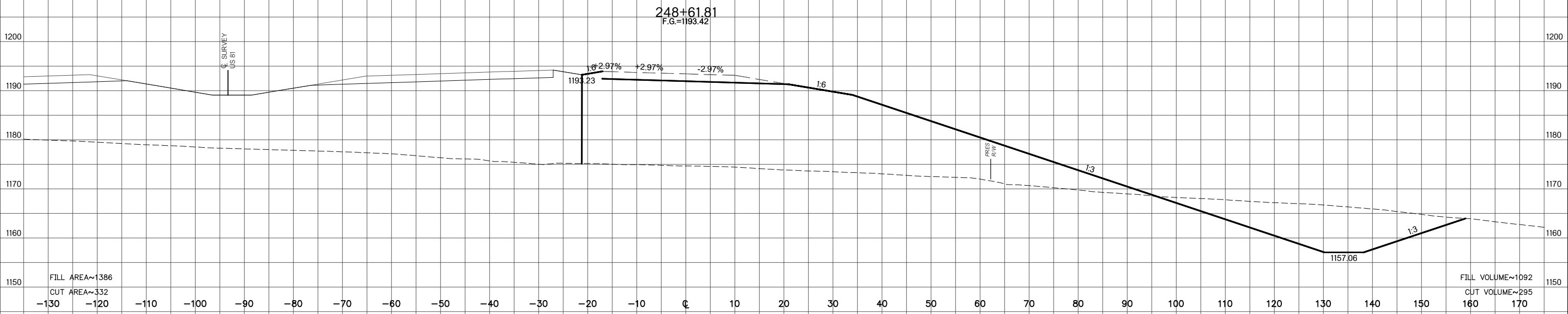
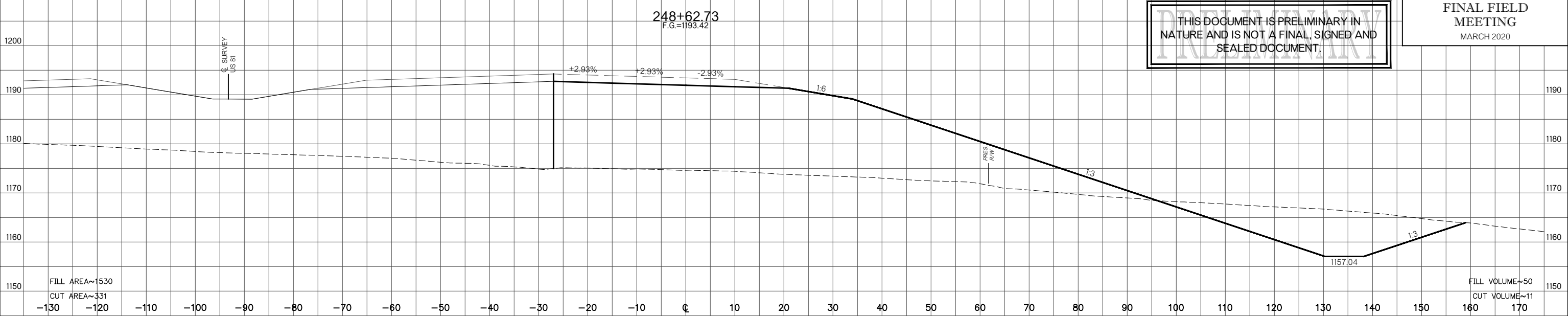
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP D
FILL VOLUME~1160
CUT VOLUME~4755

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

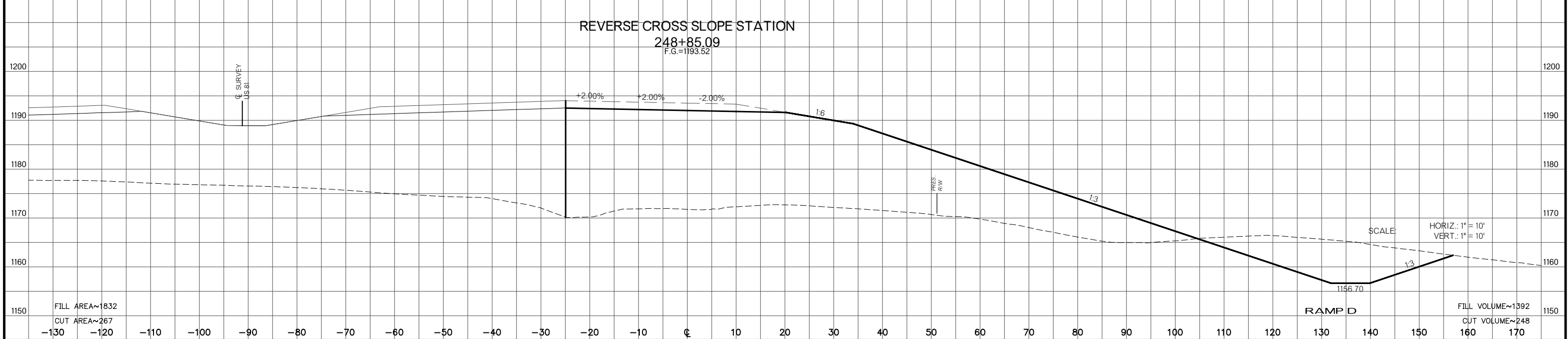
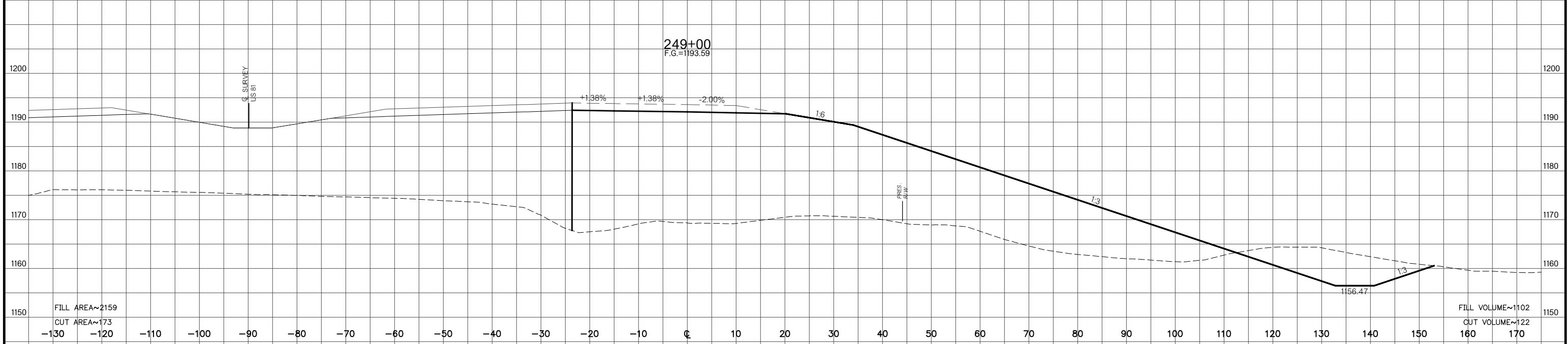
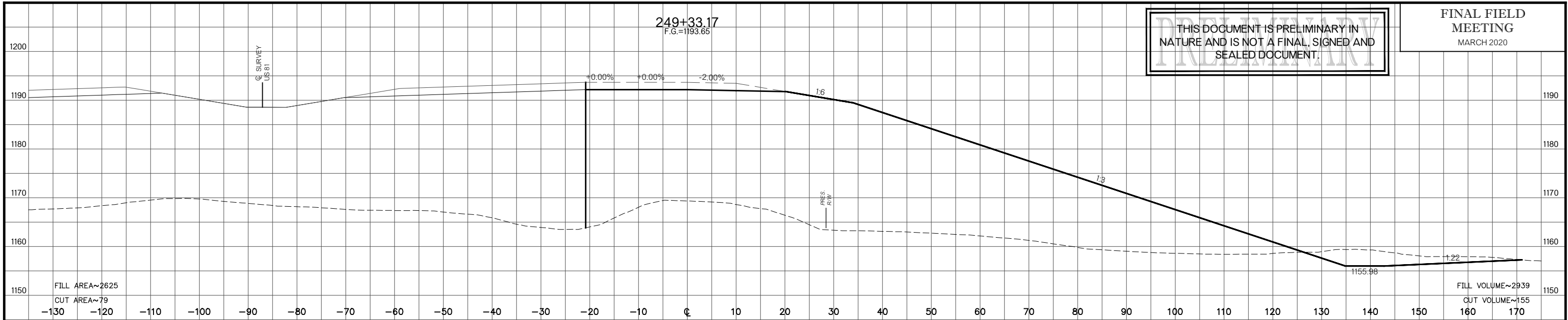
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

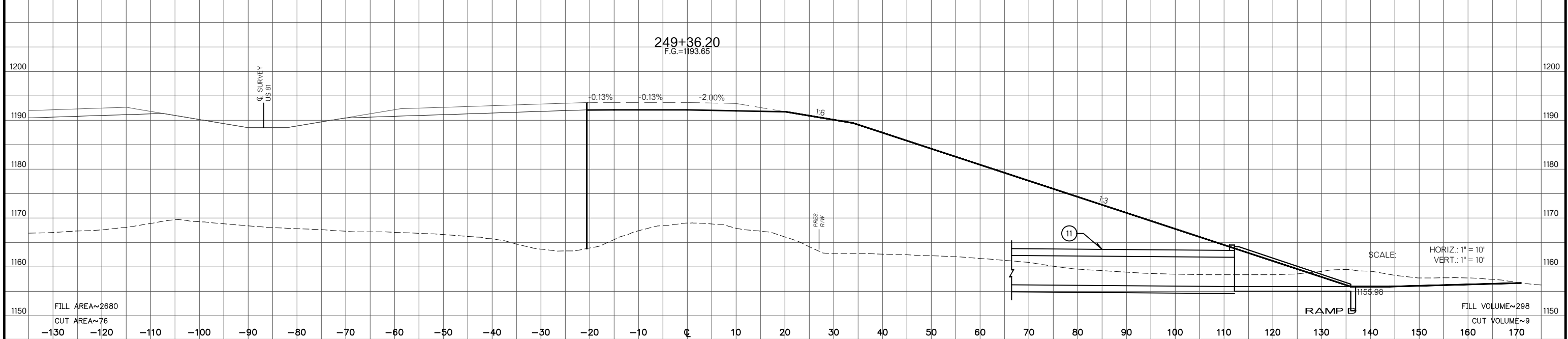
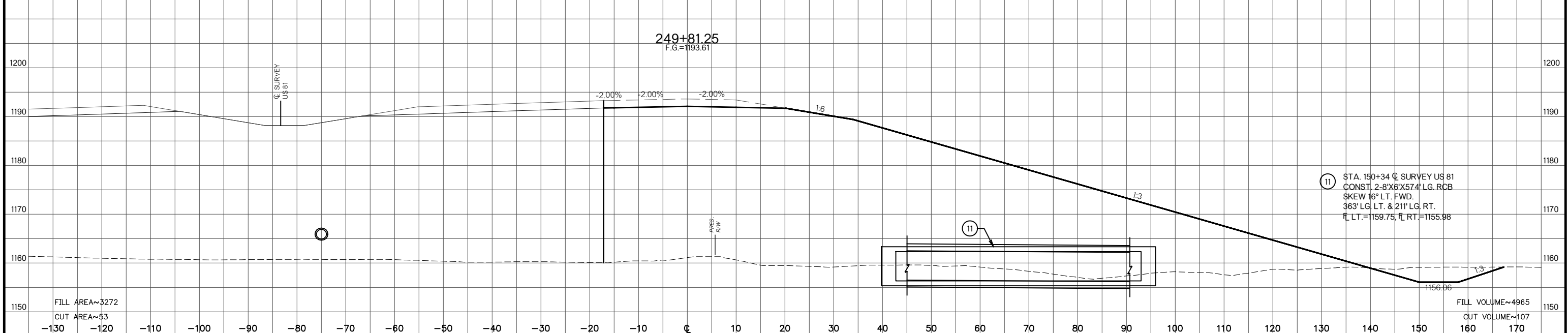
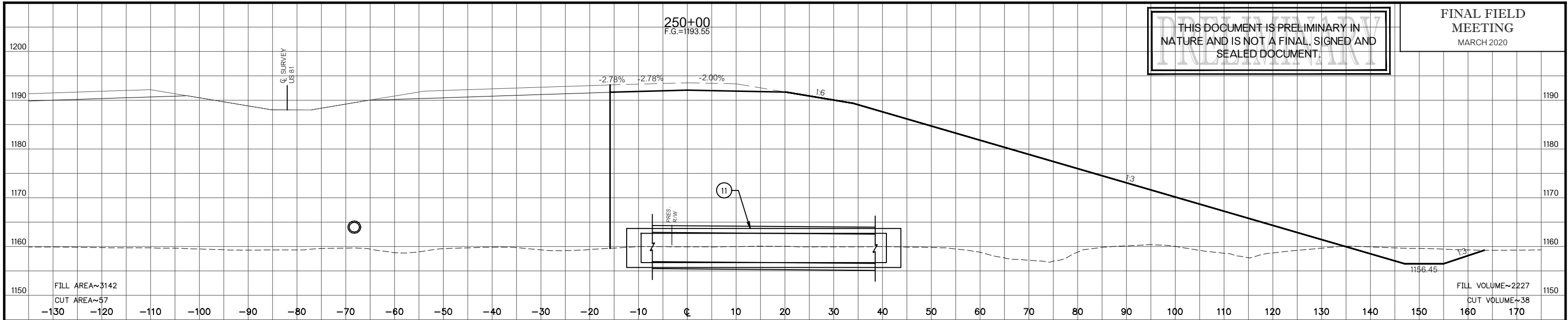
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

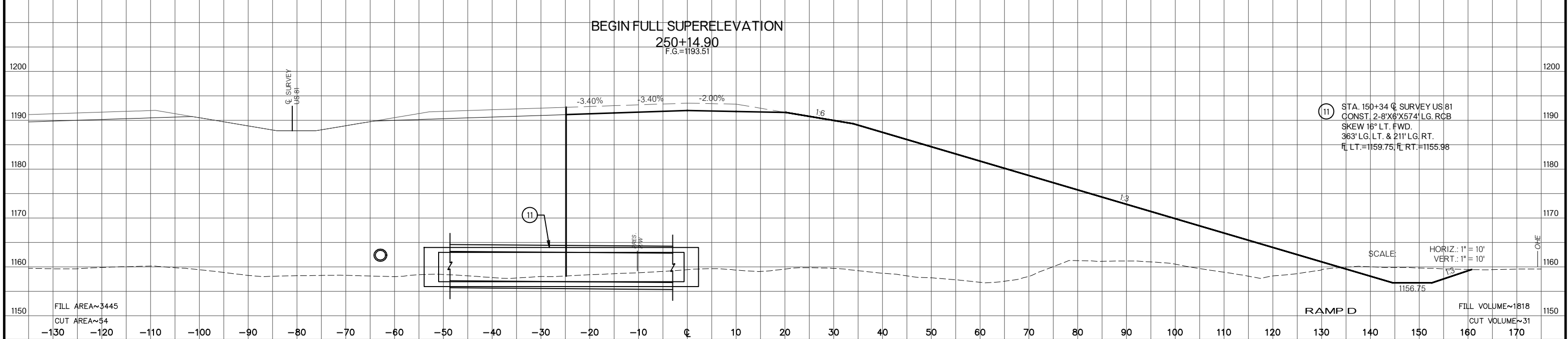
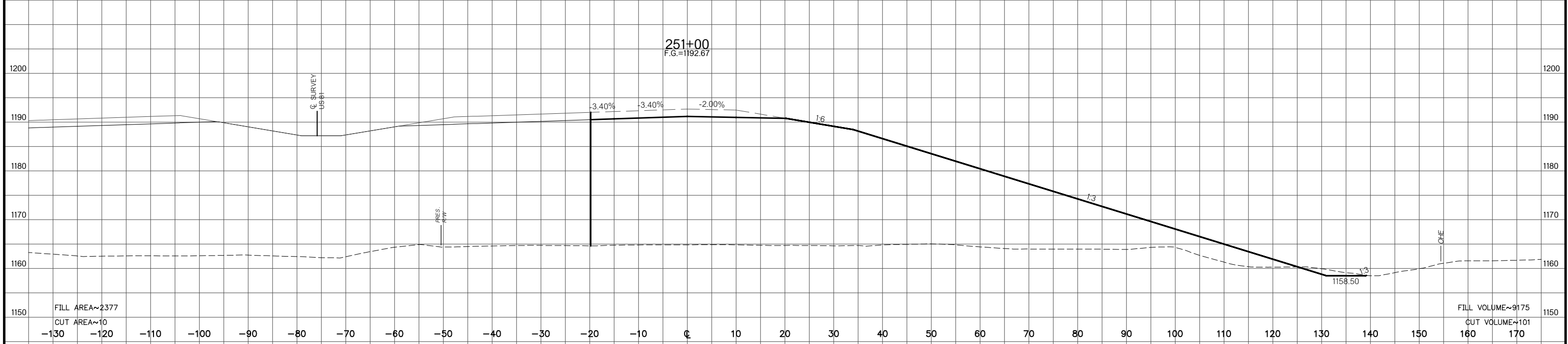
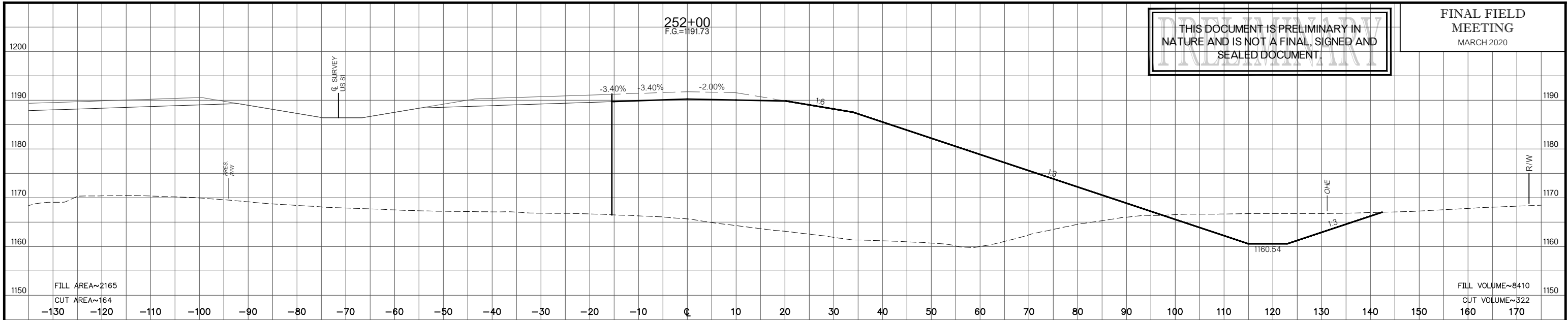
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

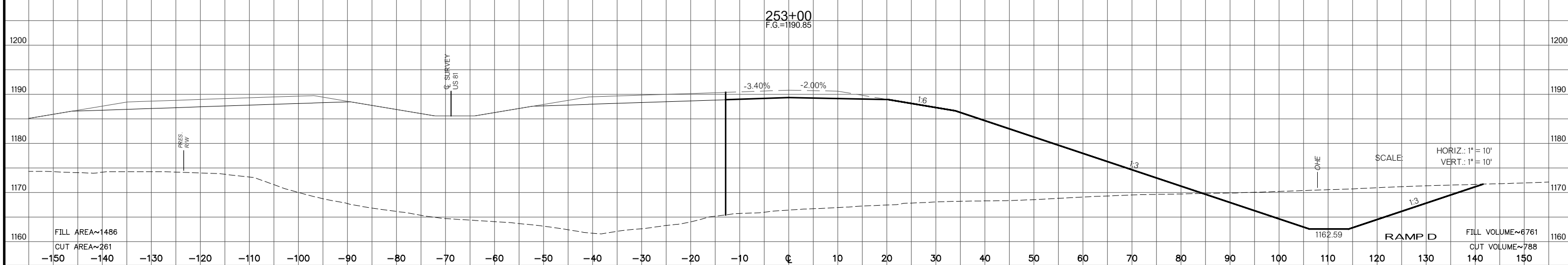
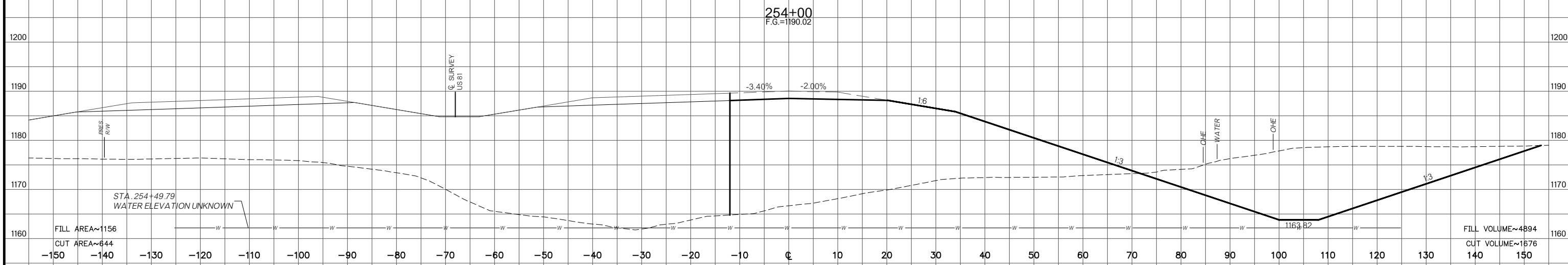
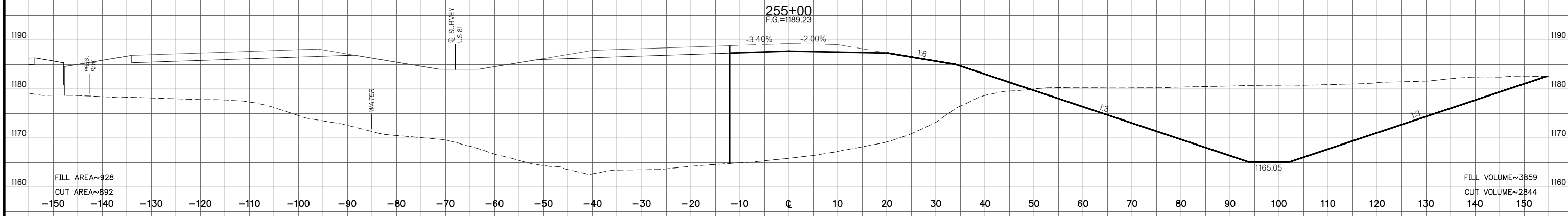
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

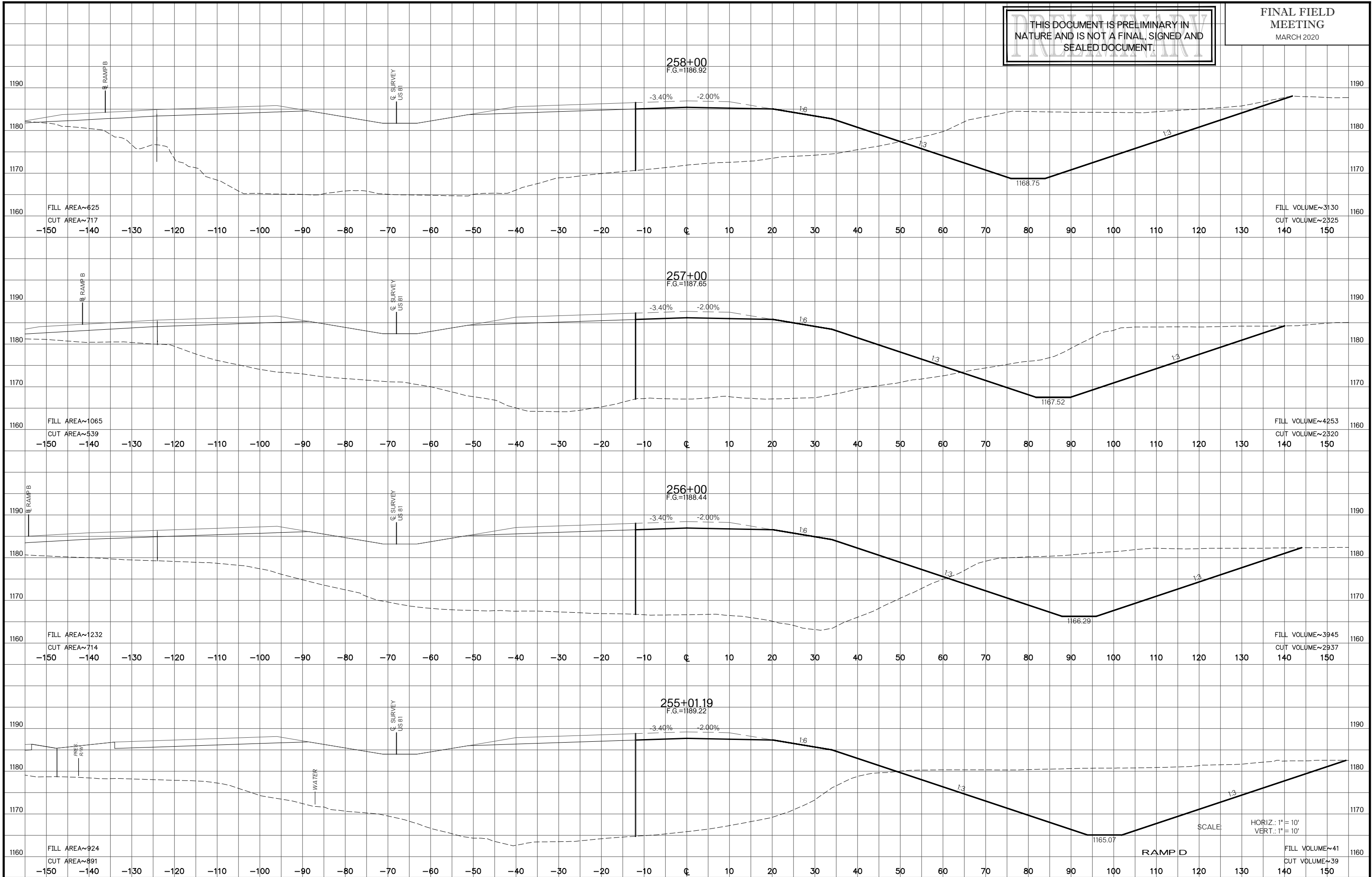
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

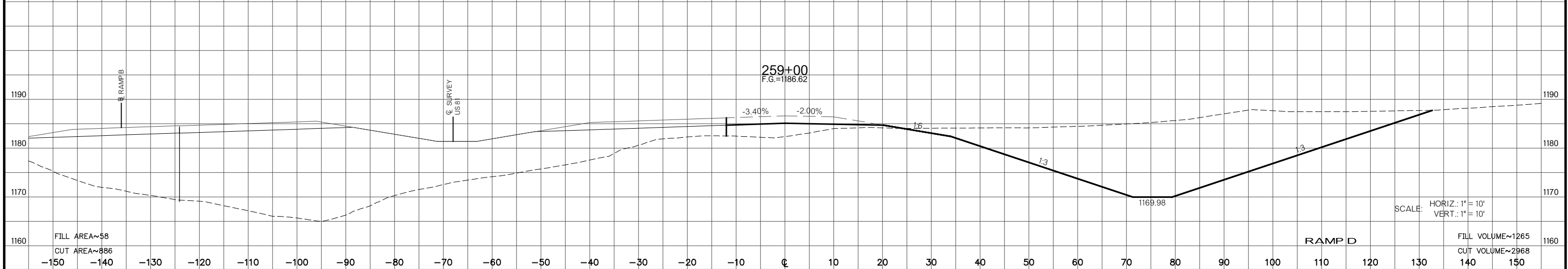
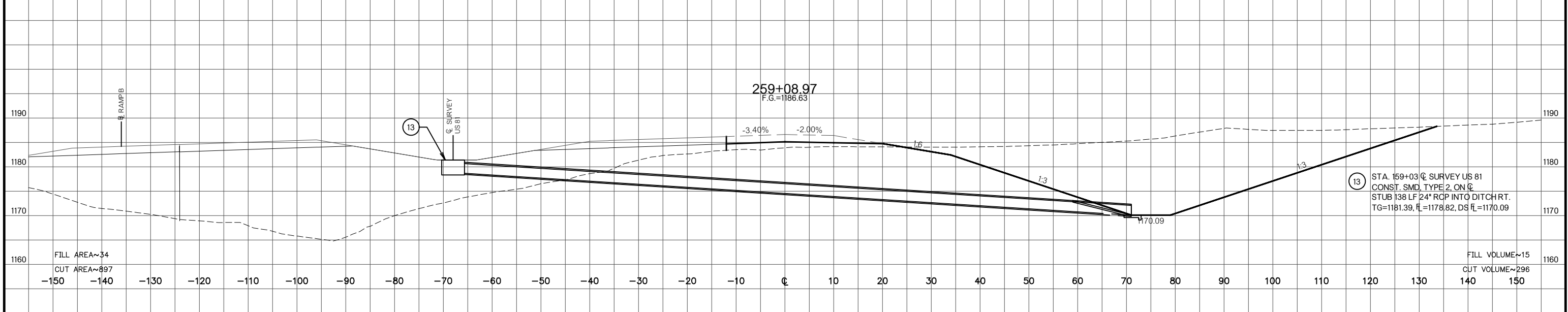
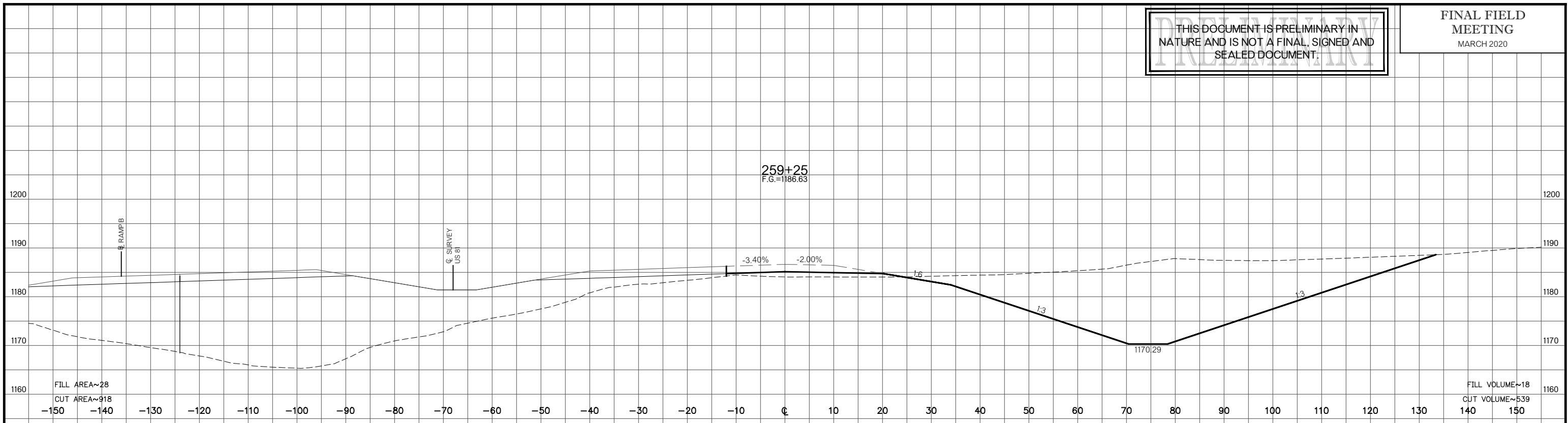


SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP D

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

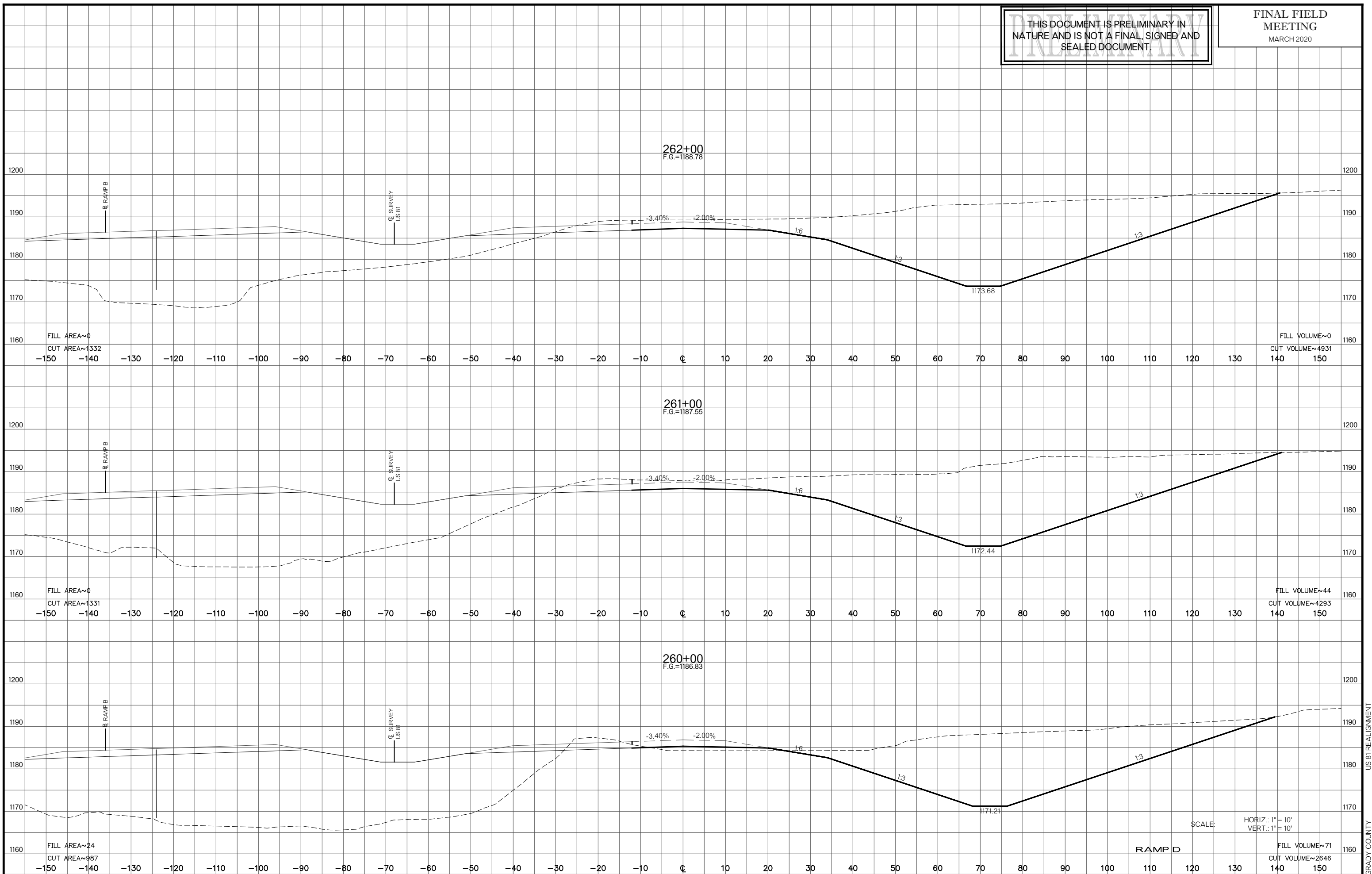


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



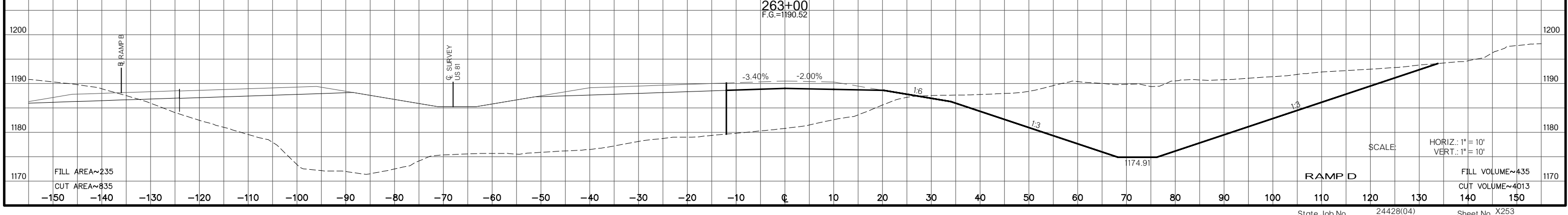
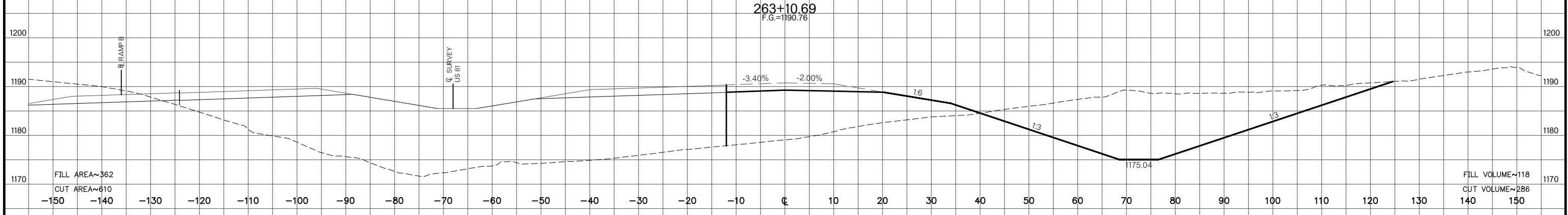
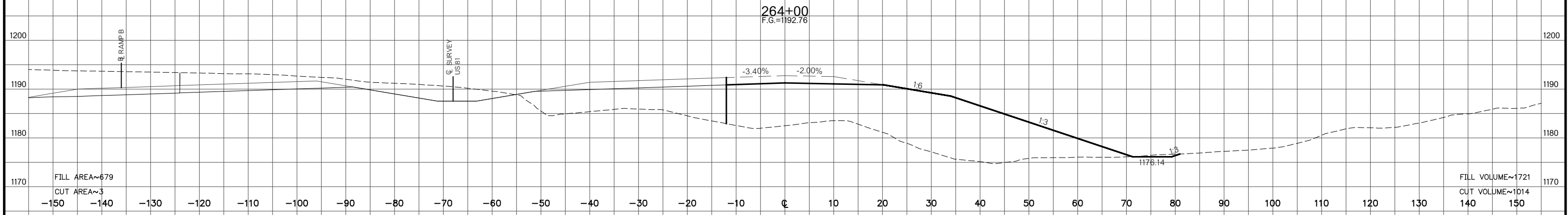
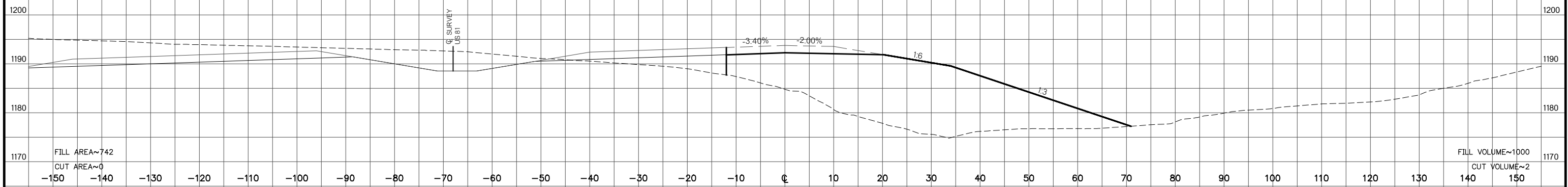
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'
RAMP D
FILL VOLUME~71
CUT VOLUME~2646
State Job No. 24428(04) Sheet No. X252

US 81 REALIGNMENT
GRADY COUNTY

END PROJECT
END FULL SUPERELEVATION
264+38
F.G.=1193.75

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

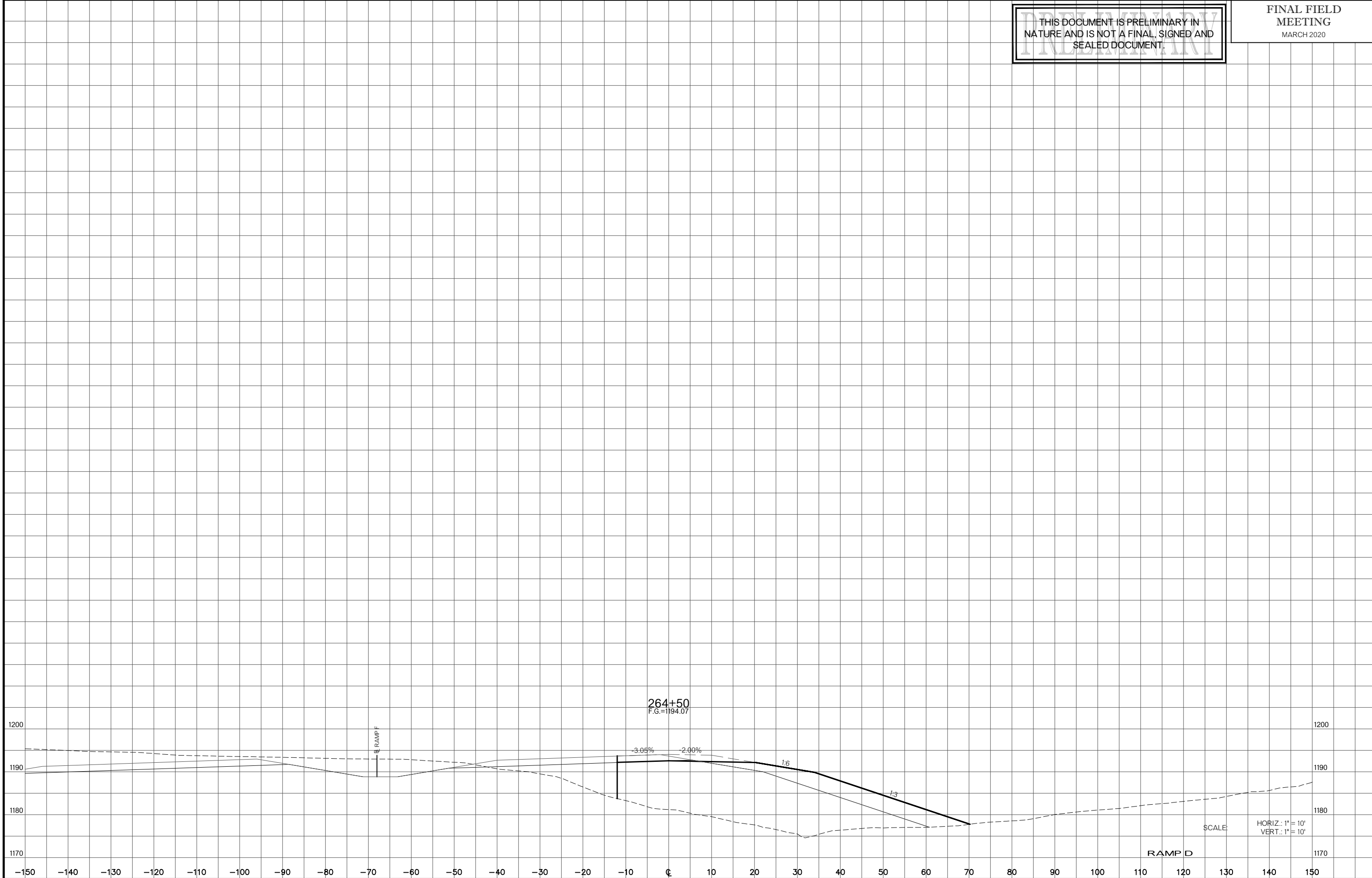
RAMP D

State Job No. 24428(04) Sheet No. X253

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



264+50
F.G.=1194.07

-3.05% 2.00%

1:6

1:3

RAMP

RAMP D

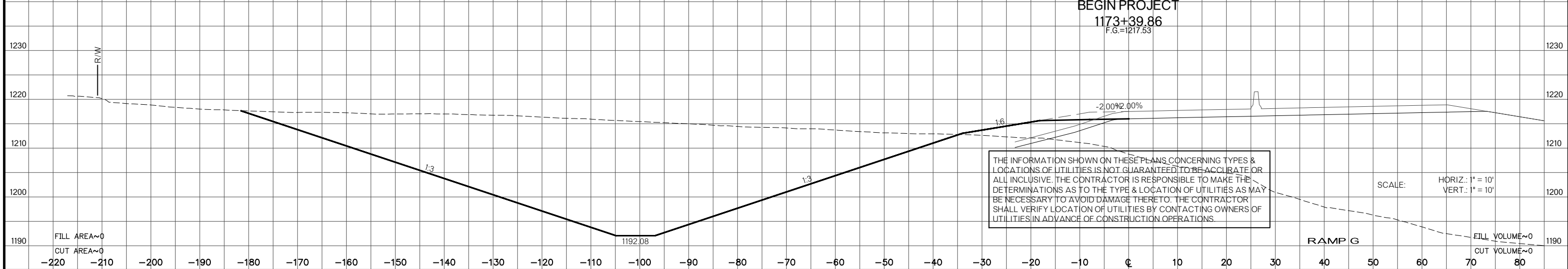
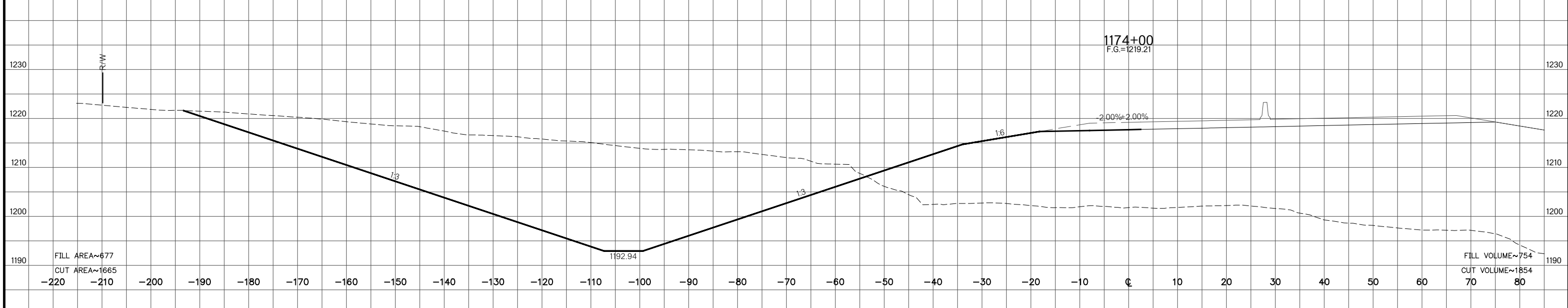
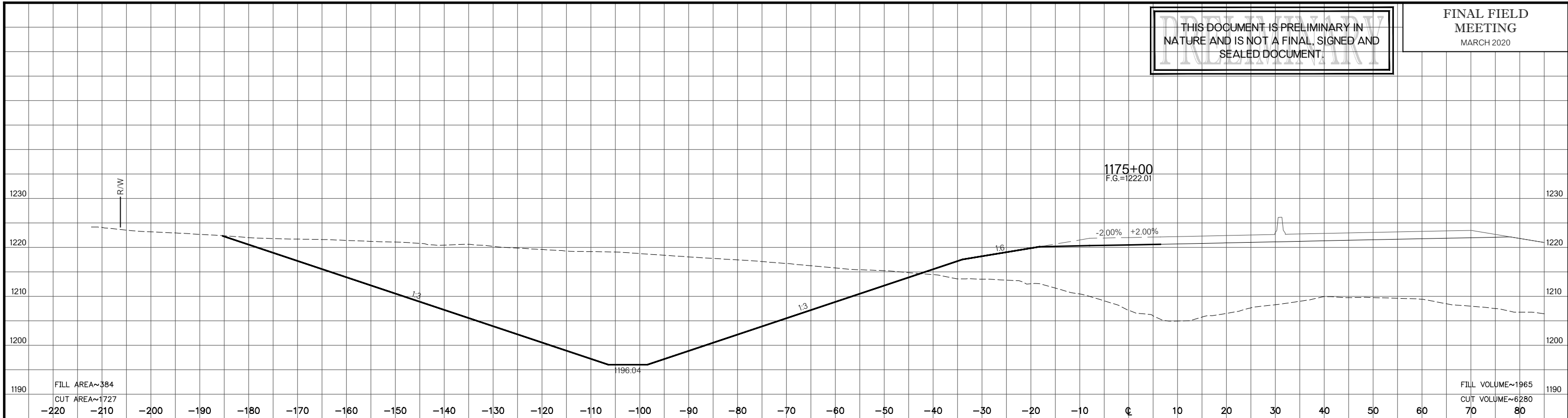
SCALE

HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

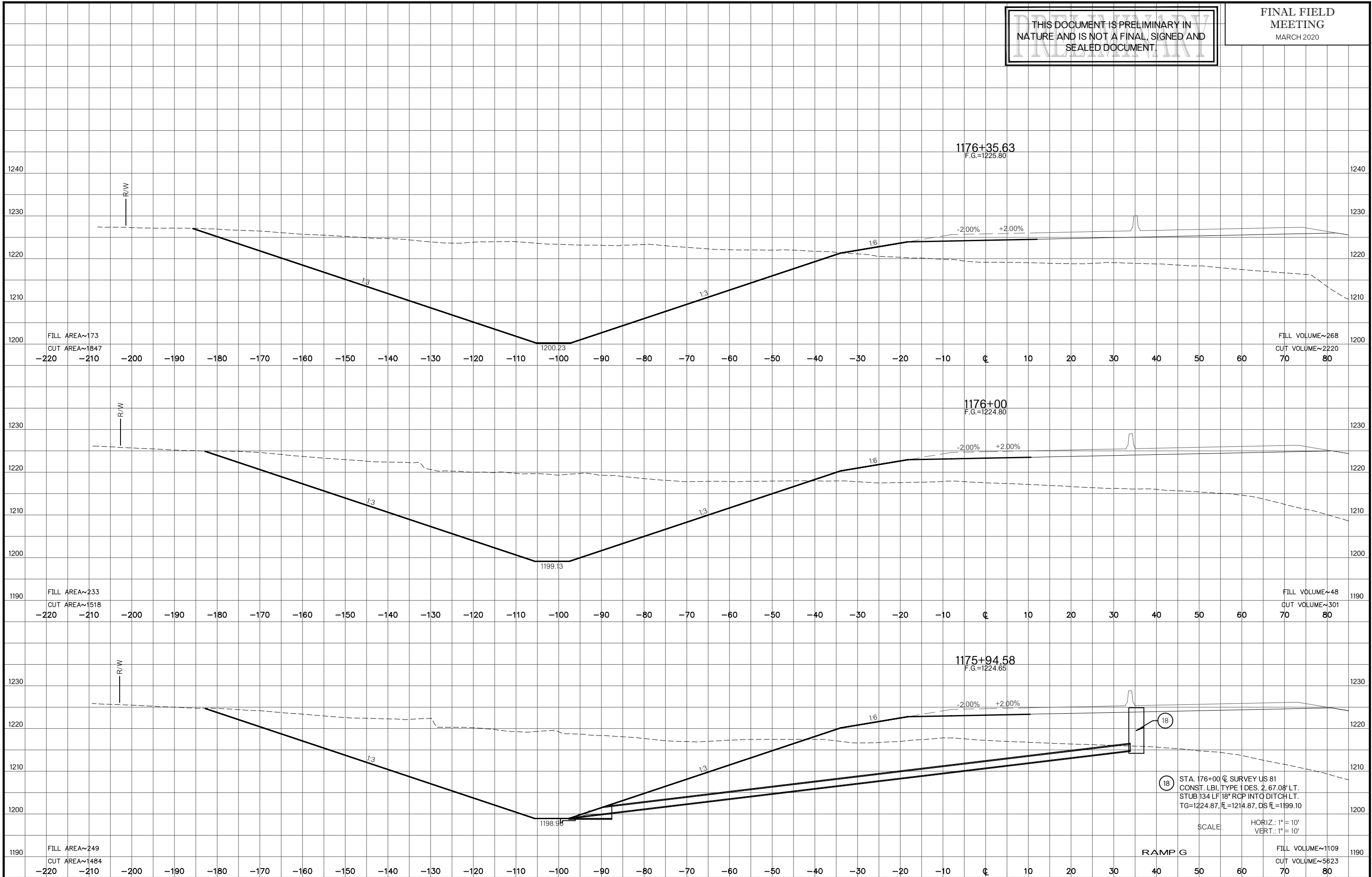
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP G

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



1176+35.63
F.G.=1225.80

1176+00
F.G.=1224.80

1175+94.58
F.G.=1224.65

FILL AREA~173
CUT AREA~1847

FILL VOLUME~268
CUT VOLUME~2220

FILL AREA~233
CUT AREA~1518

FILL VOLUME~48
CUT VOLUME~301

FILL AREA~249
CUT AREA~1484

FILL VOLUME~1109
CUT VOLUME~5823

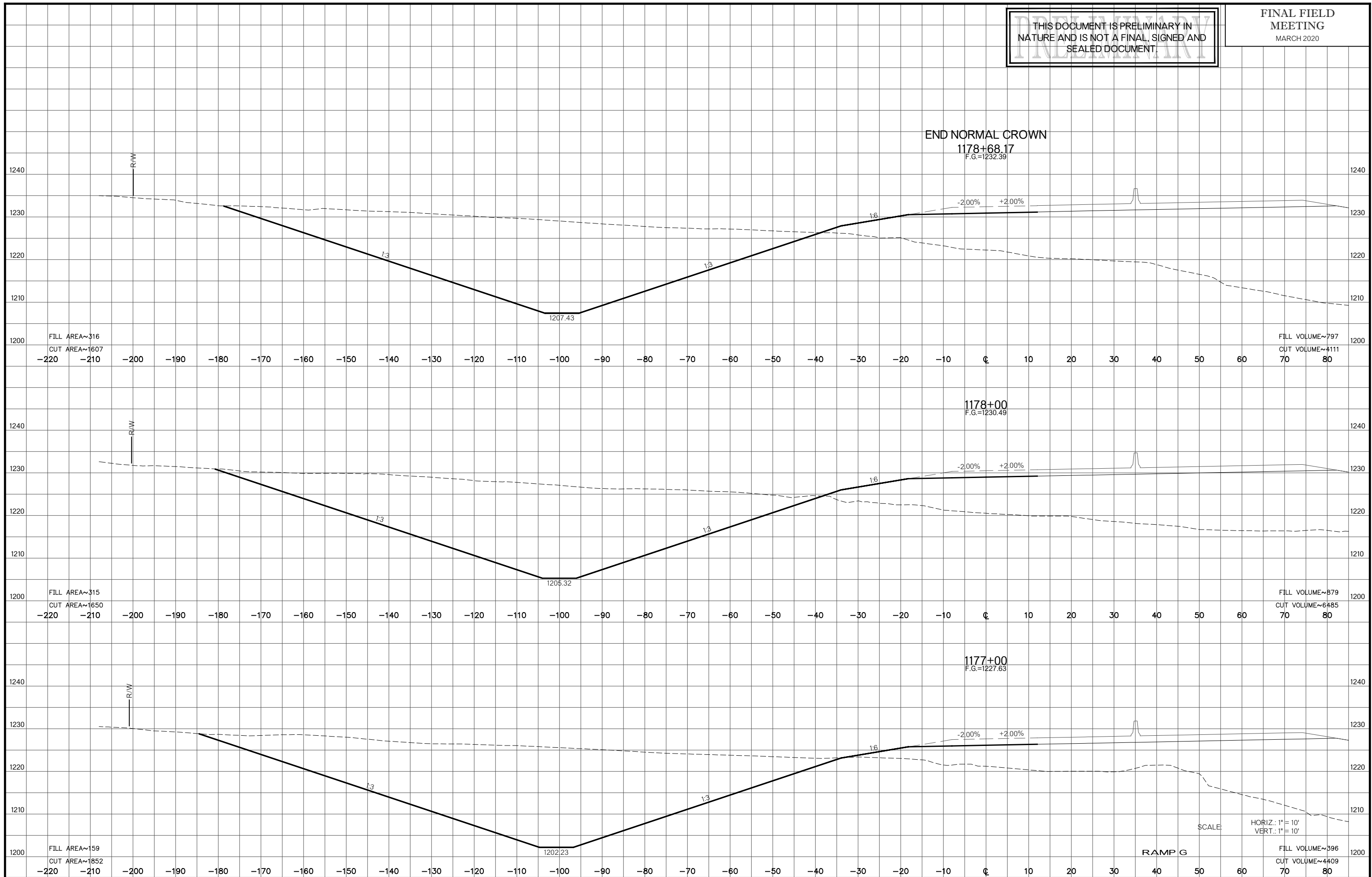
18 STA. 176+00 @ SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' LT.
STUB 134 LF 18" RCP INTO DITCH LT.
TG=1224.87, FL=1214.87, DS FL=1199.10

SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP G

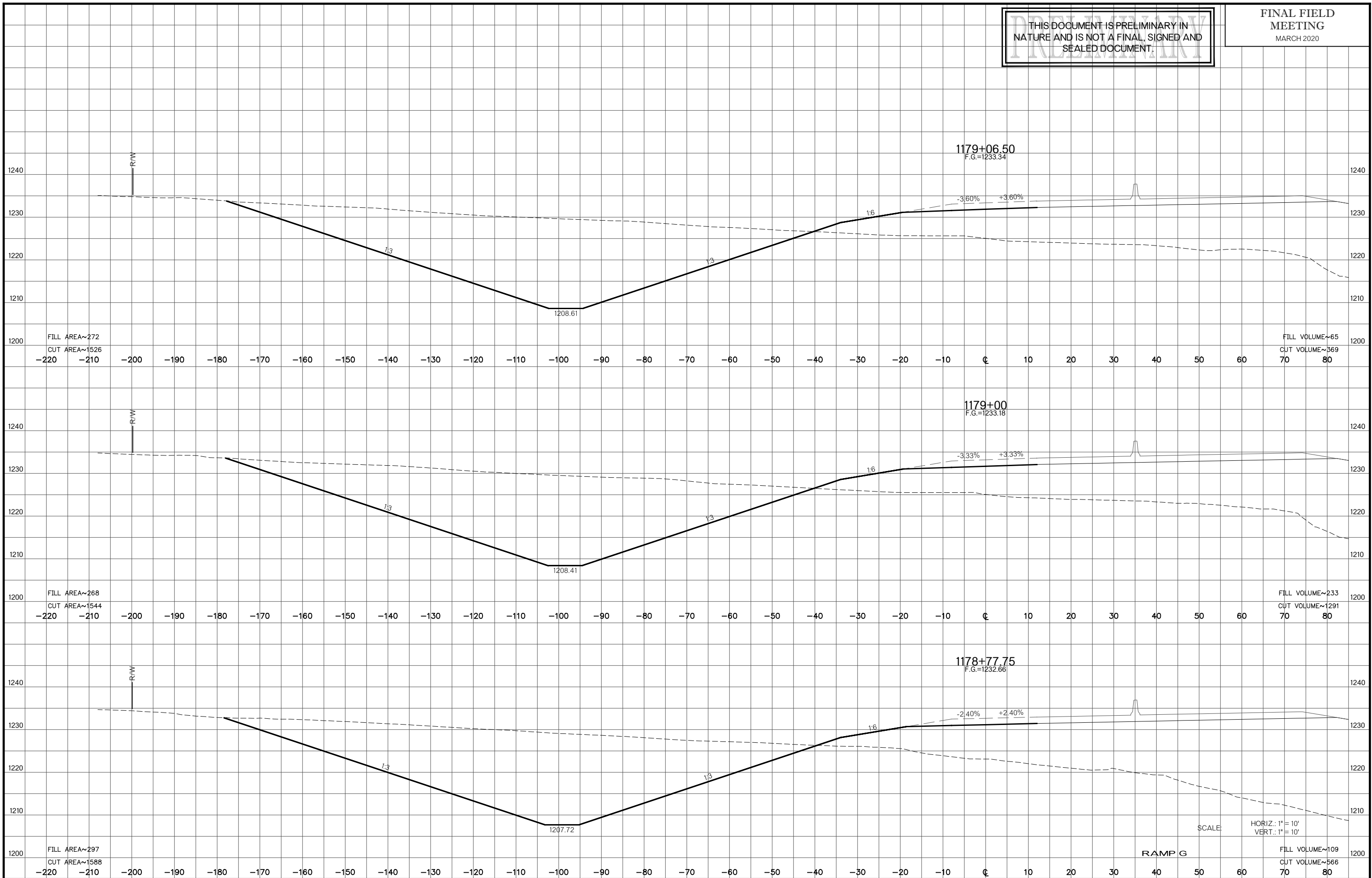
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

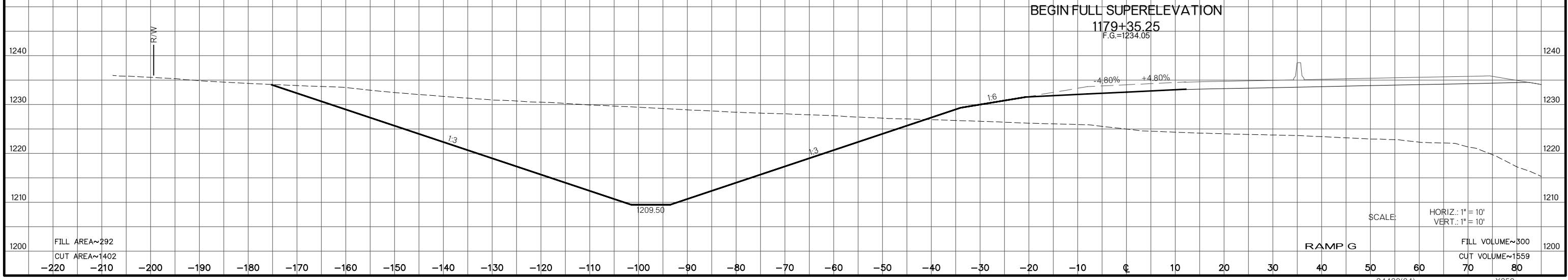
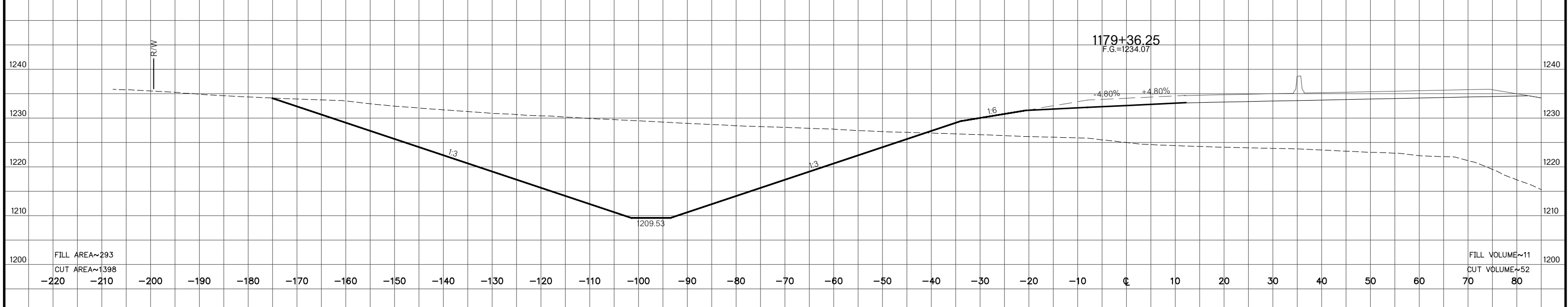
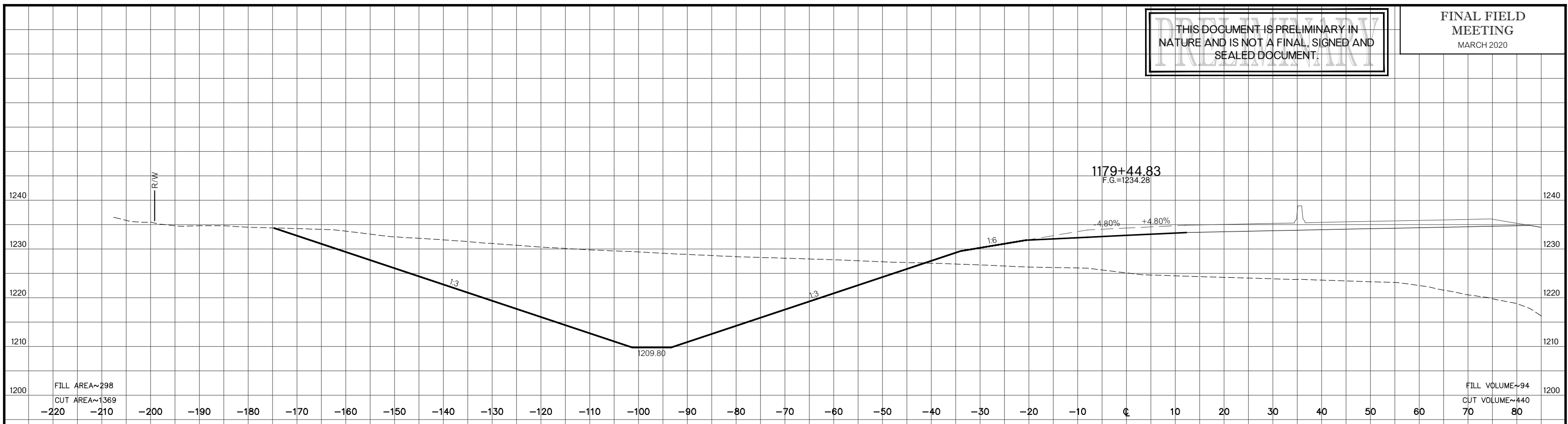
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP G

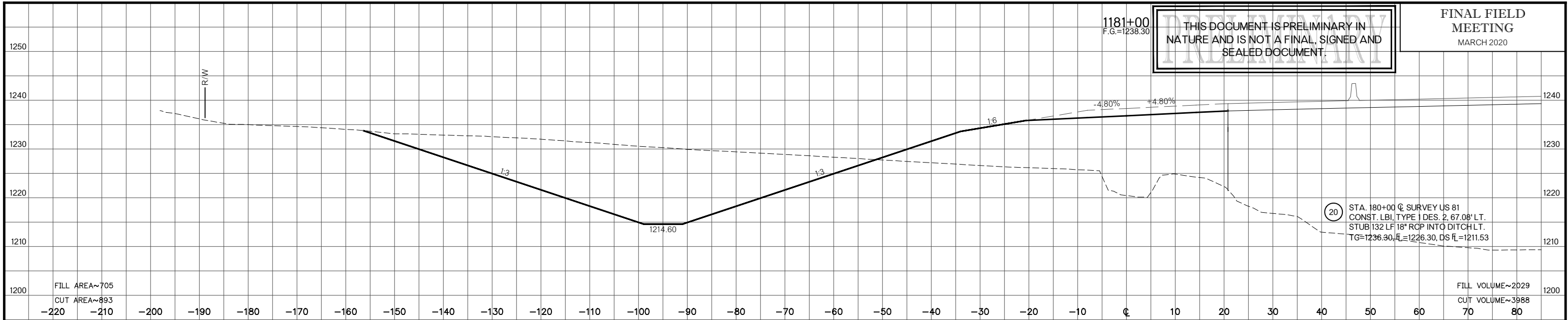
State Job No. 24428(04) Sheet No. X259

US 81 REALIGNMENT
GRADY COUNTY

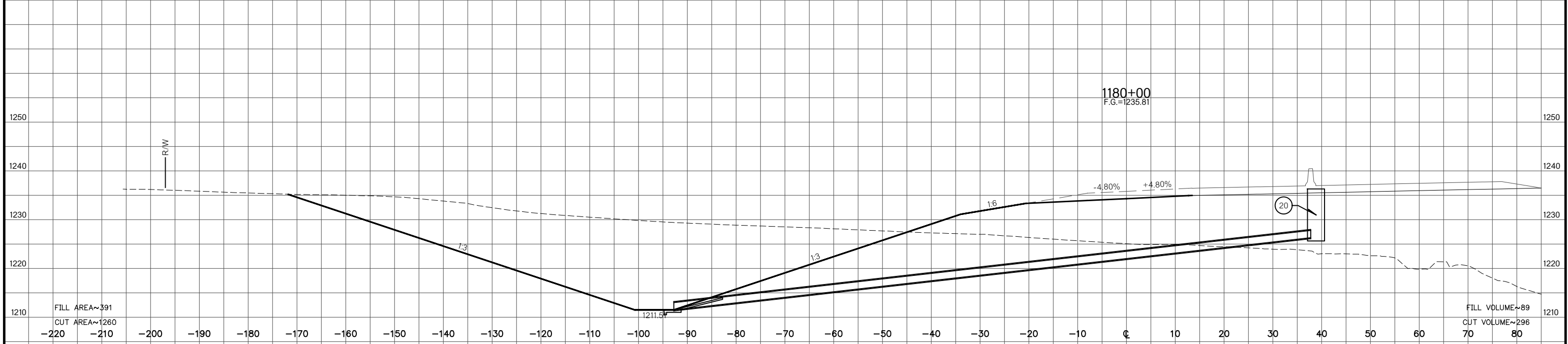
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

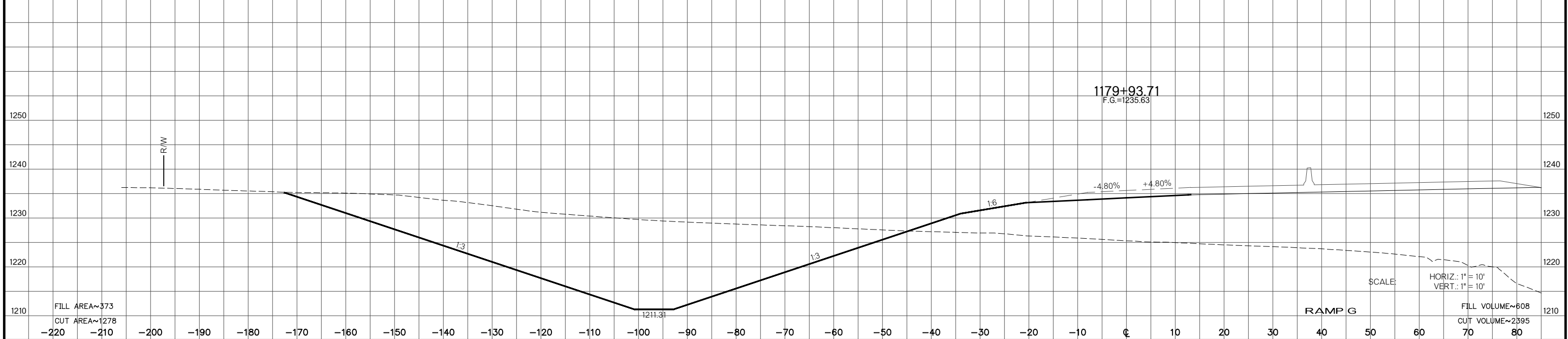
1181+00
F.G.=1238.30



1180+00
F.G.=1235.81



1179+93.71
F.G.=1235.63



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

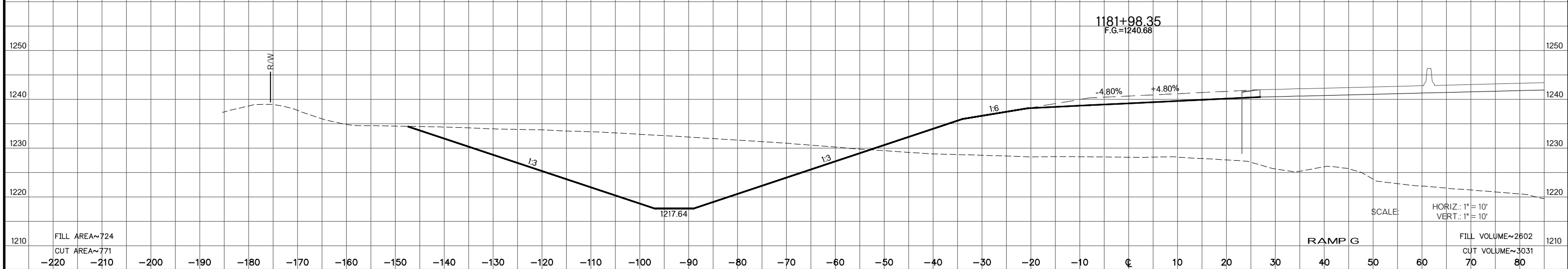
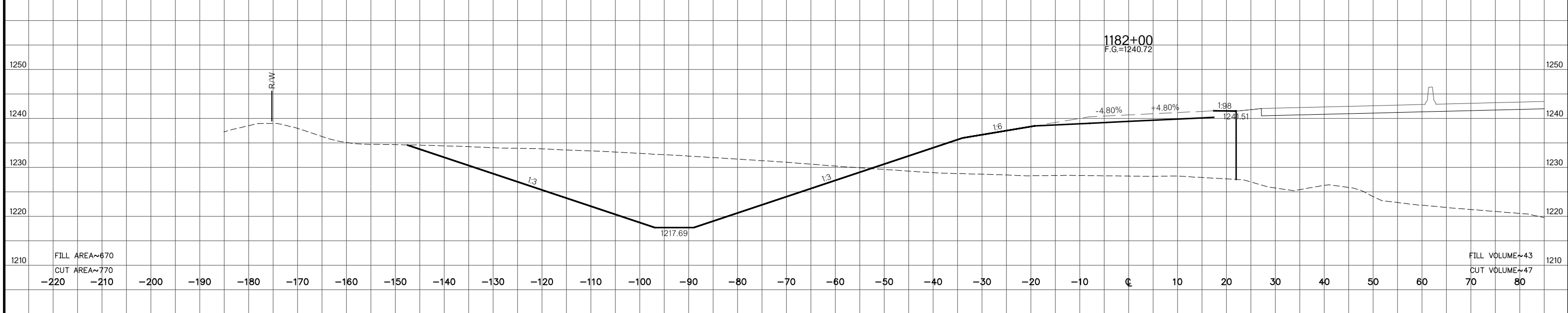
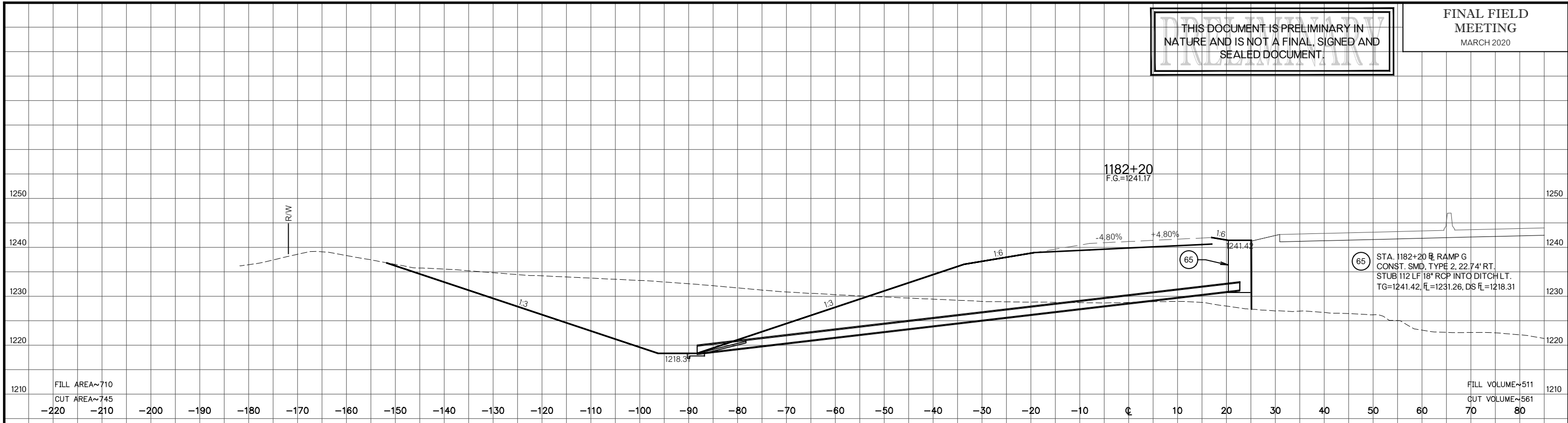
FILL VOLUME~608
CUT VOLUME~2395

State Job No. 24428(04) Sheet No. X260

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

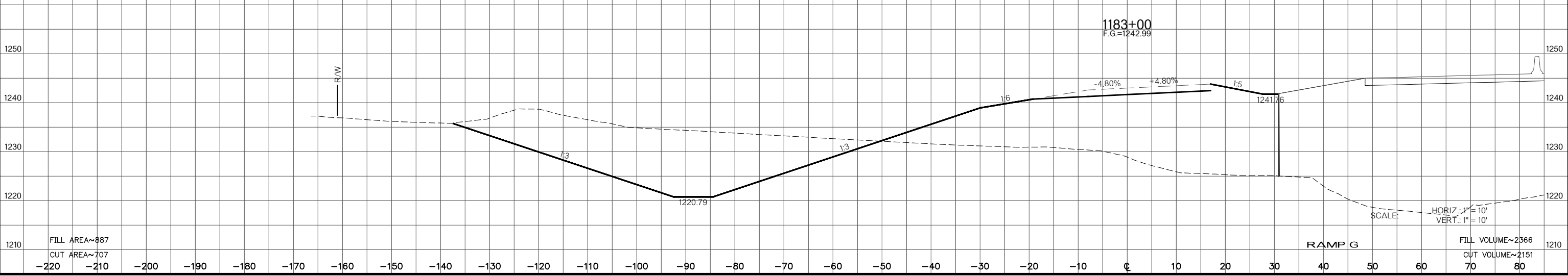
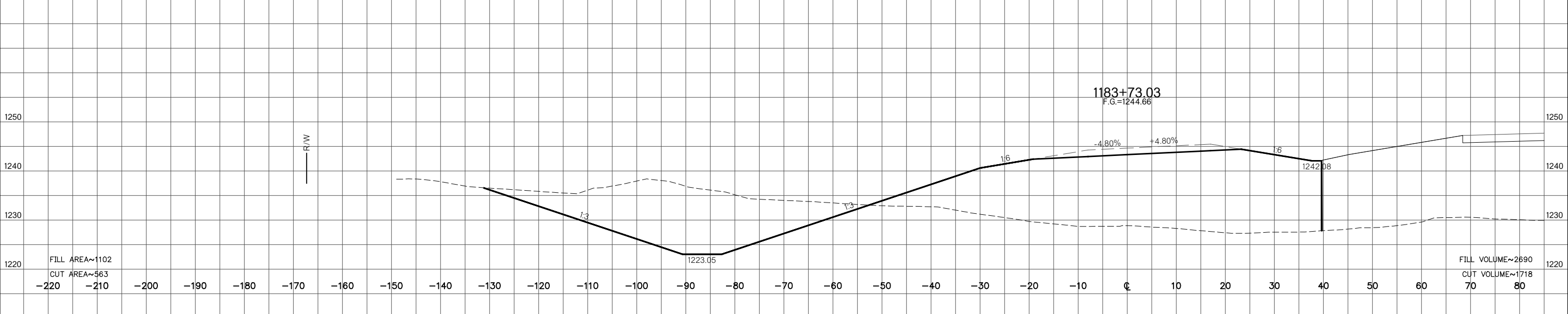
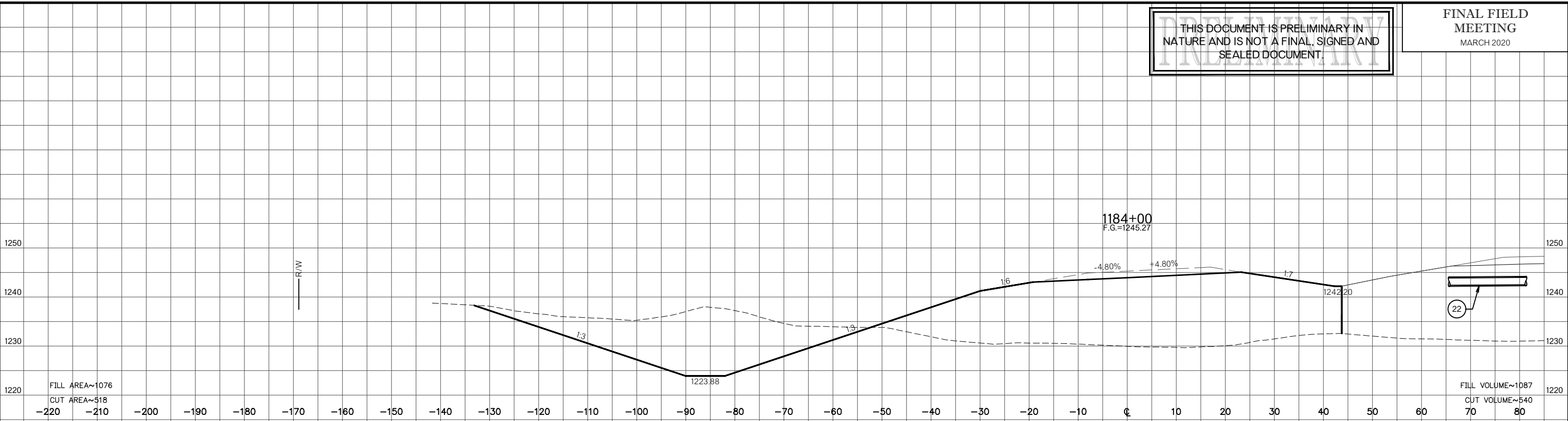
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

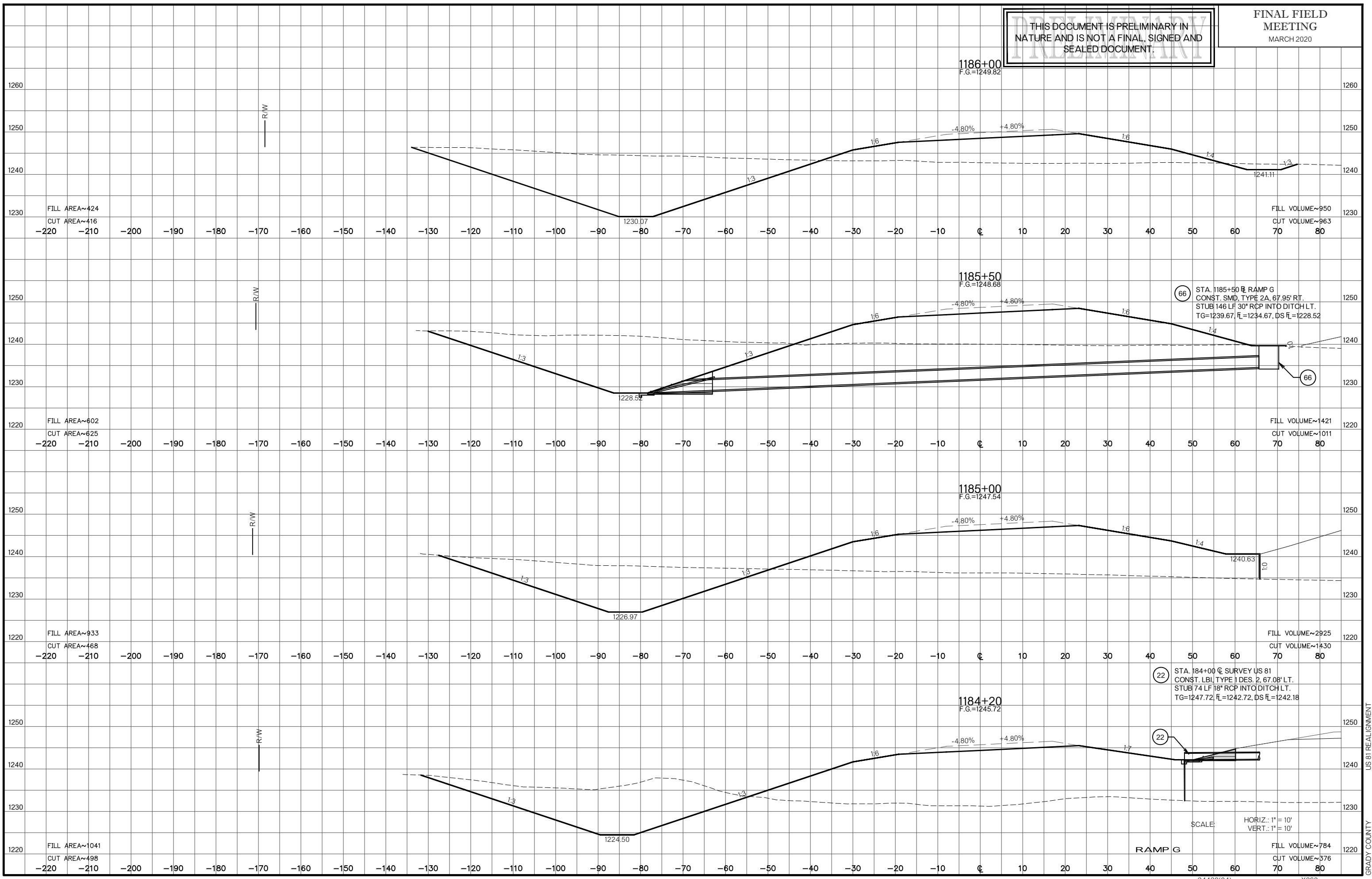
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



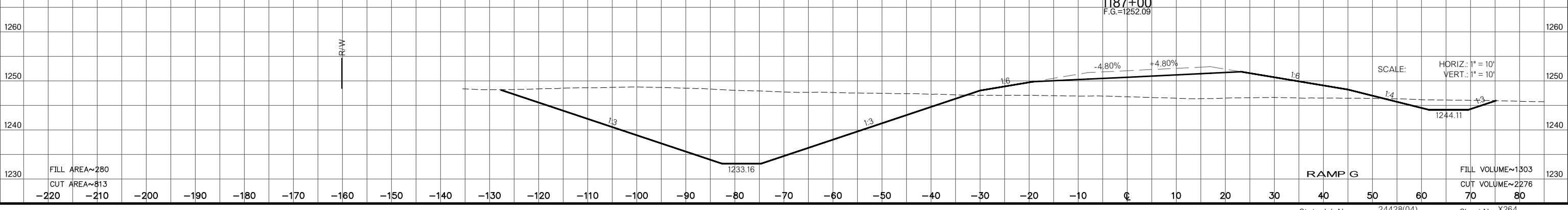
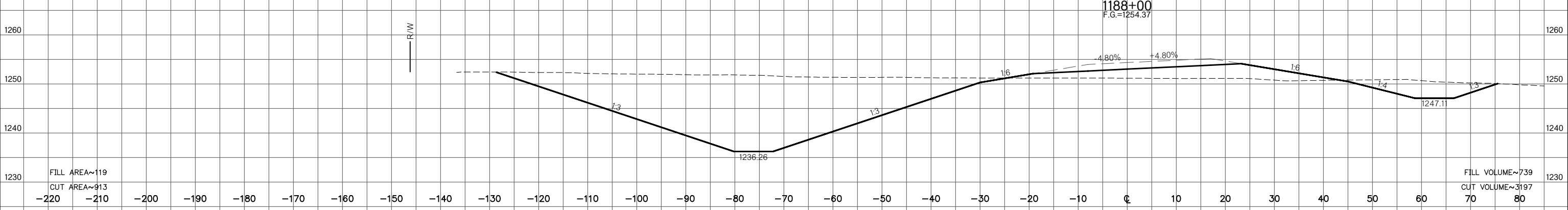
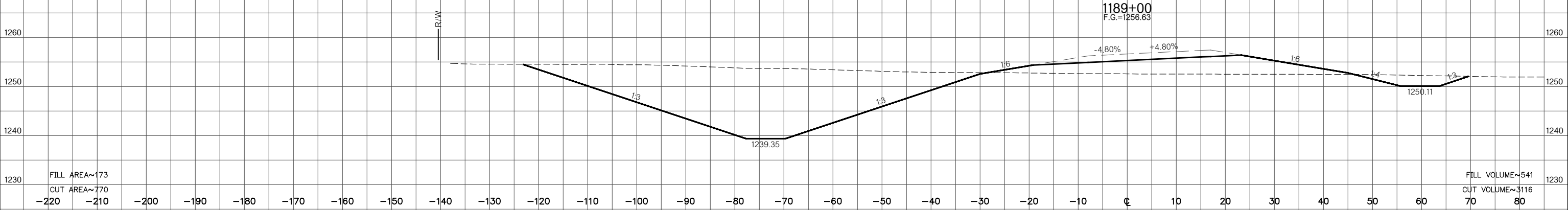
66 STA. 1185+50 @ RAMP G
CONST. SMD, TYPE 2A, 67.95' RT.
STUB 146 LF 30" RCP INTO DITCH LT.
TG=1239.67, FL=1234.67, DS FL=1228.52

22 STA. 184+00 @ SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' LT.
STUB 74 LF 18" RCP INTO DITCH LT.
TG=1247.72, FL=1242.72, DS FL=1242.18

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

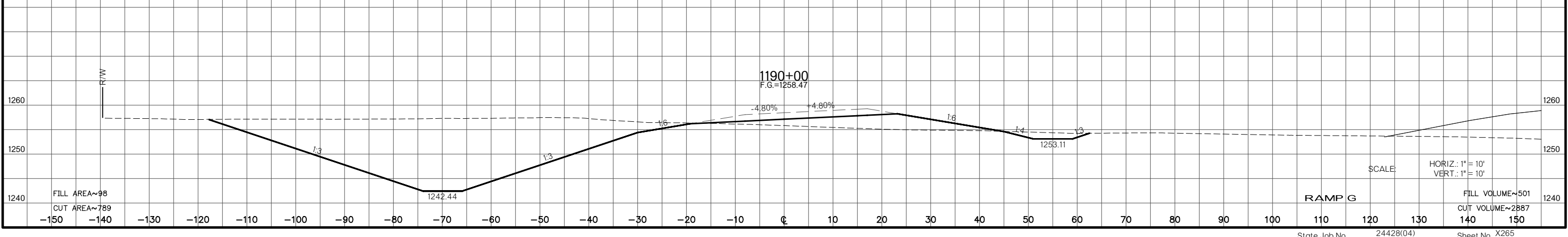
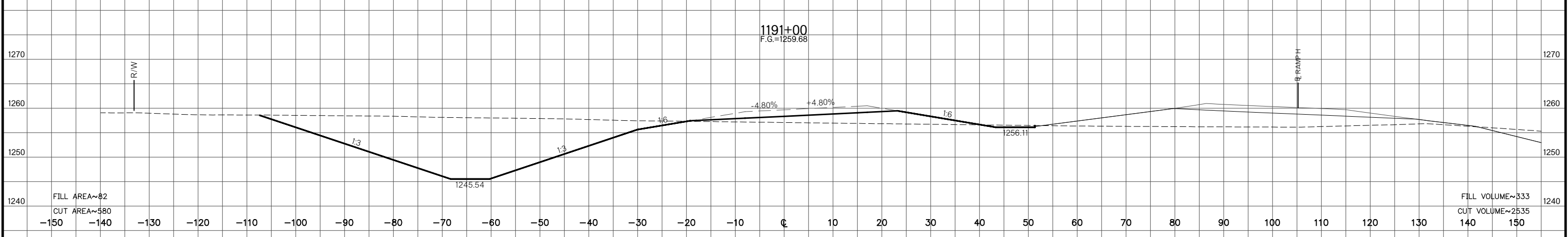
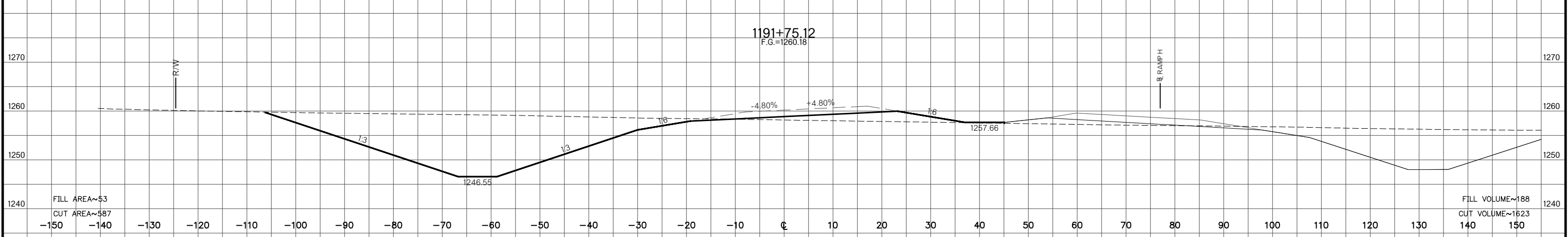
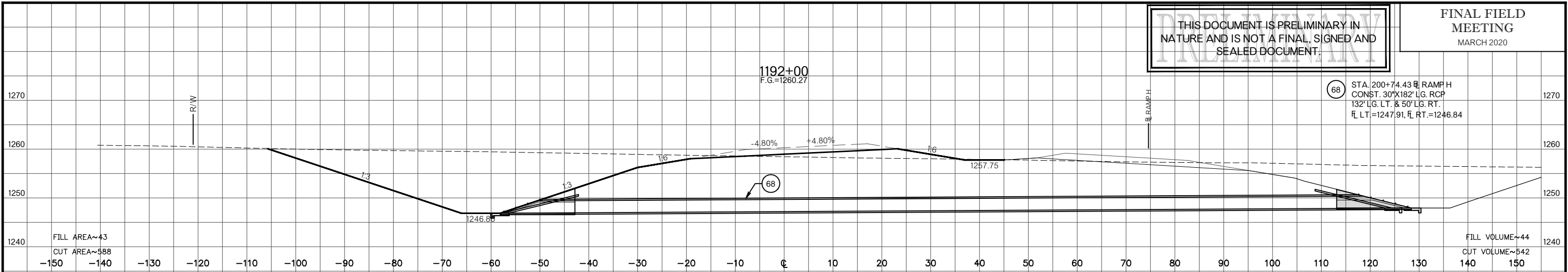


US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

68 STA. 200+74.43 B RAMP H
CONST. 30'X182' LG. RCP
132' LG. LT. & 50' LG. RT.
FL LT.=1247.91, FL RT.=1246.84

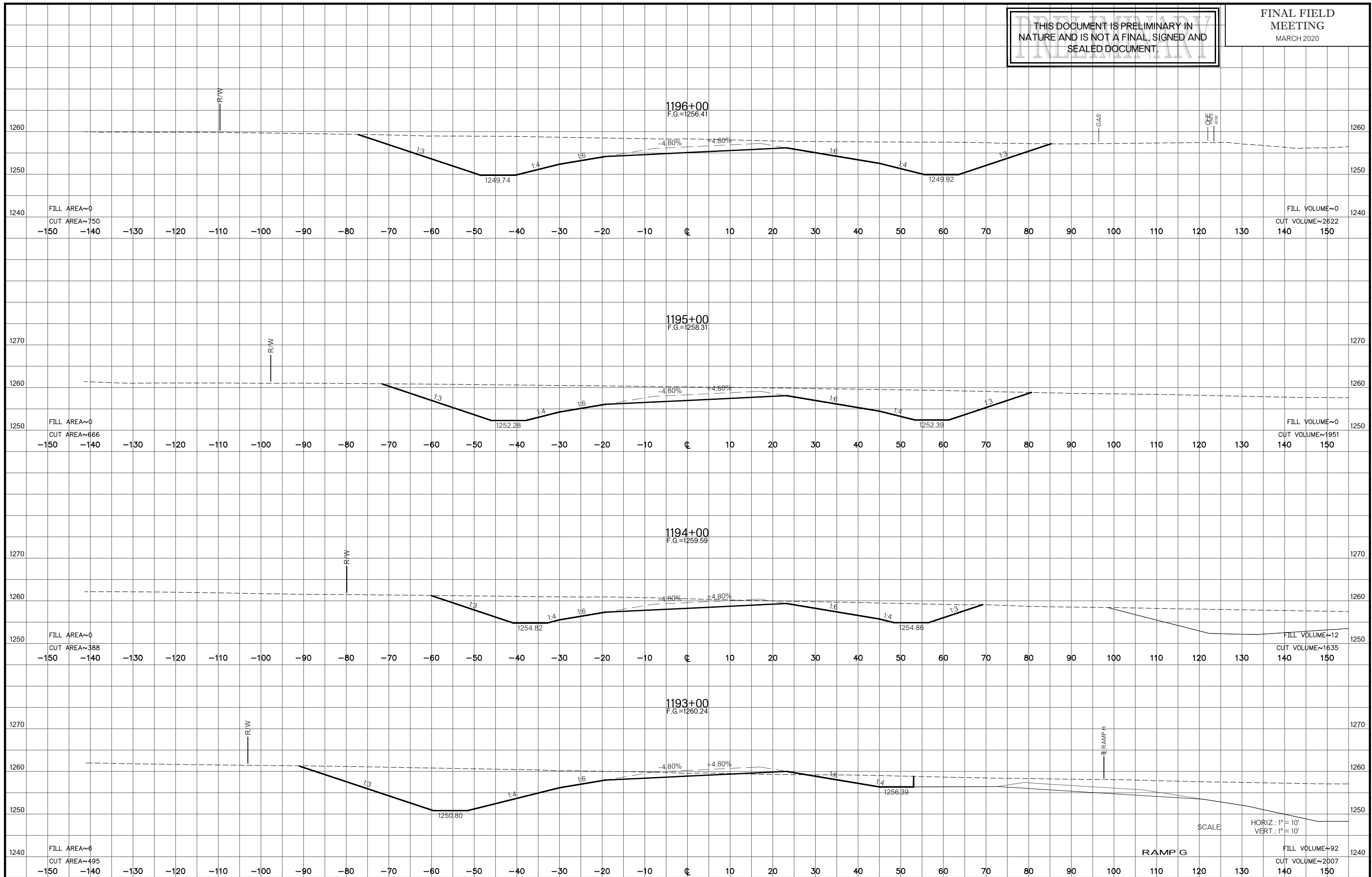


SCALE HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP G

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

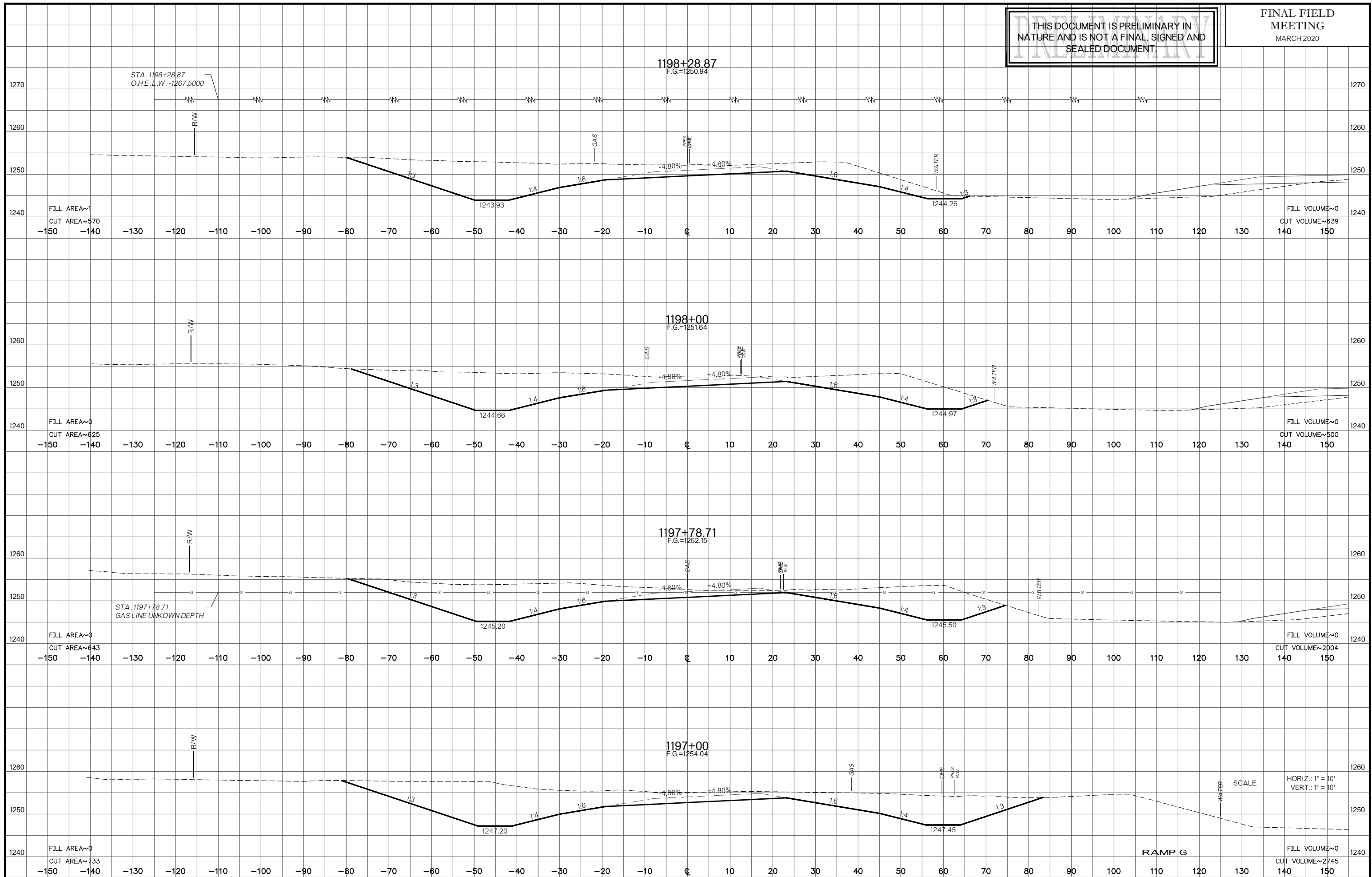


SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP G

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



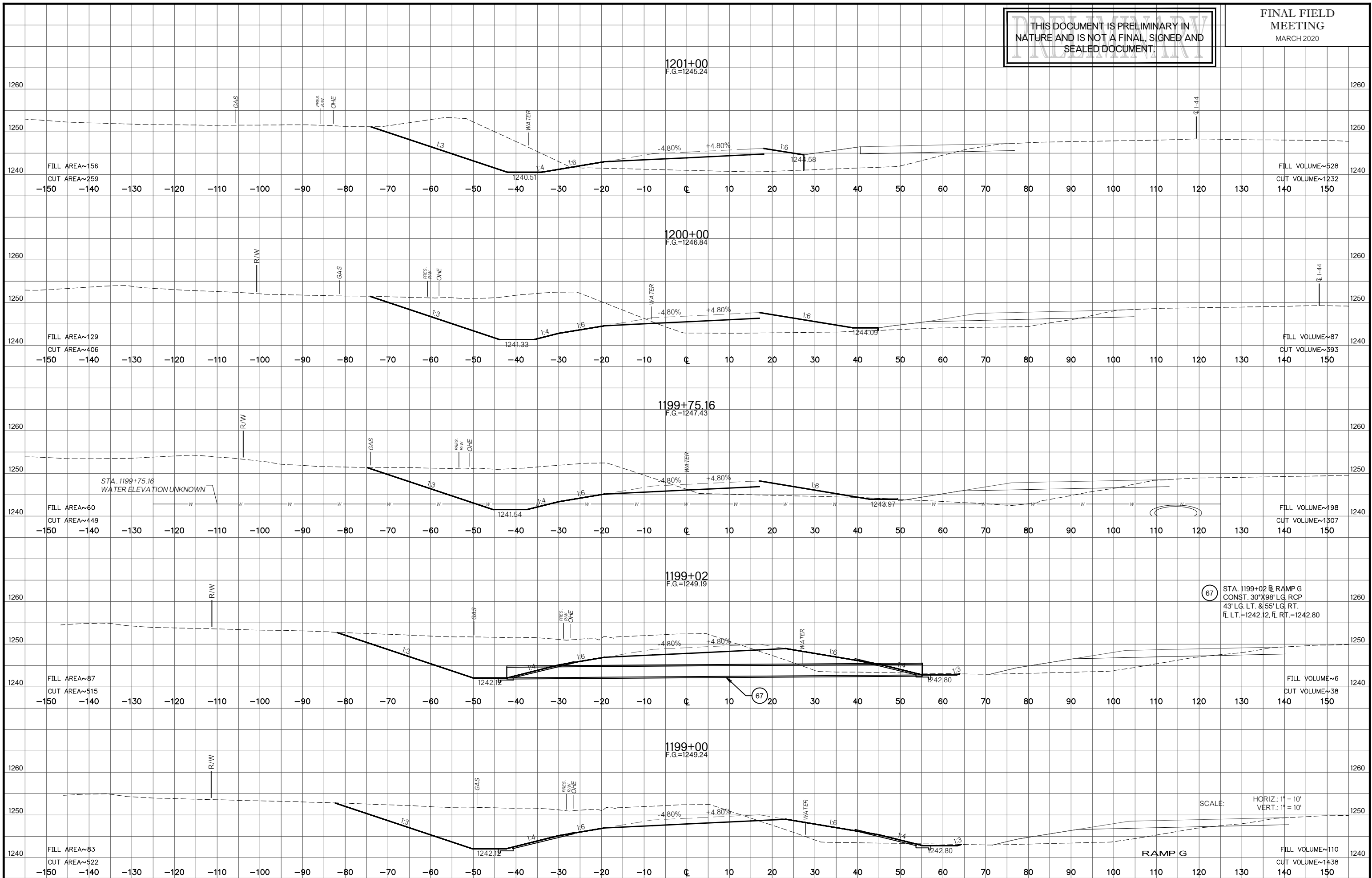
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP G

State Job No. 24428(04) Sheet No. X267

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

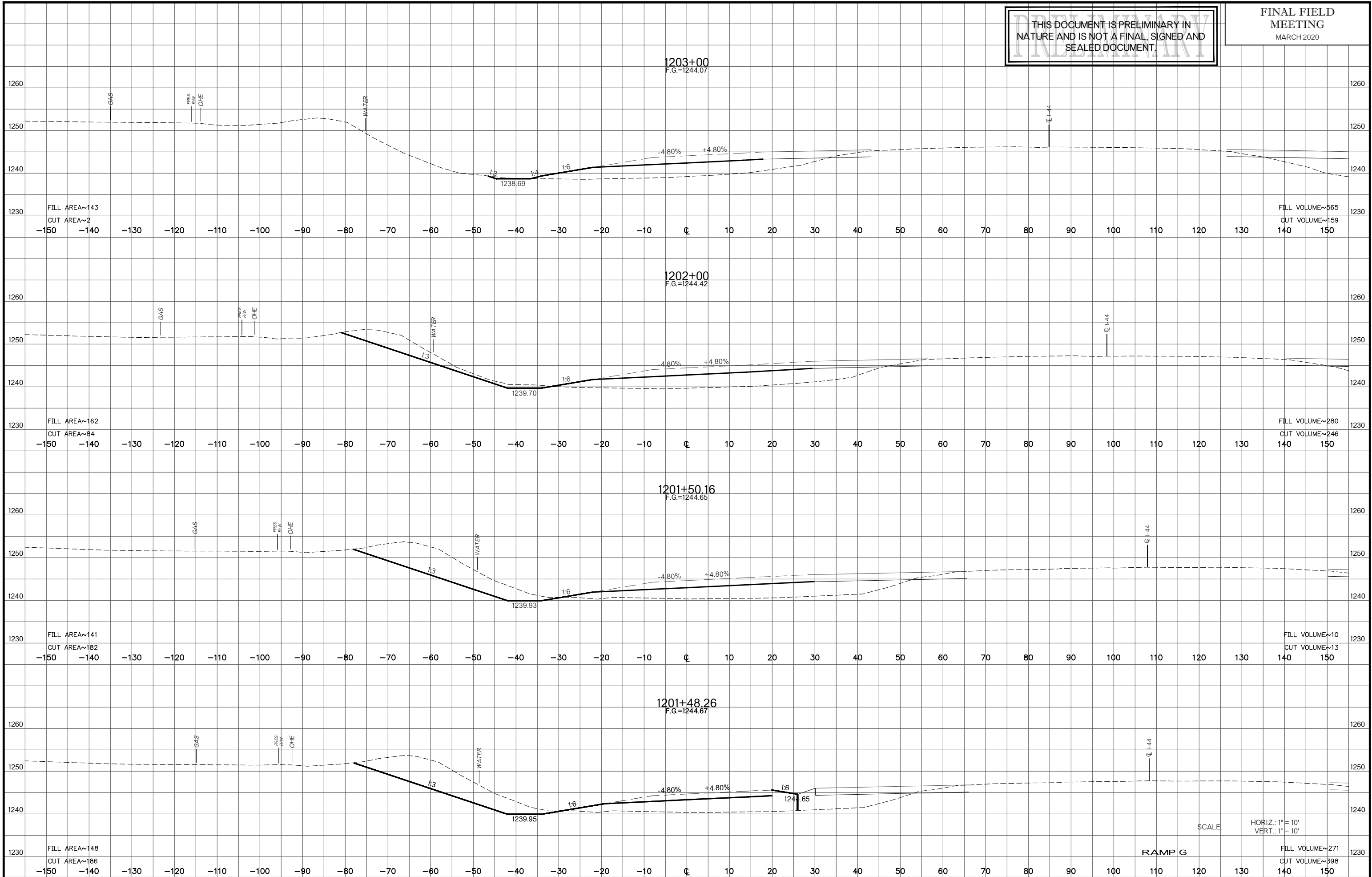


SCALE: HORIZ: 1" = 10'
VERT: 1" = 10'

RAMP G
FILL VOLUME ~110
CUT VOLUME ~1438

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



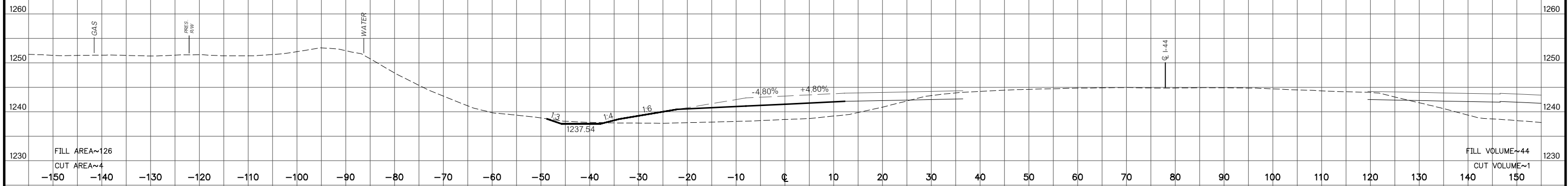
SCALE HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP G FILL VOLUME~271 CUT VOLUME~398

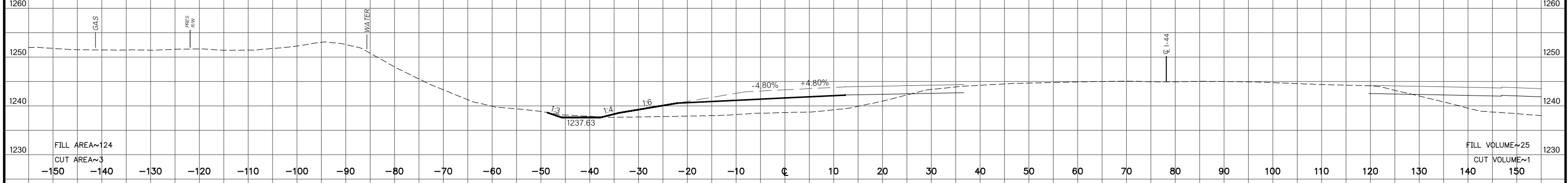
US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

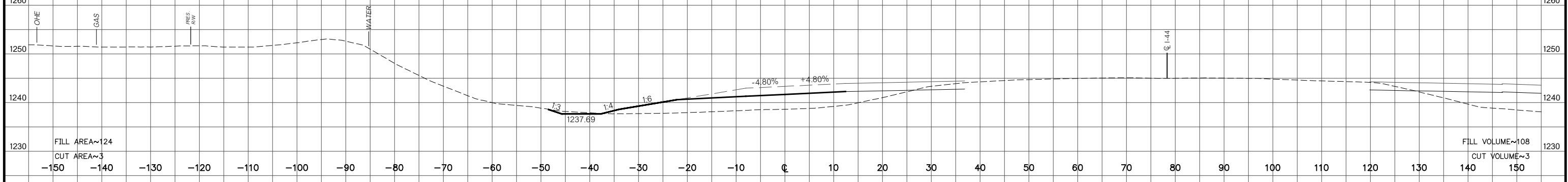
END FULL SUPERELEVATION 1204+14.99 F.G.=1243.20



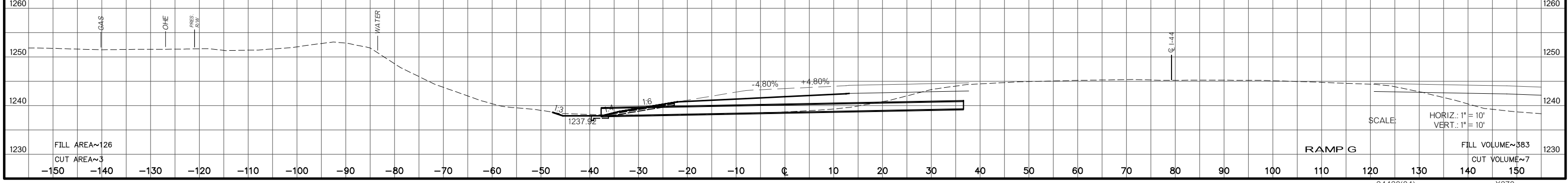
1204+05.41 F.G.=1243.29



1204+00 F.G.=1243.34



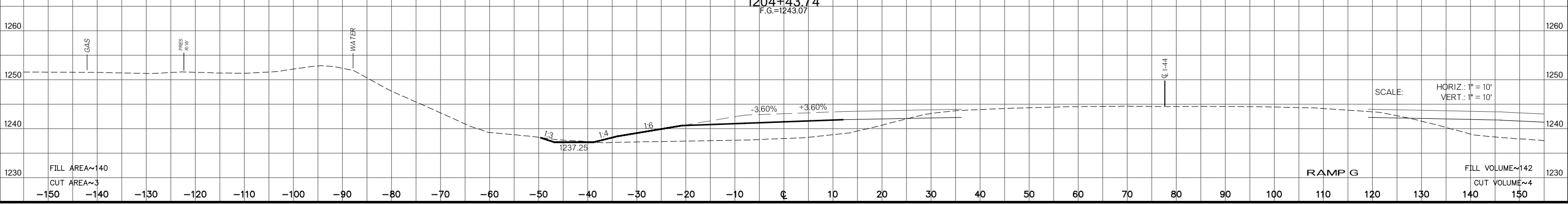
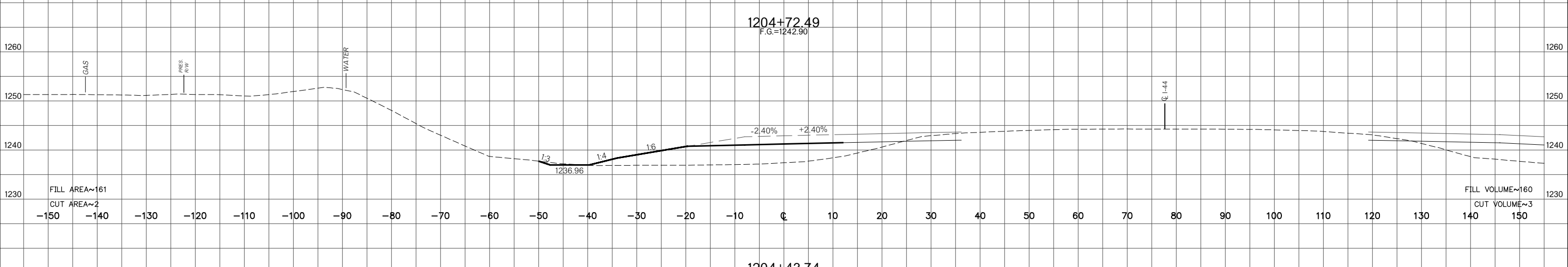
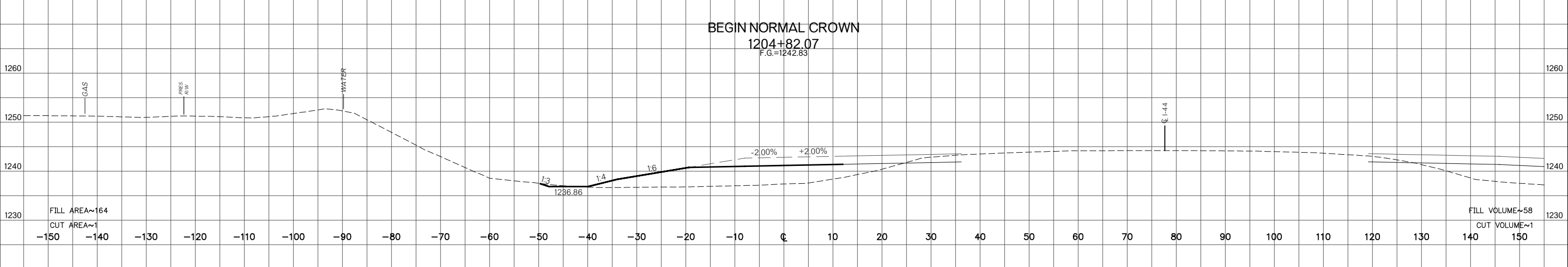
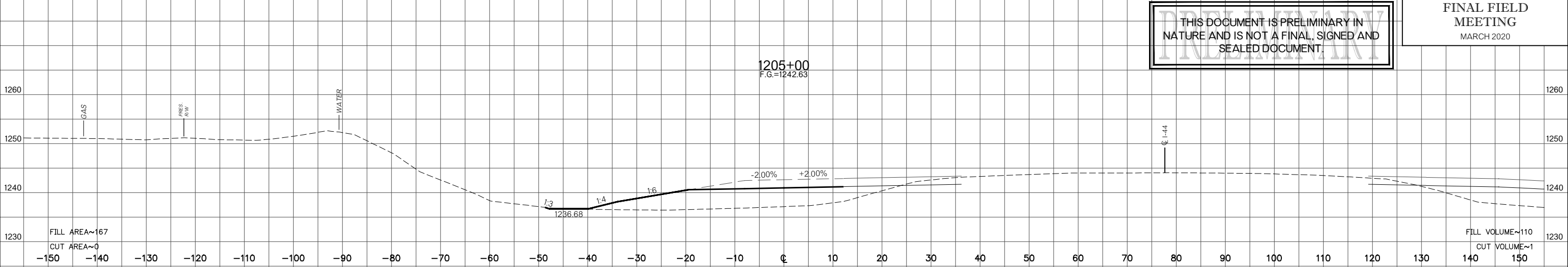
1203+76.79 F.G.=1243.51



SCALE HORIZ.: 1" = 10' VERT.: 1" = 10' RAMP G FILL VOLUME~383 CUT VOLUME~7 State Job No. 24428(04) Sheet No. X270

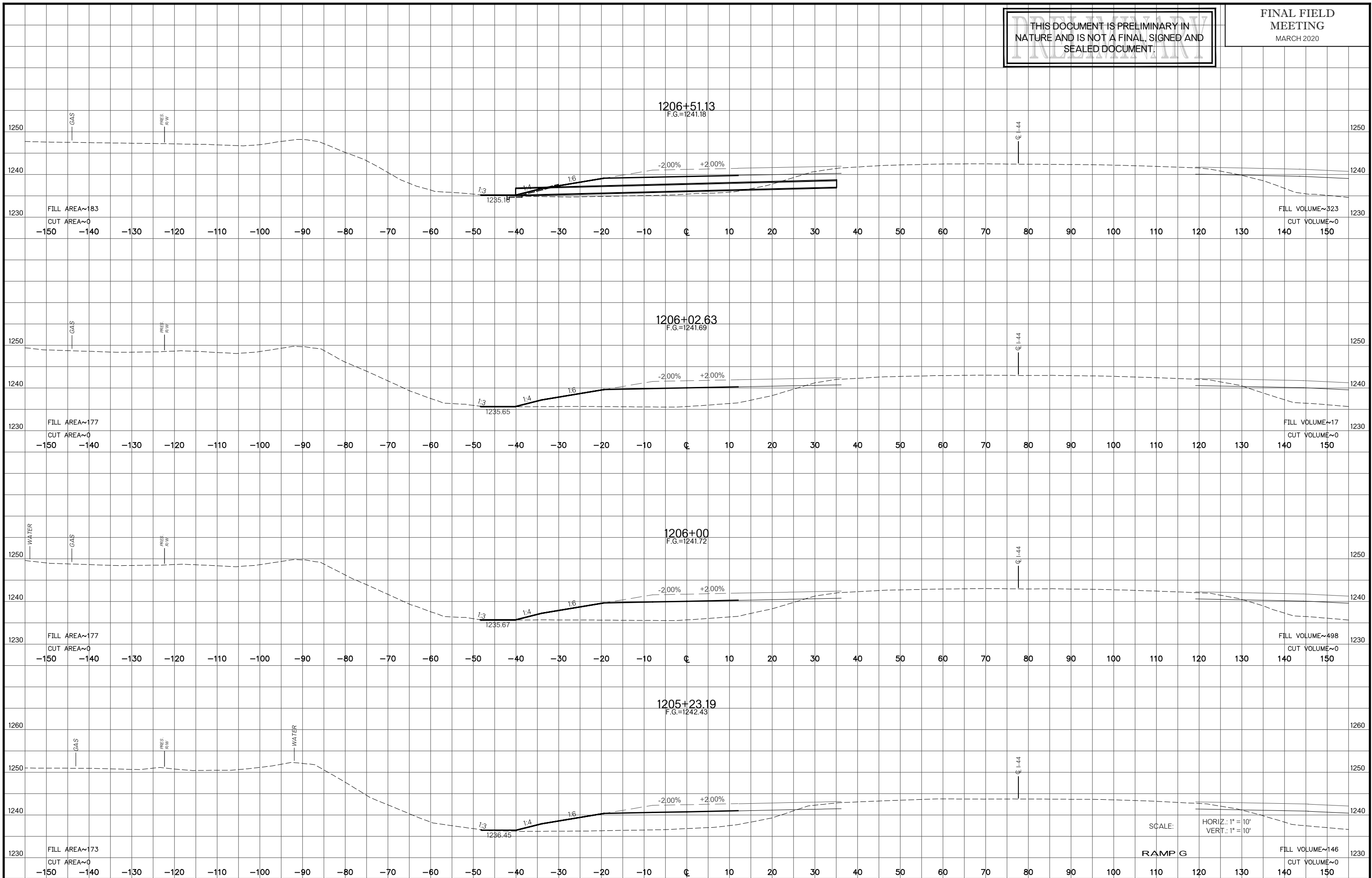
US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

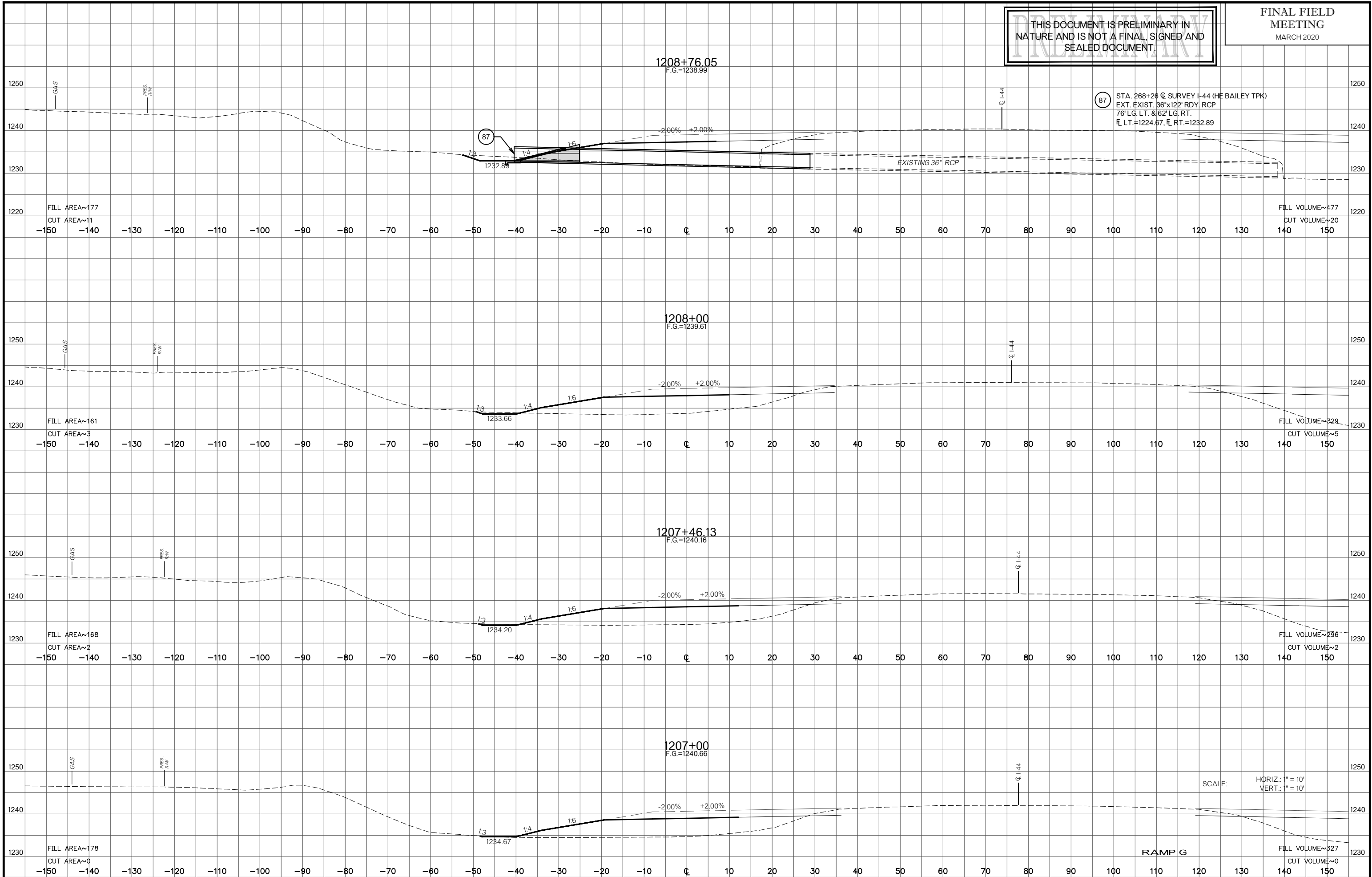


US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

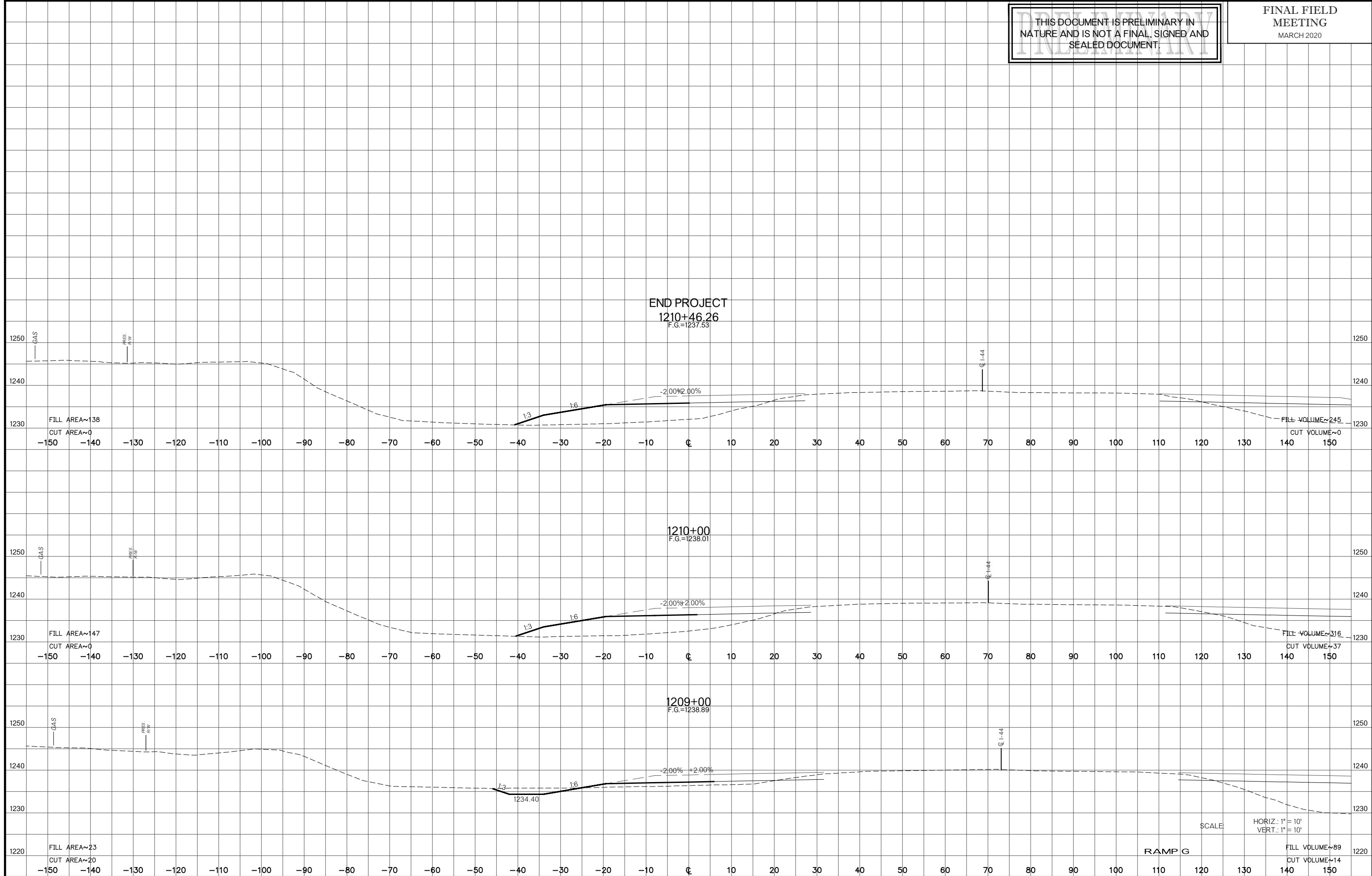


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



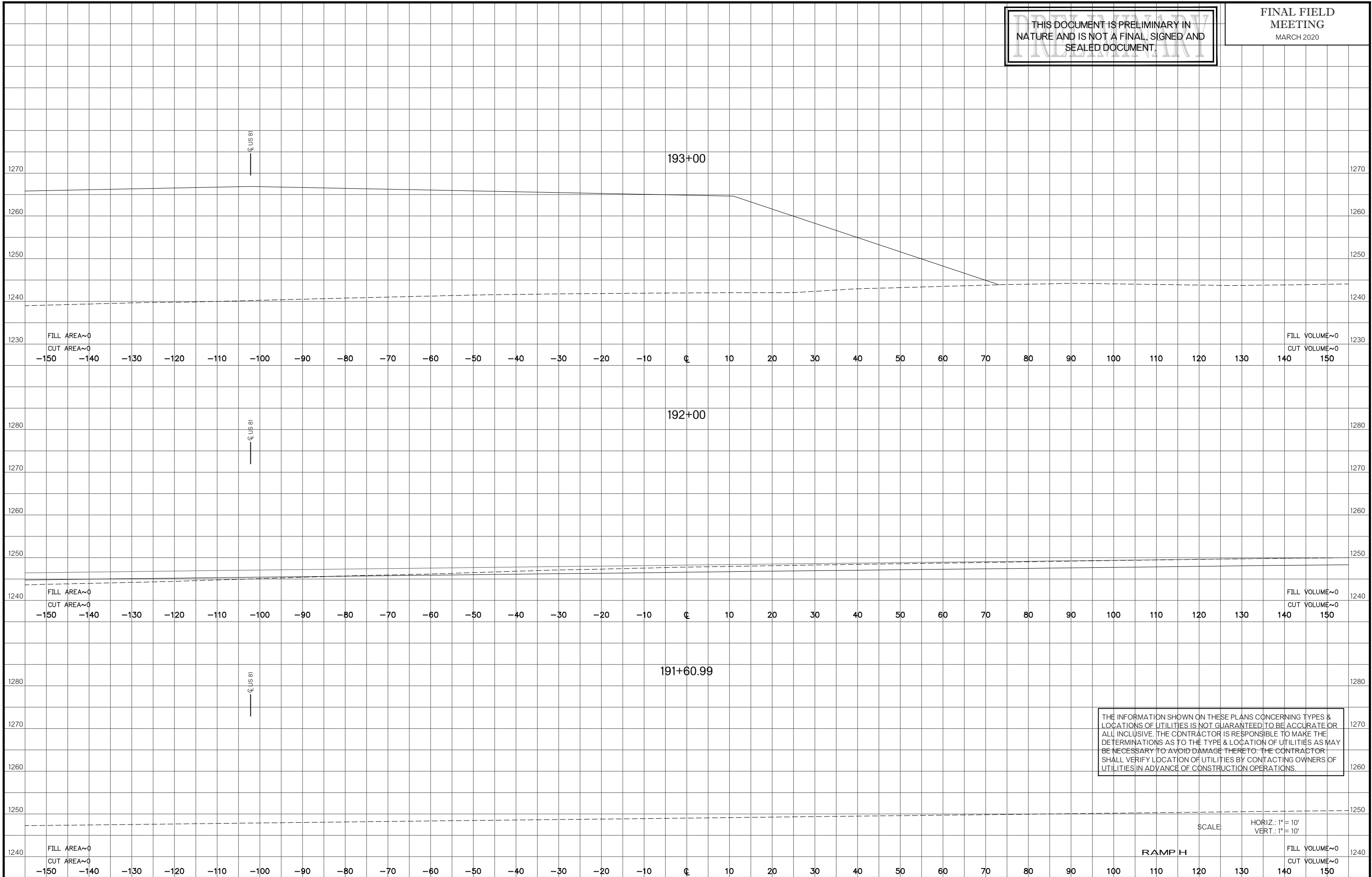
US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



FILL AREA~0
CUT AREA~0

FILL VOLUME~0
CUT VOLUME~0

FILL AREA~0
CUT AREA~0

FILL VOLUME~0
CUT VOLUME~0

FILL AREA~0
CUT AREA~0

FILL VOLUME~0
CUT VOLUME~0

THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

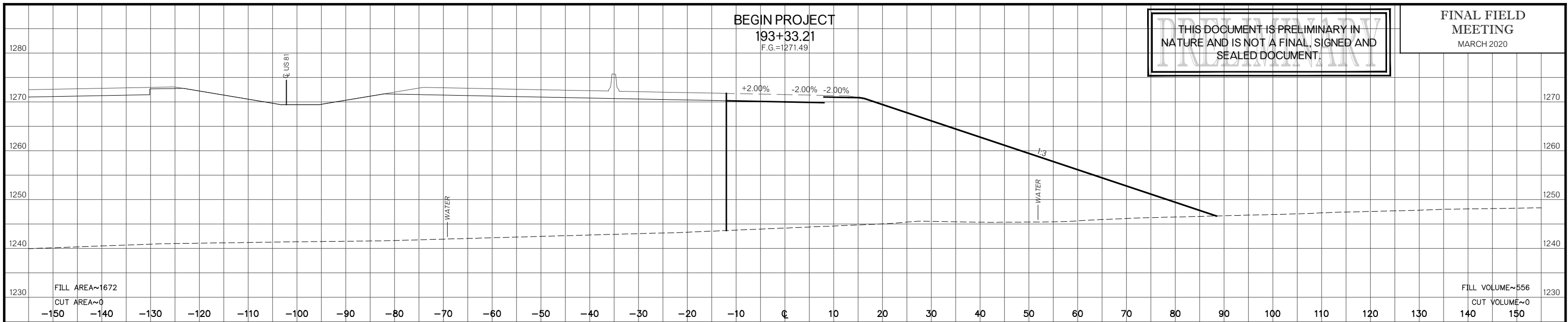
RAMP H

US 81 REALIGNMENT
GRADY COUNTY

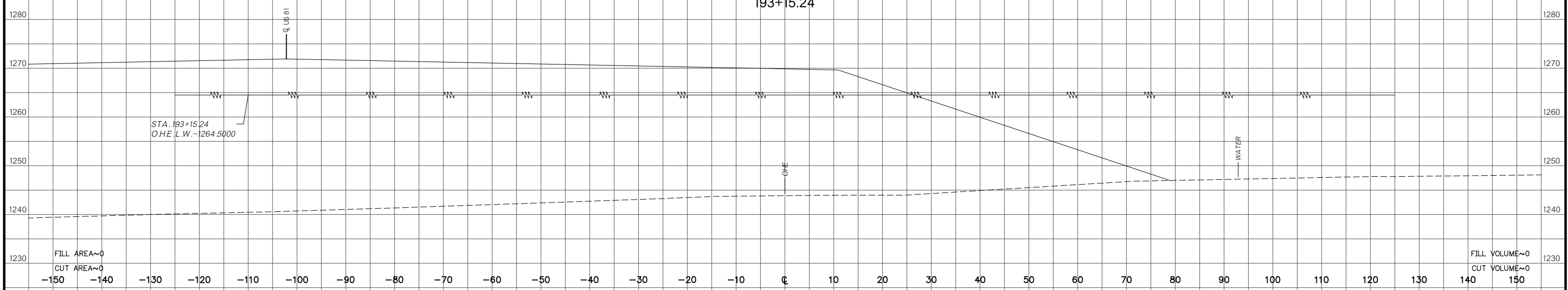
BEGIN PROJECT
193+33.21
F.G.=1271.49

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

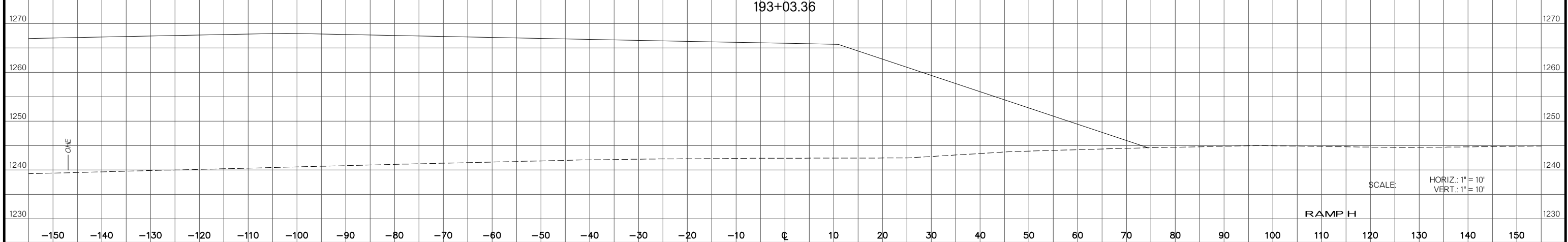
FINAL FIELD MEETING
MARCH 2020



193+15.24

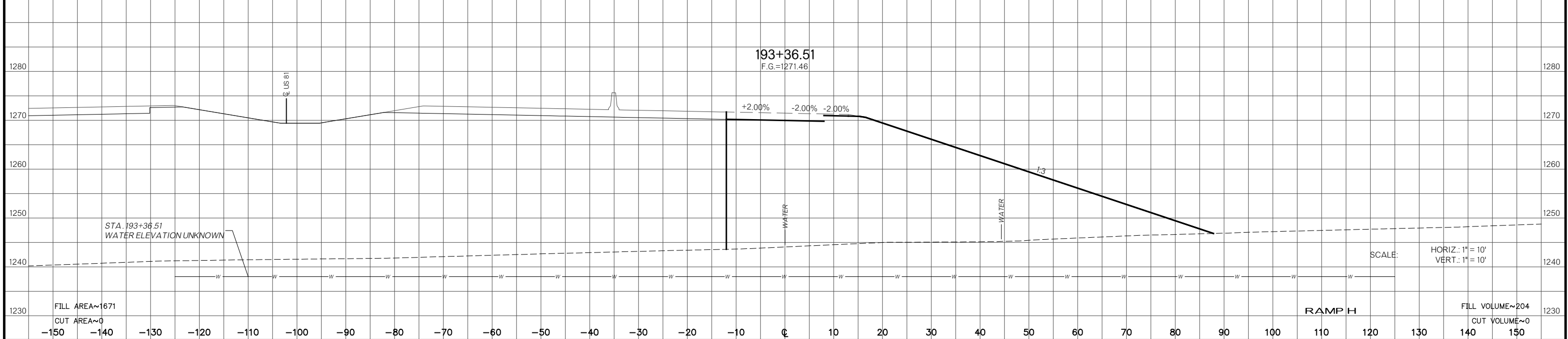
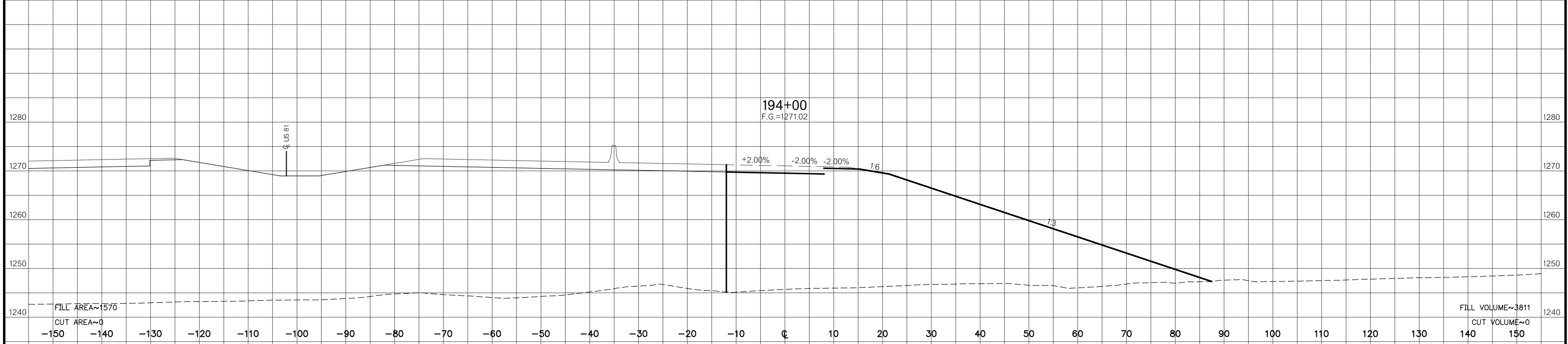
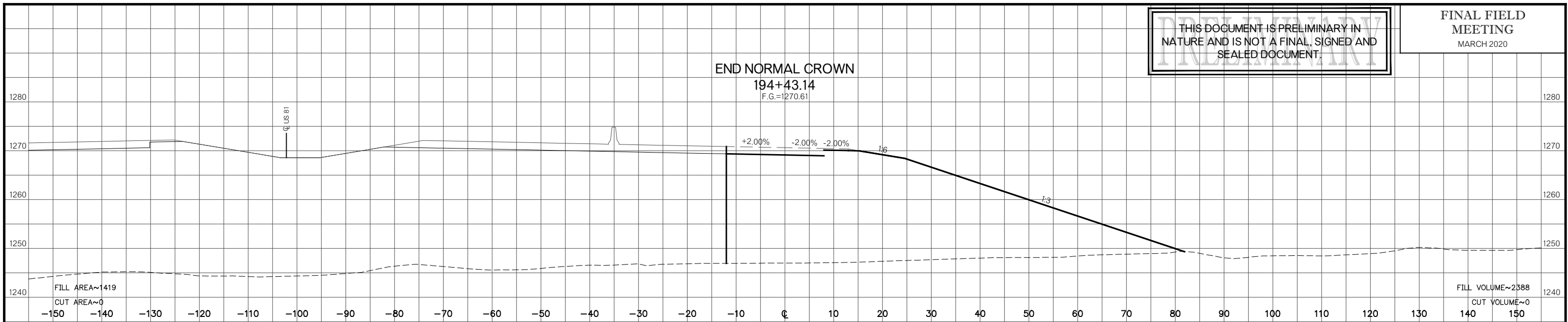


BEGIN/END BRIDGE
193+03.36

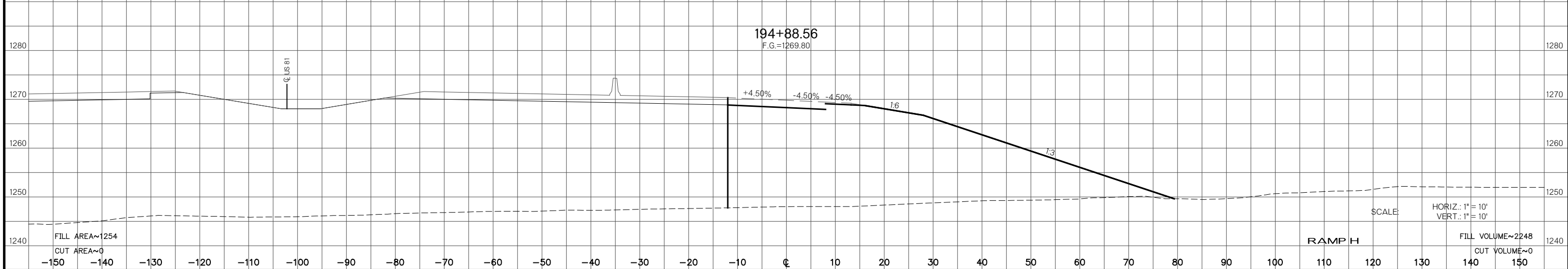
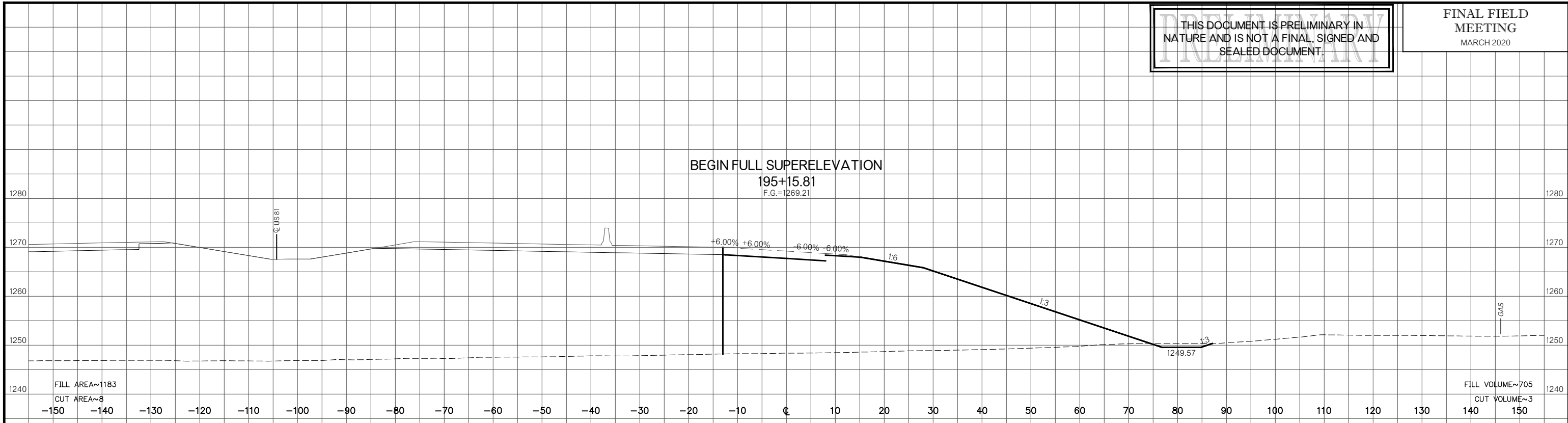


SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

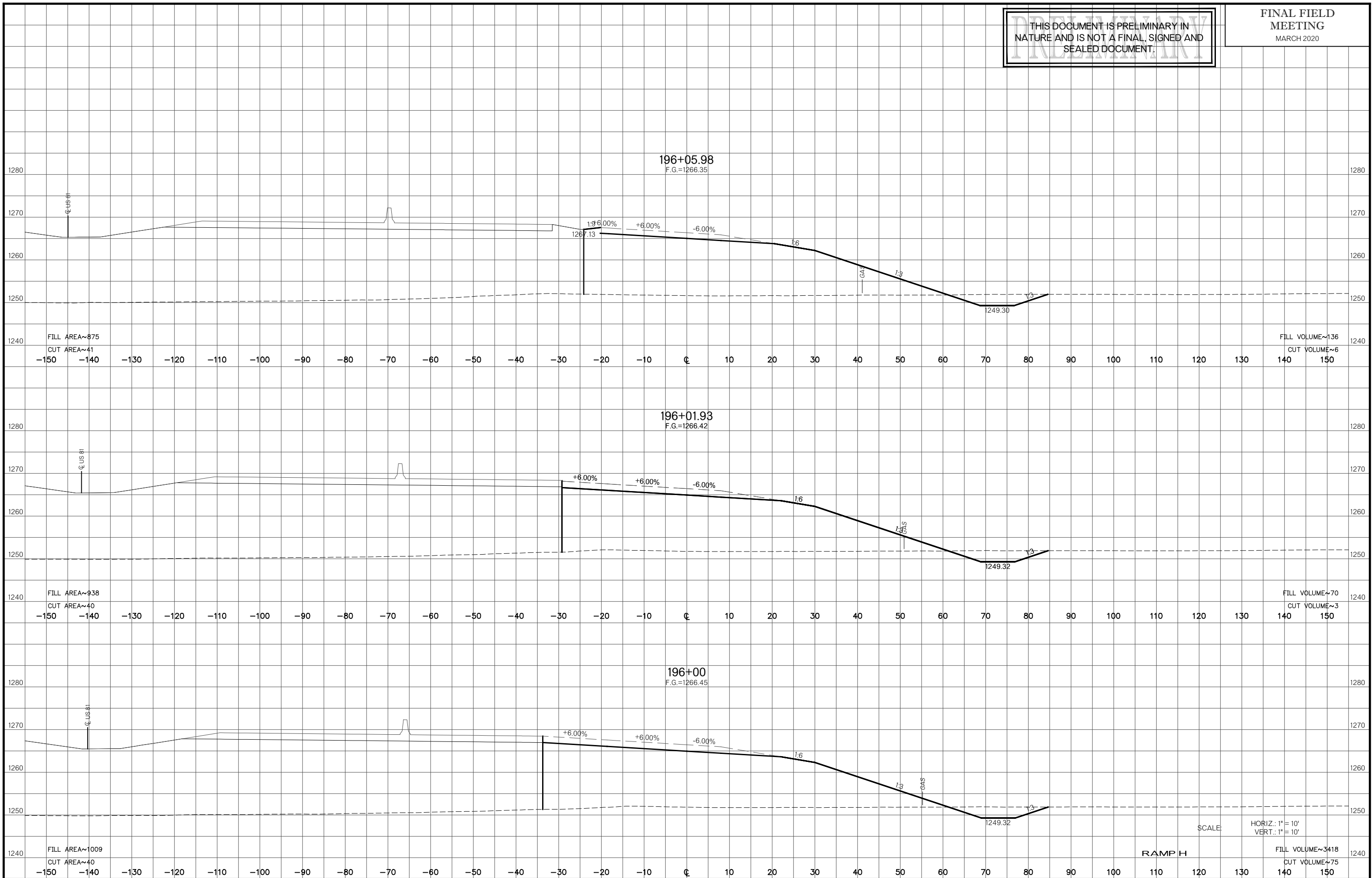
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



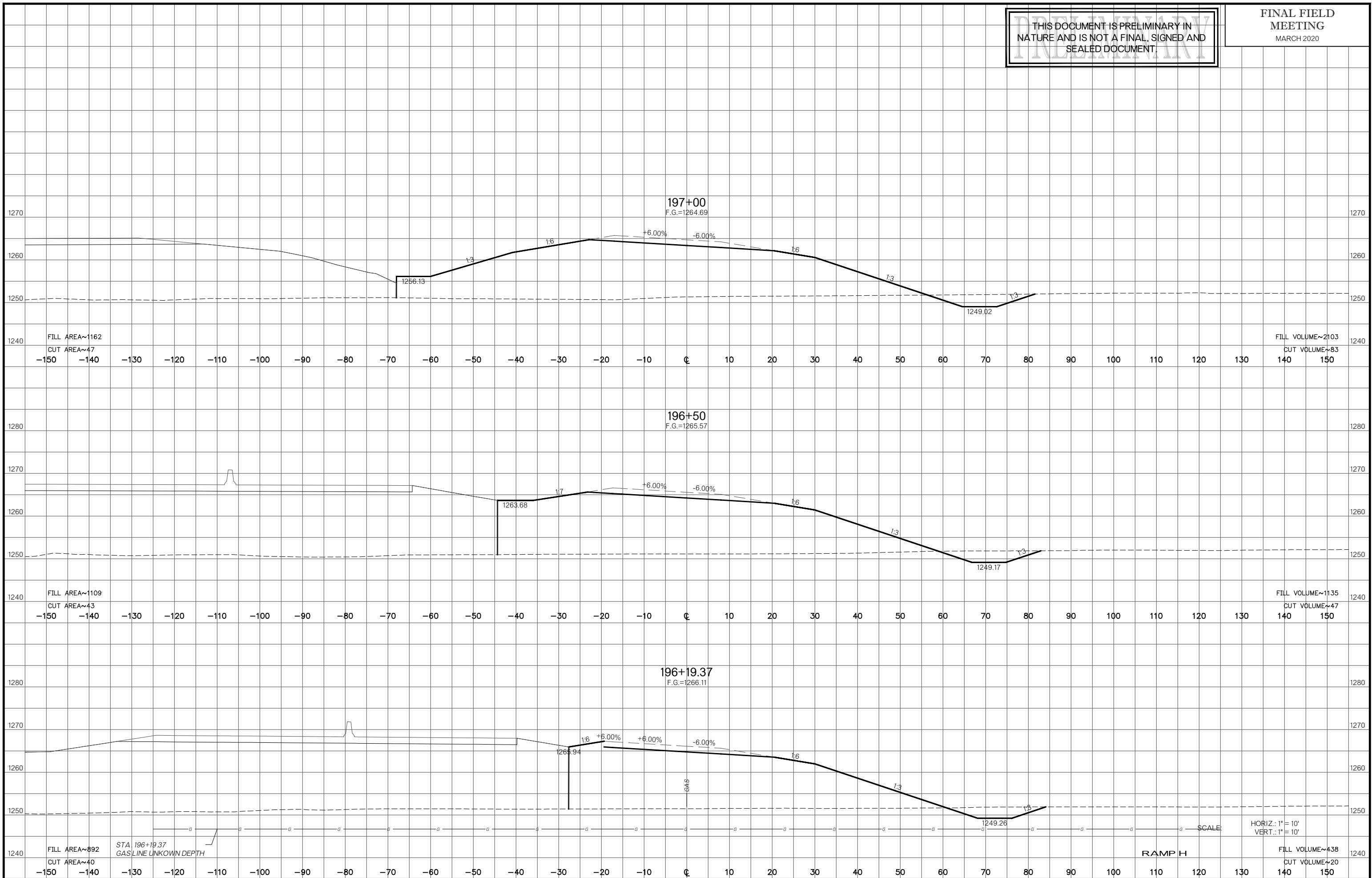
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP H

State Job No. 24428(04) Sheet No. X279

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

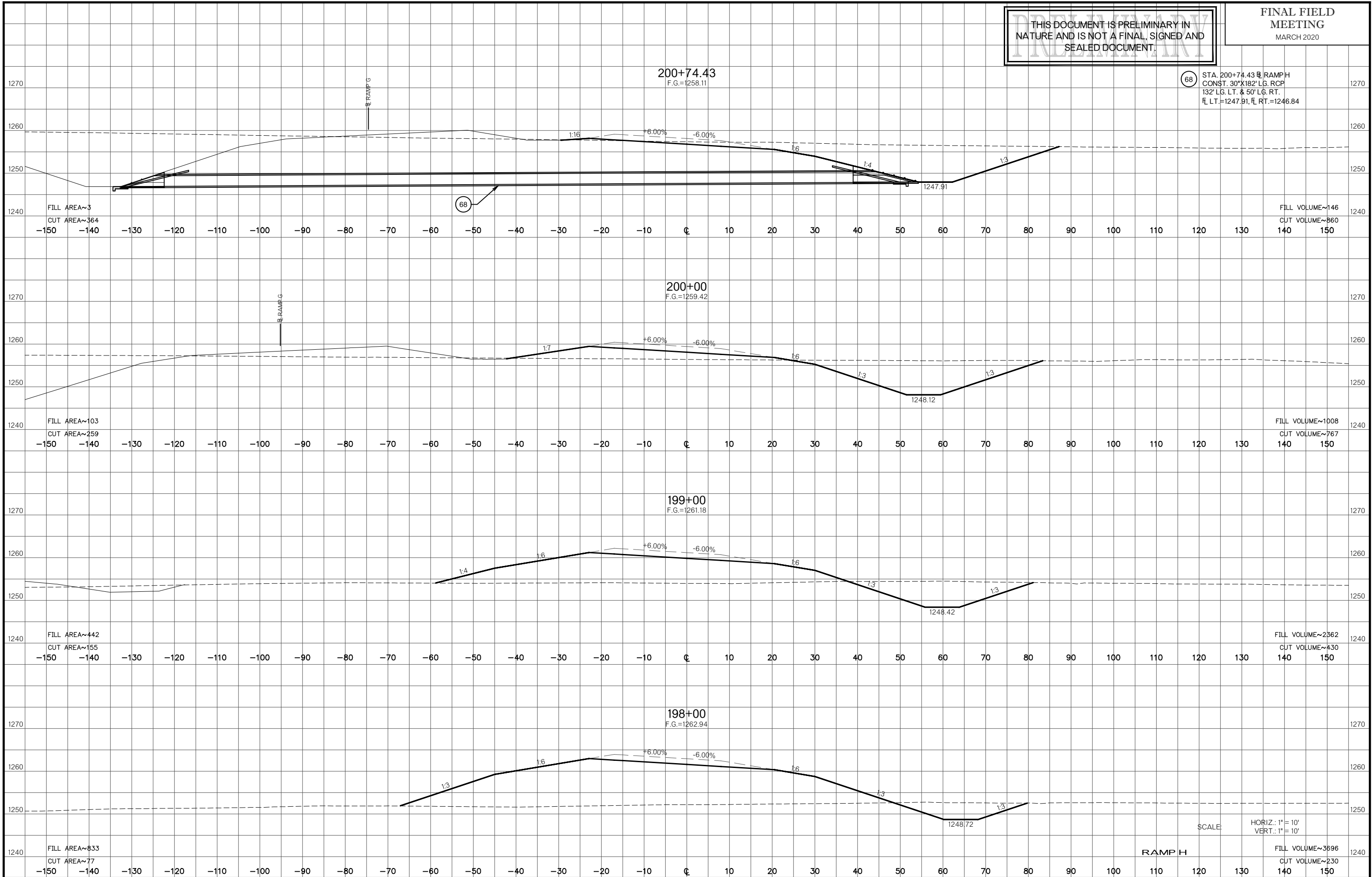


SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP H
FILL VOLUME~438
CUT VOLUME~20

US 81 REALIGNMENT
GRADY COUNTY

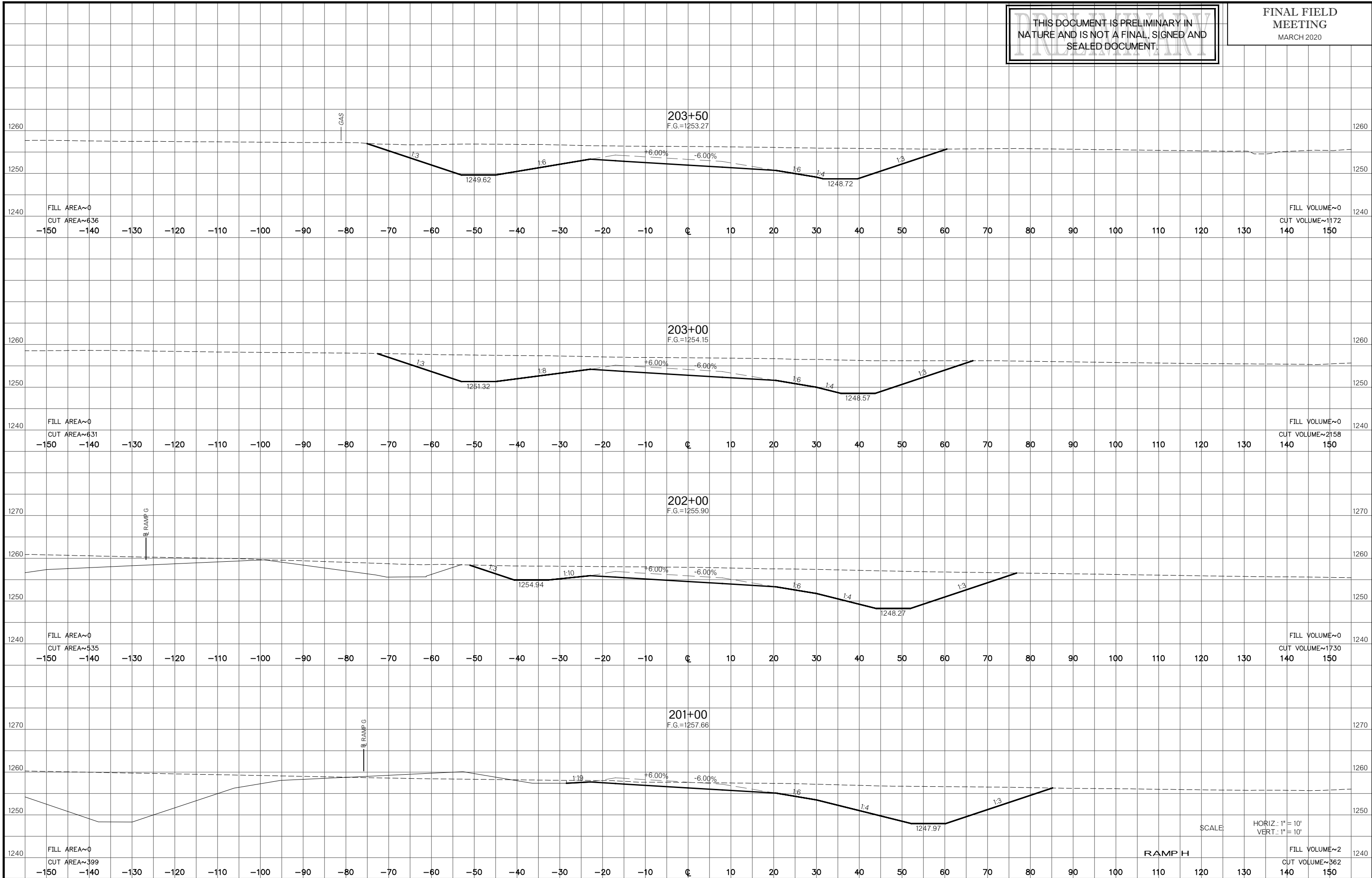
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



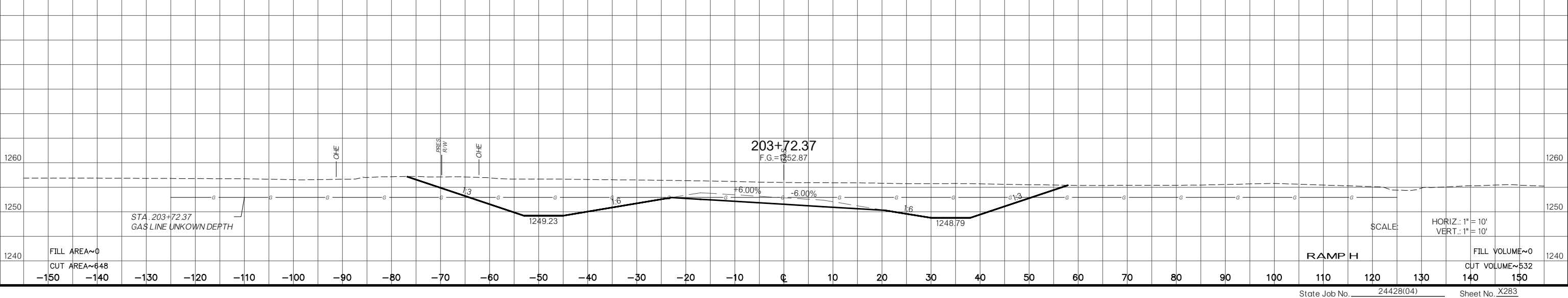
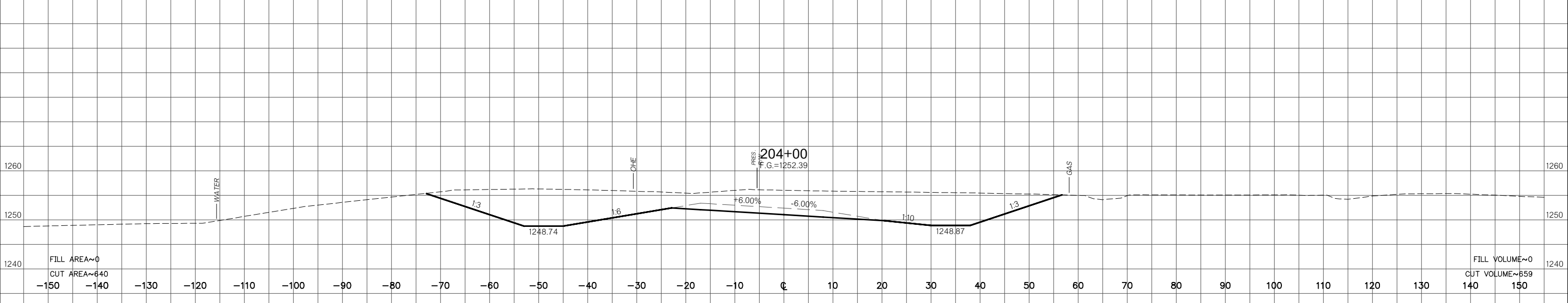
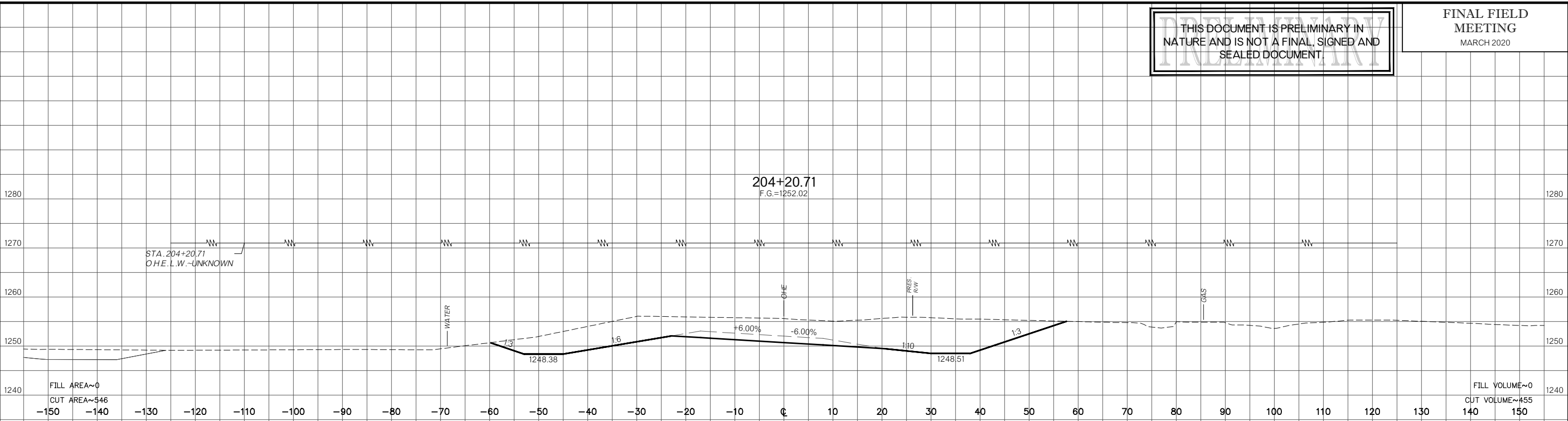
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP H
FILL VOLUME~2
CUT VOLUME~362

State Job No. 24428(04) Sheet No. X282

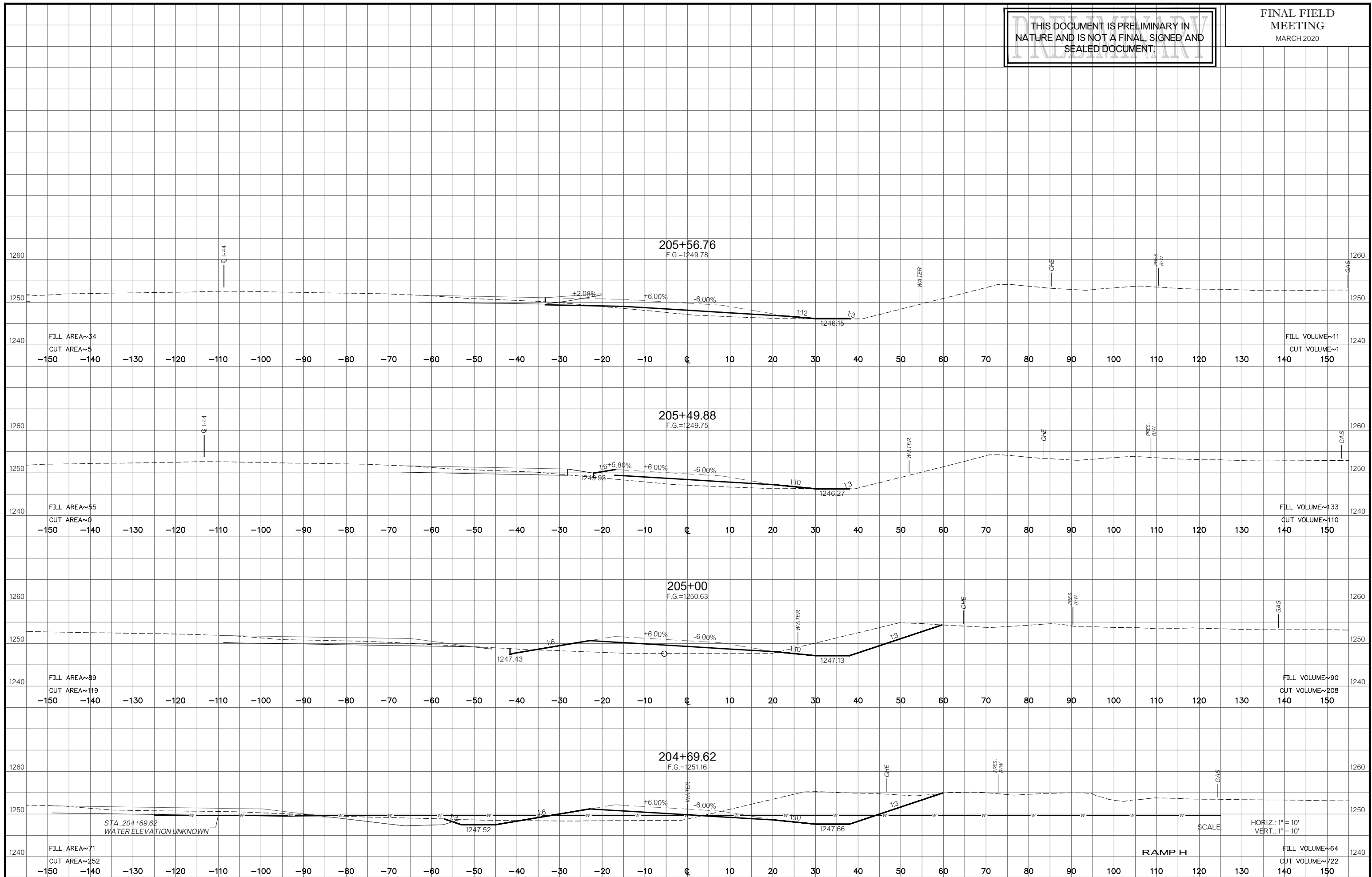
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



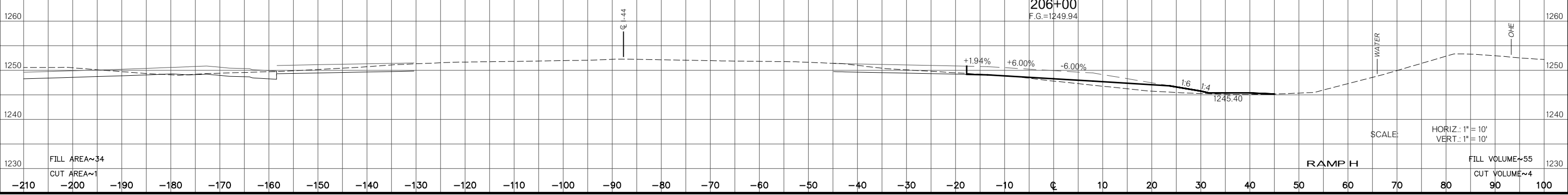
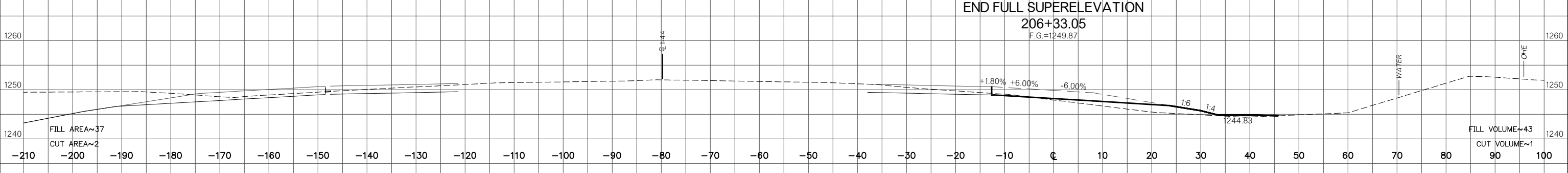
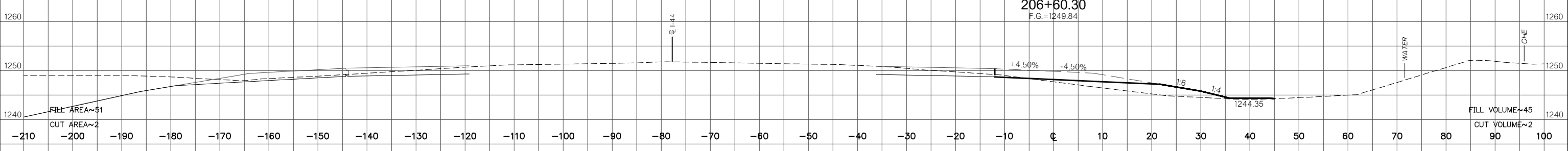
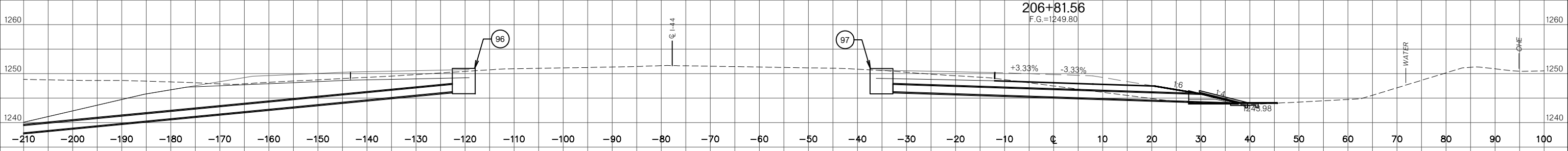
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT GRADY COUNTY

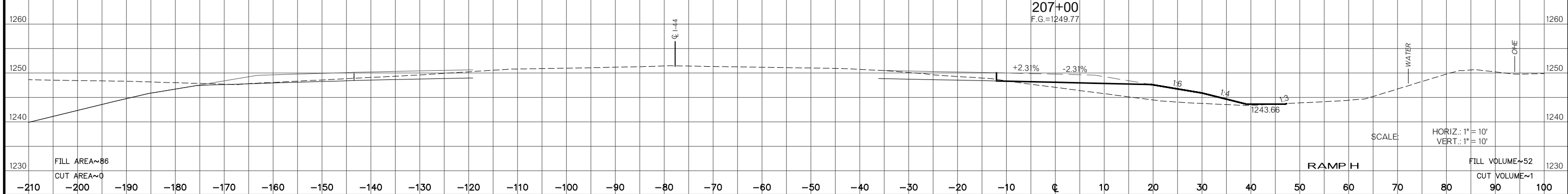
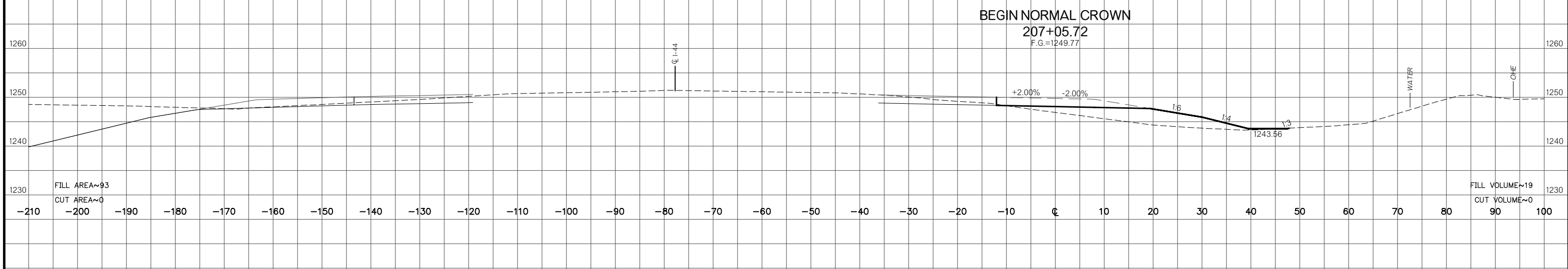
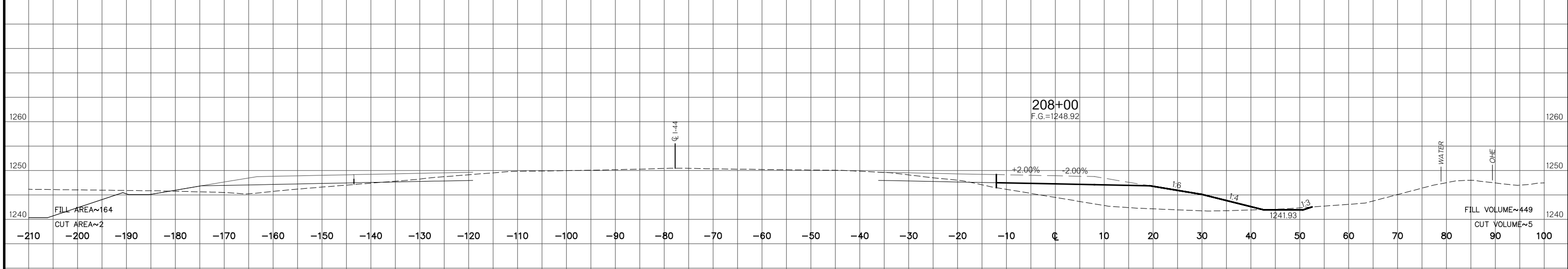
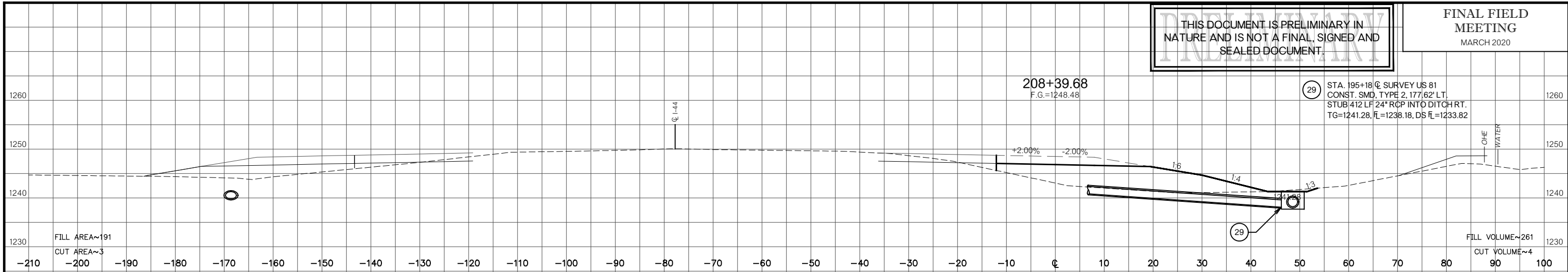
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



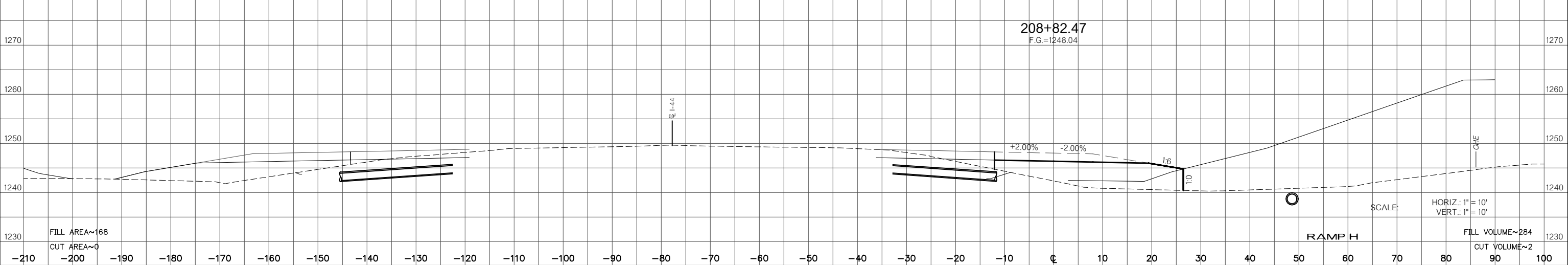
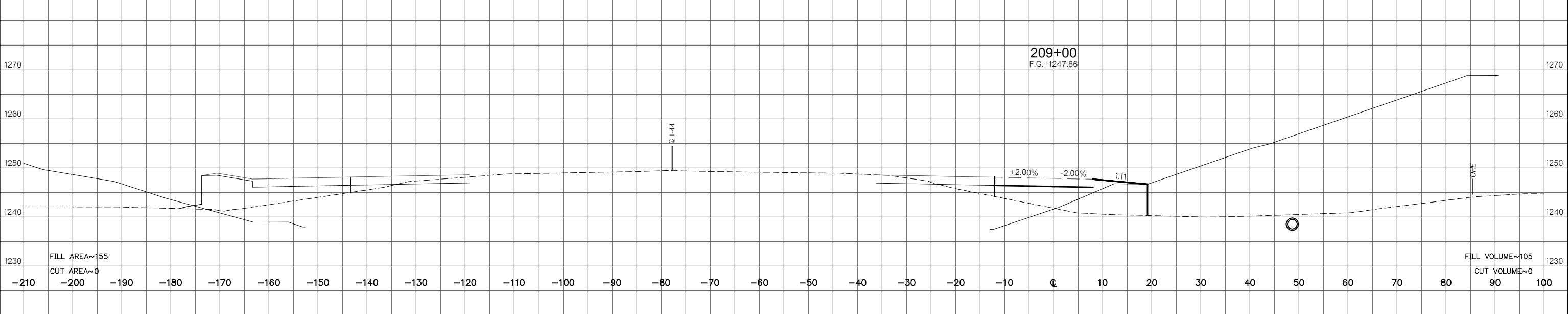
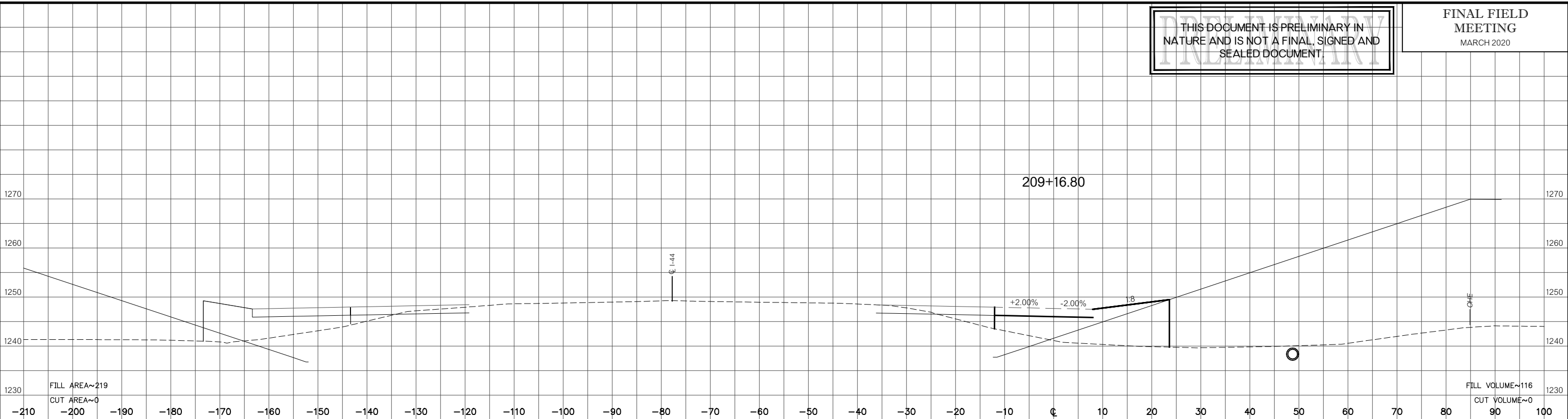
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP H

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

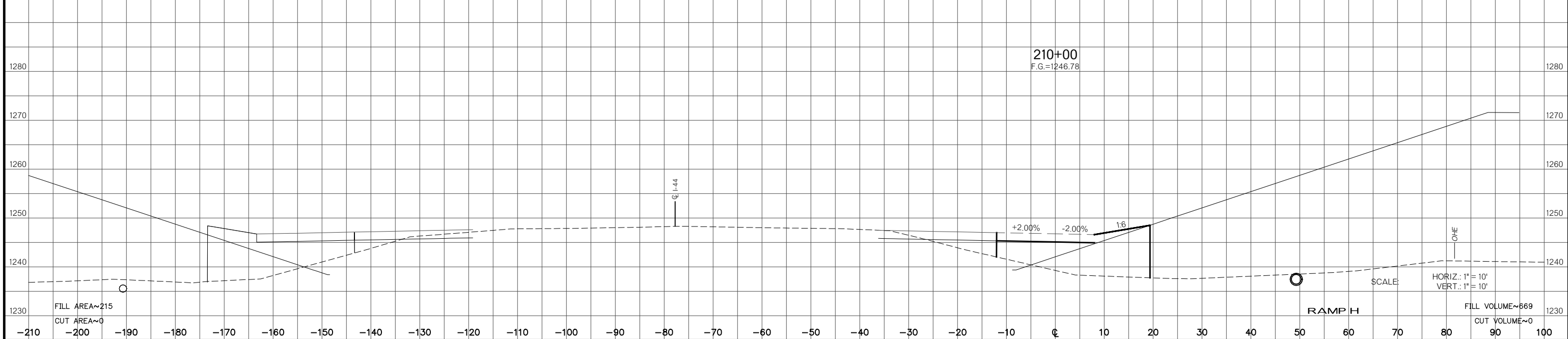
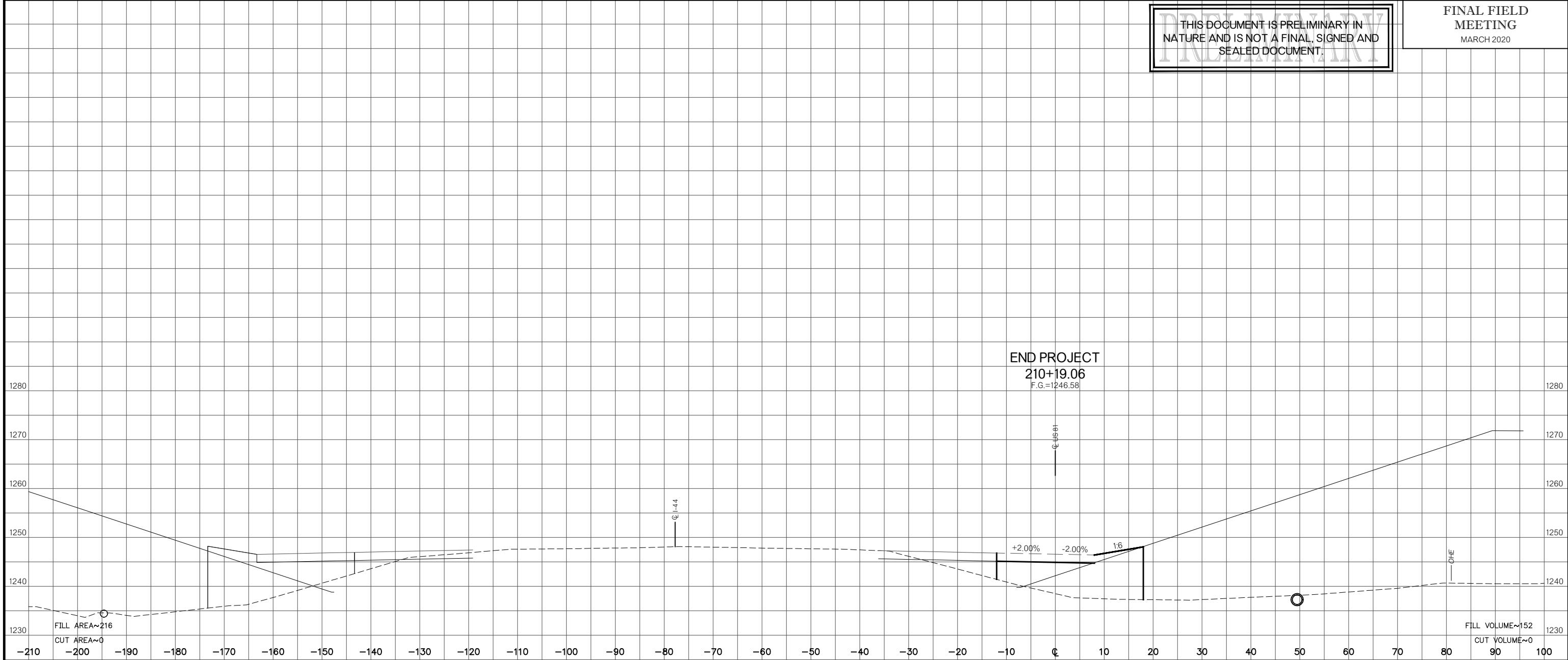
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



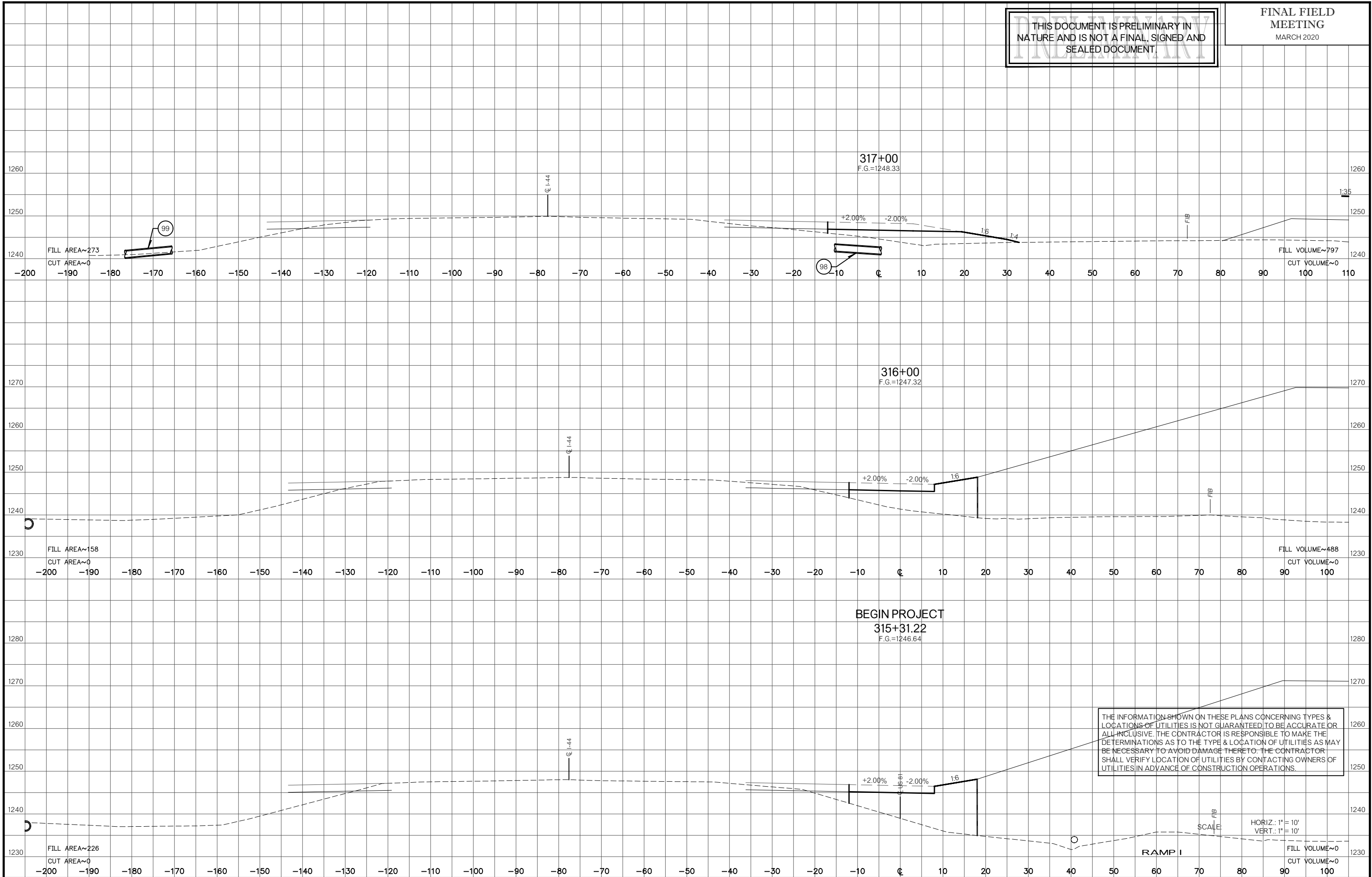
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP H

State Job No. 24428(04) Sheet No. X288

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL-INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

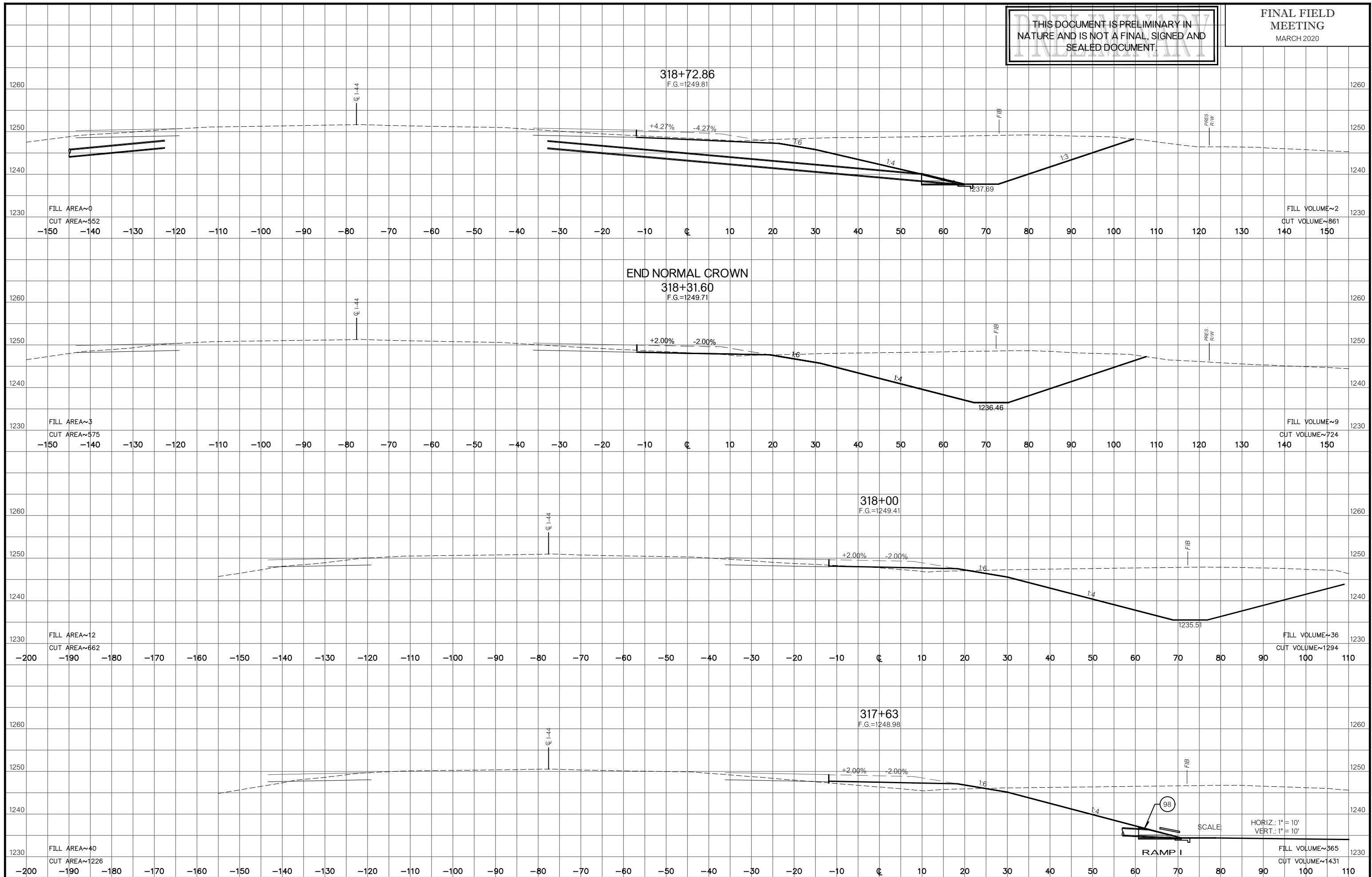
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP I

US 81 REALIGNMENT
GRADY COUNTY

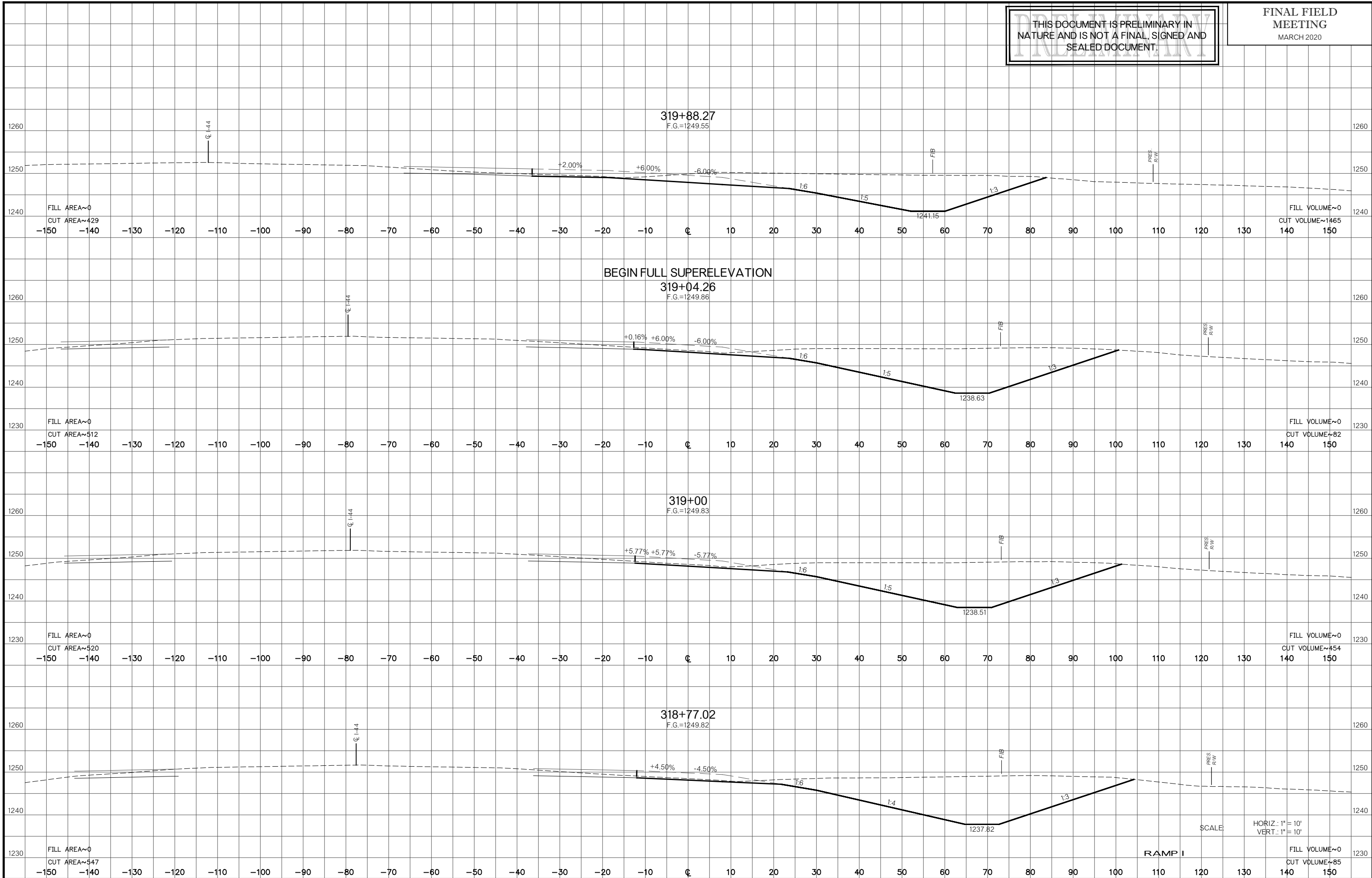
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

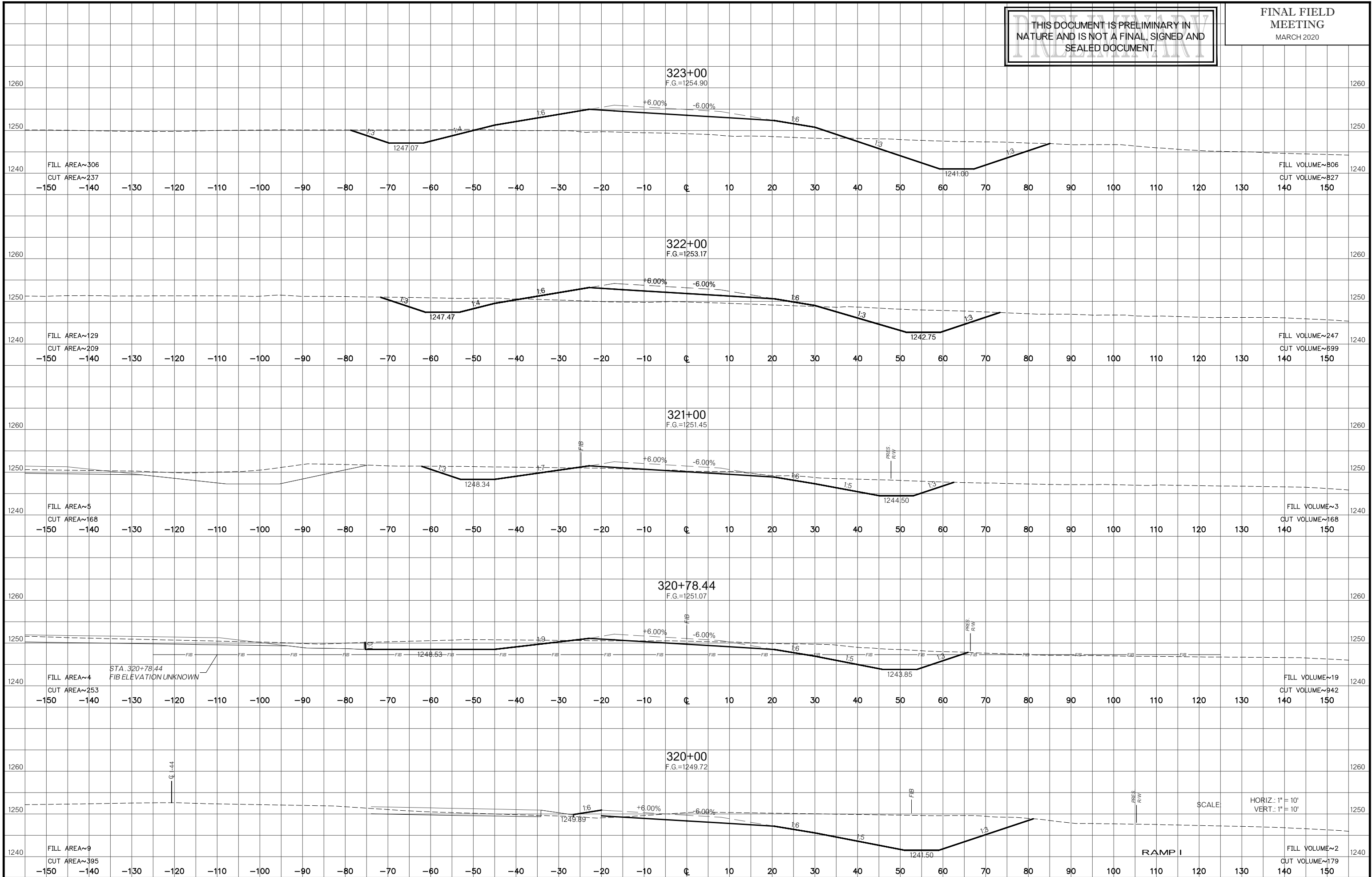
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



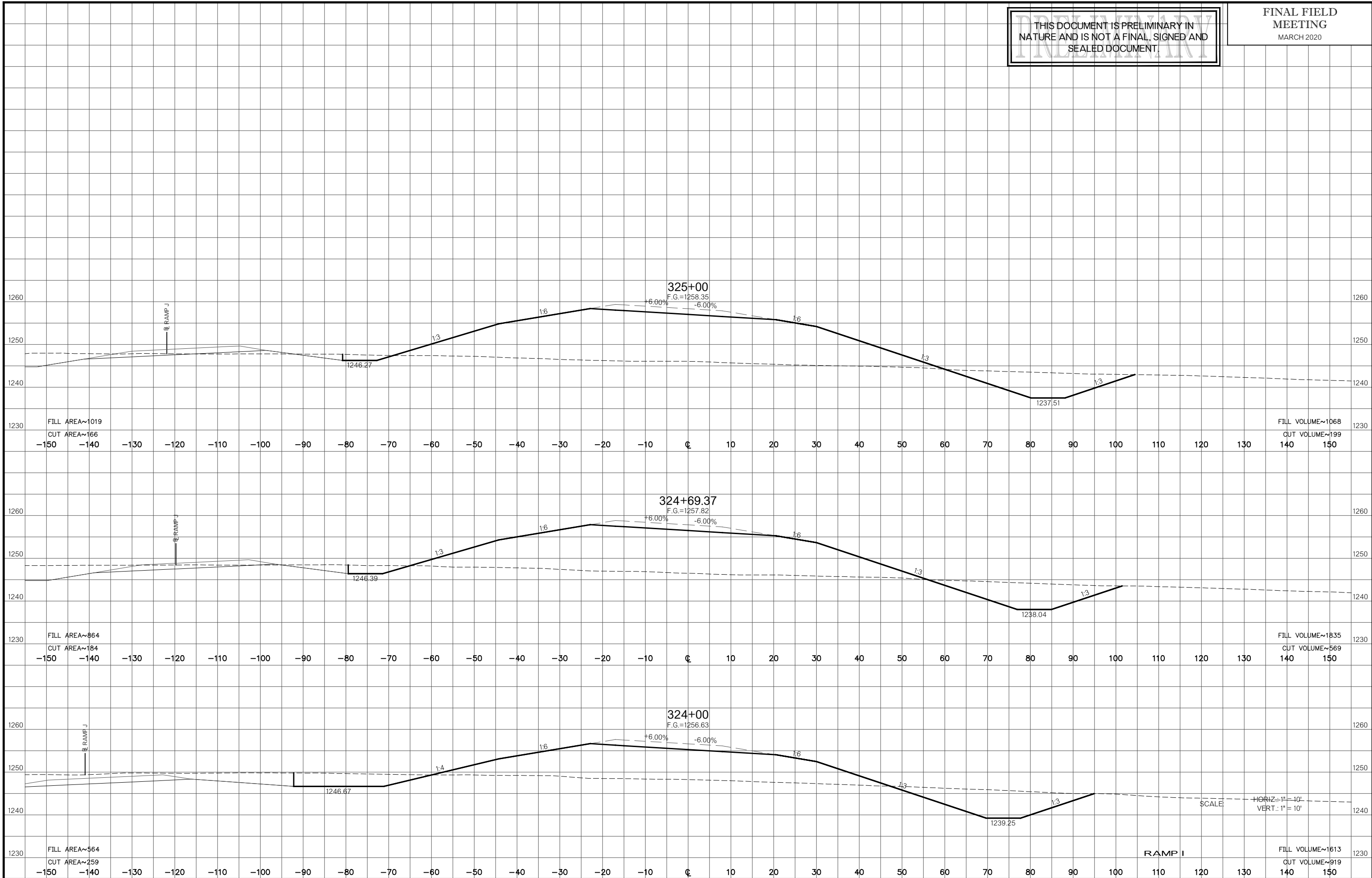
STA. 320+78.44
FIB ELEVATION UNKNOWN

SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP I

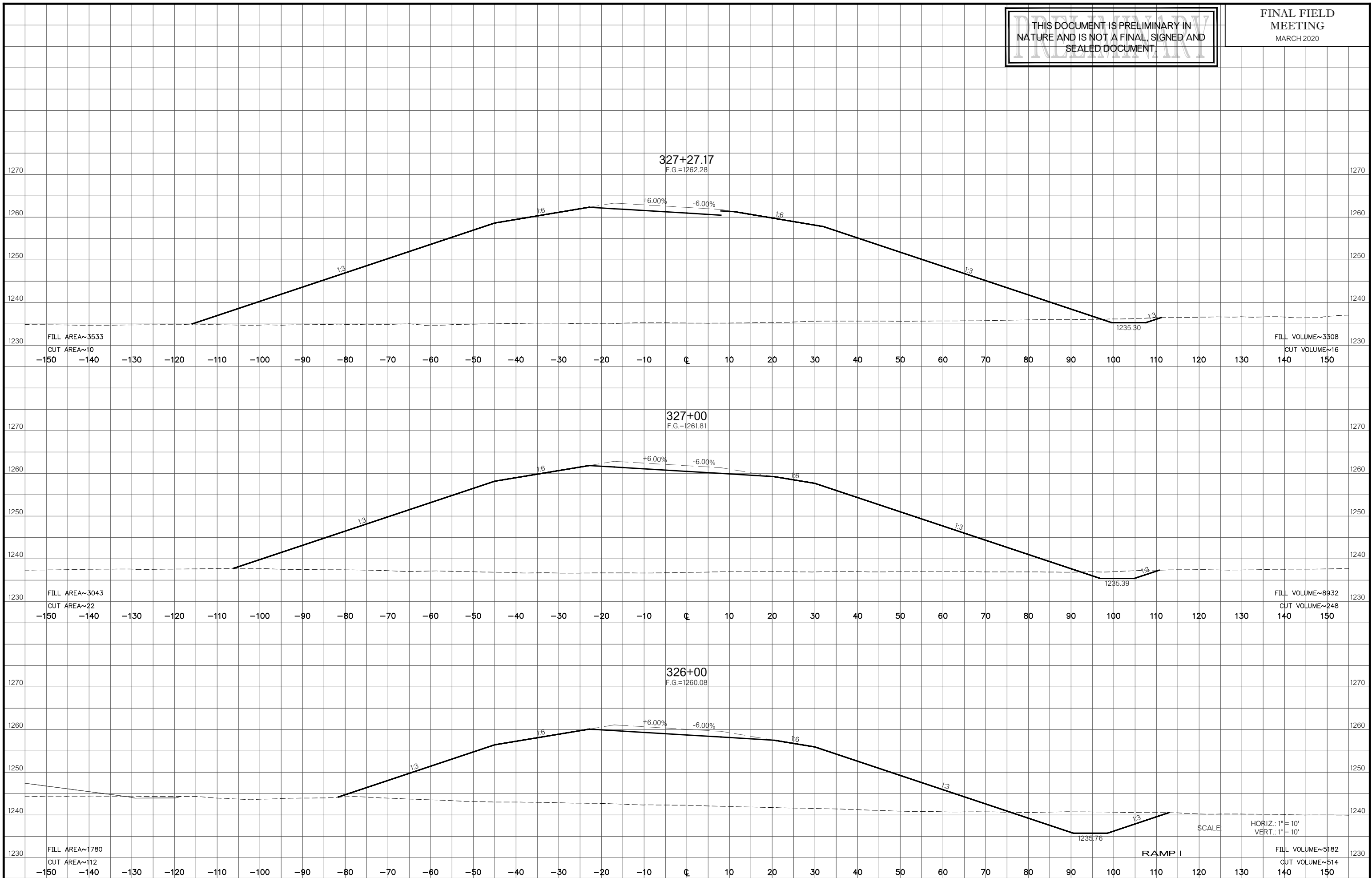
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

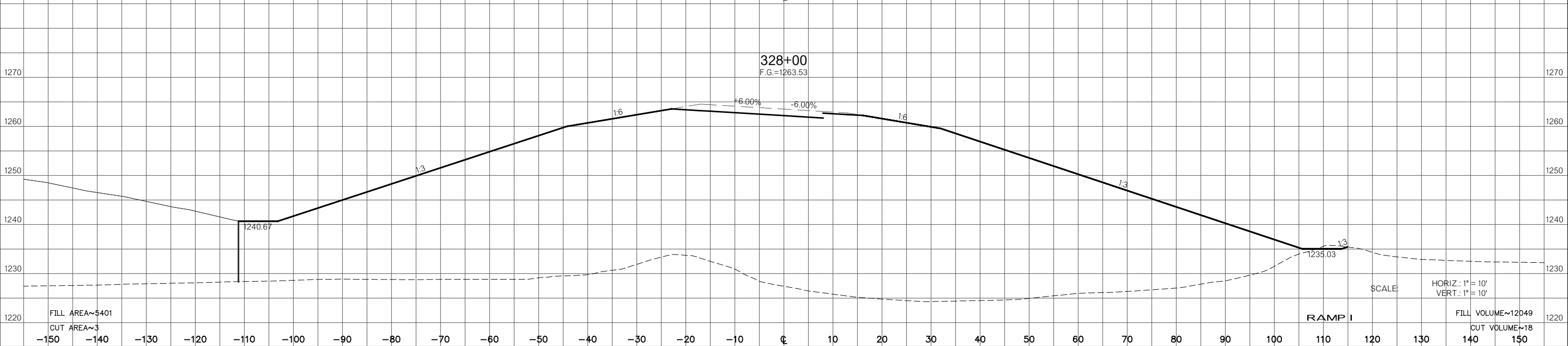
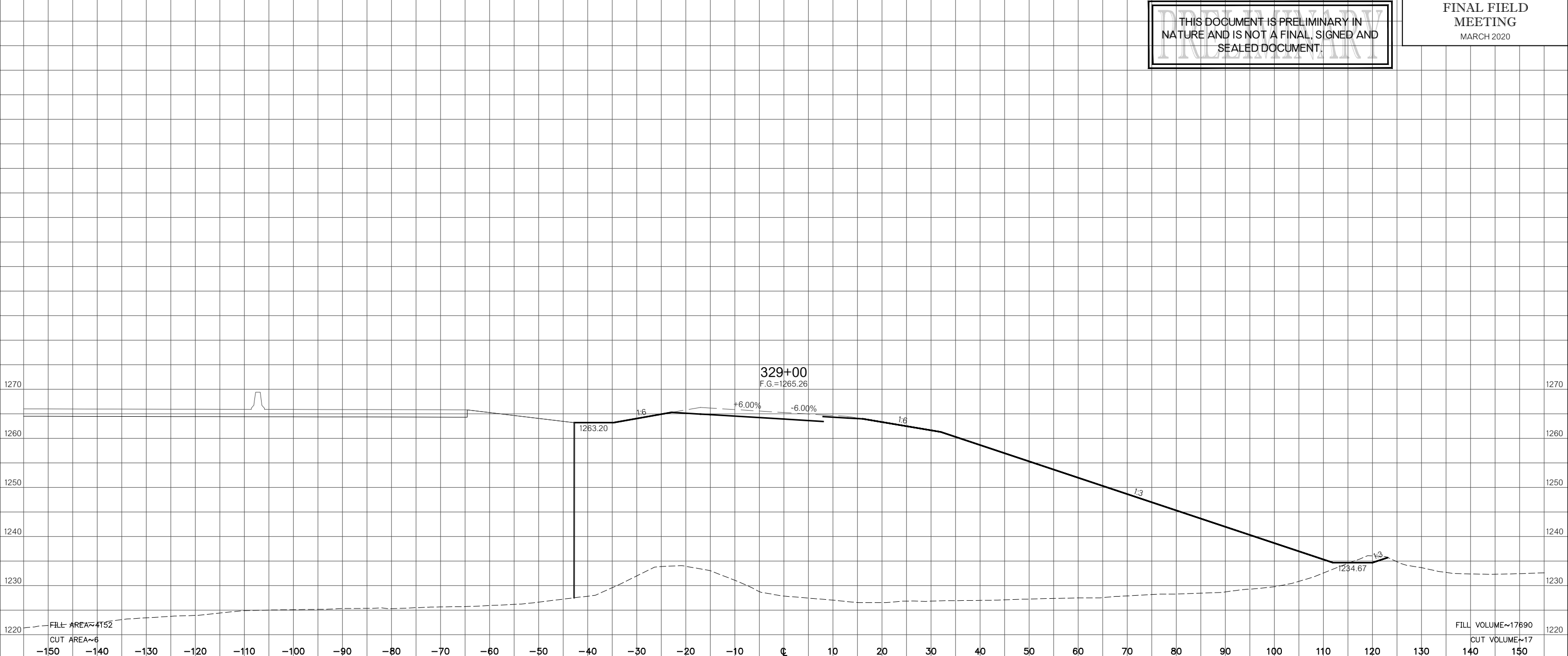
RAMP 1

State Job No. 24428(04) Sheet No. X294

US 81 REALIGNMENT
GRADY COUNTY

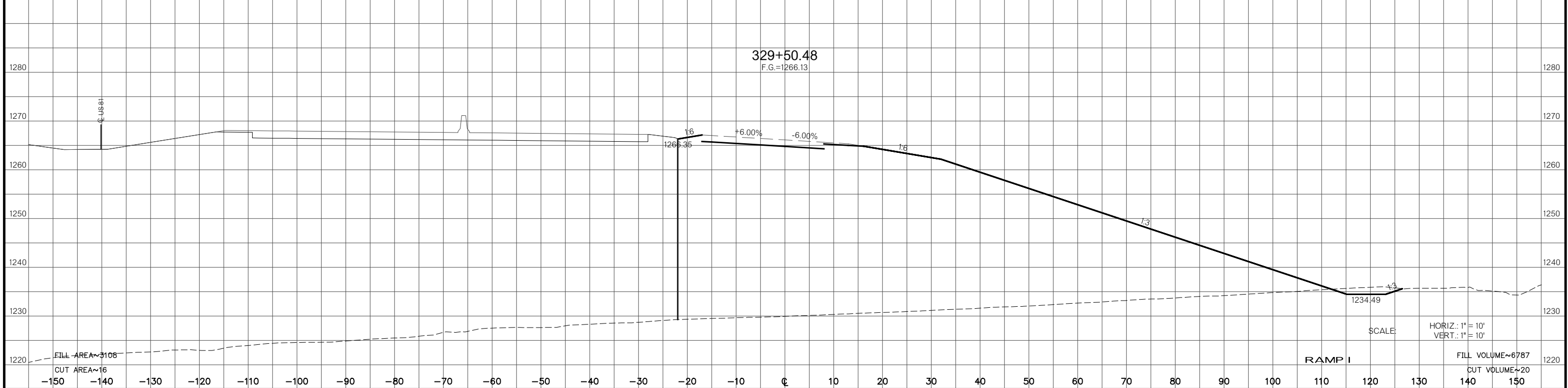
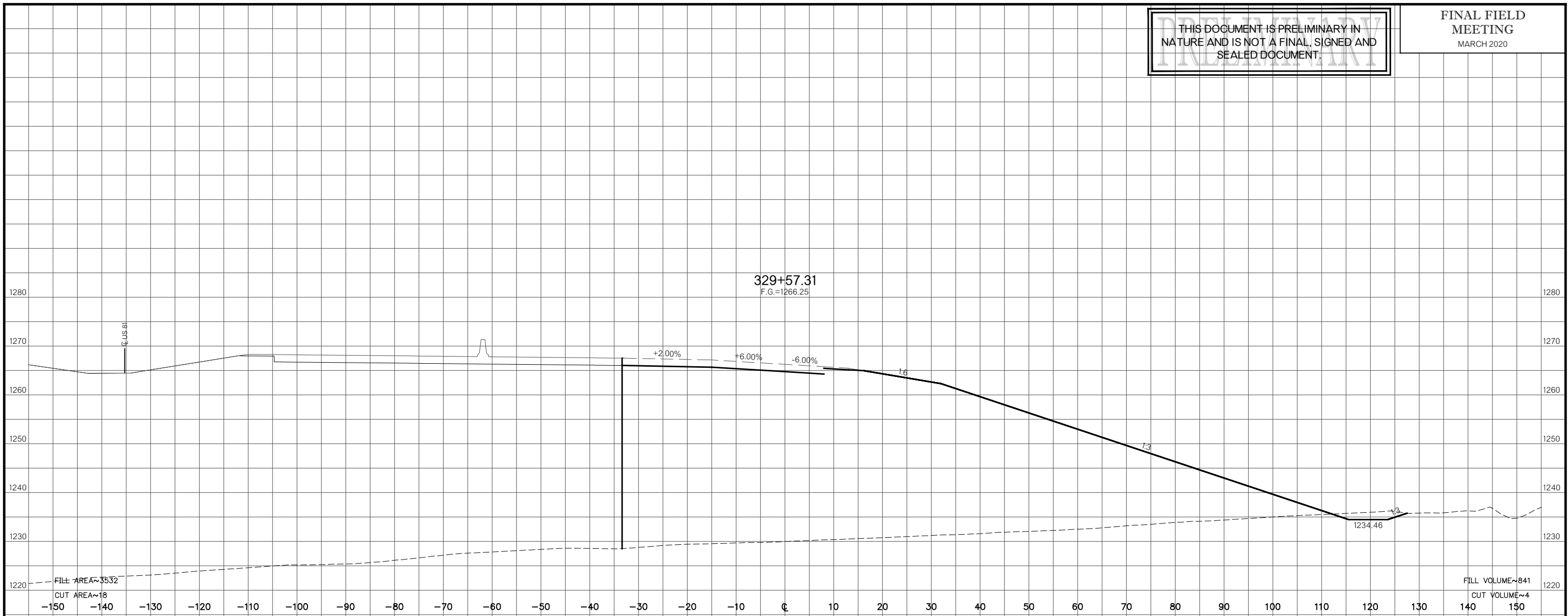
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

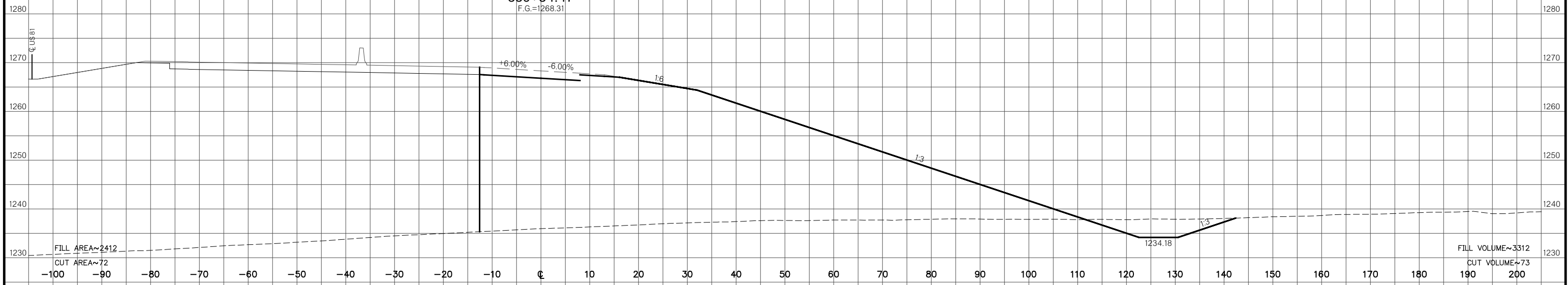
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



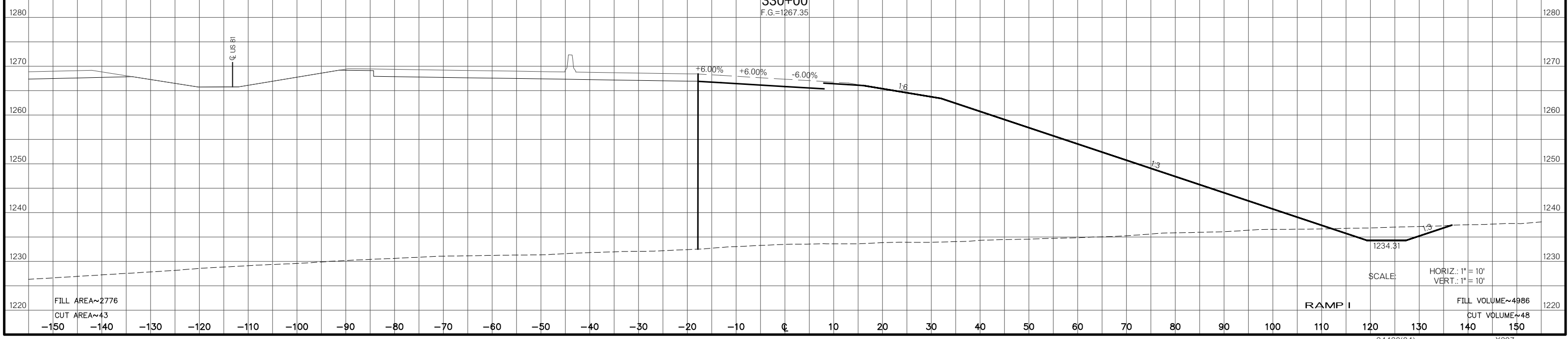
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

END FULL SUPERELEVATION
330+34.47
F.G.=1268.31



330+00
F.G.=1267.35



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP I
FILL VOLUME~4986
CUT VOLUME~48

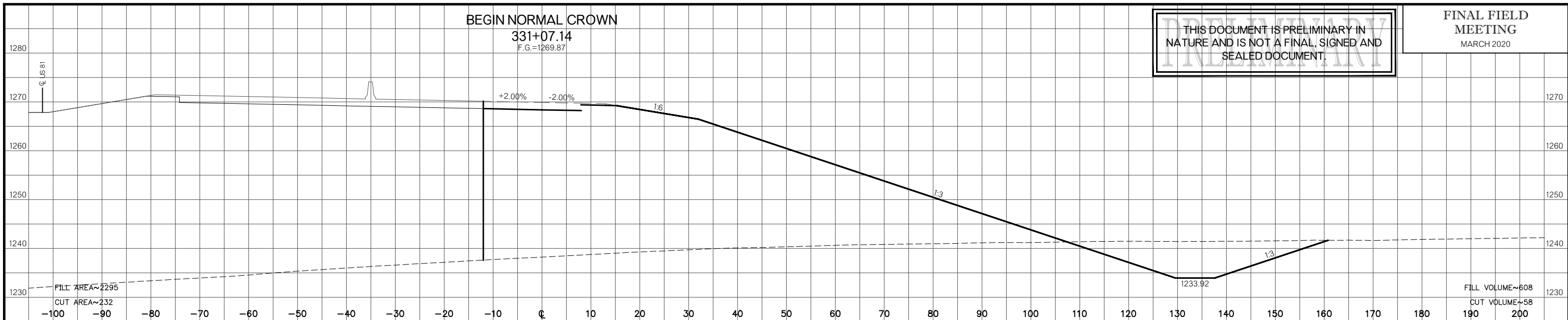
GRADY COUNTY
US 81 REALIGNMENT

BEGIN NORMAL CROWN

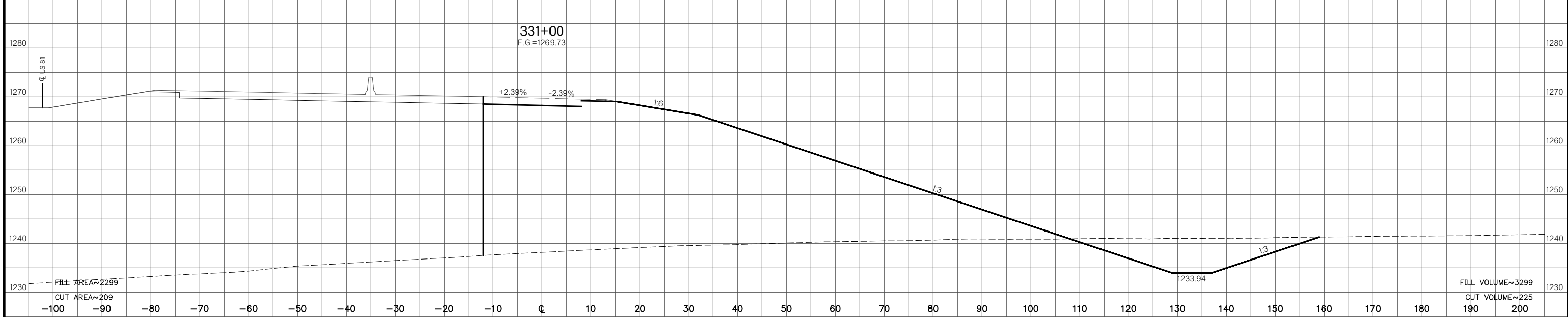
331+07.14
F.G.=1269.87

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

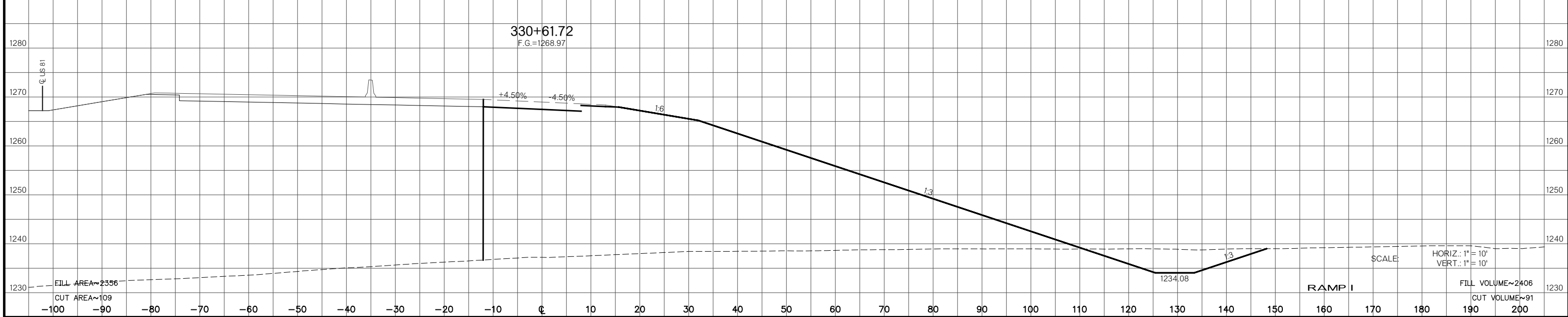
FINAL FIELD MEETING
MARCH 2020



331+00
F.G.=1269.73



330+61.72
F.G.=1268.97



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'
RAMP I
State Job No. 24428(04) Sheet No. X298

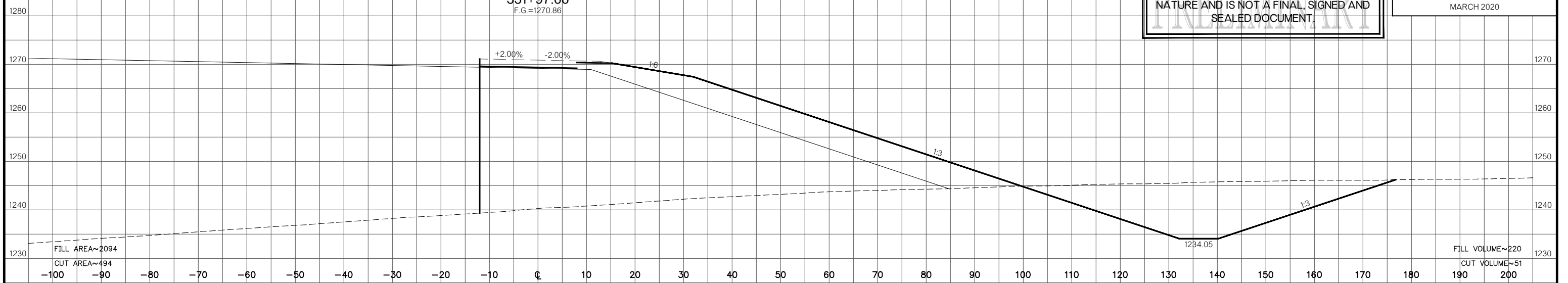
US 81 REALIGNMENT
GRADY COUNTY

END INCIDENTAL CONSTRUCTION

331+97.66
F.G.=1270.86

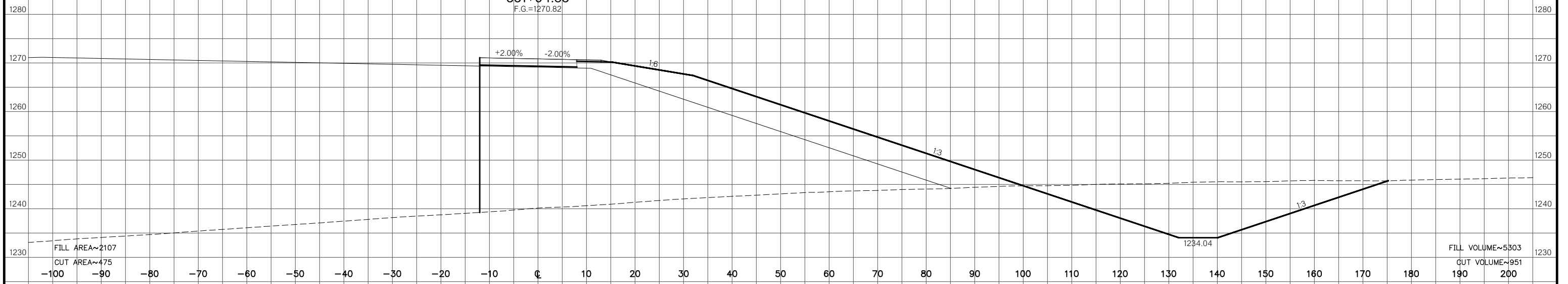
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

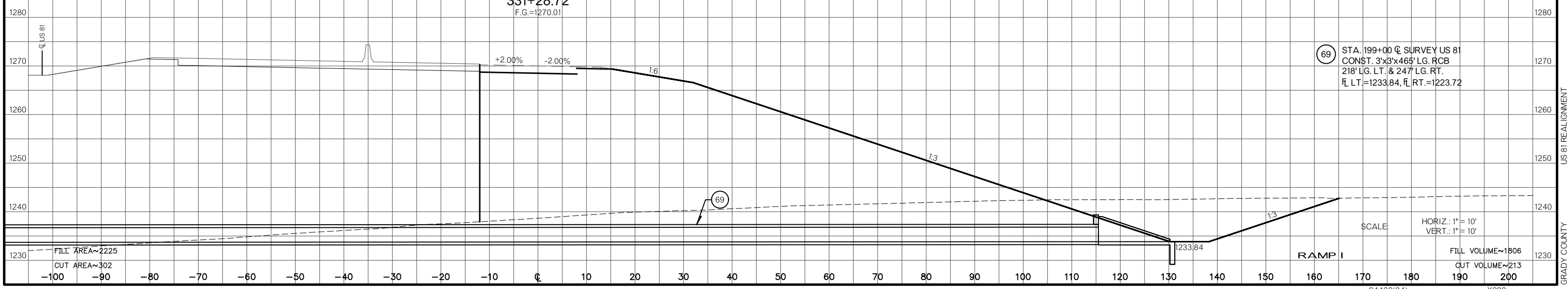


END PROJECT

331+94.83
F.G.=1270.82



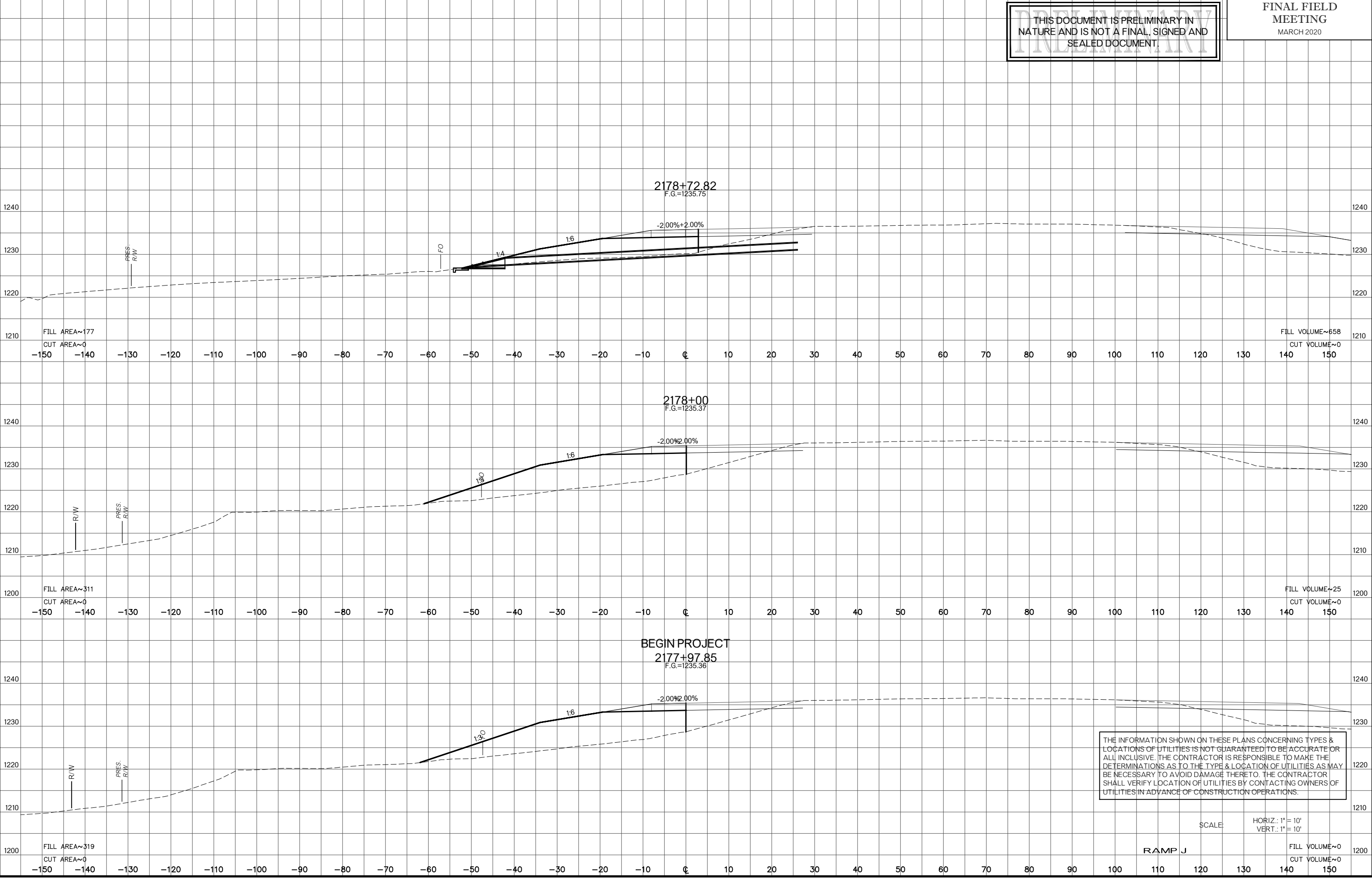
331+28.72
F.G.=1270.01



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

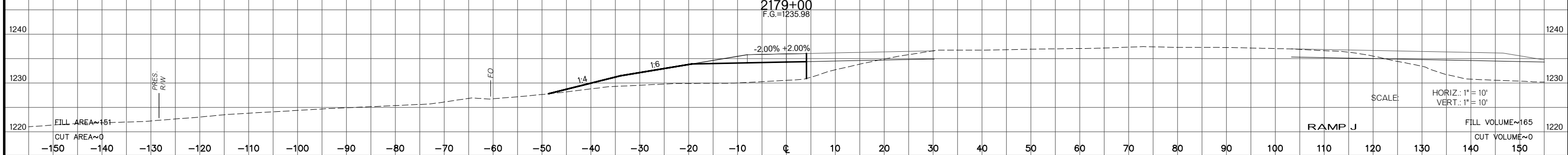
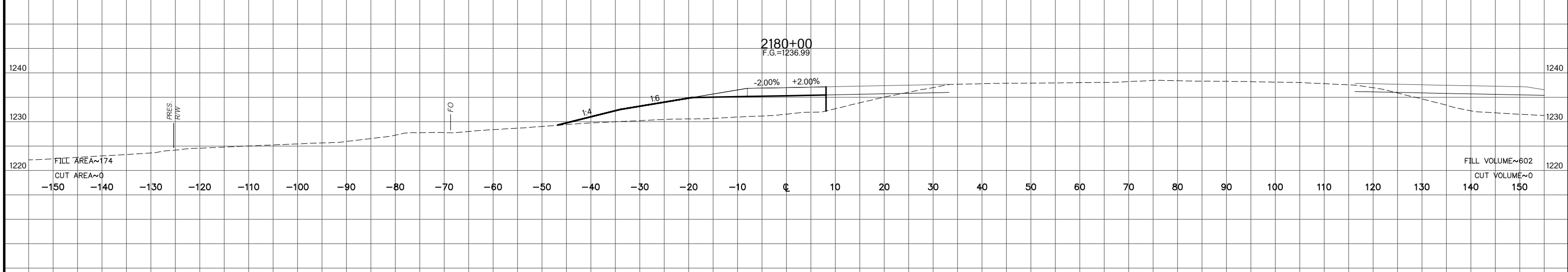
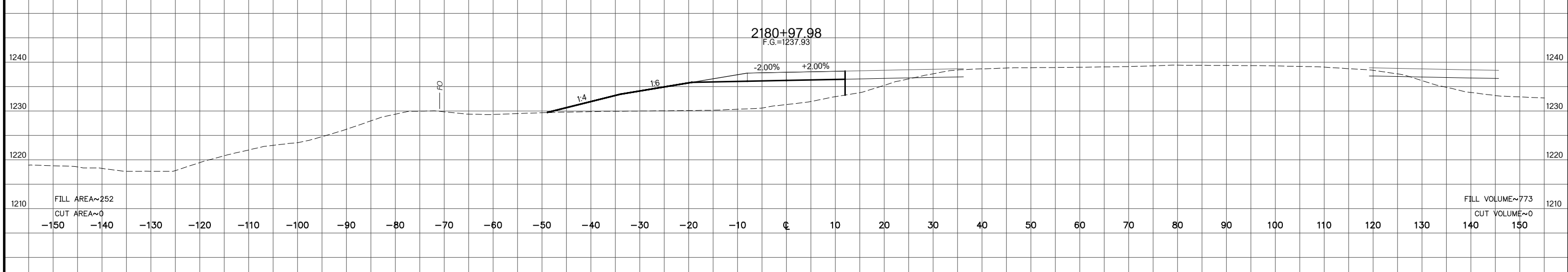
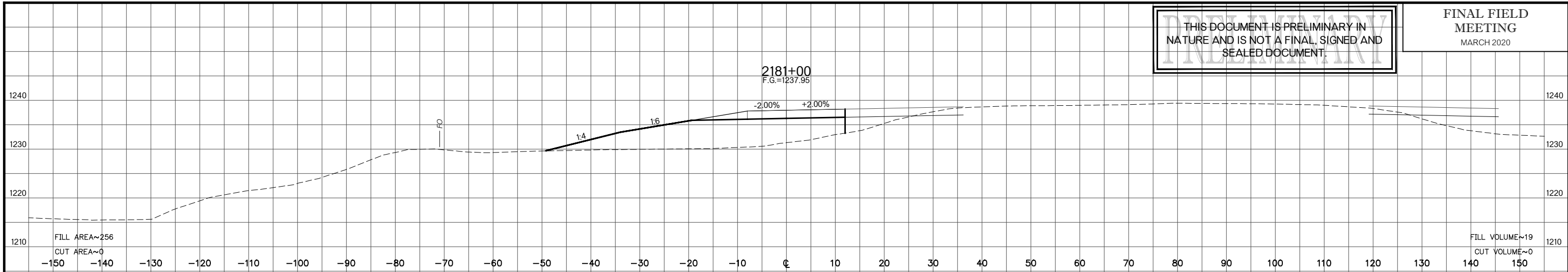


THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

US 81 REALIGNMENT
GRADY COUNTY

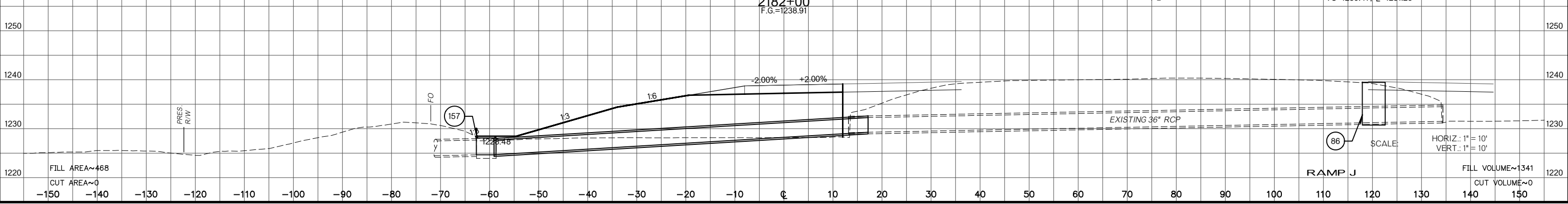
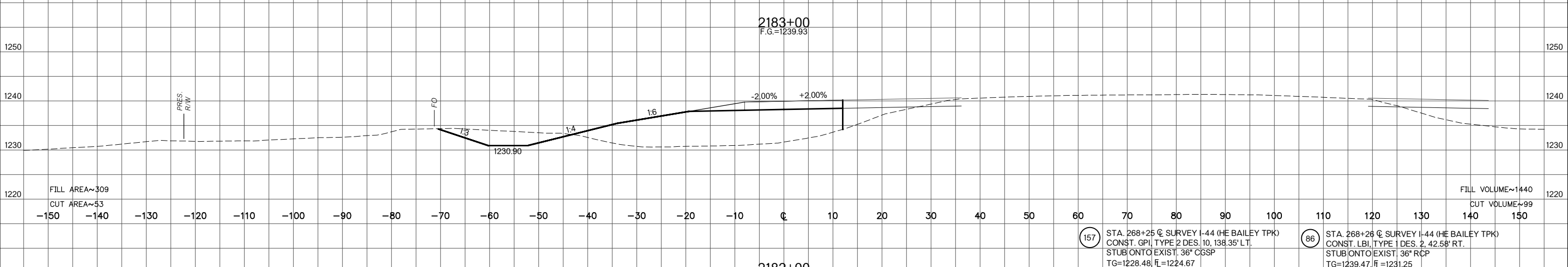
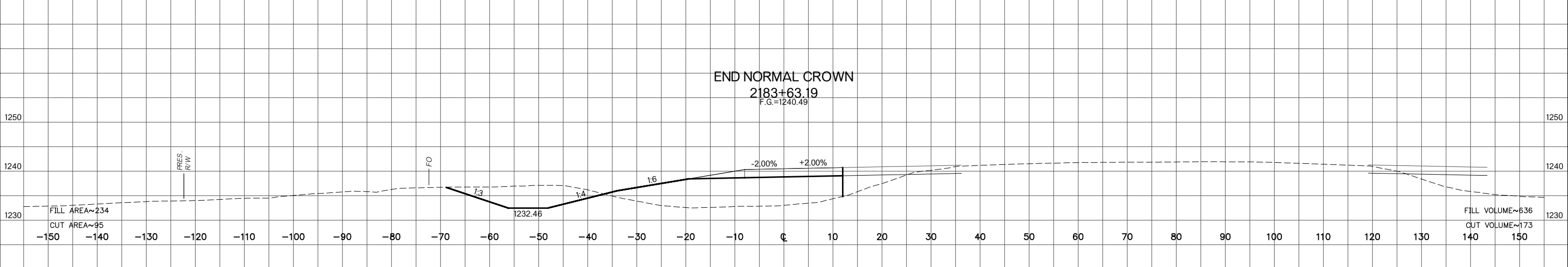
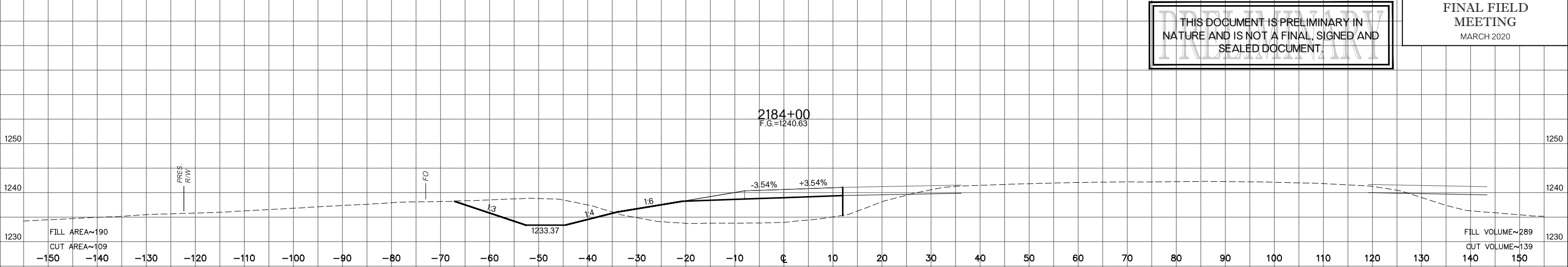
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



157 STA. 268+25 @ SURVEY I-44 (HE BAILEY TPK)
CONST. GPI, TYPE 2 DES. 10, 138.35' LT.
STUB ON TO EXIST. 36" CGSP
TG=1228.48, FL=1224.67

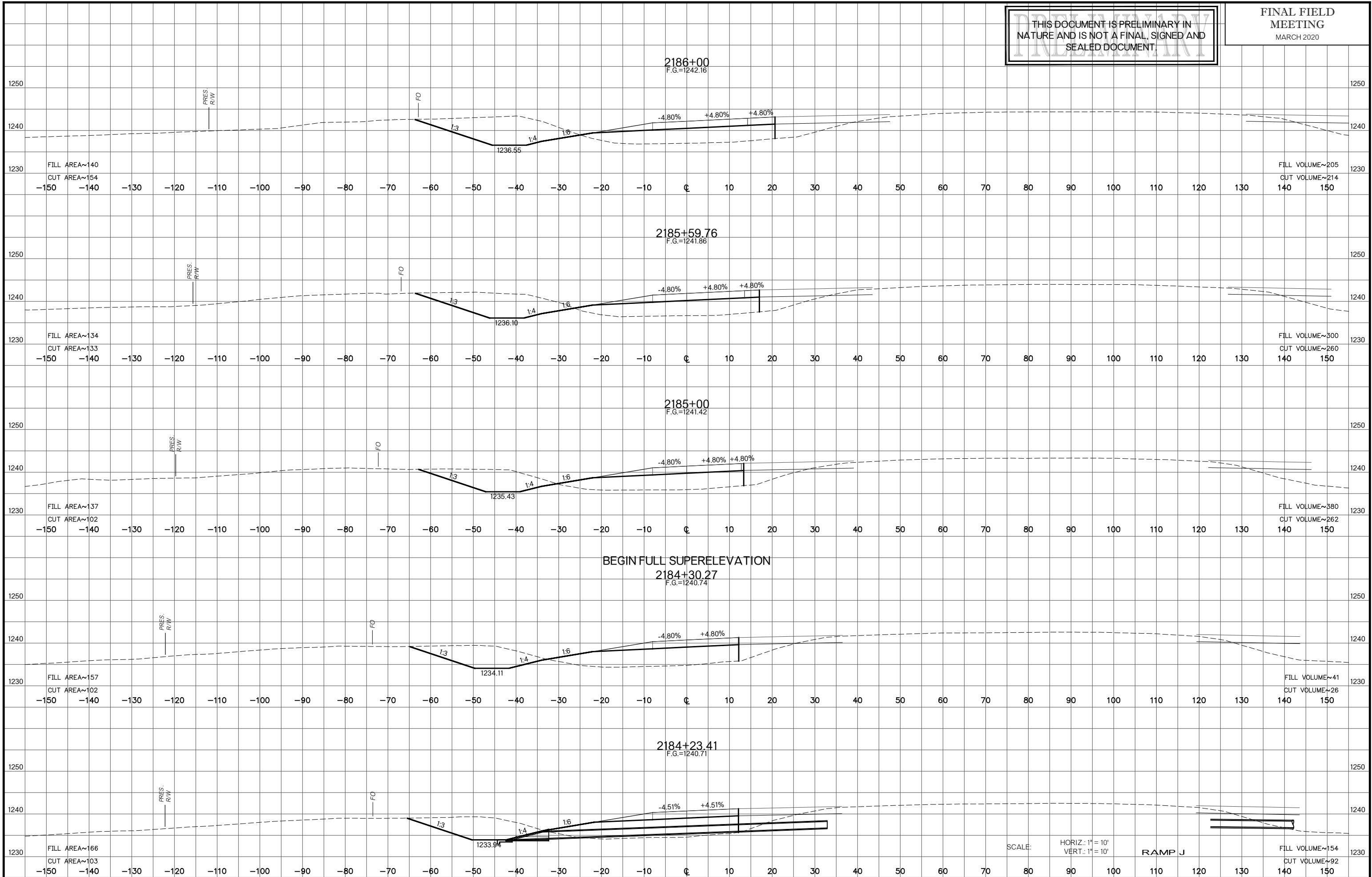
86 STA. 268+26 @ SURVEY I-44 (HE BAILEY TPK)
CONST. LBI, TYPE 1 DES. 2, 42.58' RT.
STUB ON TO EXIST. 36" RCP
TG=1239.47, FL=1231.25

SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP J

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

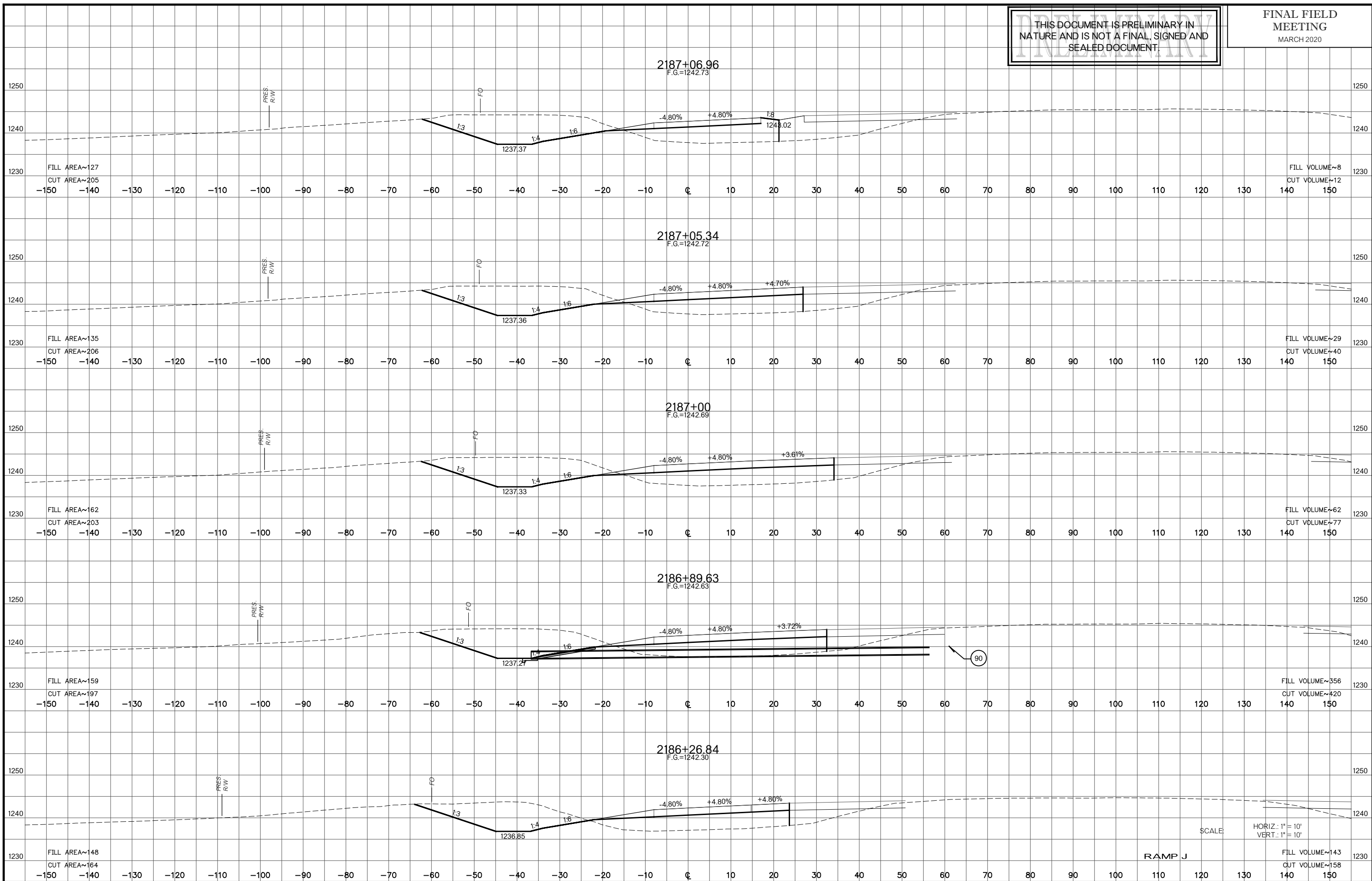


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP J

US 81 REALIGNMENT
GRADY COUNTY

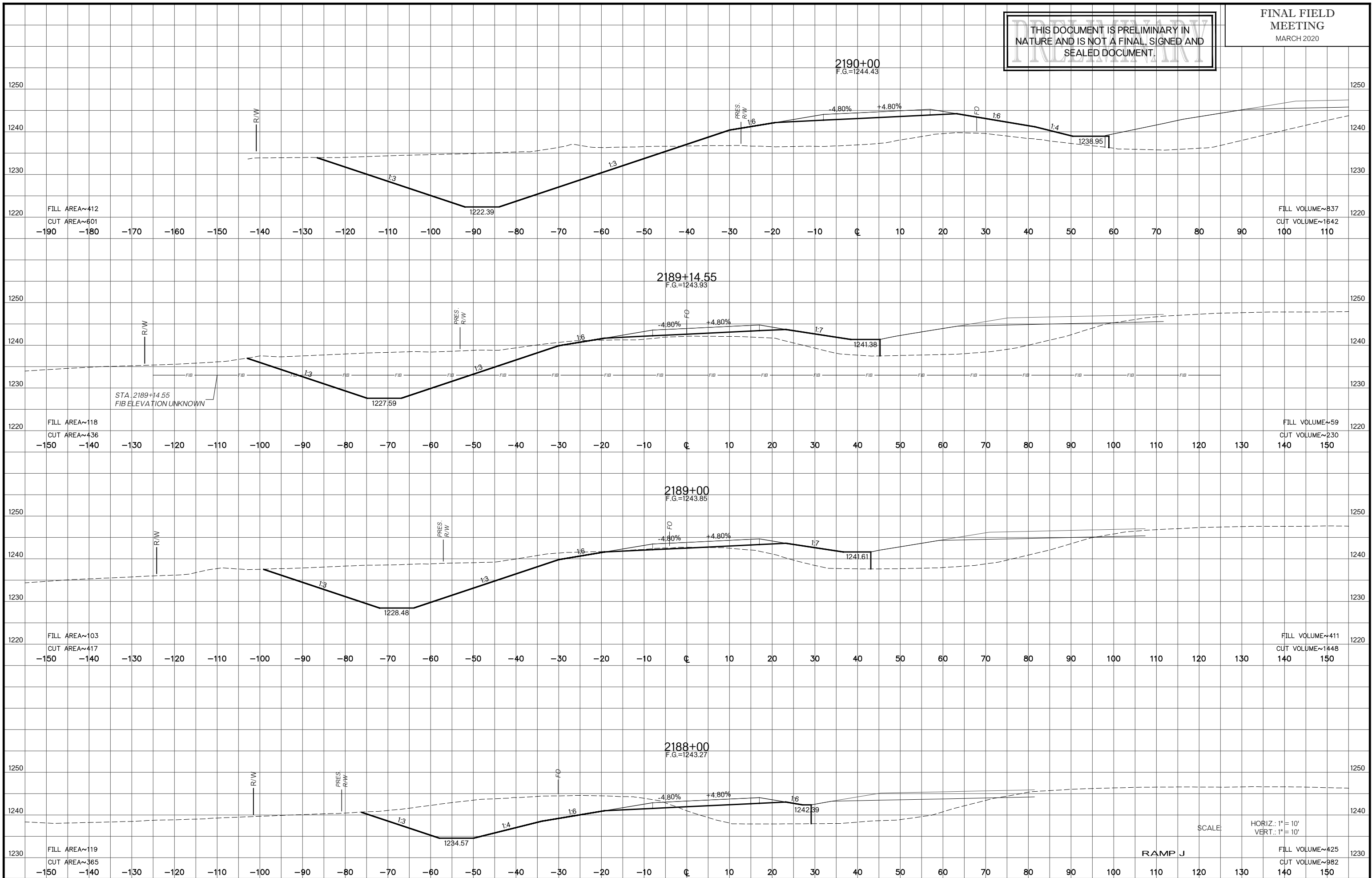
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'
RAMP J
State Job No. 24428(04) Sheet No. X304

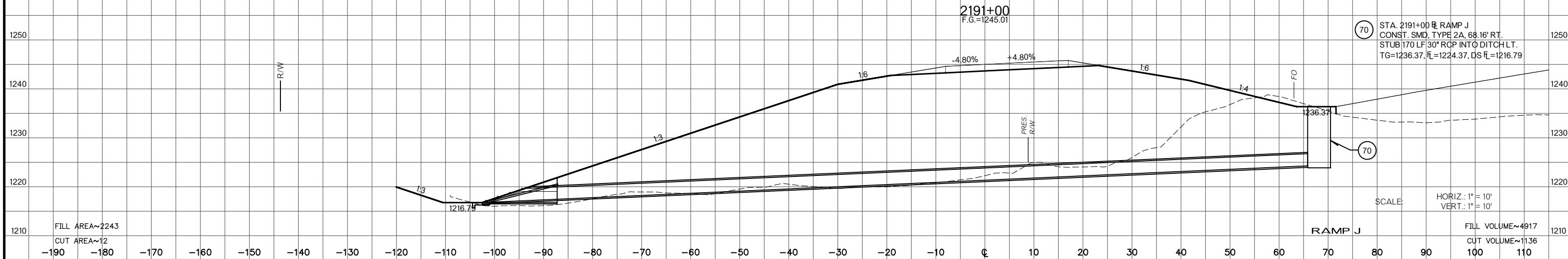
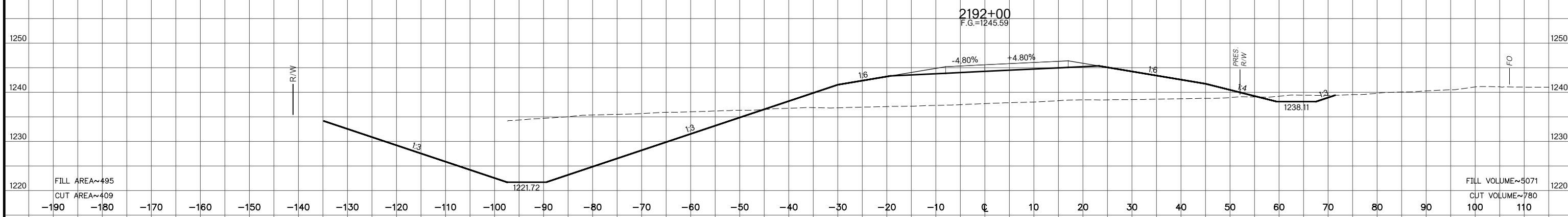
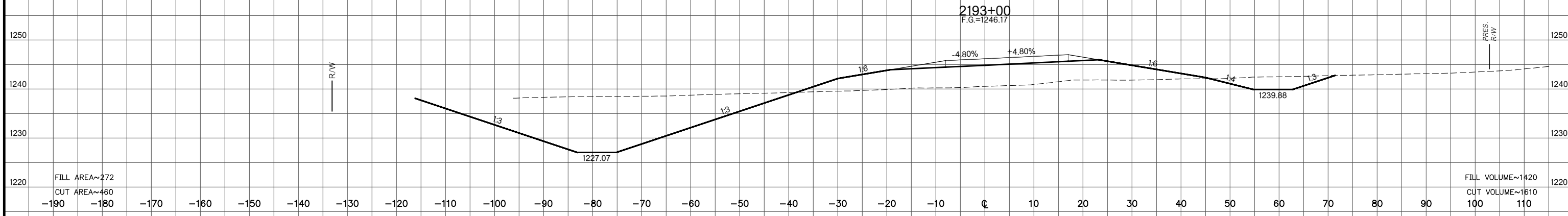
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



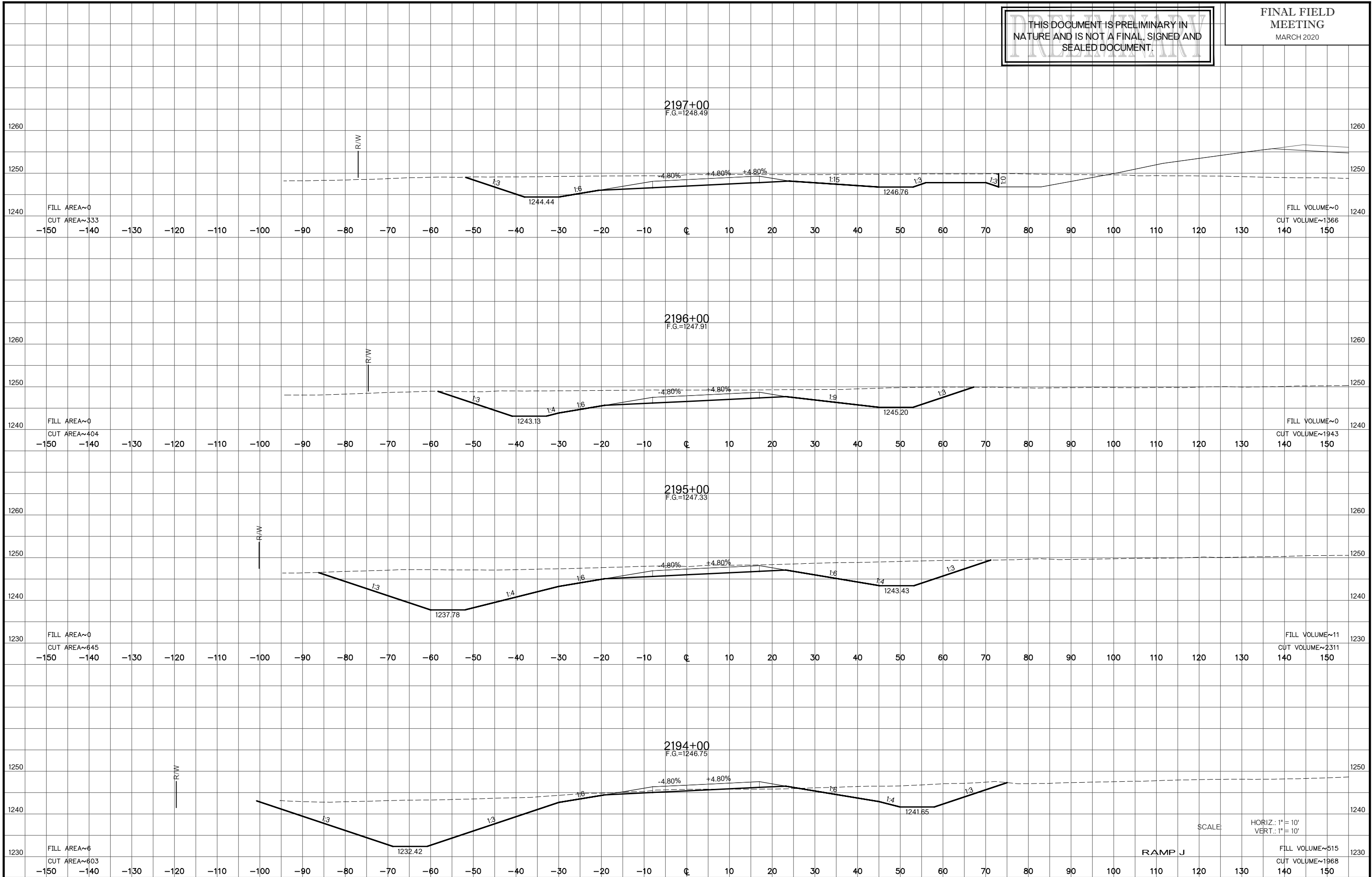
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE
 HORIZ.: 1" = 10'
 VERT.: 1" = 10'

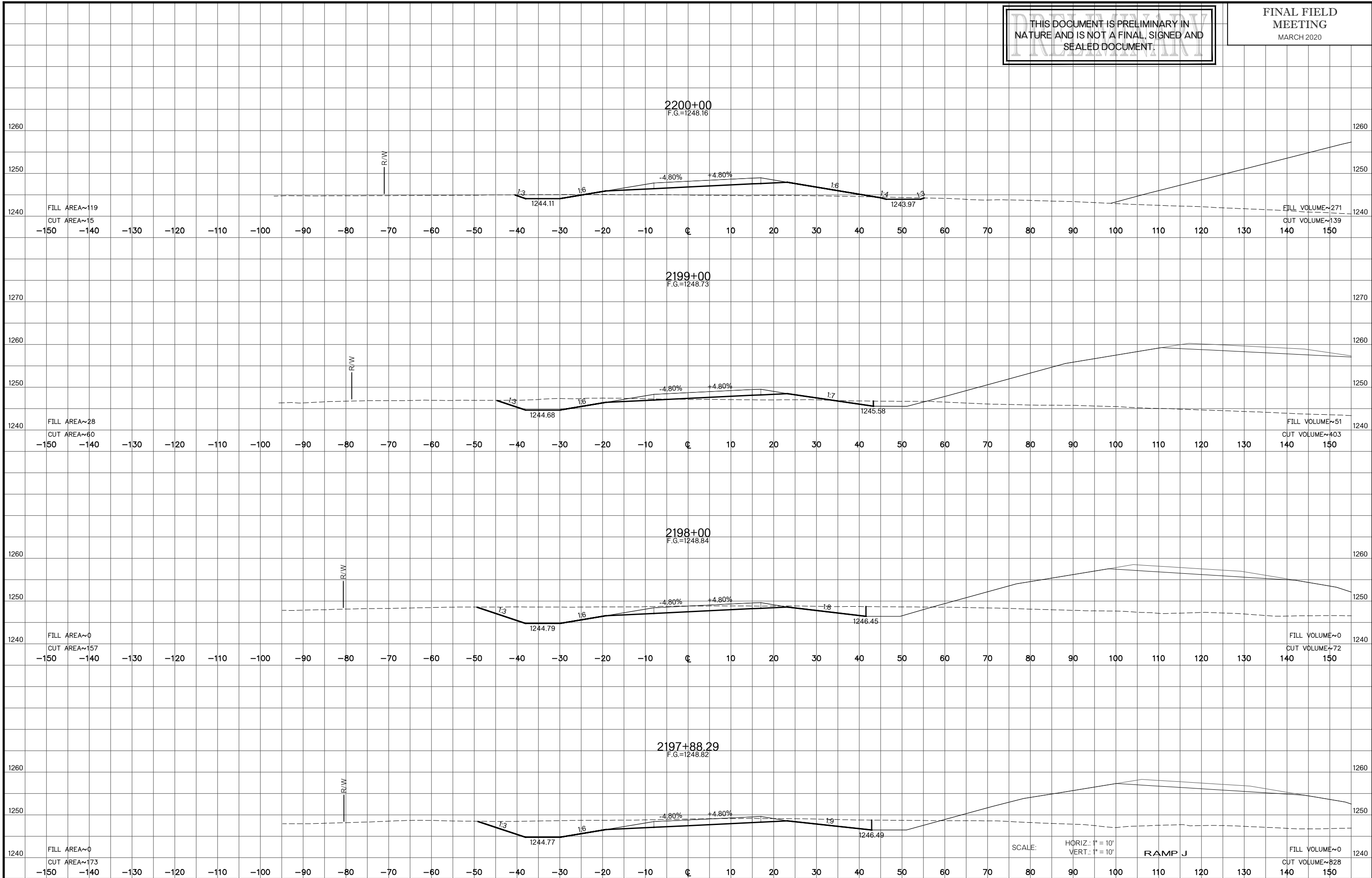
RAMP J

State Job No. 24428(04) Sheet No. X307

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



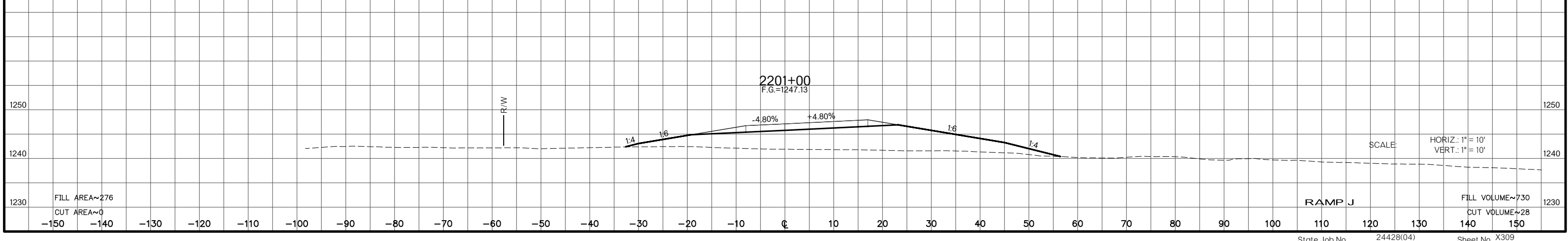
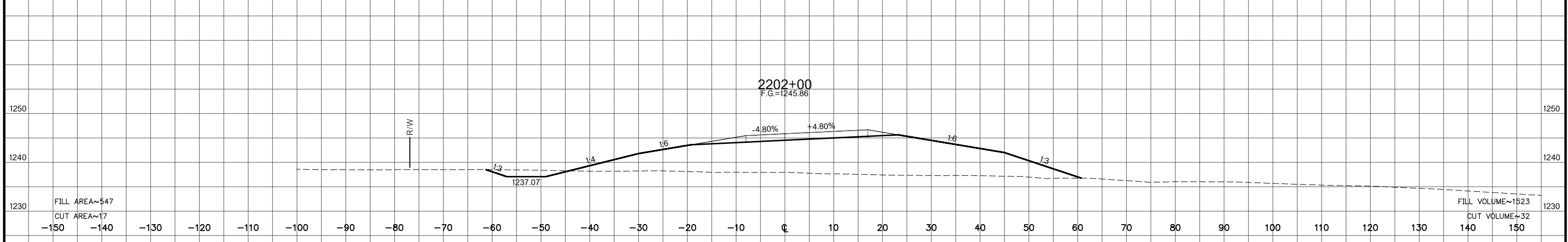
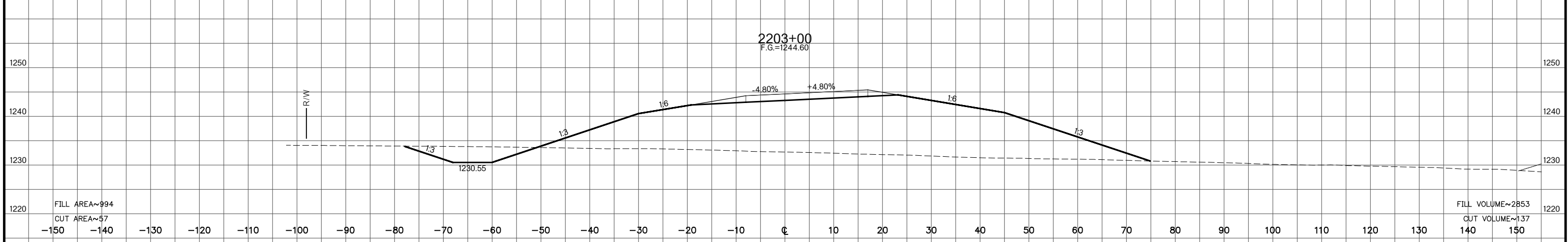
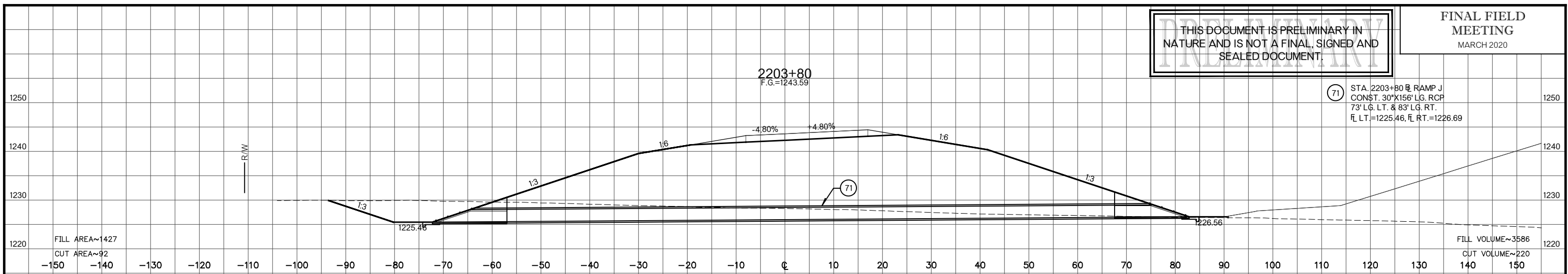
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP J

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

71 STA. 2203+80 RAMP J CONST. 30'X156' LG. RCP 73' LG. LT. & 83' LG. RT. FL LT.=1225.46, FL RT.=1226.69

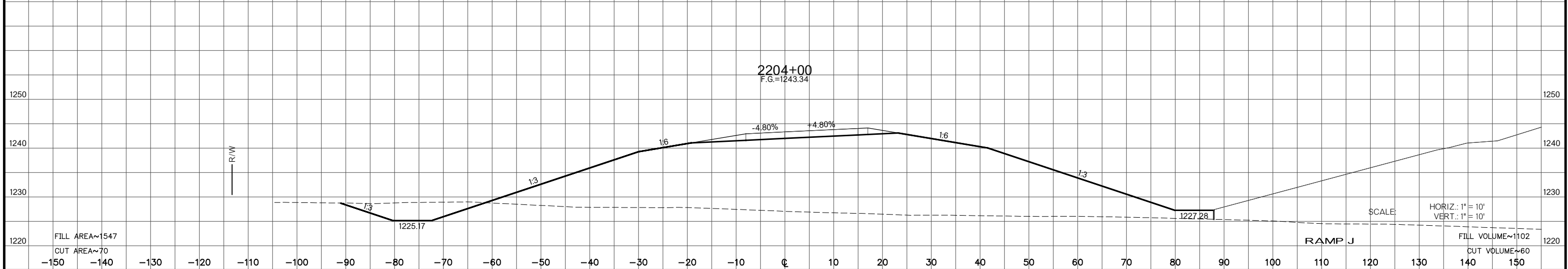
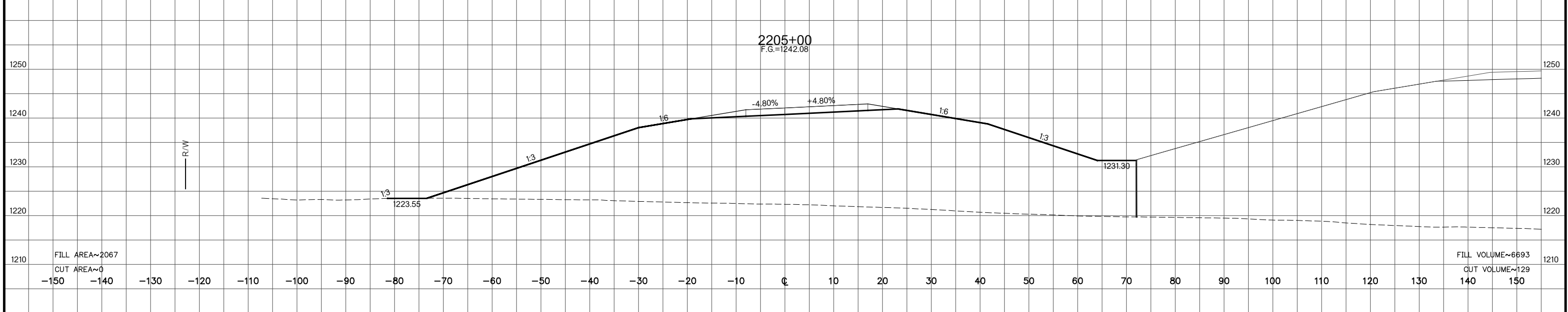
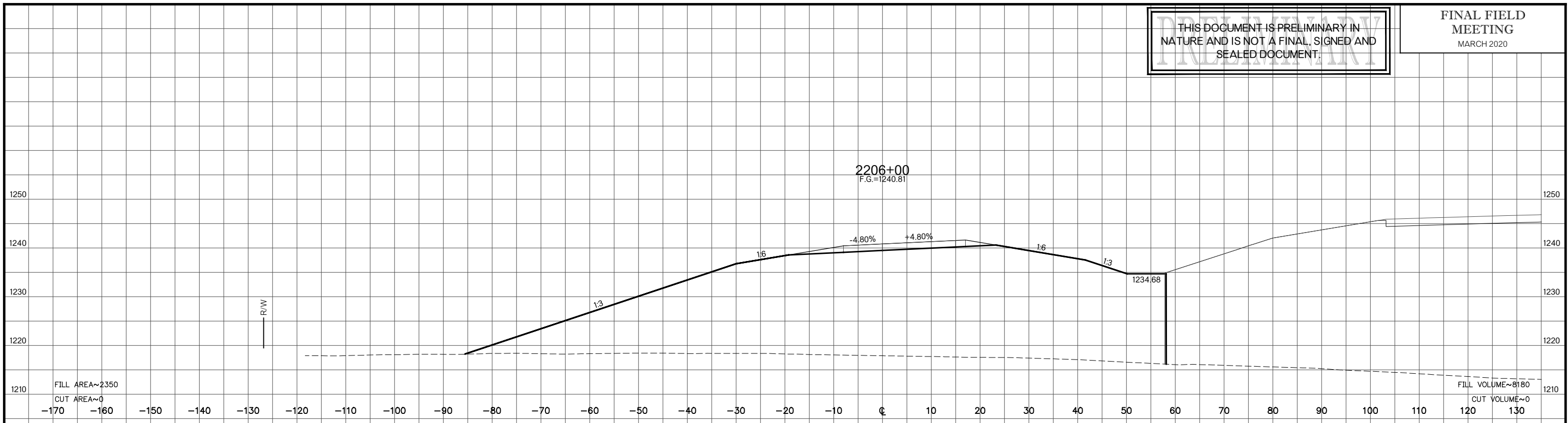


SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP J

US 81 REALIGNMENT GRADY COUNTY

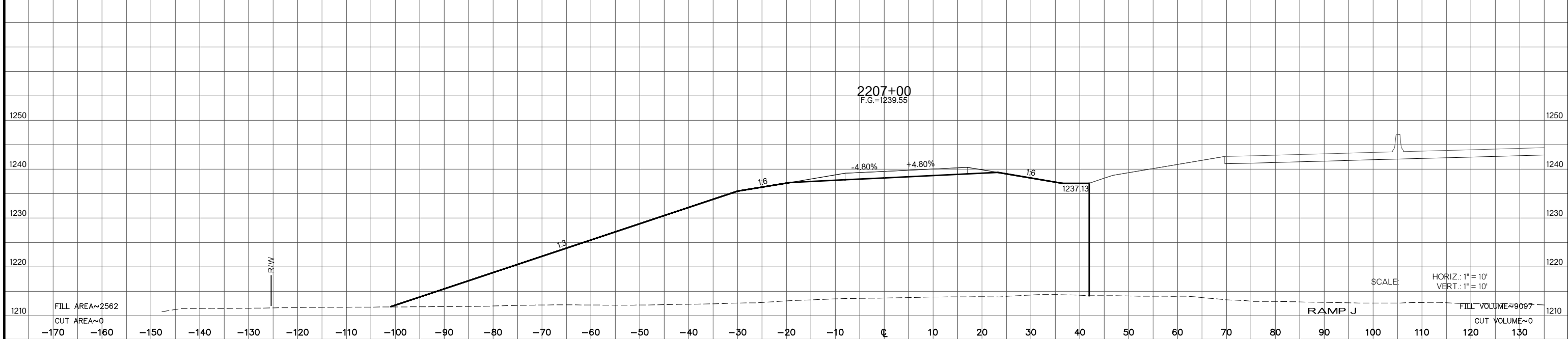
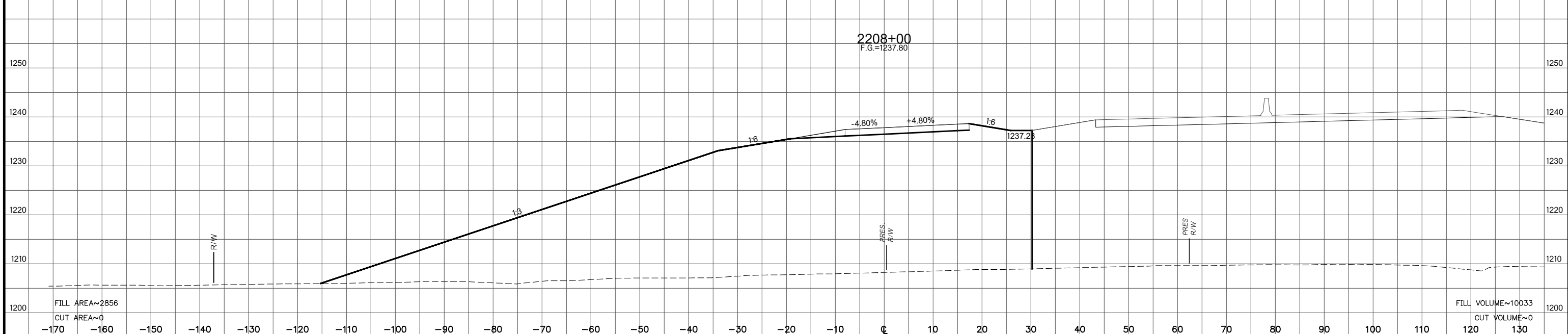
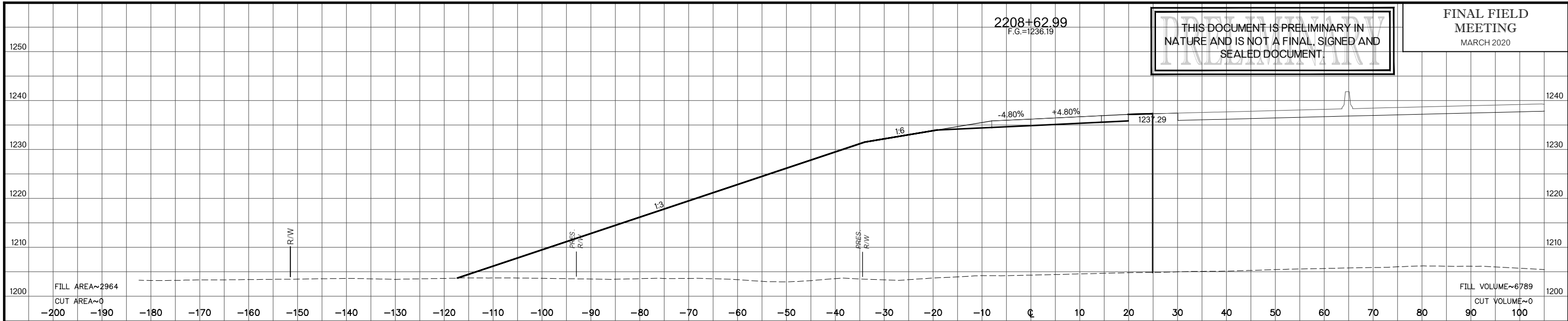
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



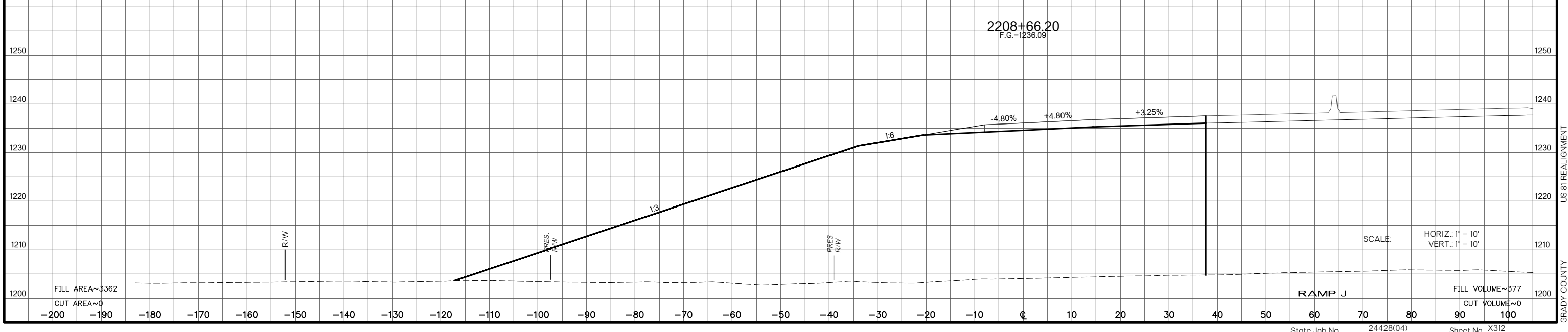
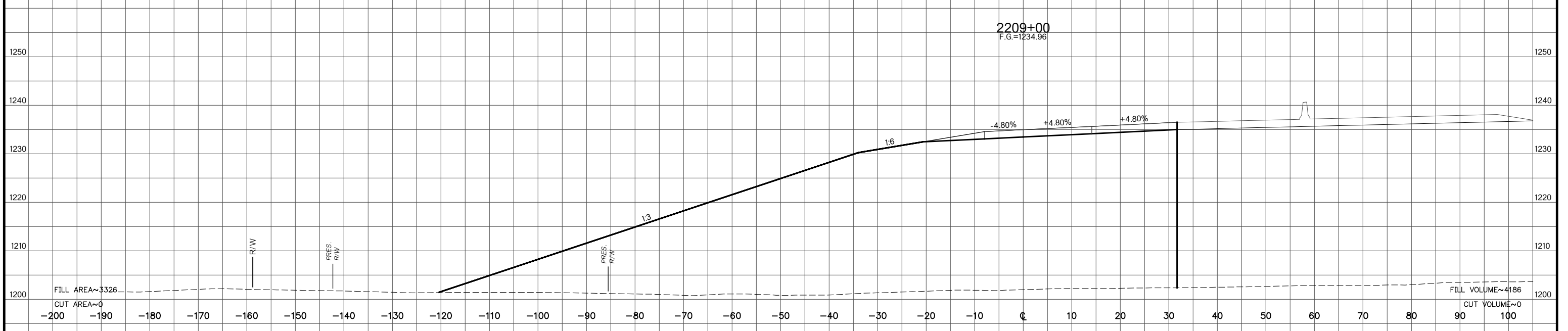
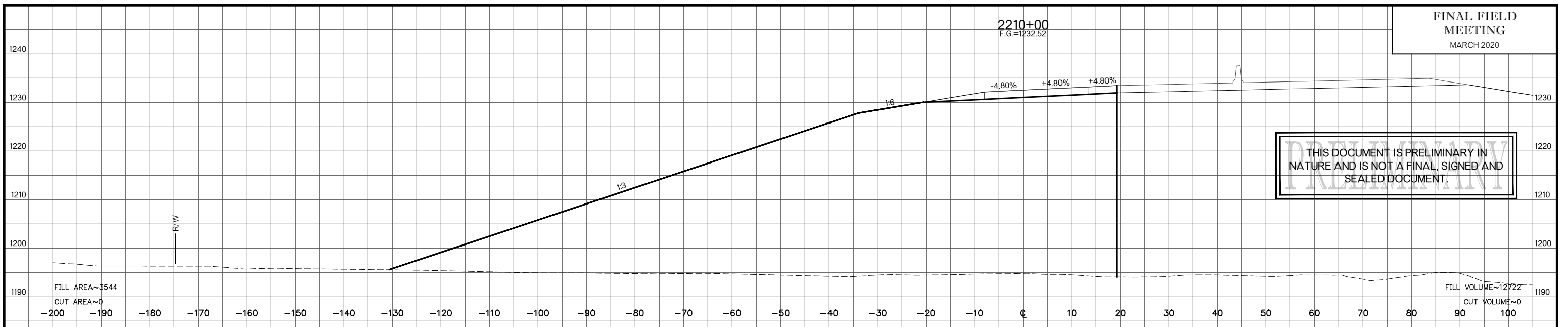
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP J

State Job No. 24428(04) Sheet No. X311

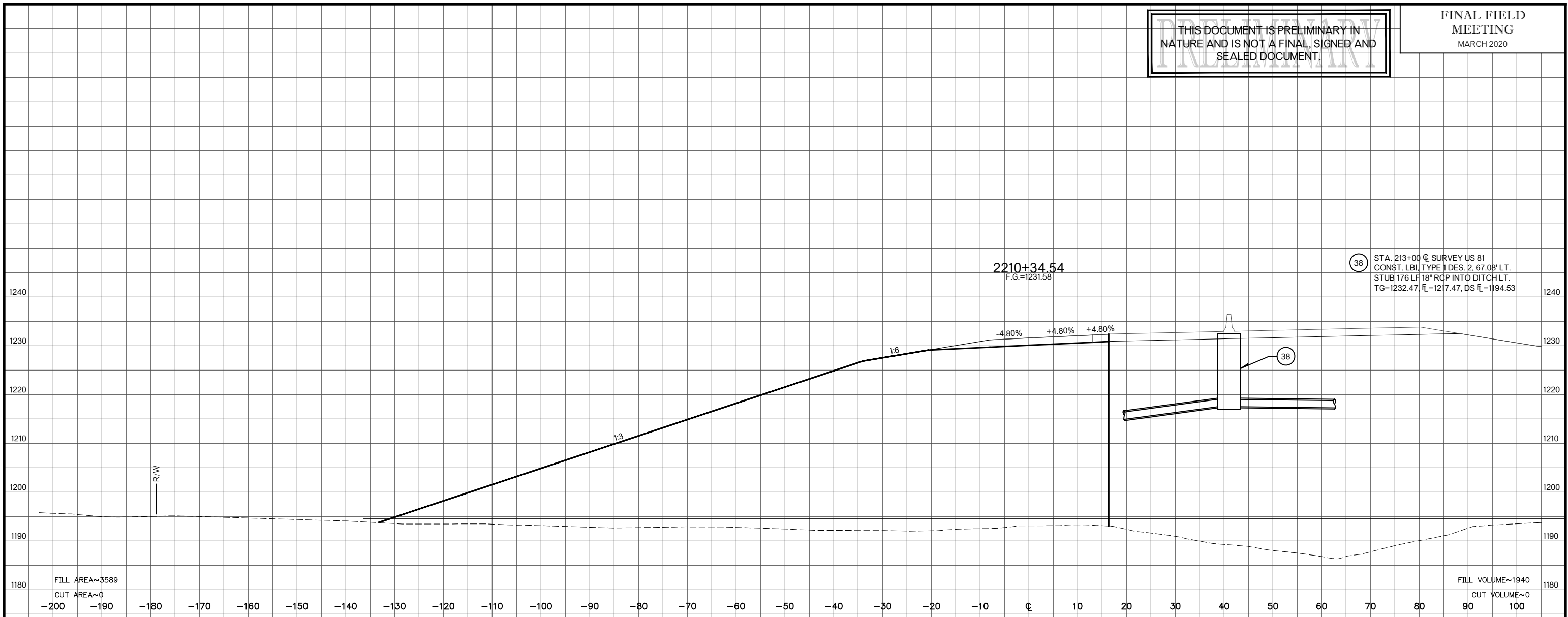
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

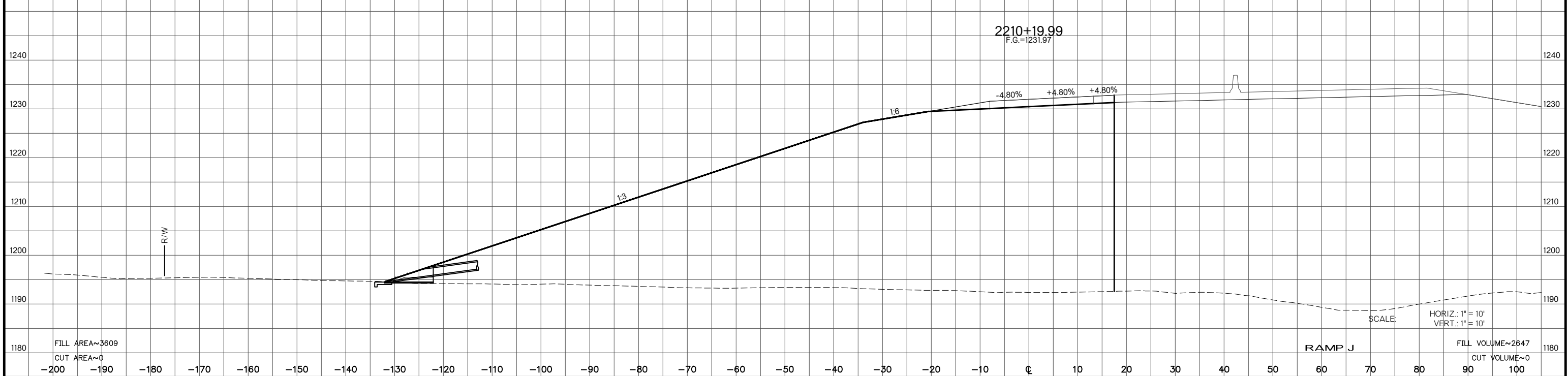


US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



38 STA. 213+00 @ SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' LT.
STUB 176 LF 18" RCP INTO DITCH LT.
TG=1232.47, f_L=1217.47, DS f_L=1194.53

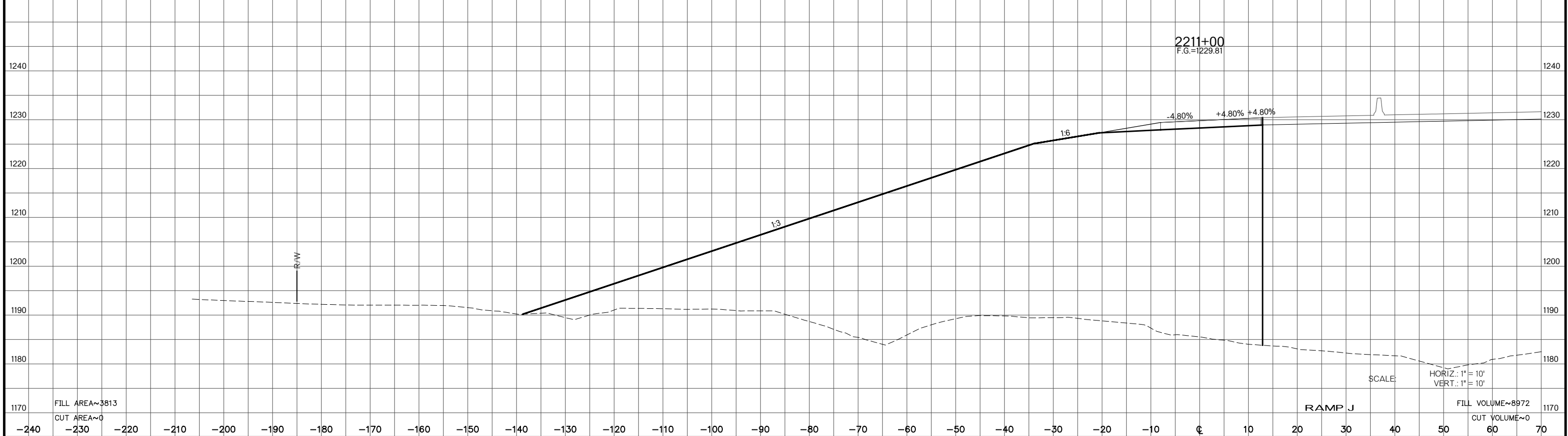


SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

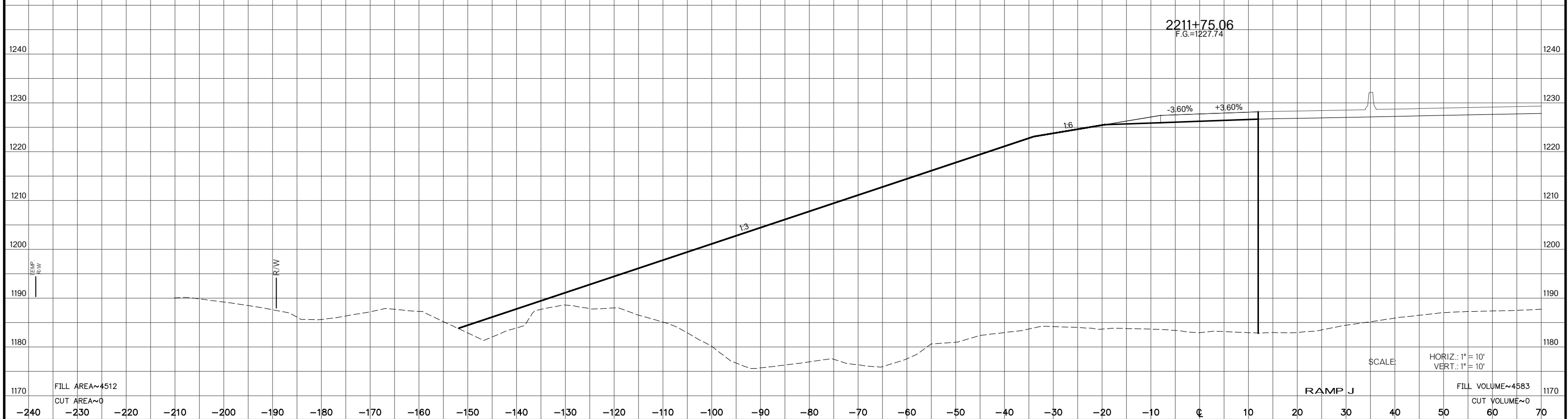
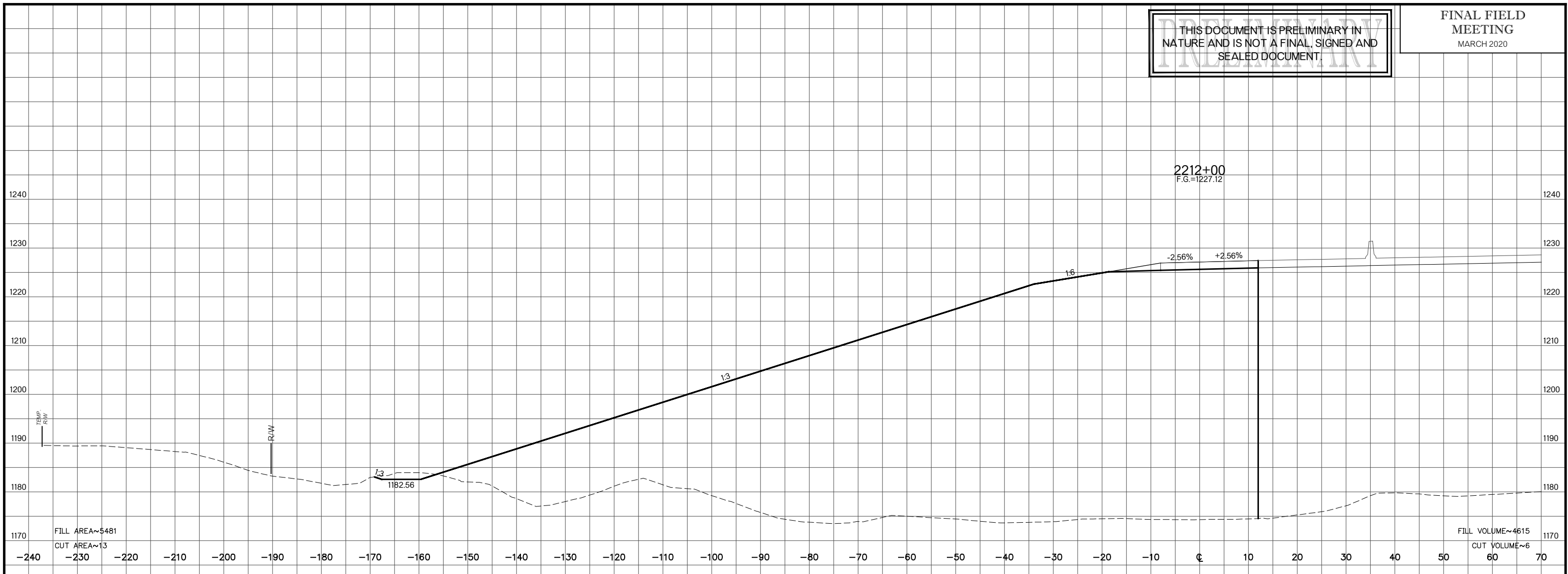
RAMP J

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



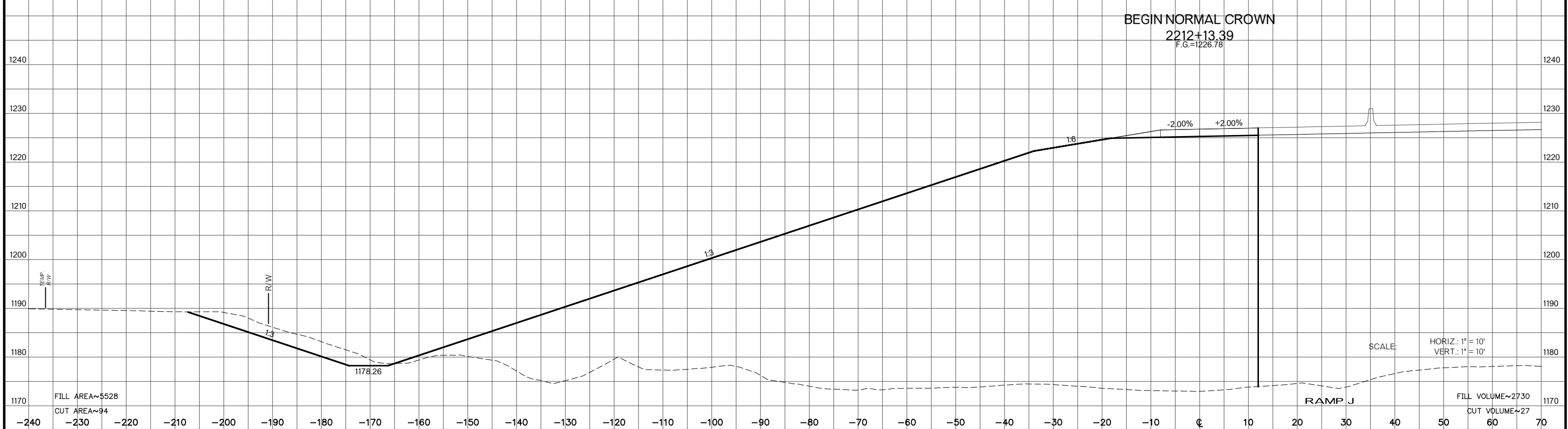
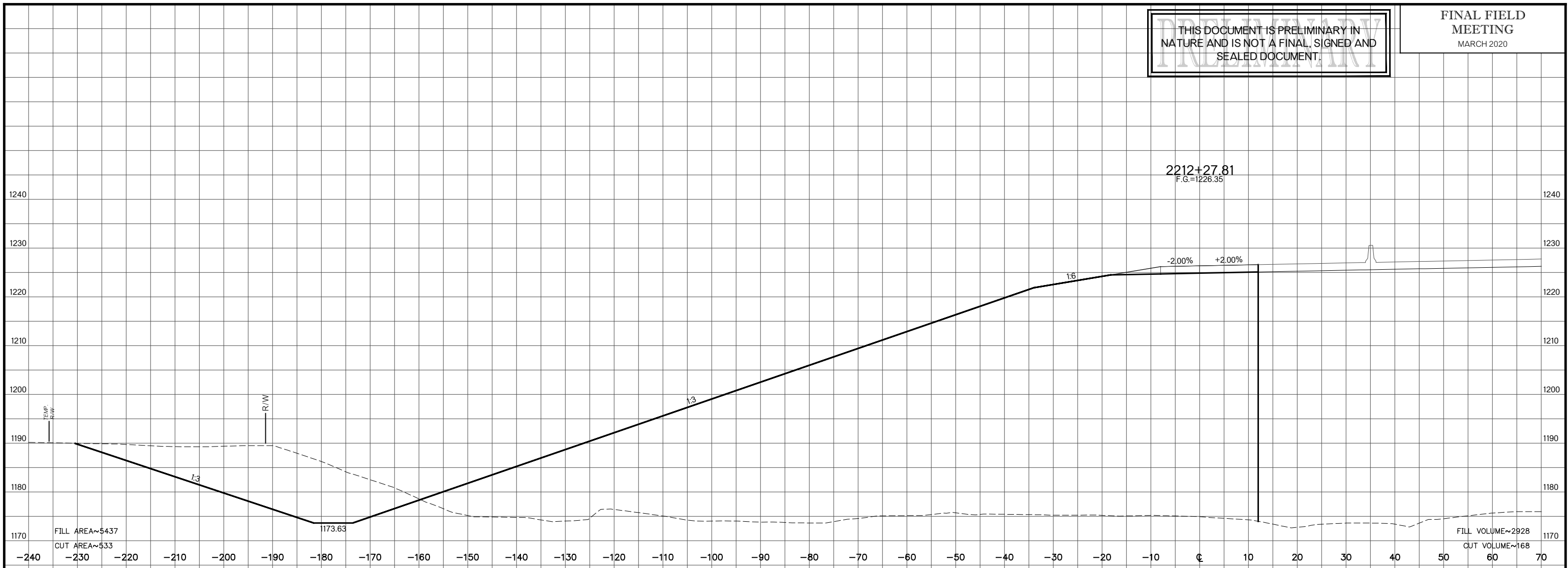
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE
RAMP J
HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



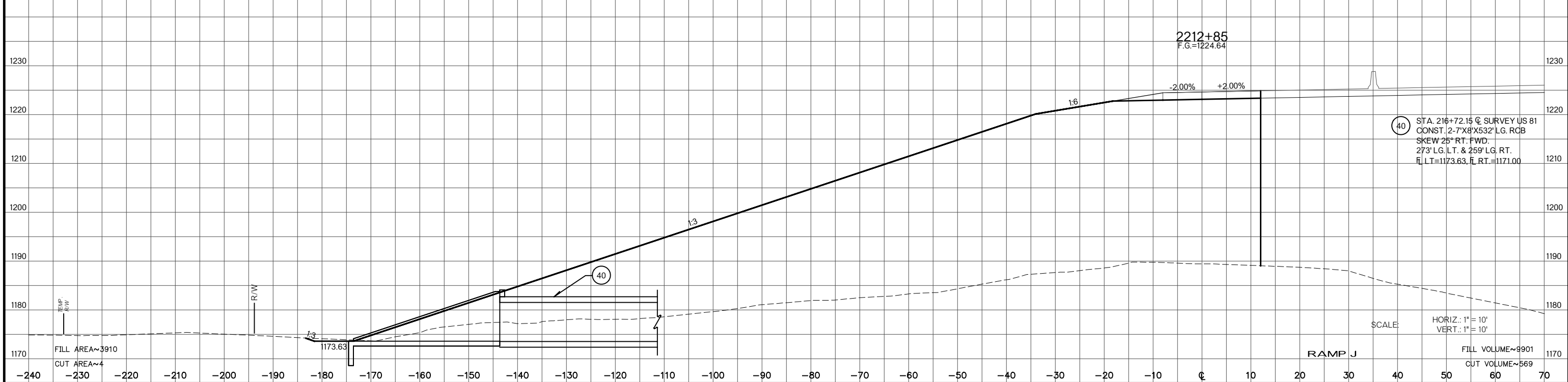
BEGIN NORMAL CROWN
2212+13.39
F.G.=1226.78

SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



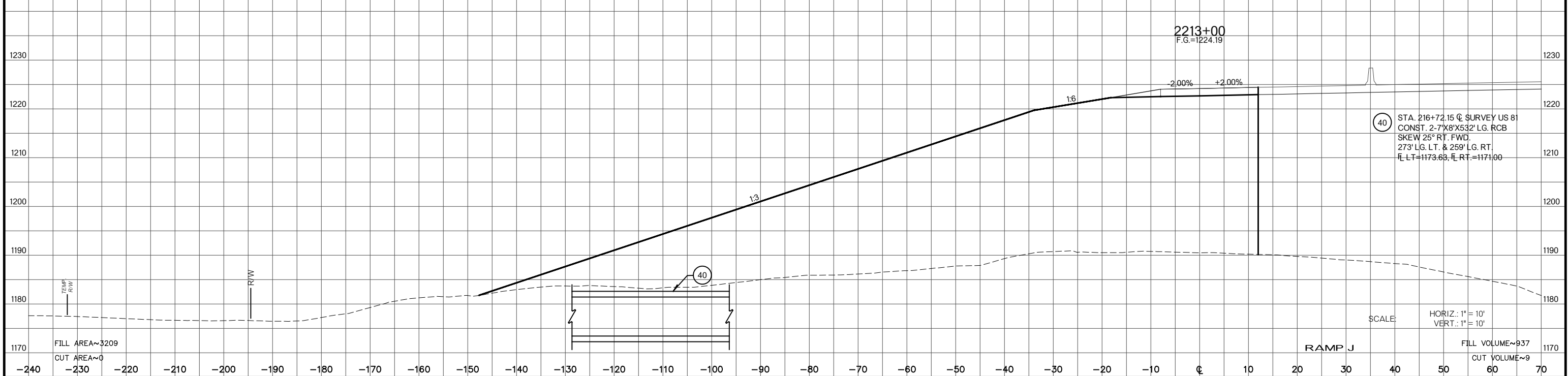
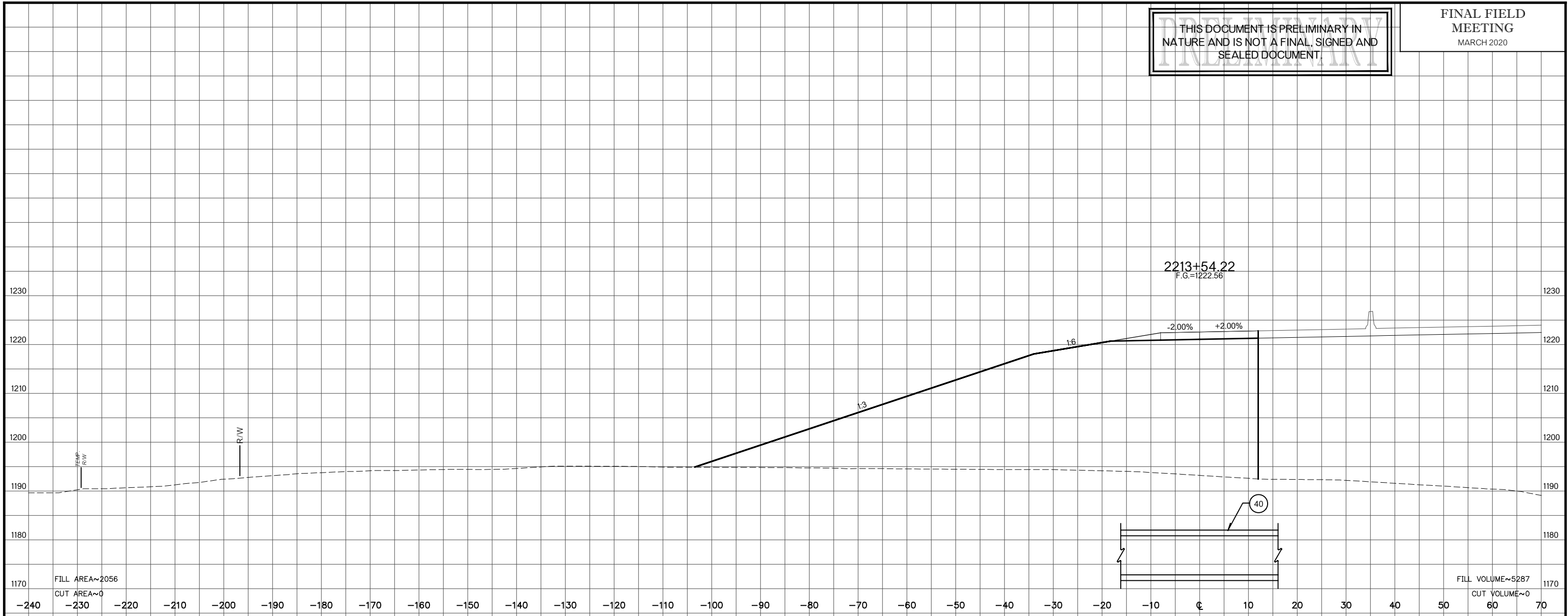
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP J

State Job No. 24428(04) Sheet No. X317

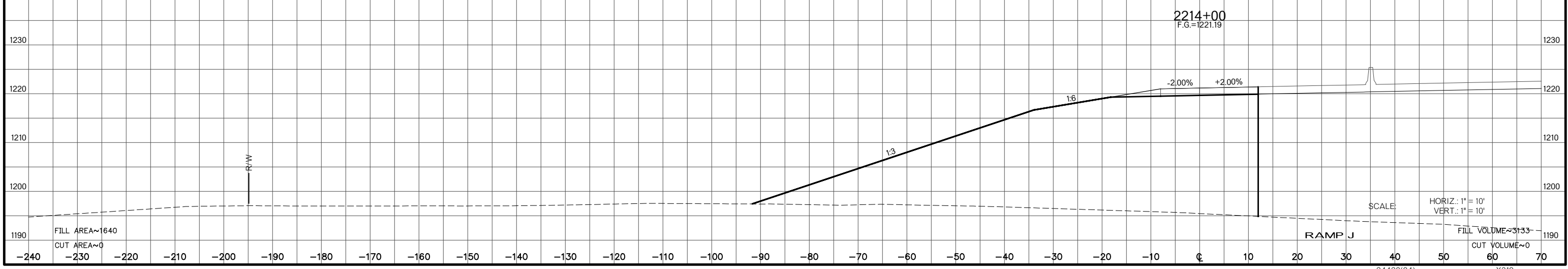
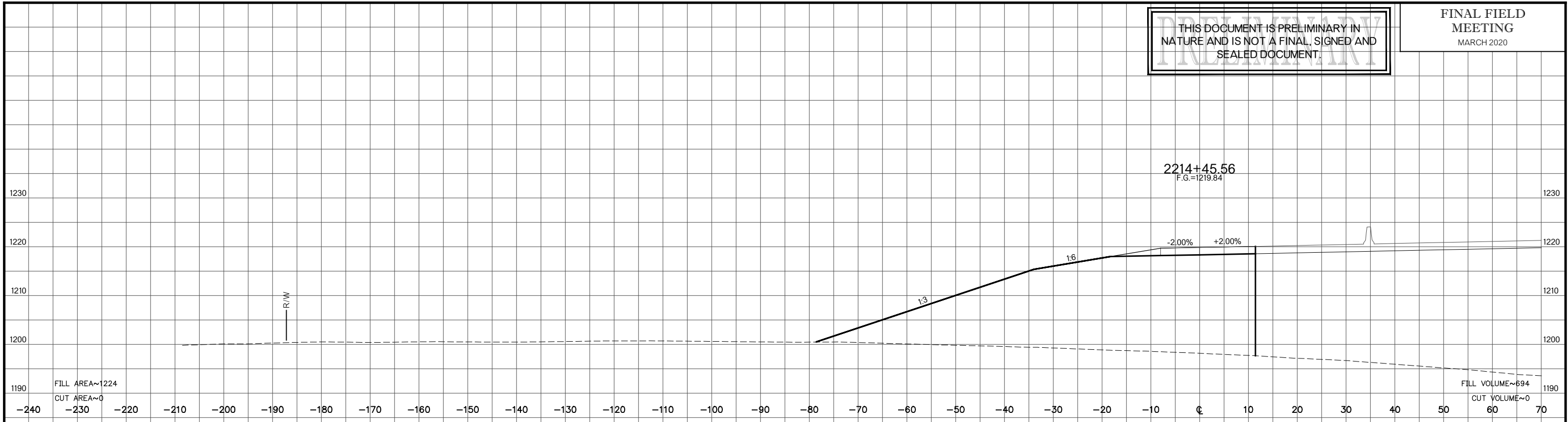
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

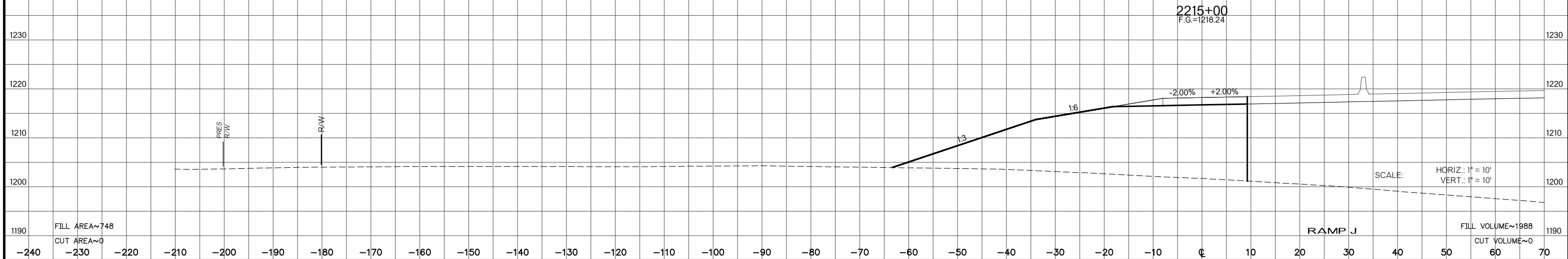
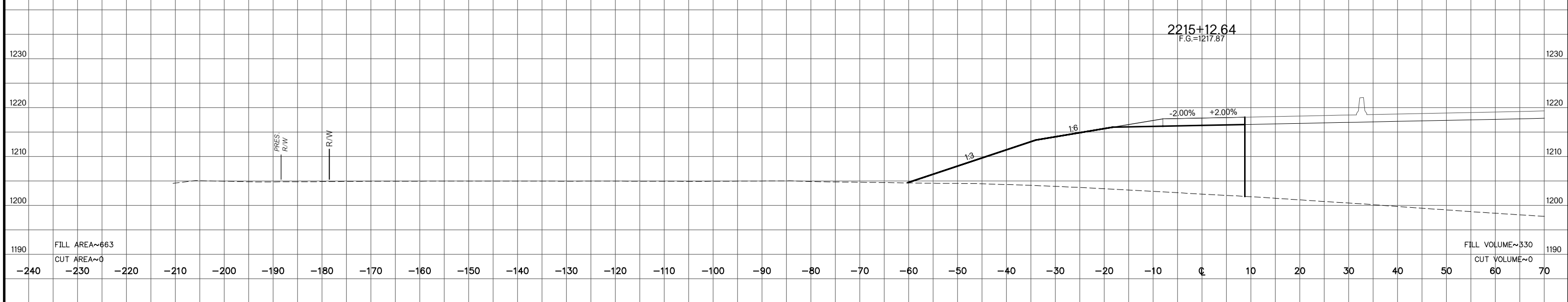
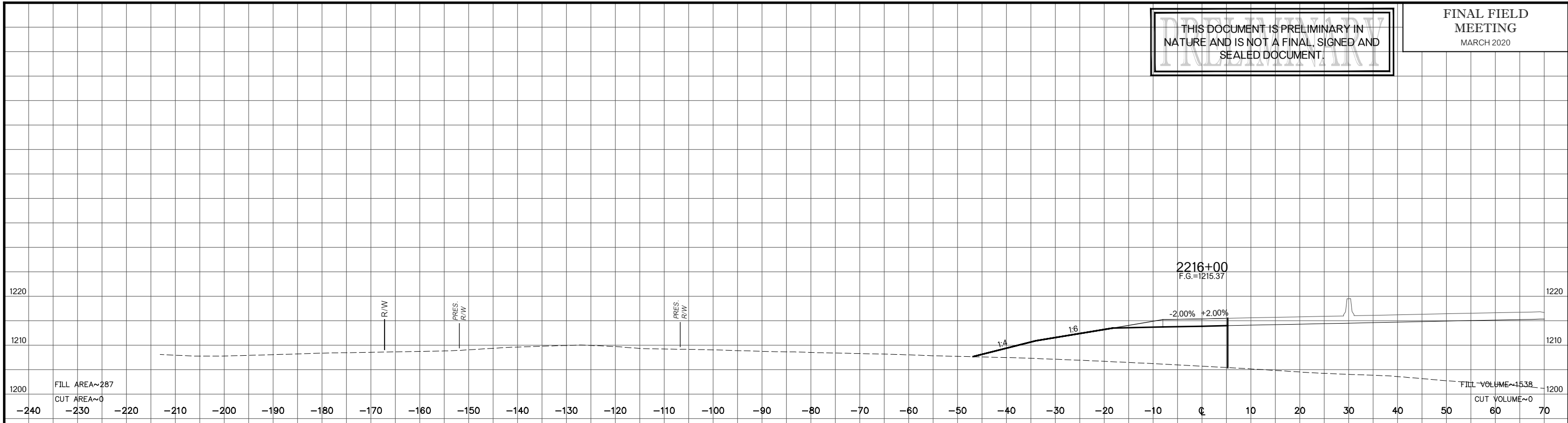


US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



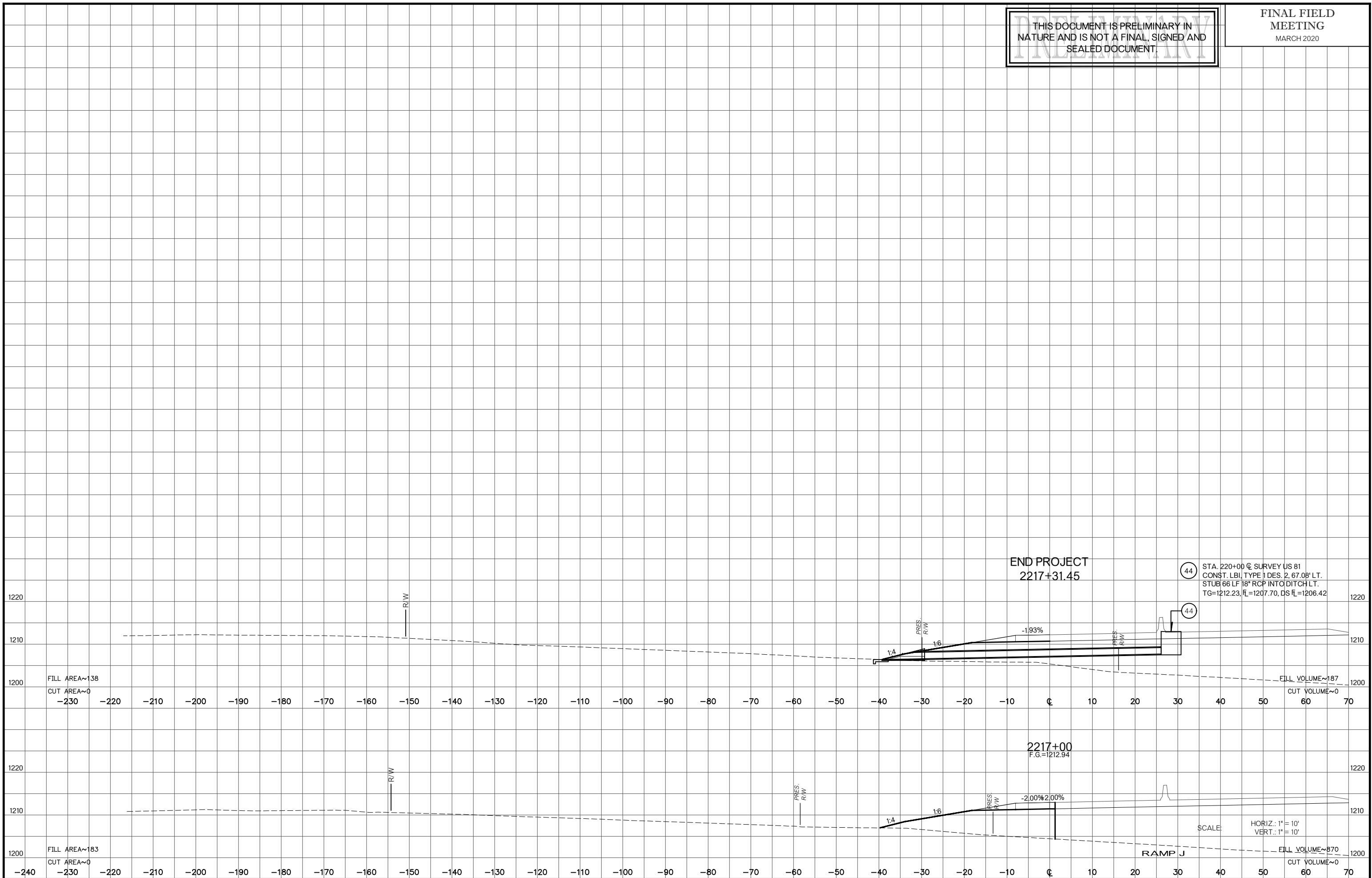
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



44 STA. 220+00 @ SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' LT.
STUB 66 LF 18" ROP INTO DITCH LT.
TG=1212.23, ft=1207.70, DS ft=1206.42

FILL AREA~138
CUT AREA~0

FILL VOLUME~187
CUT VOLUME~0

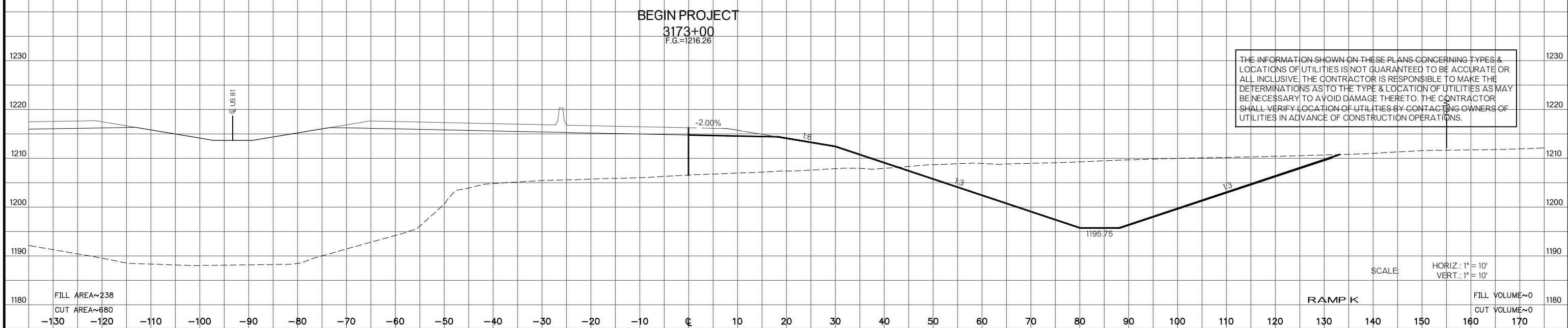
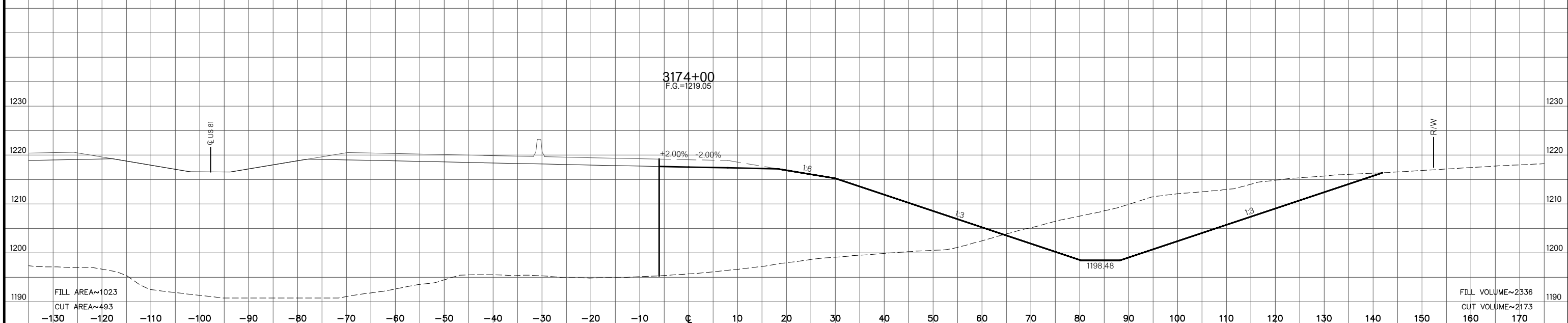
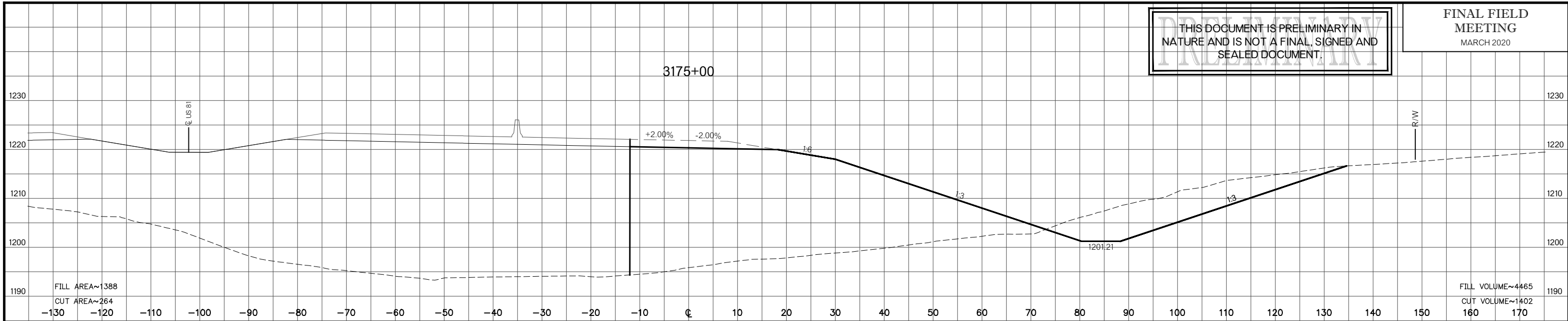
FILL AREA~183
CUT AREA~0

FILL VOLUME~870
CUT VOLUME~0

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

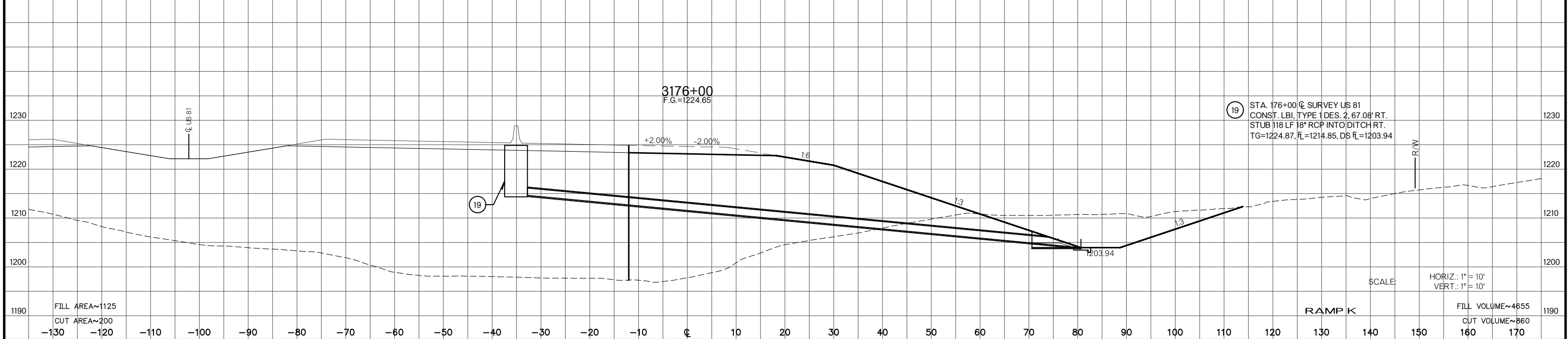
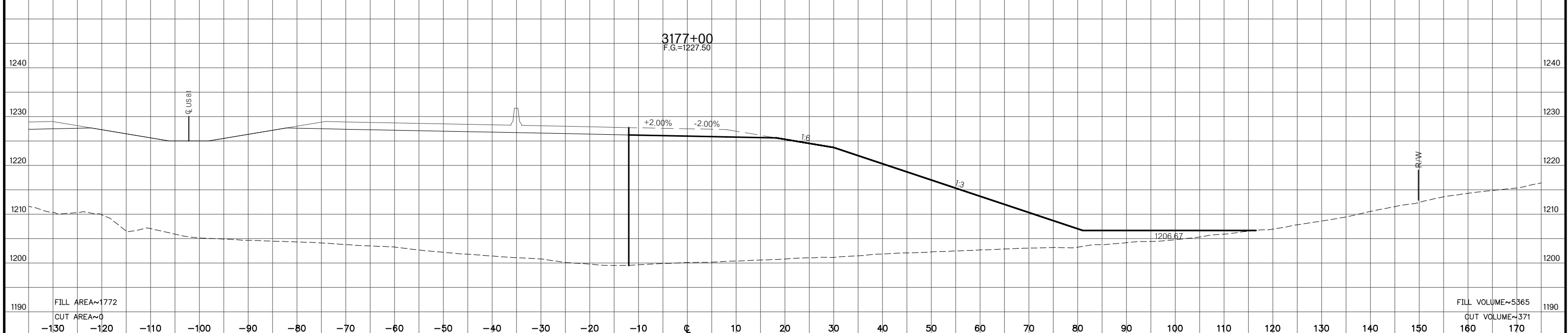
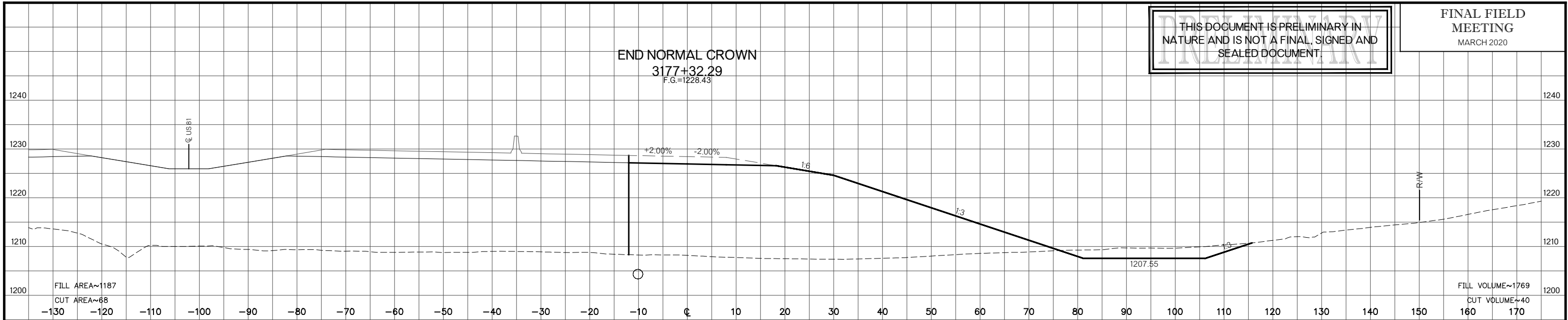
SCALE HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP K

GRADY COUNTY
US 81 REALIGNMENT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



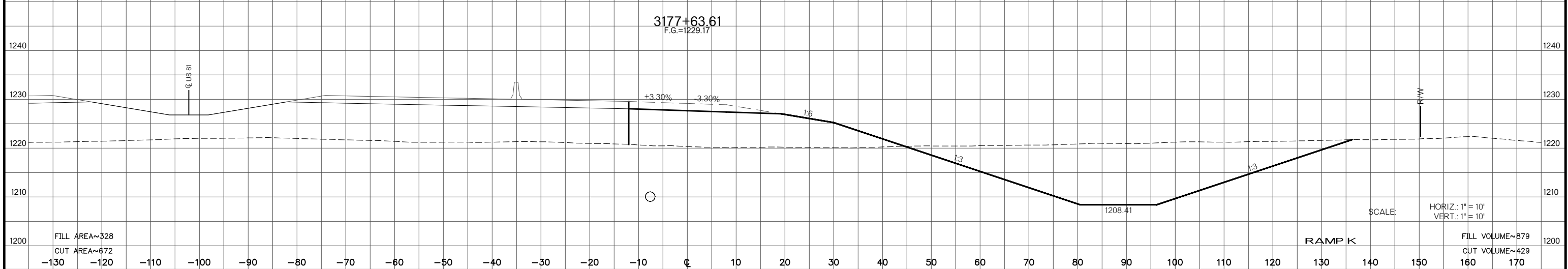
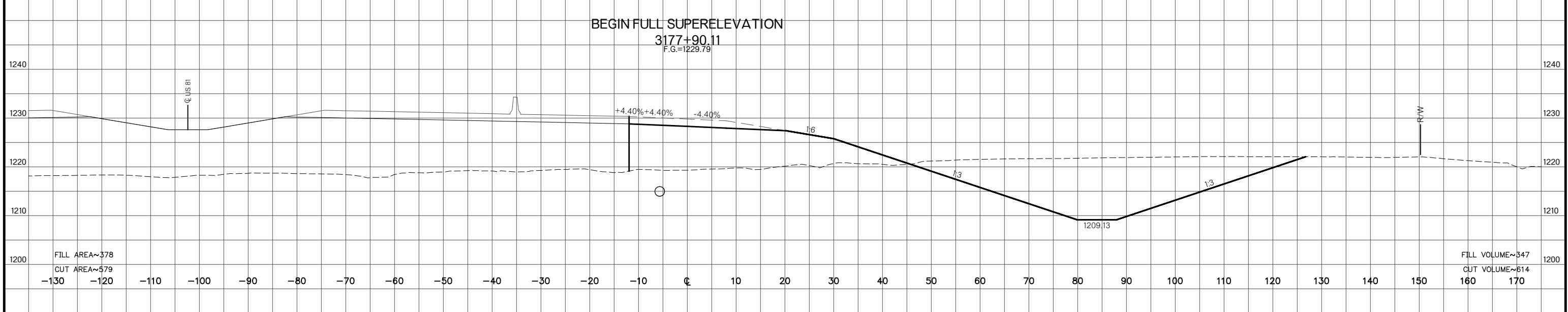
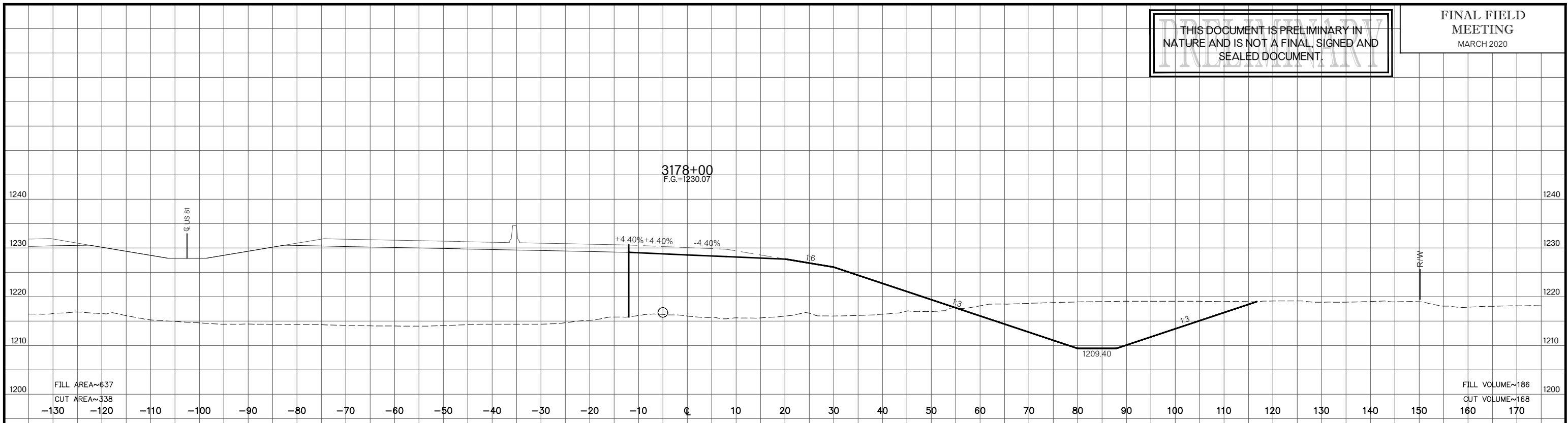
19 STA. 176+00 Q SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' RT.
STUB 118 LF 18" RCP INTO DITCH RT.
TG=1224.87, FL=1214.85, DS FL=1203.94

SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP K

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

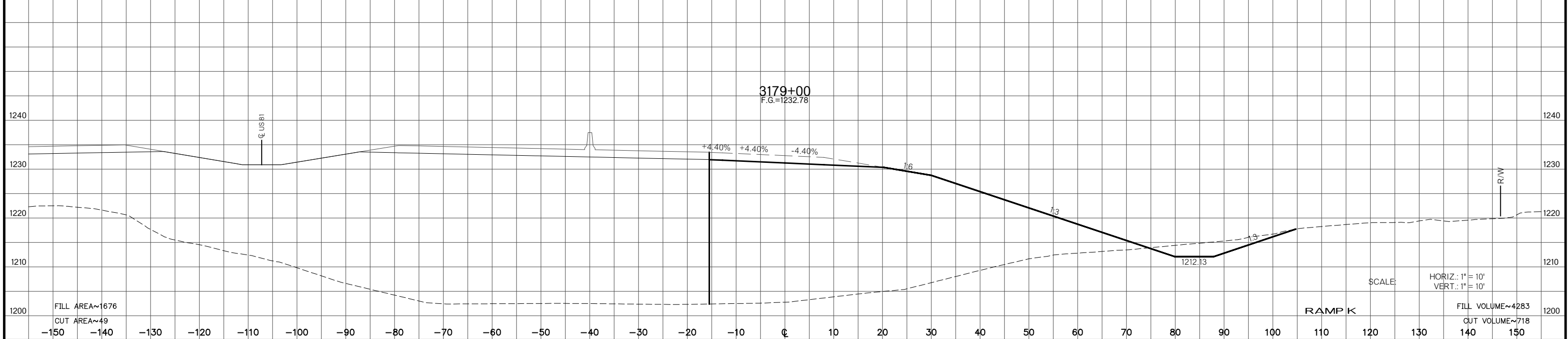
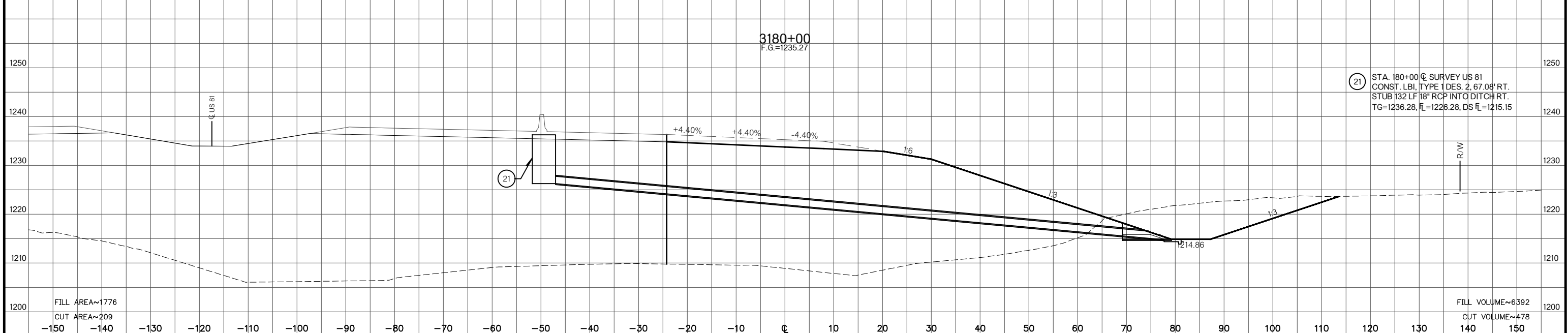
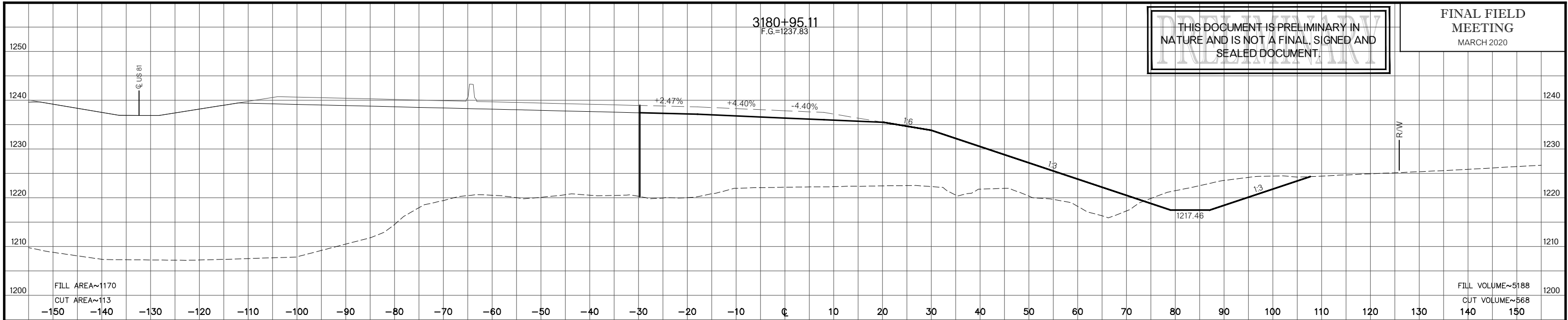
RAMP K

State Job No. 24428(04) Sheet No. X325

GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



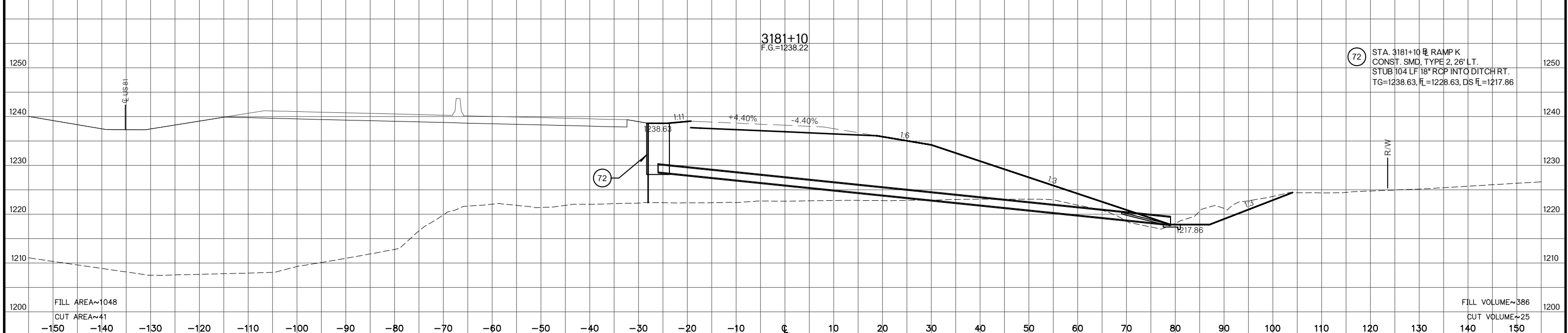
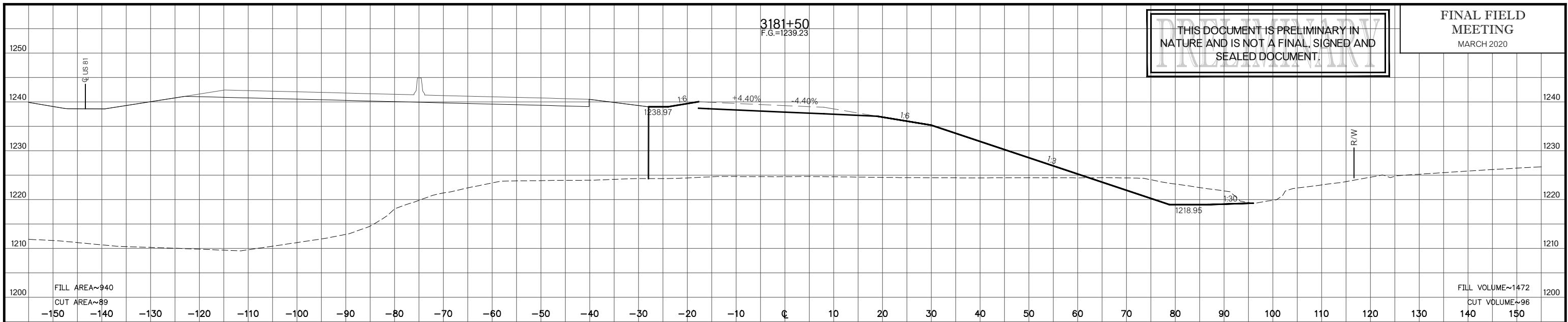
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

State Job No. 24428(04) Sheet No. X326

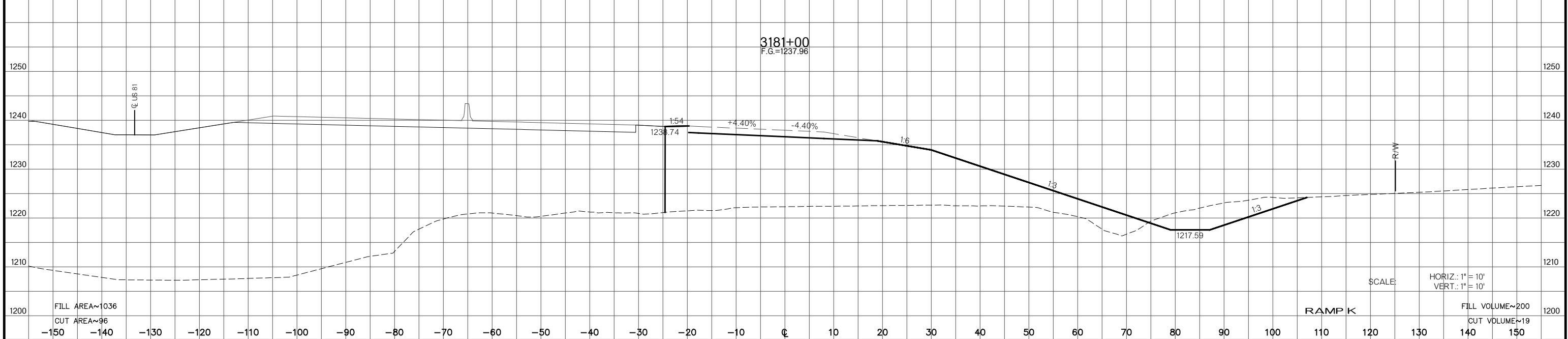
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



(72) STA. 3181+10 RAMP K
CONST. SMD, TYPE 2, 26' LT.
STUB 104 LF 18" RCP INTO DITCH RT.
TG=1238.63, \bar{f}_L =1228.63, DS \bar{f}_L =1217.86



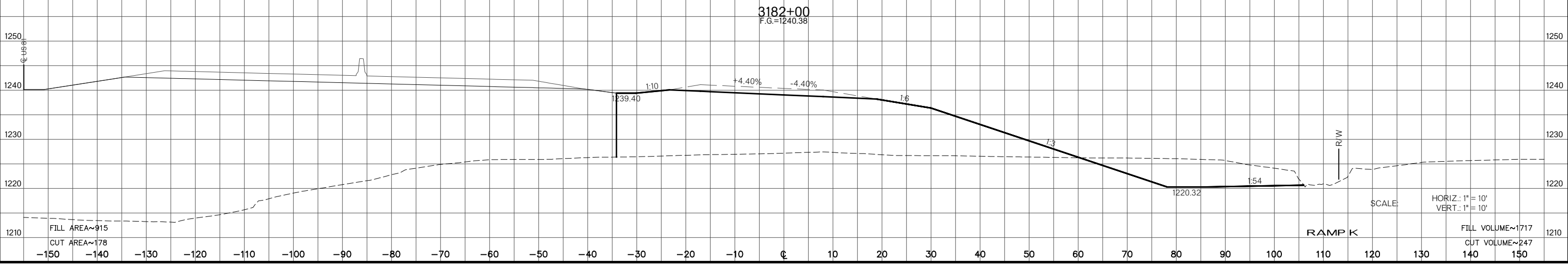
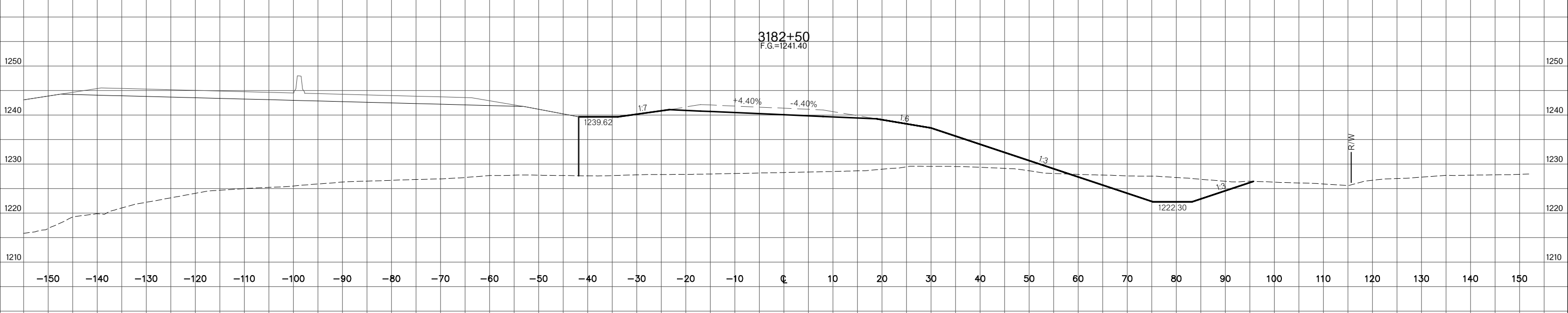
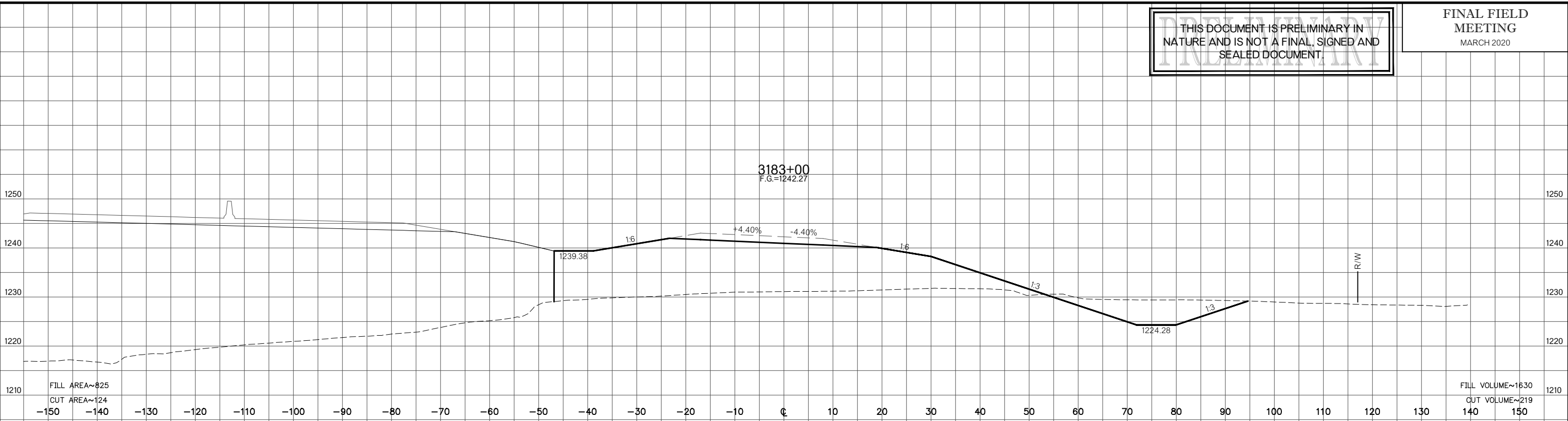
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP K
FILL VOLUME~200
CUT VOLUME~19

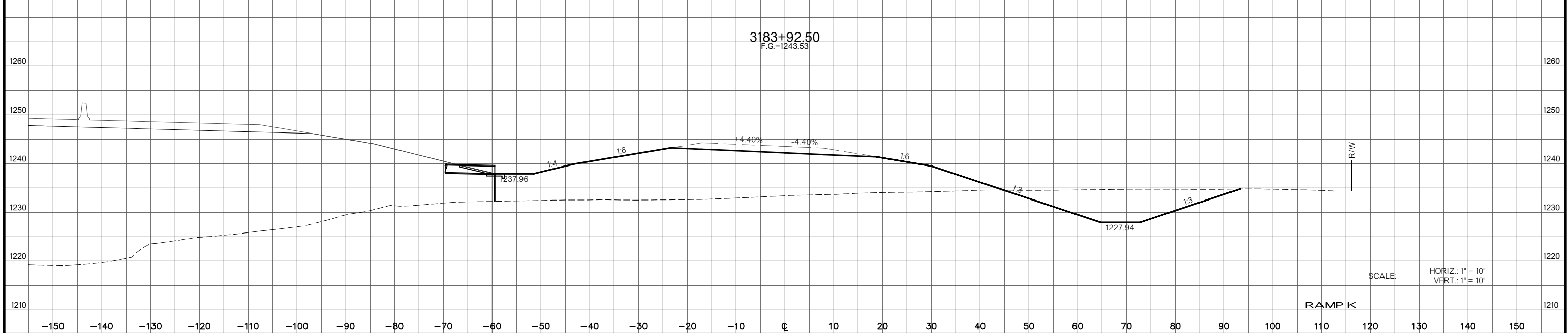
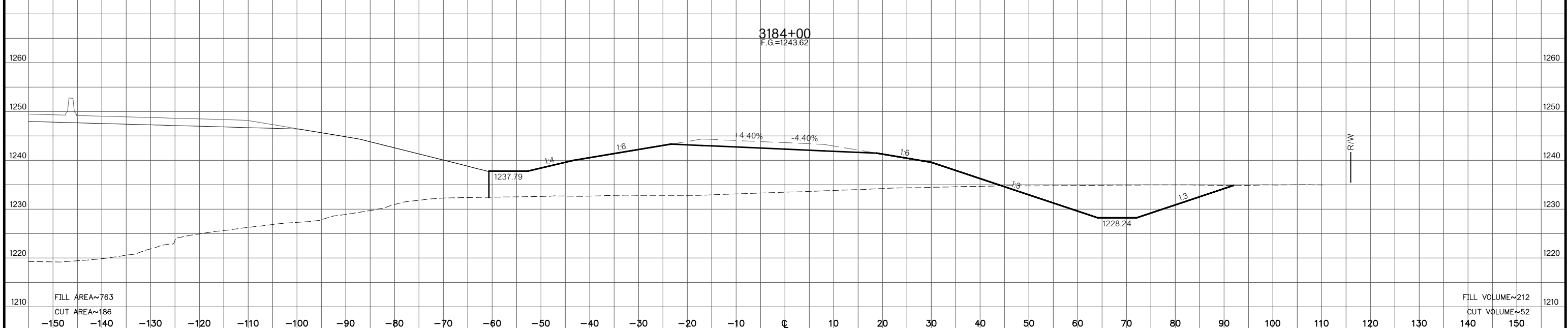
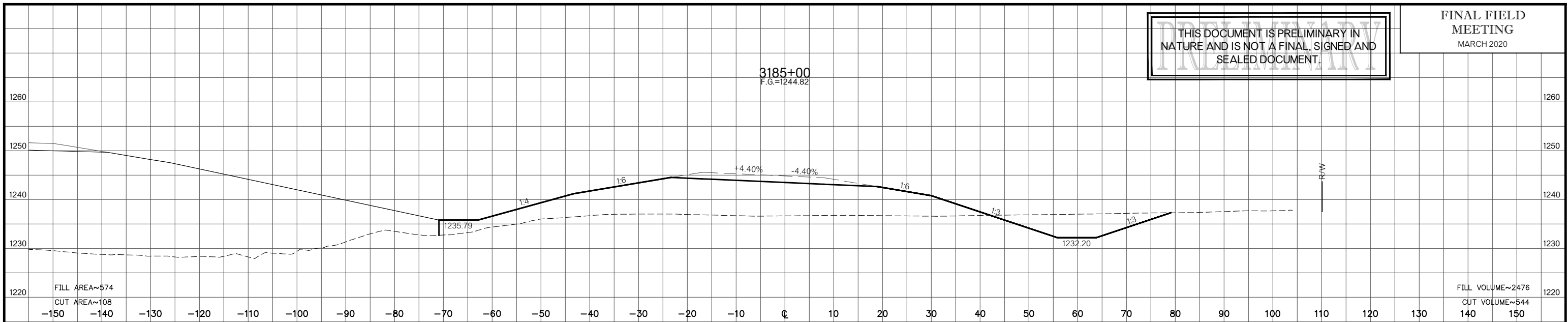
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

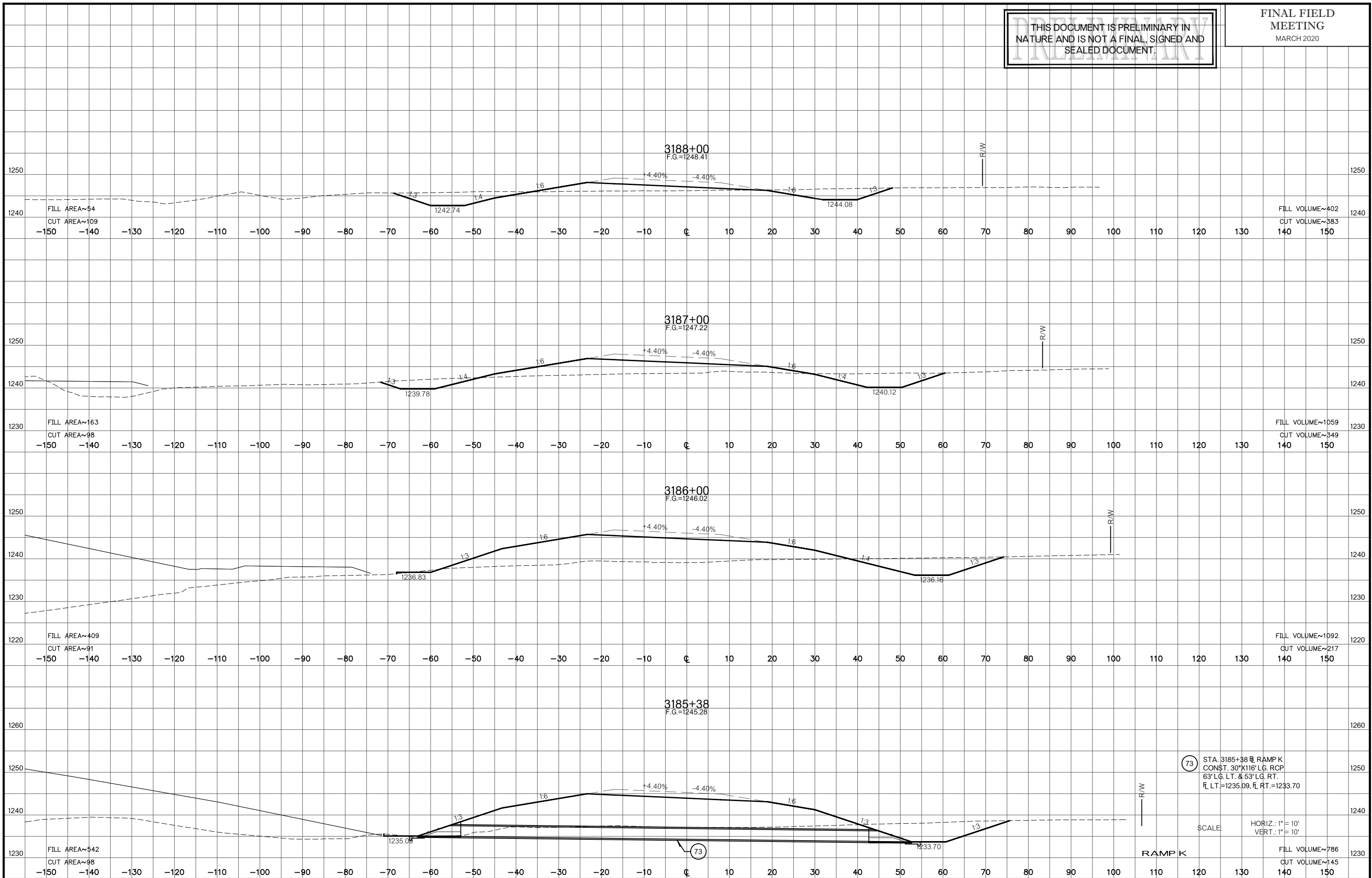


SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

State Job No. 24428(04) Sheet No. X329

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



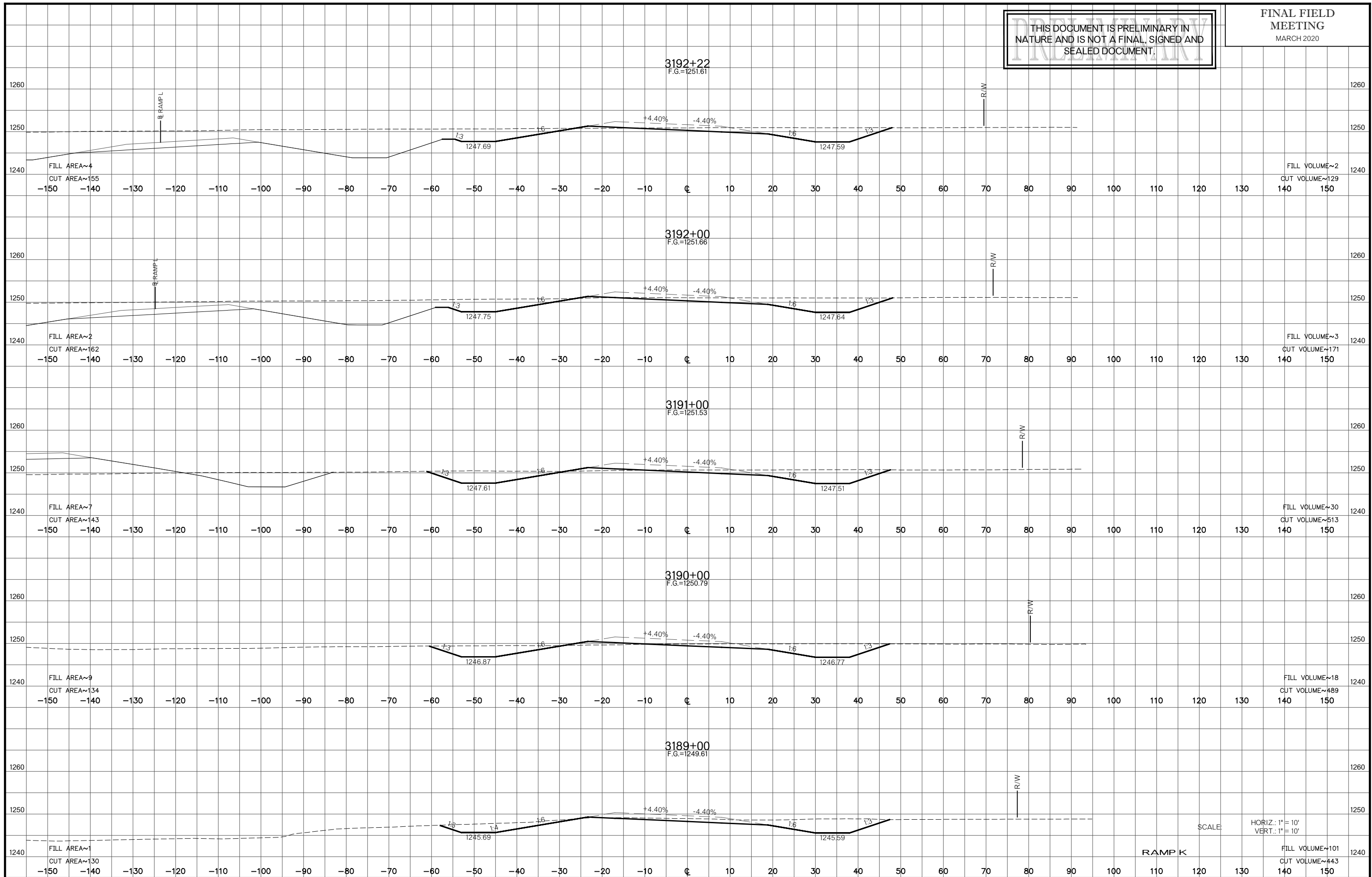
(73) STA. 3185+38 RAMP K
CONST. 30'X116' LG. RCP
63' LG. LT. & 53' LG. RT.
FL LT=1235.09, FL RT.=1233.70

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP K

US 81 REALIGNMENT
GRADY COUNTY

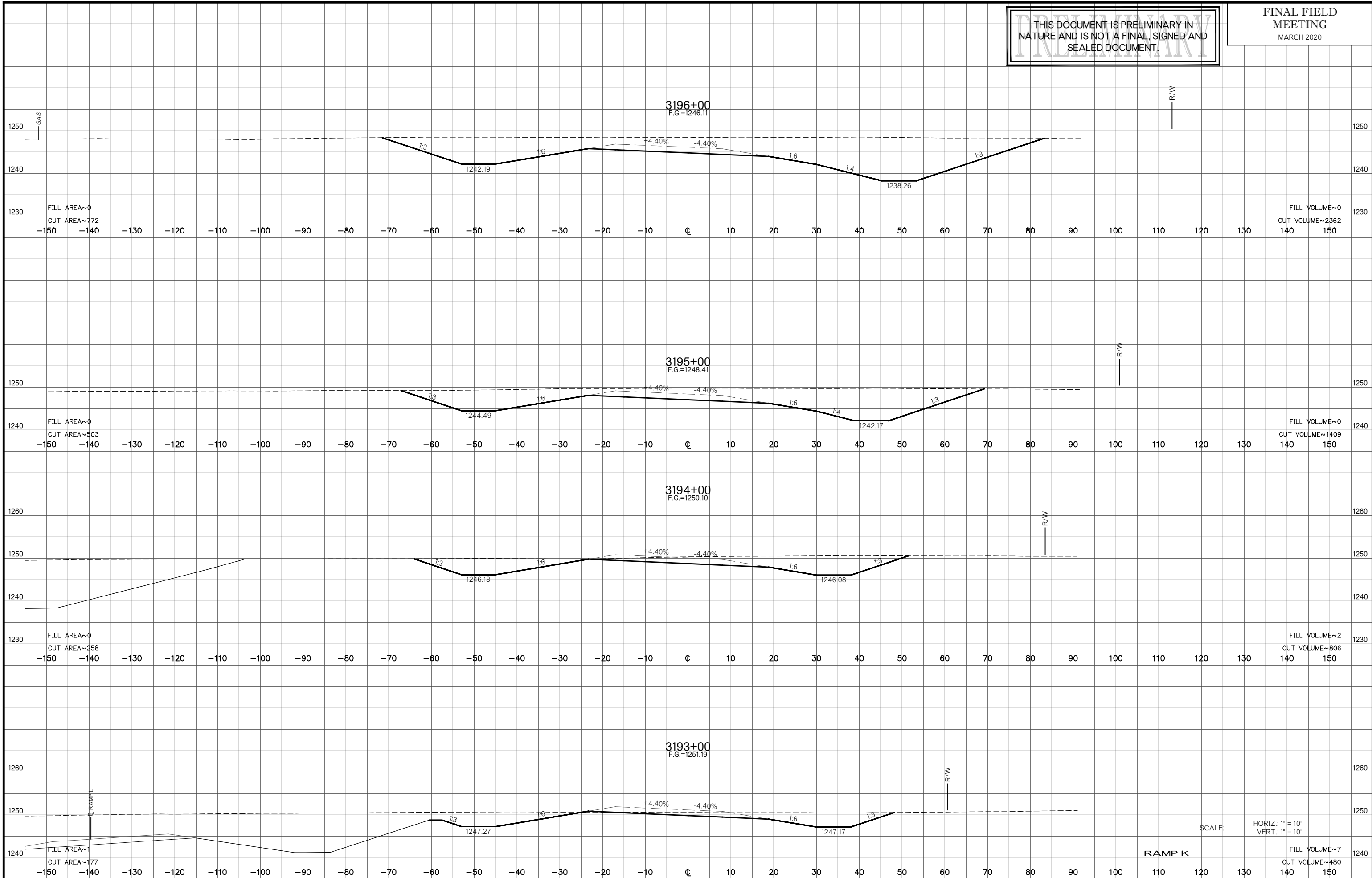
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE: RAMP K
 HORIZ.: 1" = 10'
 VERT.: 1" = 10'

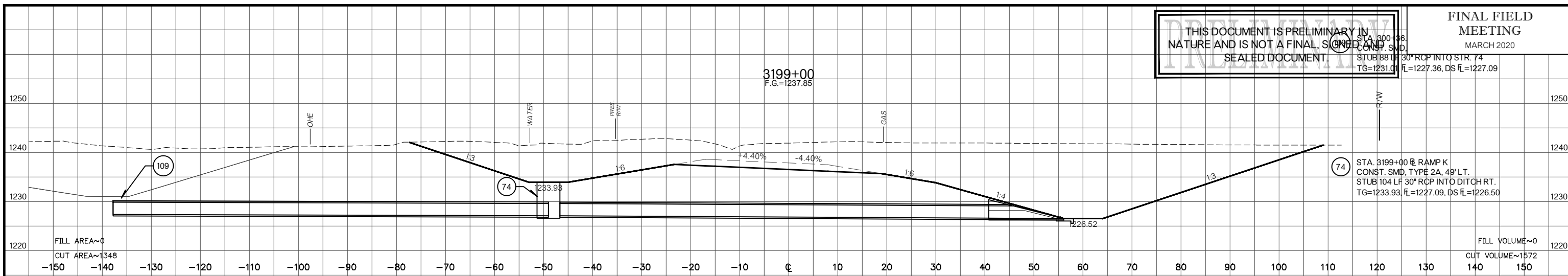
State Job No. 24428(04) Sheet No. X332

US 81 REALIGNMENT GRADY COUNTY

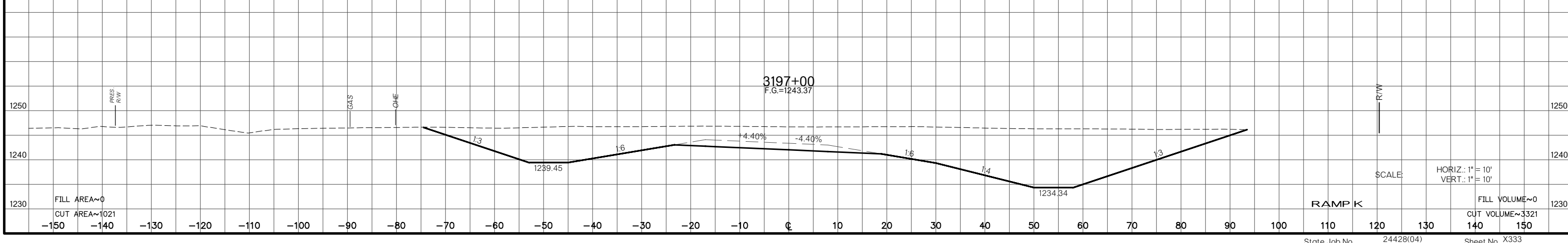
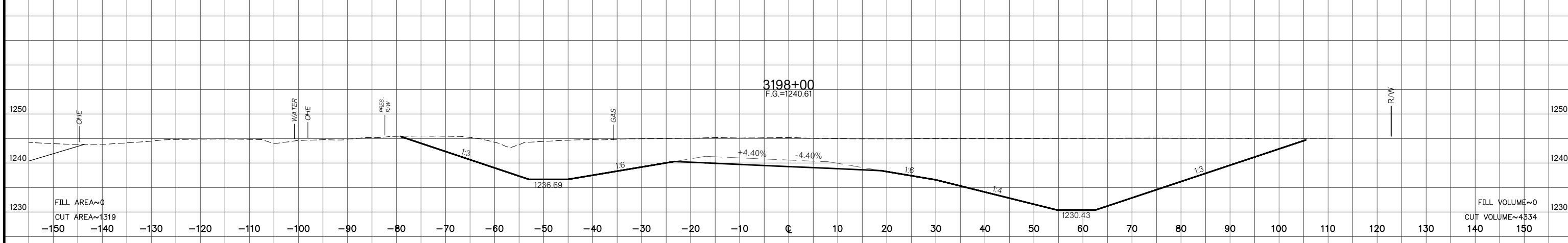
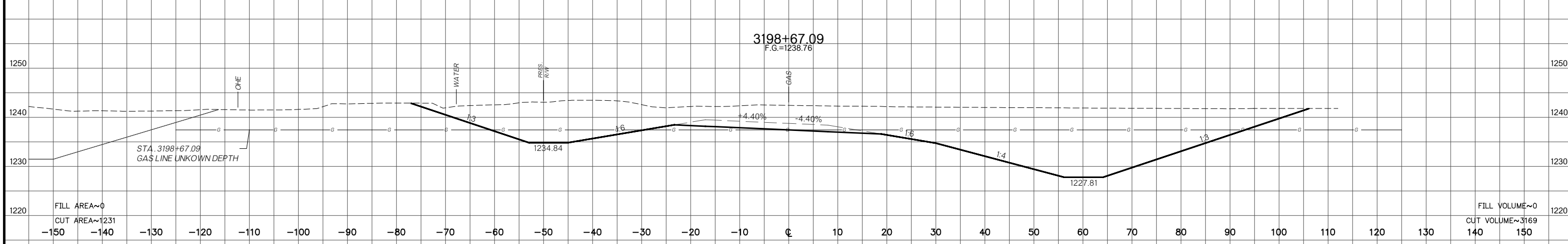
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

STA. 3199+00 RAMP K
CONST. SMD, TYPE 2A, 49' LT.
STUB 88 LF 30" RCP INTO STR. 74
TG=1231.01, f_i=1227.36, DS f_e=1227.09



STA. 3199+00 RAMP K
CONST. SMD, TYPE 2A, 49' LT.
STUB 104 LF 30" RCP INTO DITCH RT.
TG=1233.93, f_i=1227.09, DS f_e=1226.50

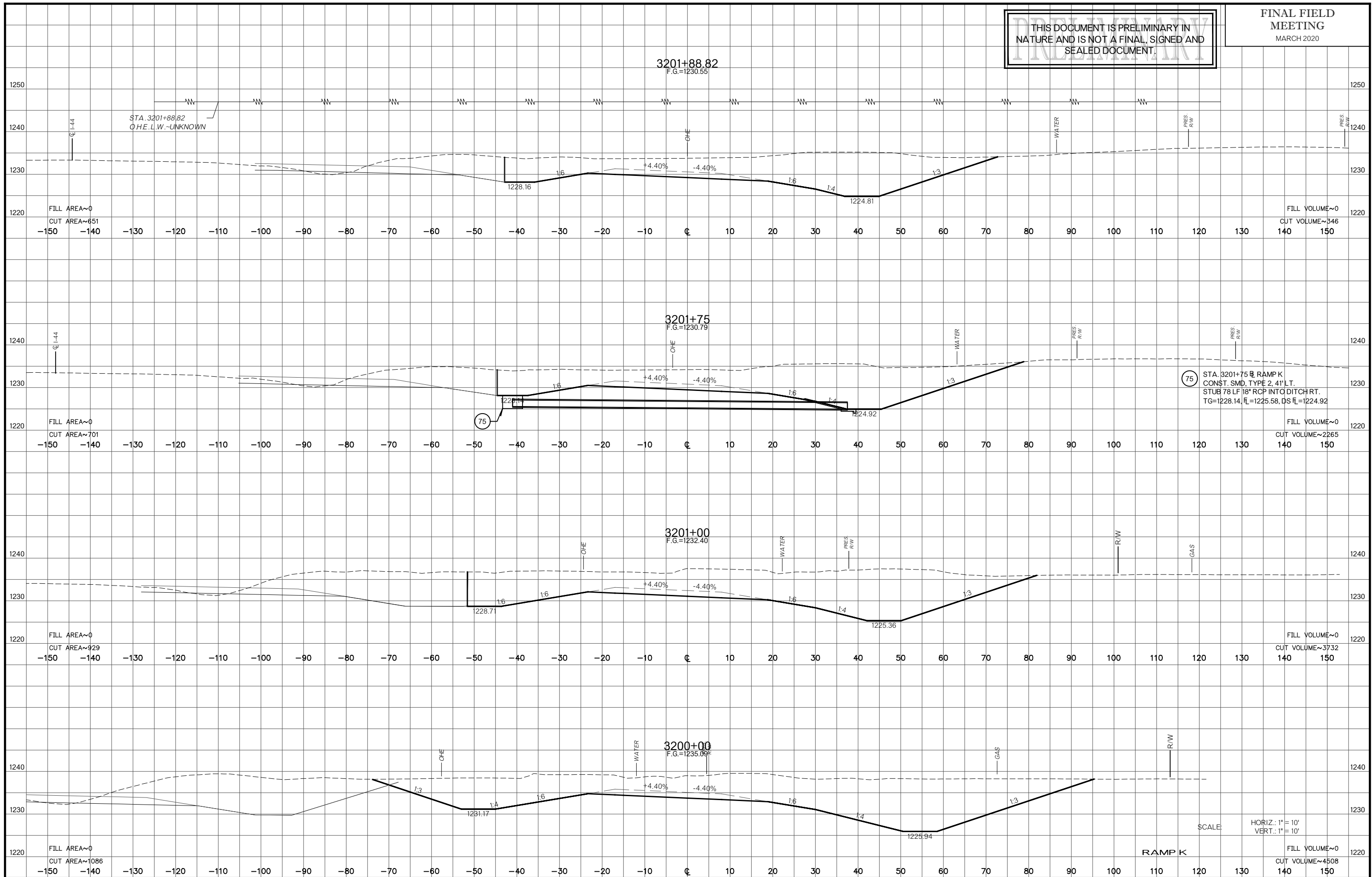


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP K
FILL VOLUME~0
CUT VOLUME~3321

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

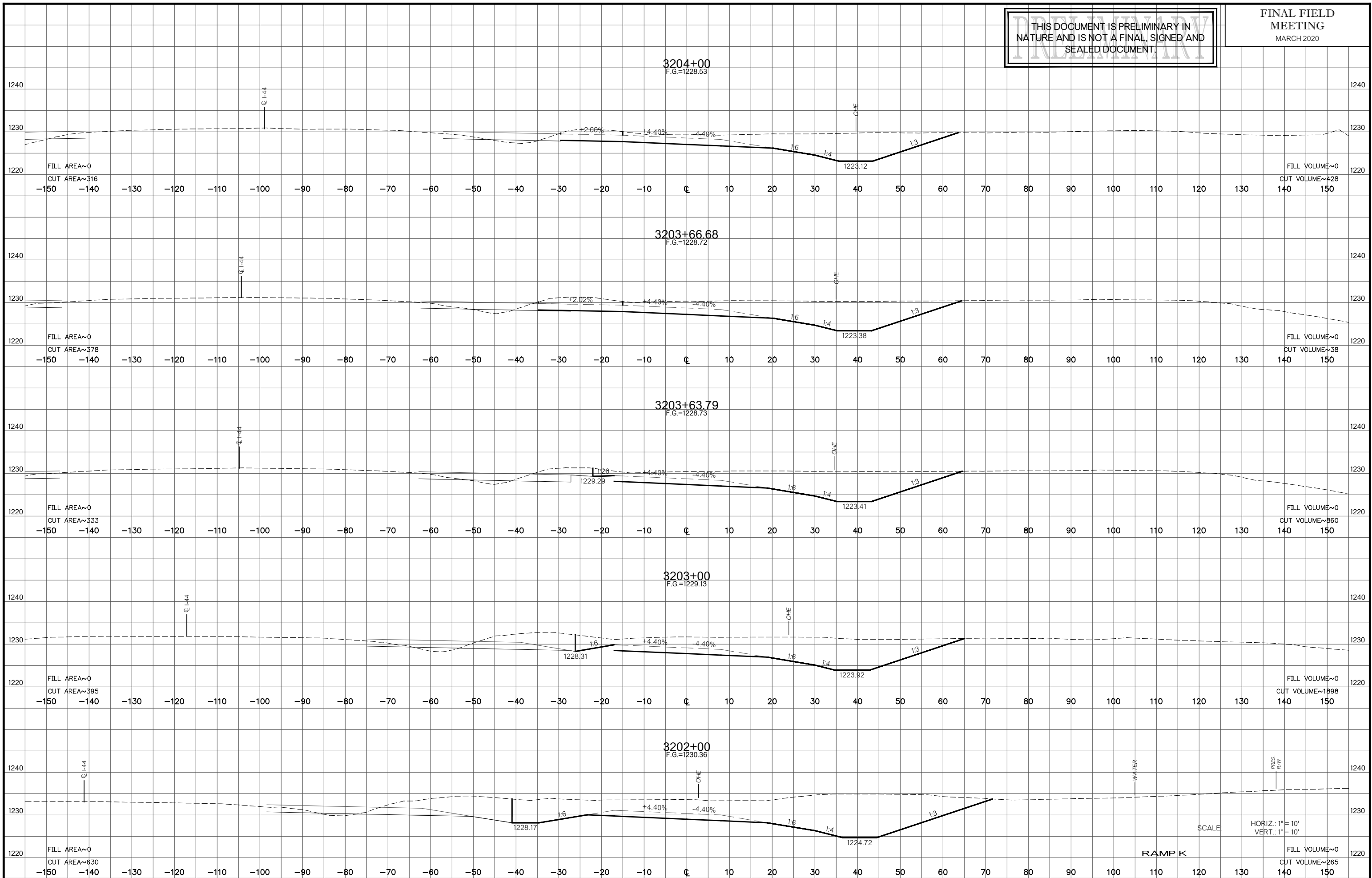


SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP K
FILL VOLUME~0
CUT VOLUME~4508

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



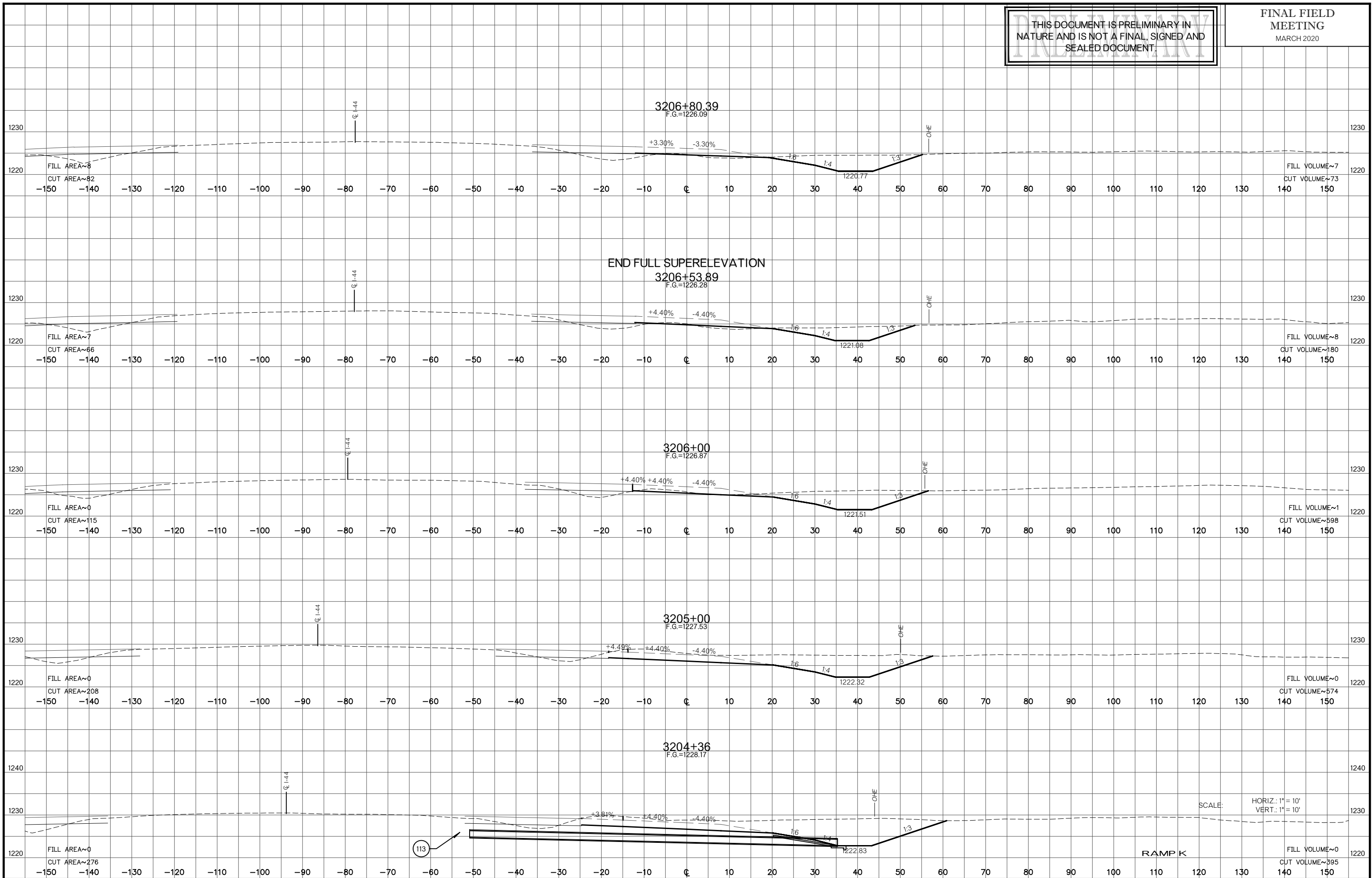
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP K

State Job No. 24428(04) Sheet No. X335

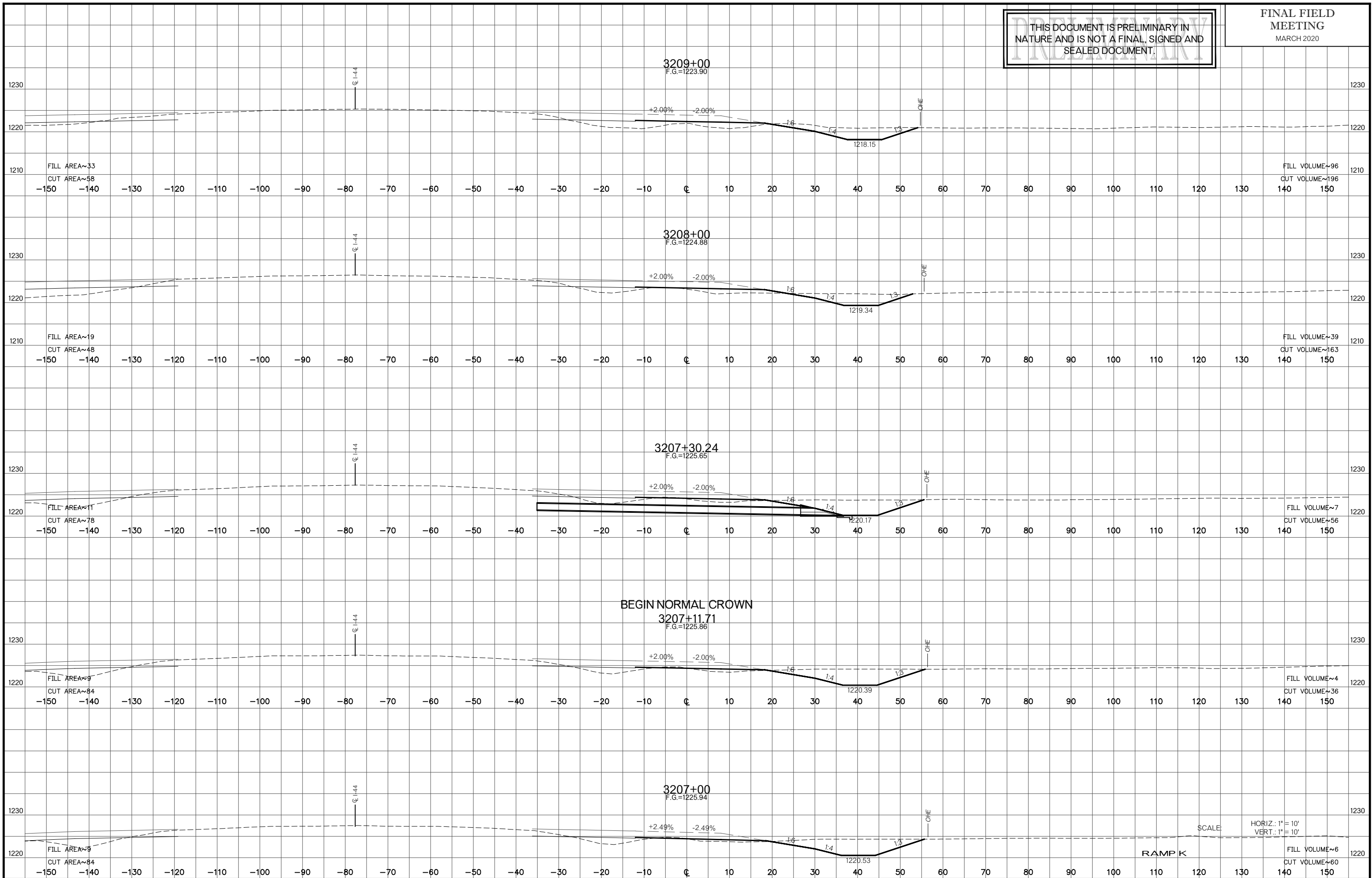
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



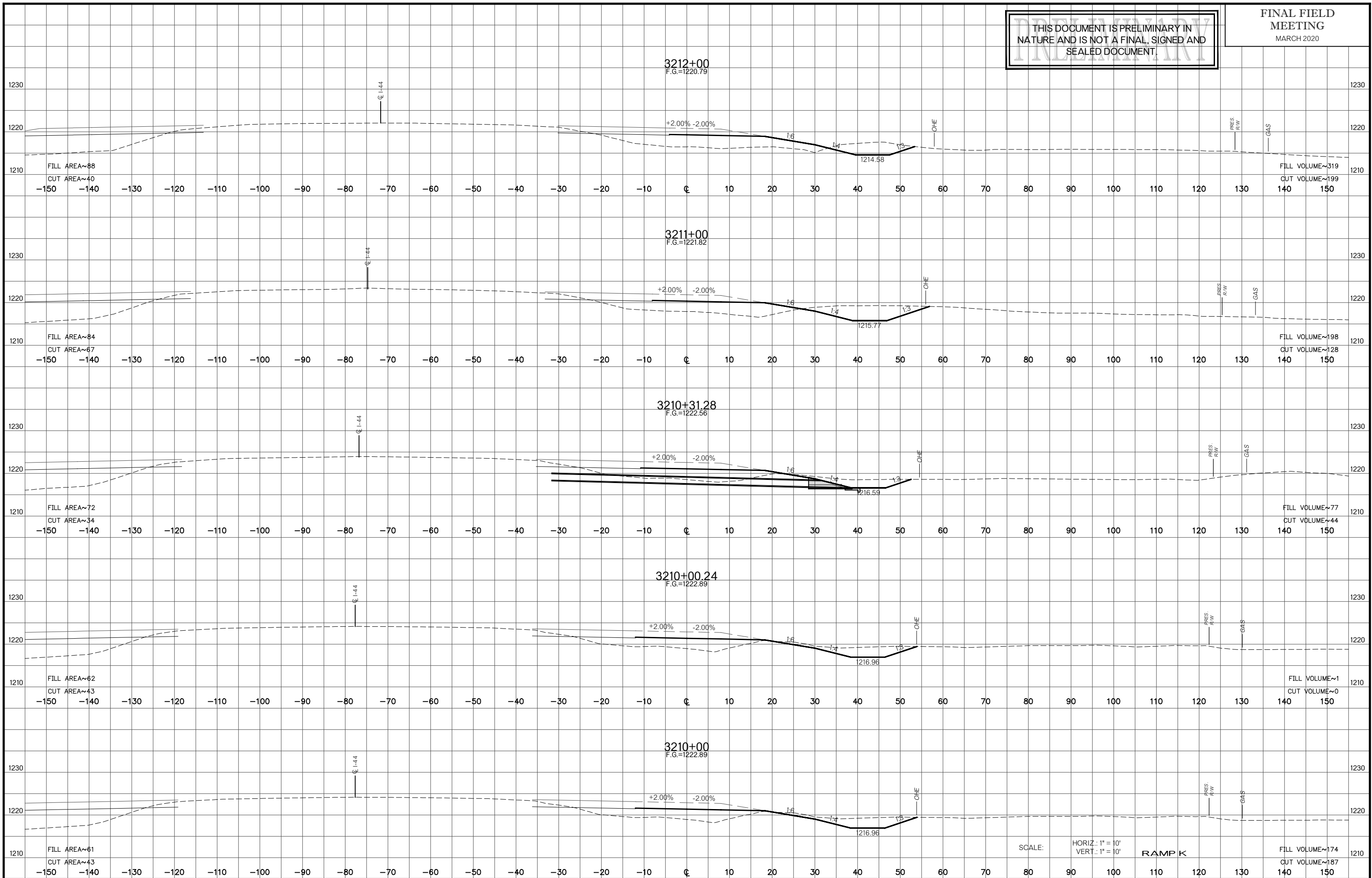
BEGIN NORMAL CROWN

SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP K

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

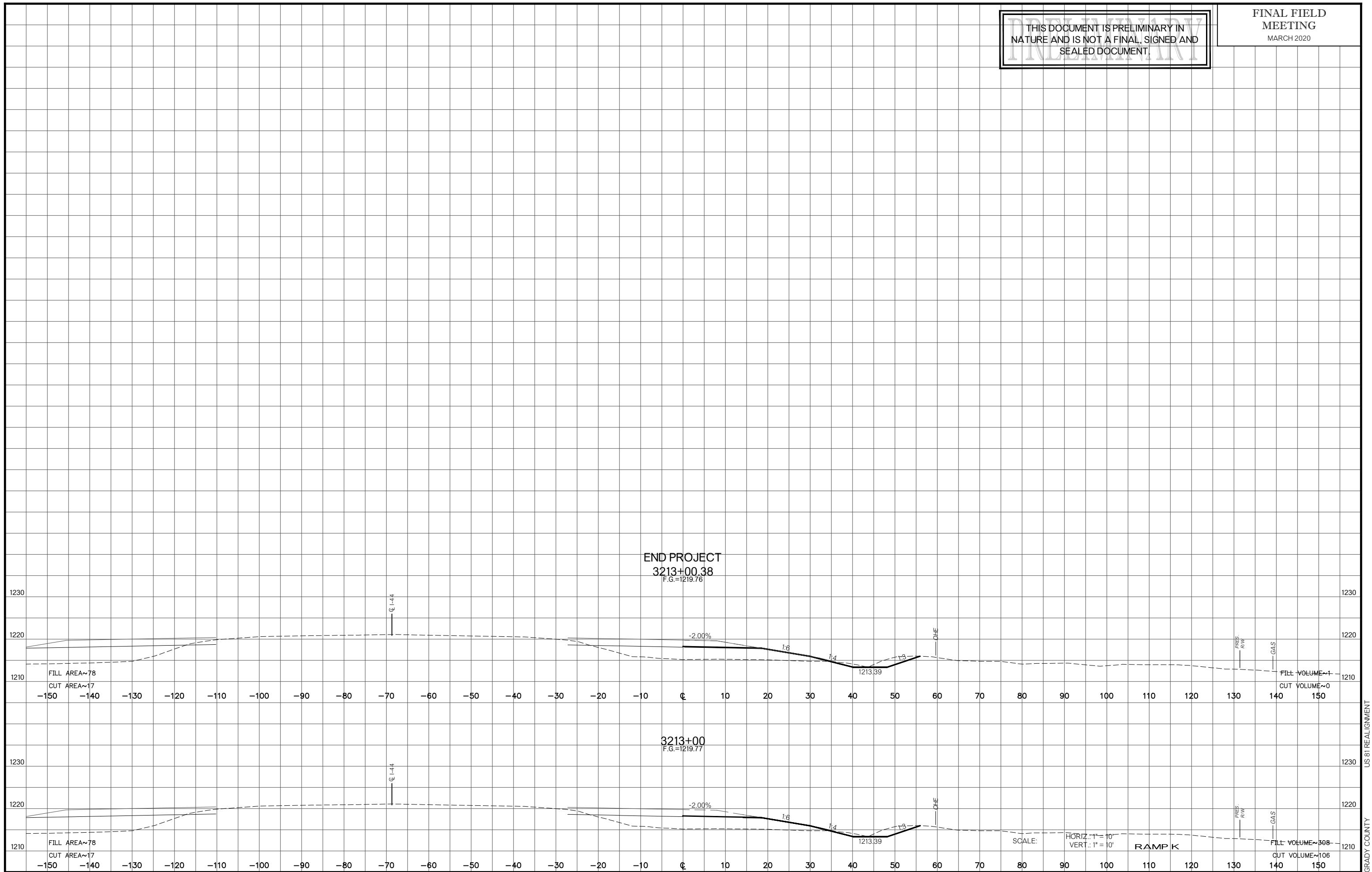
RAMP K

FILL VOLUME~174
CUT VOLUME~187

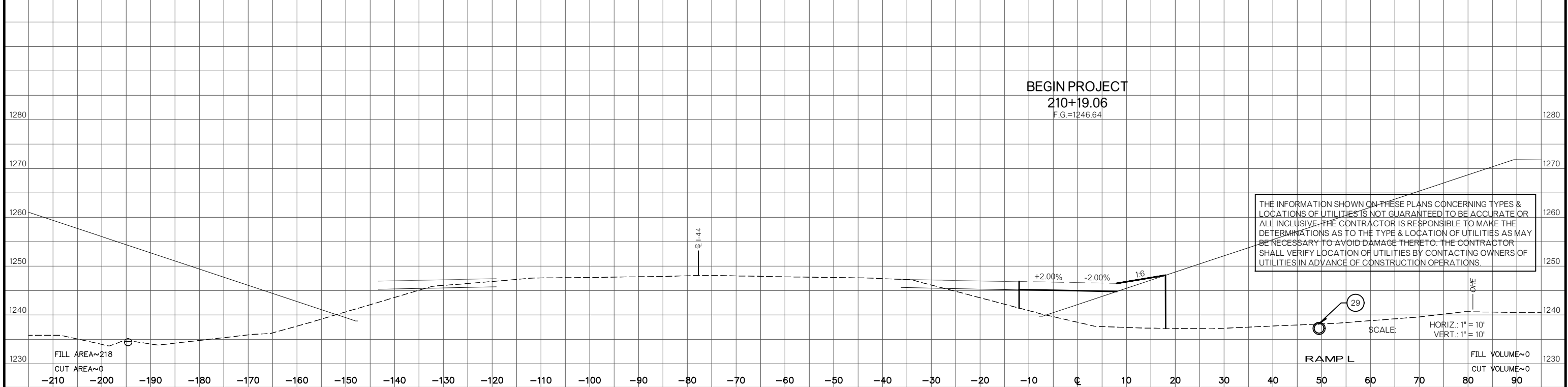
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

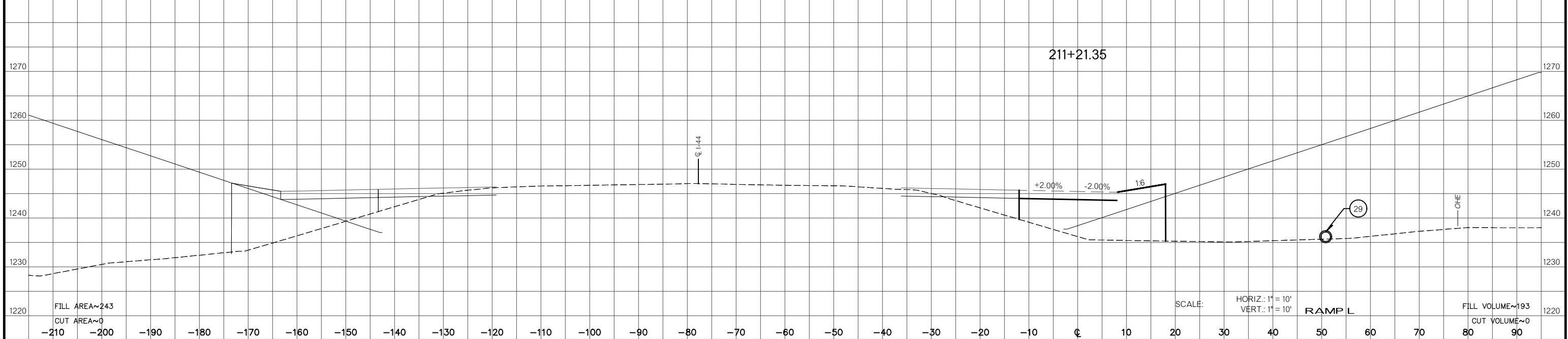
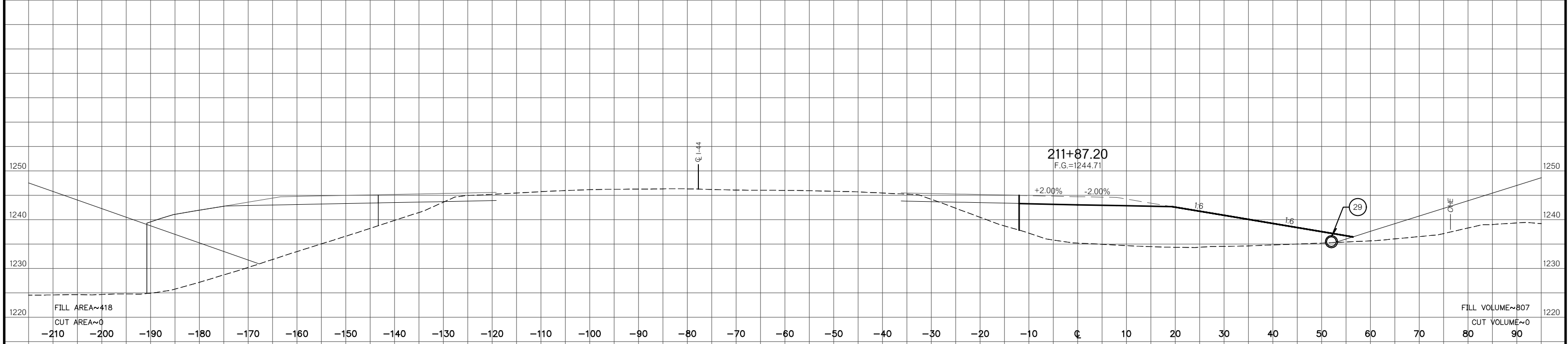
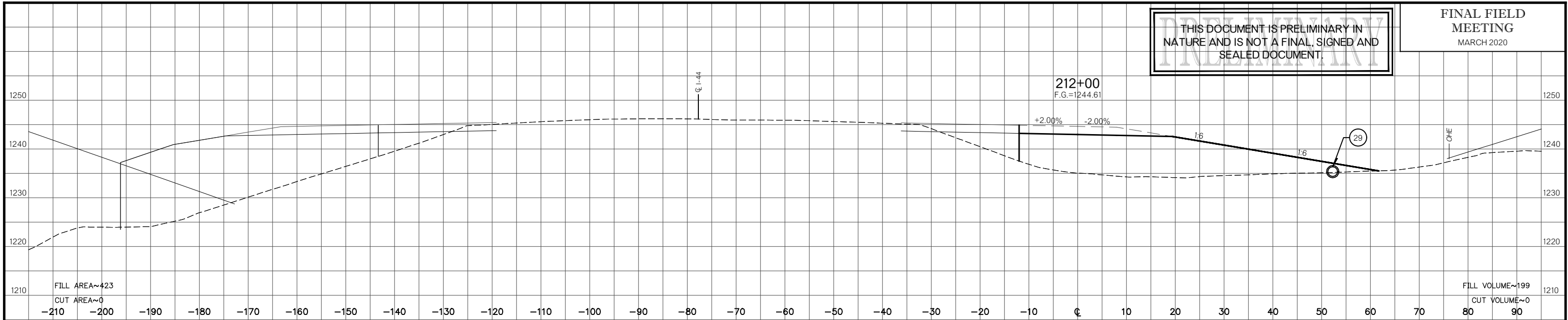
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP L
FILL VOLUME~0
CUT VOLUME~0

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

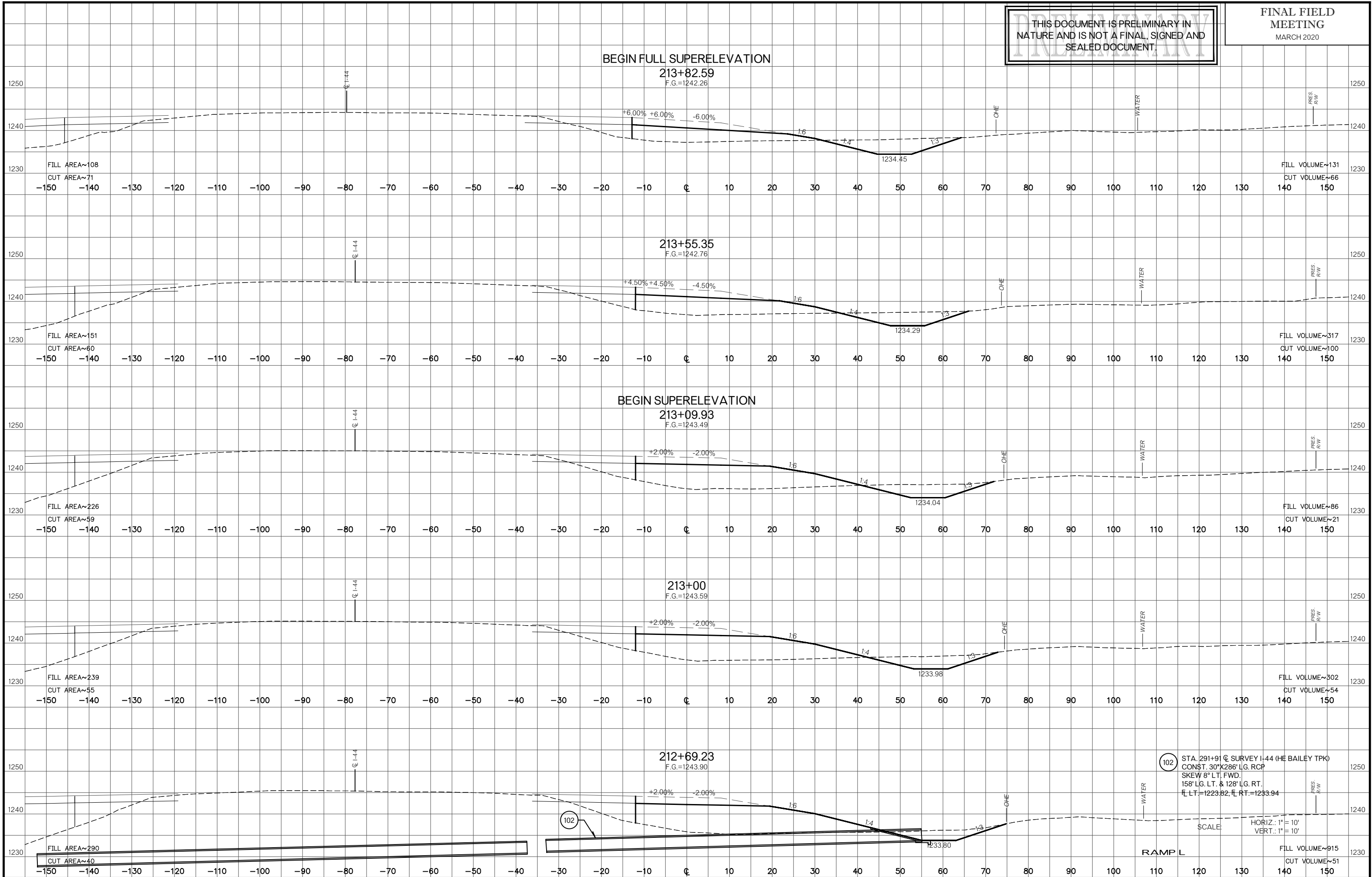


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



BEGIN FULL SUPERELEVATION

213+82.59
F.G.=1242.26

+6.00% +6.00% -6.00%

FILL VOLUME~131
CUT VOLUME~66

BEGIN SUPERELEVATION

213+55.35
F.G.=1242.76

+4.50% +4.50% -4.50%

FILL VOLUME~317
CUT VOLUME~100

BEGIN SUPERELEVATION

213+09.93
F.G.=1243.49

+2.00% -2.00%

FILL VOLUME~86
CUT VOLUME~21

213+00
F.G.=1243.59

+2.00% -2.00%

FILL VOLUME~302
CUT VOLUME~54

212+69.23
F.G.=1243.90

+2.00% -2.00%

FILL VOLUME~915
CUT VOLUME~51

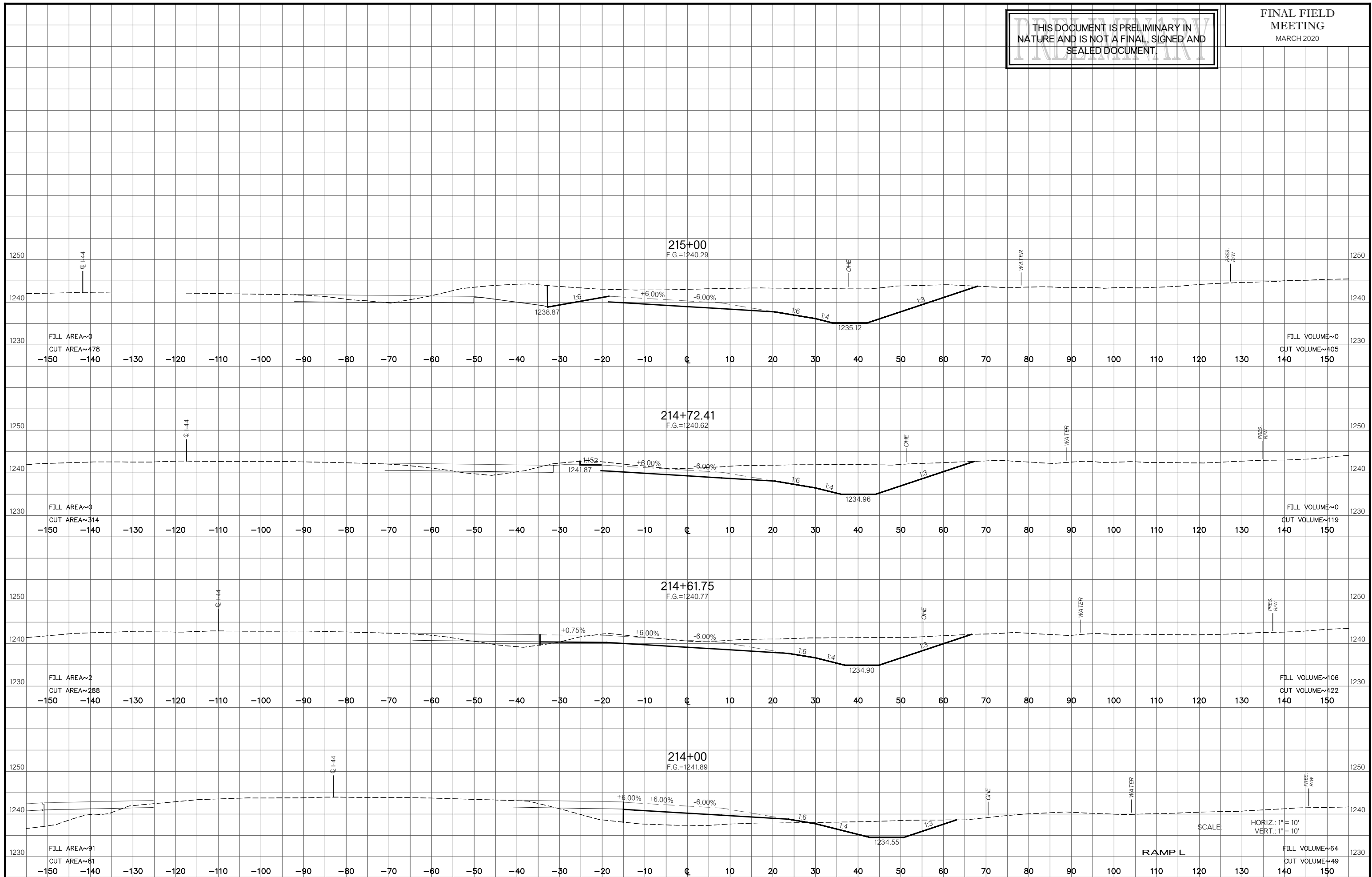
102 STA. 291+91 @ SURVEY I-44 (HE BAILEY TPK)
CONST. 30'X286' LG. RCP
SKEW 8° LT. FWD.
158' LG. LT. & 128' LG. RT.
FL LT.=1223.82, FL RT.=1233.94

SCALE HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP L

US 81 REALIGNMENT GRADY COUNTY

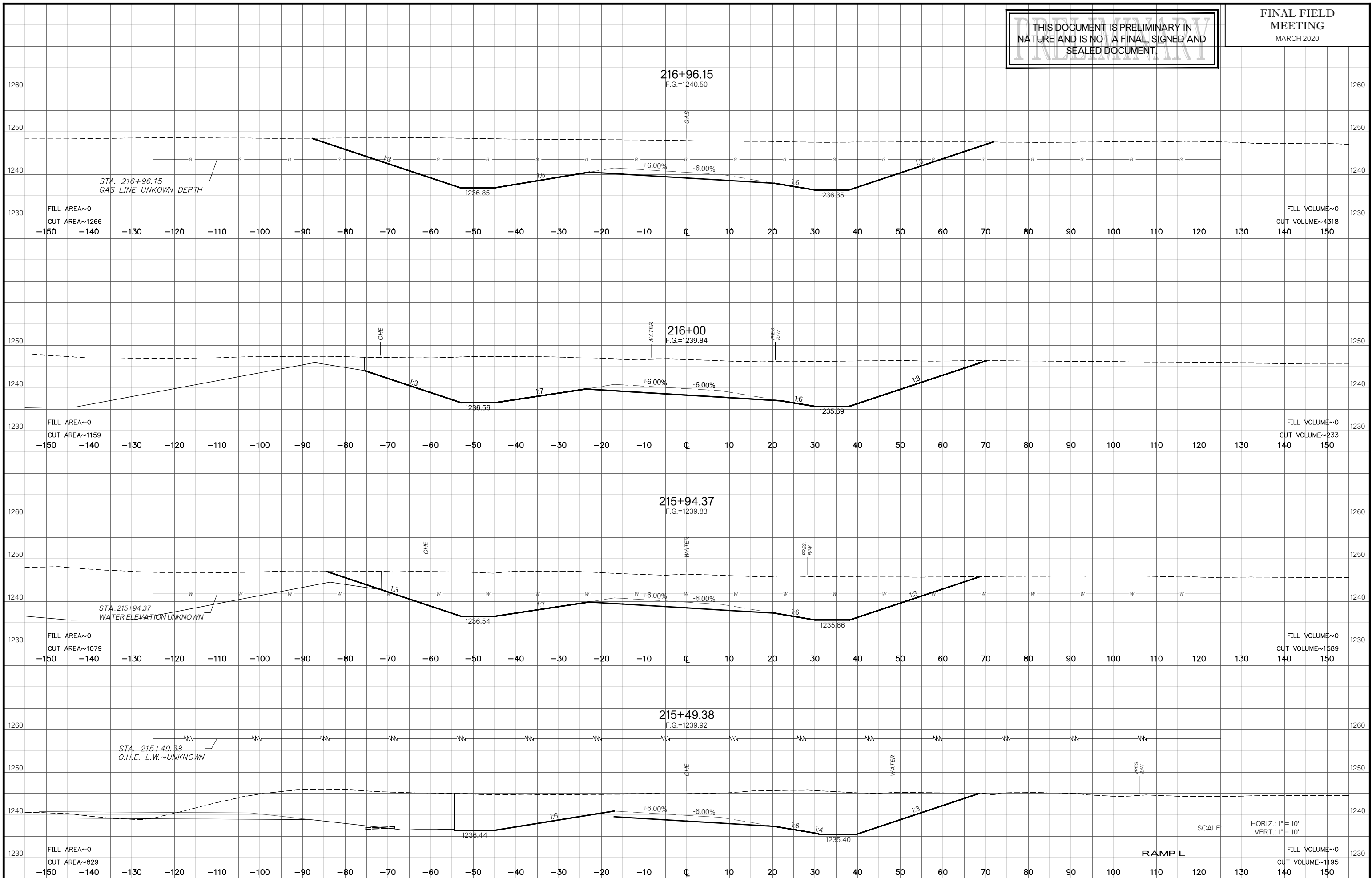
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'
RAMP L
State Job No. 24428(04) Sheet No. X343

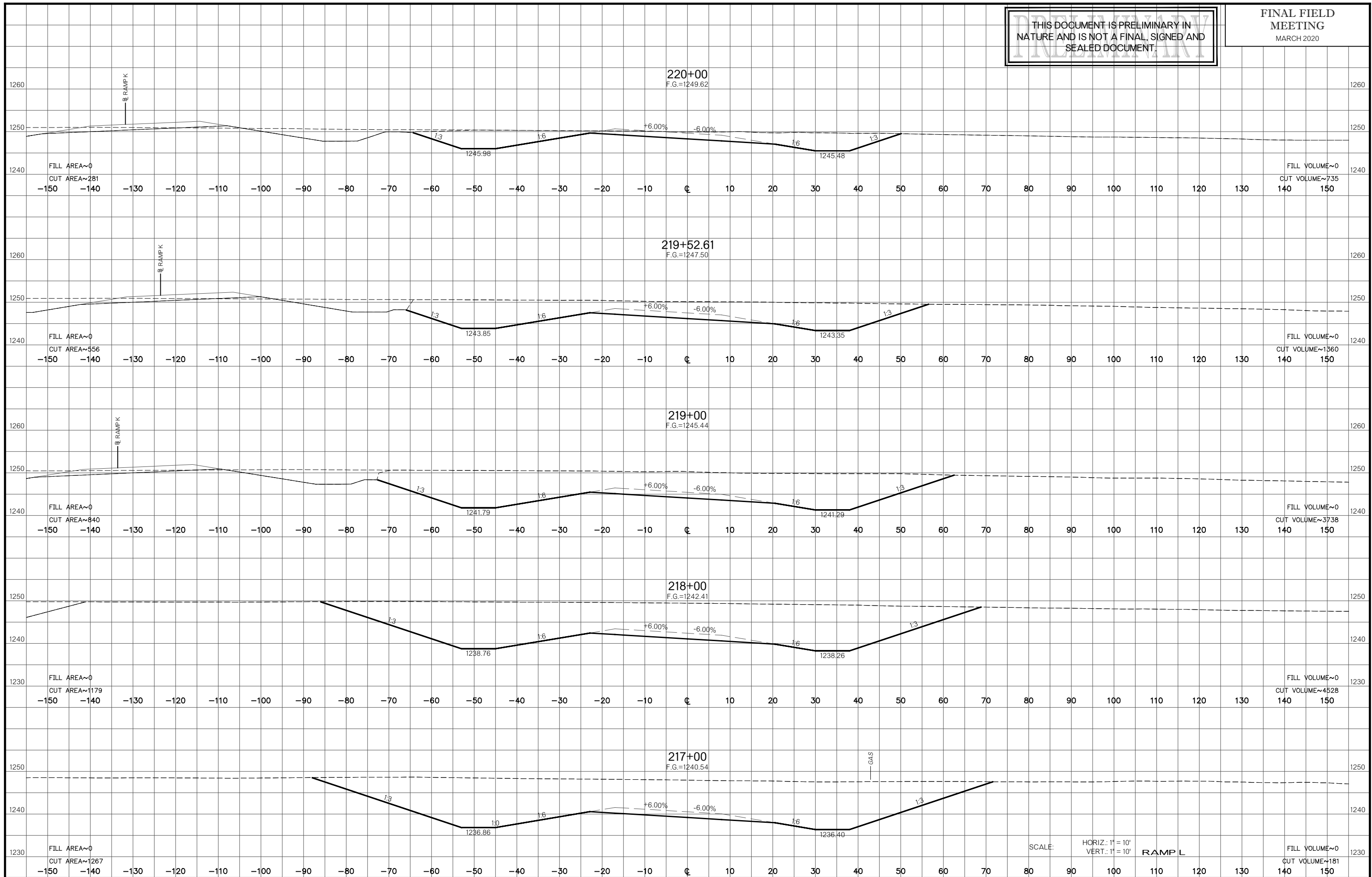
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



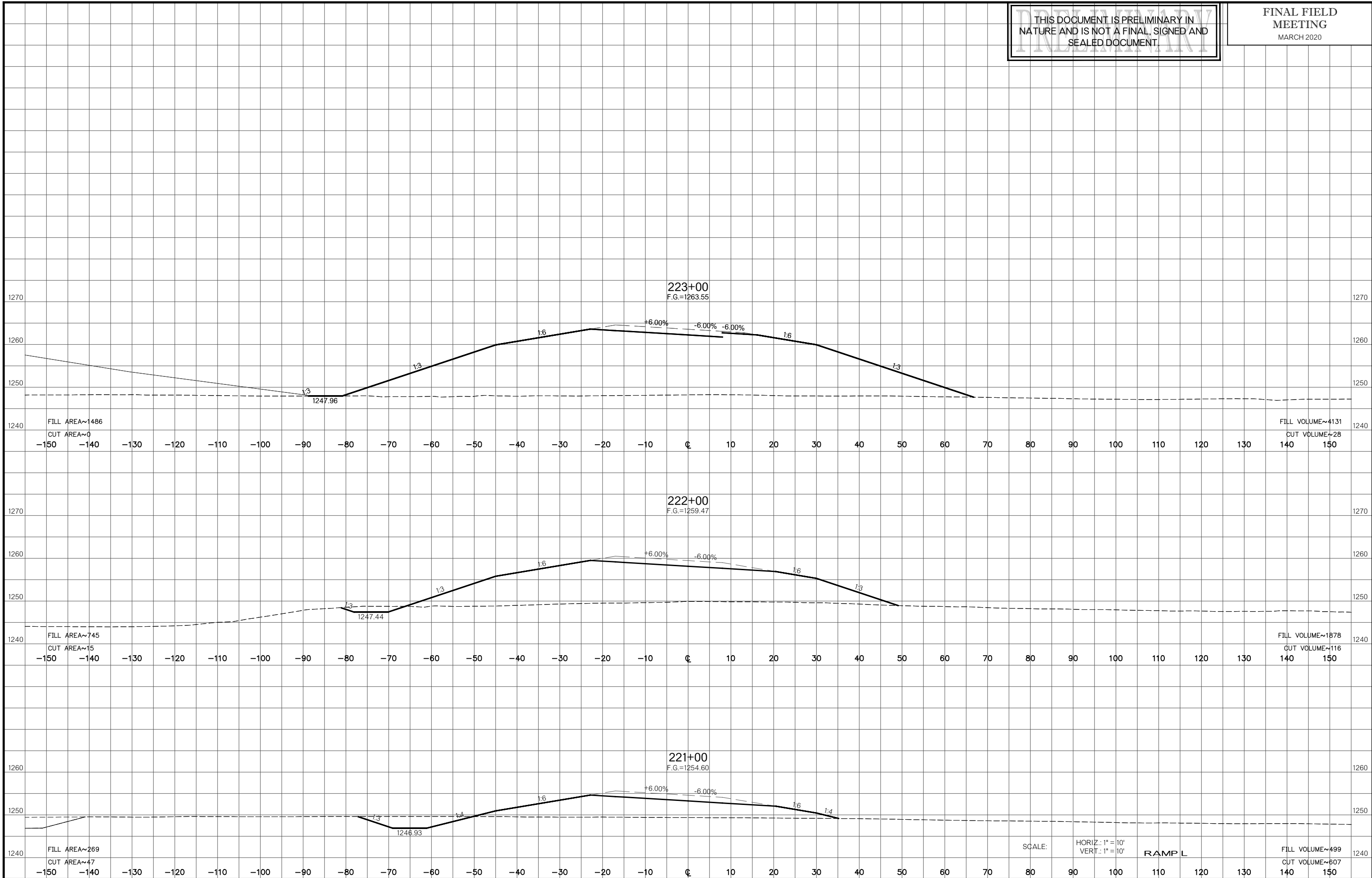
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP L

US 81 REALIGNMENT
GRADY COUNTY

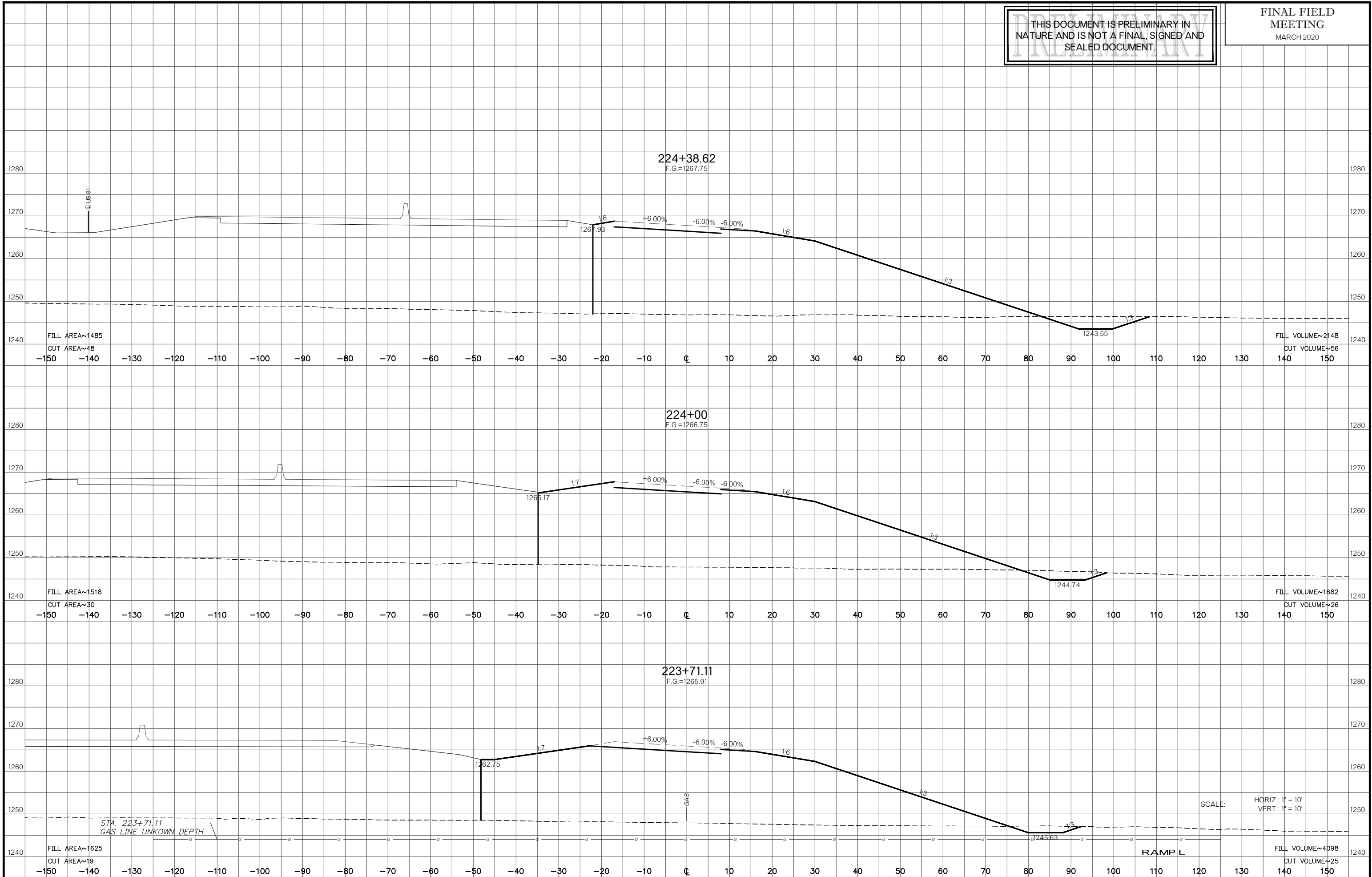
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



US 81 REALIGNMENT GRADY COUNTY

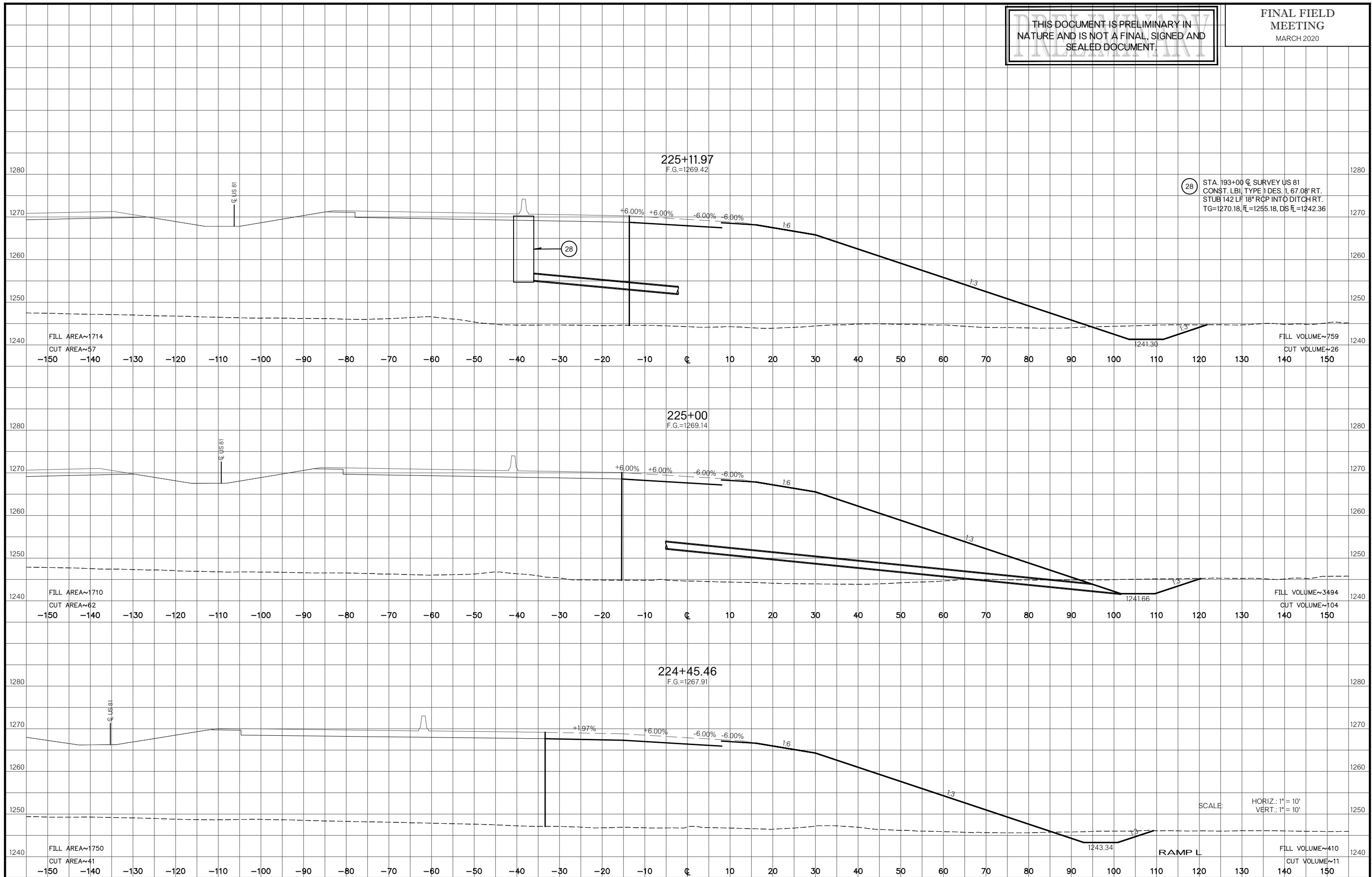
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

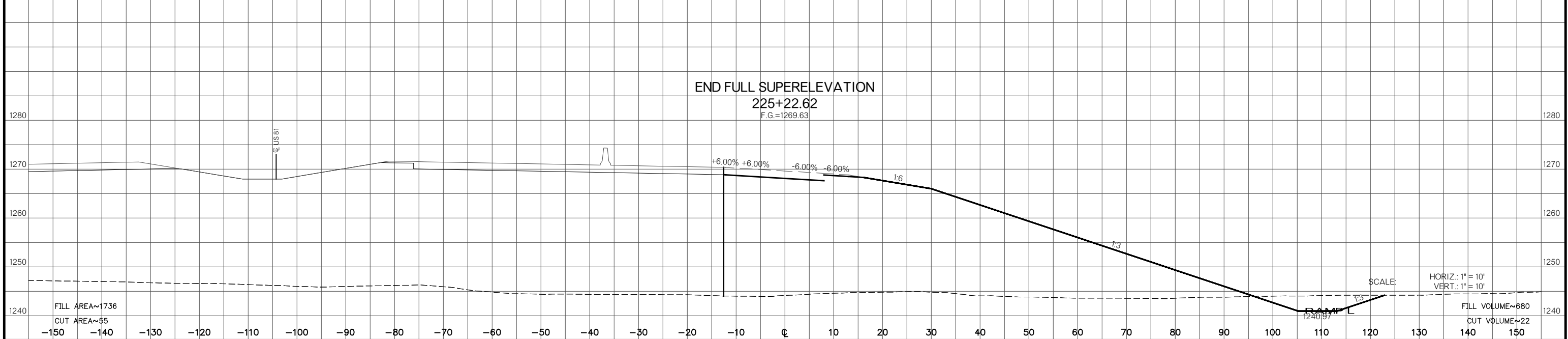
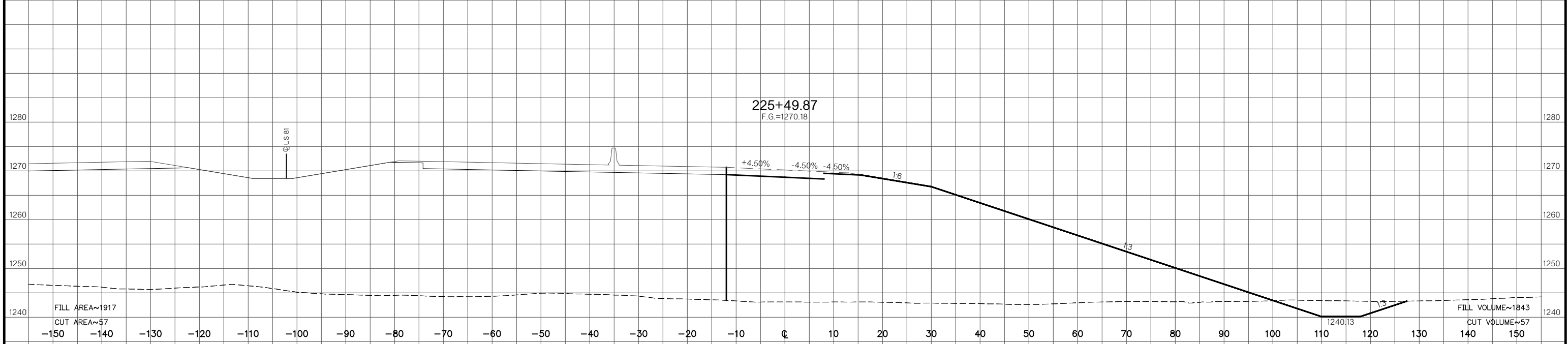
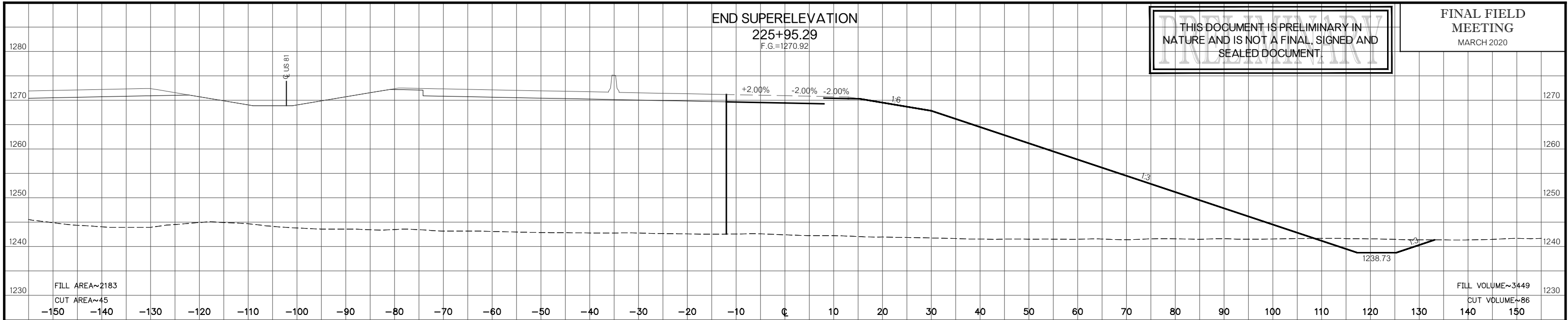
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

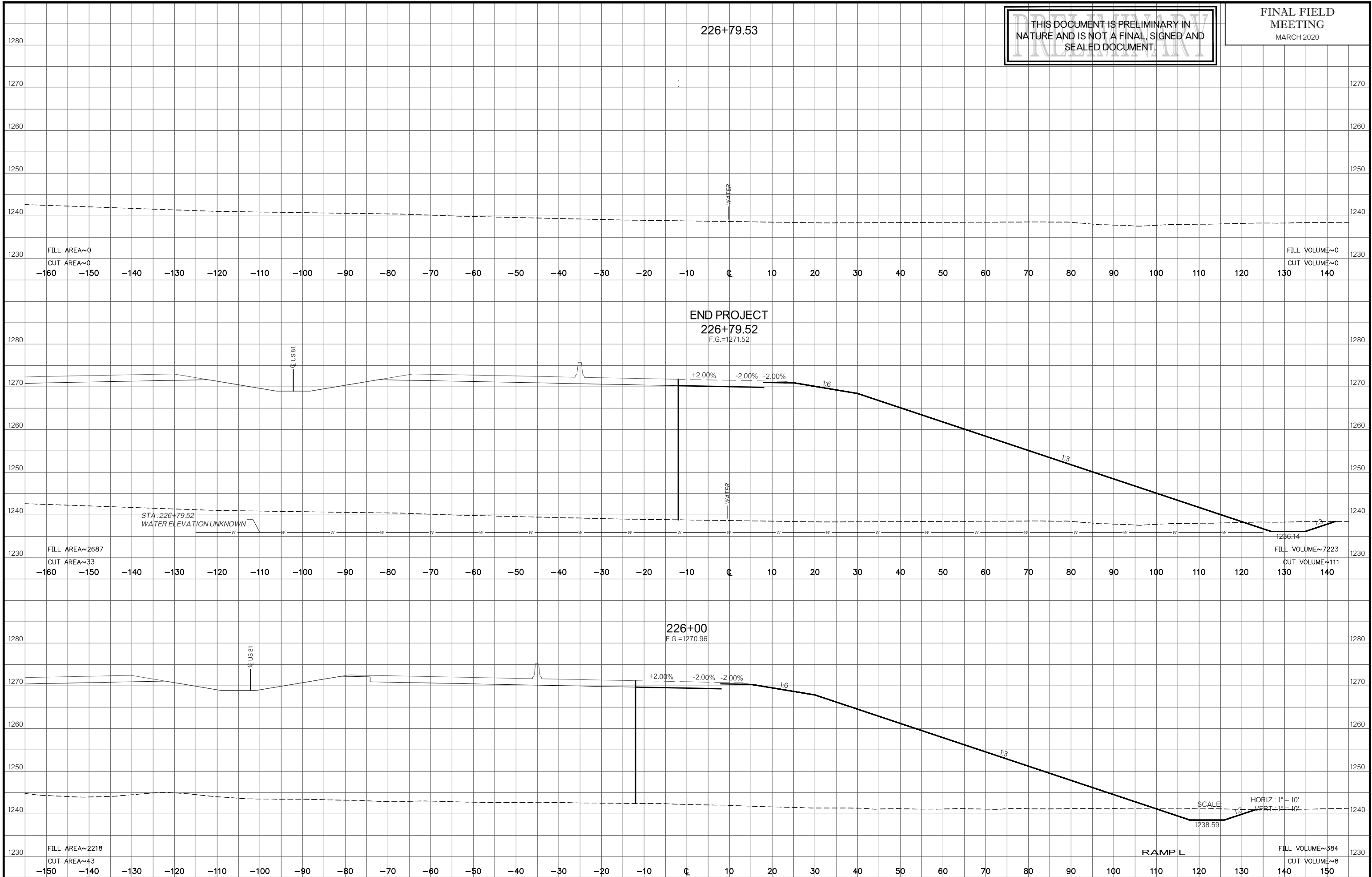
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



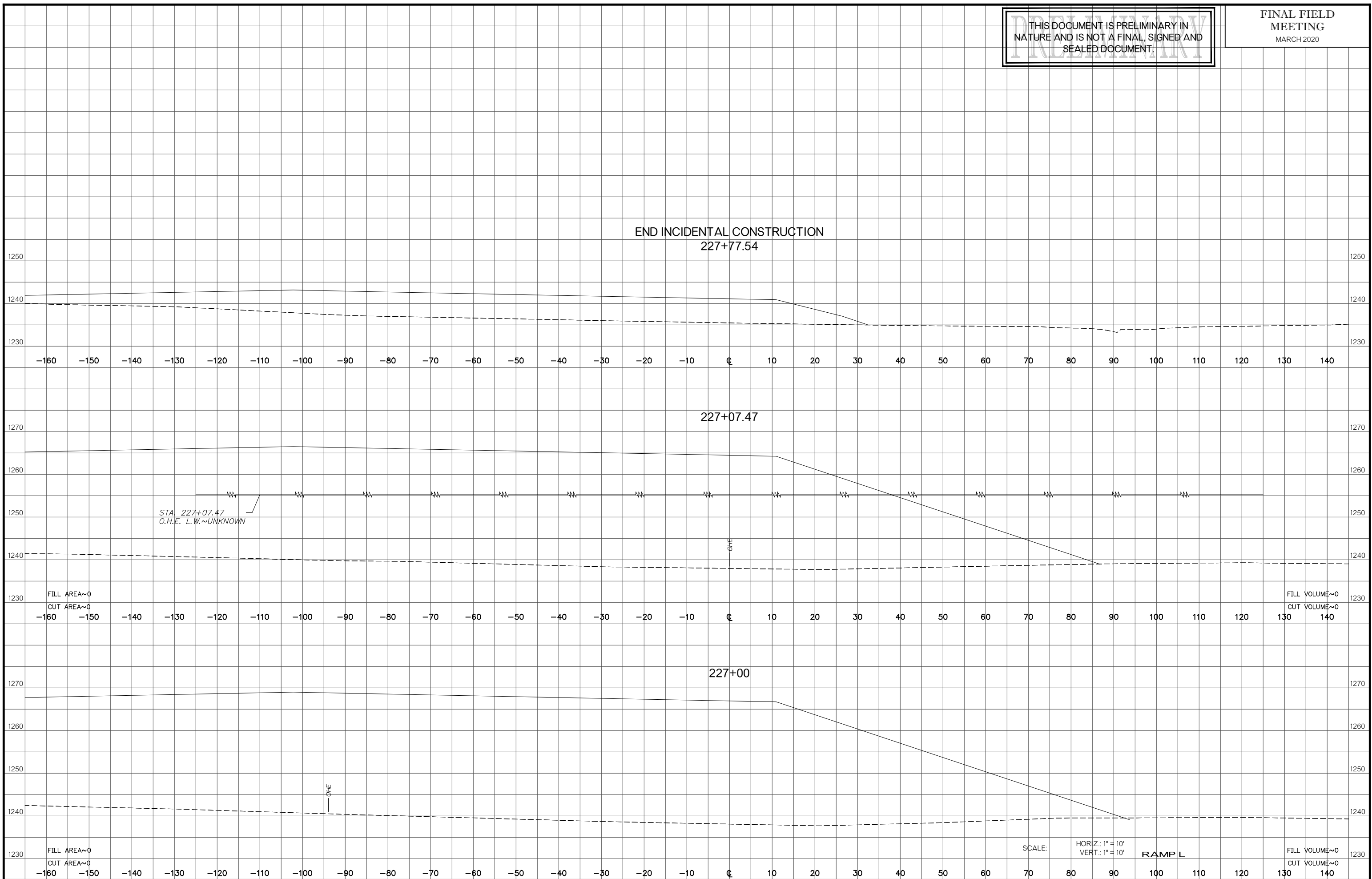
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



END INCIDENTAL CONSTRUCTION
227+77.54

227+07.47

STA. 227+07.47
O.H.E. L.W.~UNKNOWN

227+00

FILL AREA~0
CUT AREA~0

FILL VOLUME~0
CUT VOLUME~0

FILL AREA~0
CUT AREA~0

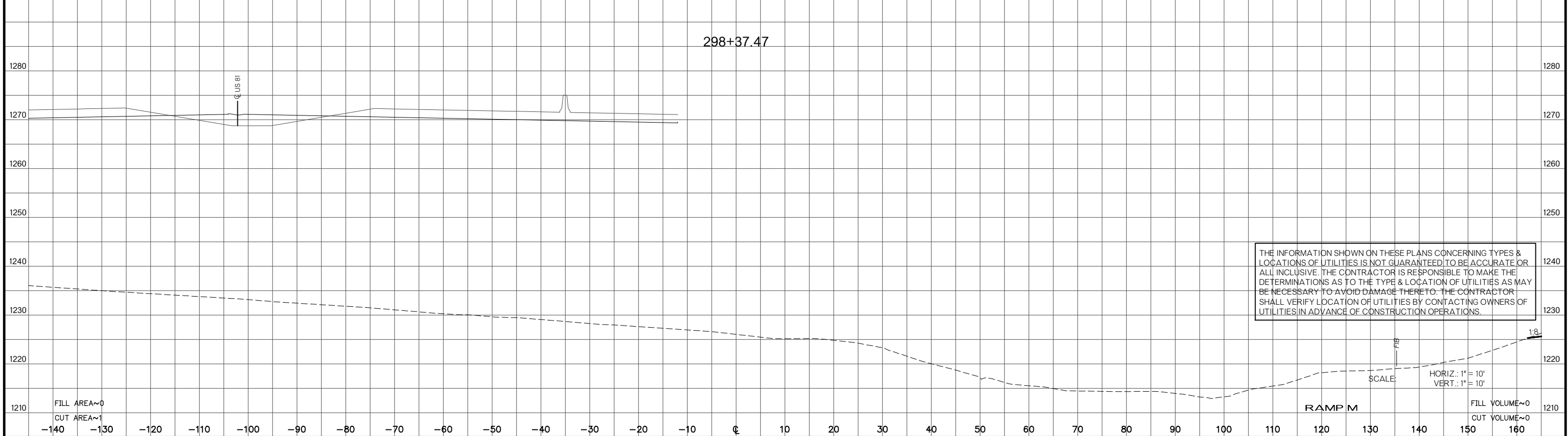
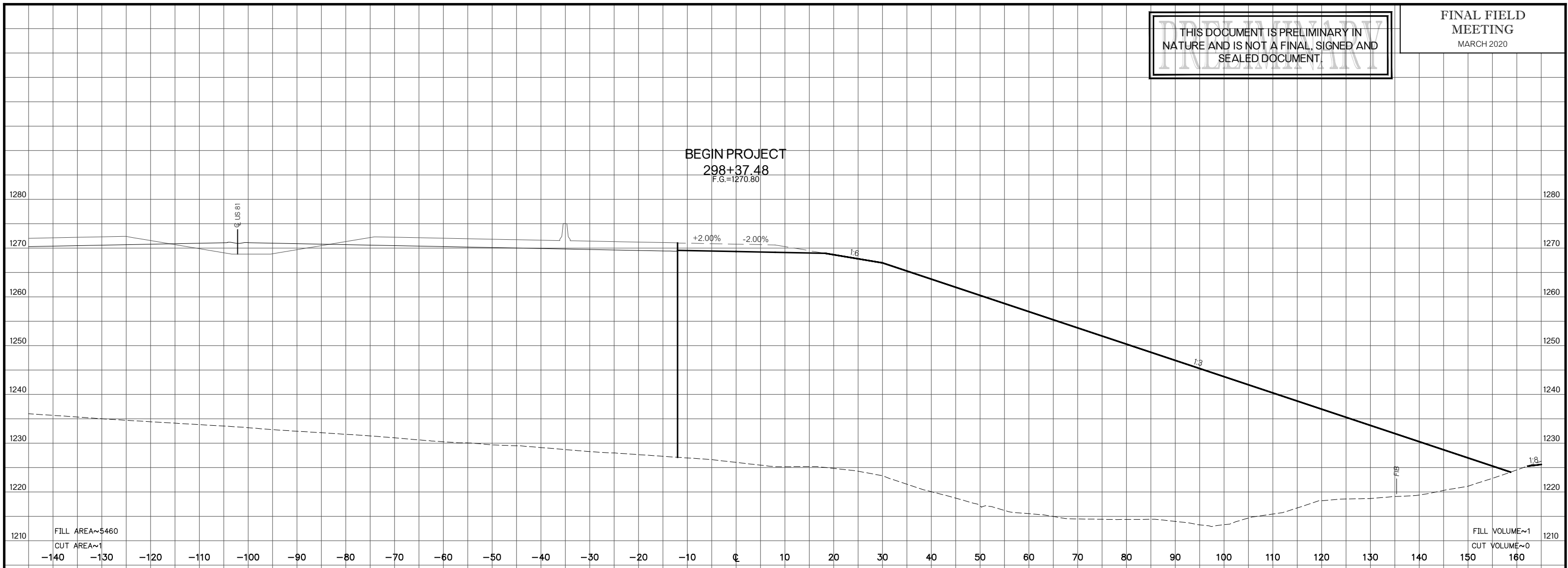
FILL VOLUME~0
CUT VOLUME~0

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP L

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

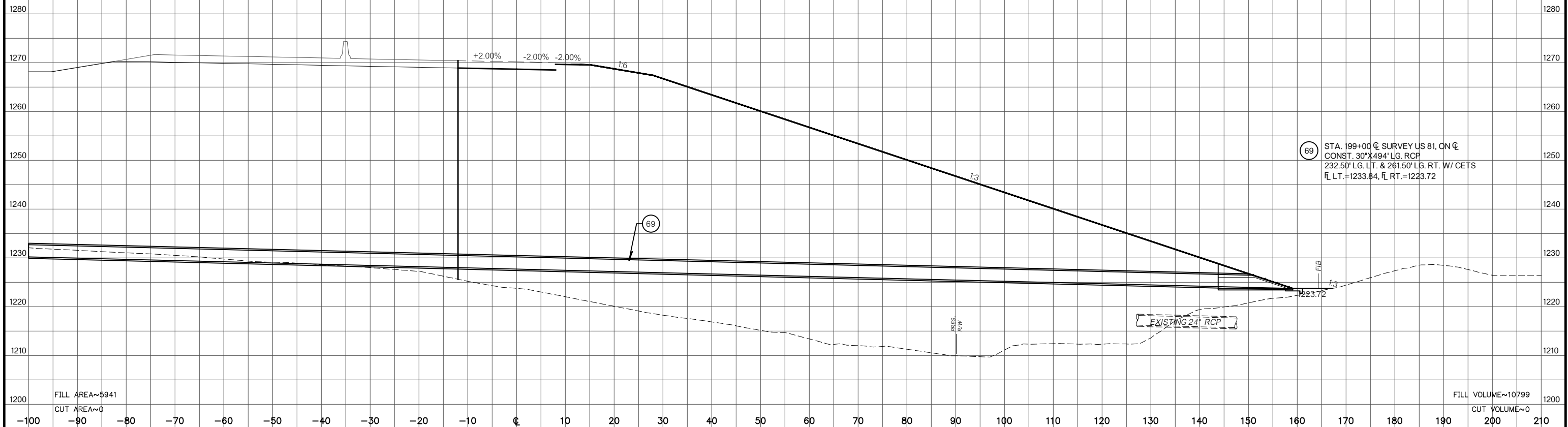
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP M

US 81 REALIGNMENT
GRADY COUNTY

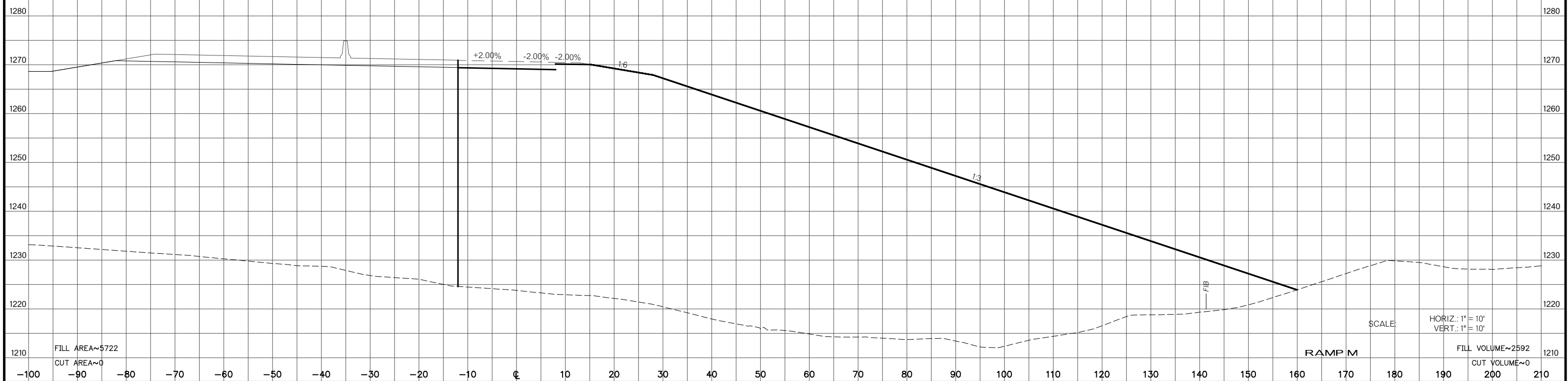
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

299+00
F.G.=1270.16



(69) STA. 199+00 @ SURVEY US 81, ON @ CONST. 30"X494' LG. RCP
232.50' LG. LT. & 261.50' LG. RT. W/ CETS
FL LT.=1233.84, FL RT.=1223.72

298+50
F.G.=1270.67



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP M

FILL VOLUME~2592
CUT VOLUME~0

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

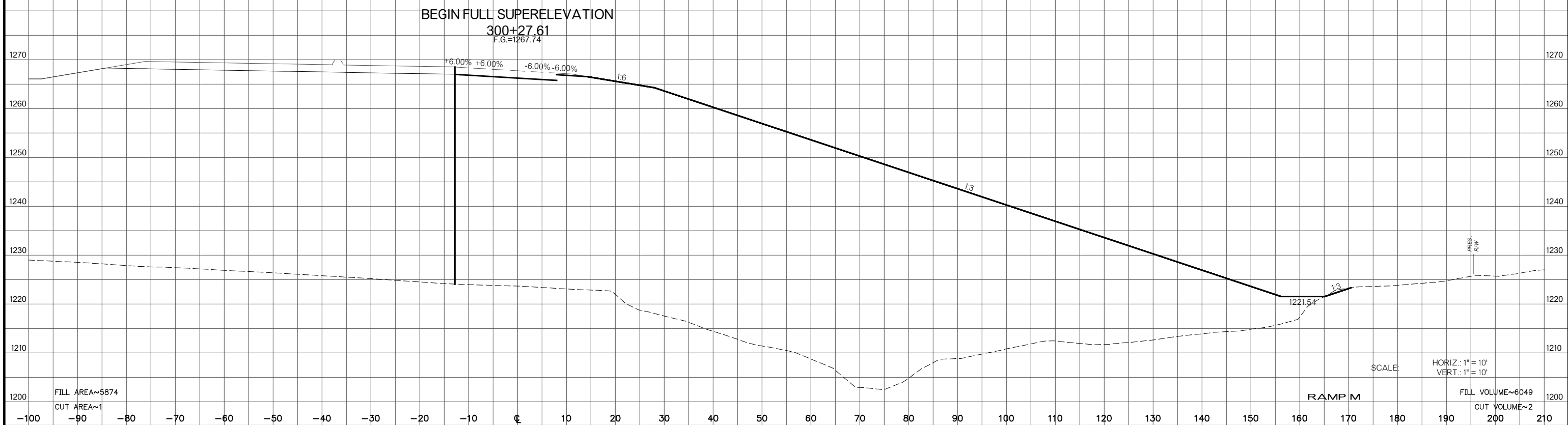
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

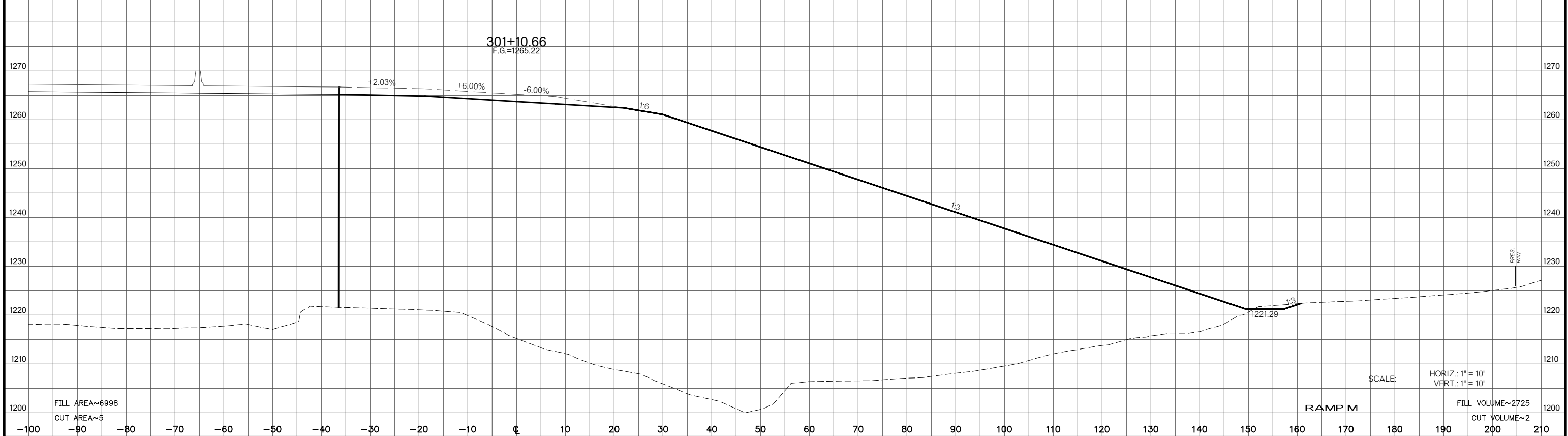
FINAL FIELD MEETING
MARCH 2020



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

State Job No. 24428(04) Sheet No. X355

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

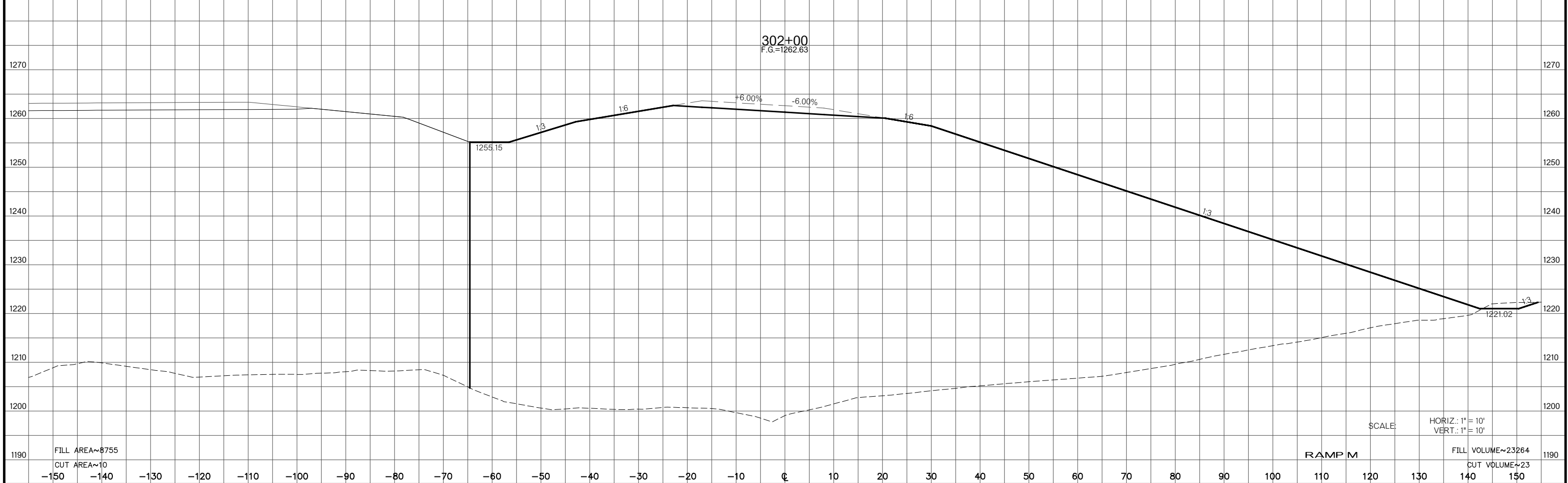
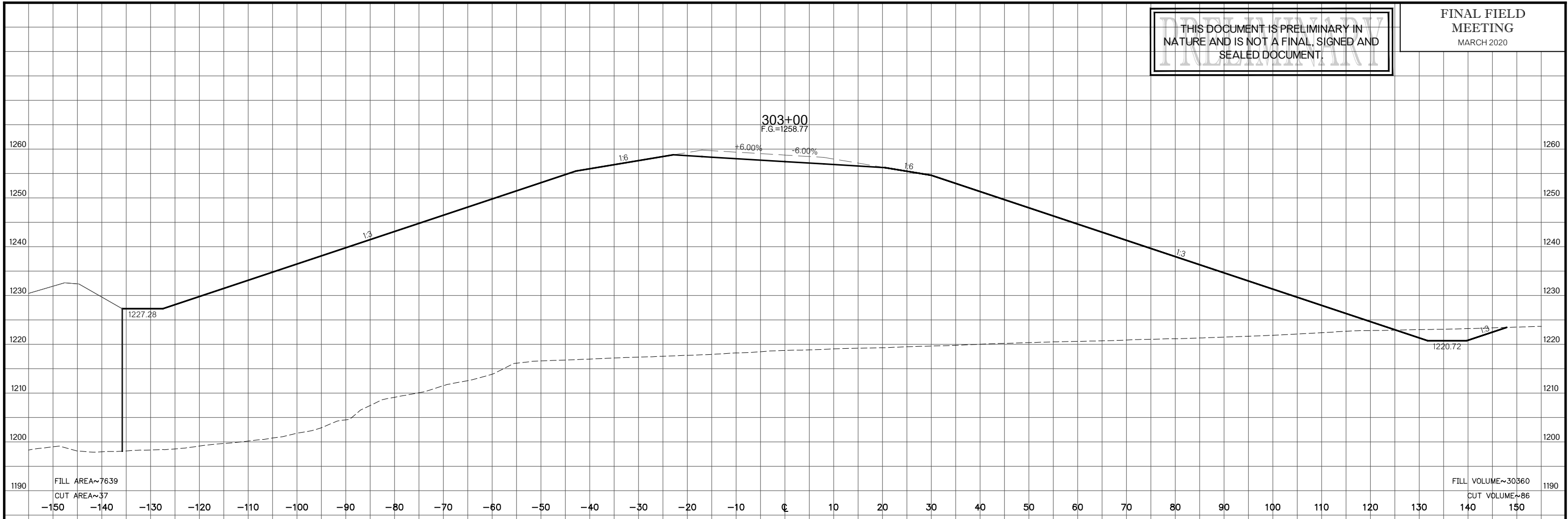


SCALE
RAMP M
HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

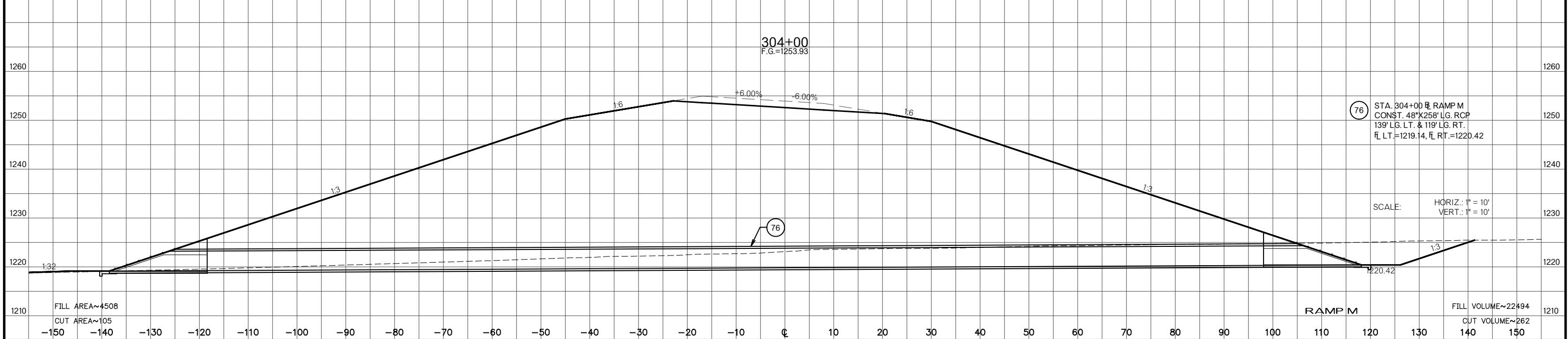
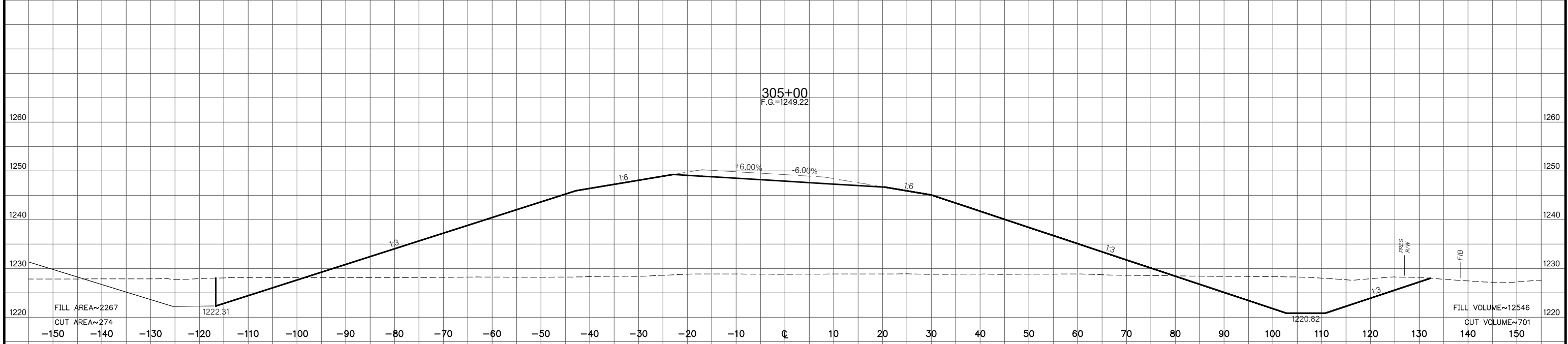
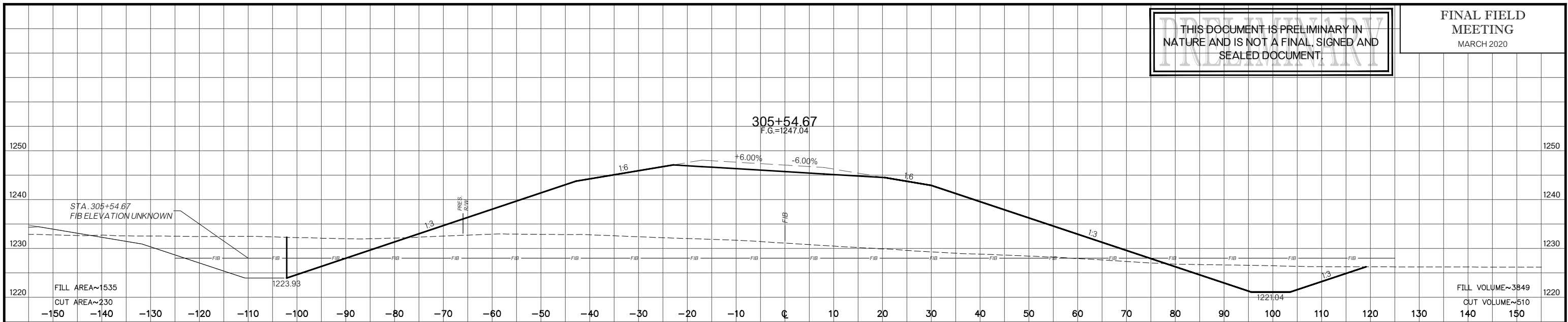
RAMP M

State Job No. 24428(04) Sheet No. X357

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

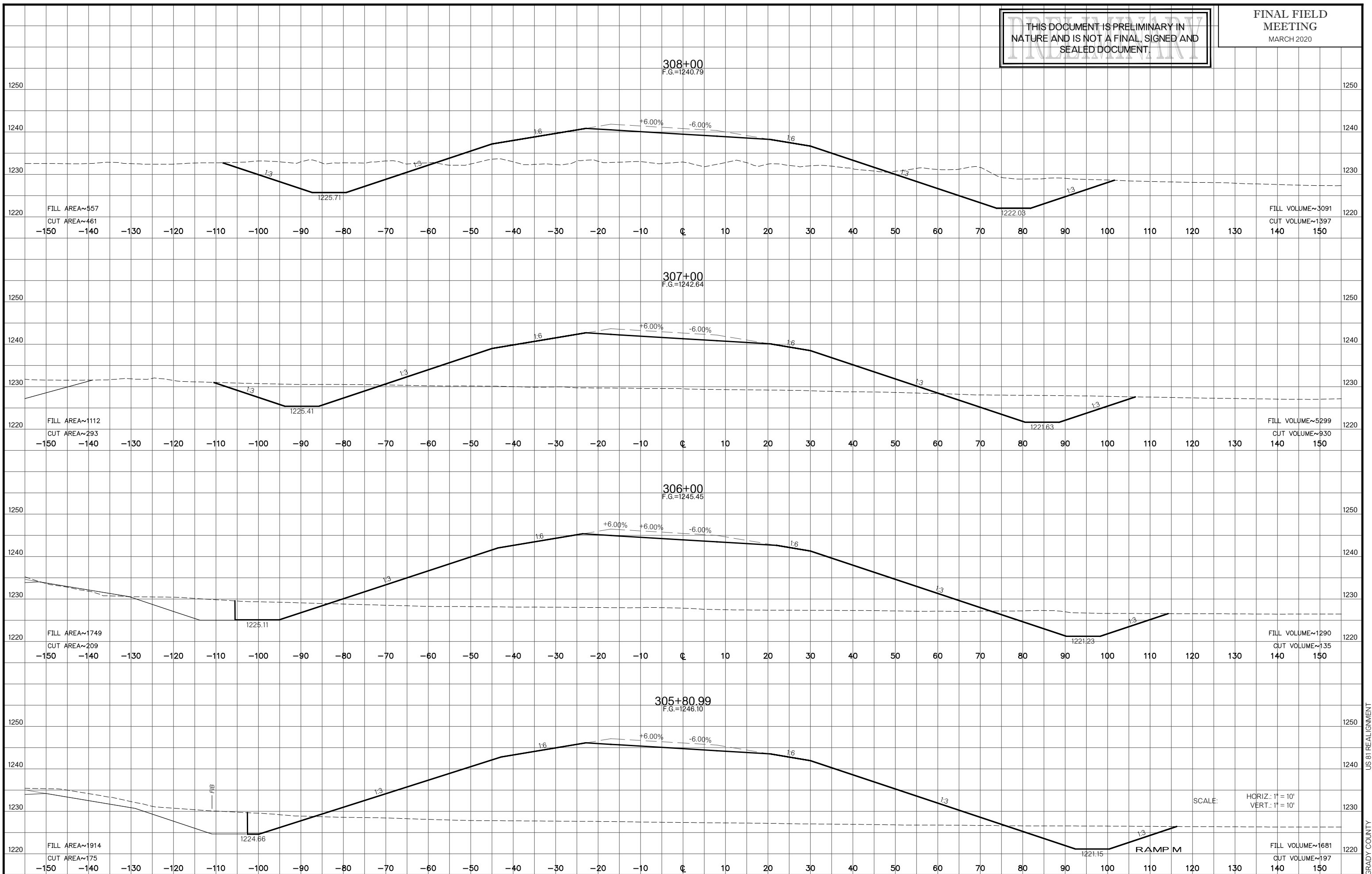


(76) STA. 304+00 RAMP M
CONST. 48"X258" LG. RCP
139' LG. LT. & 119' LG. RT.
FL LT.=1219.14, FL RT.=1220.42

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

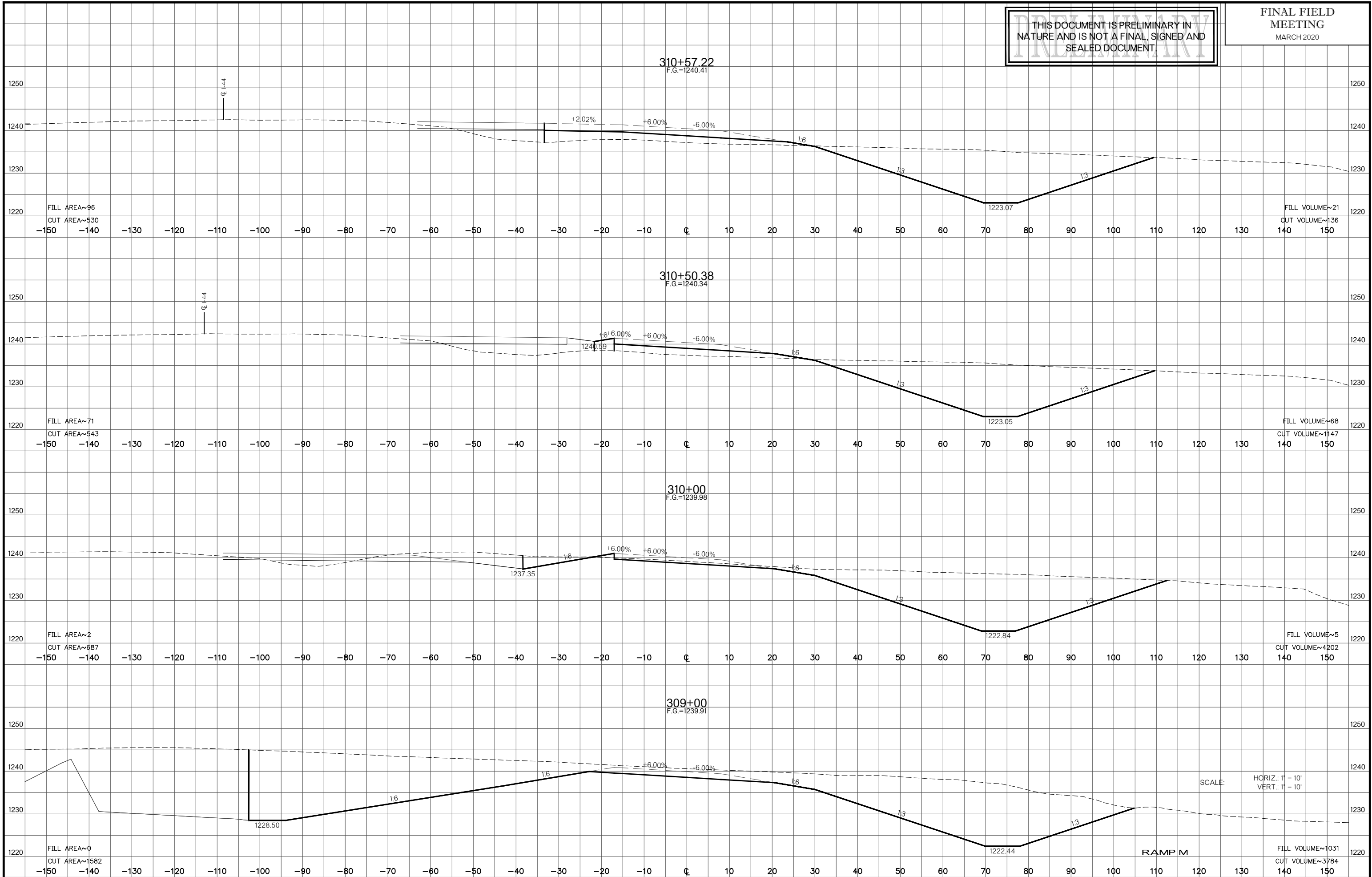
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

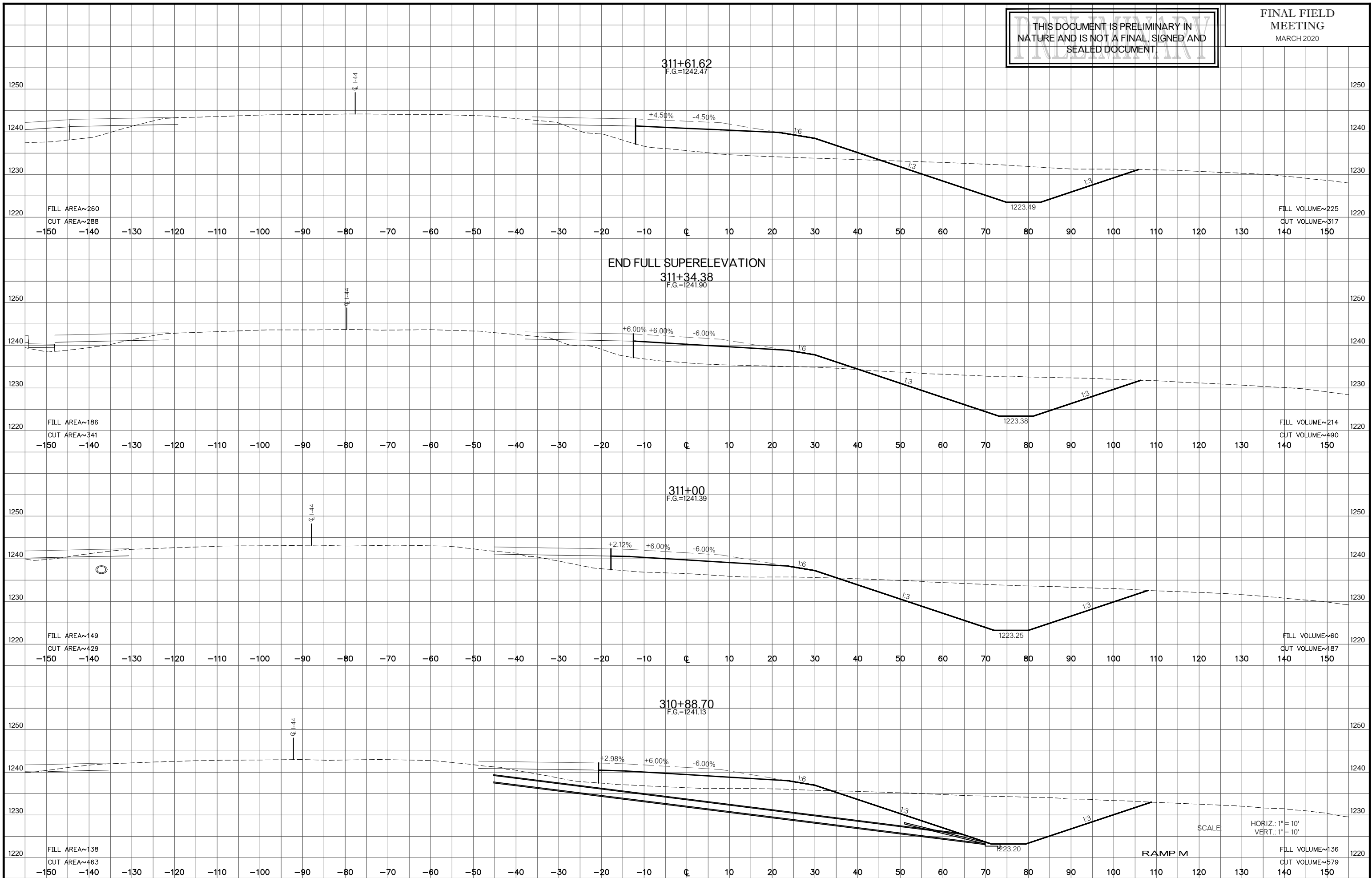
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

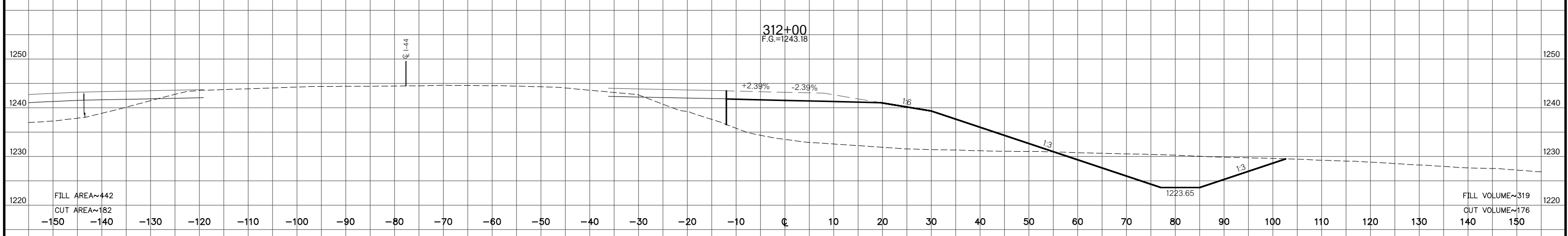
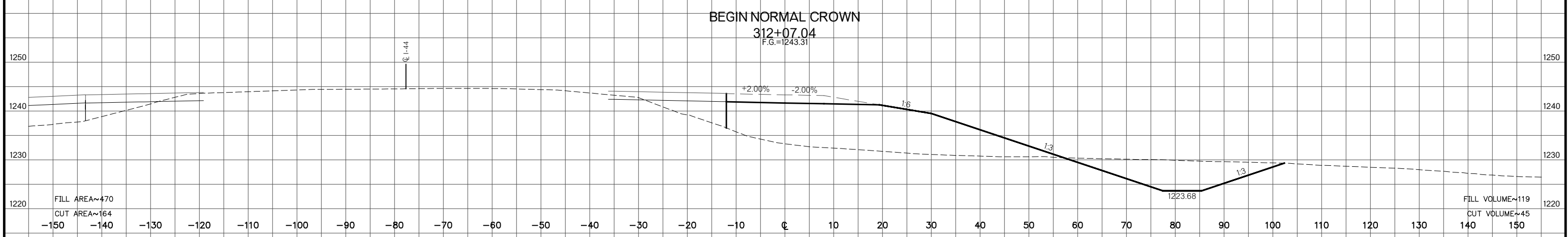
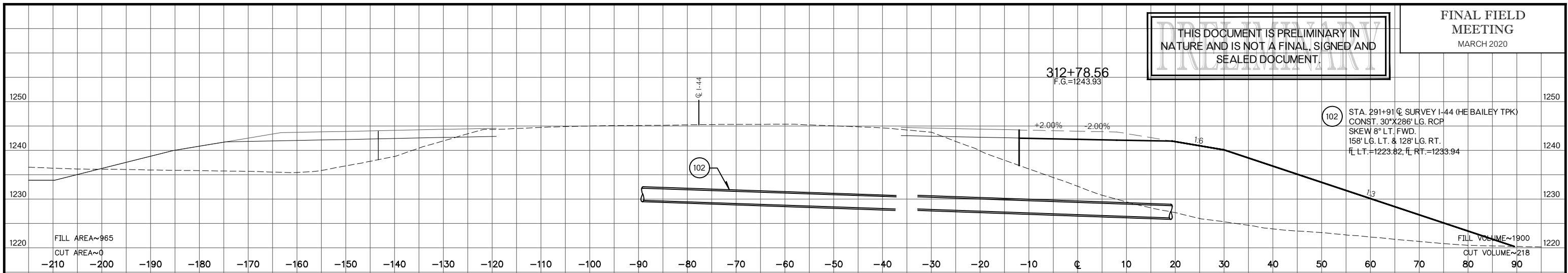
RAMP M

State Job No. 24428(04) Sheet No. X361

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

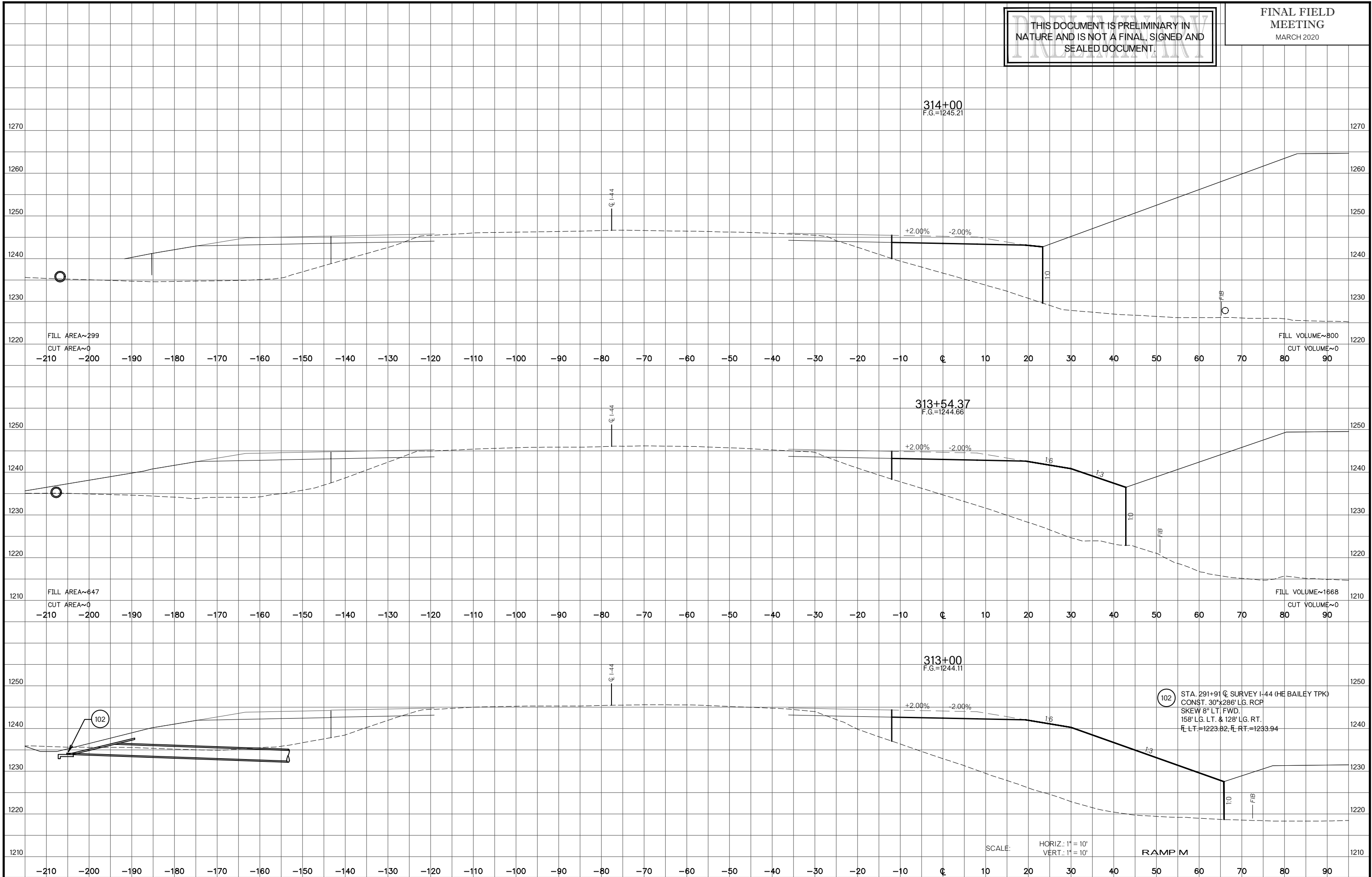
102 STA. 291+91 @ SURVEY I-44 (HE BAILEY TPK)
CONST. 30"x286" LG. RCP
SKEW 8° LT. FWD.
158' LG. LT. & 128' LG. RT.
fl. LT.=1223.82; fl. RT.=1233.94



SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

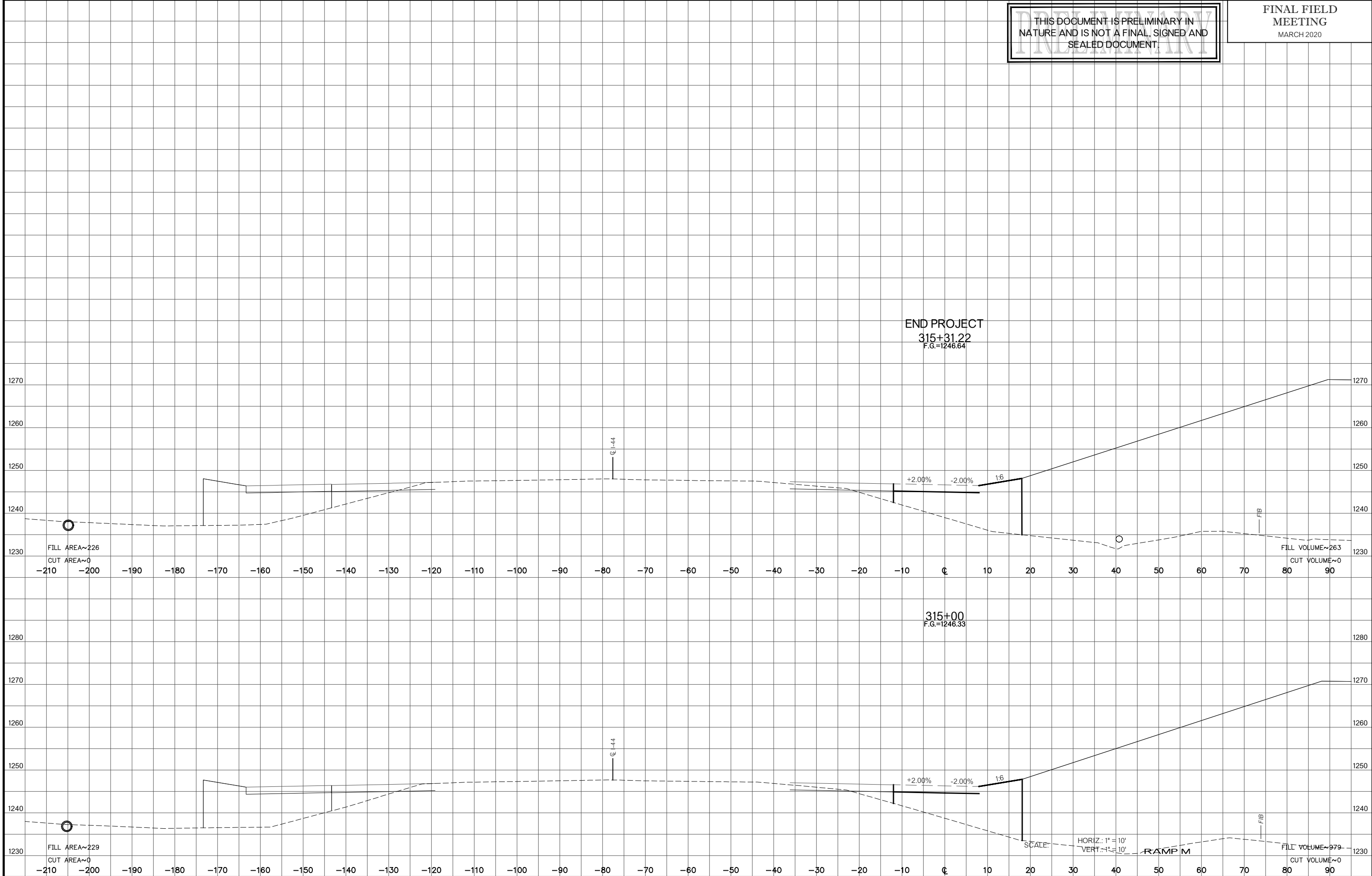
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

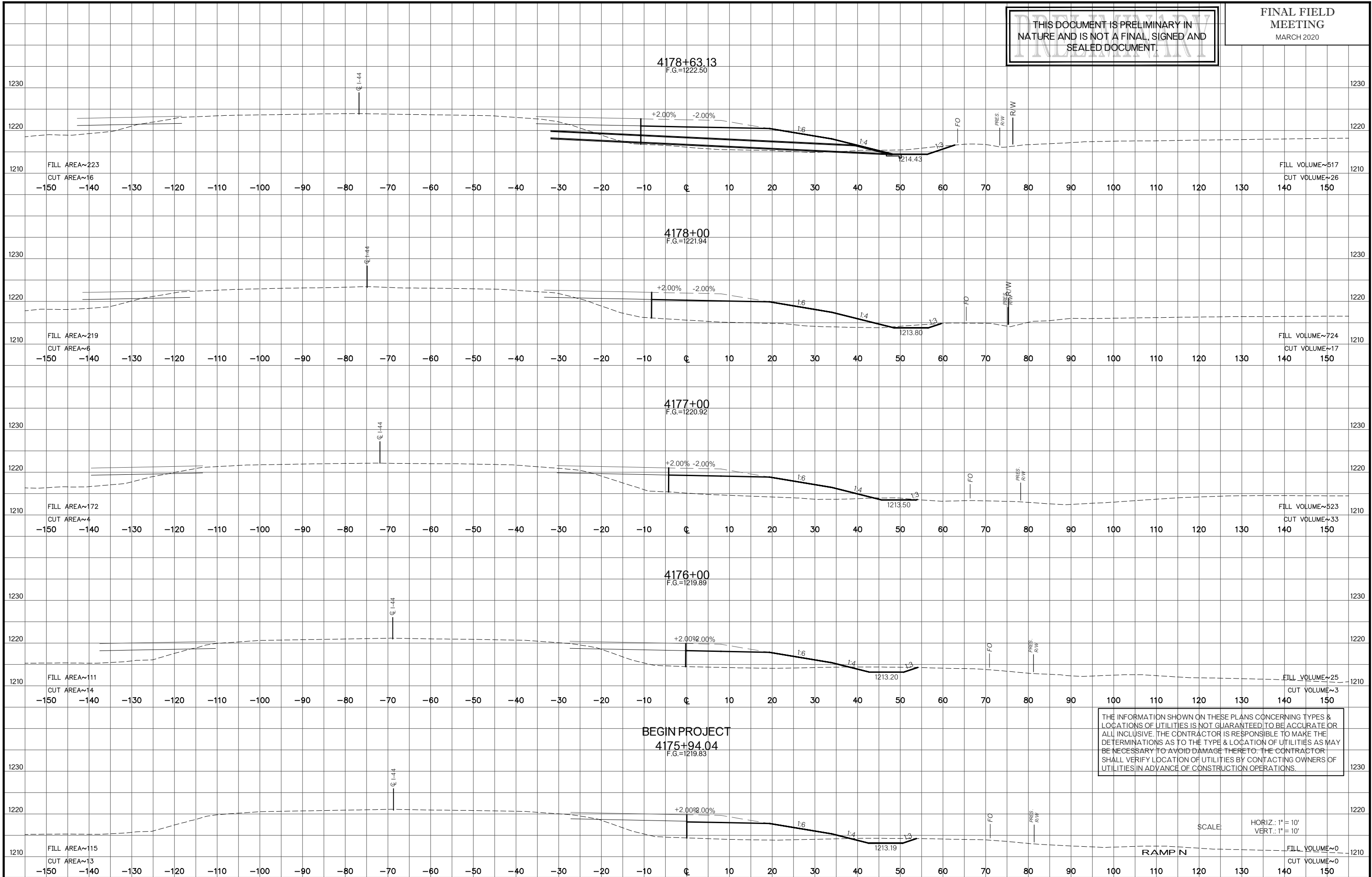
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



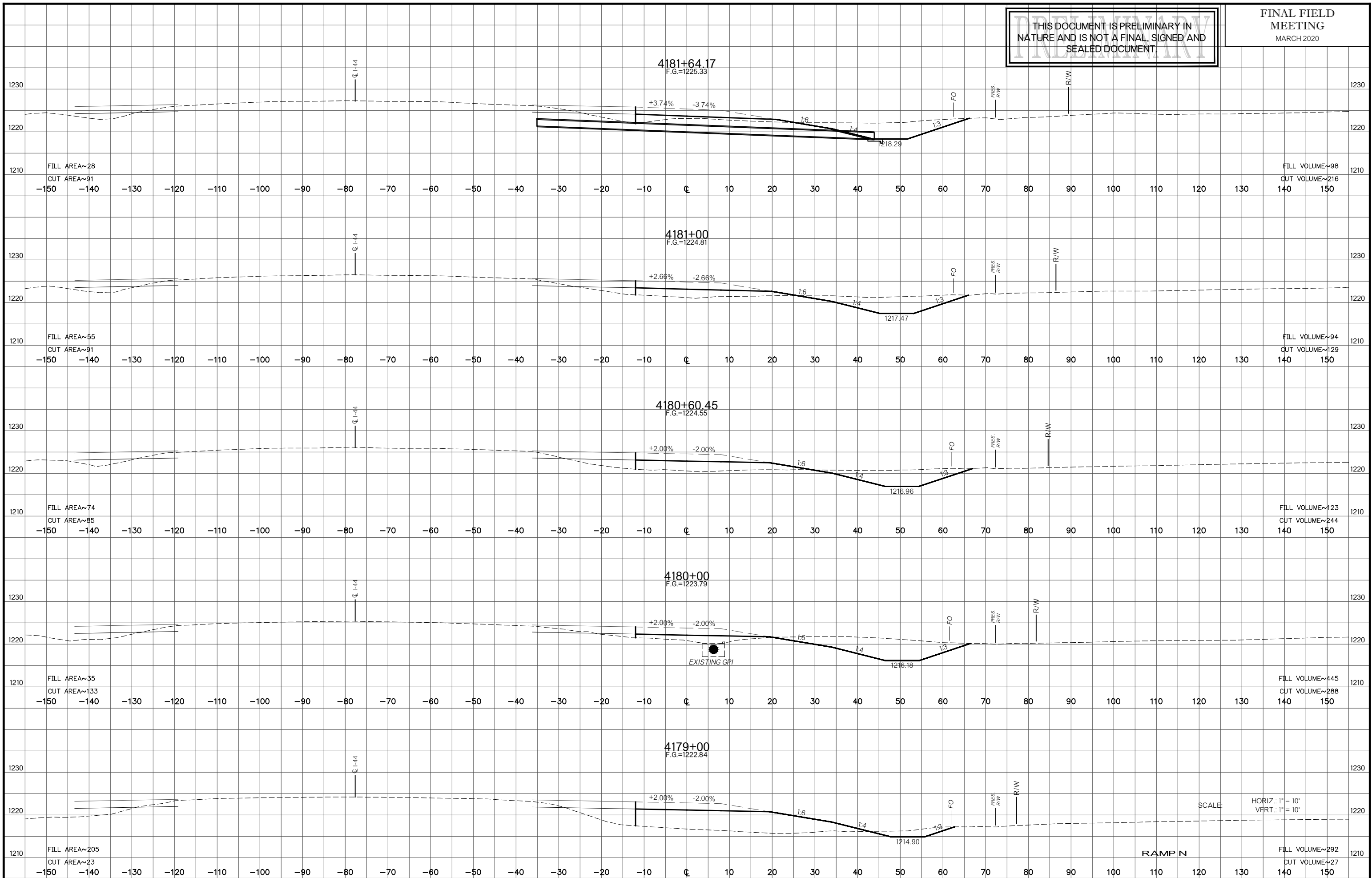
BEGIN PROJECT
4175+94.04
F.G.=1219.83

THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

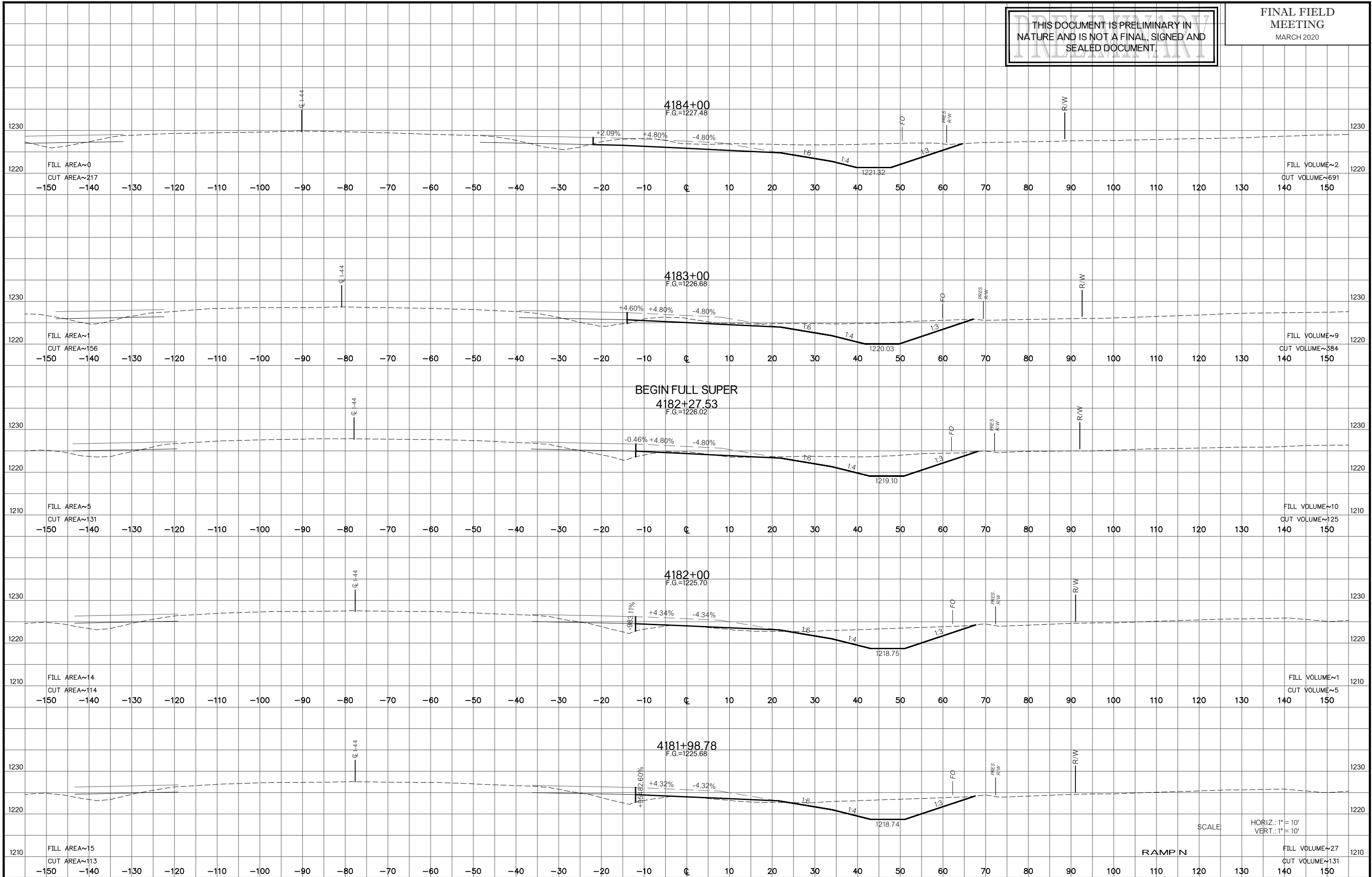
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

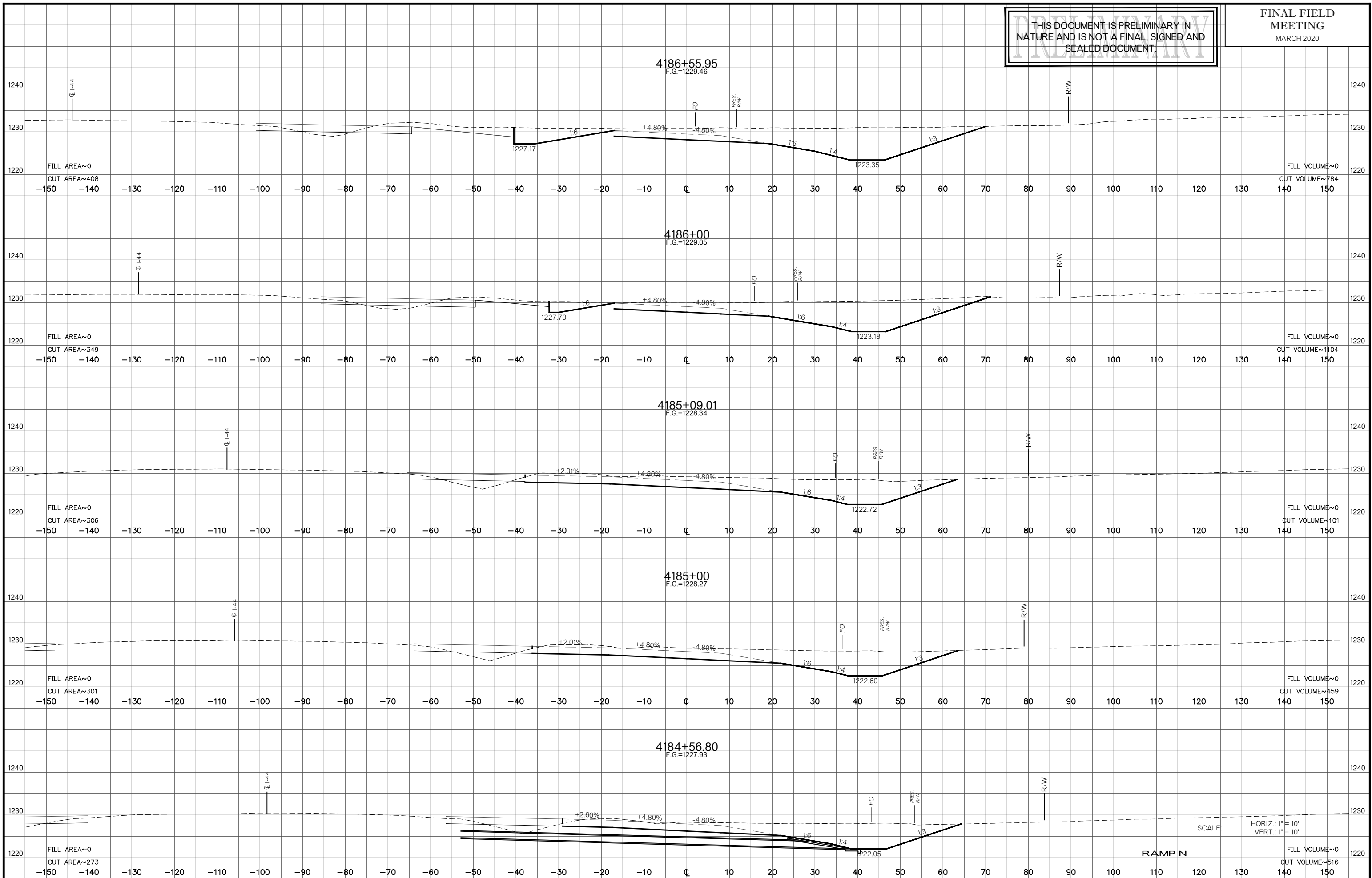
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE HORIZ.: 1" = 10' VERT.: 1" = 10'

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE
 HORIZ.: 1" = 10'
 VERT.: 1" = 10'

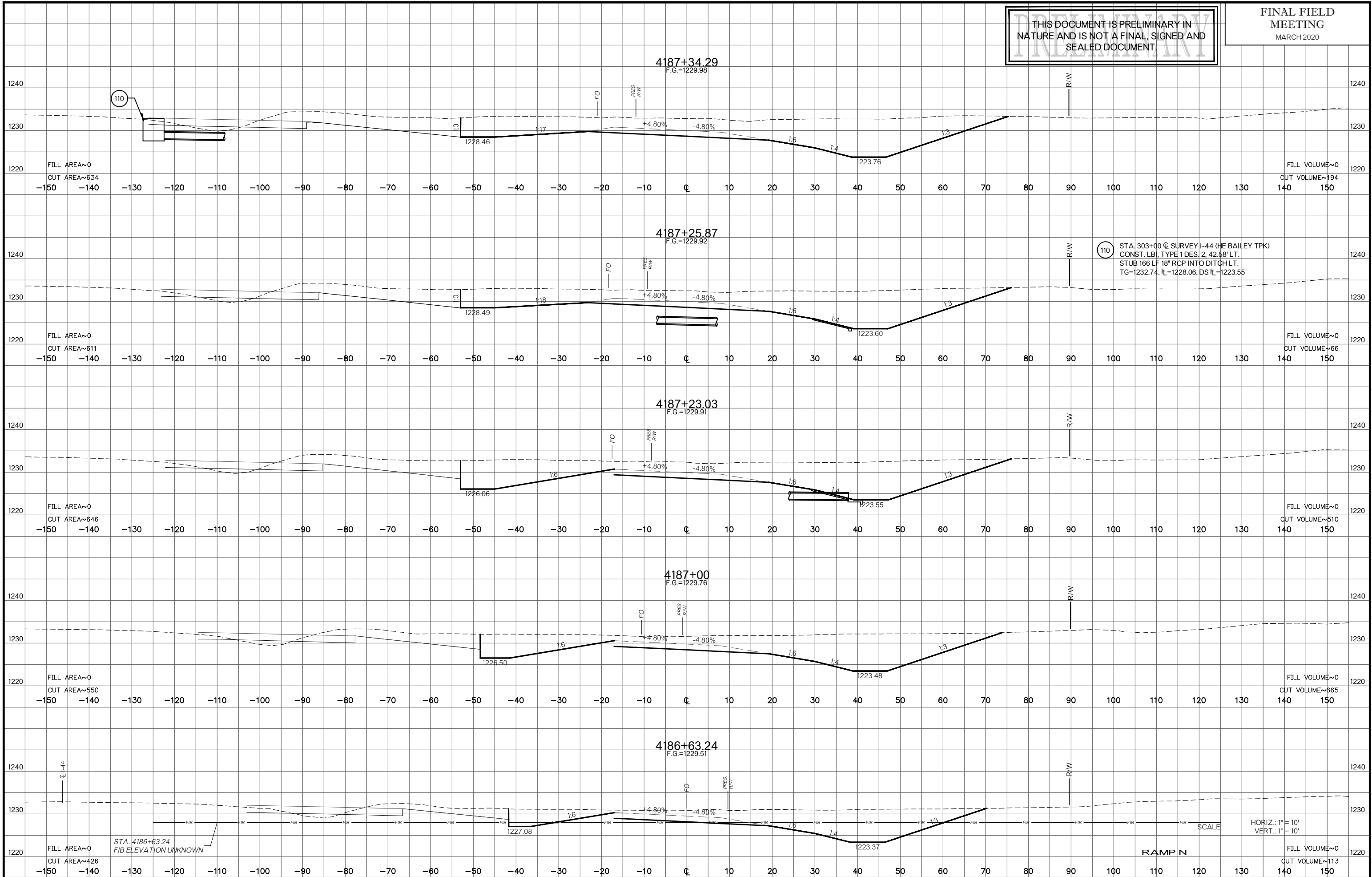
RAMP N

FILL VOLUME~0
 CUT VOLUME~516

State Job No. 24428(04) Sheet No. X368

US 81 REALIGNMENT GRADY COUNTY

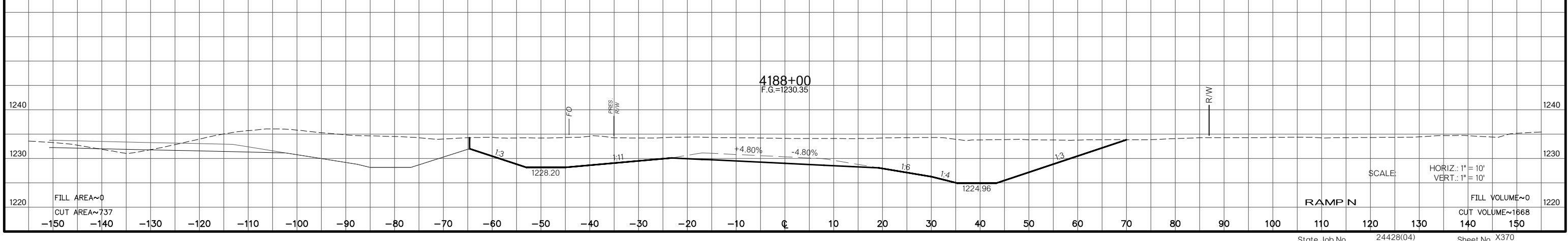
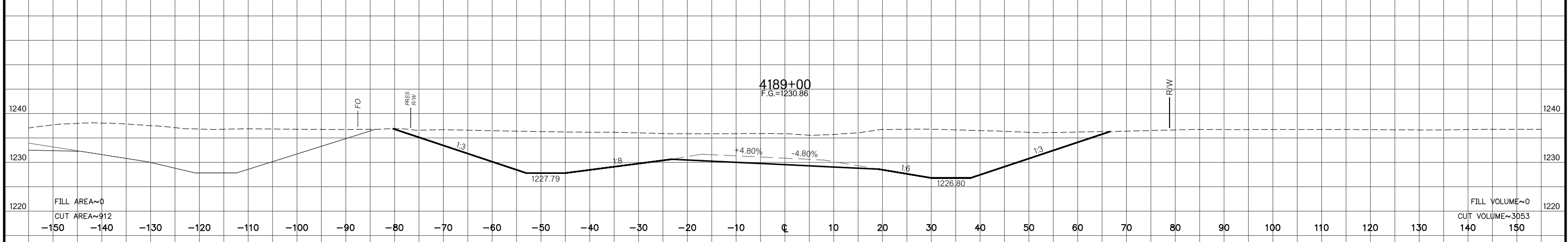
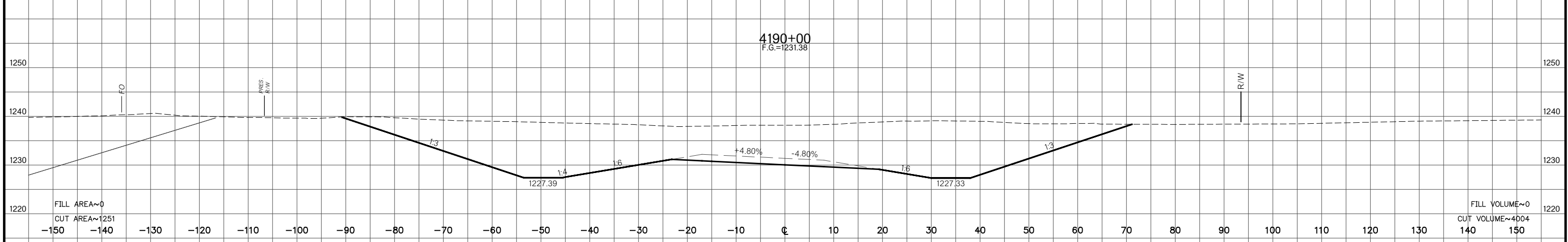
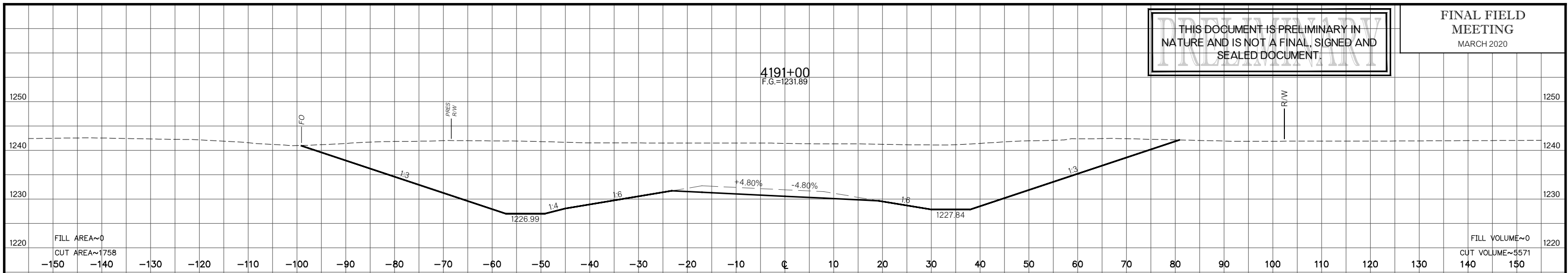
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



110 STA. 303+00 @ SURVEY I-44 (HE BAILEY TPK) CONST. LBI, TYPE 1 DES. 2, 42.58' LT. STUB 166 LF 18" RCP INTO DITCH LT. TG=1232.74, f_L=1228.06, DS f_L=1223.55

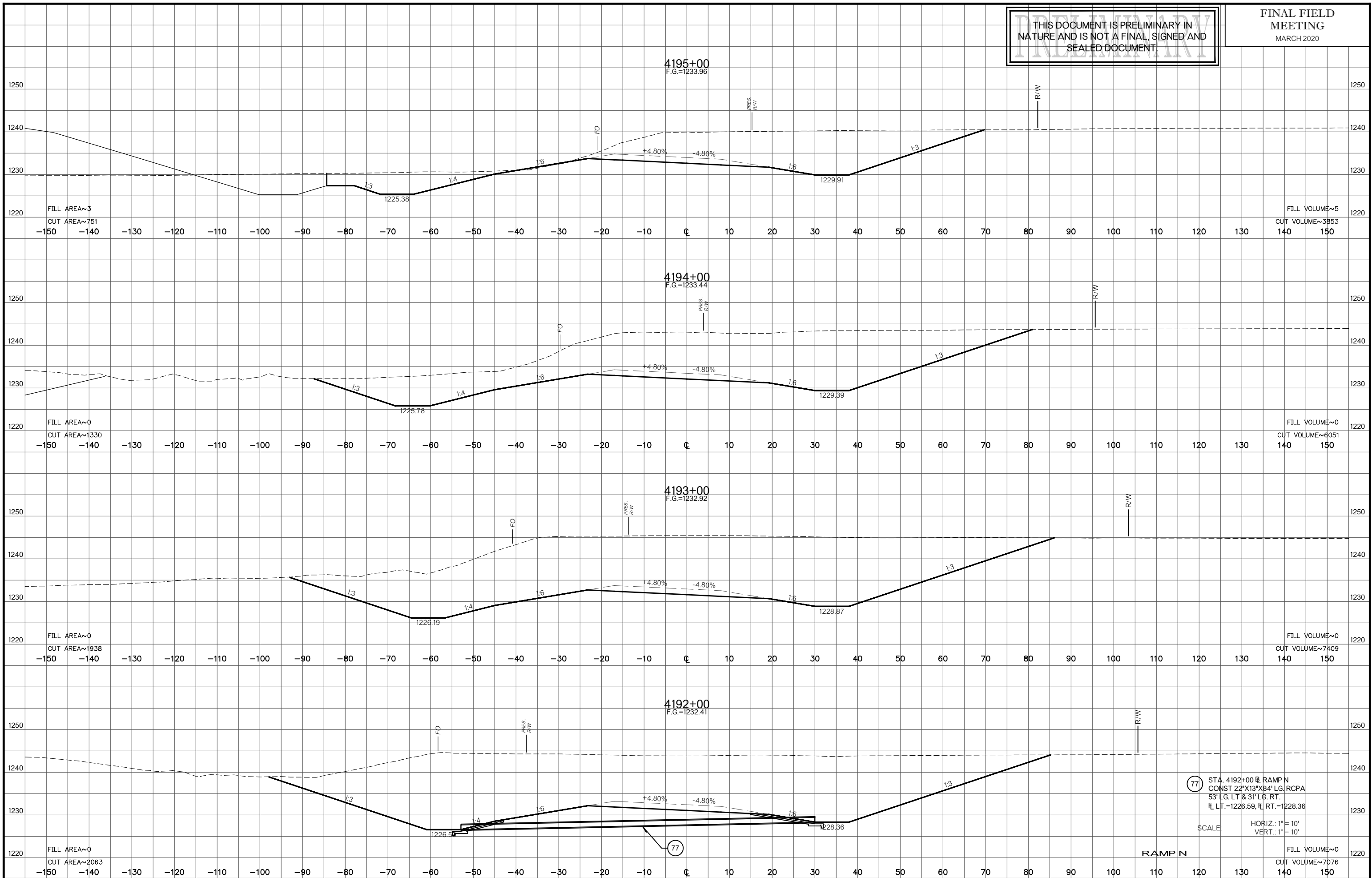
US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



77 STA. 4192+00 RAMP N
 CONST 22'X13'XB4' LG./RCPA
 53' LG. LT & 31' LG. RT.
 FL LT.=1226.59, FL RT.=1228.36

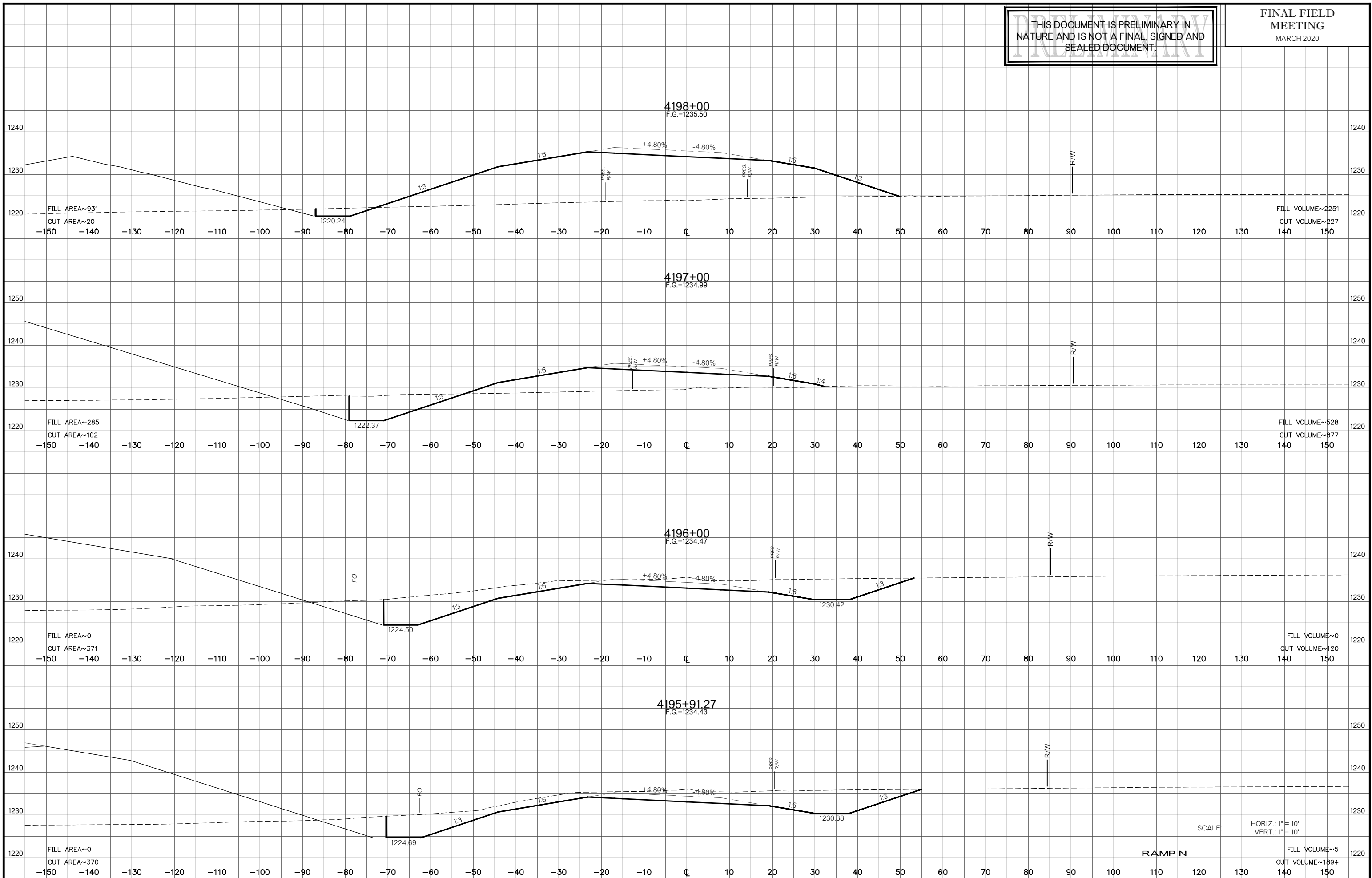
SCALE HORIZ.: 1" = 10'
 VERT.: 1" = 10'

RAMP N

FILL VOLUME~0
 CUT VOLUME~7076

US 81 REALIGNMENT
 GRADY COUNTY

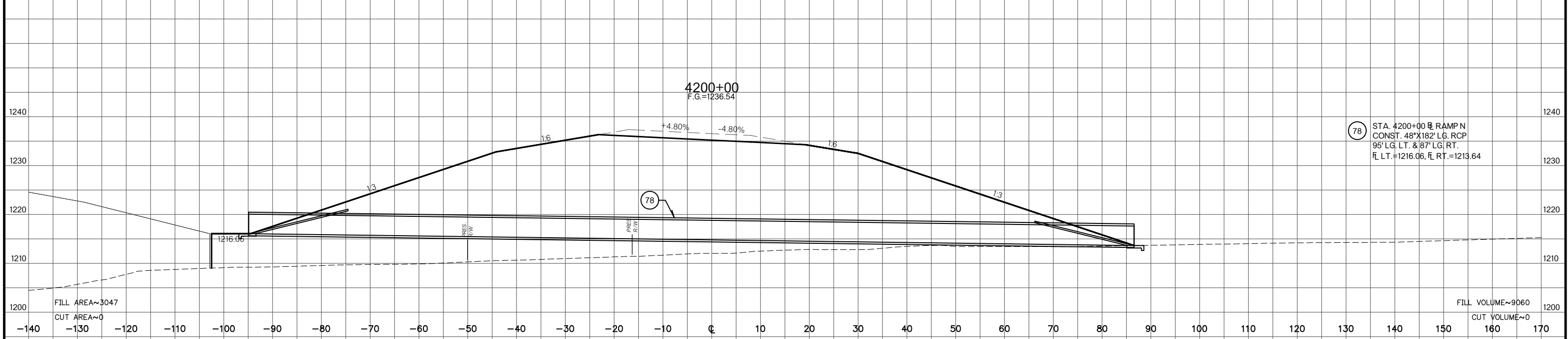
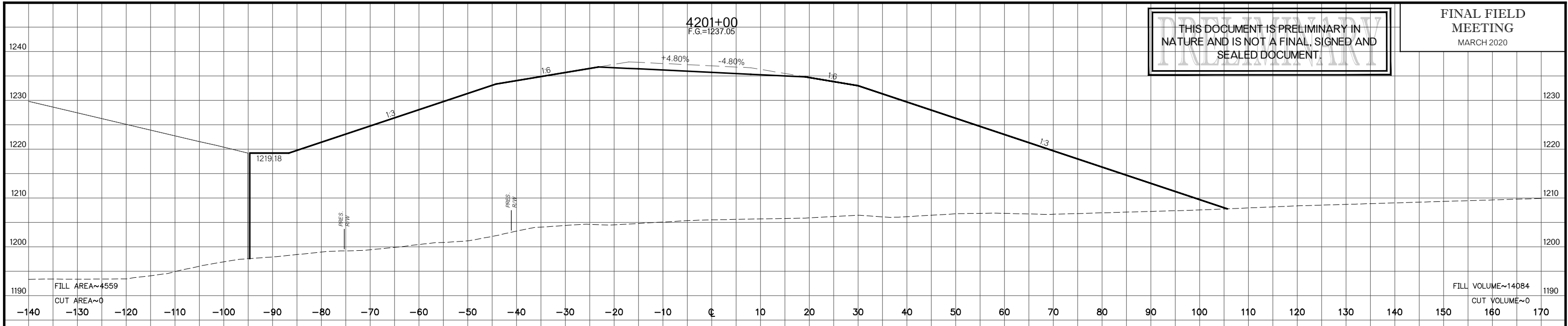
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



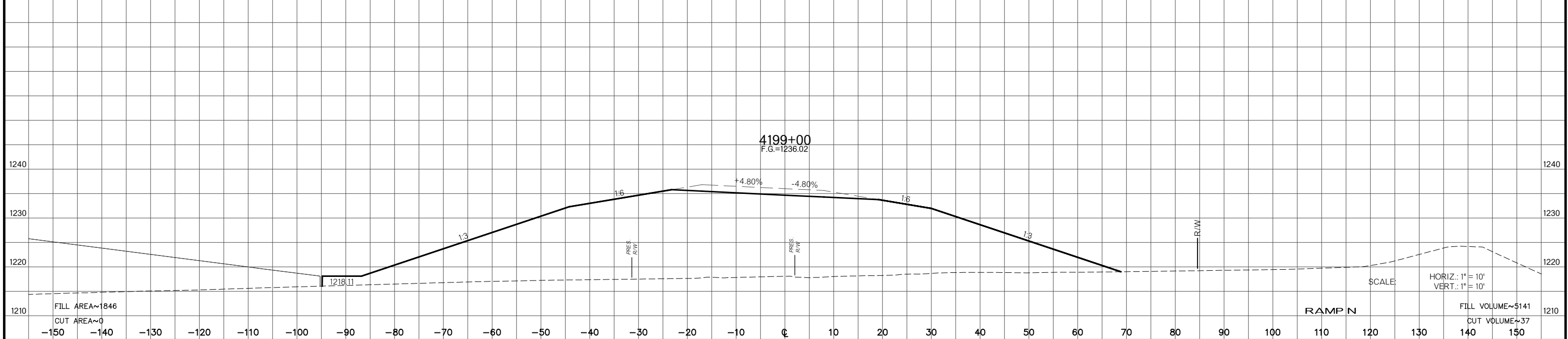
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'
RAMP N
State Job No. 24428(04) Sheet No. X372

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



(78) STA. 4200+00 R RAMP N
CONST. 48"X182' LG. RCP
95' LG. LT. & 87' LG. RT.
FL. LT.=1216.06, FL. RT.=1213.64



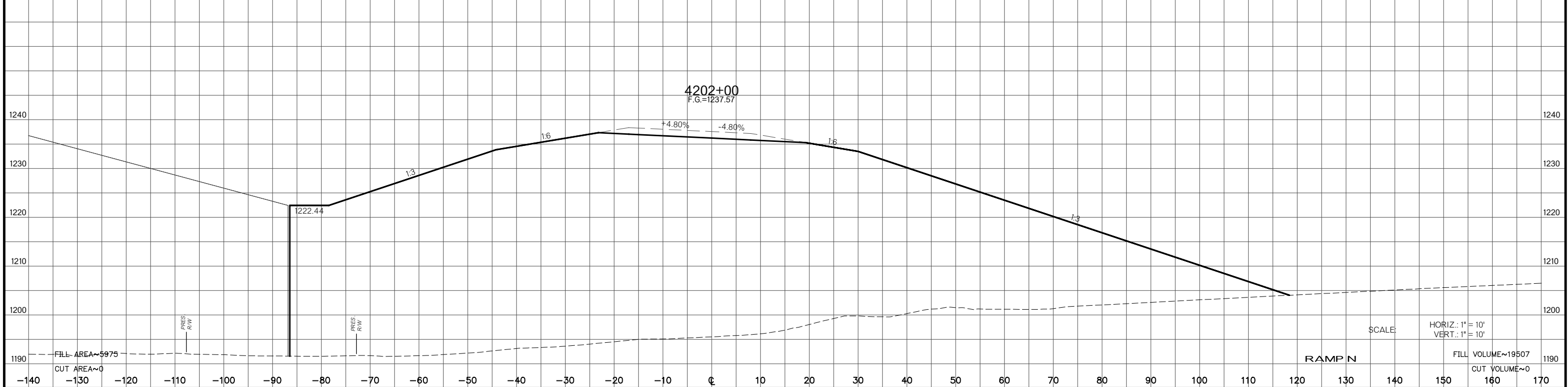
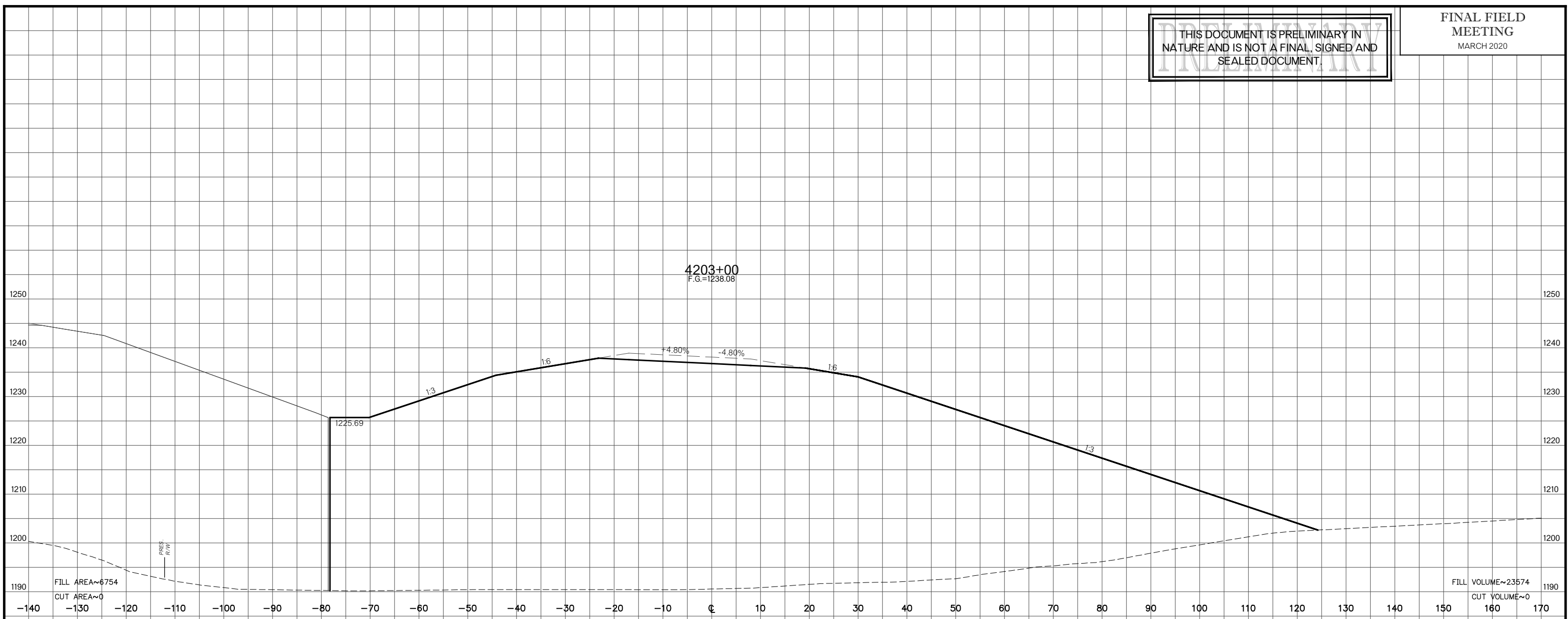
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP N

US 81 REALIGNMENT
GRADY COUNTY

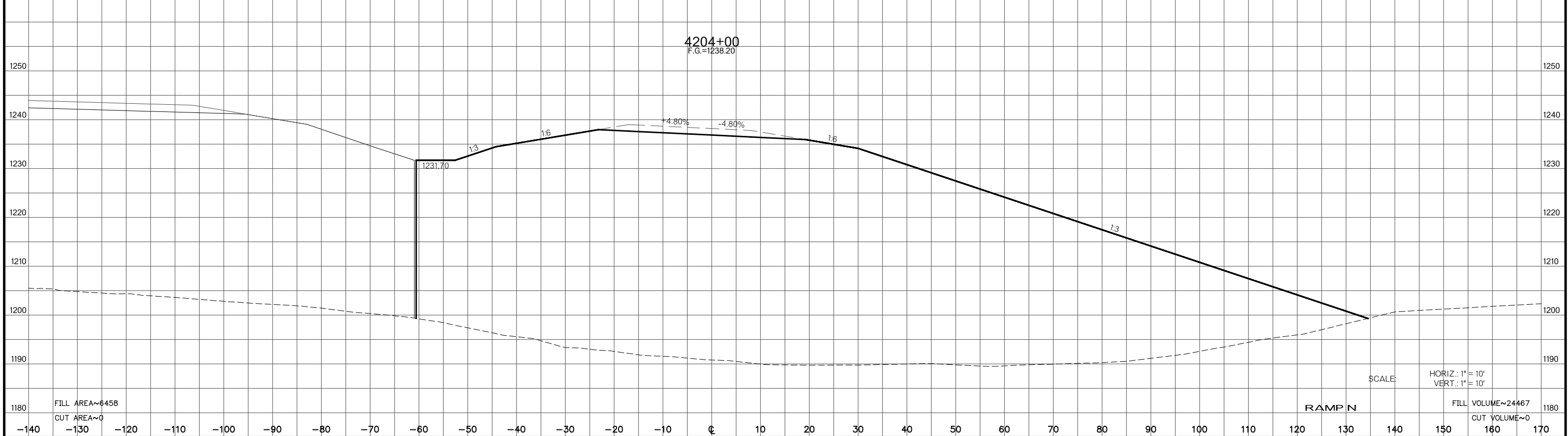
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



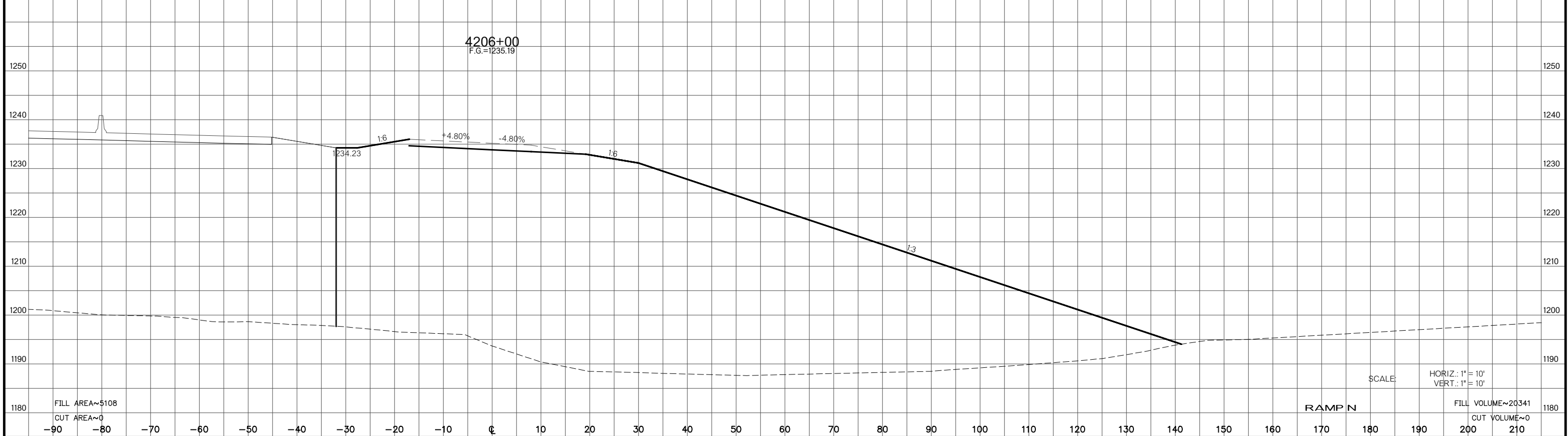
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP N

State Job No. 24428(04) Sheet No. X375

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

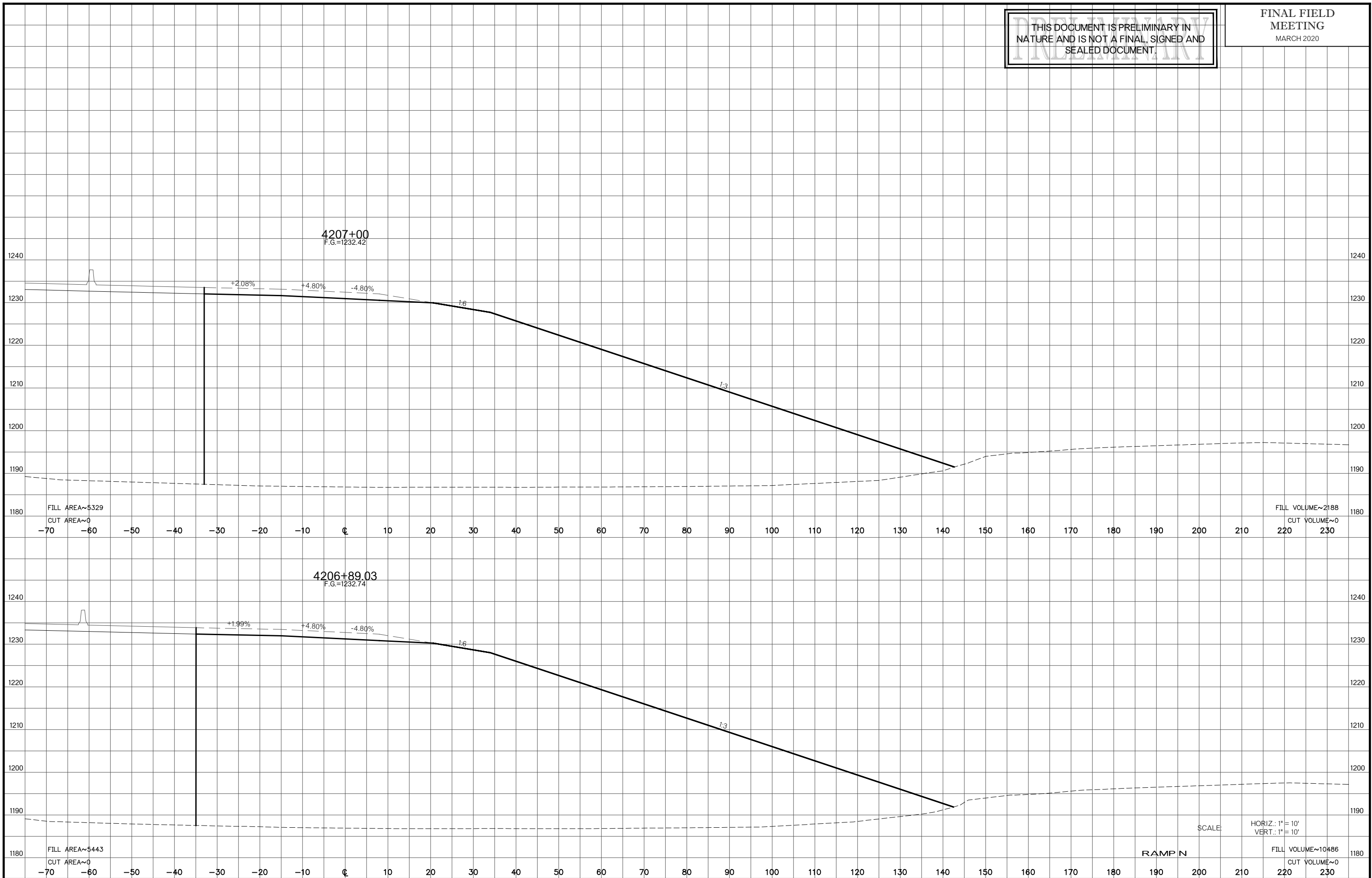
RAMP N

State Job No. 24428(04) Sheet No. X376

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

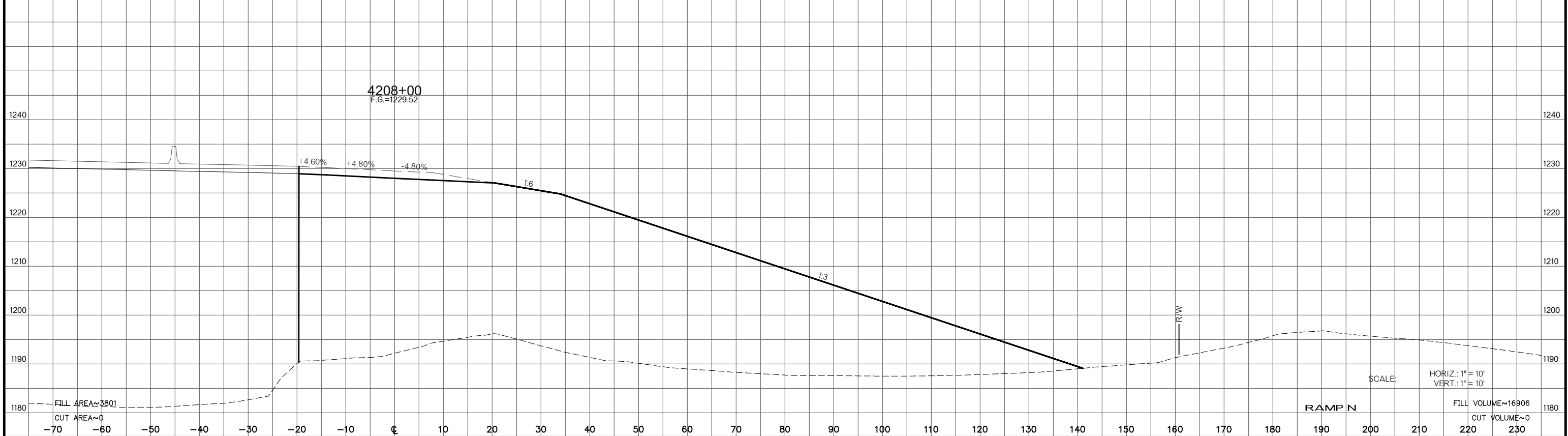
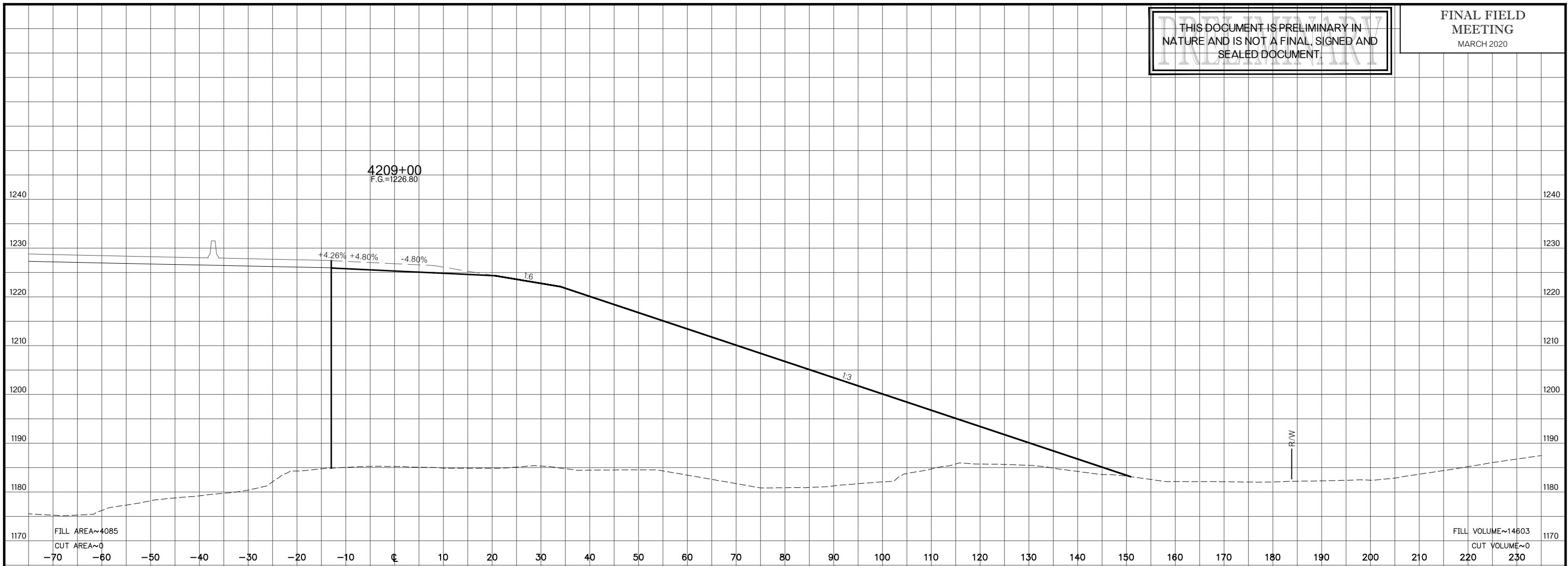
FINAL FIELD MEETING
MARCH 2020



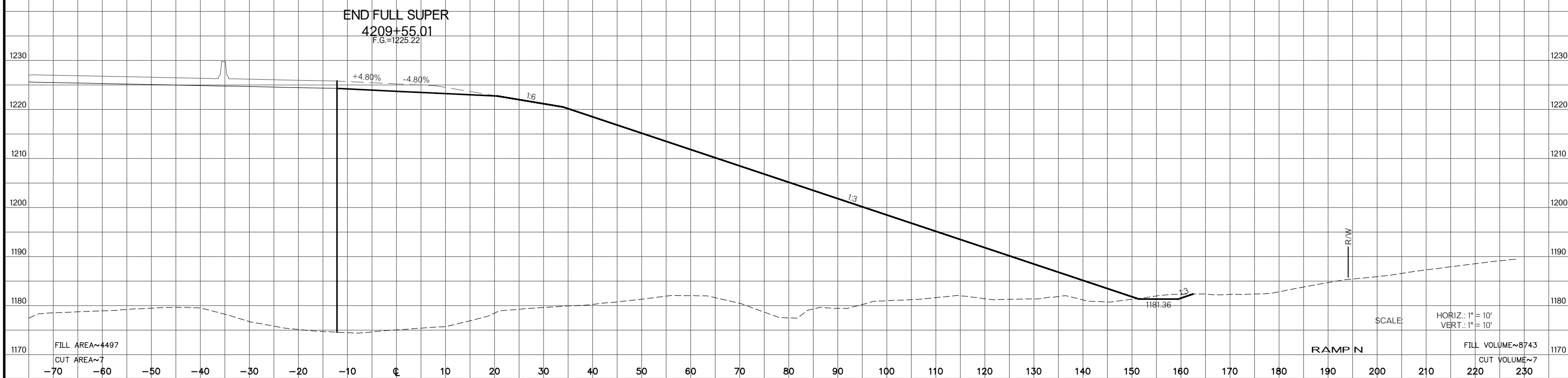
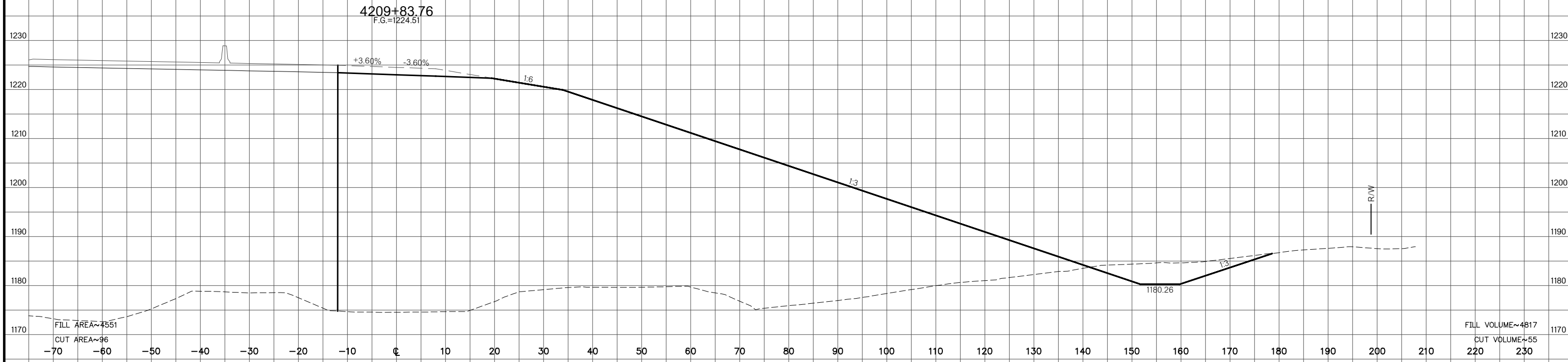
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



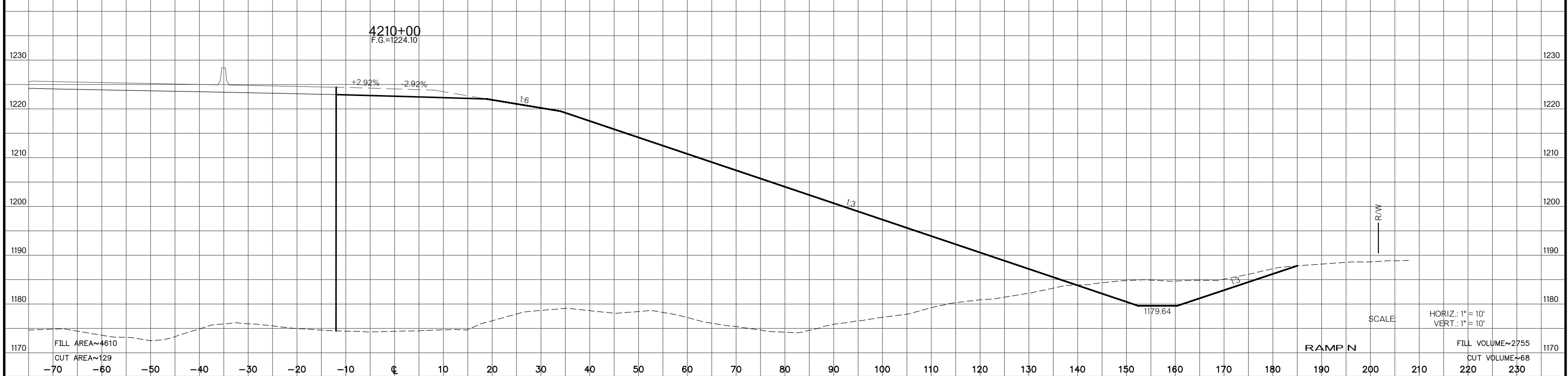
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

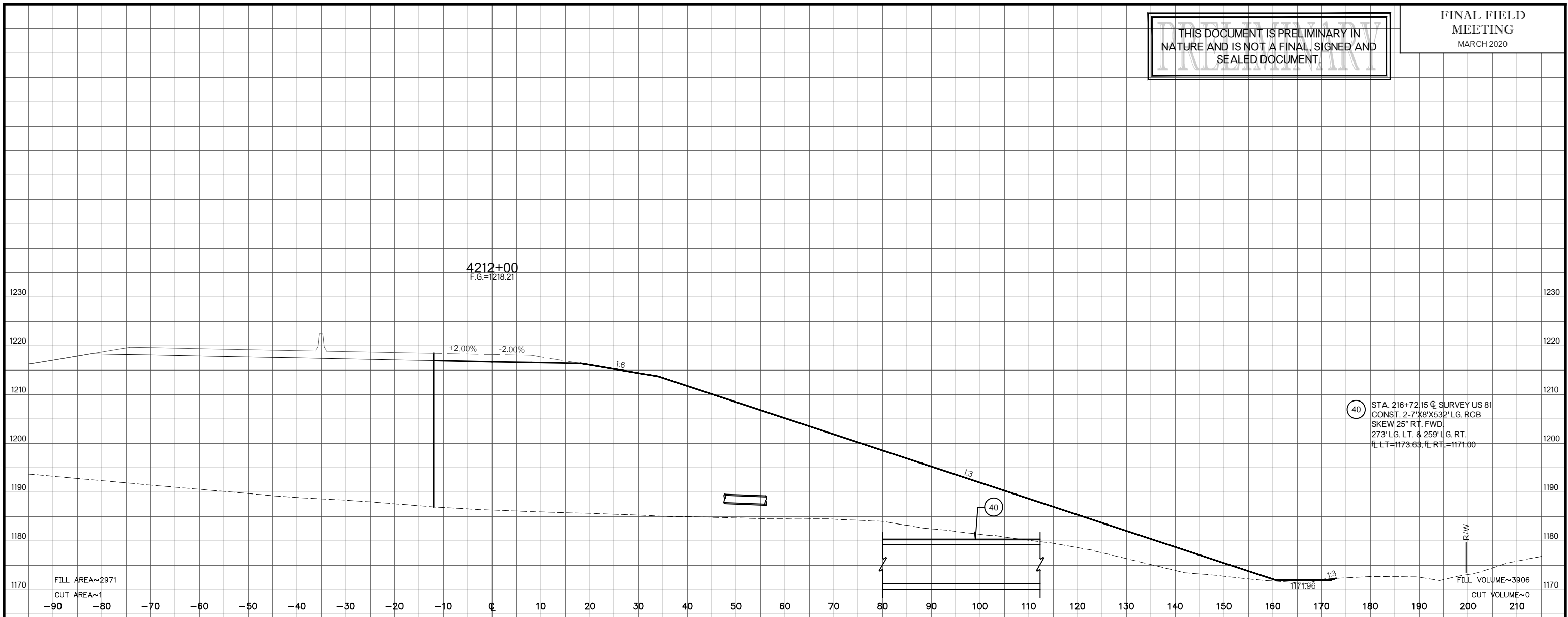
FINAL FIELD MEETING
MARCH 2020



SCALE
RAMP N
HORIZ.: 1" = 10'
VERT.: 1" = 10'

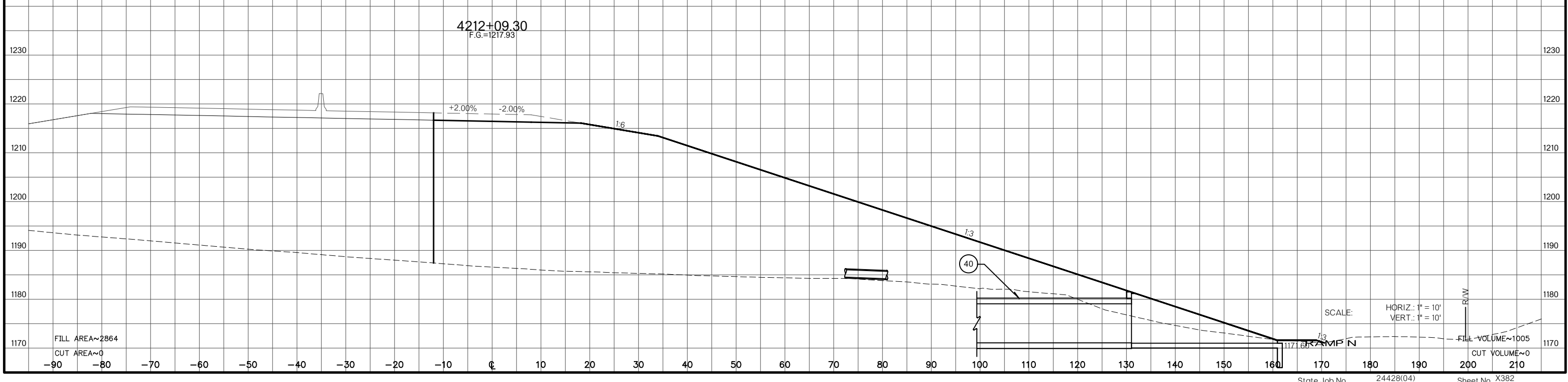
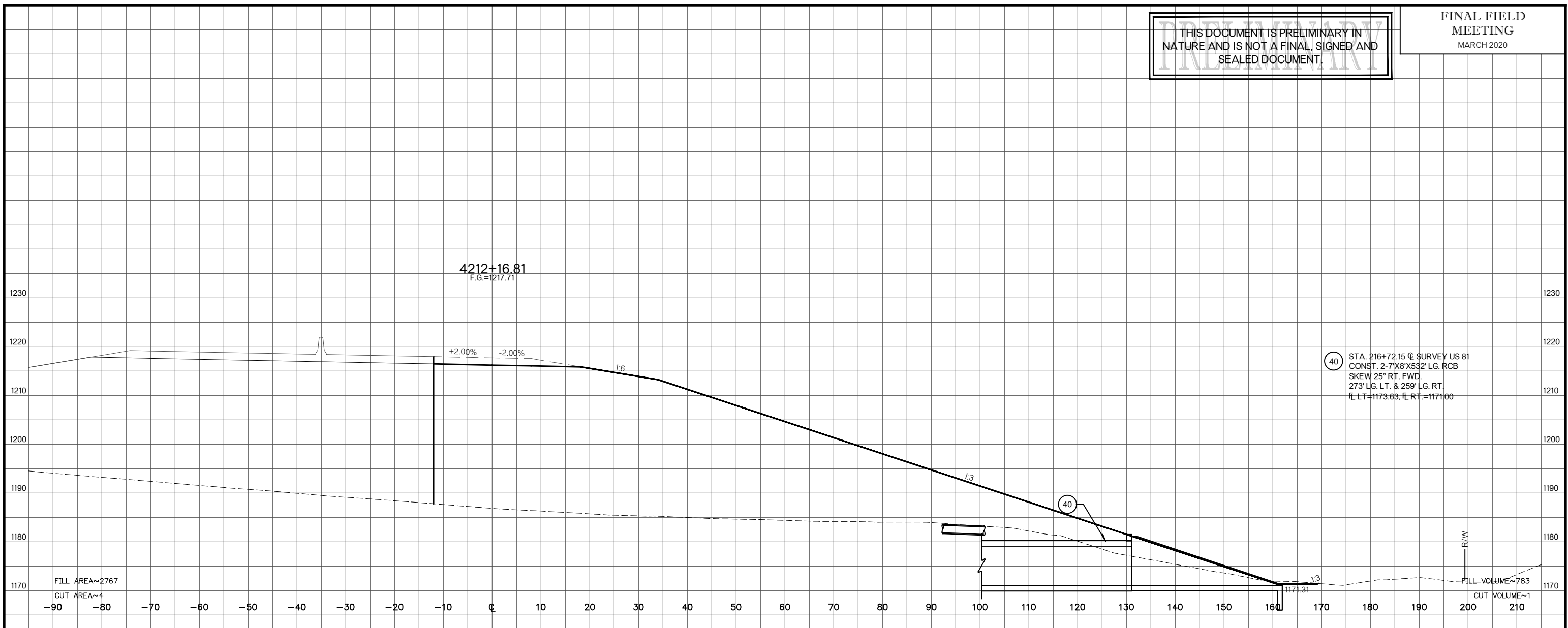
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

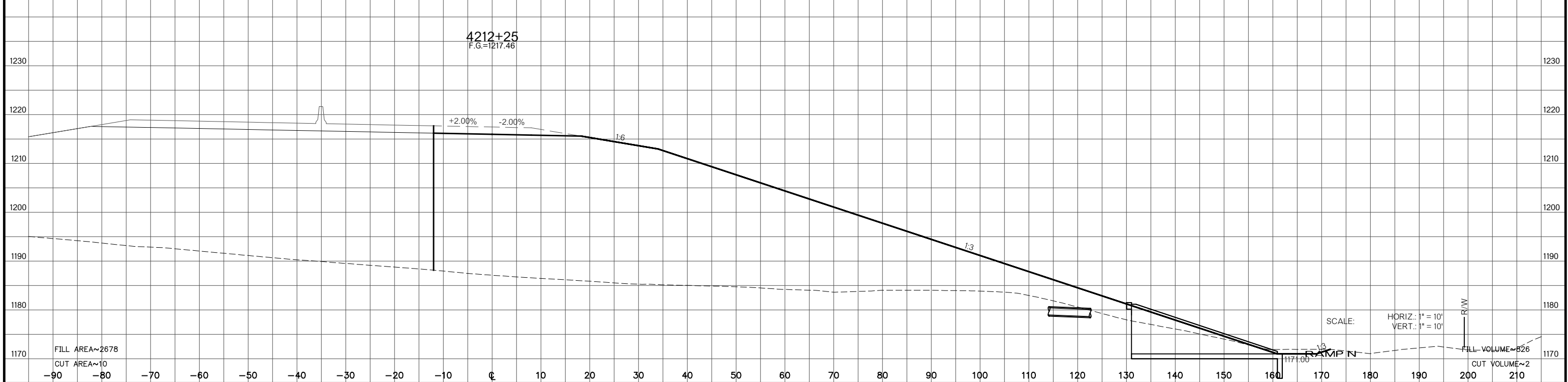


US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



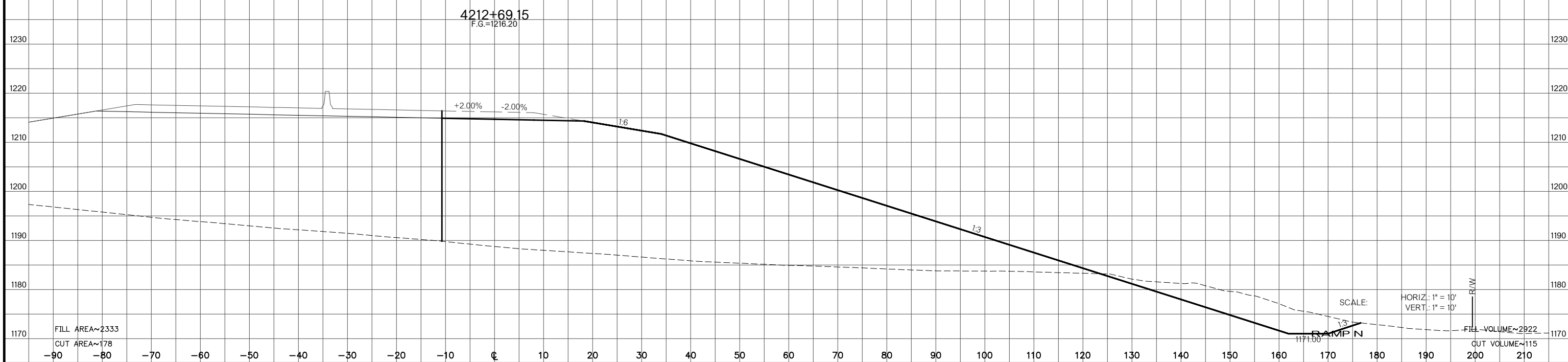
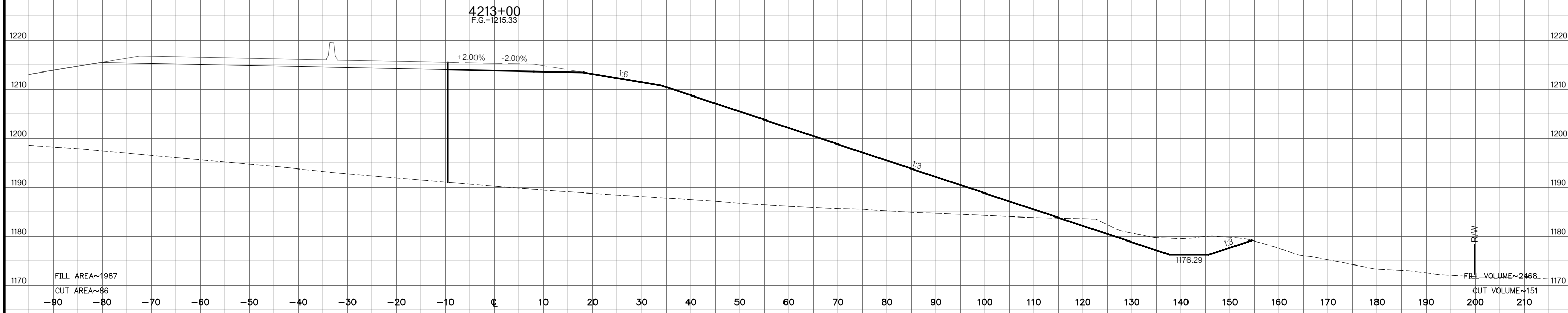
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



US 81 REALIGNMENT
GRADY COUNTY

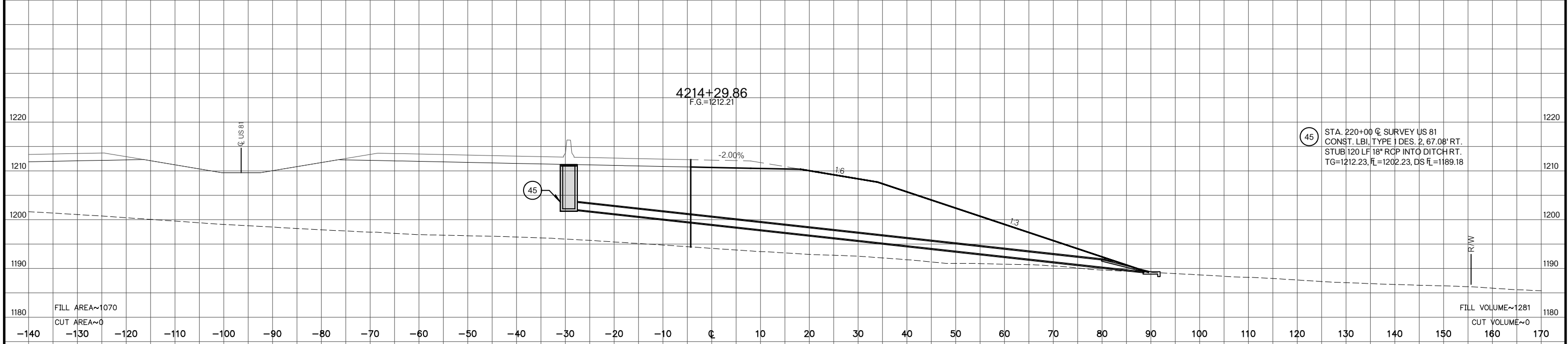
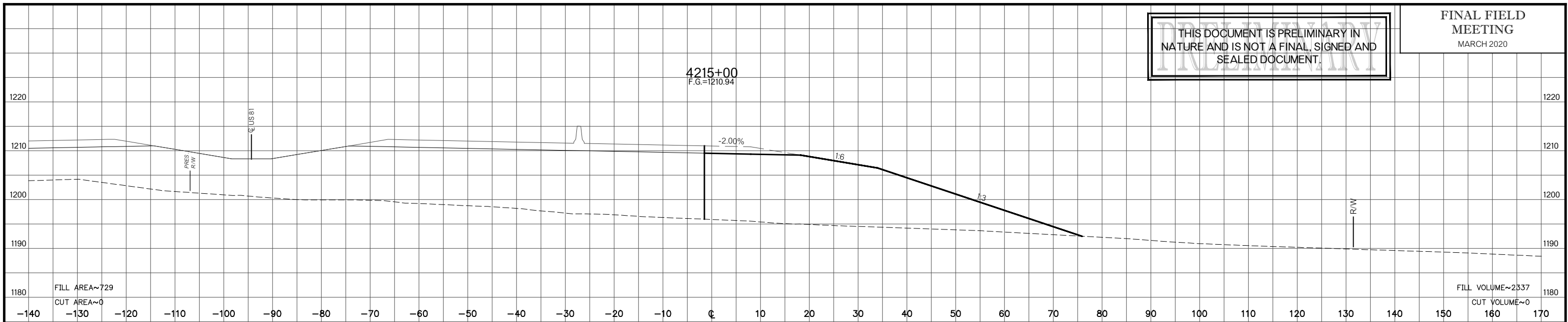
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

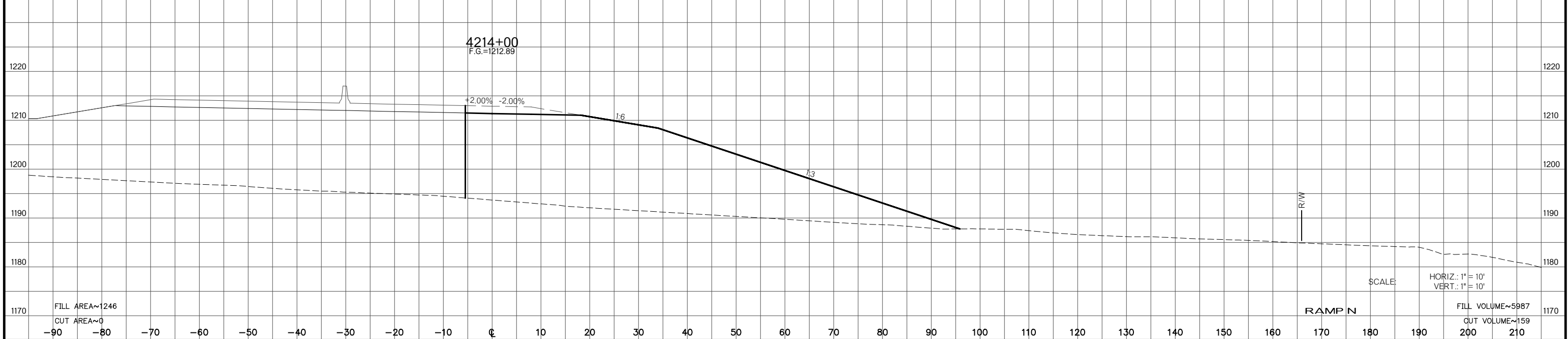


US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



45 STA. 220+00 CL SURVEY US 81
CONST. LBI, TYPE 1 DES. 2, 67.08' RT.
STUB 120 LF 18" RCP INTO DITCH RT.
TG=1212.23, FL=1202.23, DS FL=1189.18

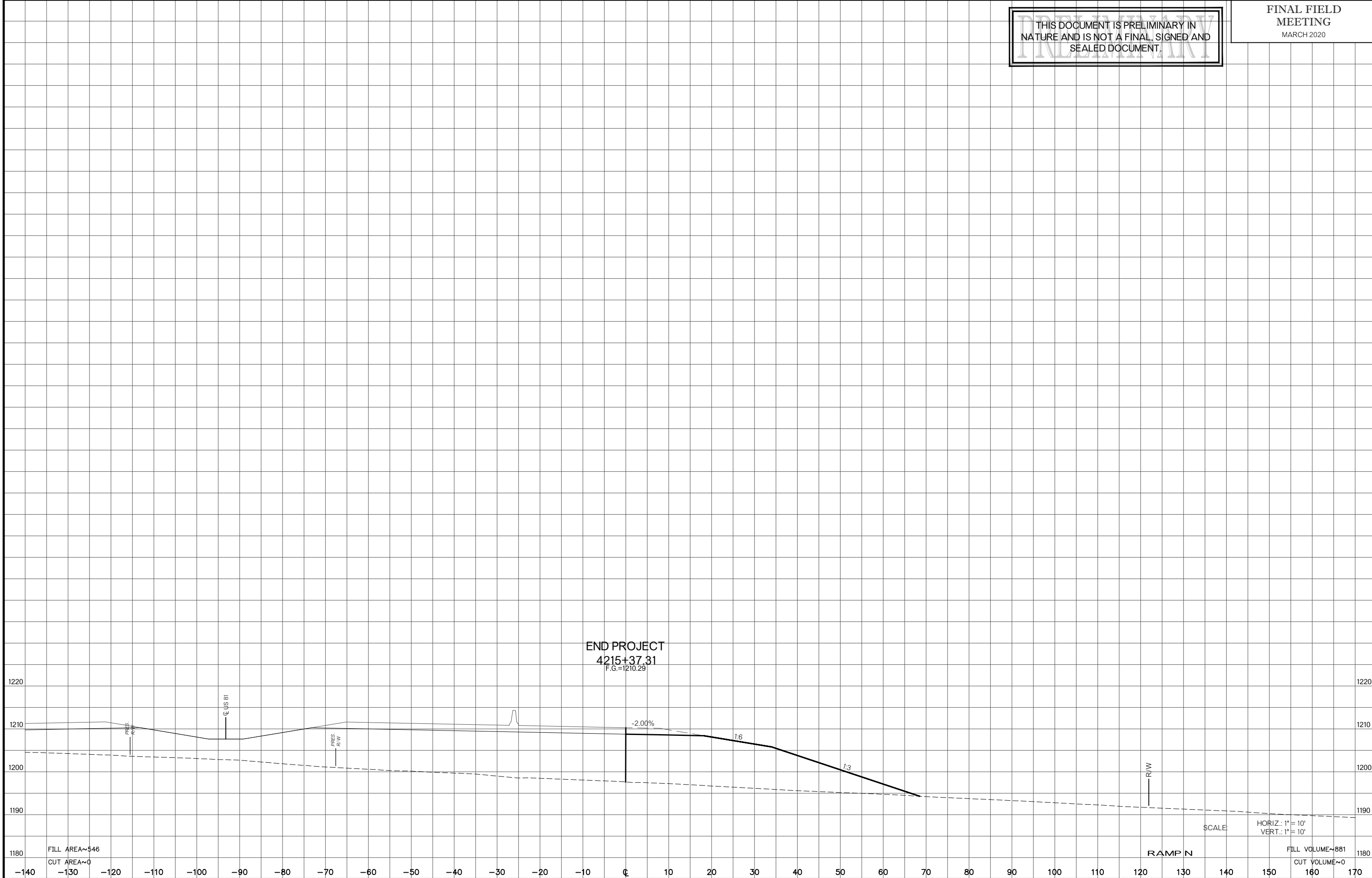


SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



FILL AREA~546
CUT AREA~0

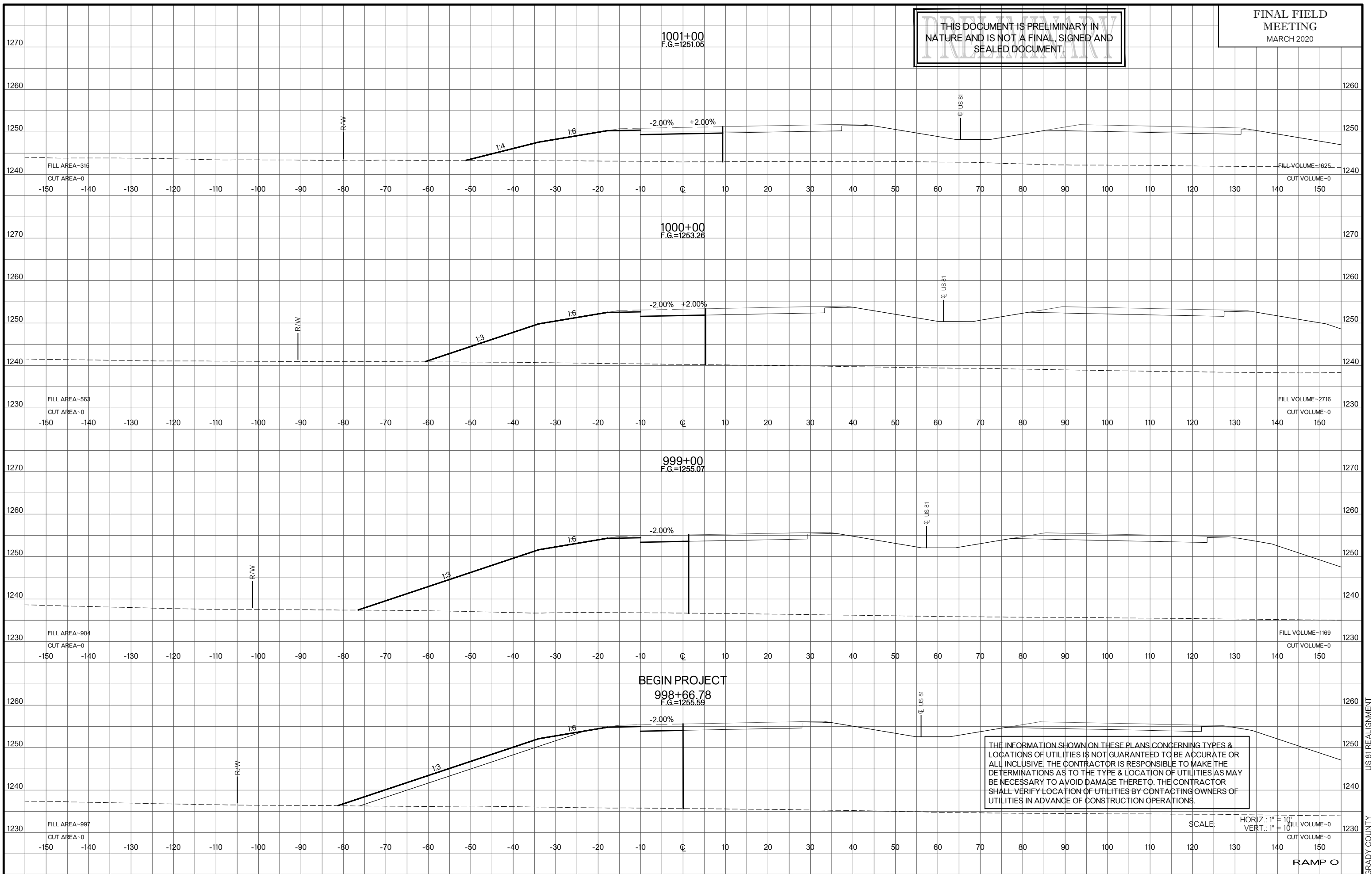
SCALE
HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP N

FILL VOLUME~881
CUT VOLUME~0

GRADY COUNTY
US 81 REALIGNMENT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

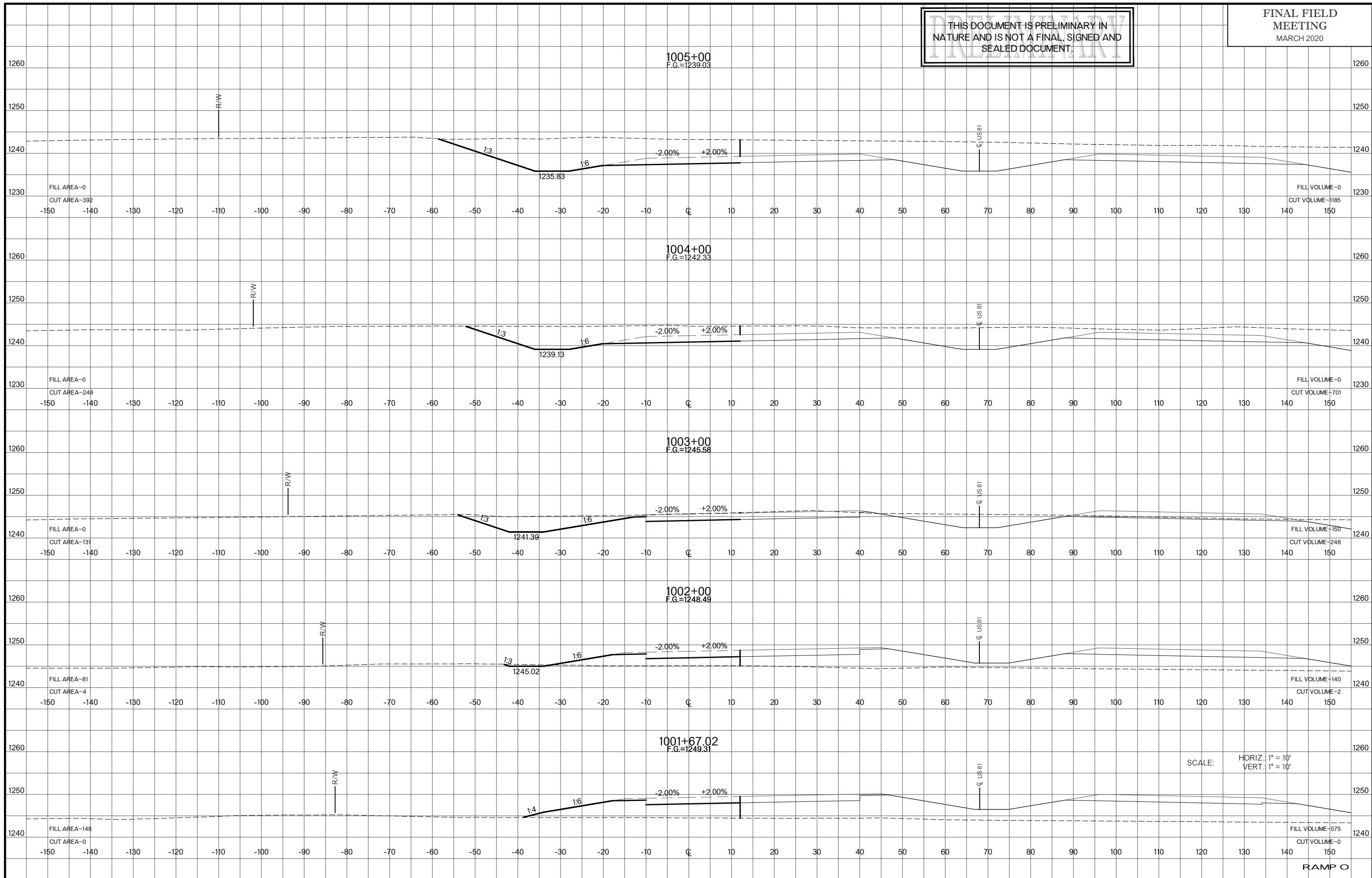
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP O

US 81 REALIGNMENT
GRADY COUNTY

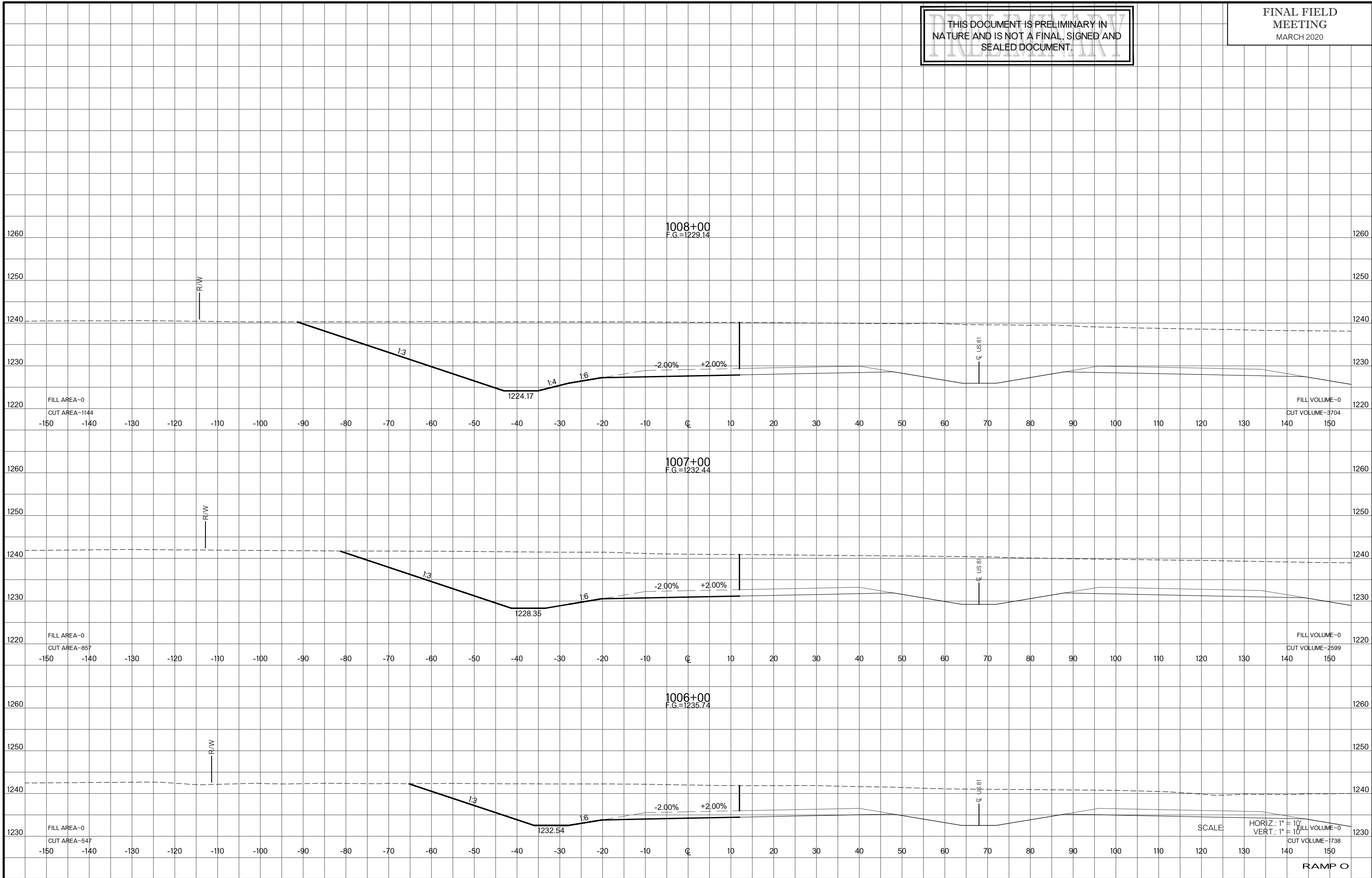
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



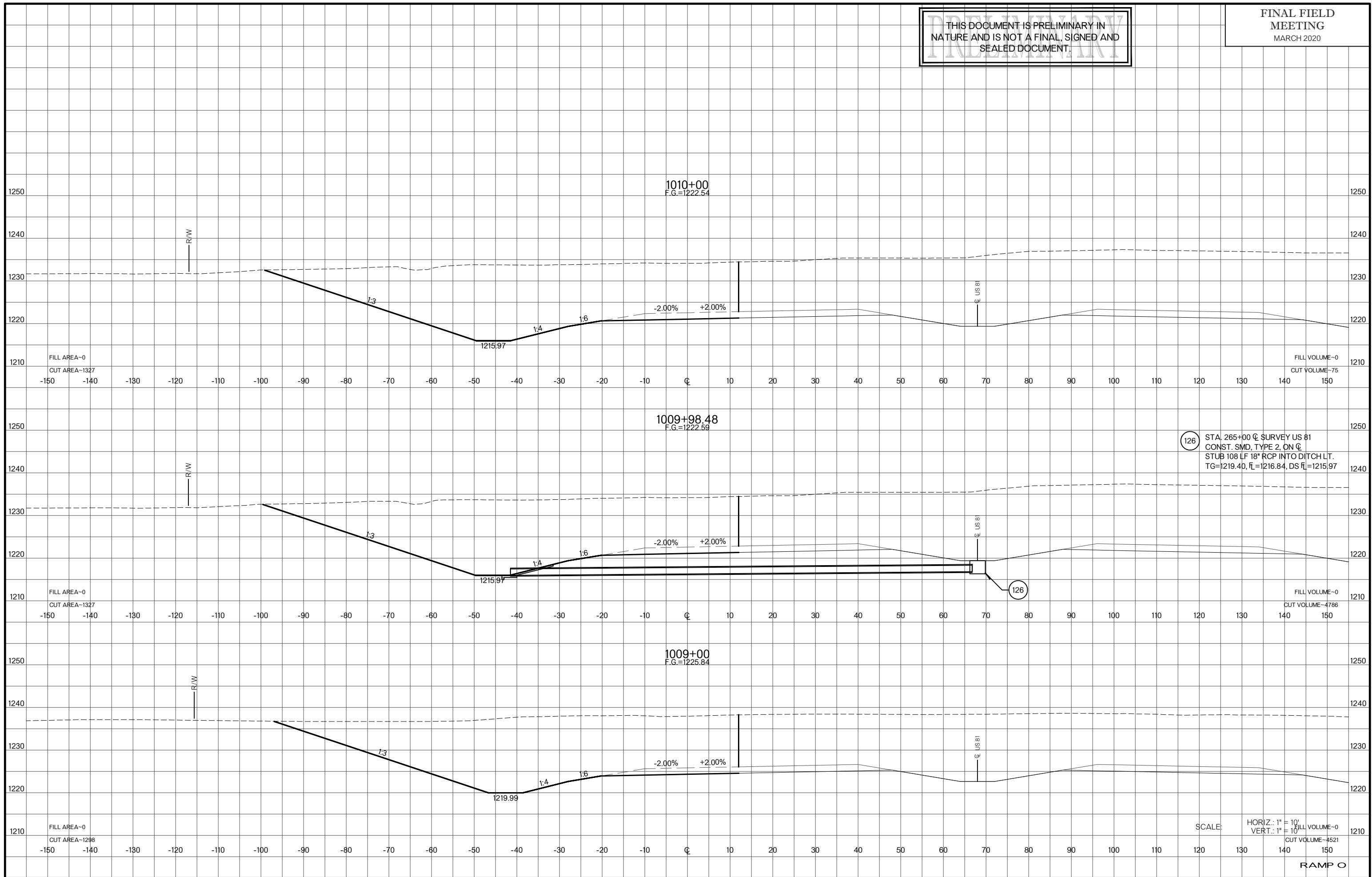
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP O

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

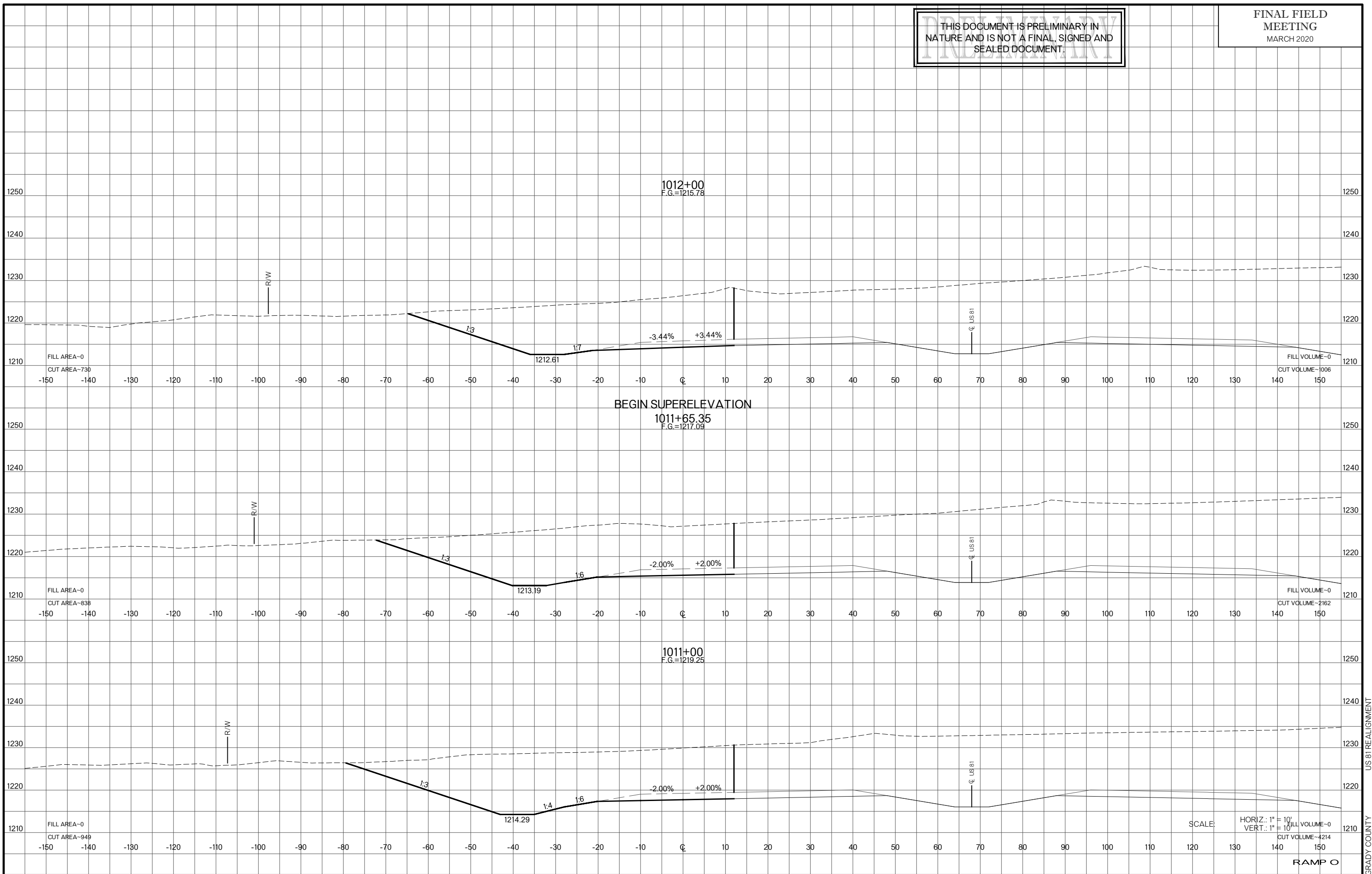


126 STA. 265+00 C SURVEY US 81
CONST. SMD, TYPE 2, ON C
STUB 108 LF 18" RCP INTO DITCH LT.
TG=1219.40, FL=1216.84, DS FL=1215.97

FILL AREA-0
CUT AREA-1298
FILL VOLUME-0
CUT VOLUME-4521

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

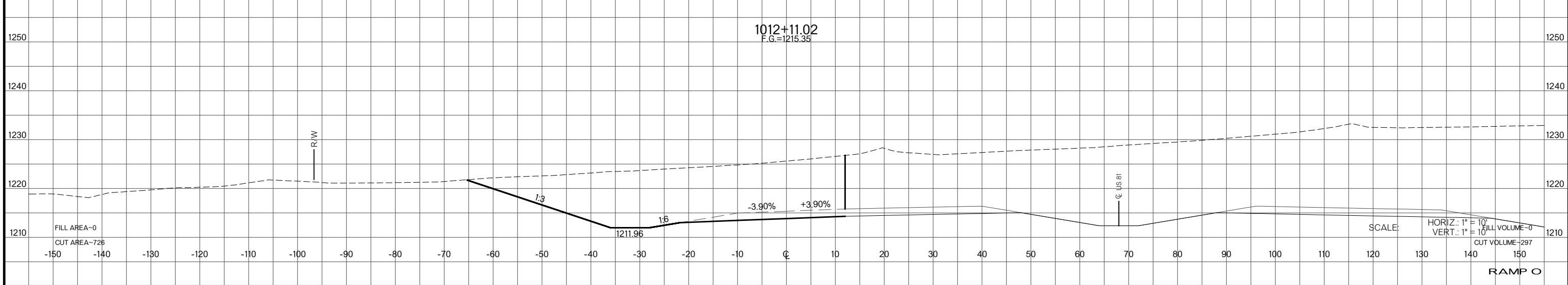
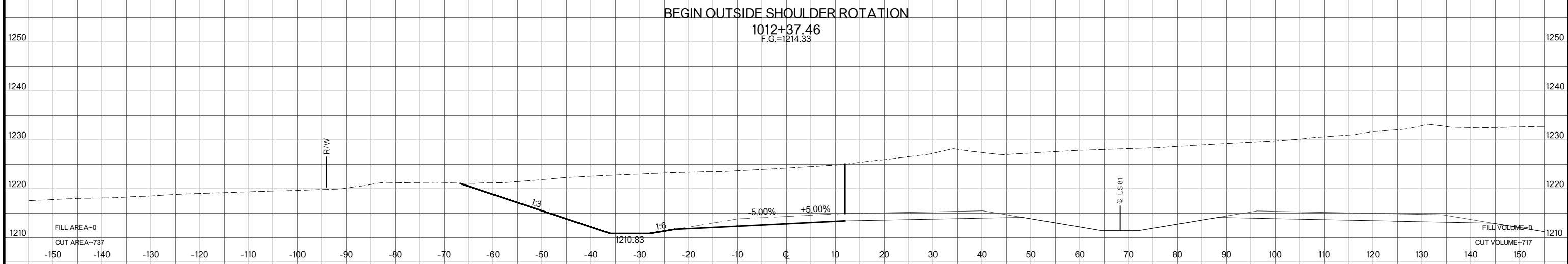
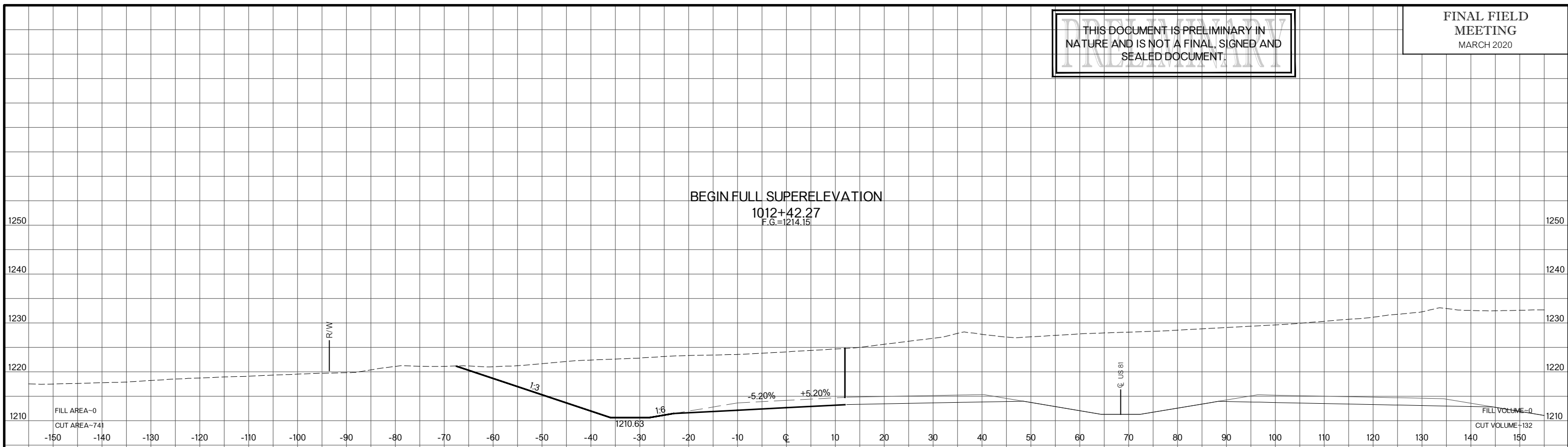


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP O

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

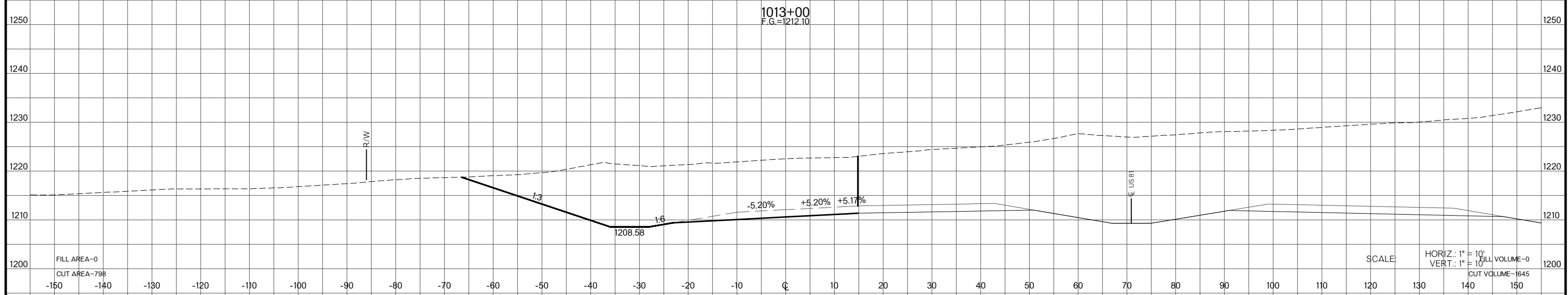
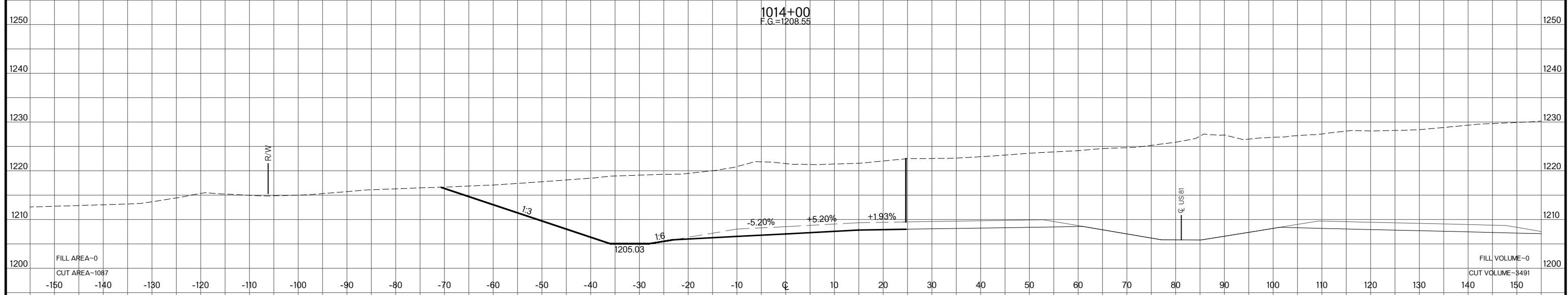
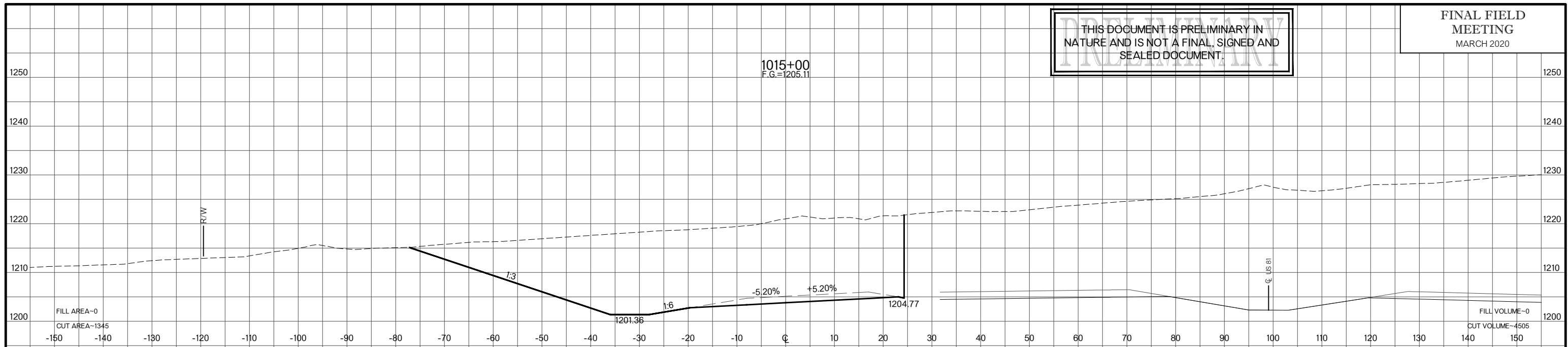
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



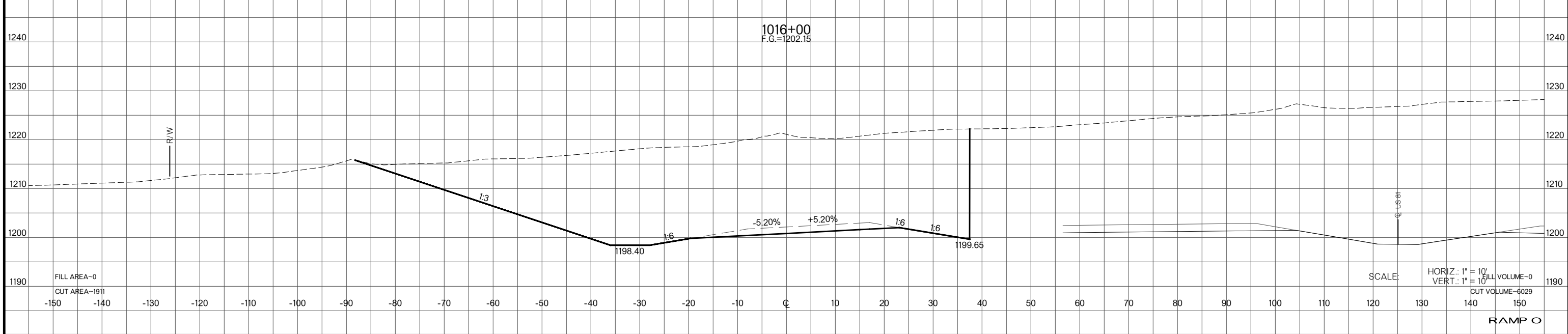
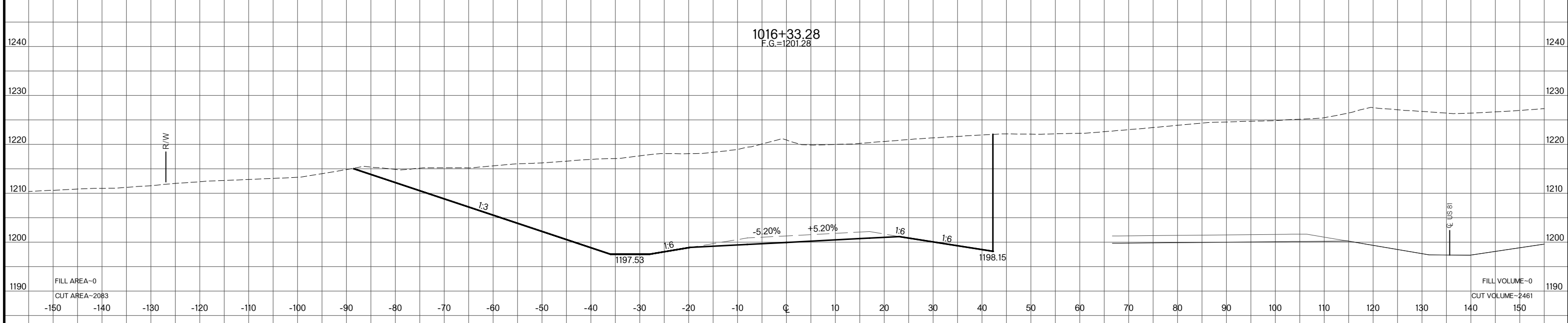
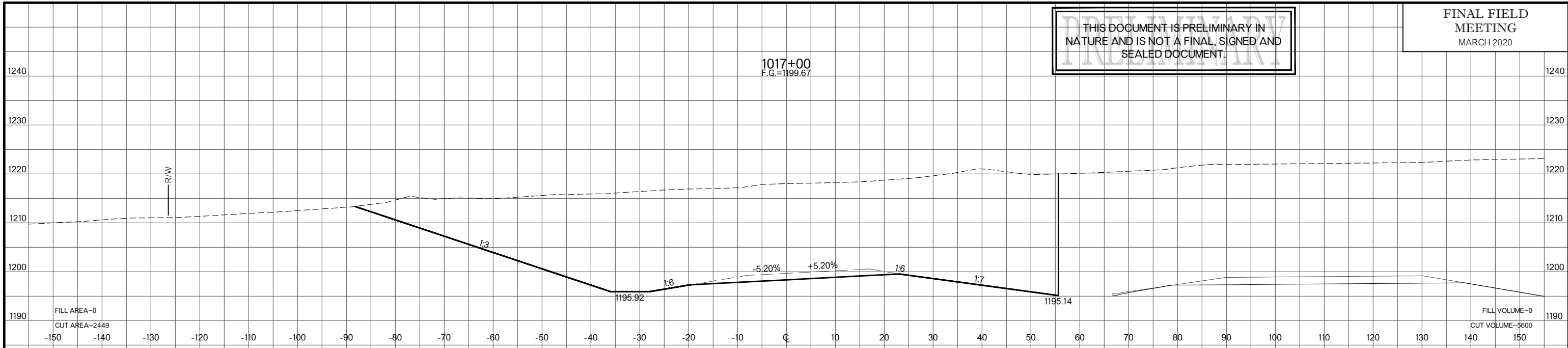
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP O

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



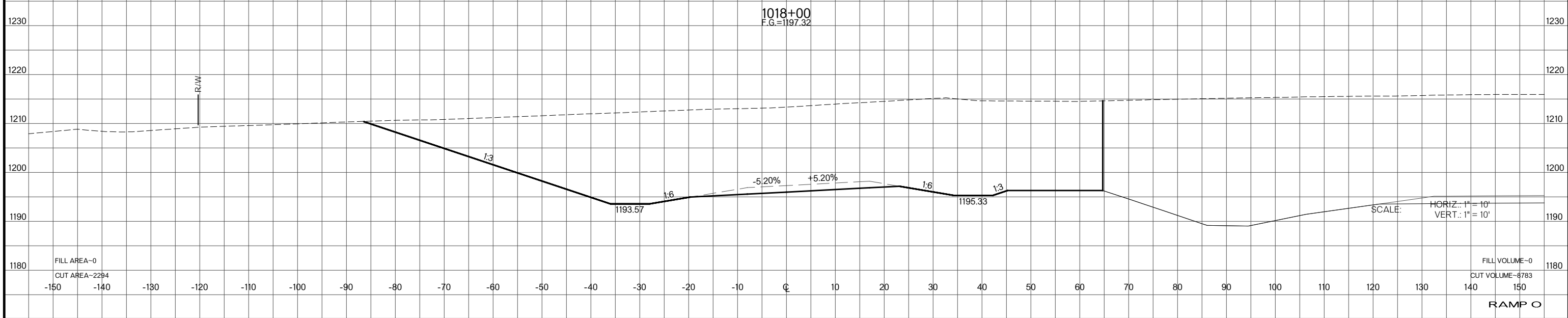
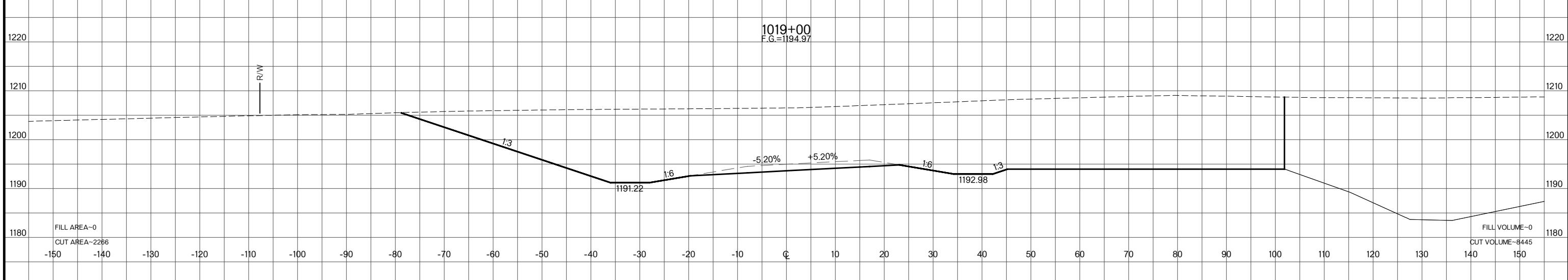
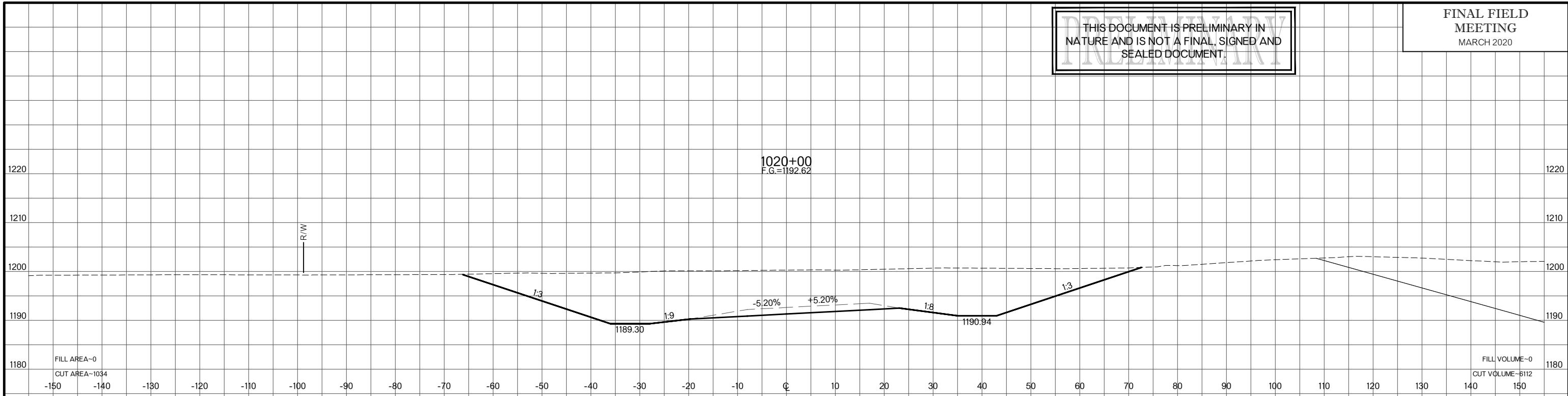
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP O

GRADY COUNTY

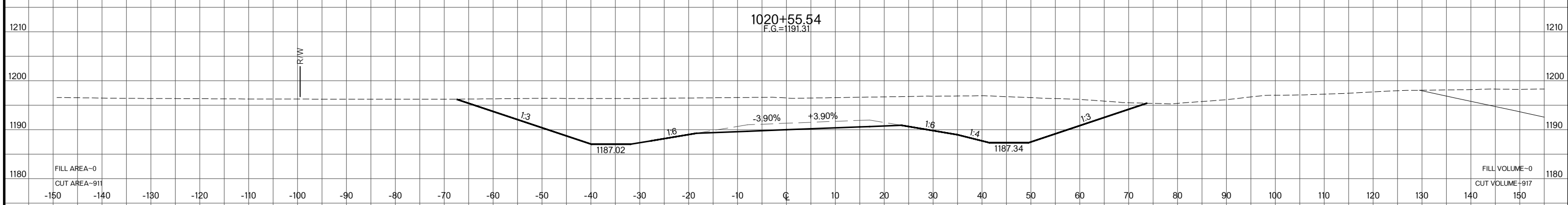
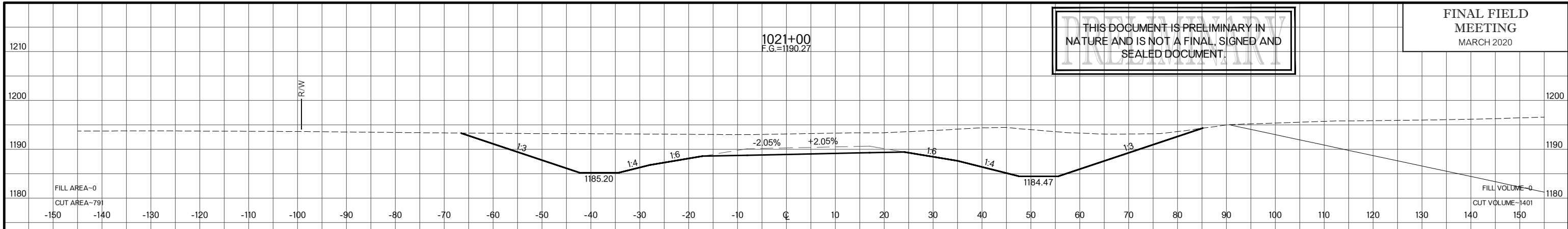
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

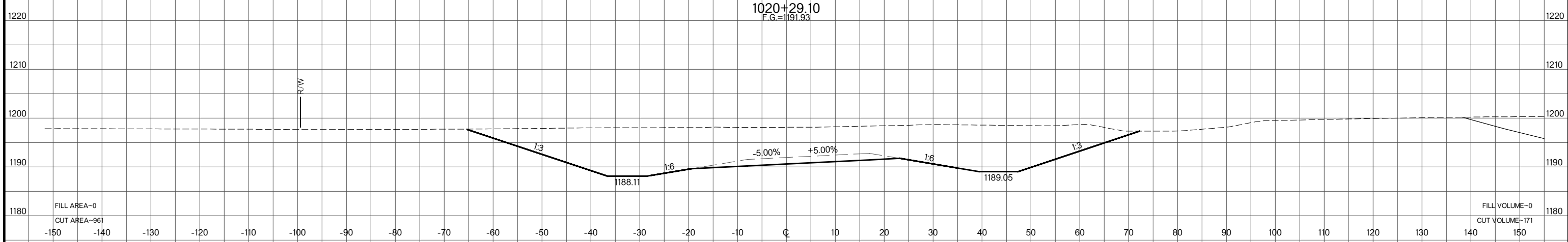


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

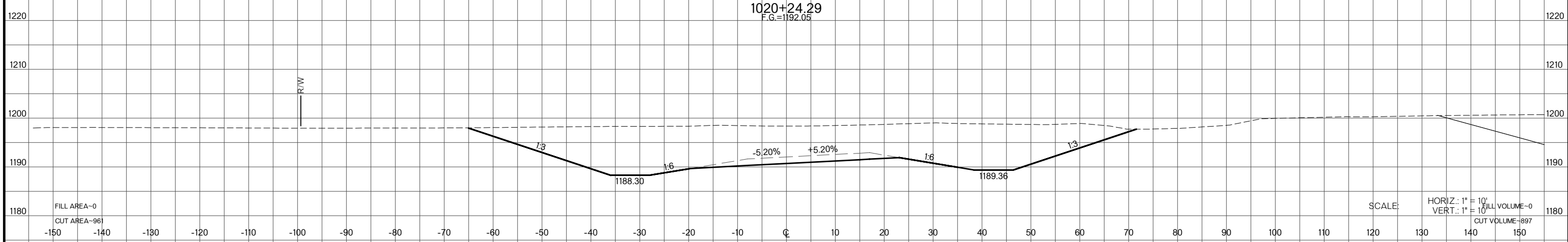
FINAL FIELD MEETING MARCH 2020



END OUTSIDE SHOULDER ROTATION



END FULL SUPERELEVATION



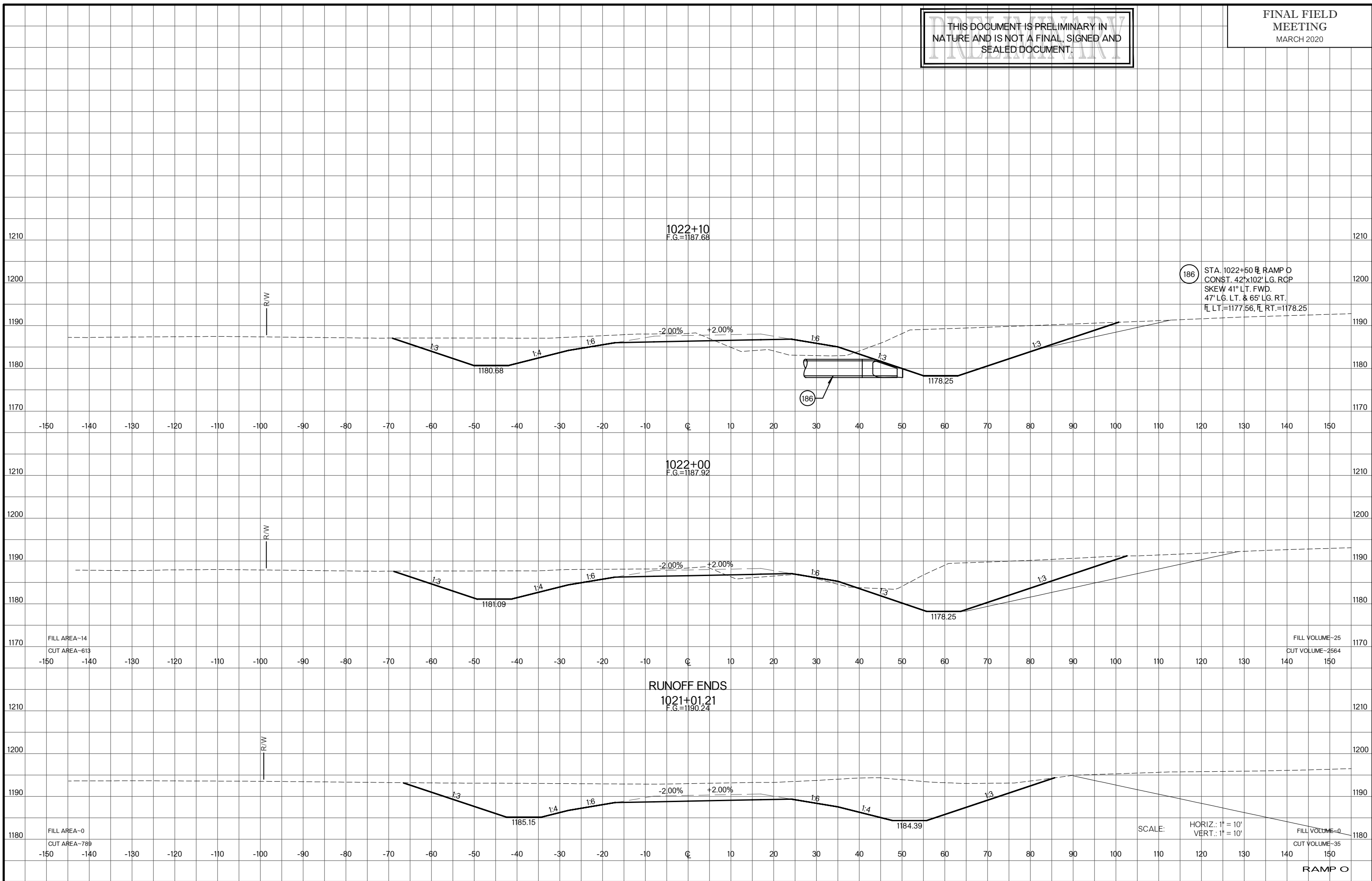
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP O

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

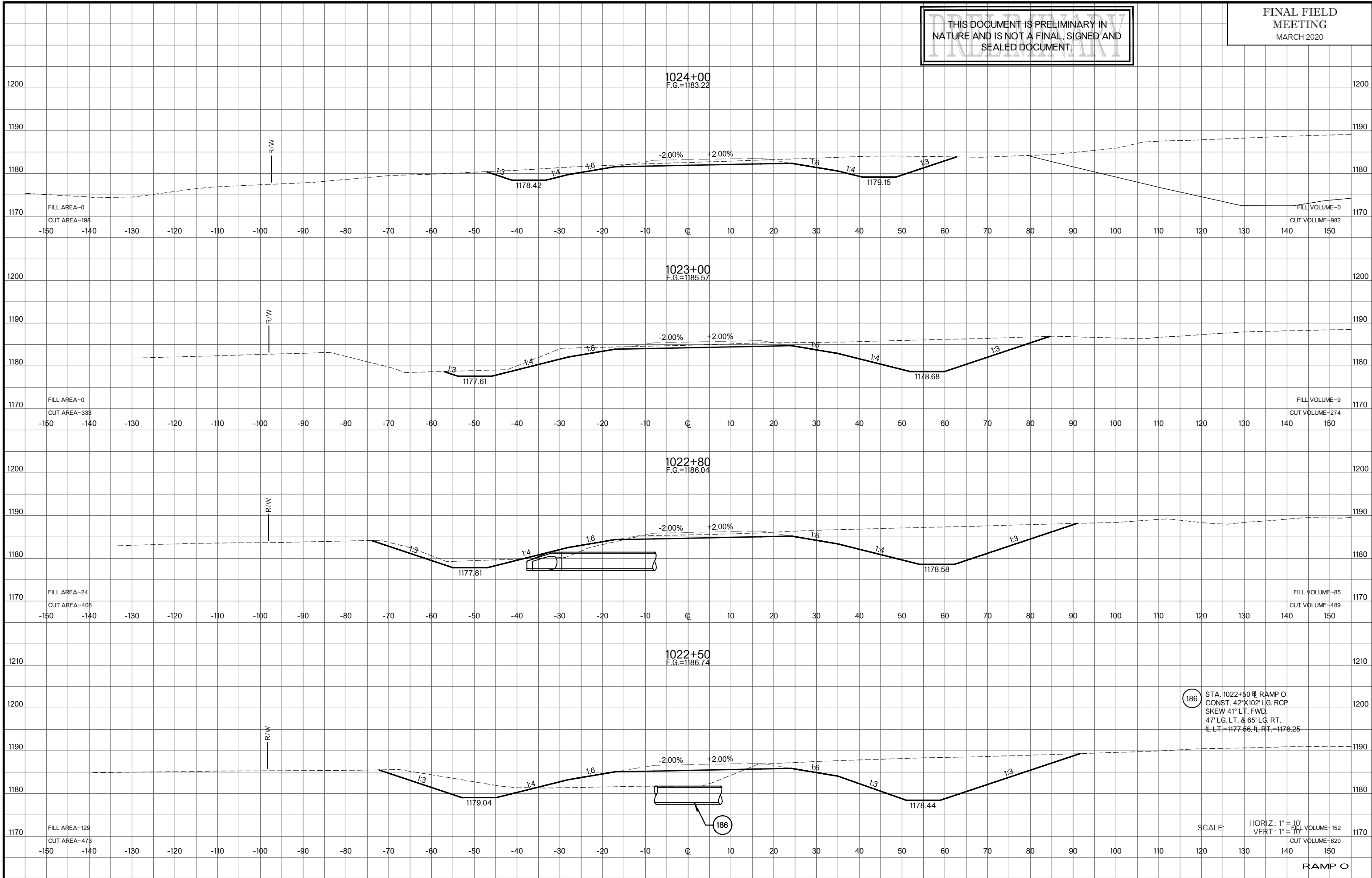
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

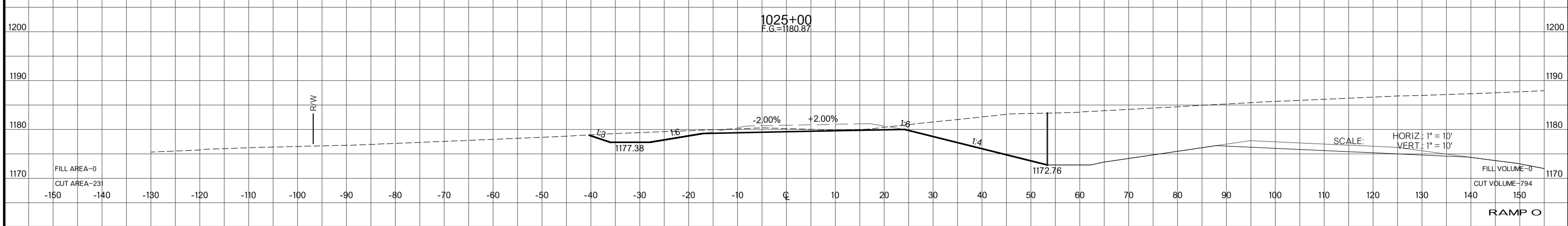
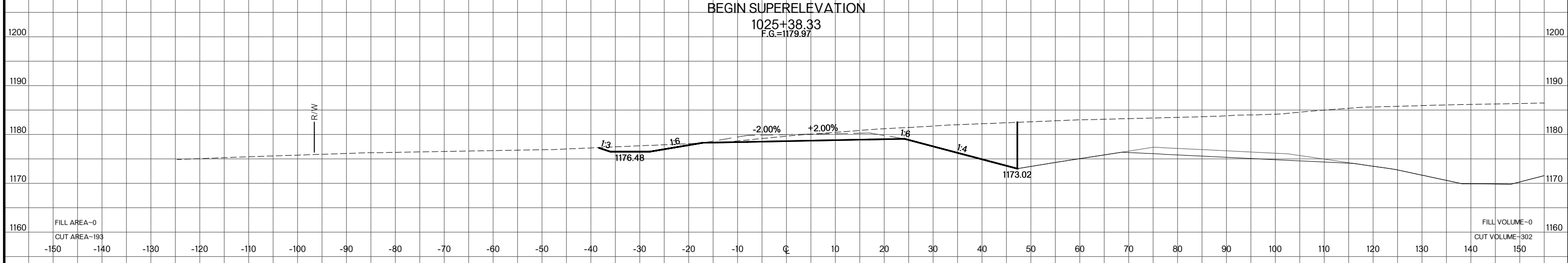
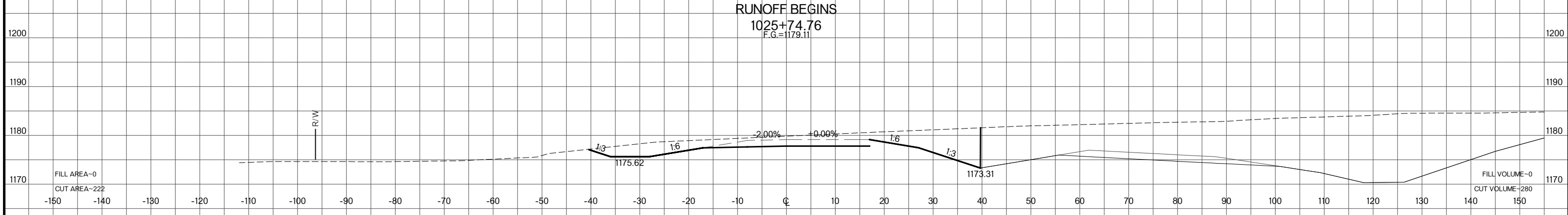
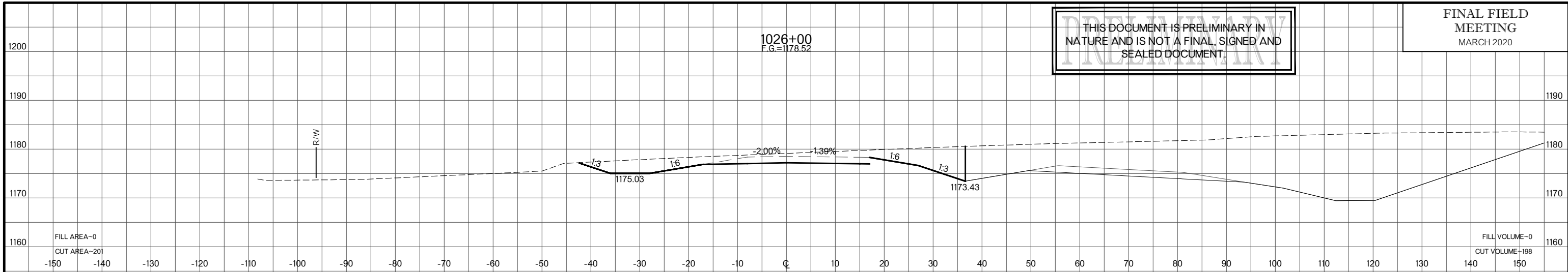
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020

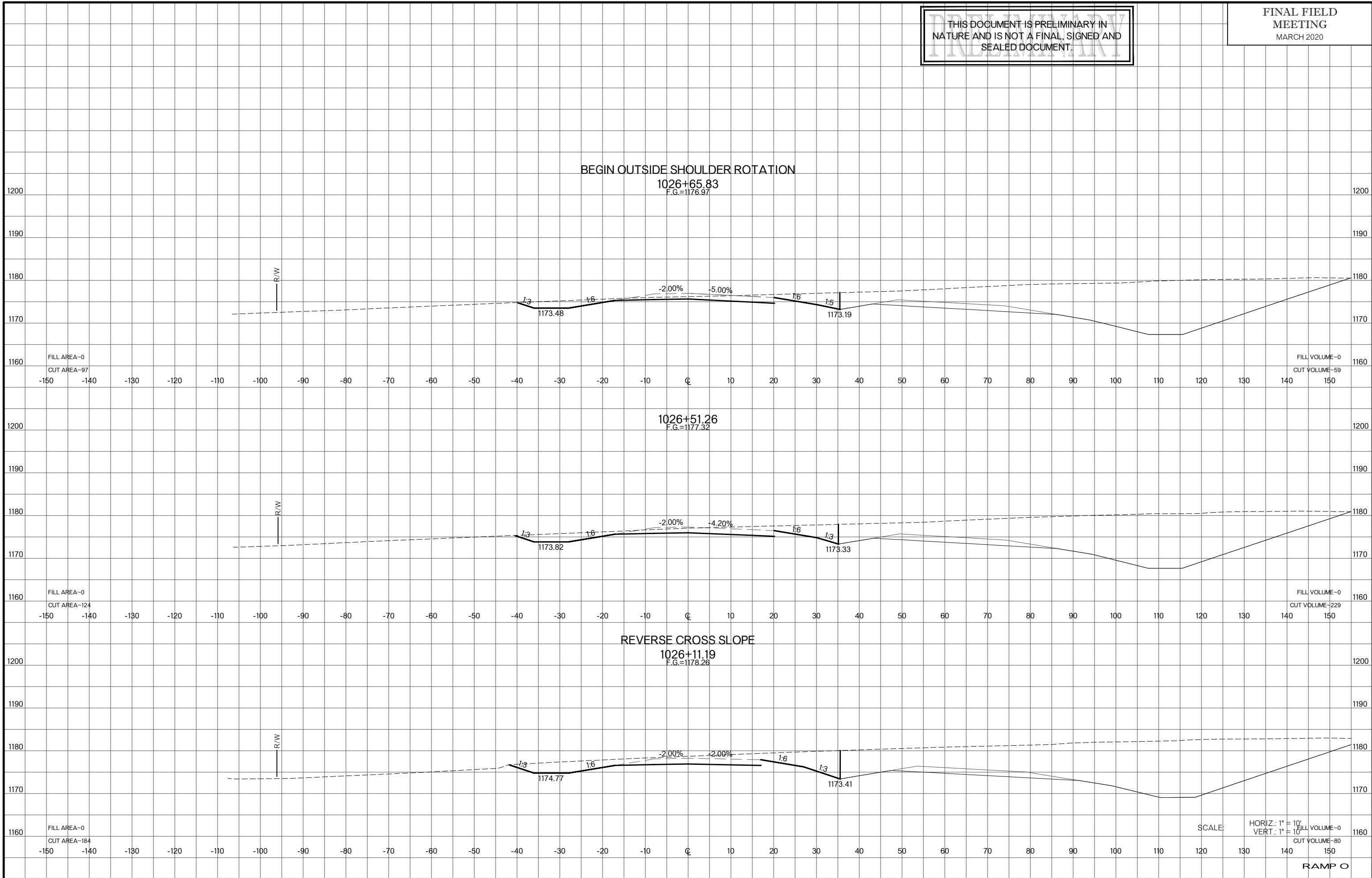


SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

US 81 REALIGNMENT GRADY COUNTY

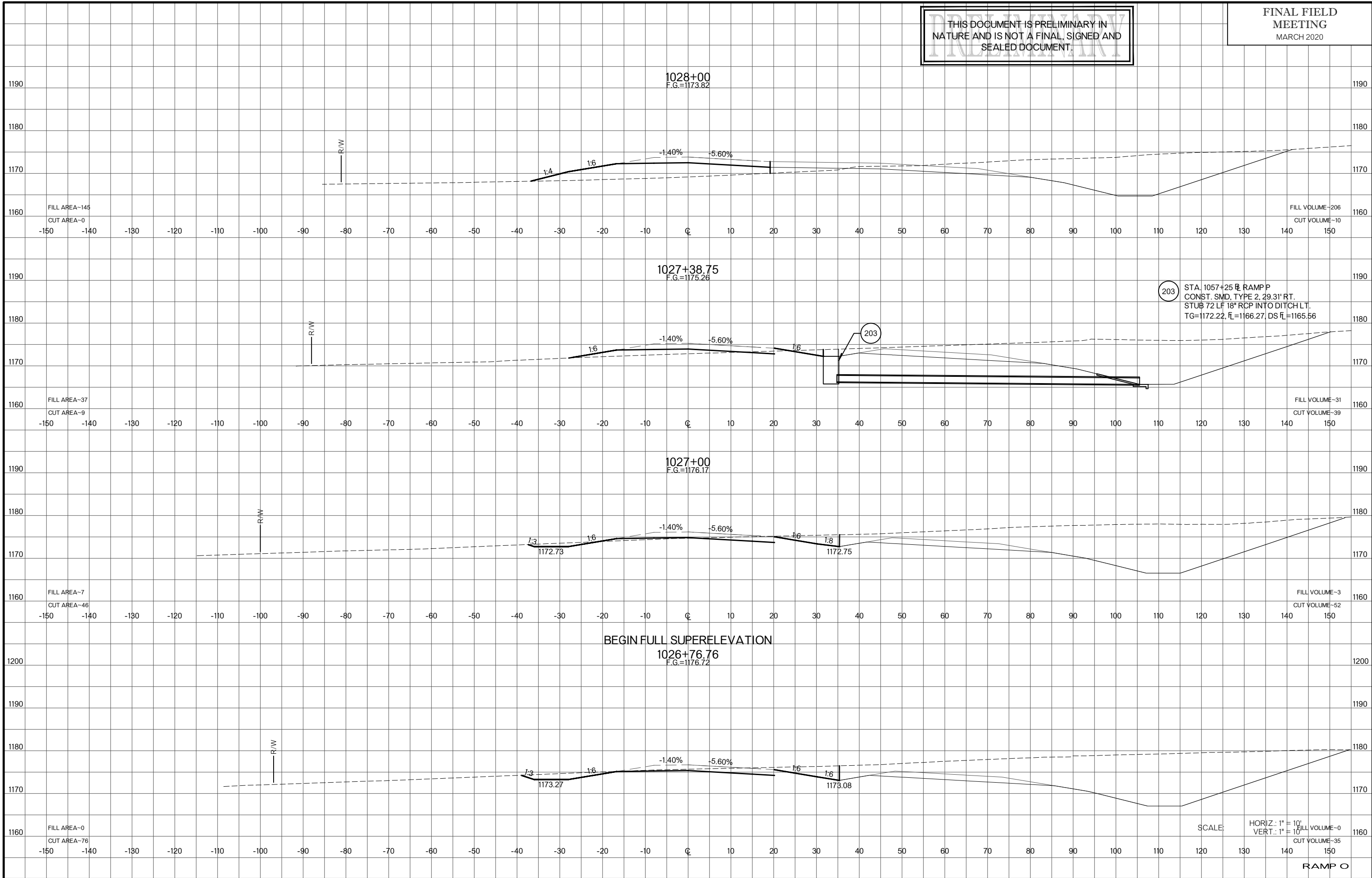
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

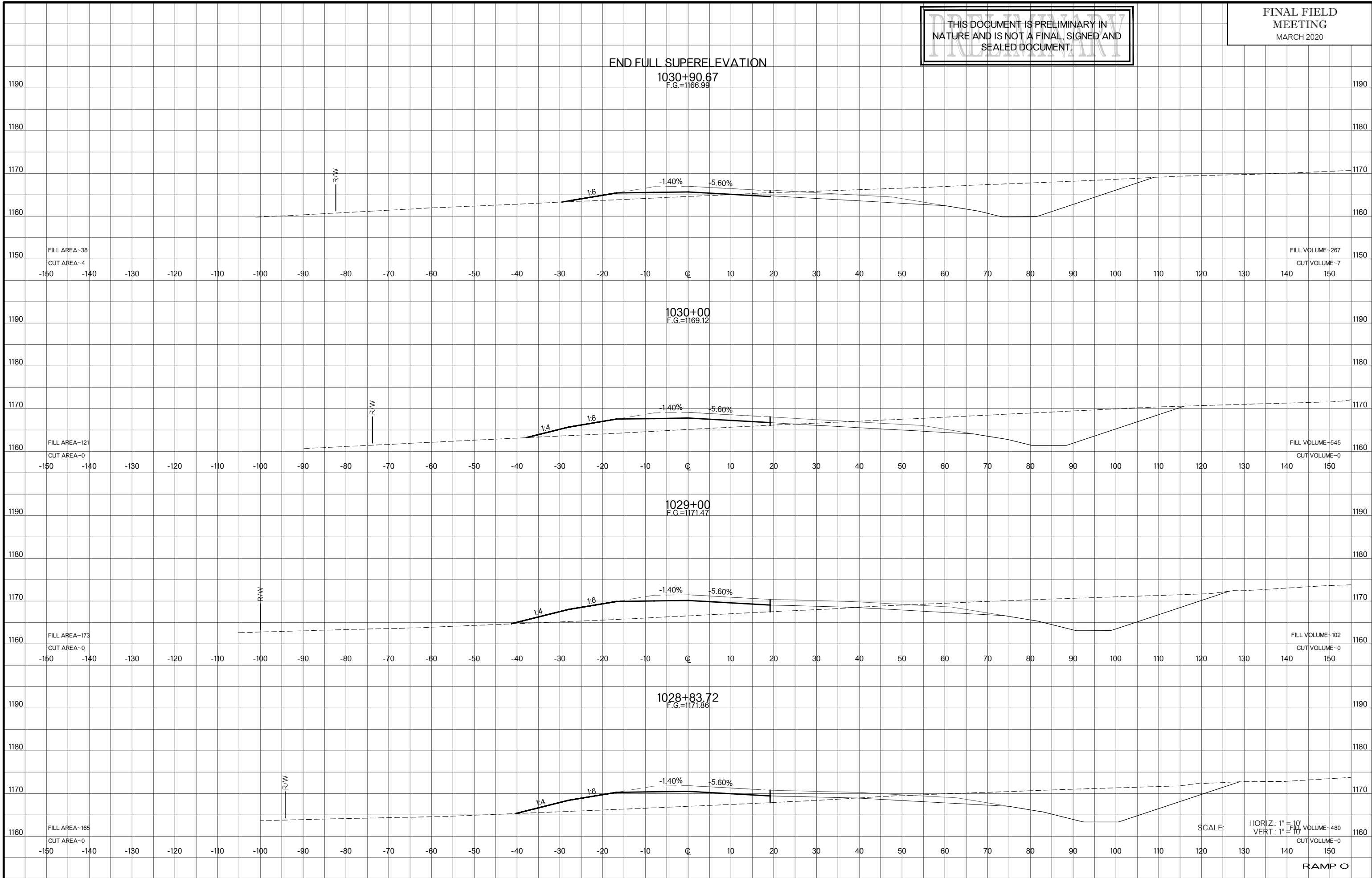
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

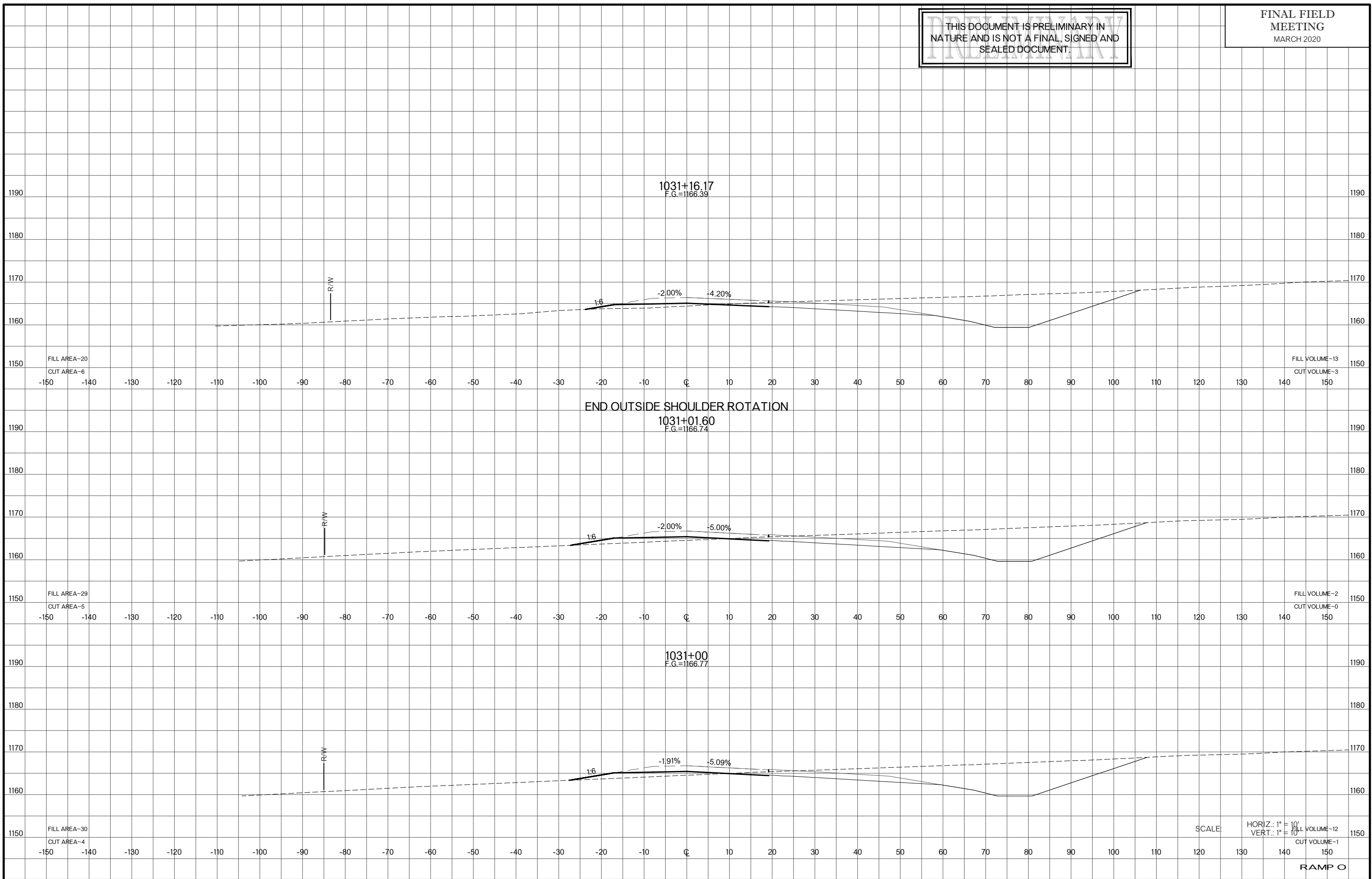


US 81 REALIGNMENT
GRADY COUNTY

RAMP O

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

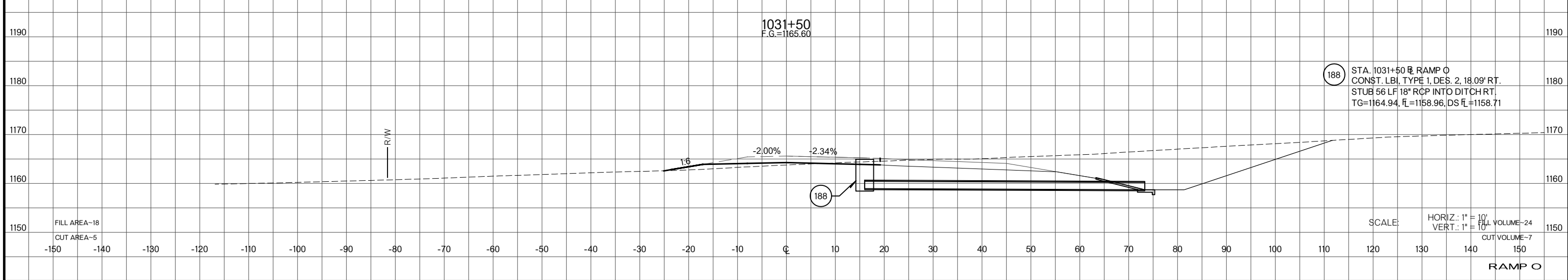
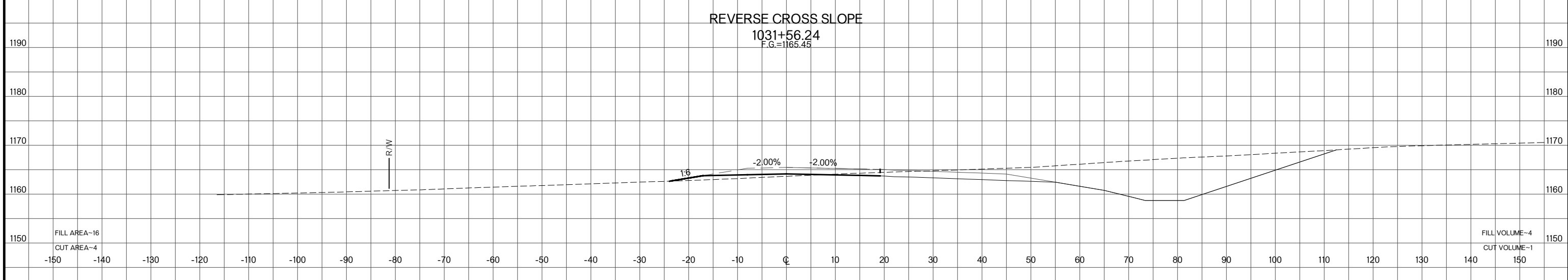
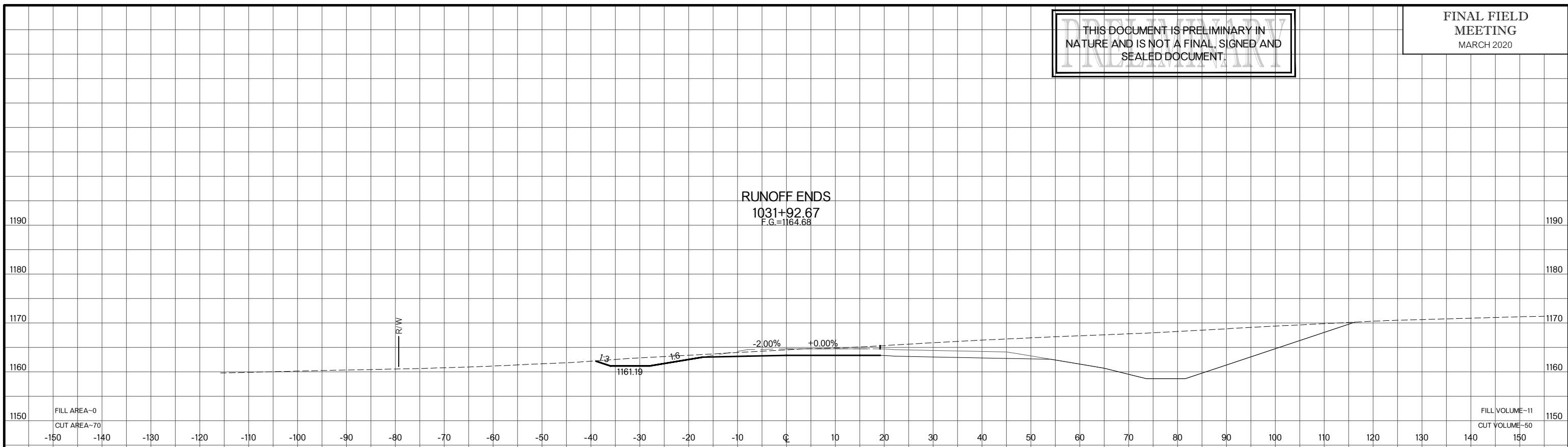
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

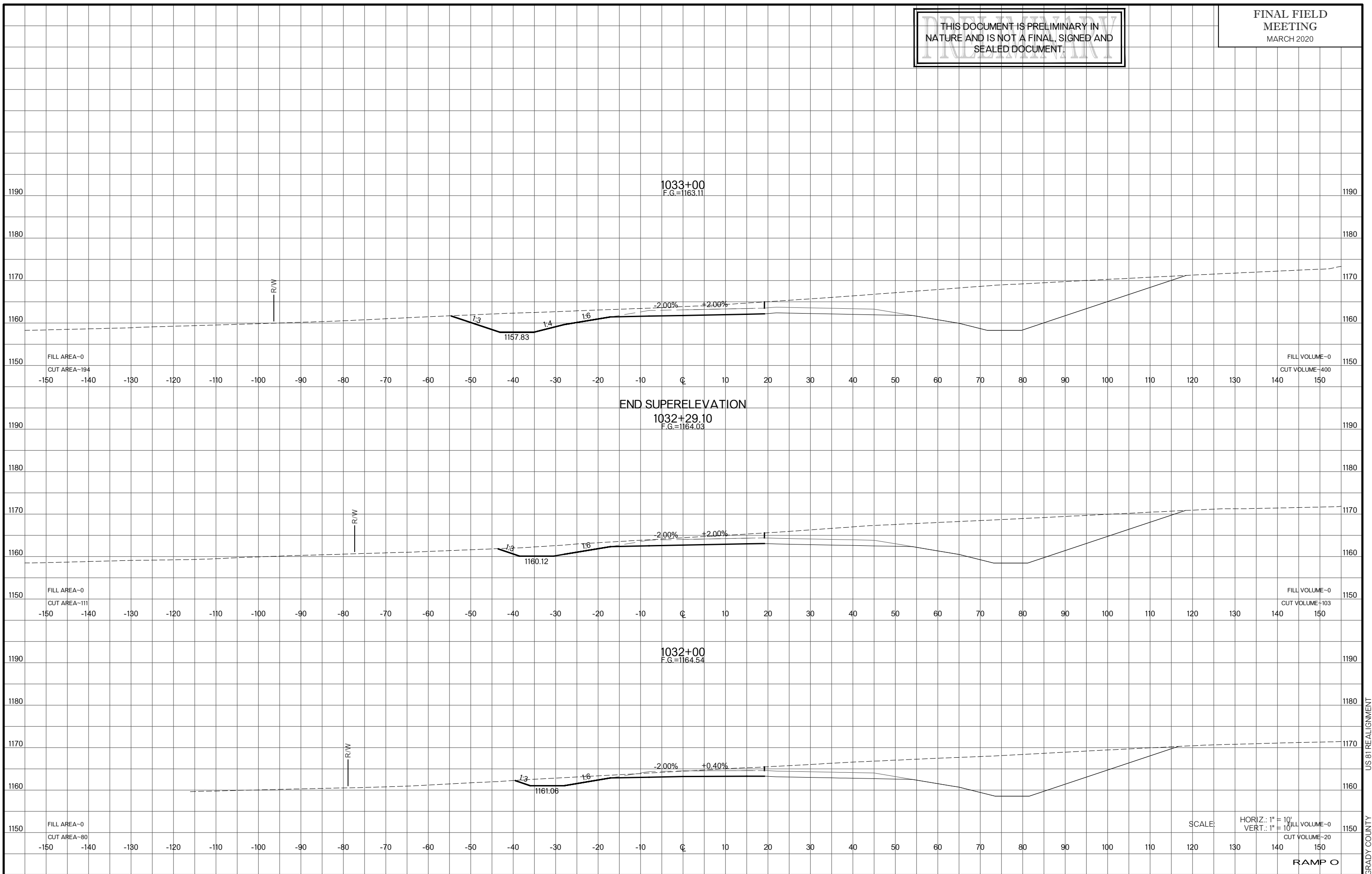


188 STA. 1031+50 RAMP O
CONST. LBI, TYPE 1, DES. 2, 18.09' RT.
STUB 56 LF 18" RCP INTO DITCH RT.
TG=1164.94, FL=1158.96, DS FL=1158.71

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

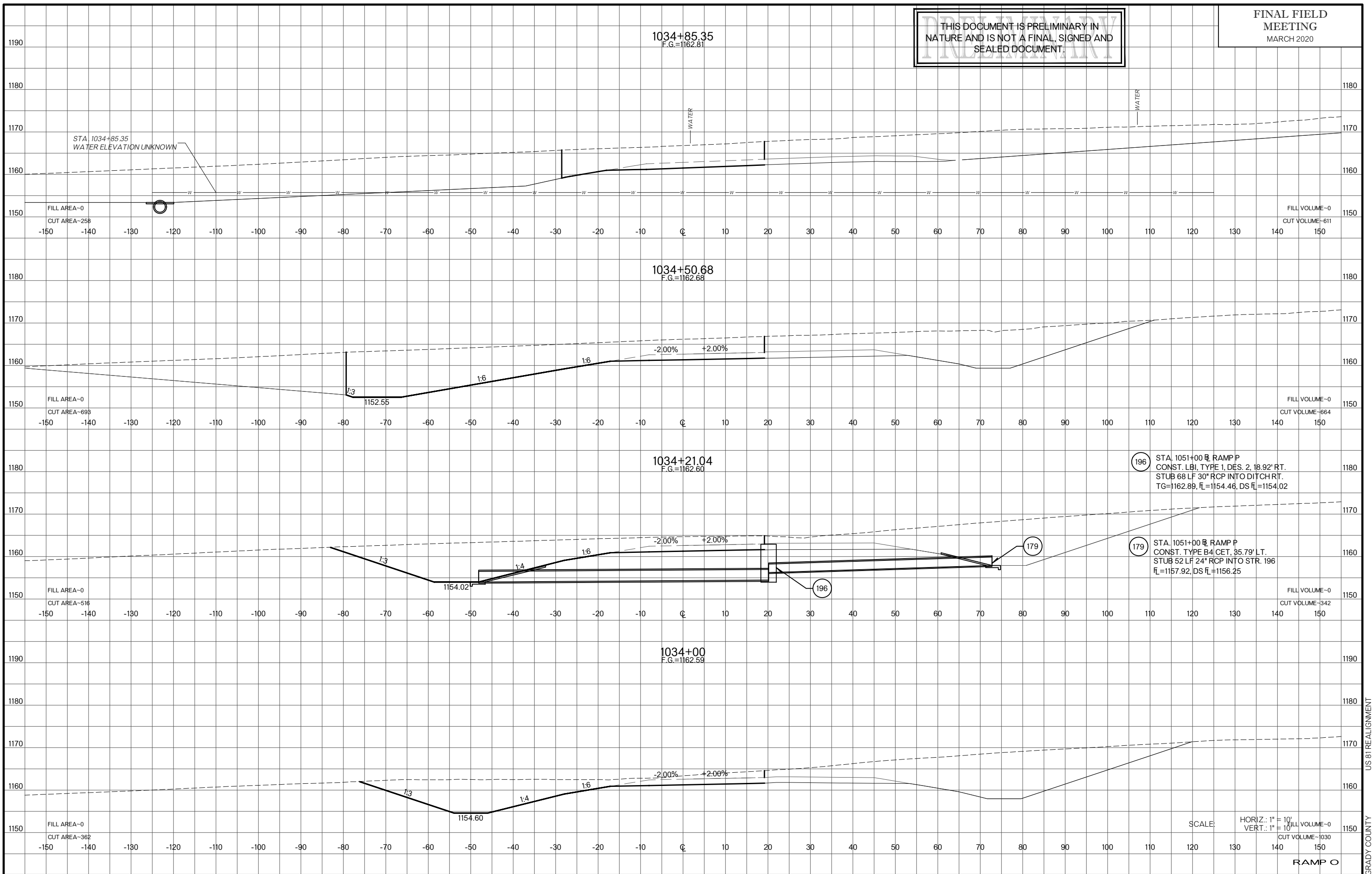


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP O

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



196 STA. 1051+00 RAMP P
CONST. LBI, TYPE 1, DES. 2, 18.92' RT.
STUB 68 LF 30" RCP INTO DITCH RT.
TG=1162.89, FL=1154.46, DS FL=1154.02

179 STA. 1051+00 RAMP P
CONST. TYPE B4 CET, 35.79' LT.
STUB 52 LF 24" RCP INTO STR. 196
FL=1157.92, DS FL=1156.25

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

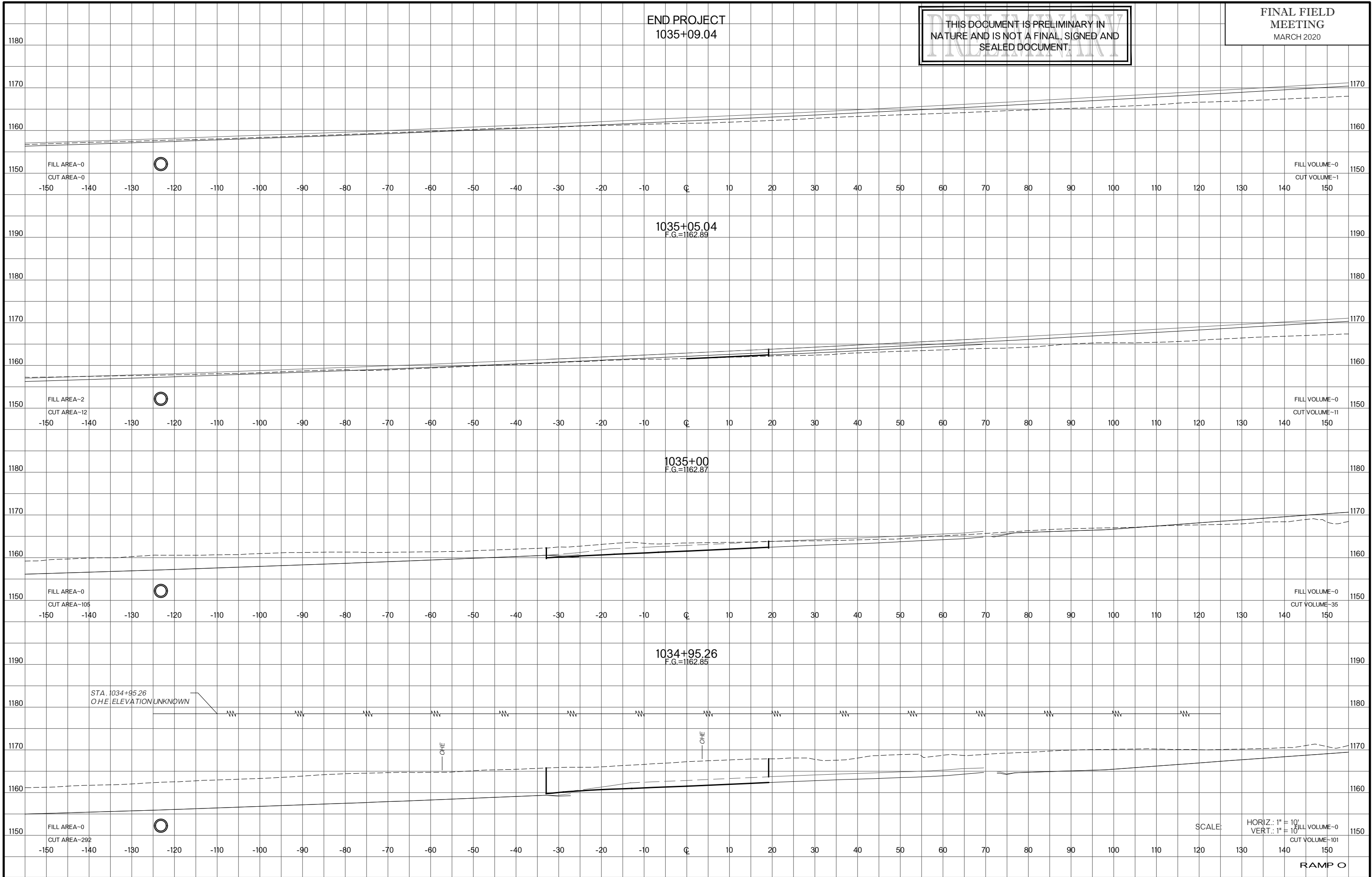
RAMP O

US 81 REALIGNMENT
GRADY COUNTY

END PROJECT
1035+09.04

THIS DOCUMENT IS PRELIMINARY IN
NATURE AND IS NOT A FINAL, SIGNED AND
SEALED DOCUMENT.

FINAL FIELD
MEETING
MARCH 2020



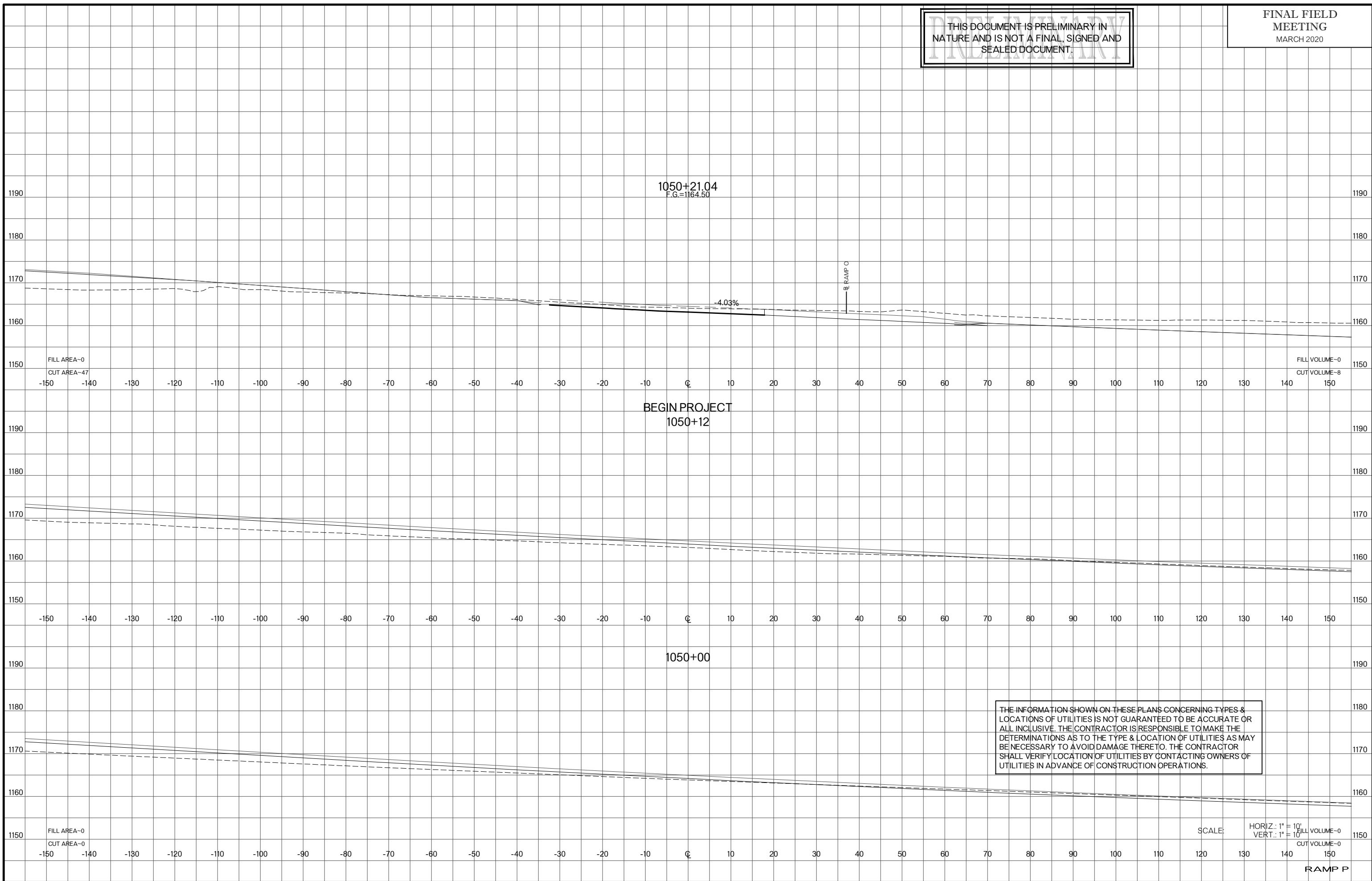
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP O

US 81 REALIGNMENT
GRADY COUNTY

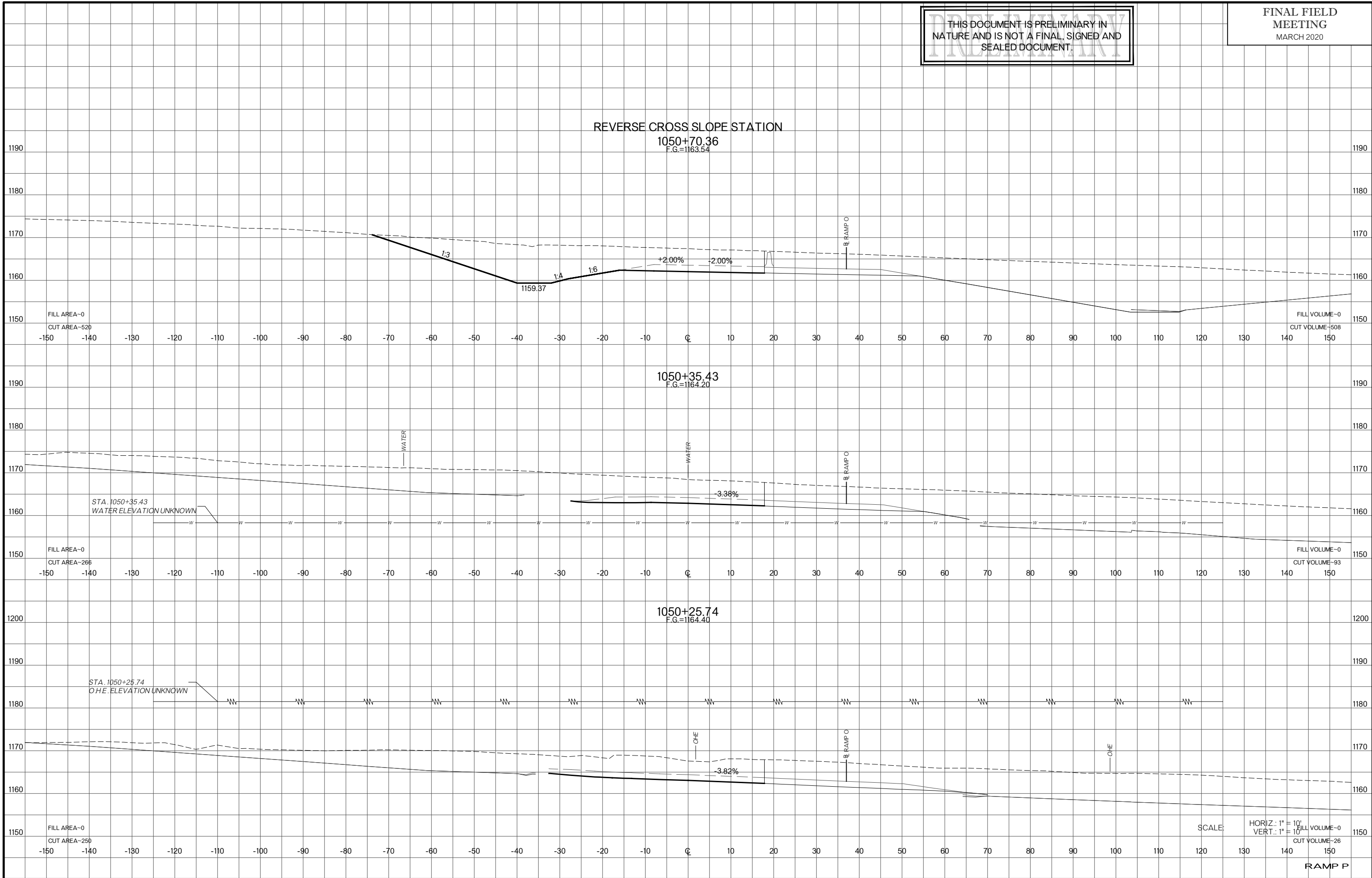
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

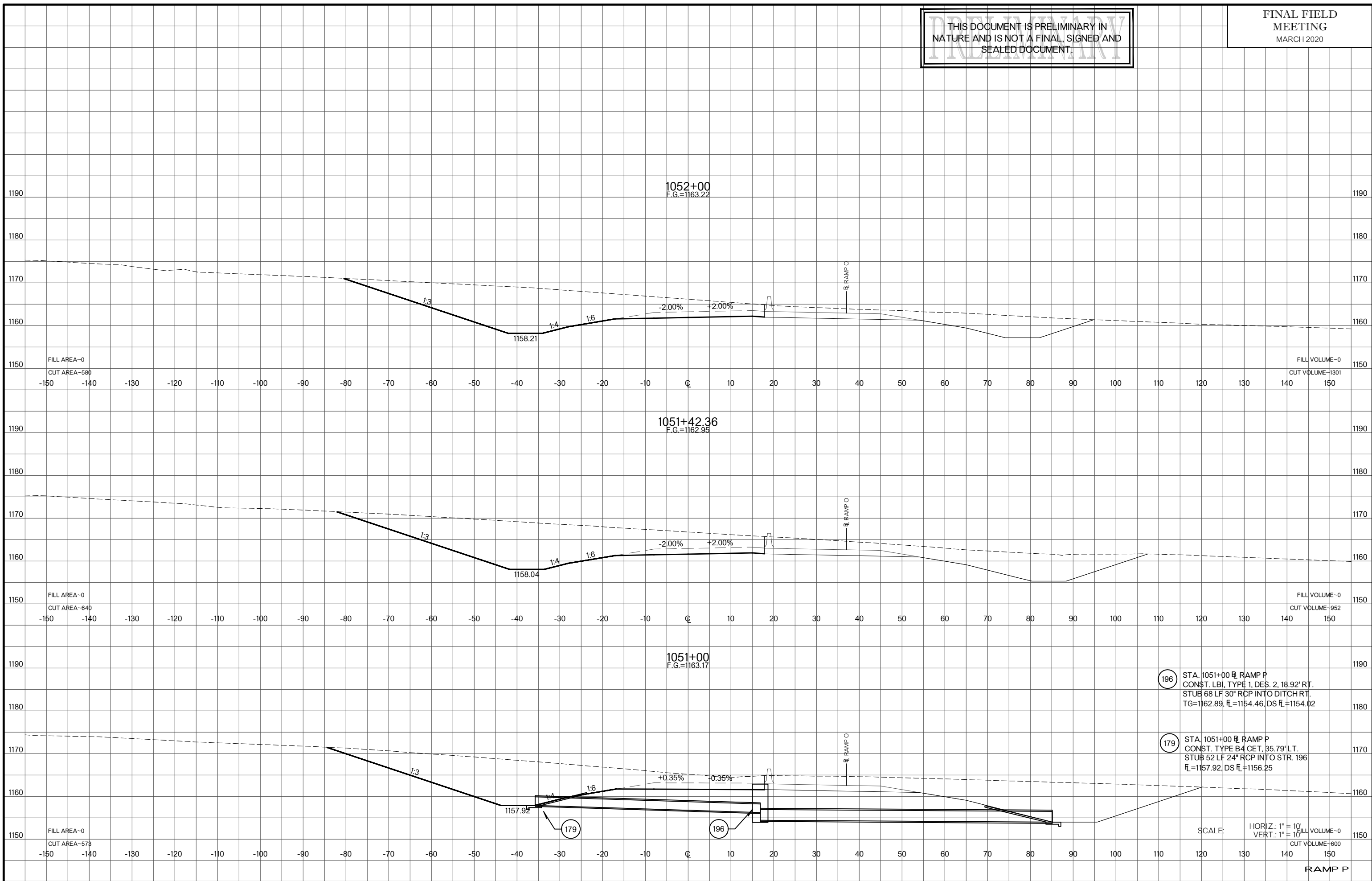
FINAL FIELD MEETING MARCH 2020



US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



196 STA. 1051+00 B RAMP P
CONST. LBI, TYPE 1, DES. 2, 18.92' RT.
STUB 68 LF 30" RCP INTO DITCH RT.
TG=1162.89, FL=1154.46, DS FL=1154.02

179 STA. 1051+00 B RAMP P
CONST. TYPE B4 CET, 35.79' LT.
STUB 52 LF 24" RCP INTO STR. 196
FL=1157.92, DS FL=1156.25

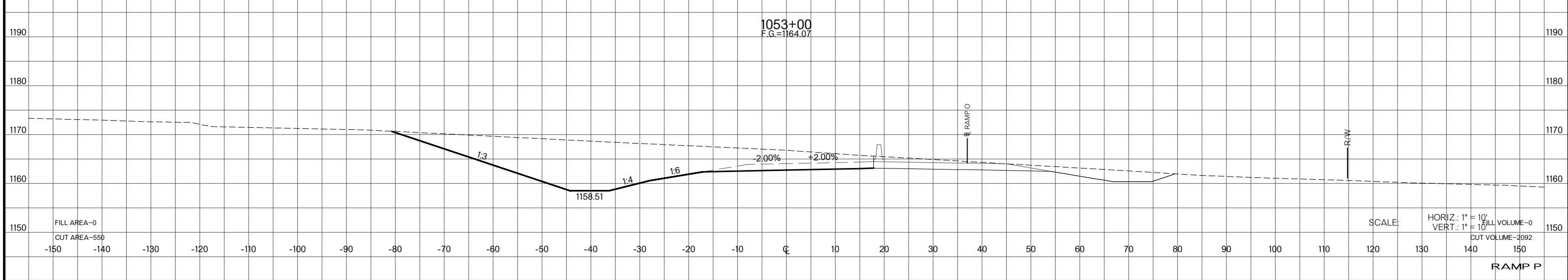
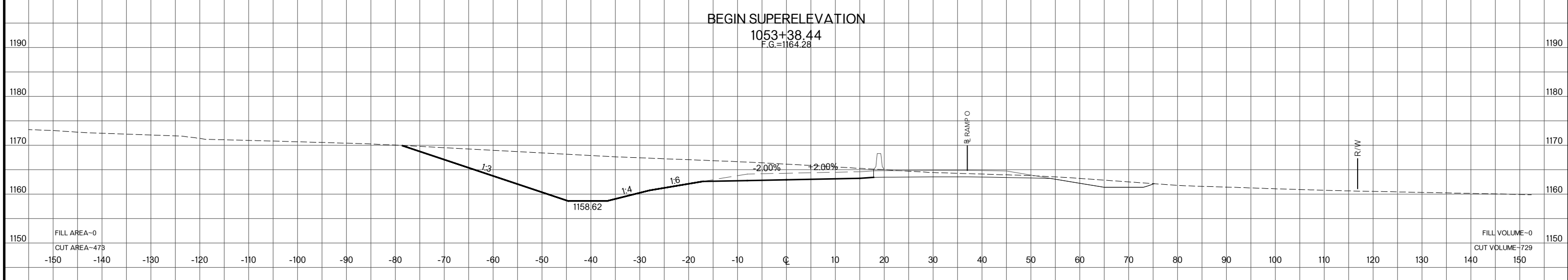
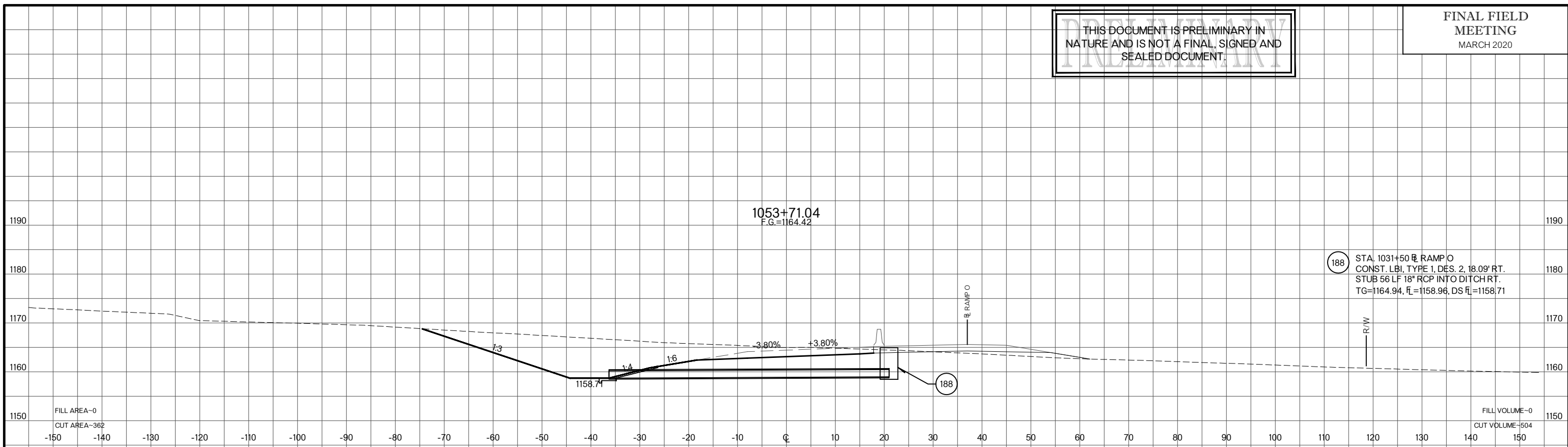
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP P

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



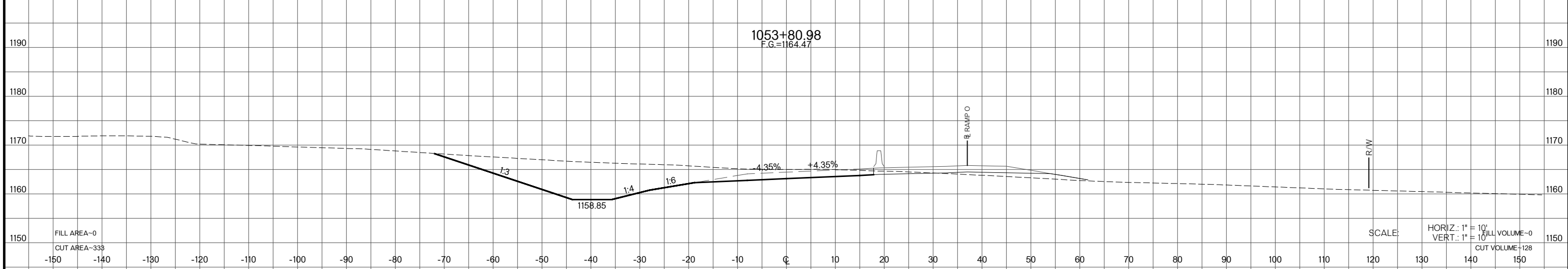
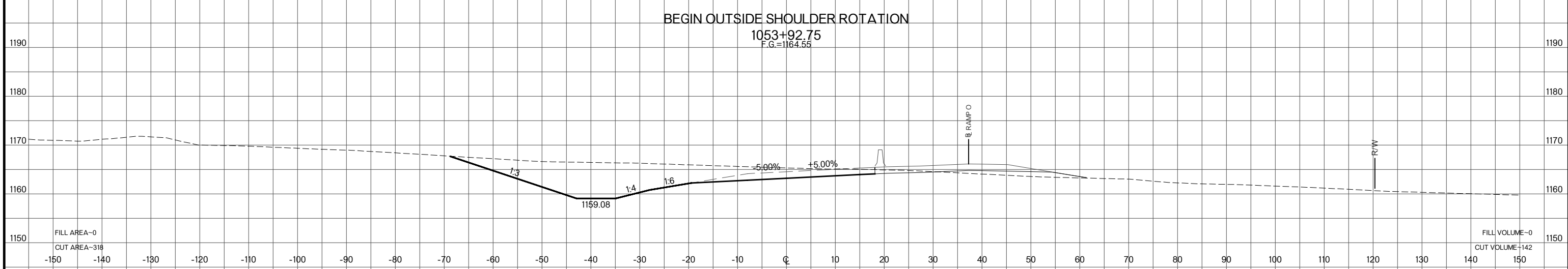
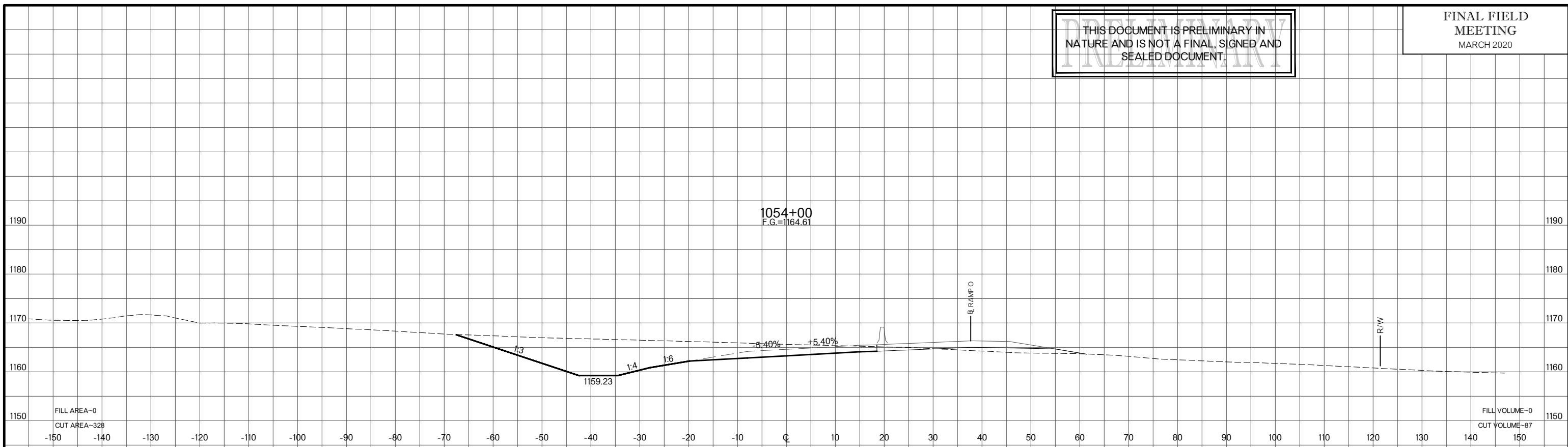
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP P

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



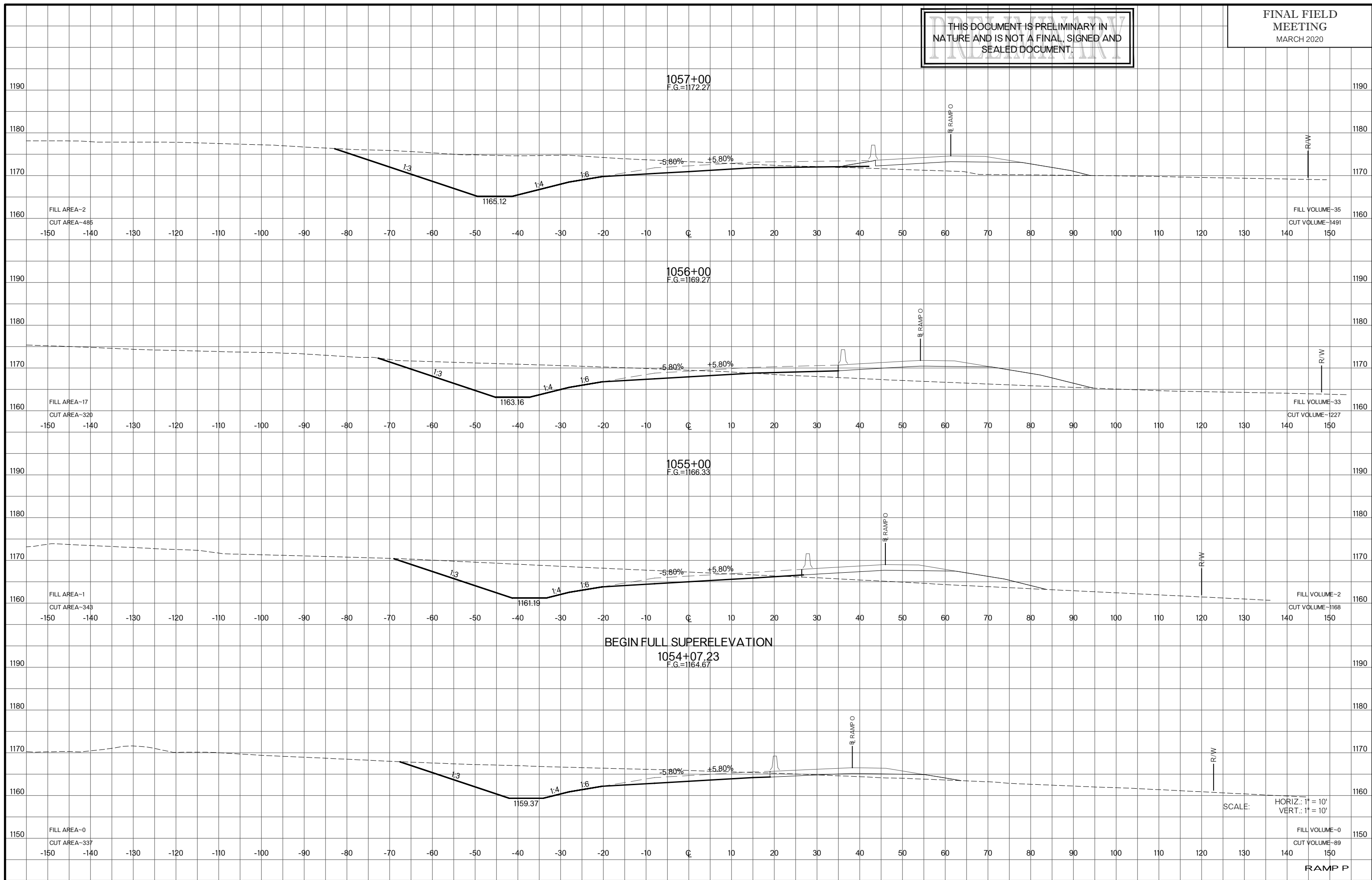
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP P

US 81 REALIGNMENT
GRADY COUNTY

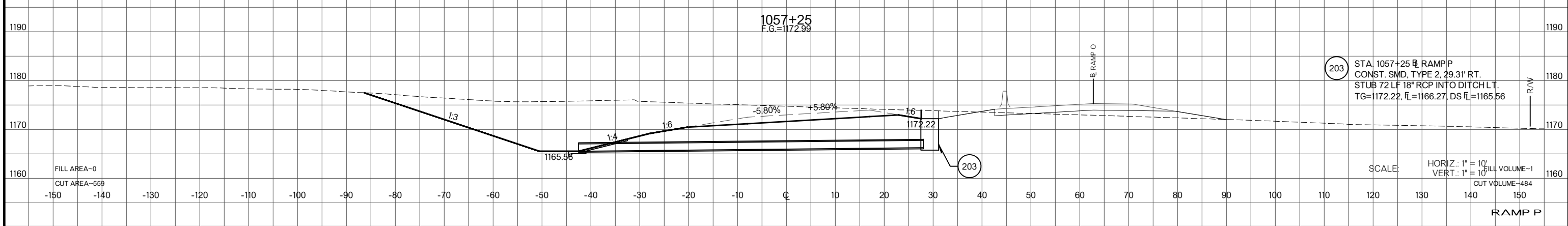
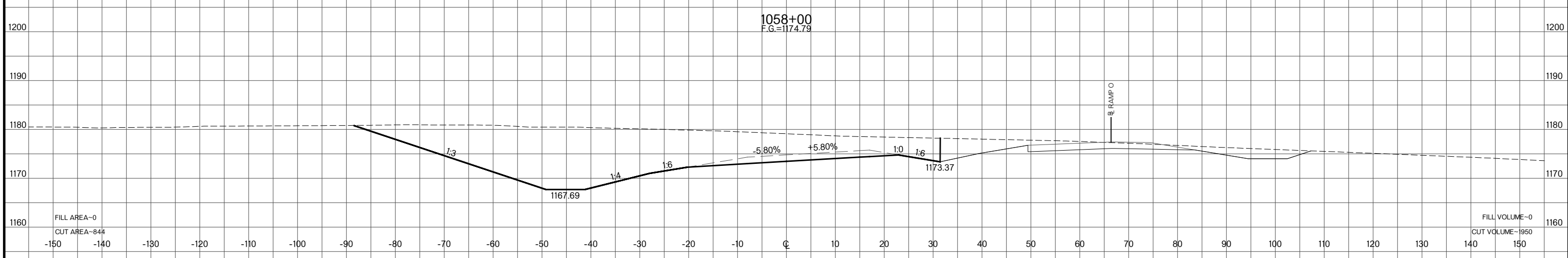
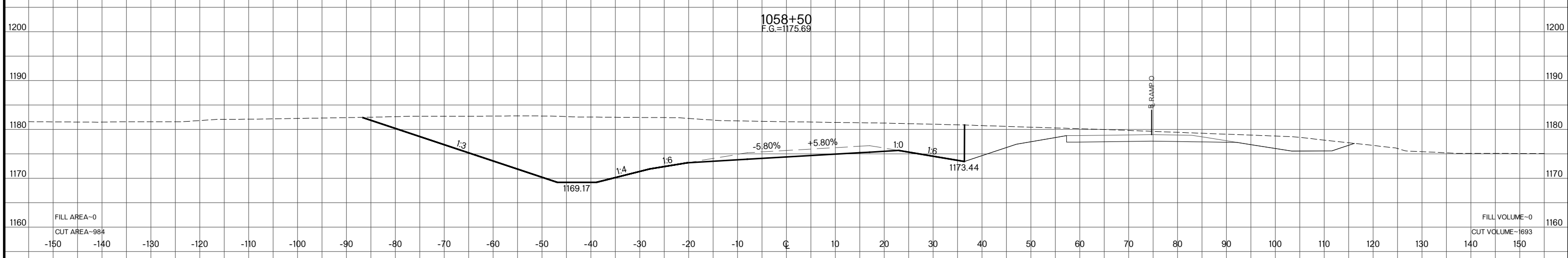
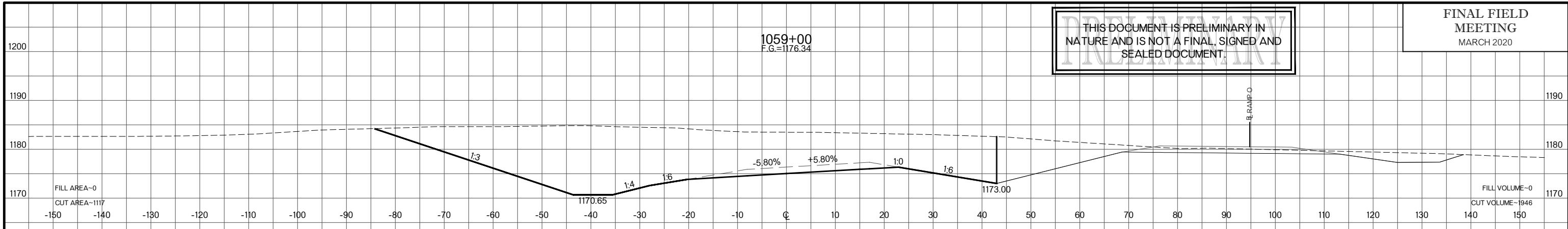
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



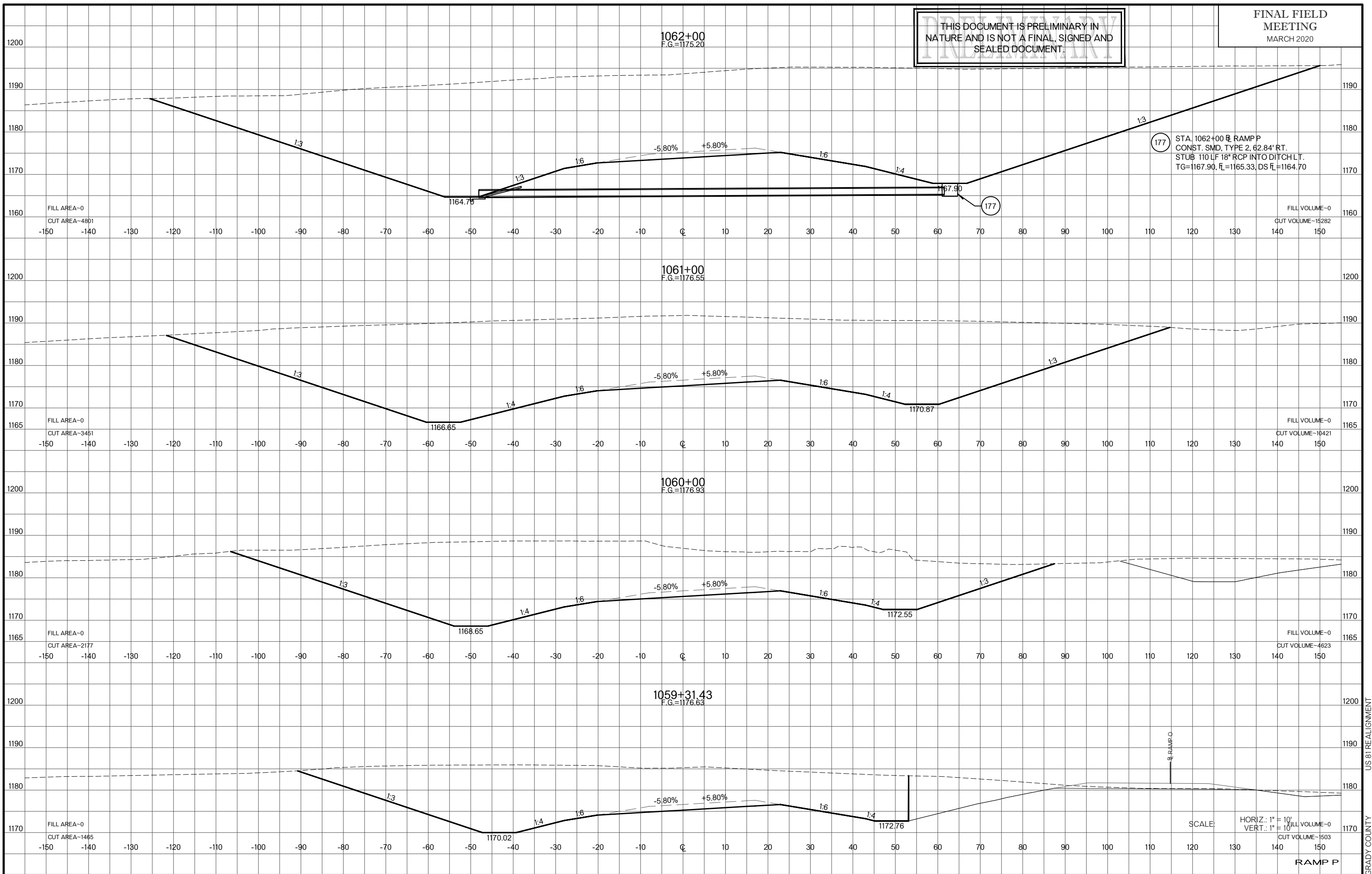
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



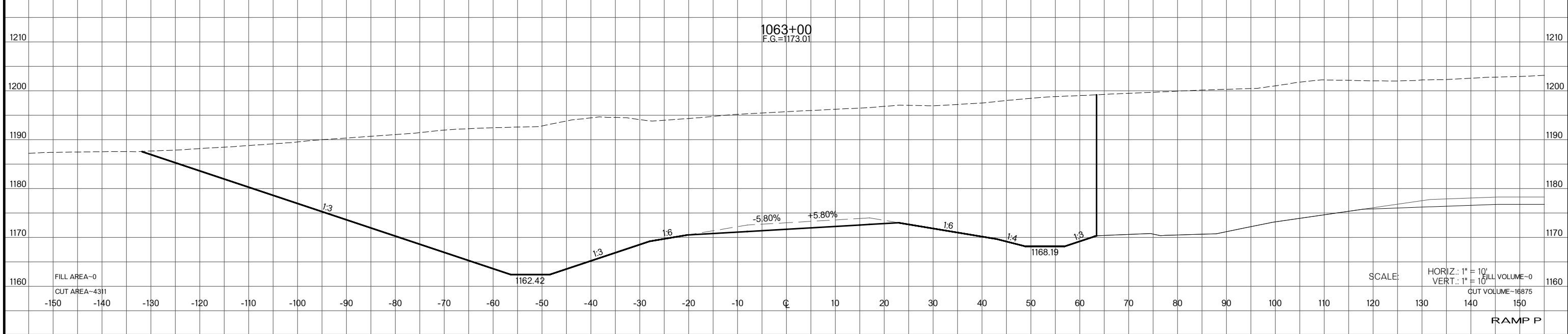
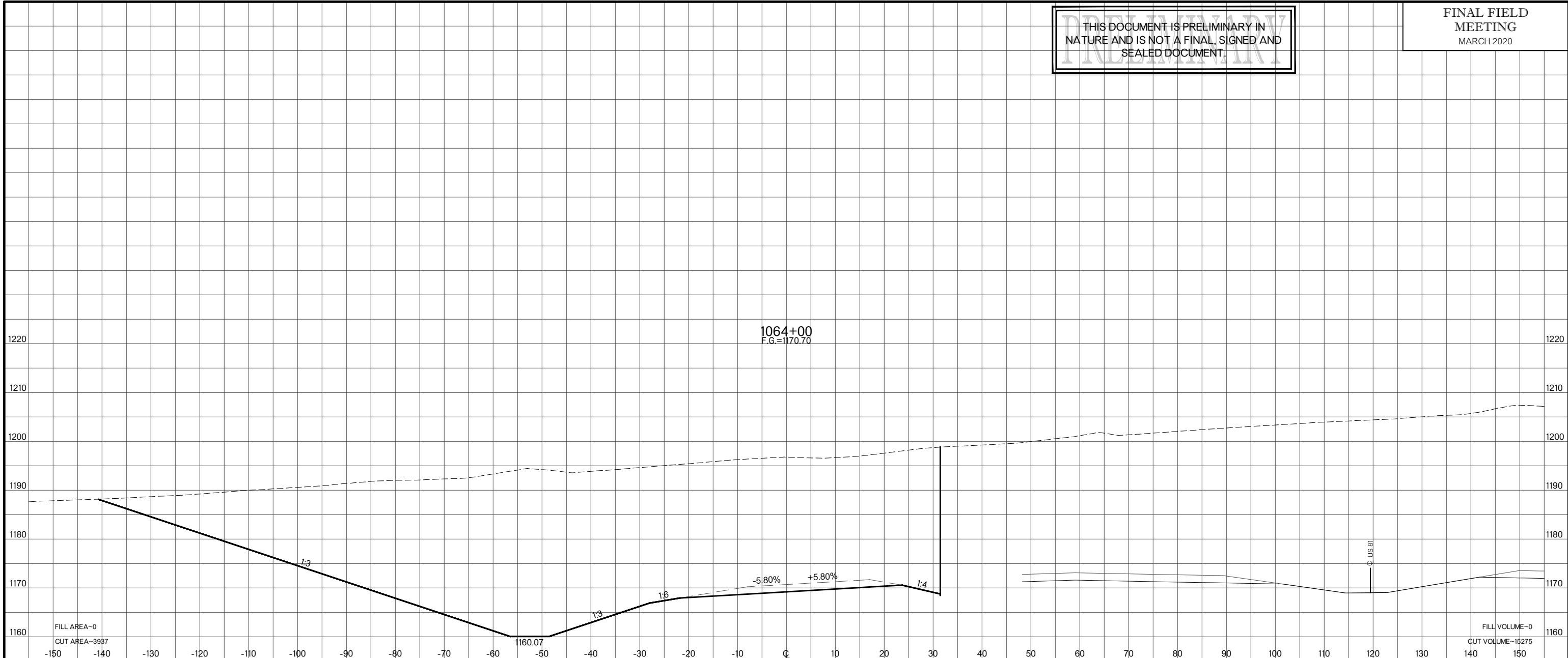
US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



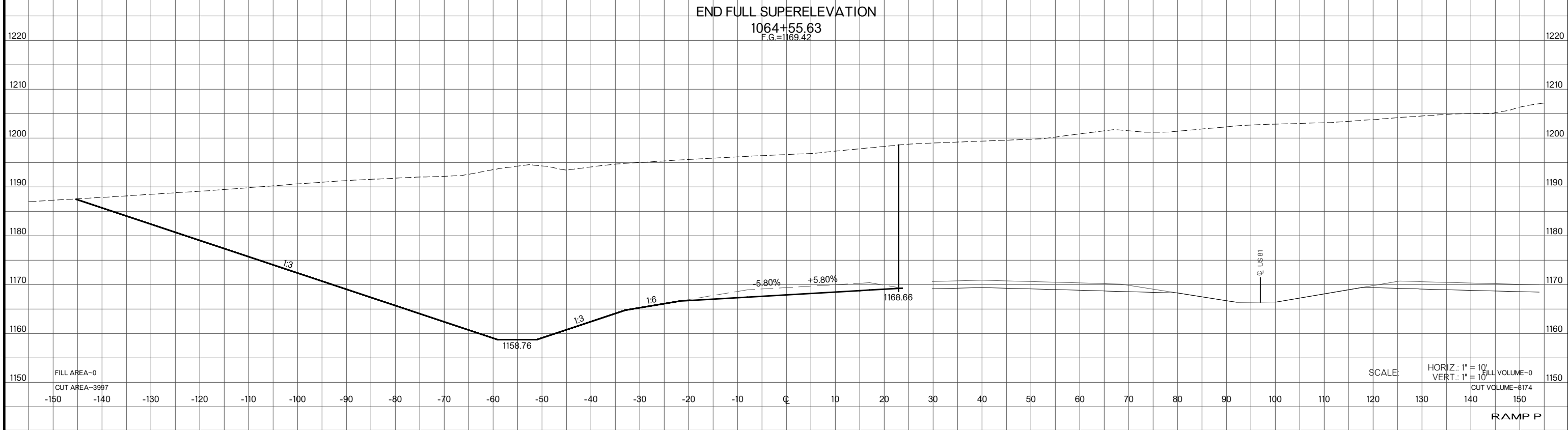
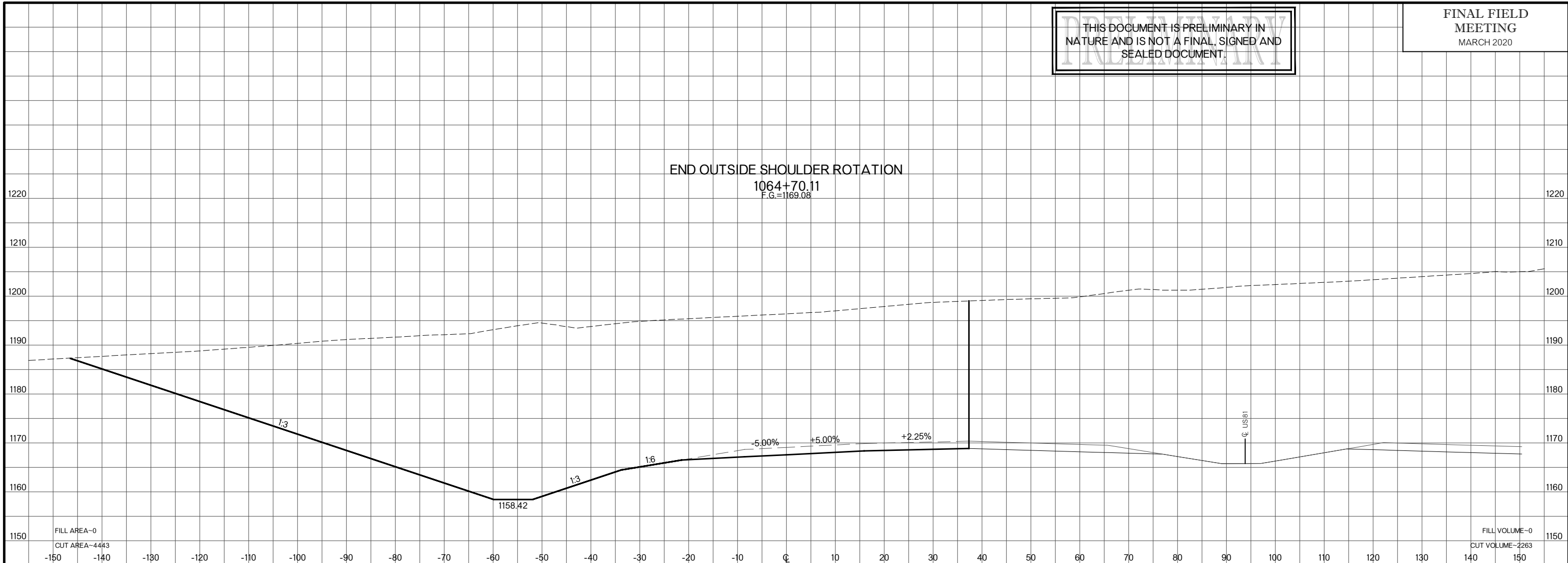
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP P

GRADY COUNTY
US 81 REALIGNMENT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



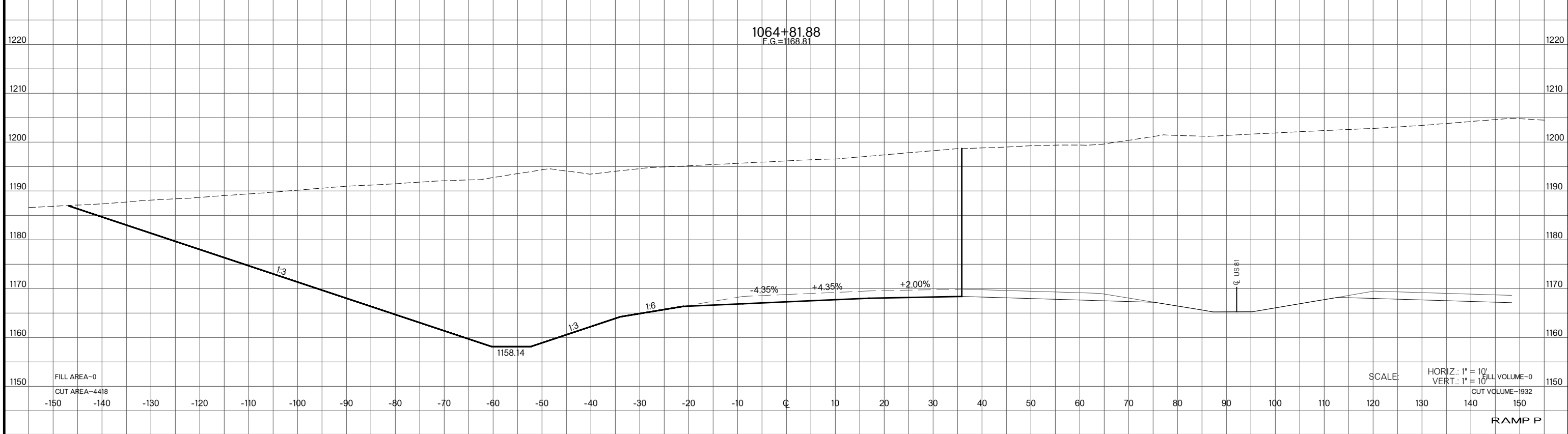
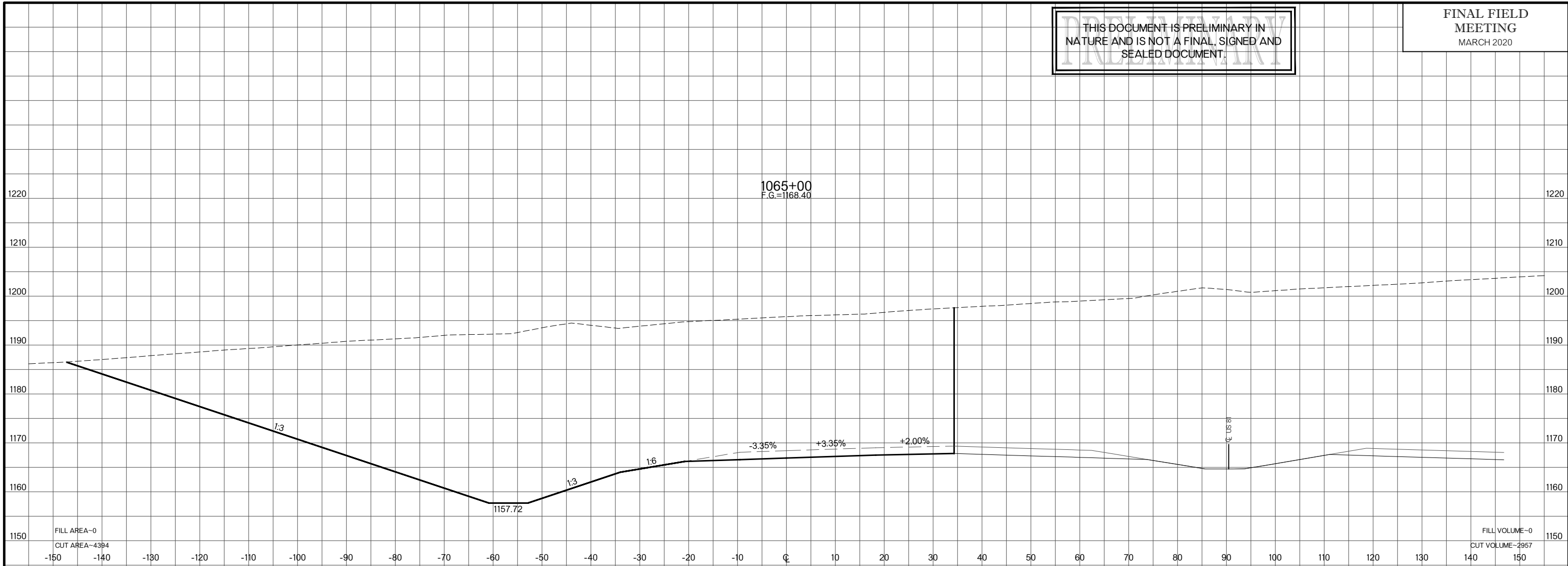
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP P

GRADY COUNTY
US 81 REALIGNMENT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

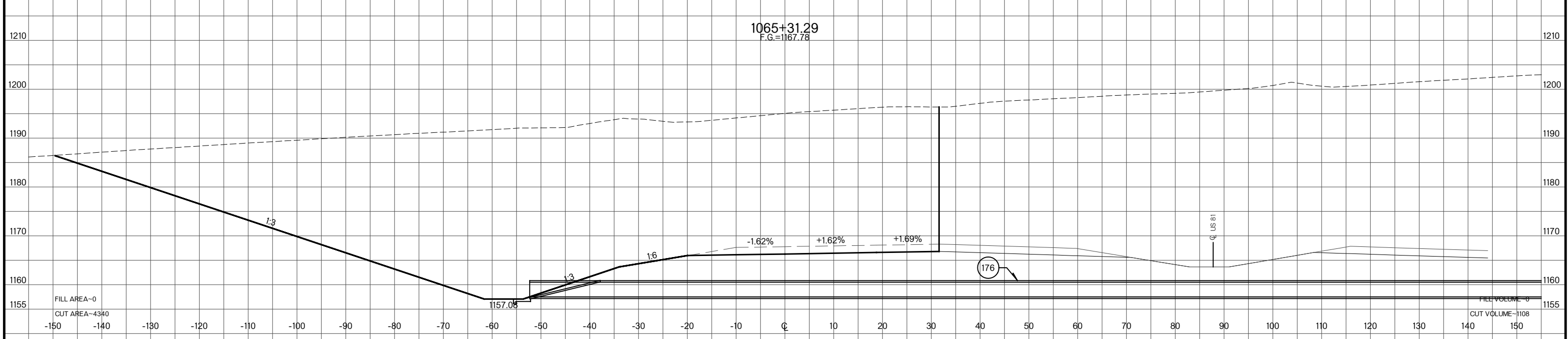
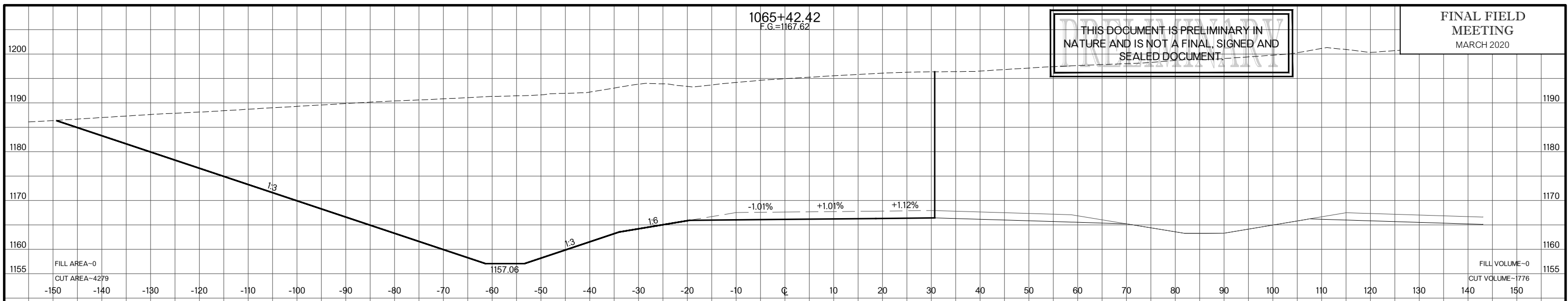


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

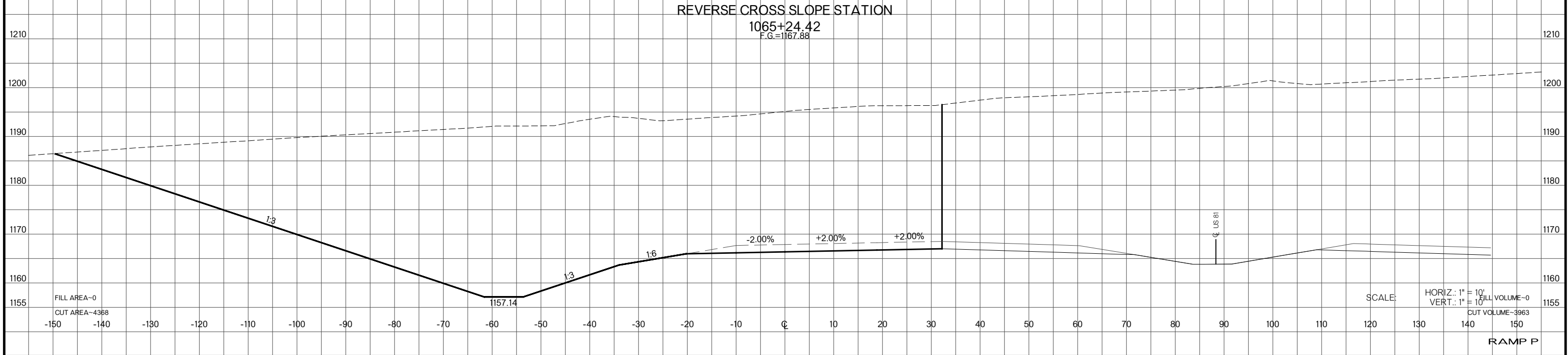
RAMP P

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



REVERSE CROSS SLOPE STATION



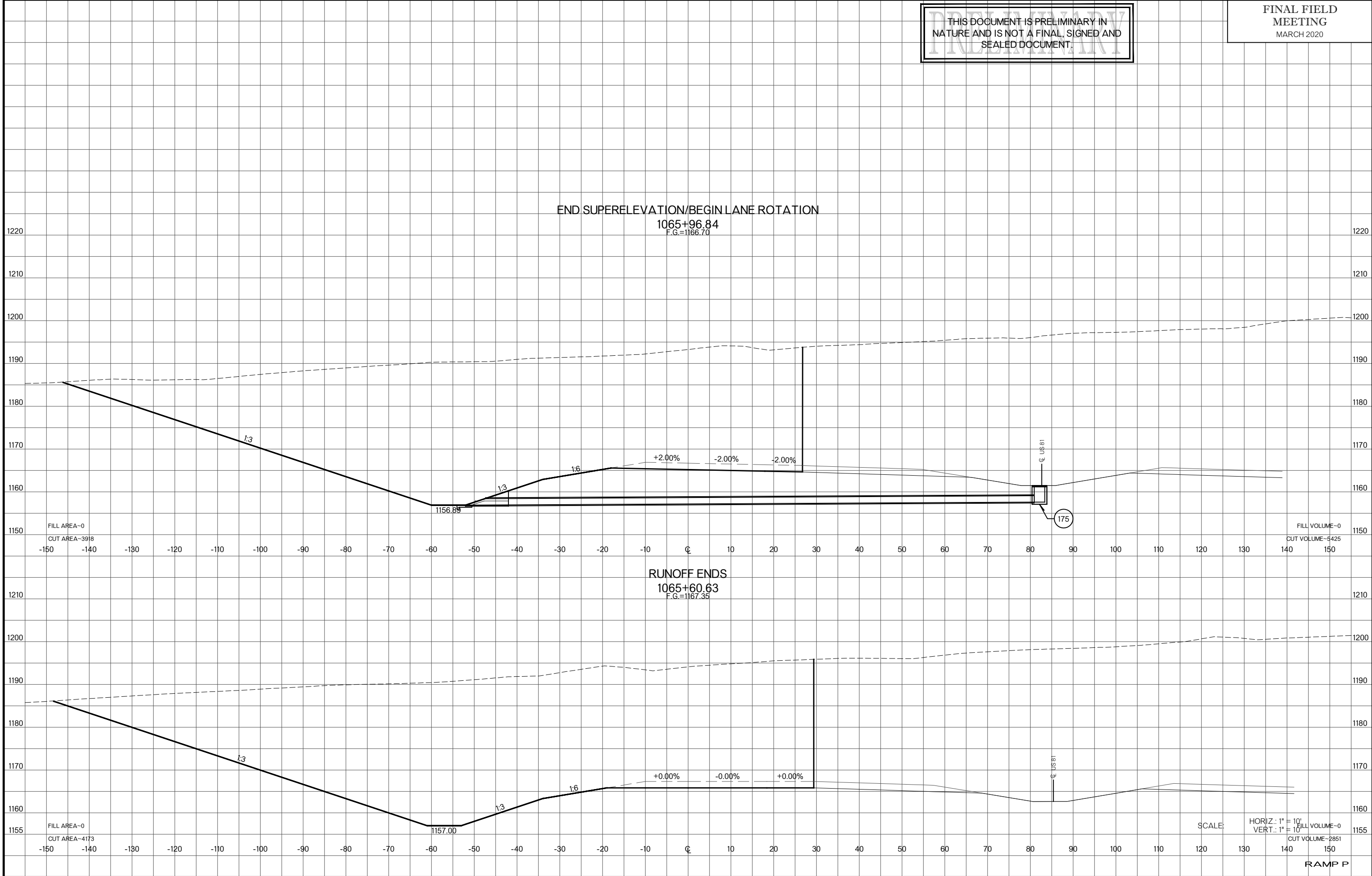
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP P

GRADY COUNTY
US 81 REALIGNMENT

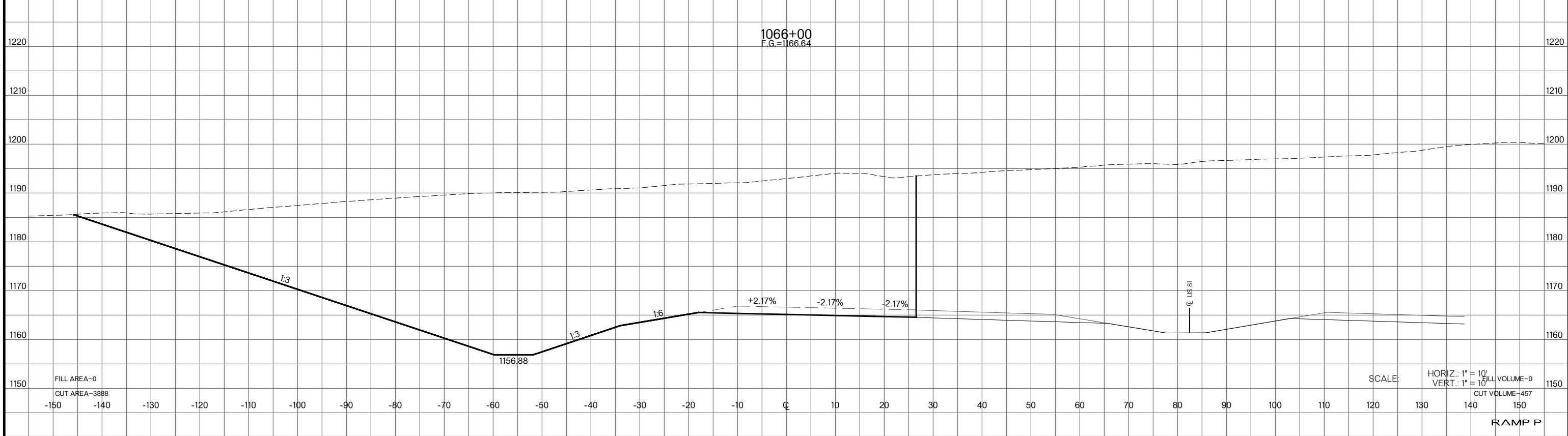
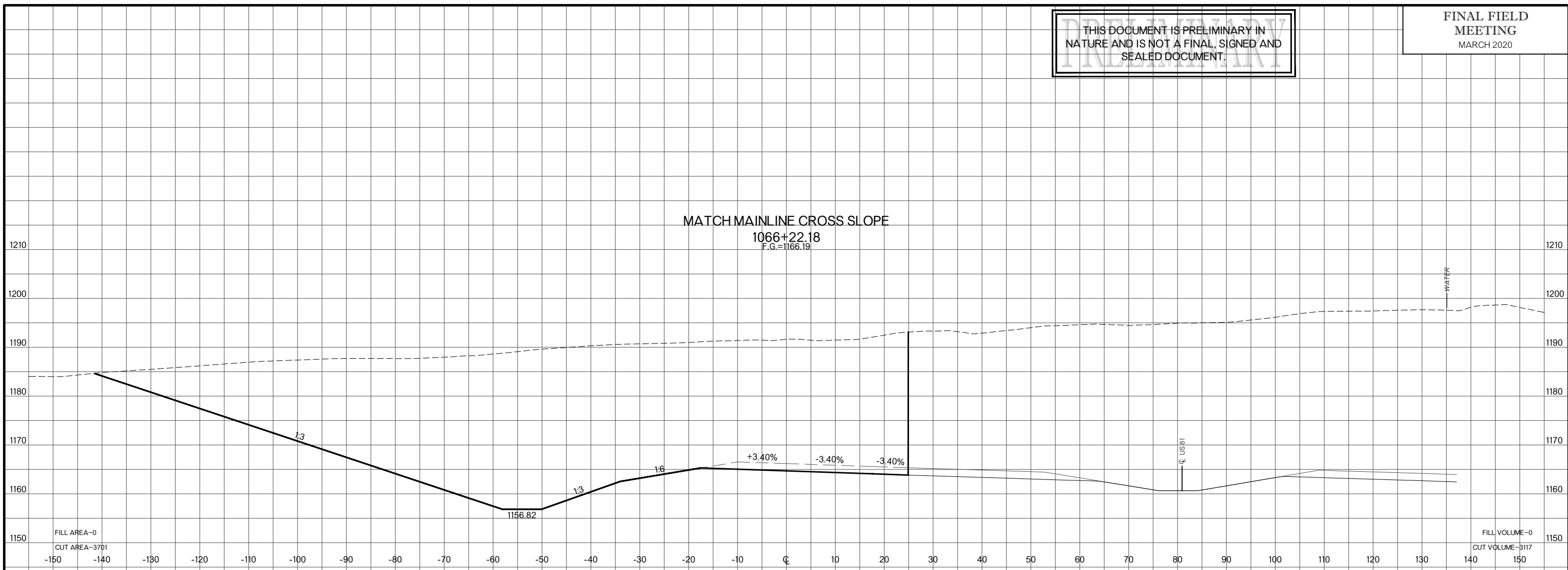
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



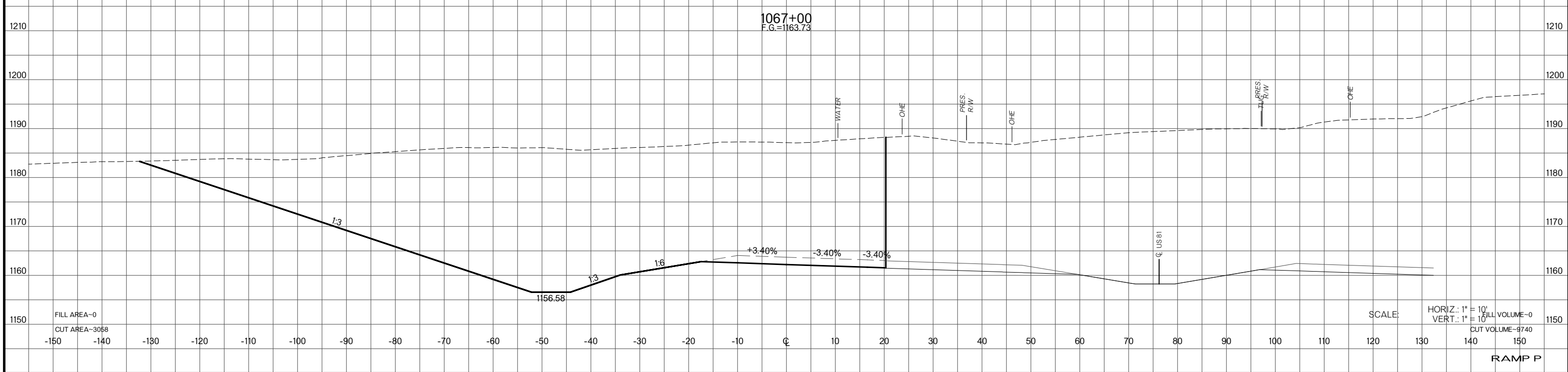
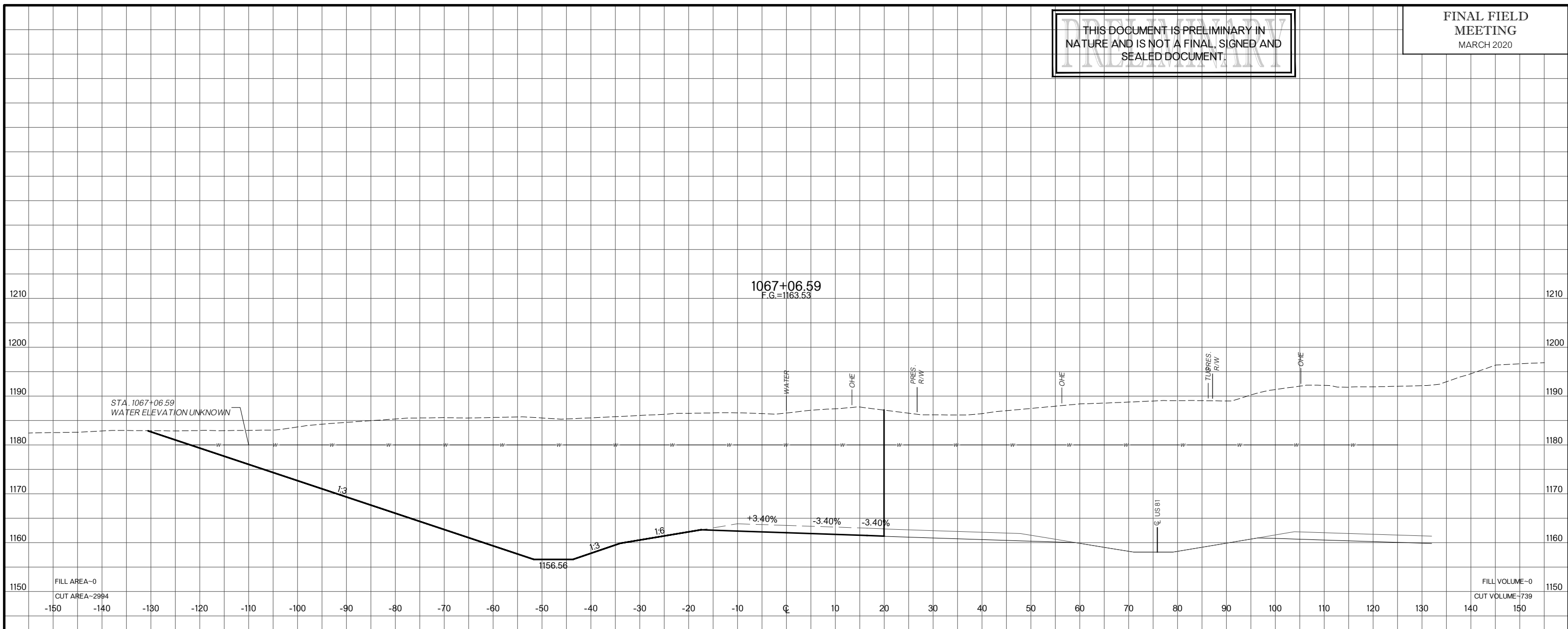
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP P

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



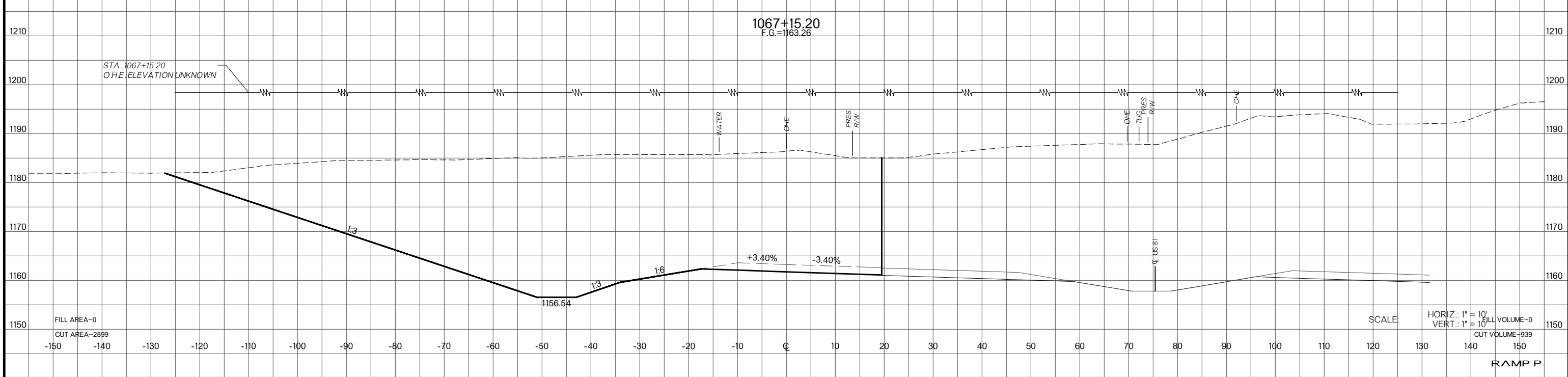
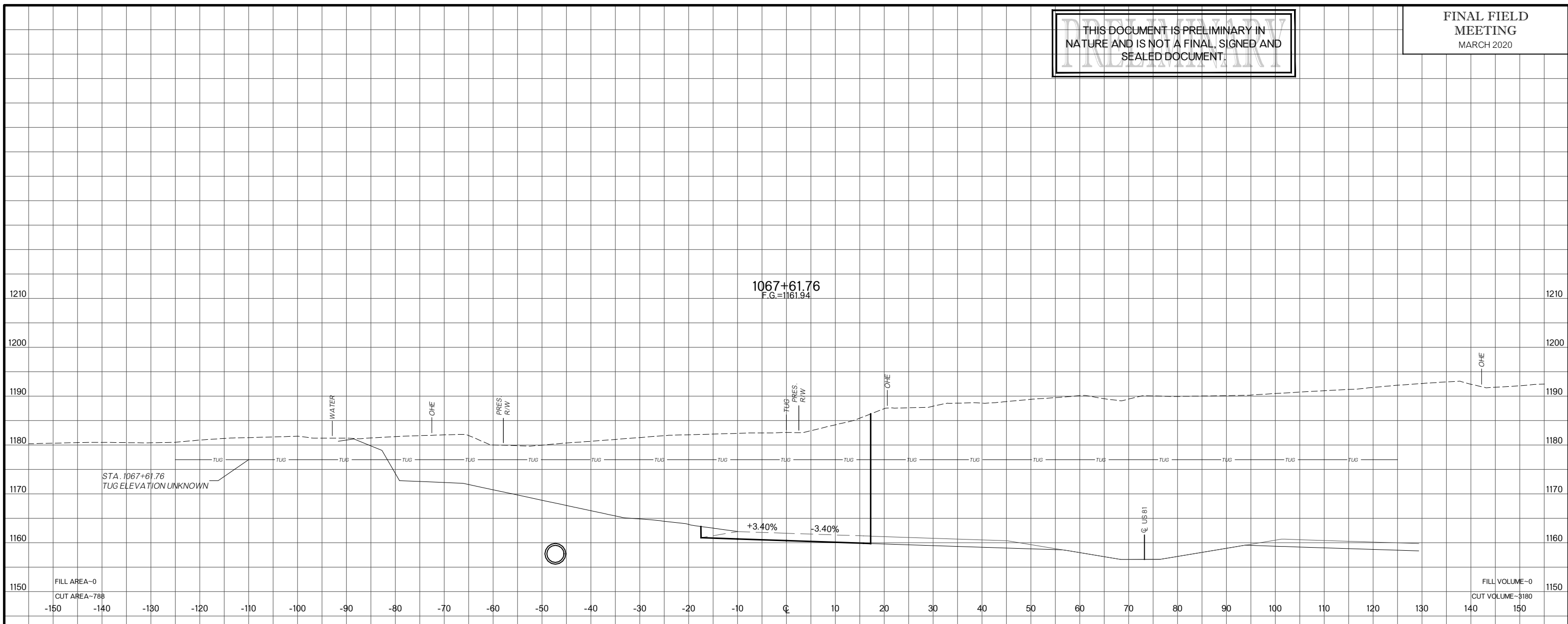
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP P

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



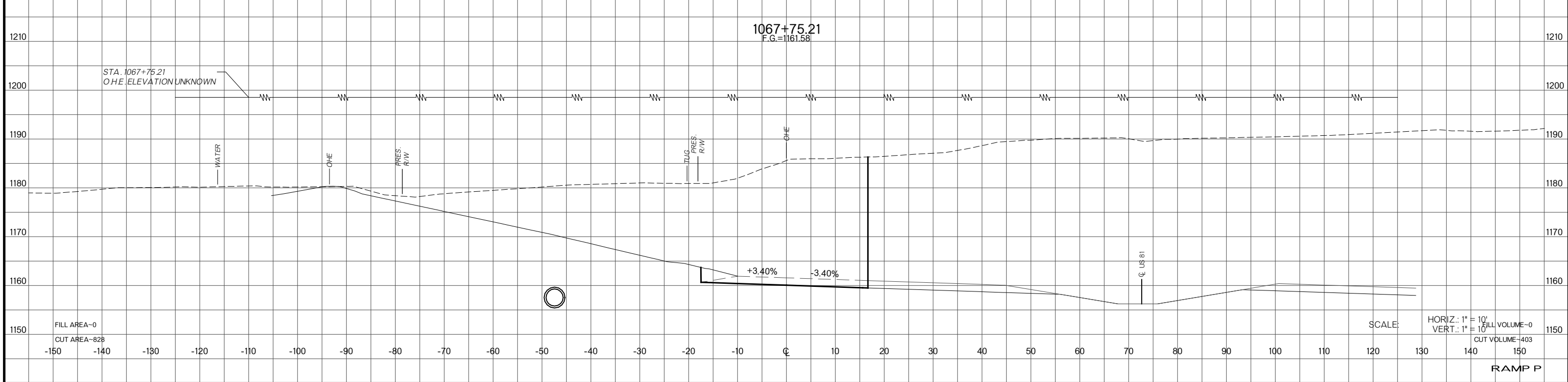
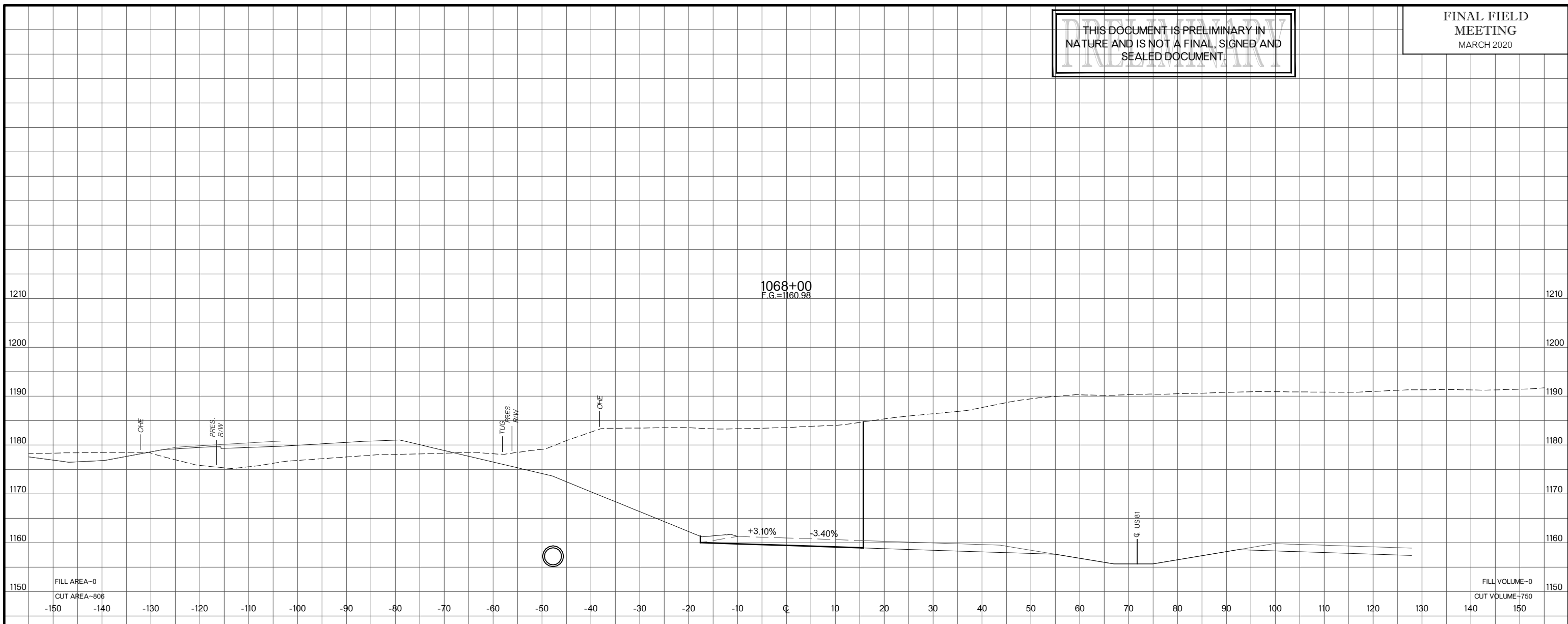
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP P

GRADY COUNTY US 81 REALIGNMENT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



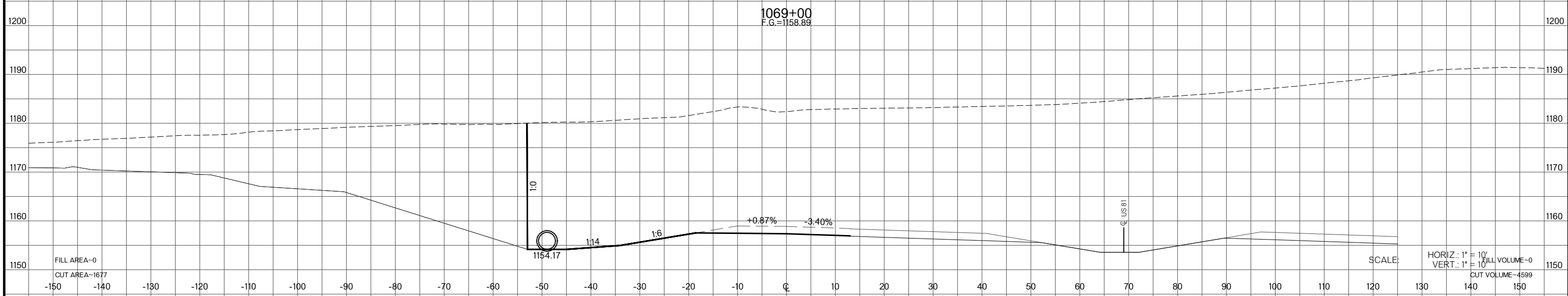
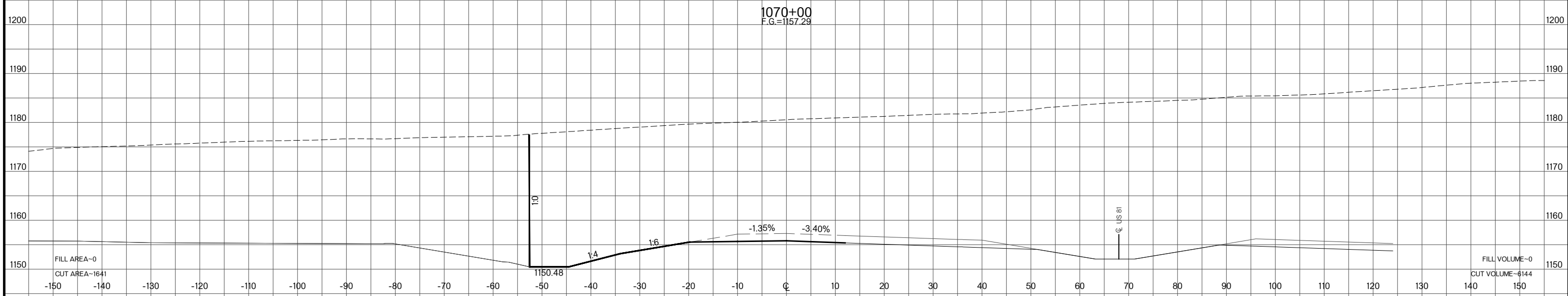
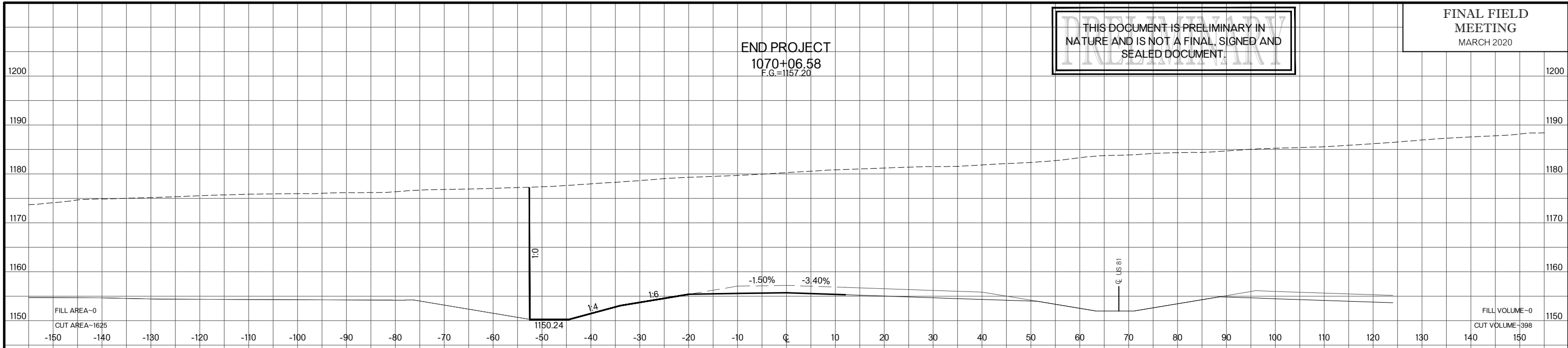
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP P

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



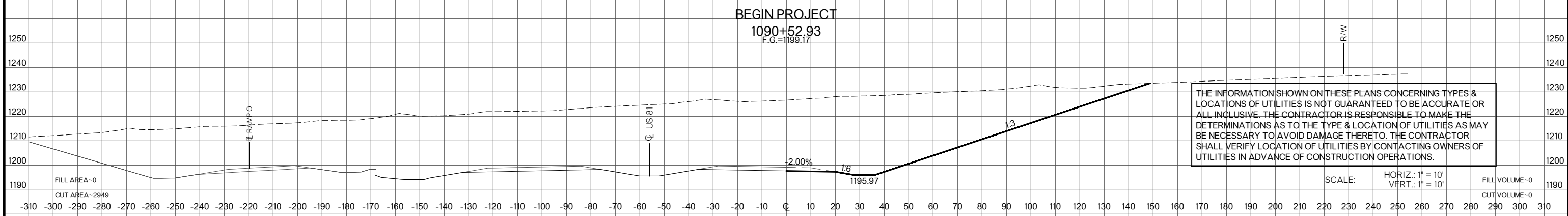
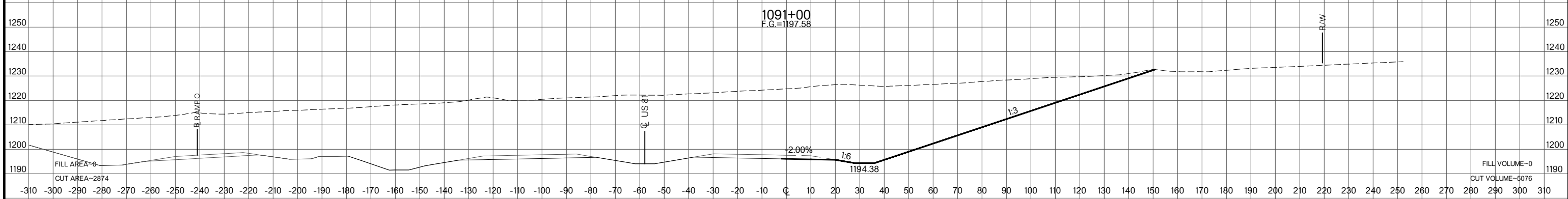
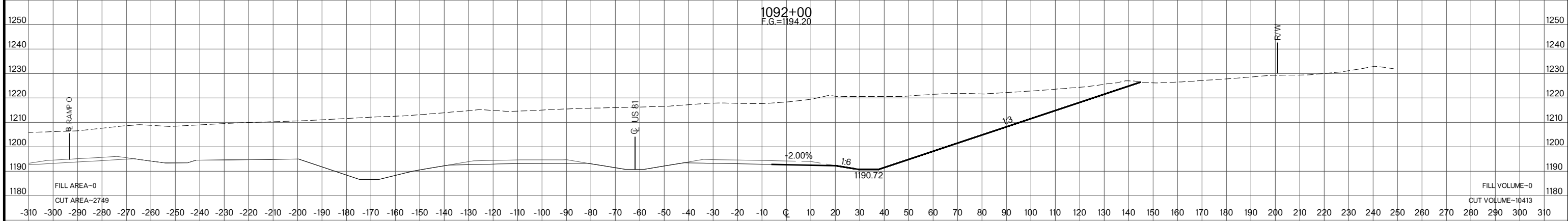
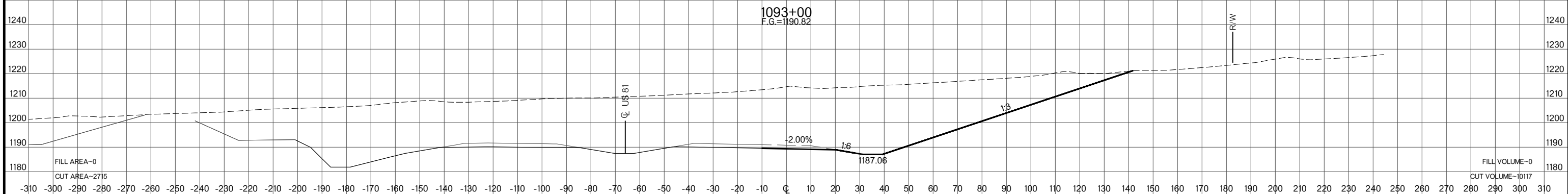
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP P

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

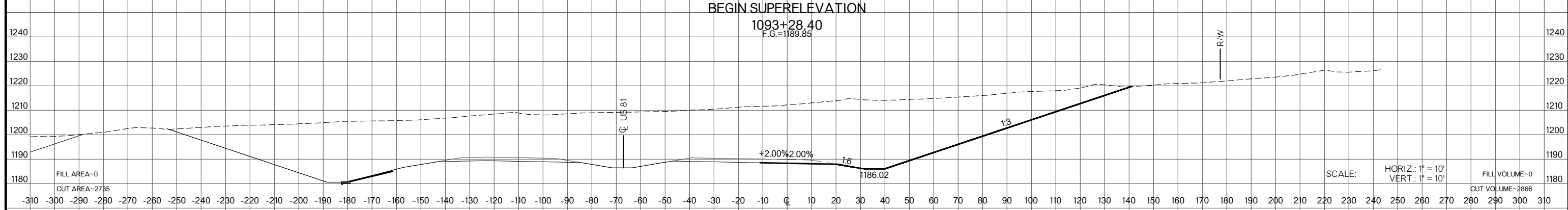
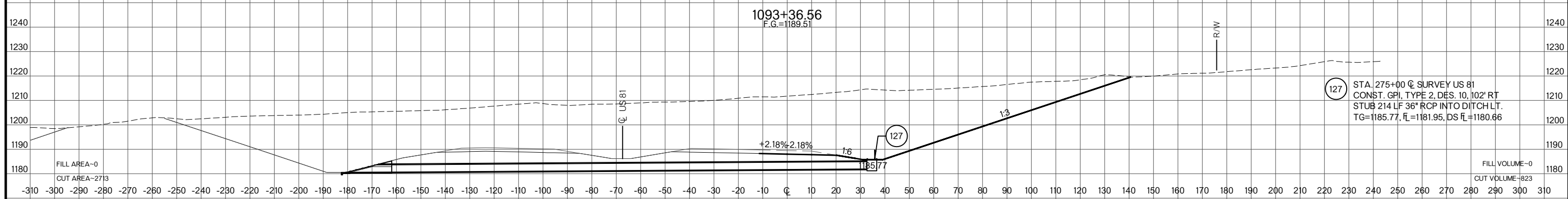
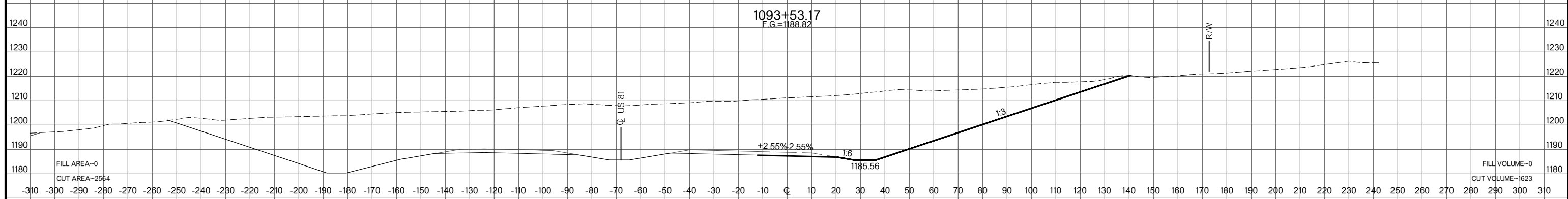
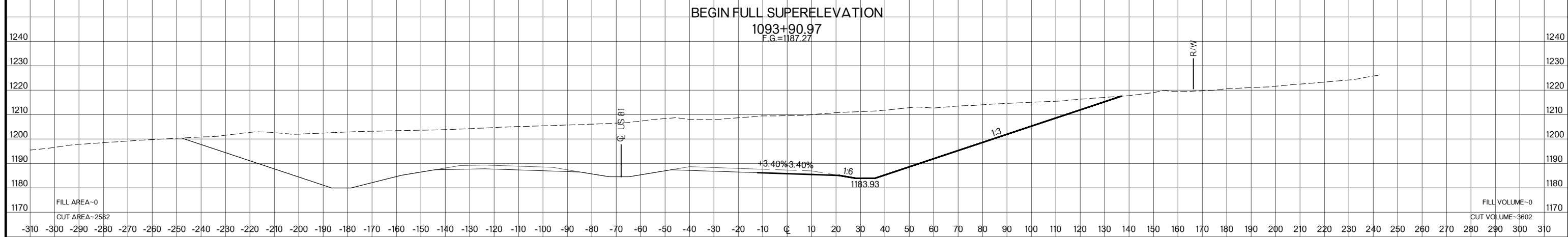
FINAL FIELD MEETING MARCH 2020



US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020

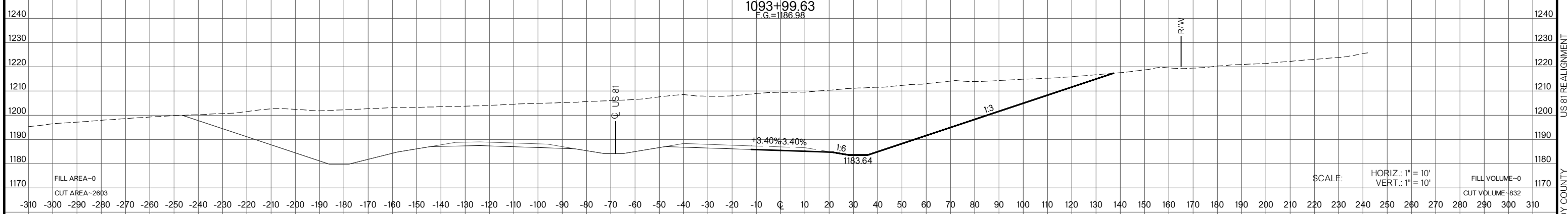
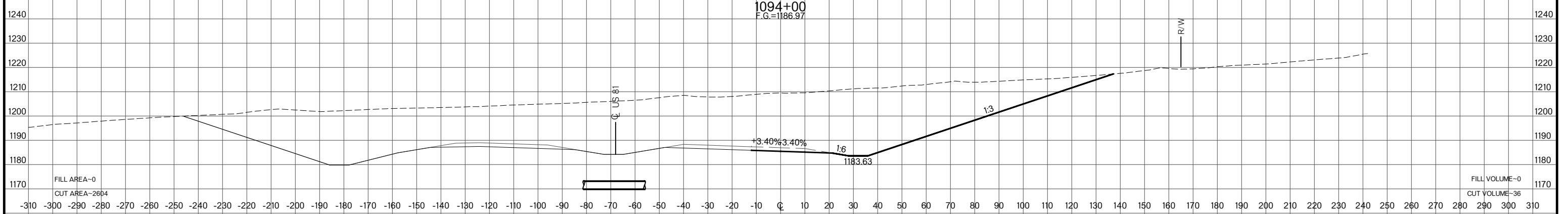
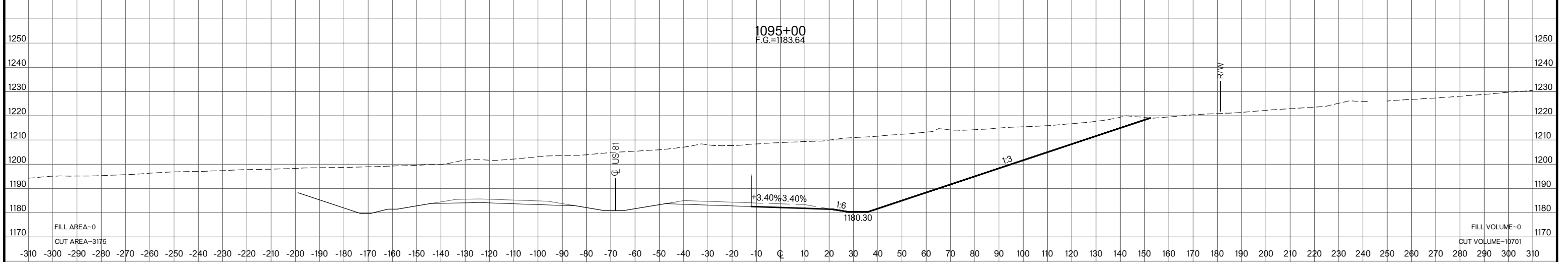
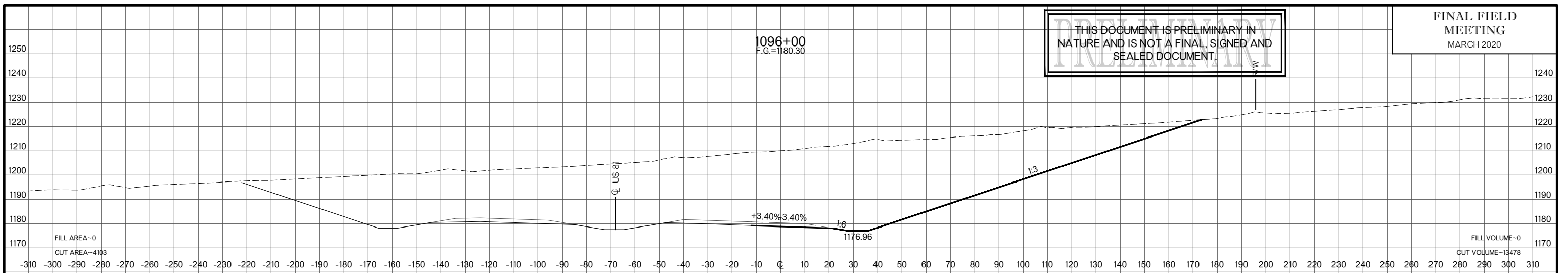


SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP Q

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



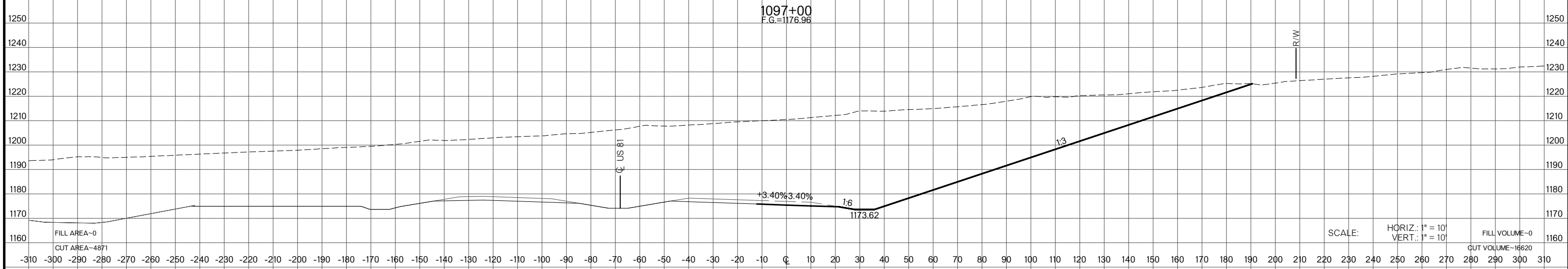
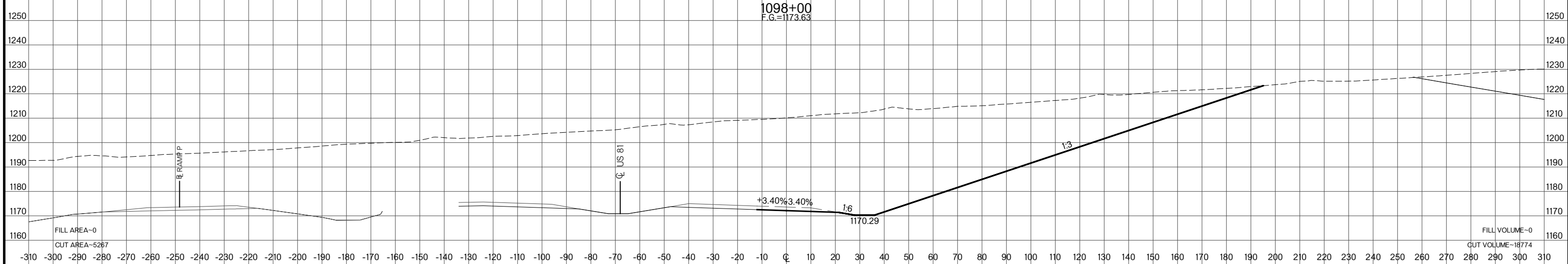
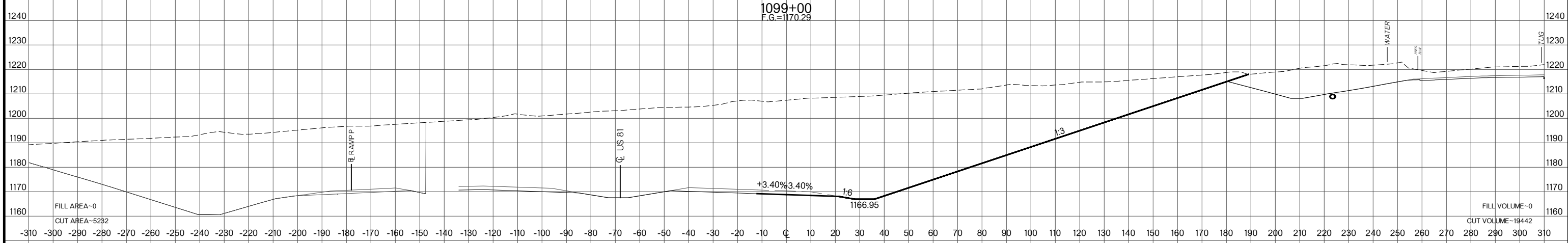
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP Q

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

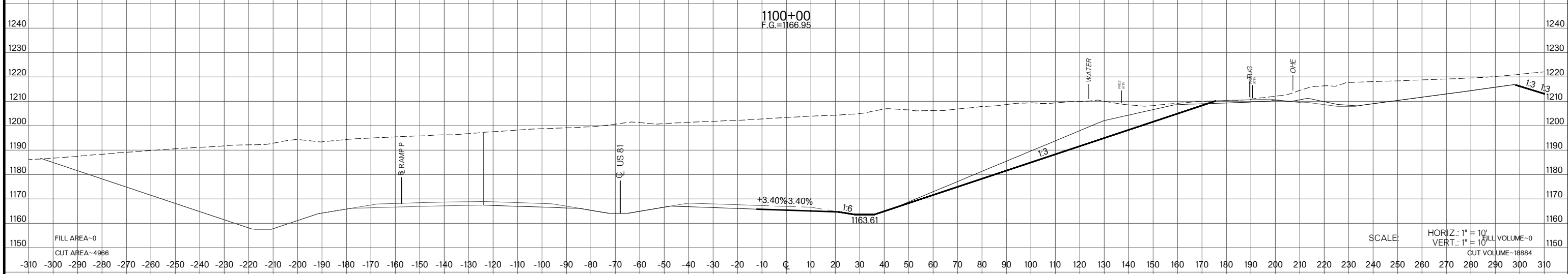
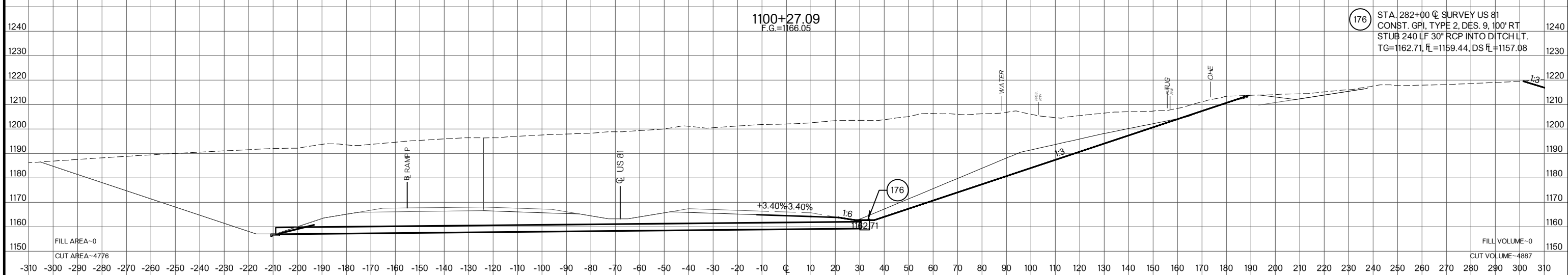
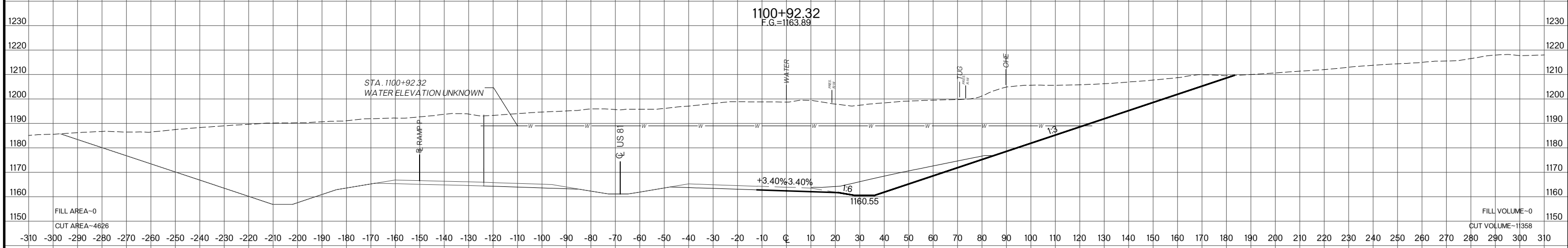


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



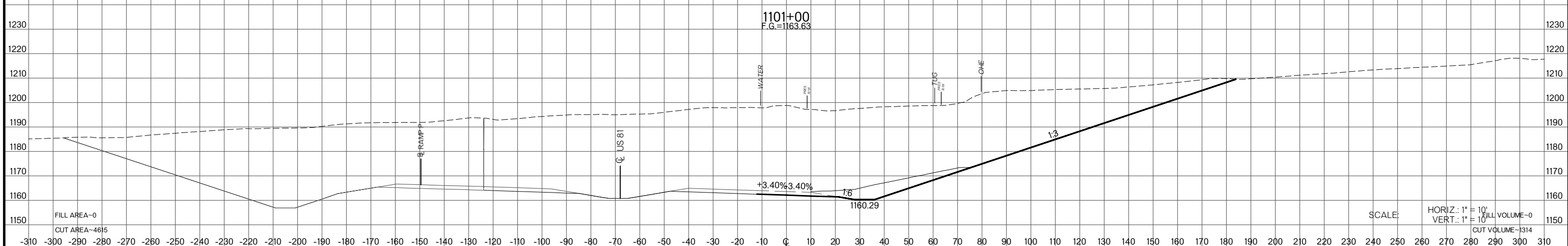
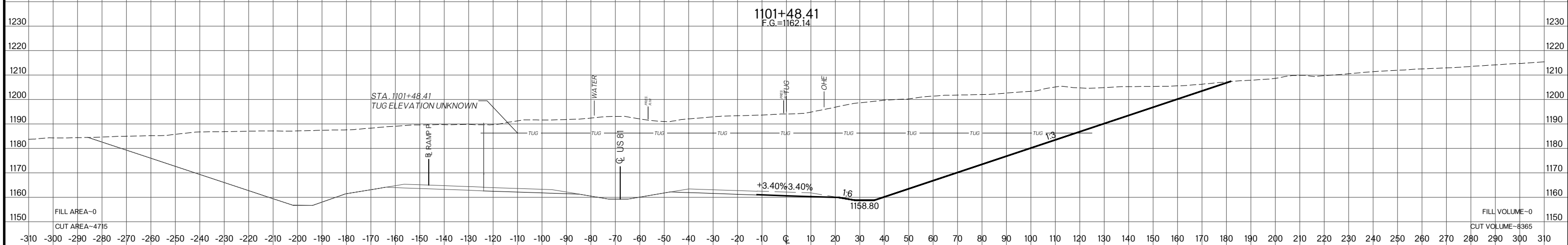
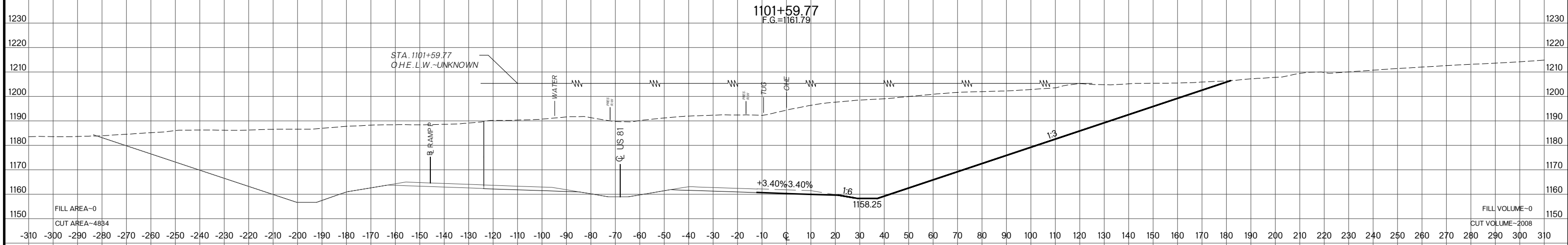
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP Q

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

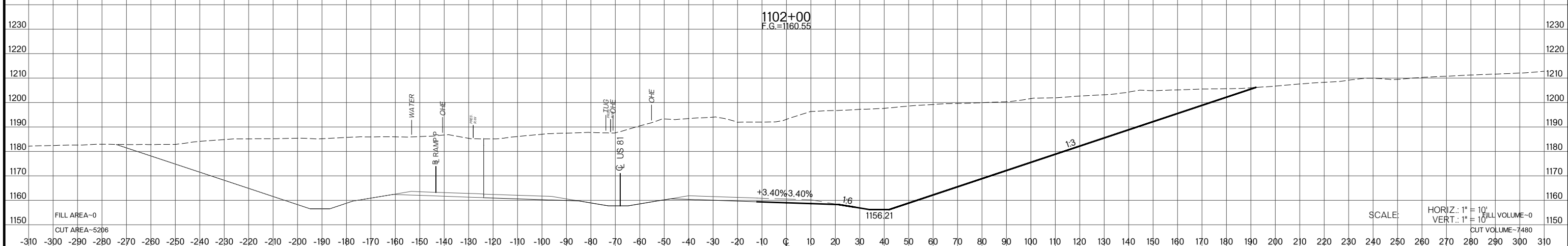
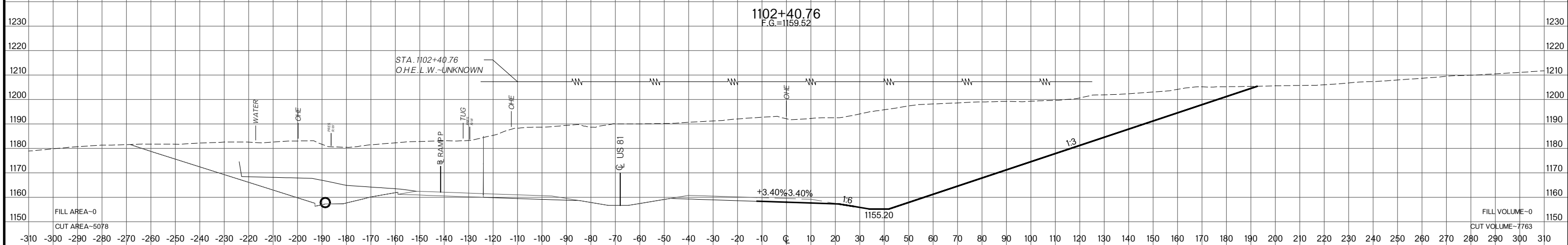
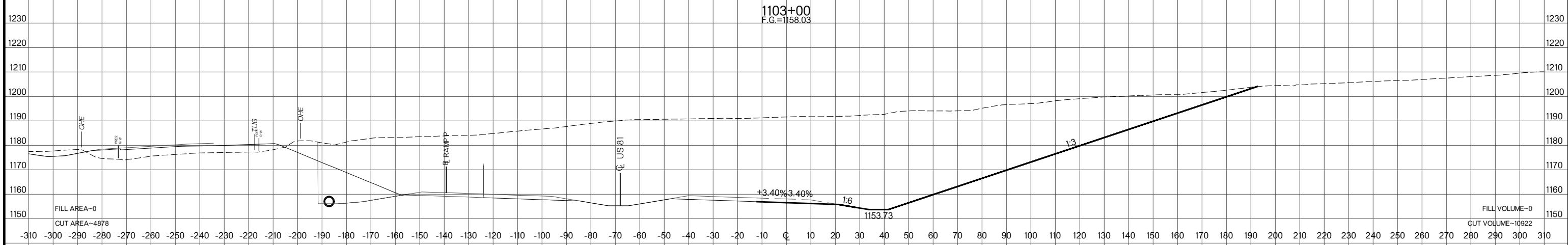


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP Q

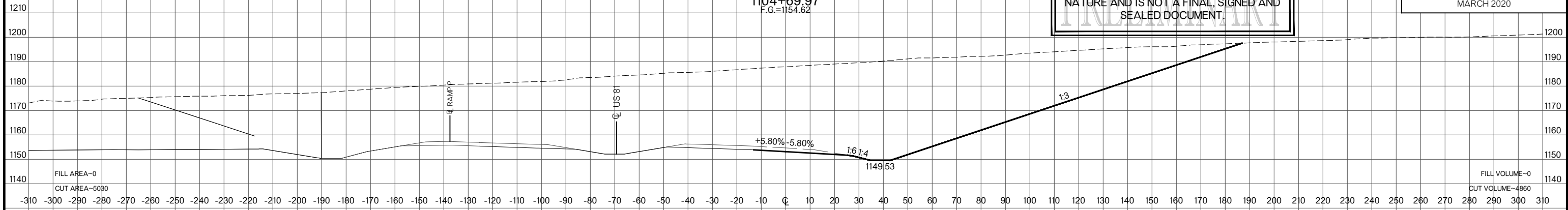
US 81 REALIGNMENT GRADY COUNTY

BEGIN FULL SUPERELEVATION

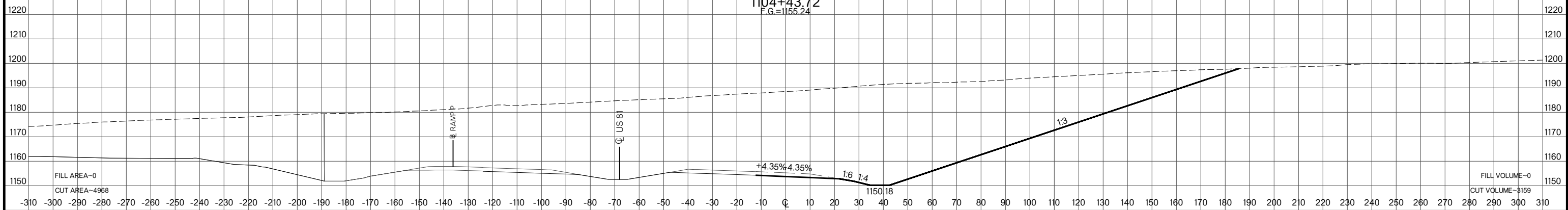
1104+69.97
F.G.=1154.62

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

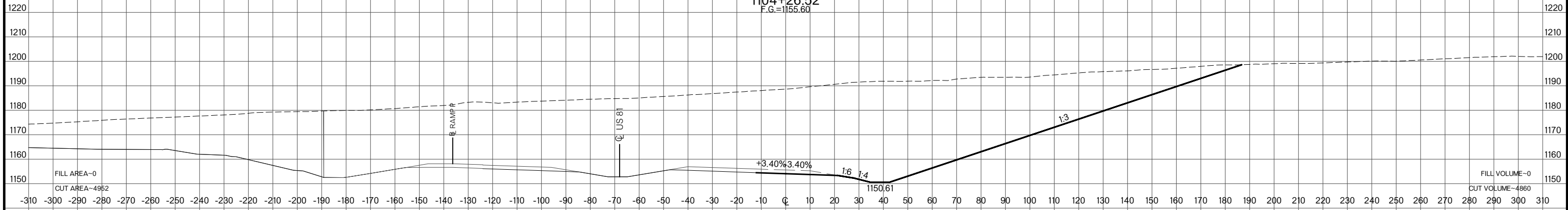


1104+43.72
F.G.=1155.24

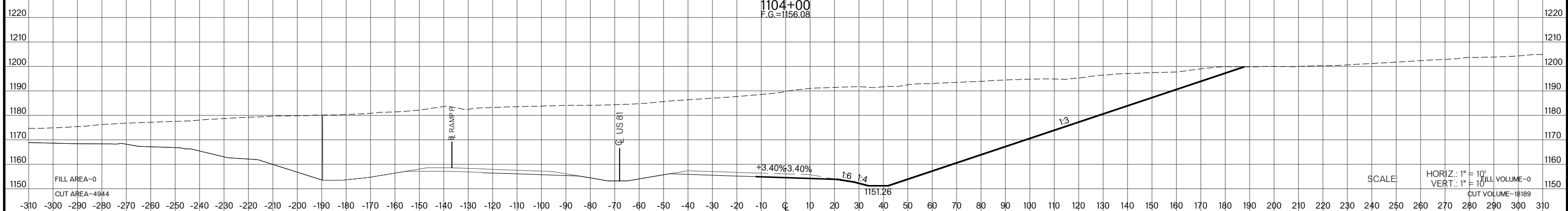


END FULL SUPERELEVATION

1104+26.52
F.G.=1155.60



1104+00
F.G.=1156.08



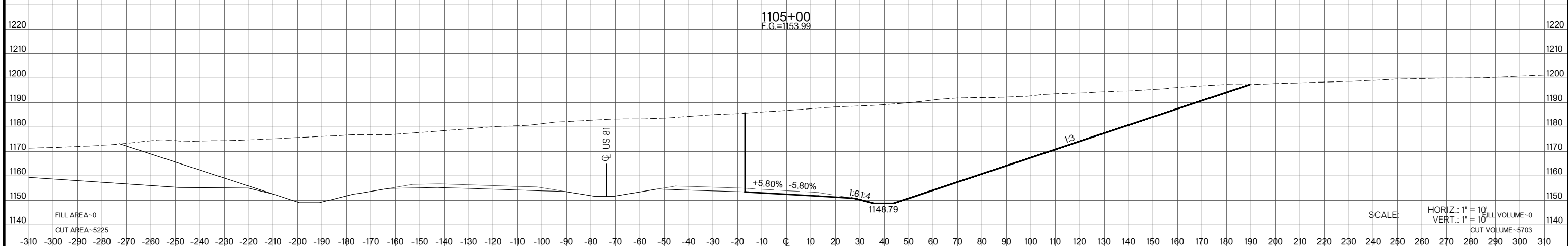
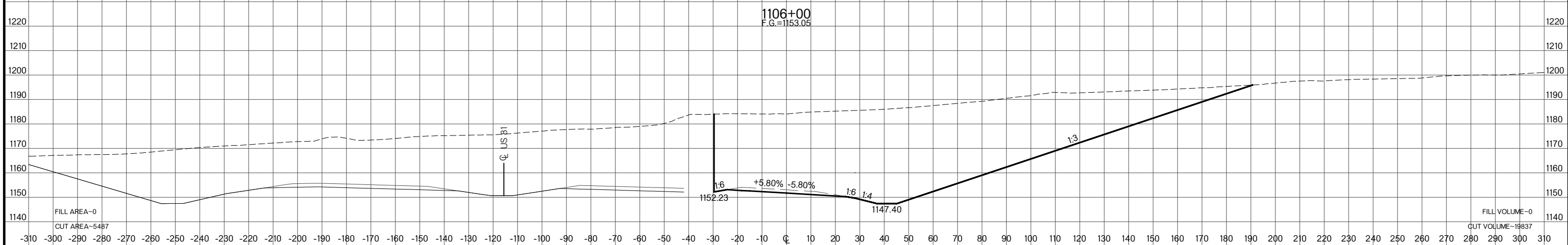
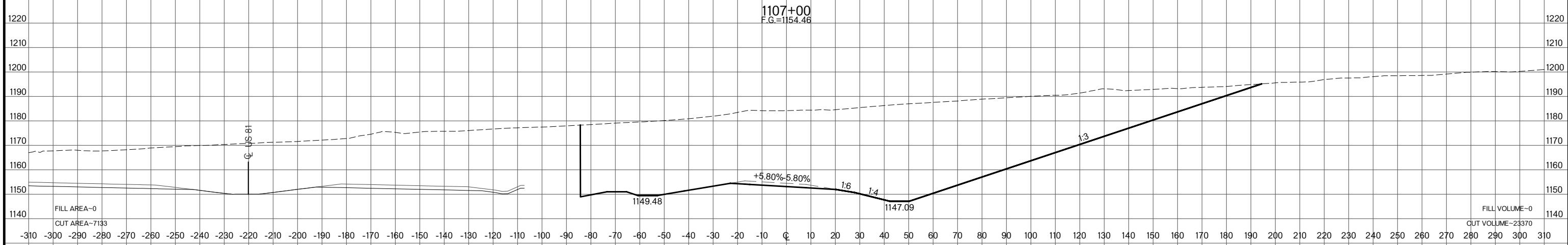
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

FILL VOLUME=0
CUT VOLUME=18189

RAMP Q

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



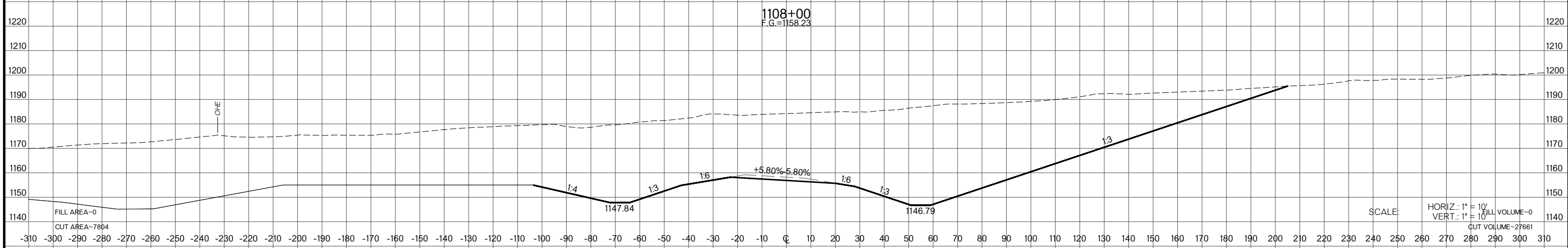
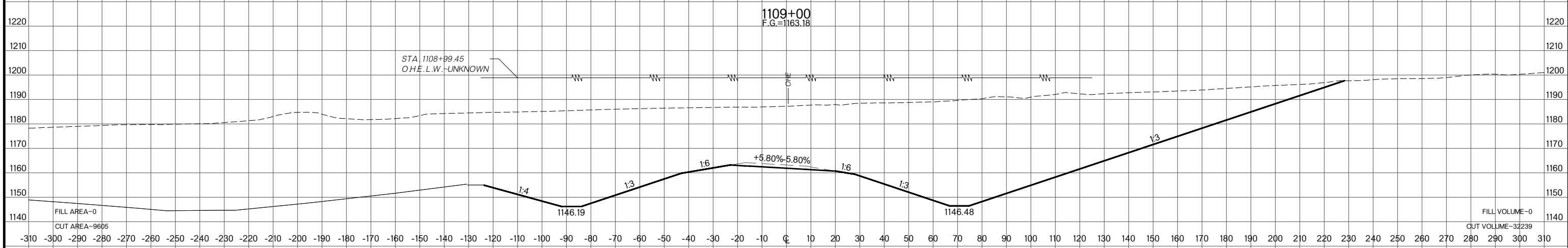
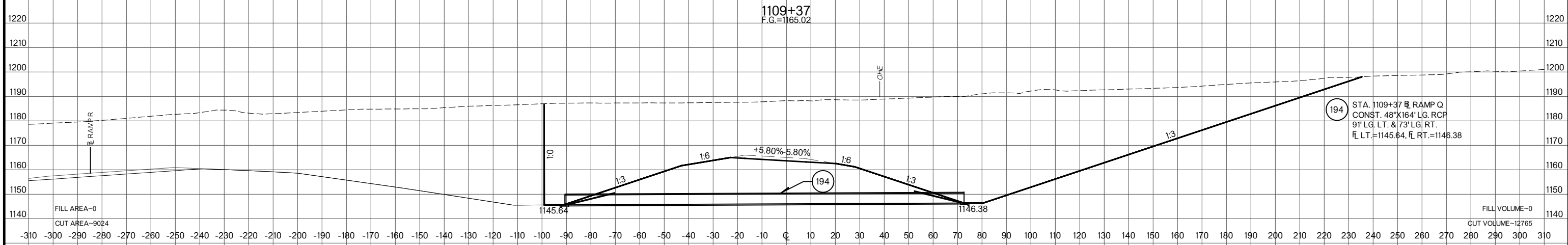
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP Q

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

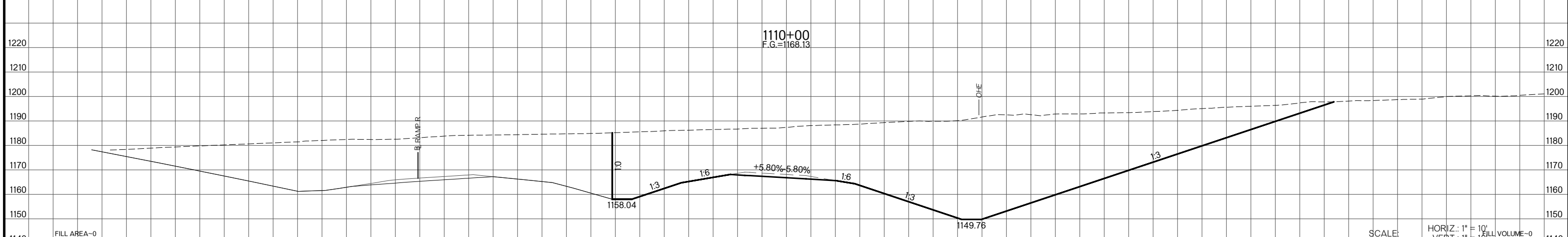
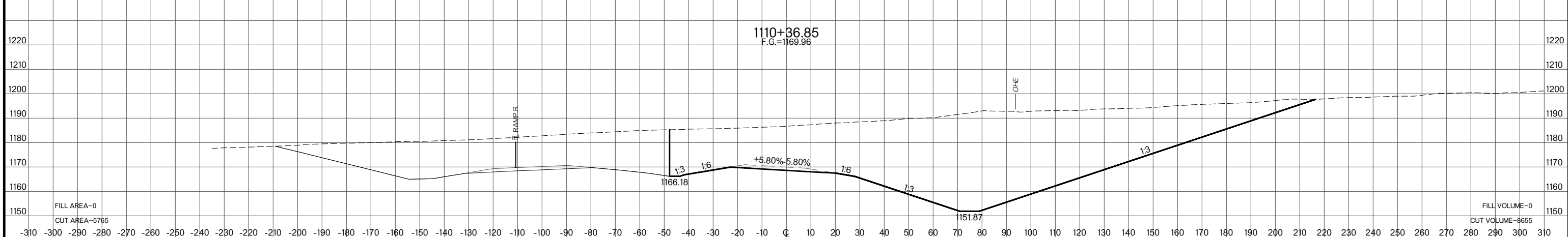
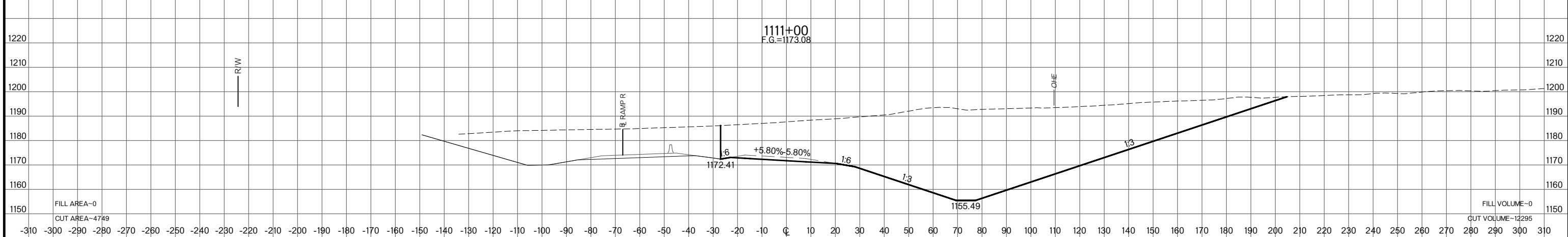
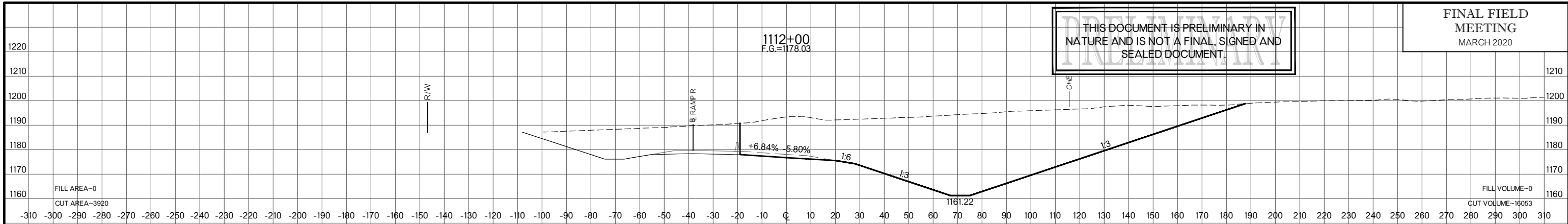
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



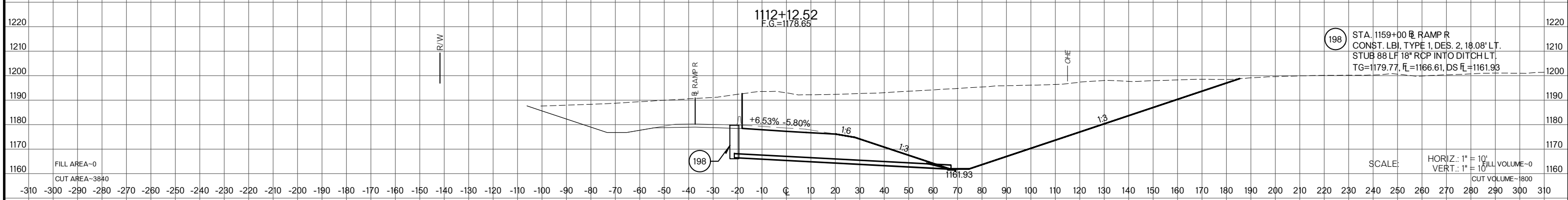
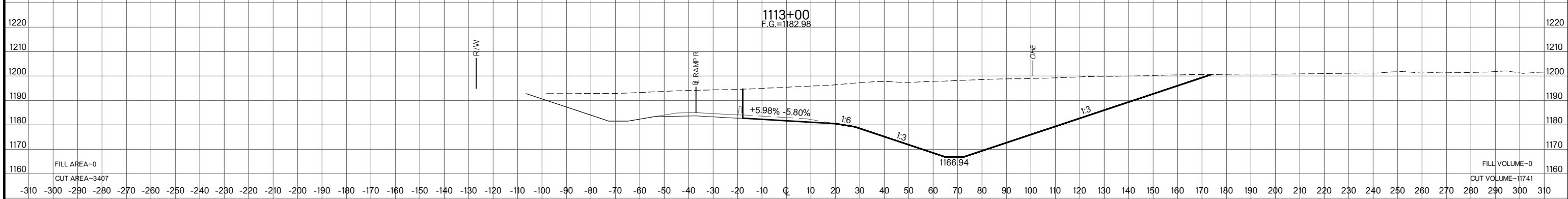
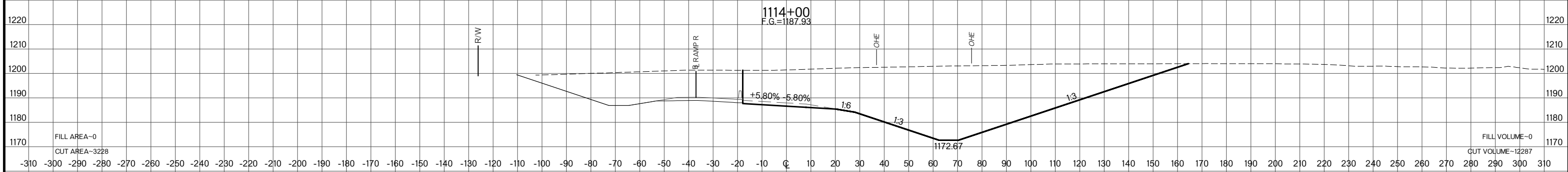
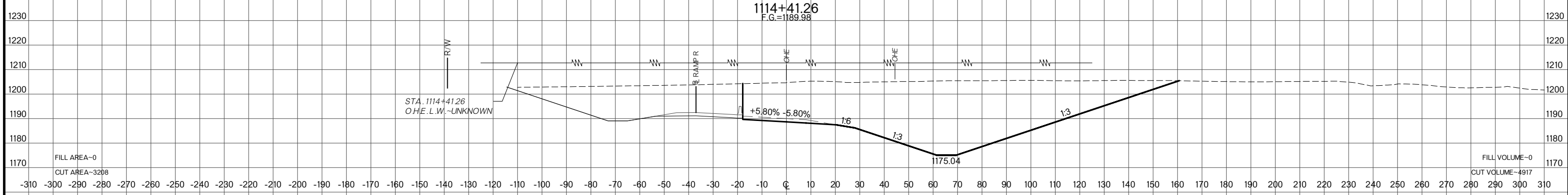
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP Q

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



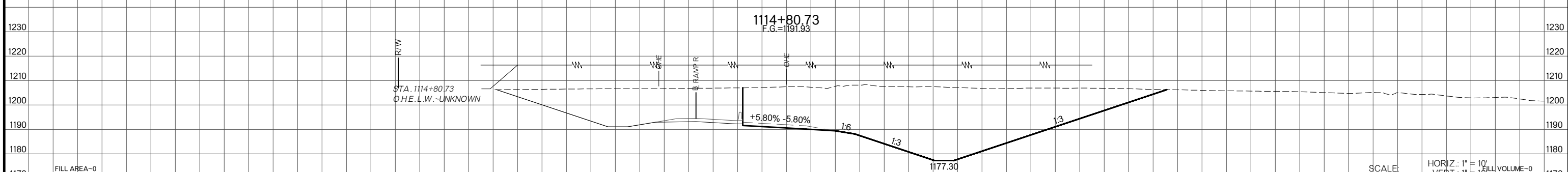
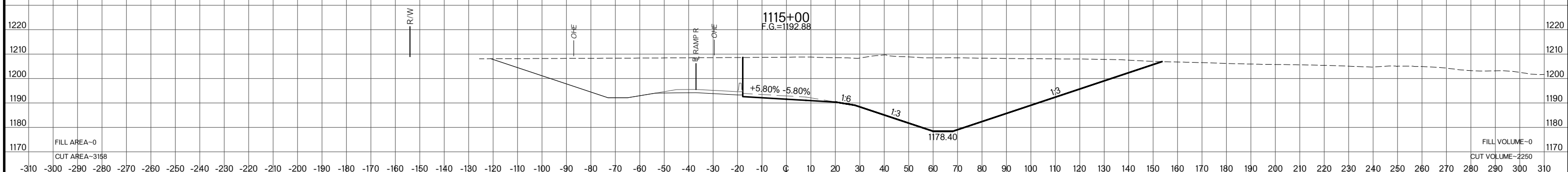
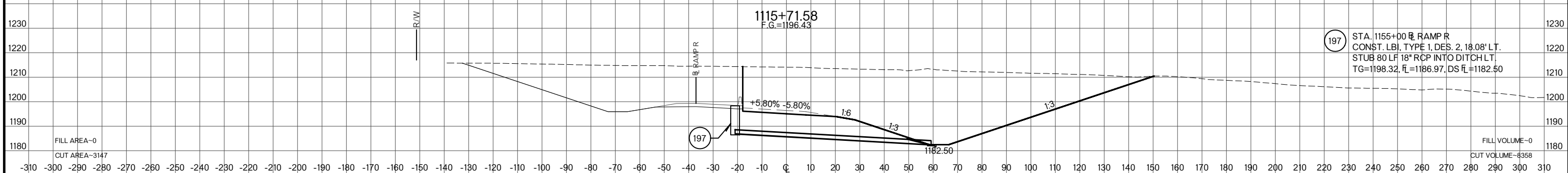
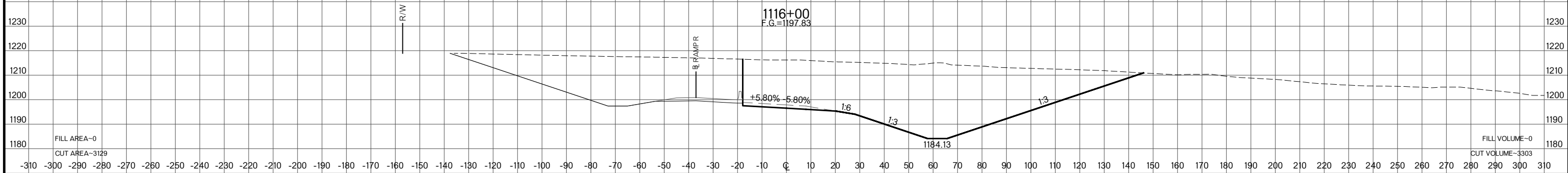
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP Q

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



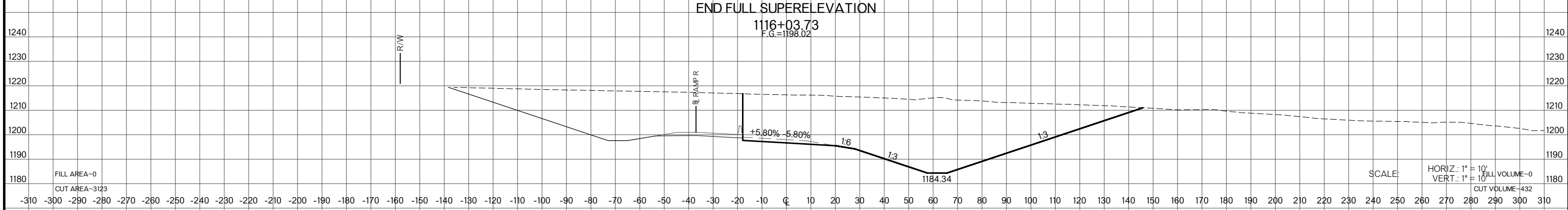
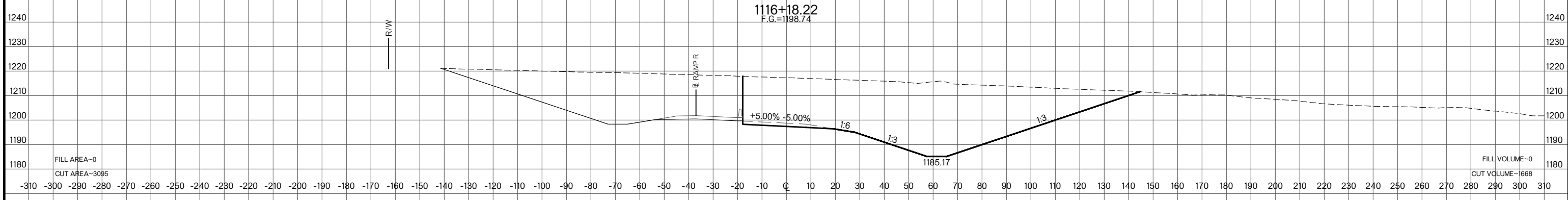
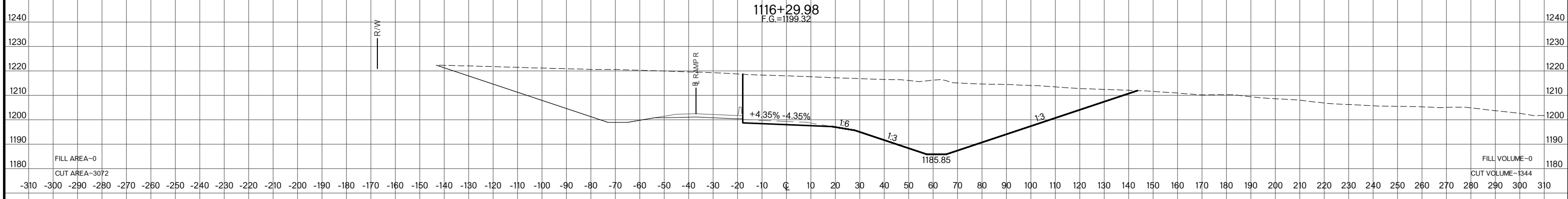
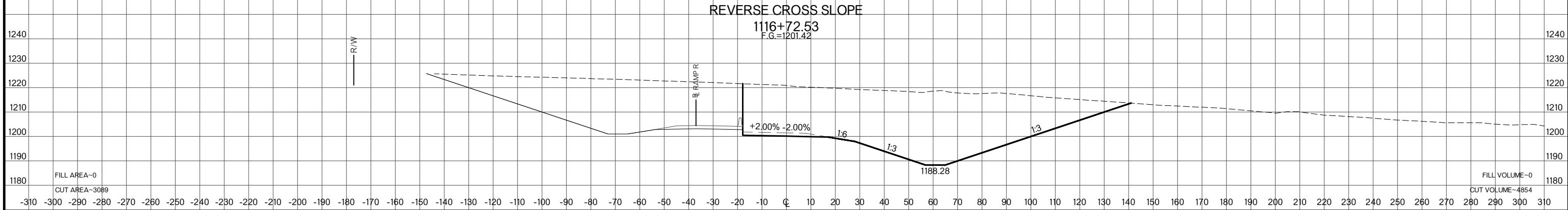
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP Q

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

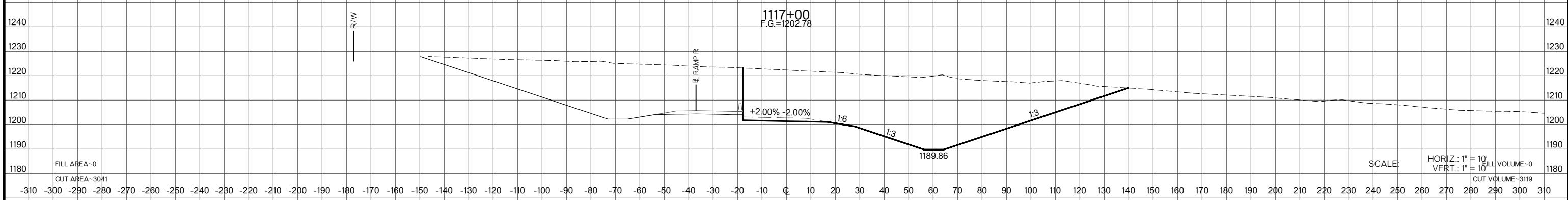
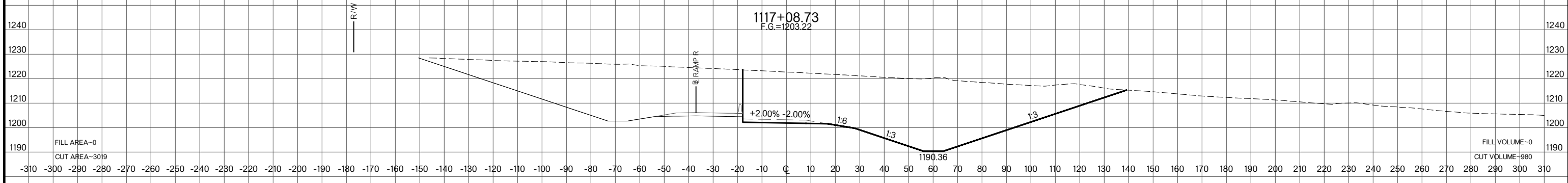
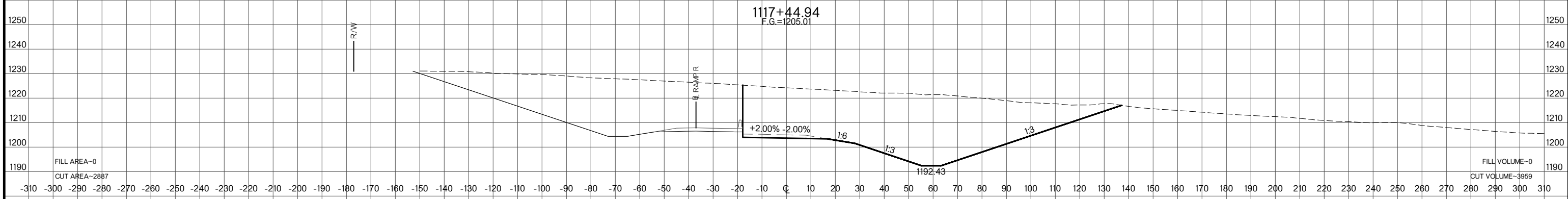
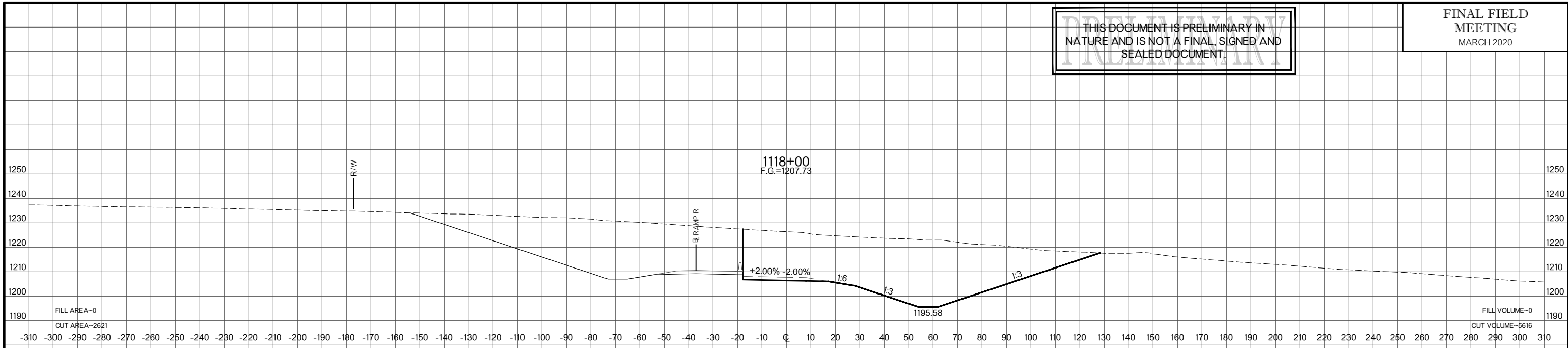


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



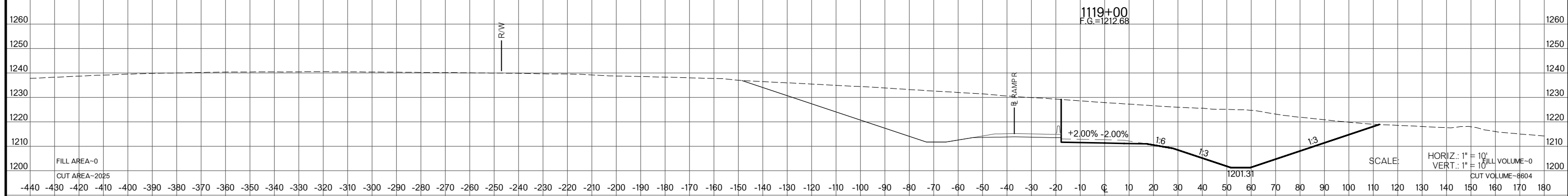
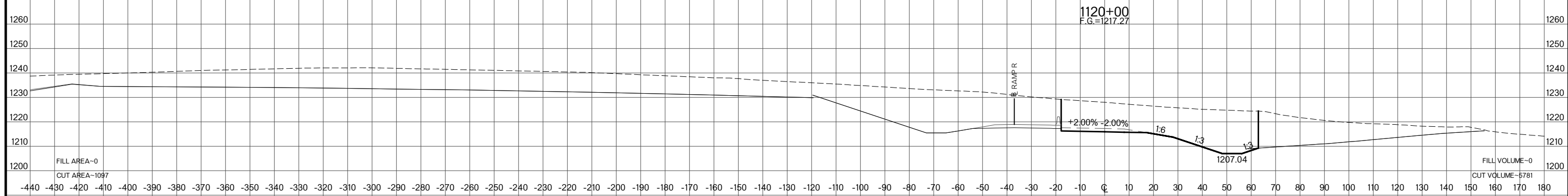
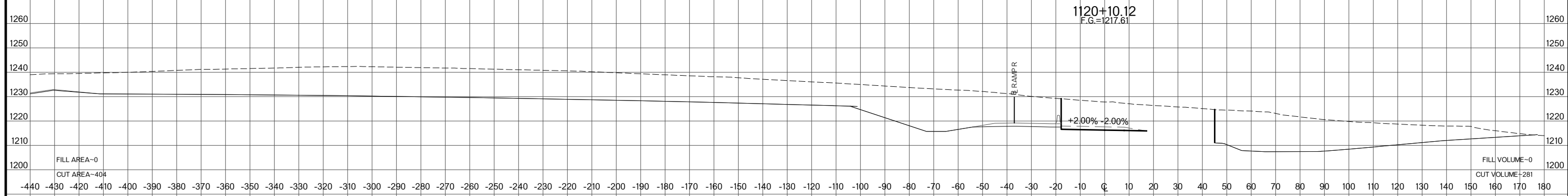
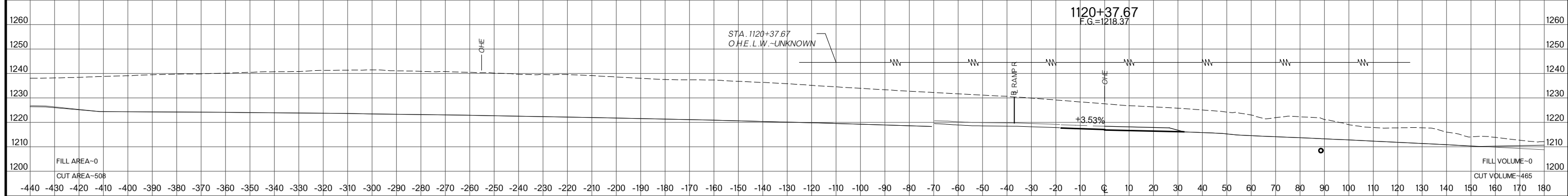
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP Q

US 81 REALIGNMENT
GRADY COUNTY

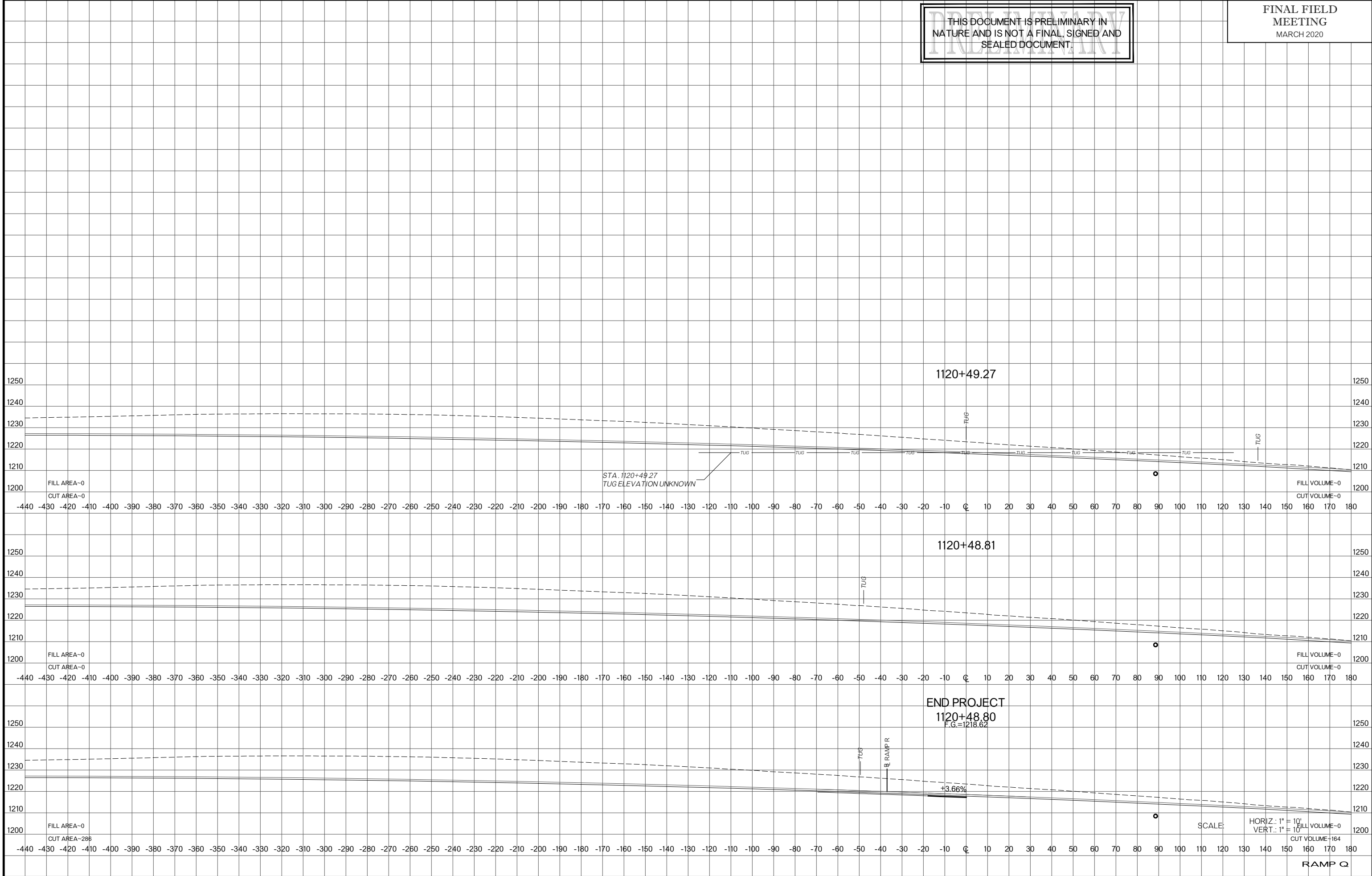
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



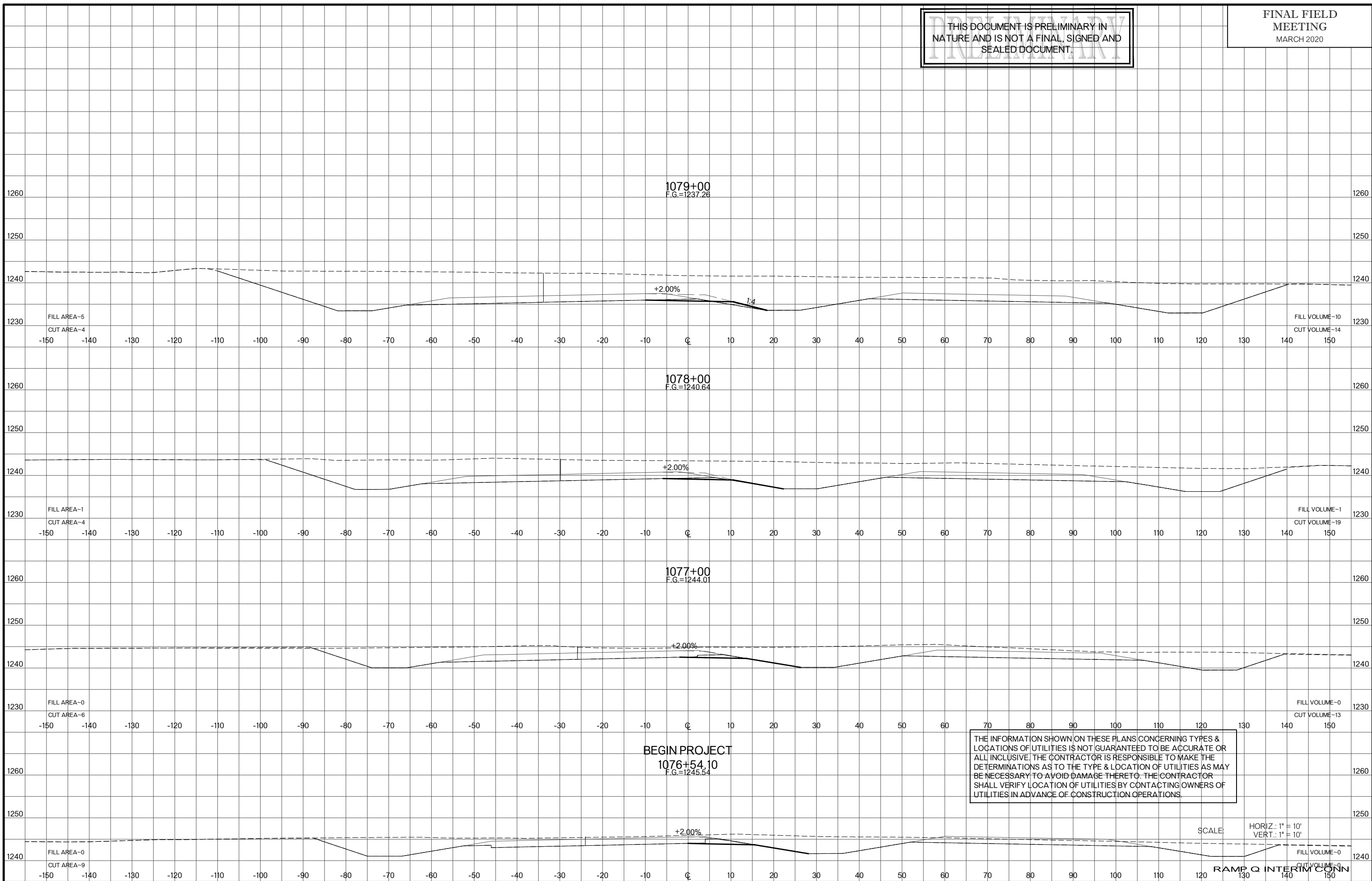
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

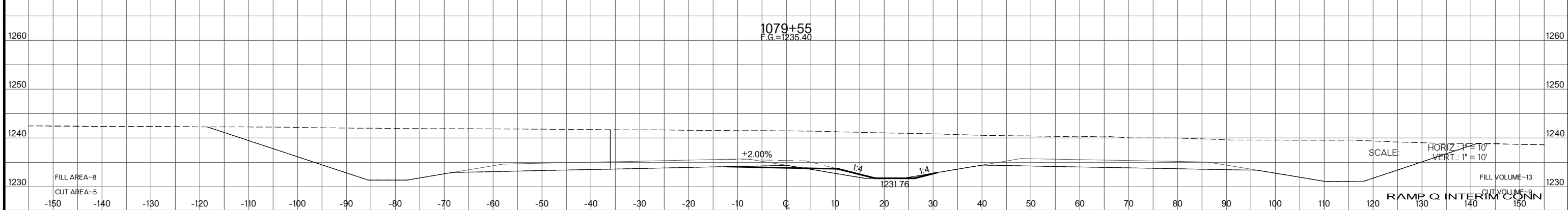
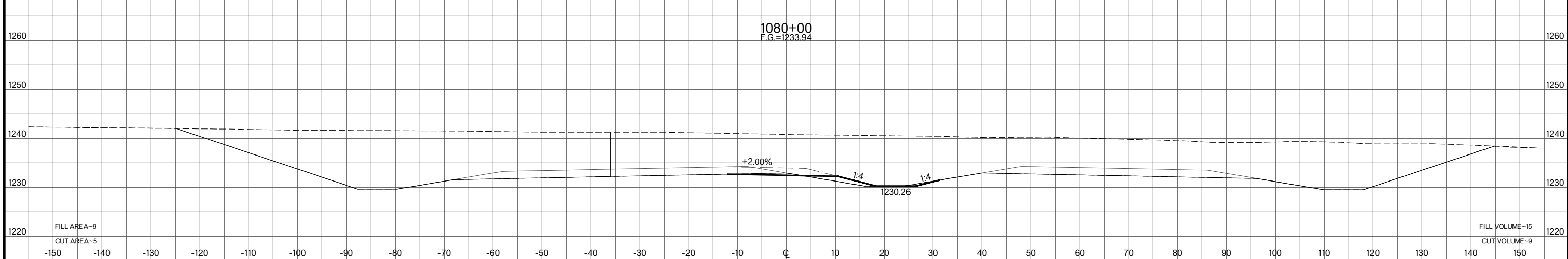
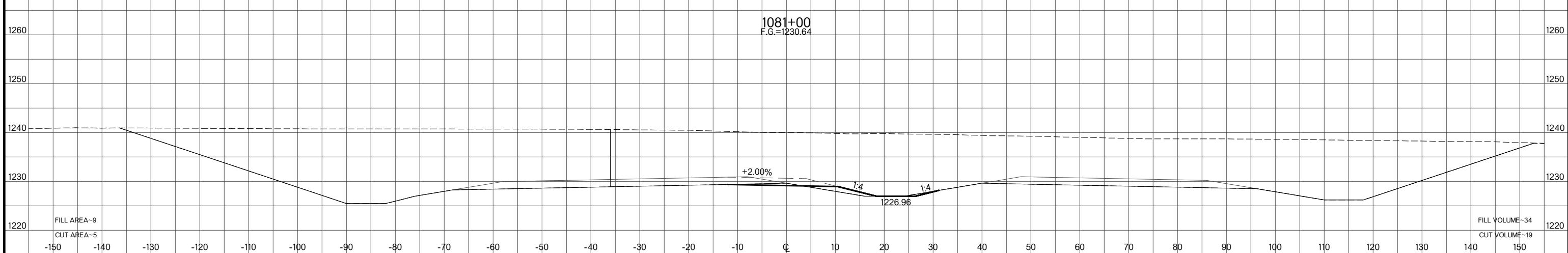
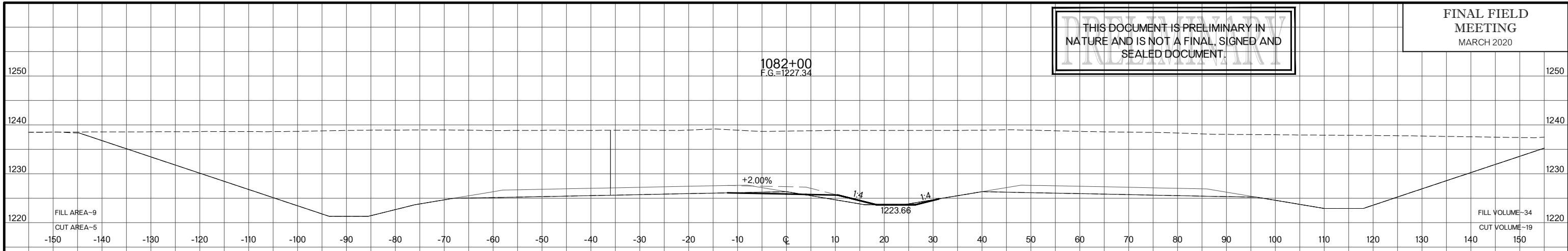
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

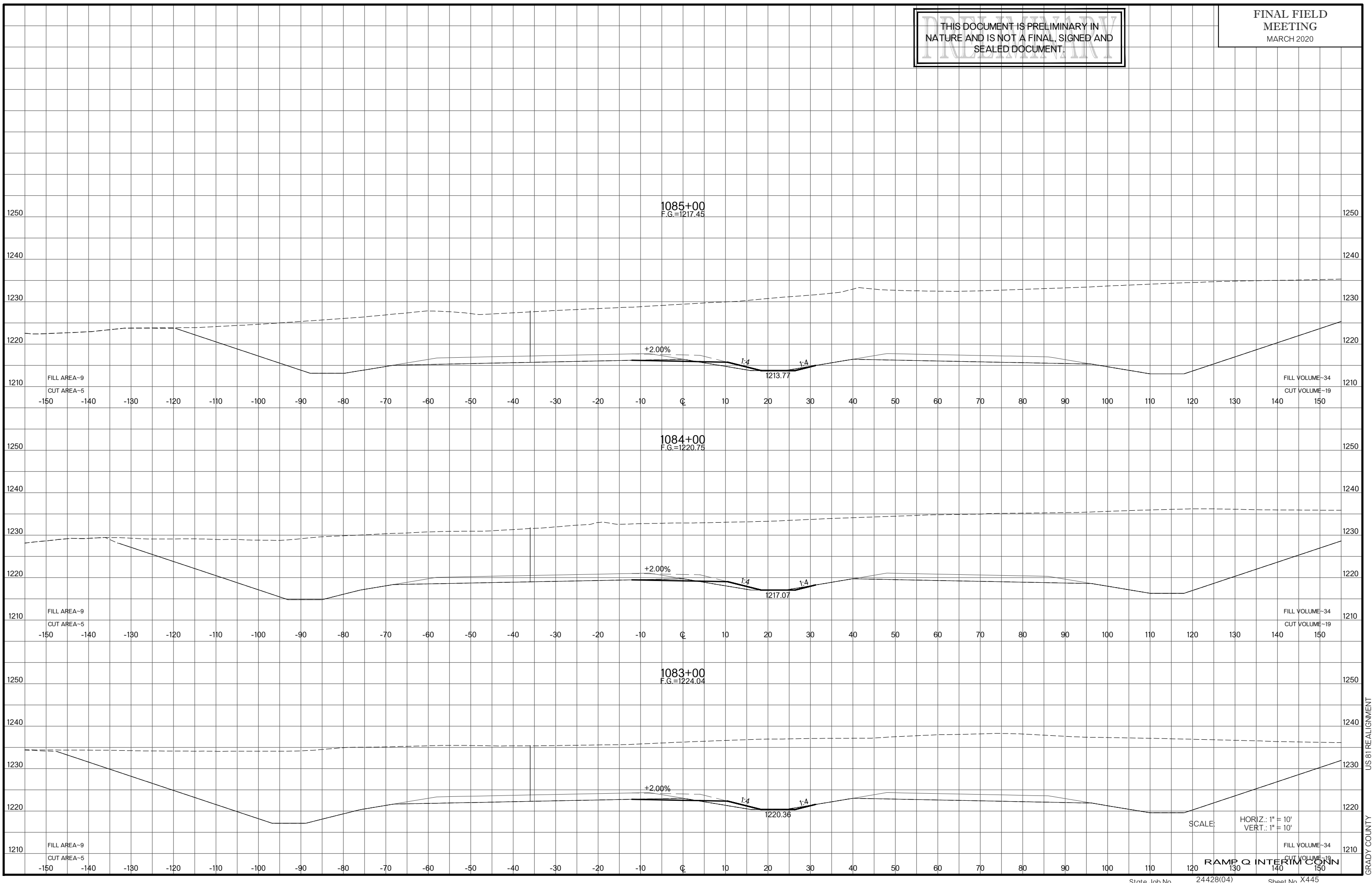
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

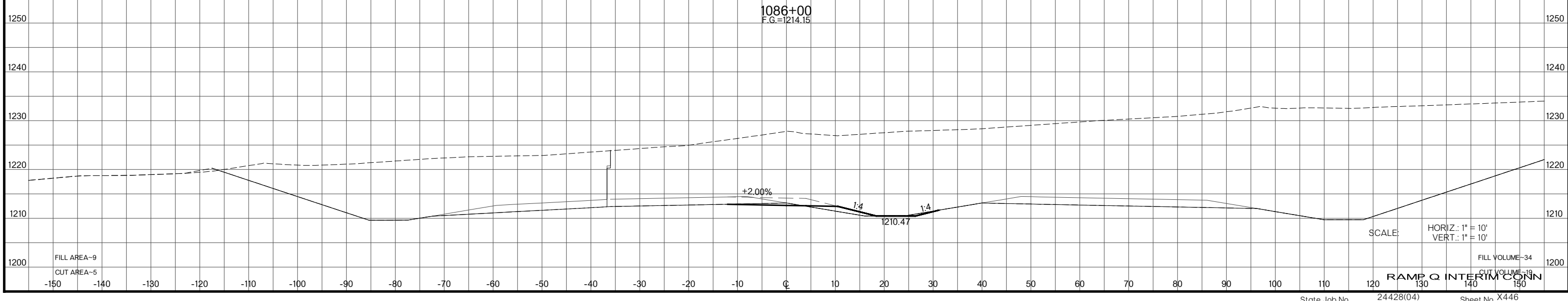
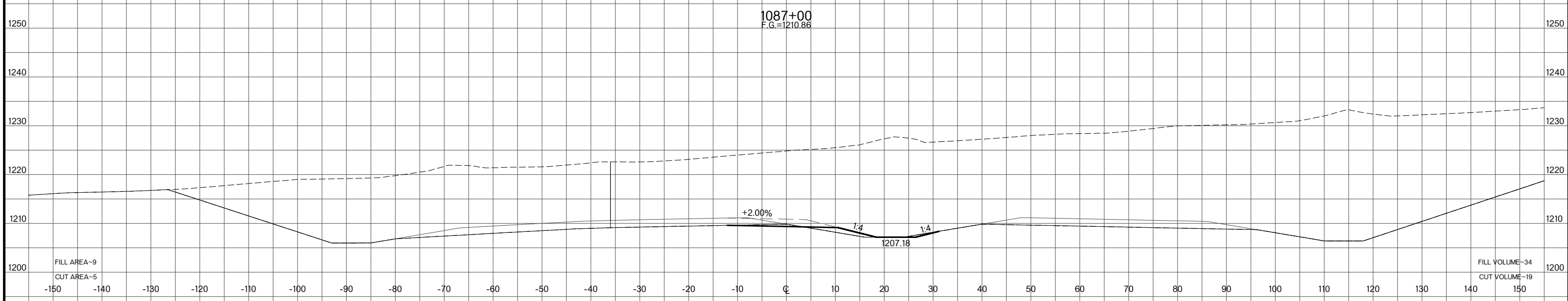
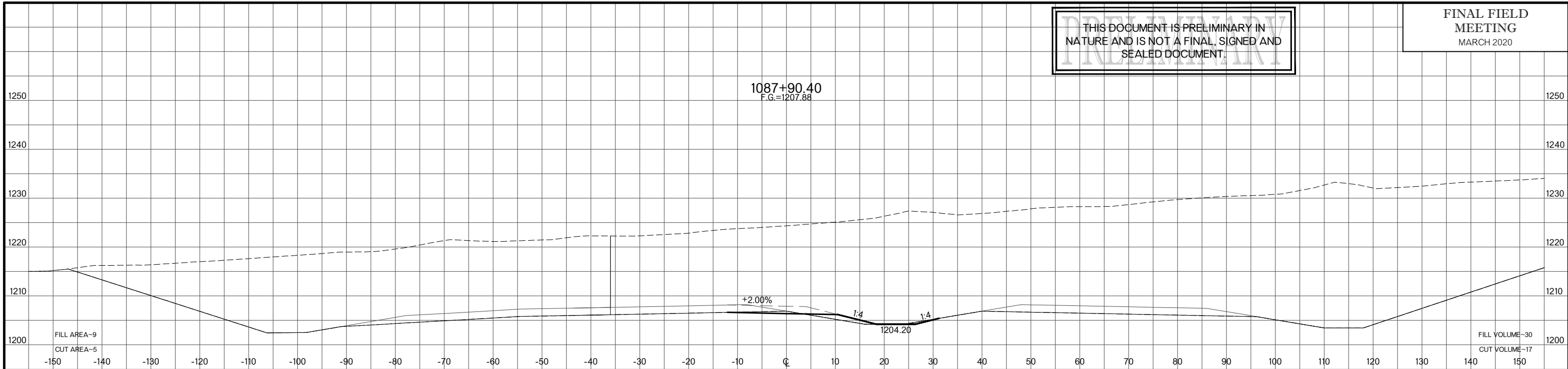
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

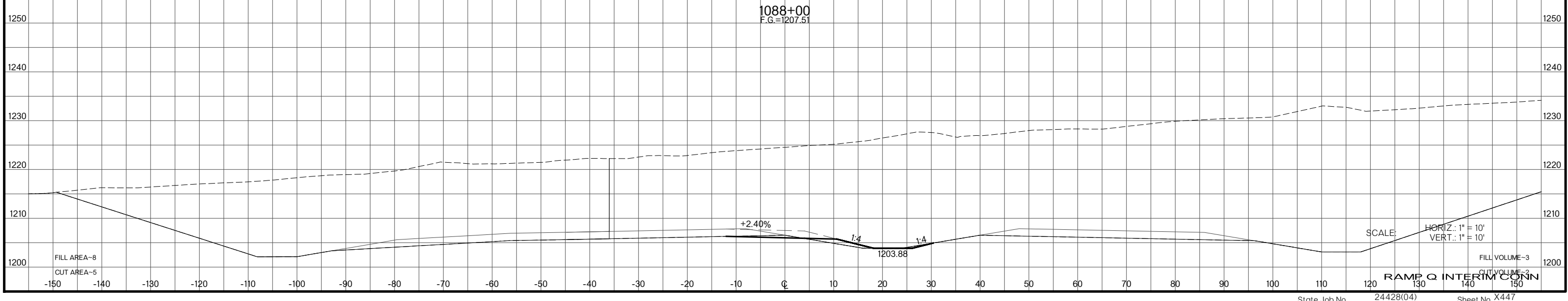
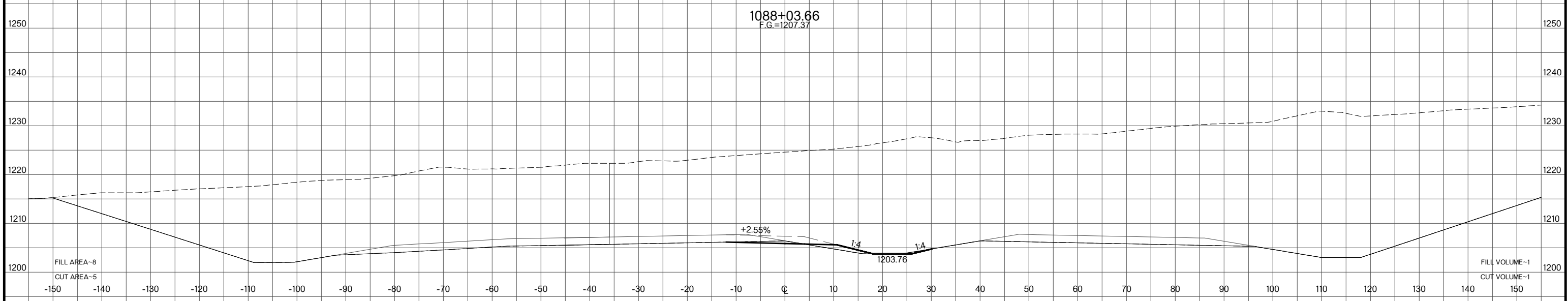
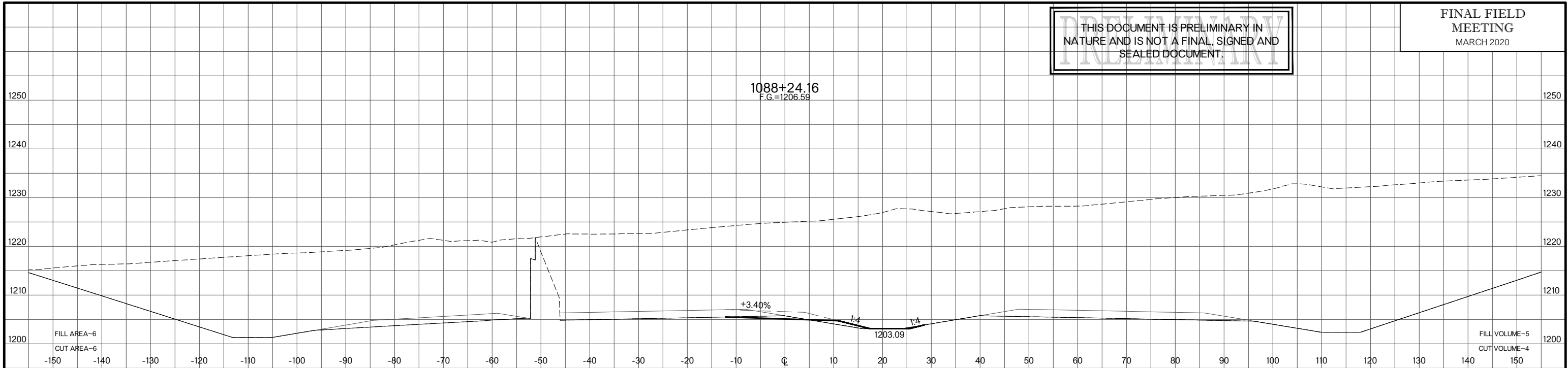
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



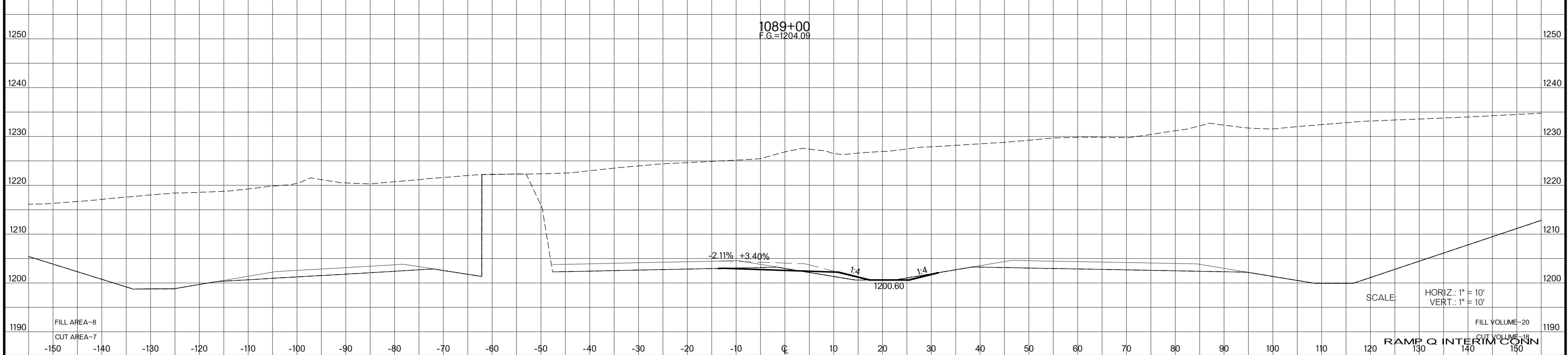
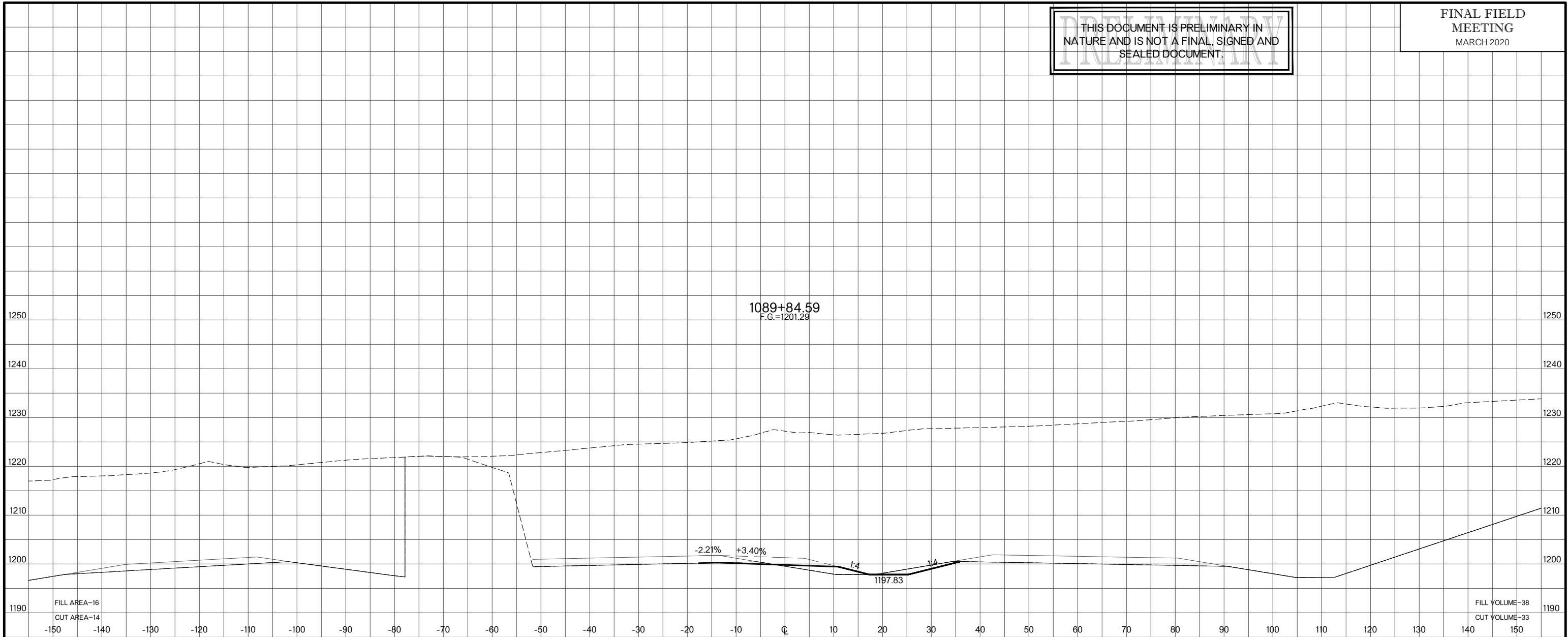
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



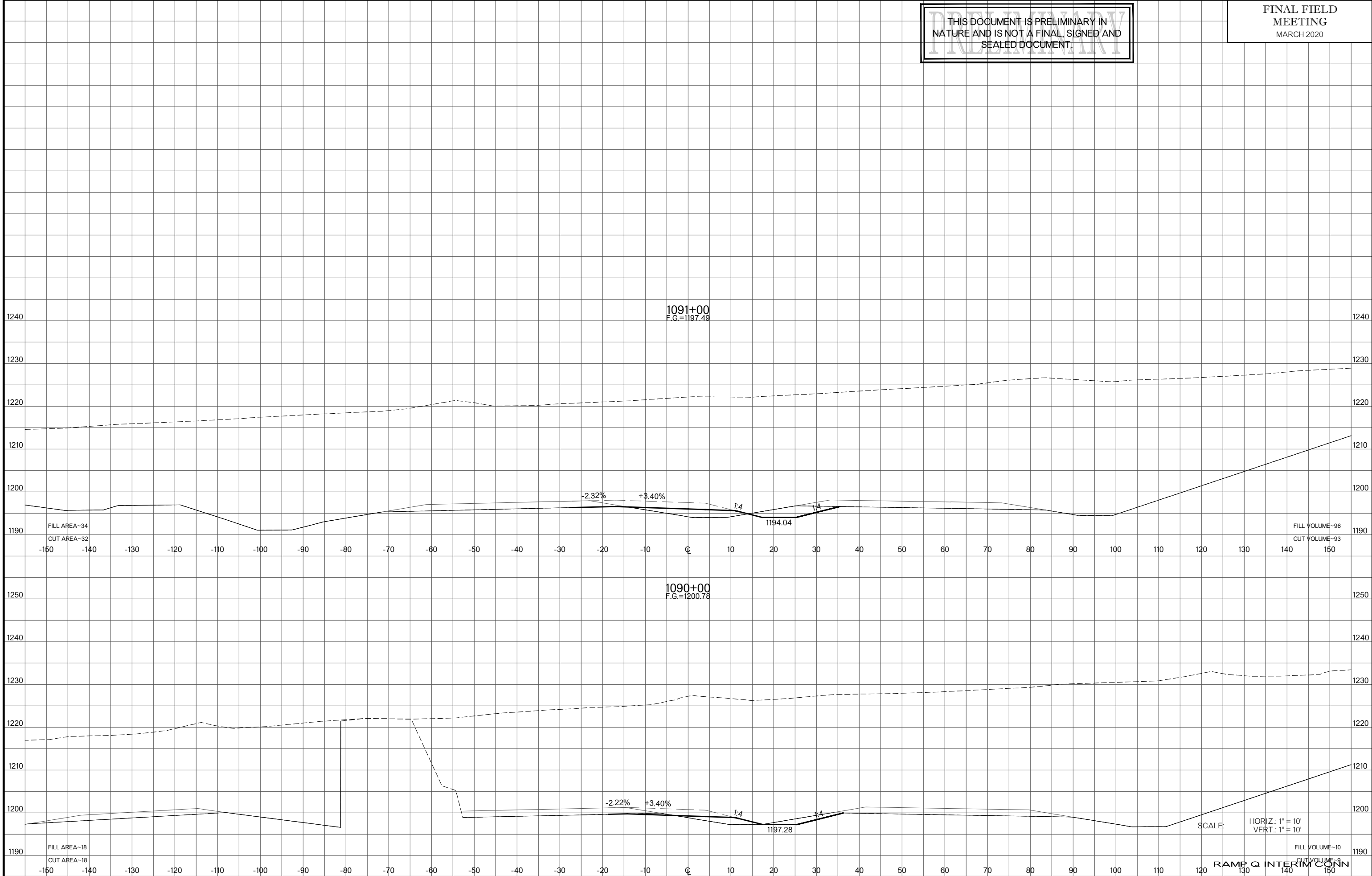
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



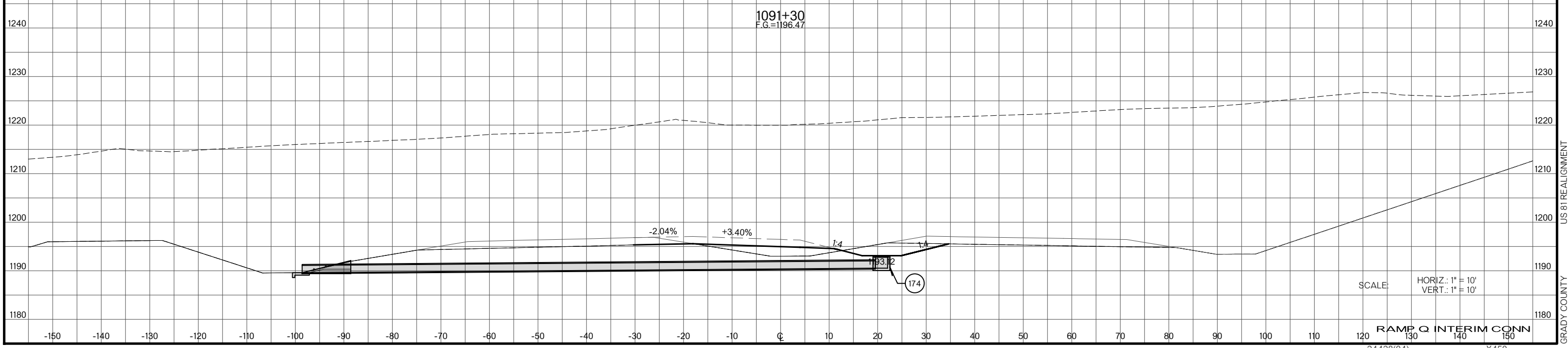
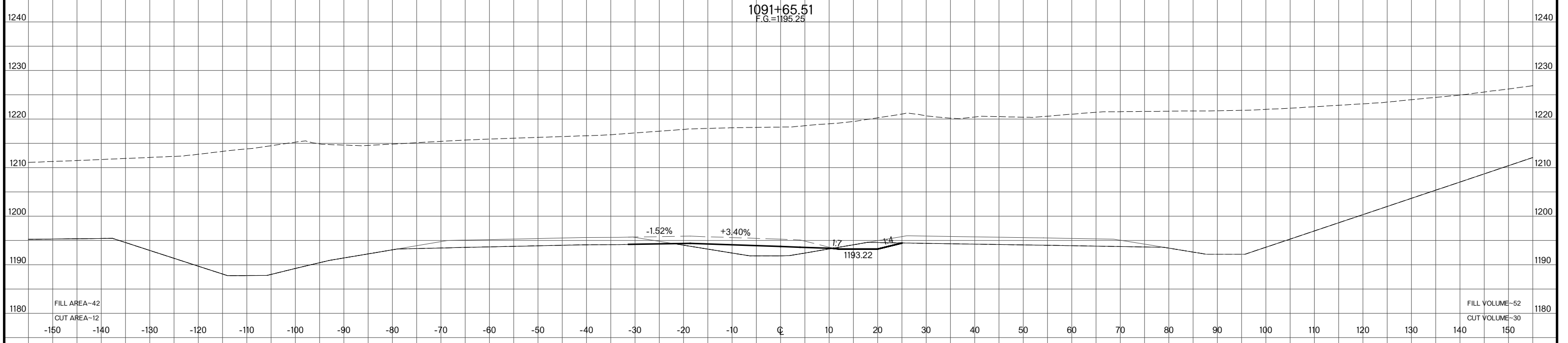
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

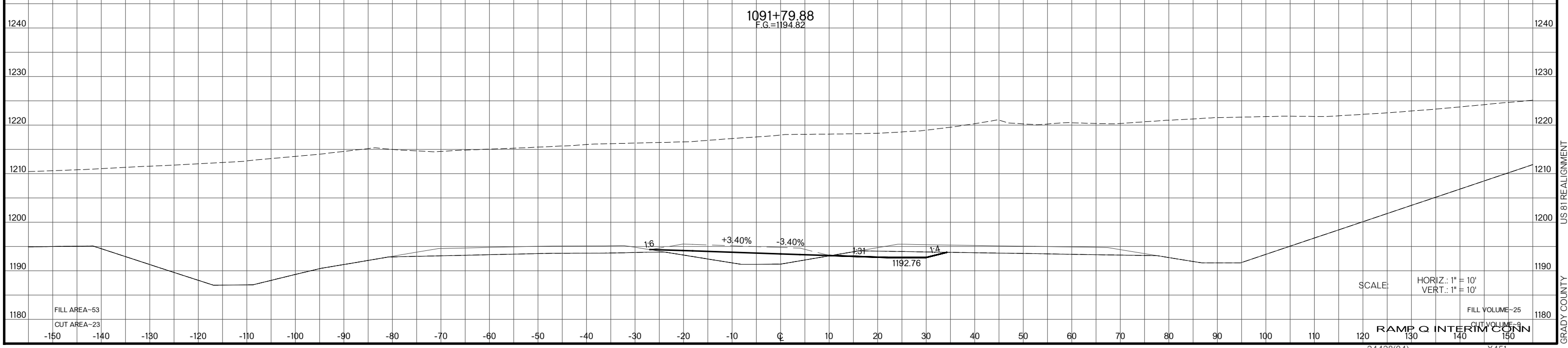
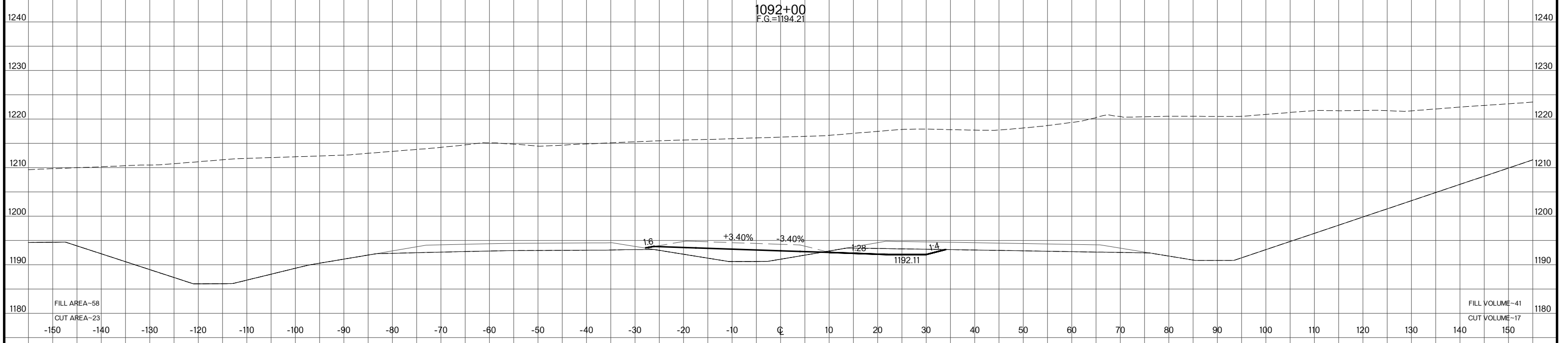
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

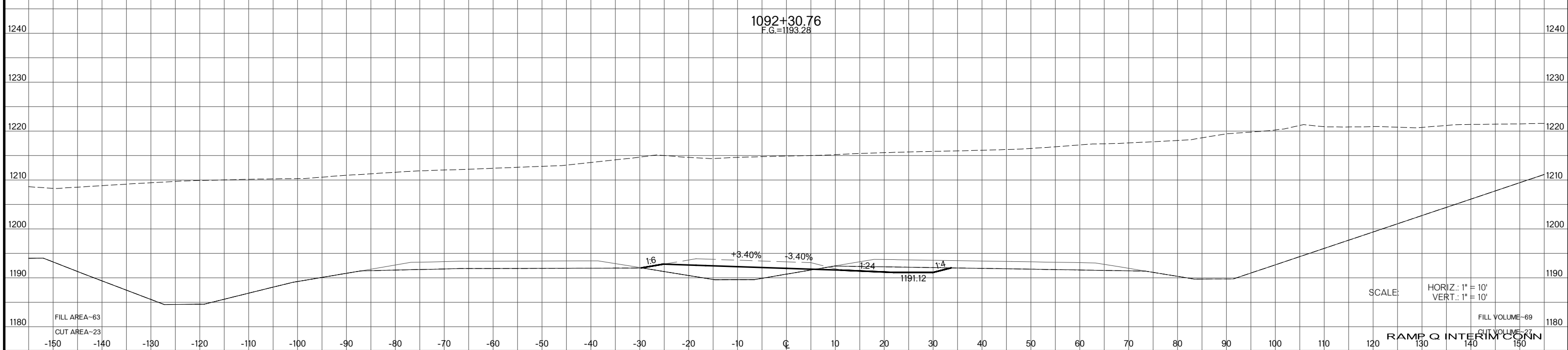
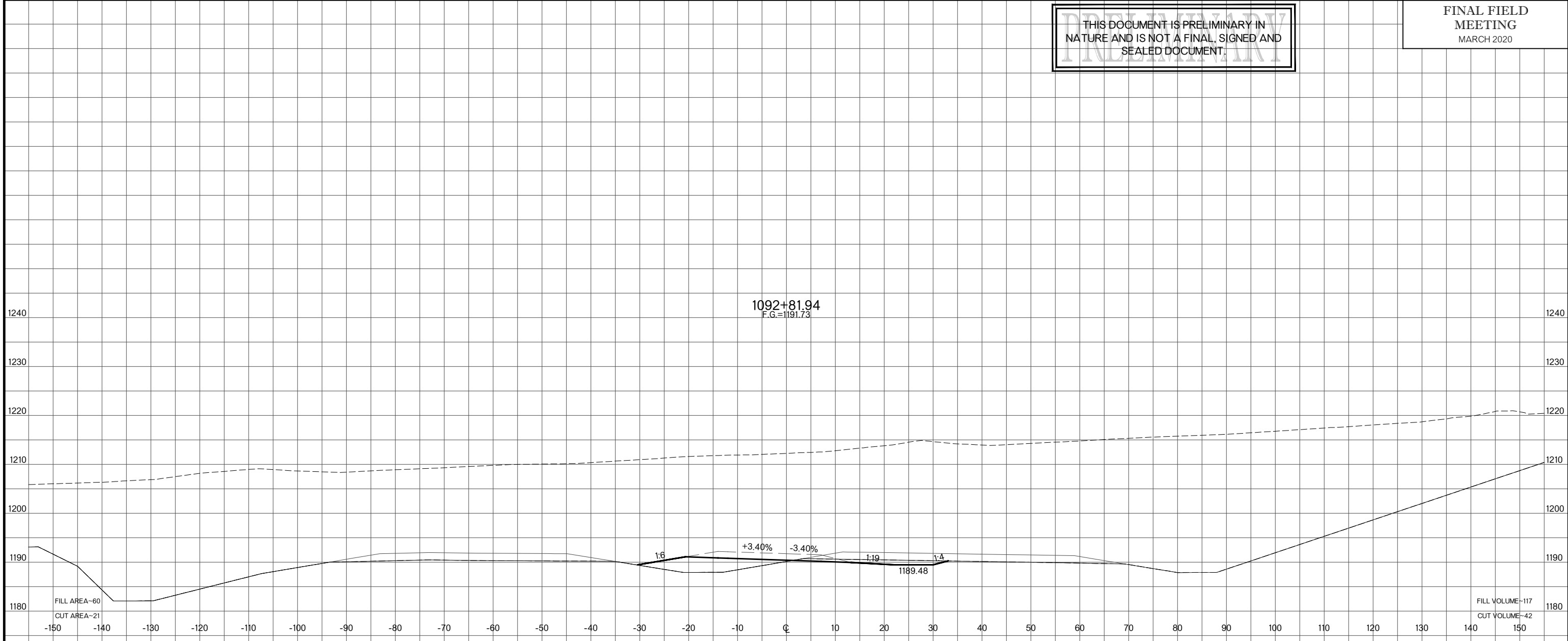
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



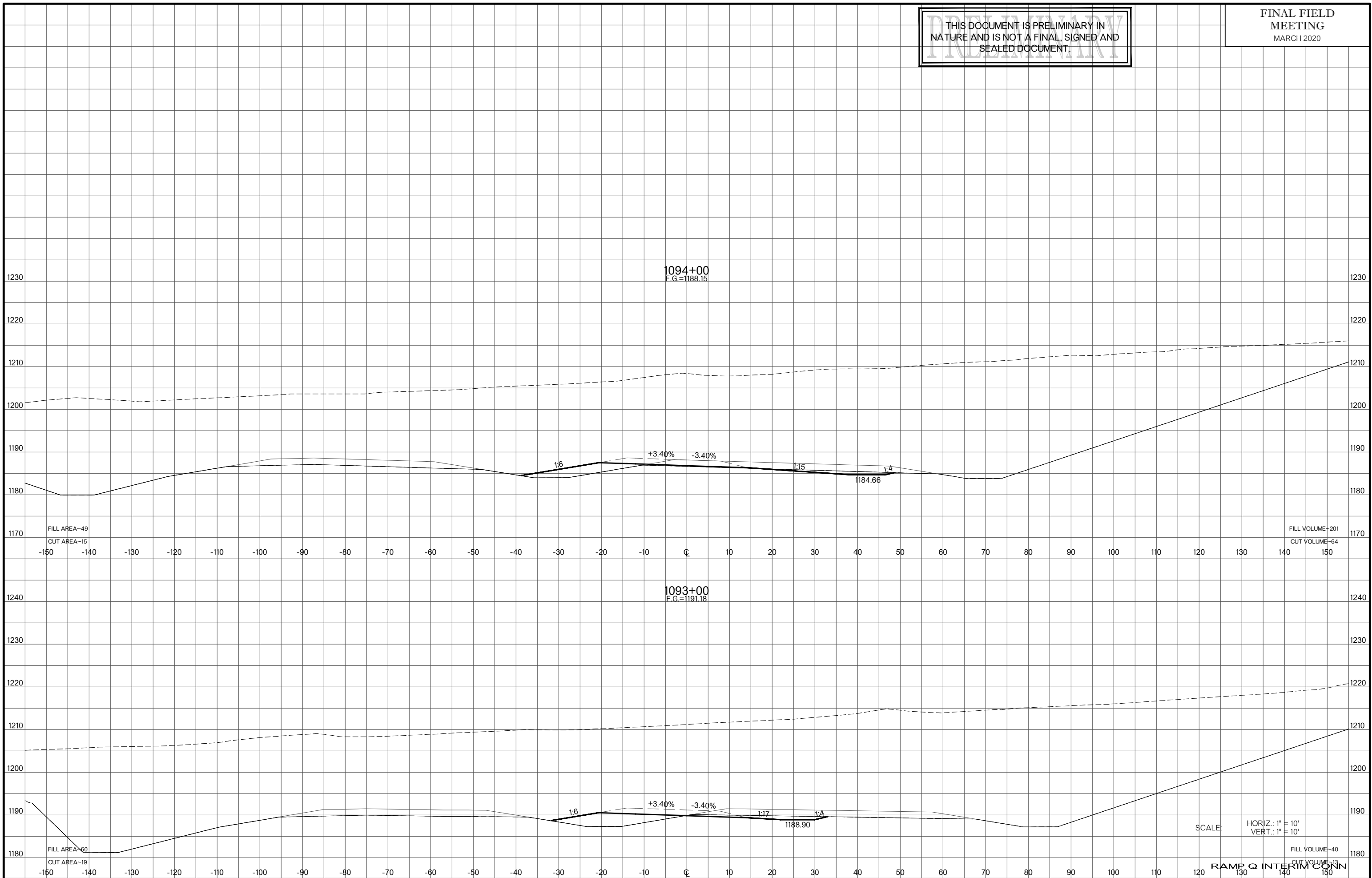
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP Q INTERIM CONN

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

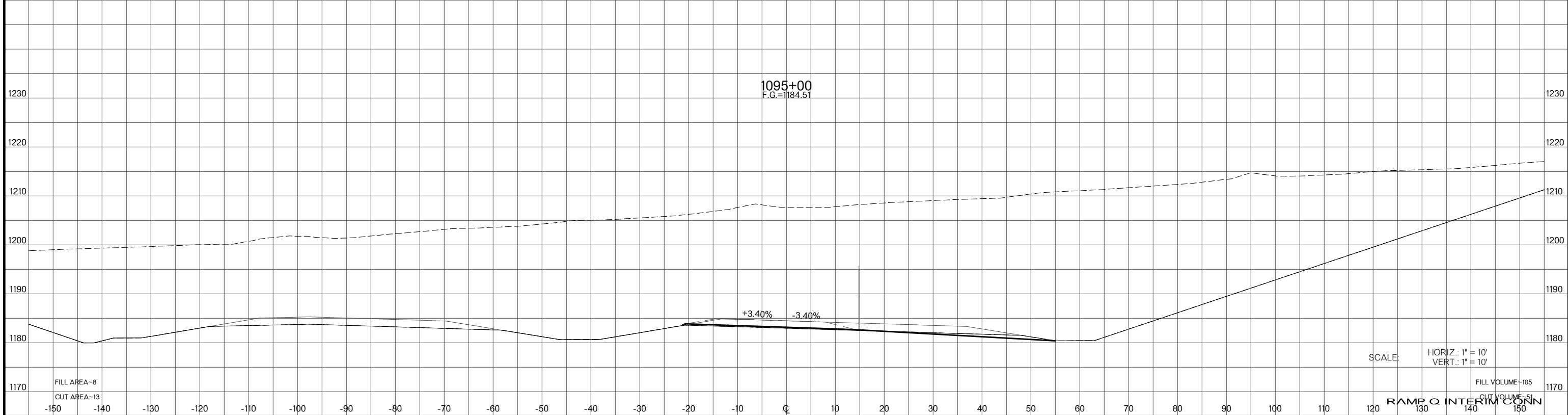
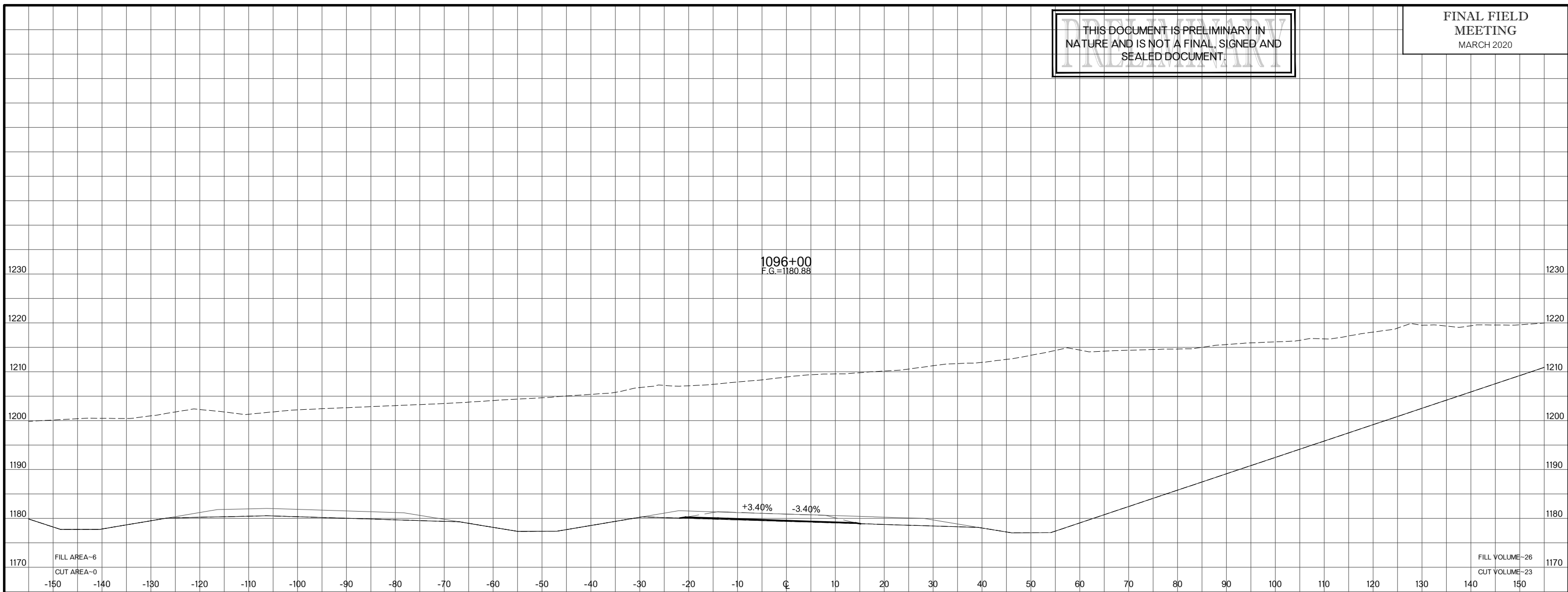


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP Q INTERIM CONN

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

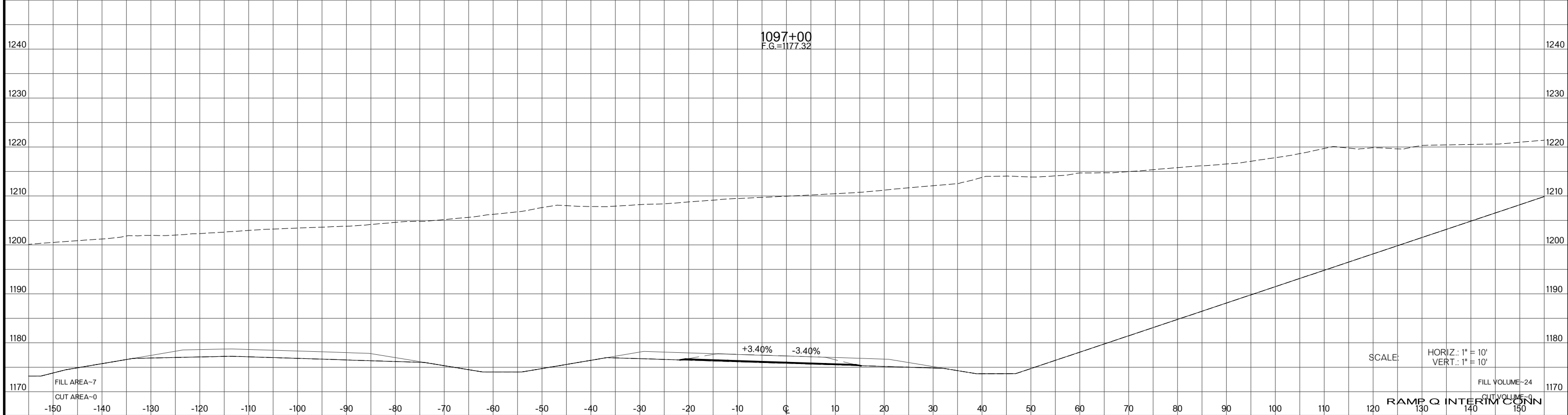
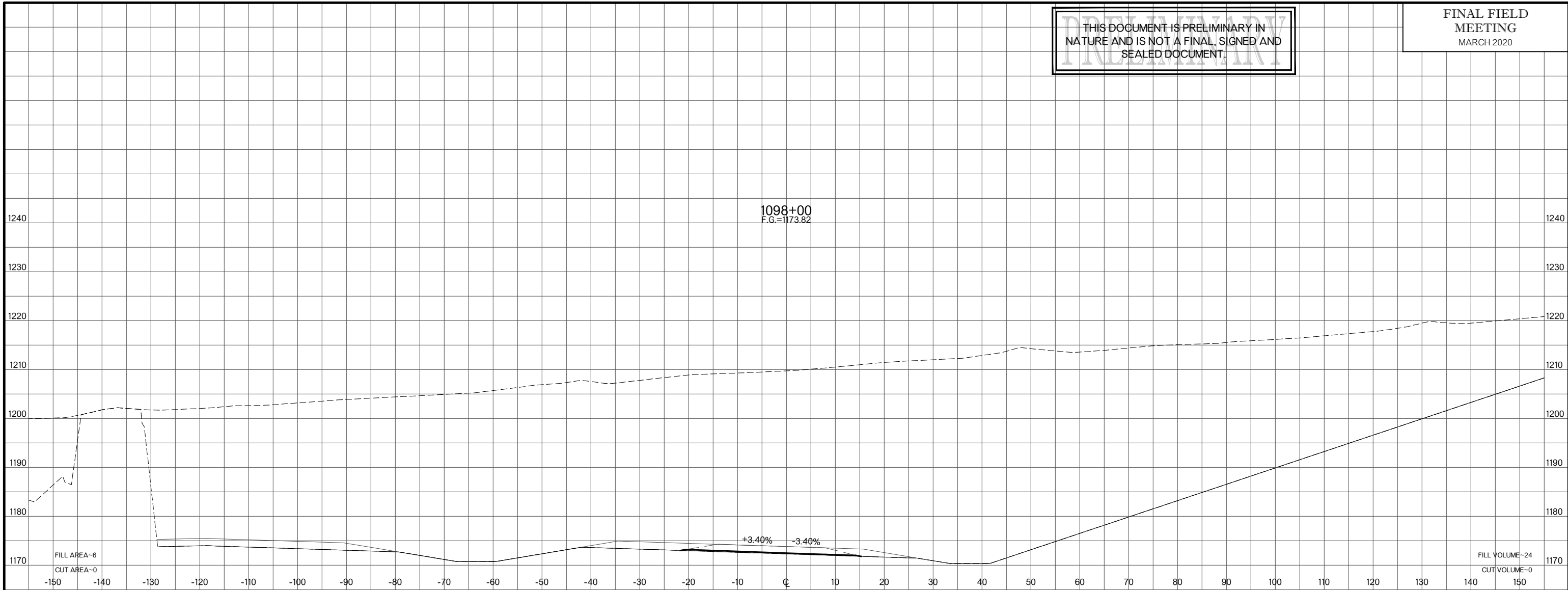


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP Q INTERIM CONN

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

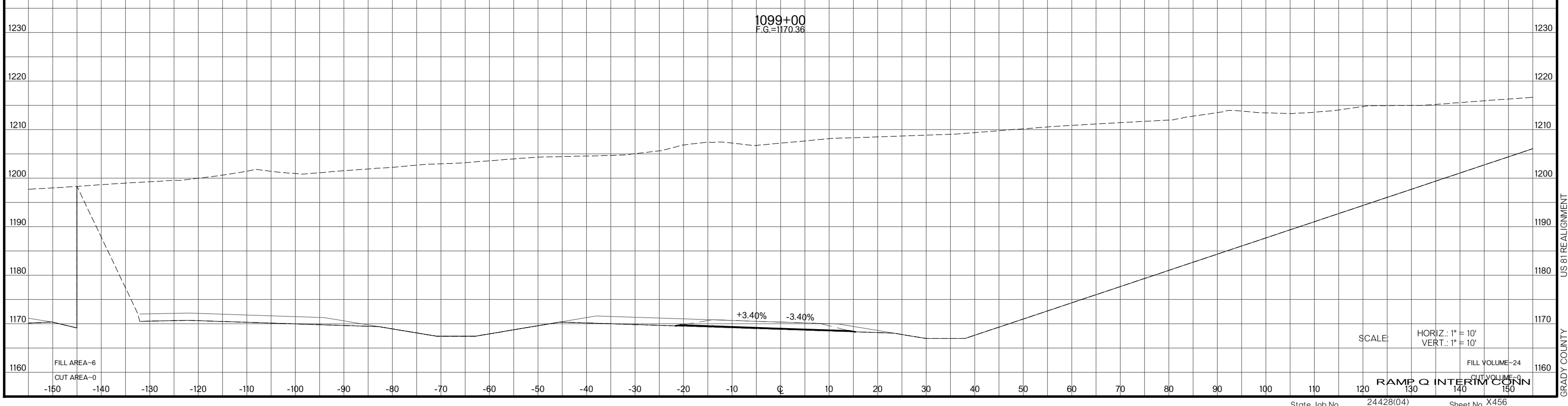
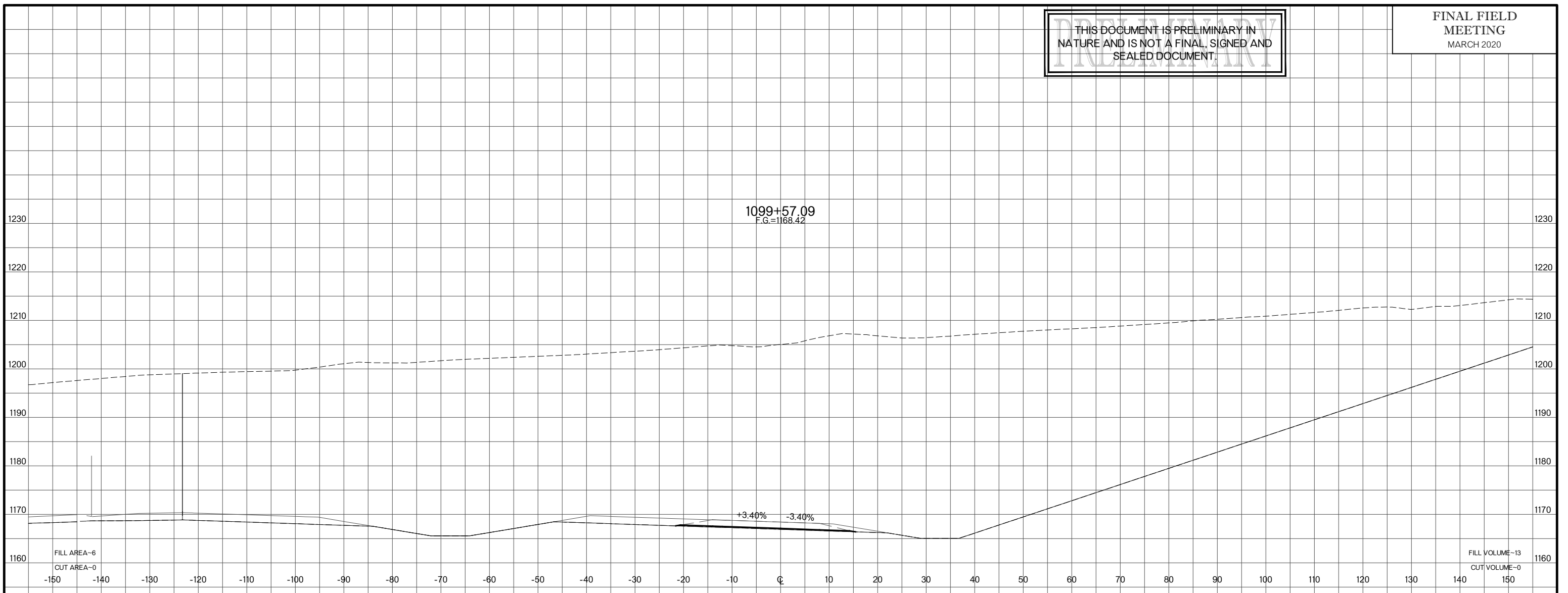


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

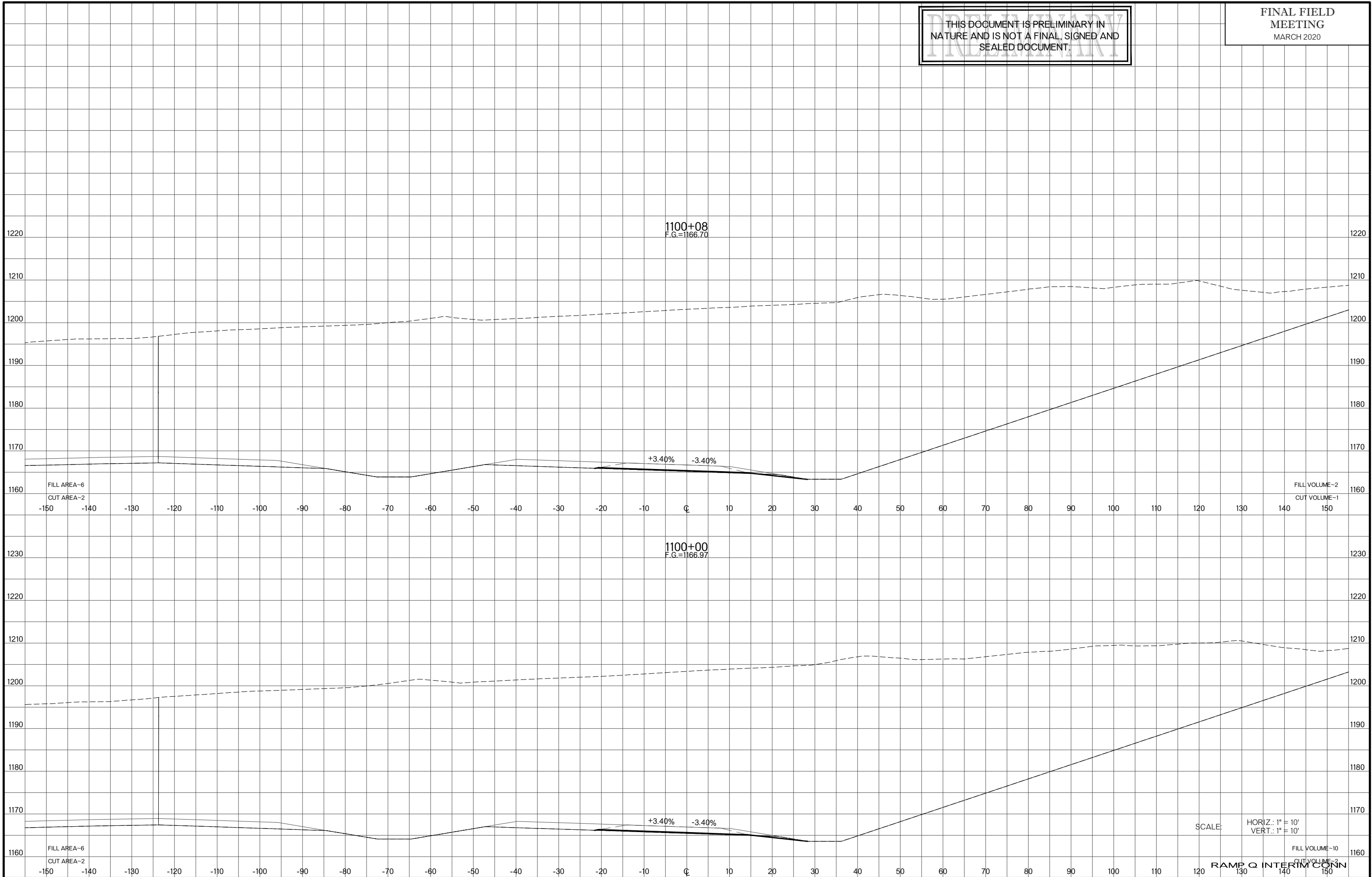
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



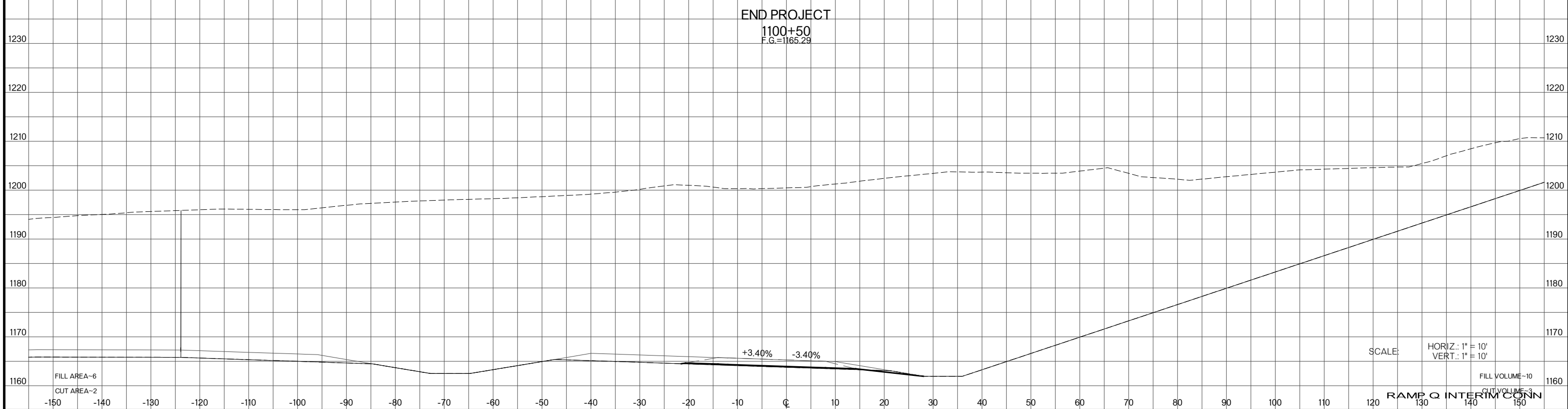
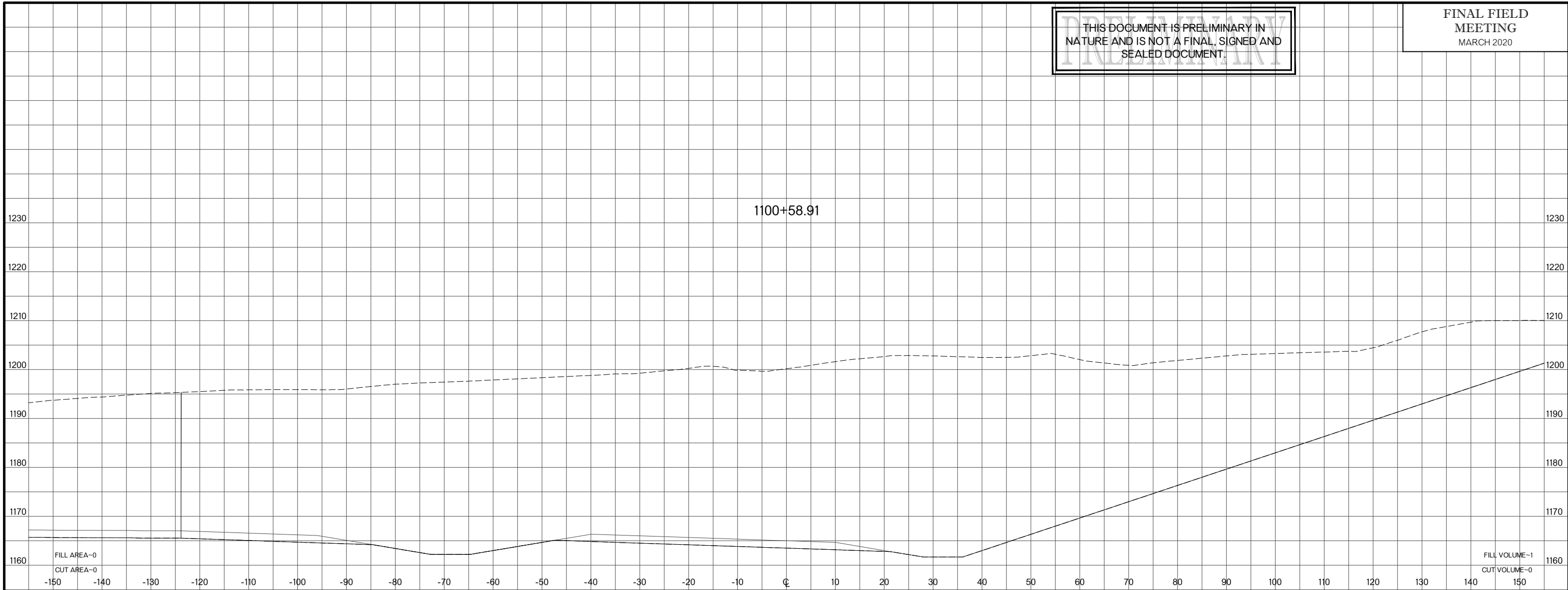
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



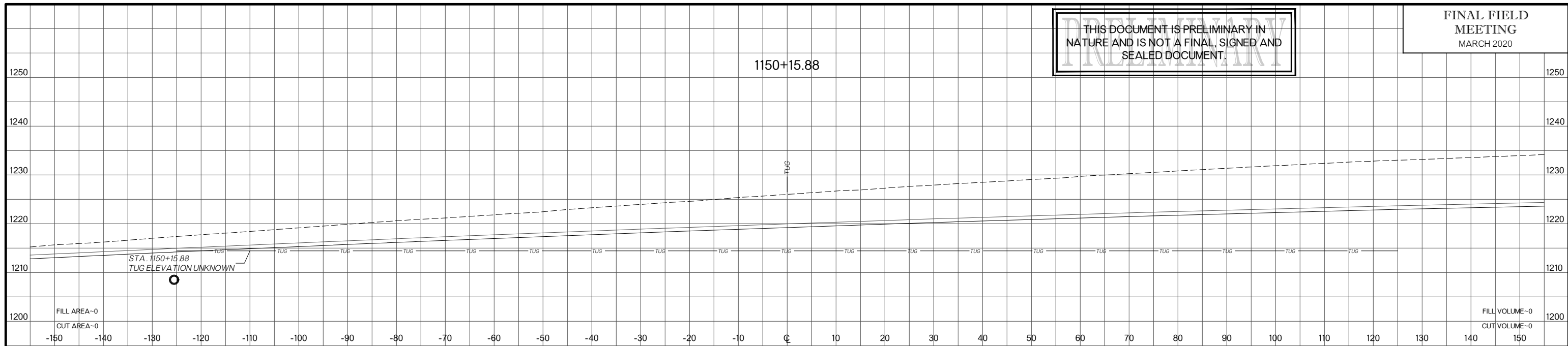
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP Q INTERIM CONN

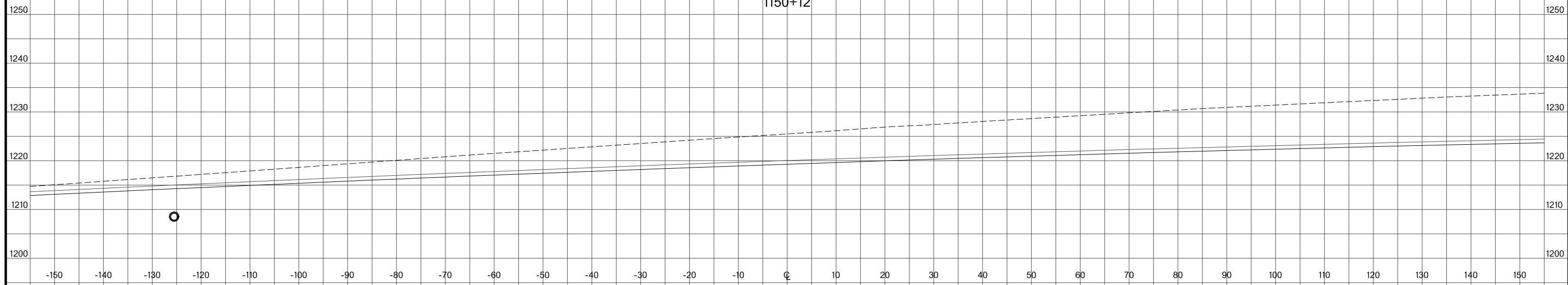
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

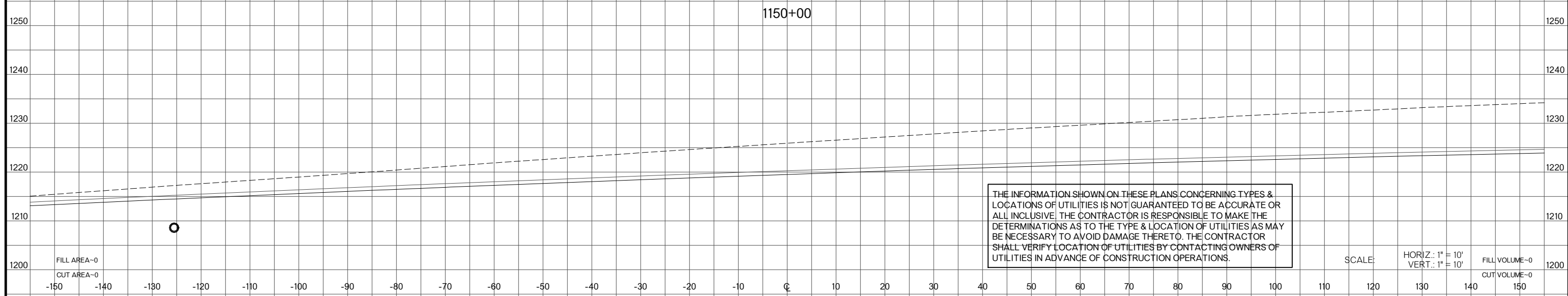
FINAL FIELD MEETING
MARCH 2020



BEGIN PROJECT
1150+12



1150+00



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

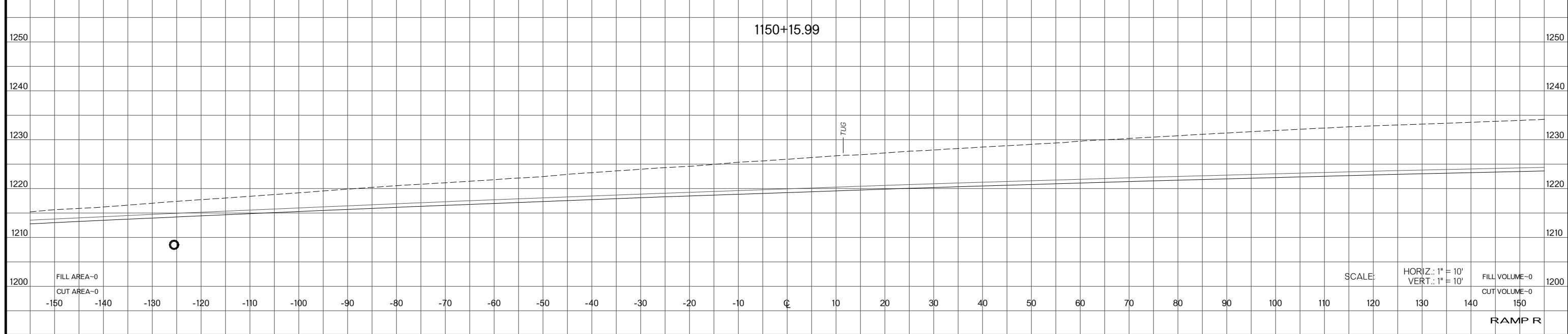
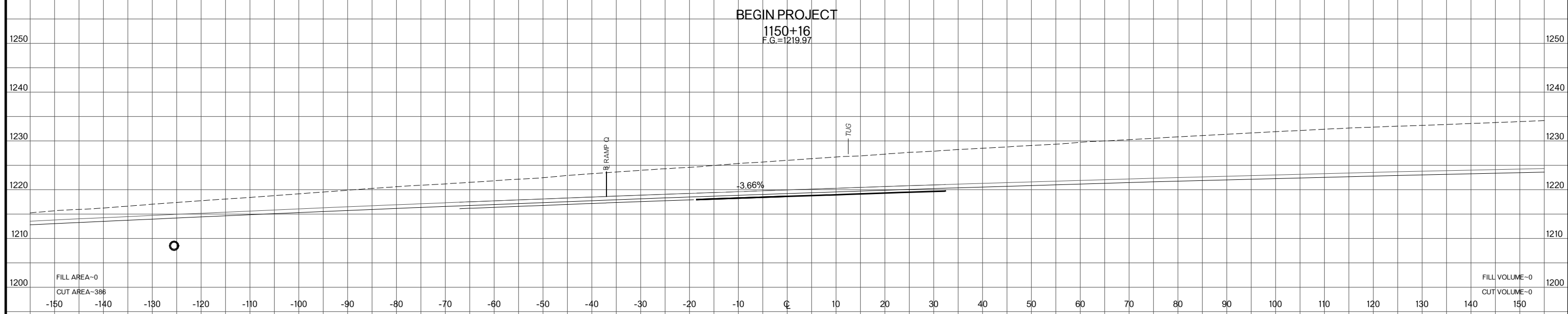
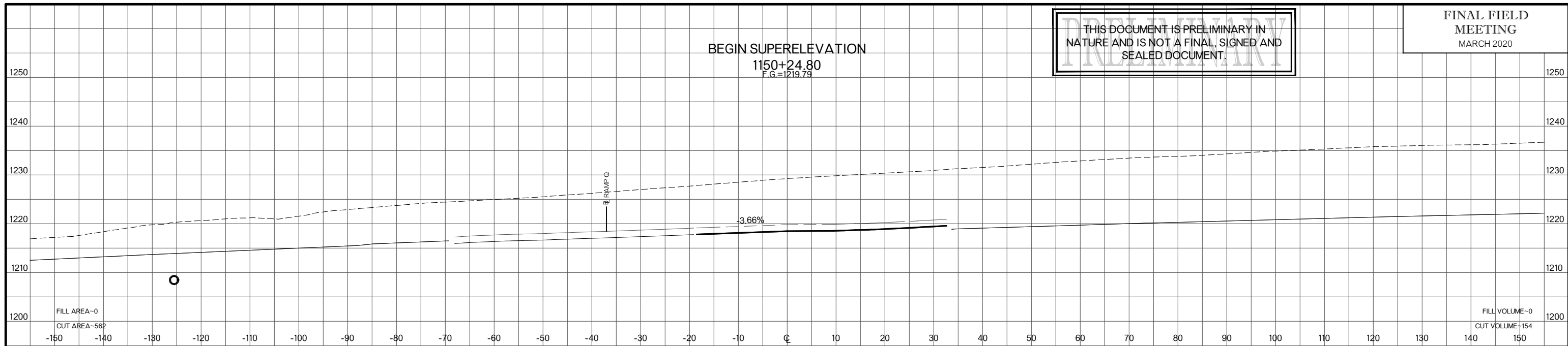
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP R

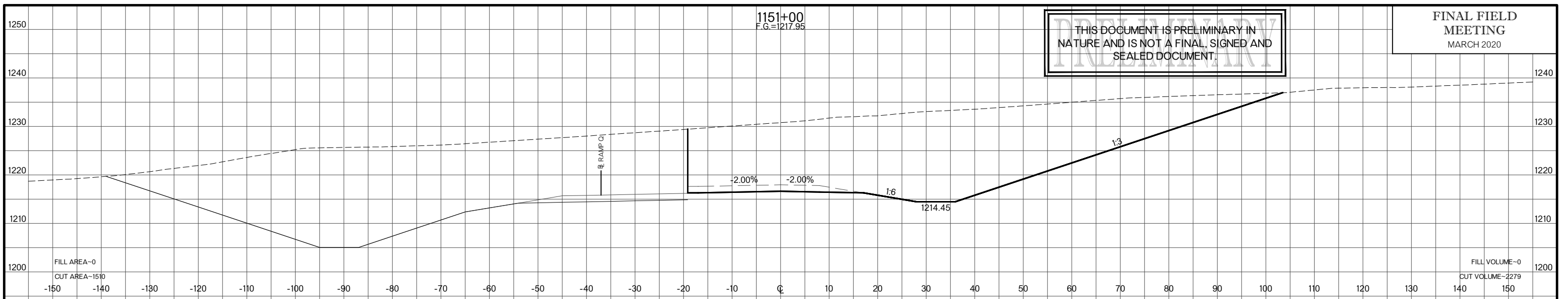
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

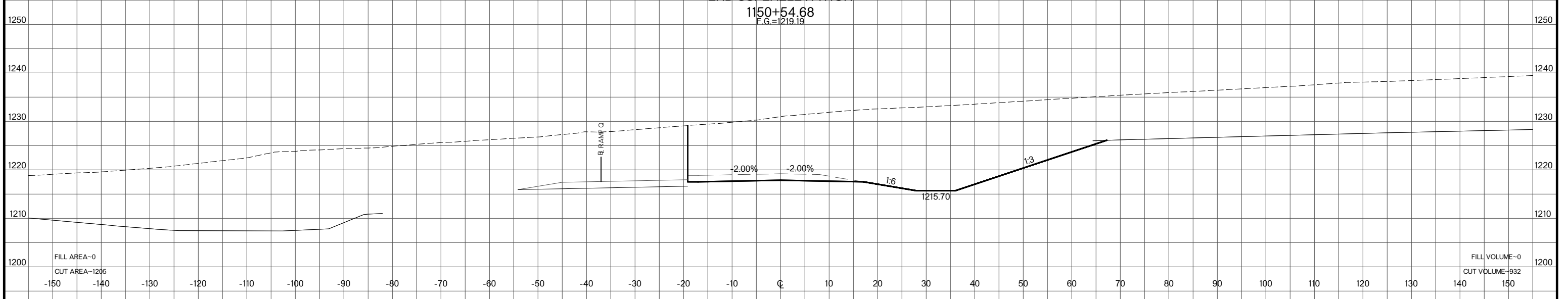
FINAL FIELD MEETING MARCH 2020



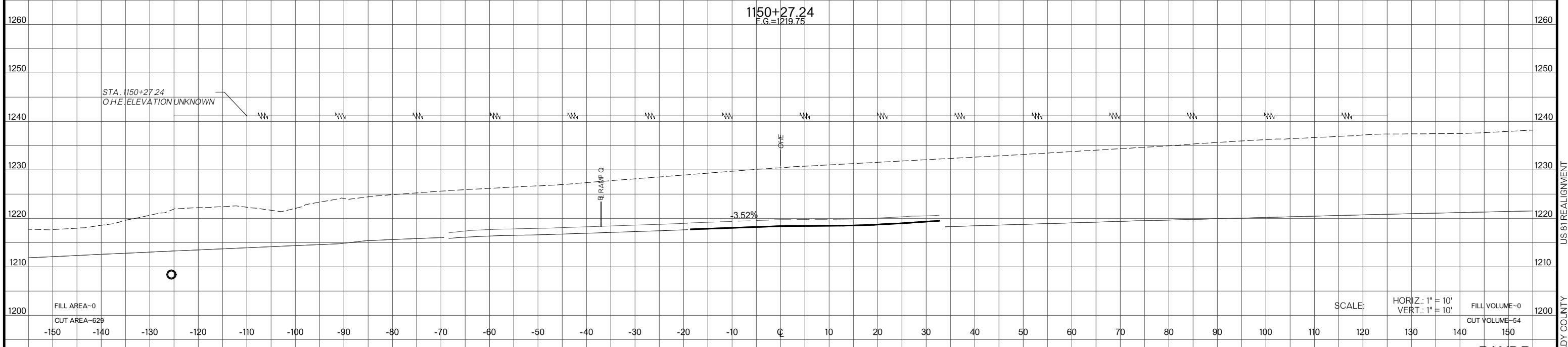
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



END SUPERELEVATION



1150+27.24
F.G.=1219.75



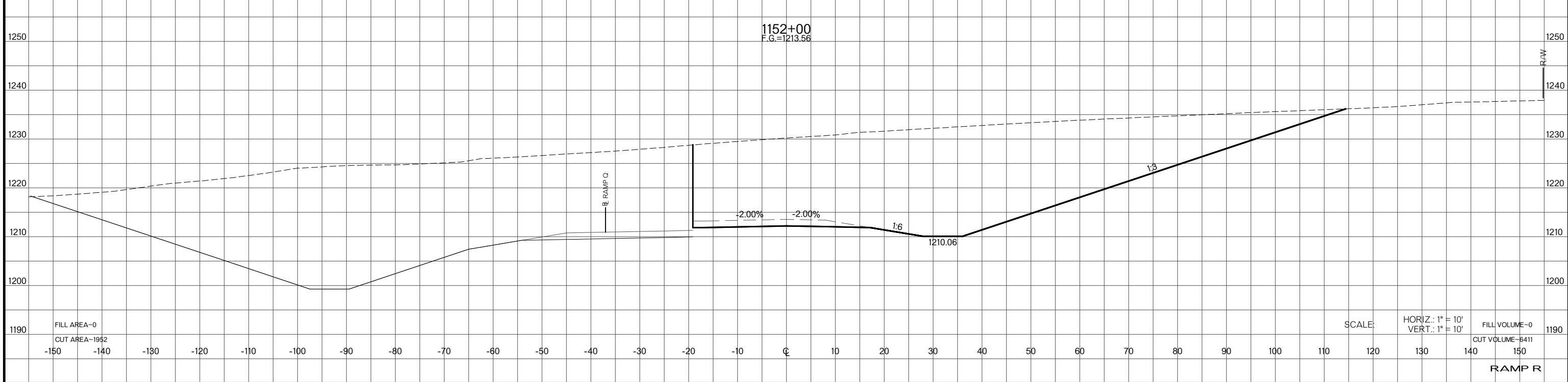
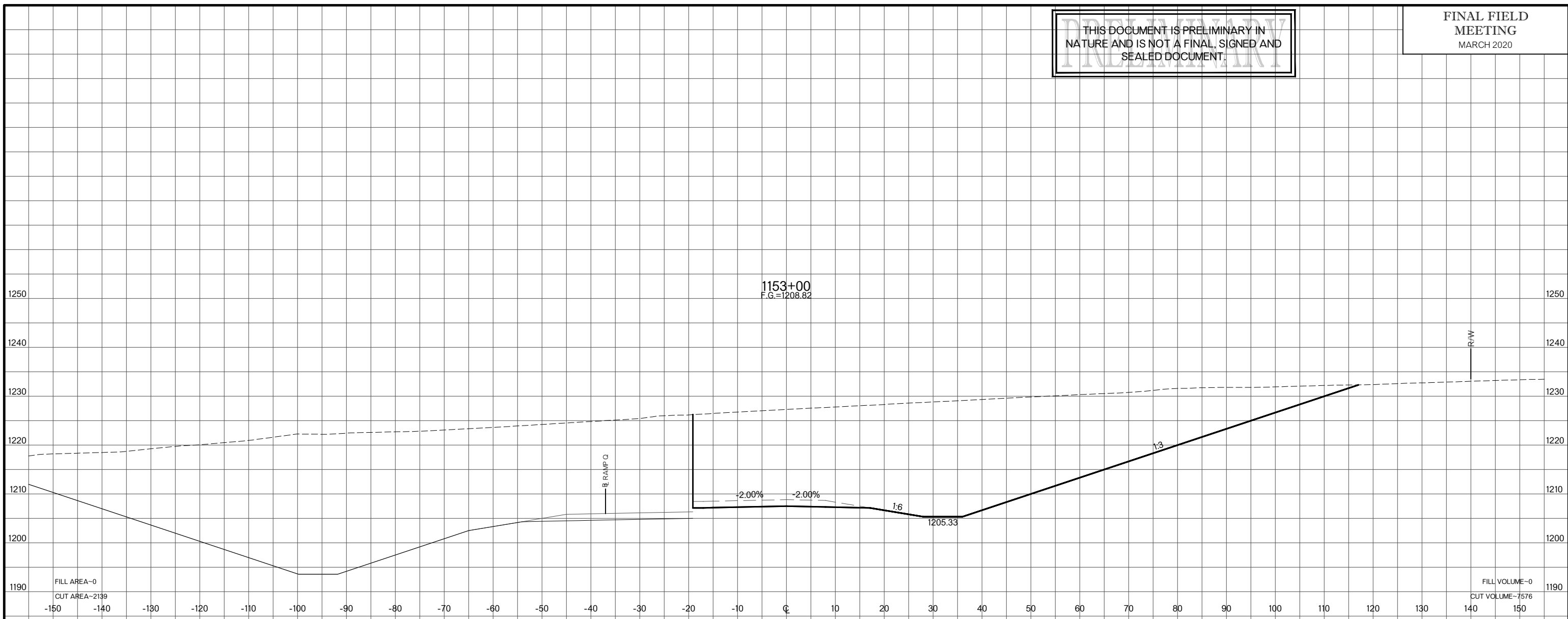
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP R

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



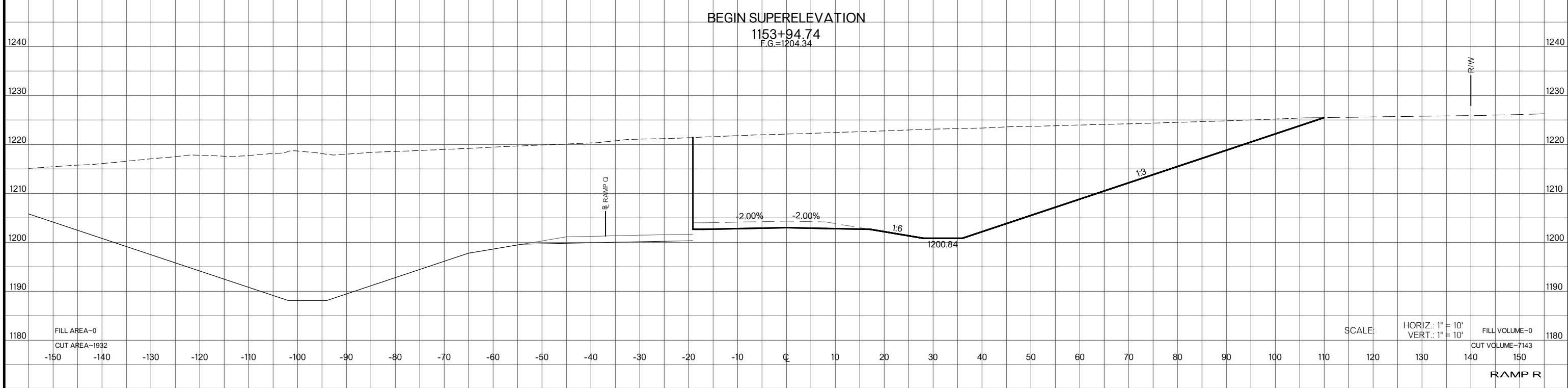
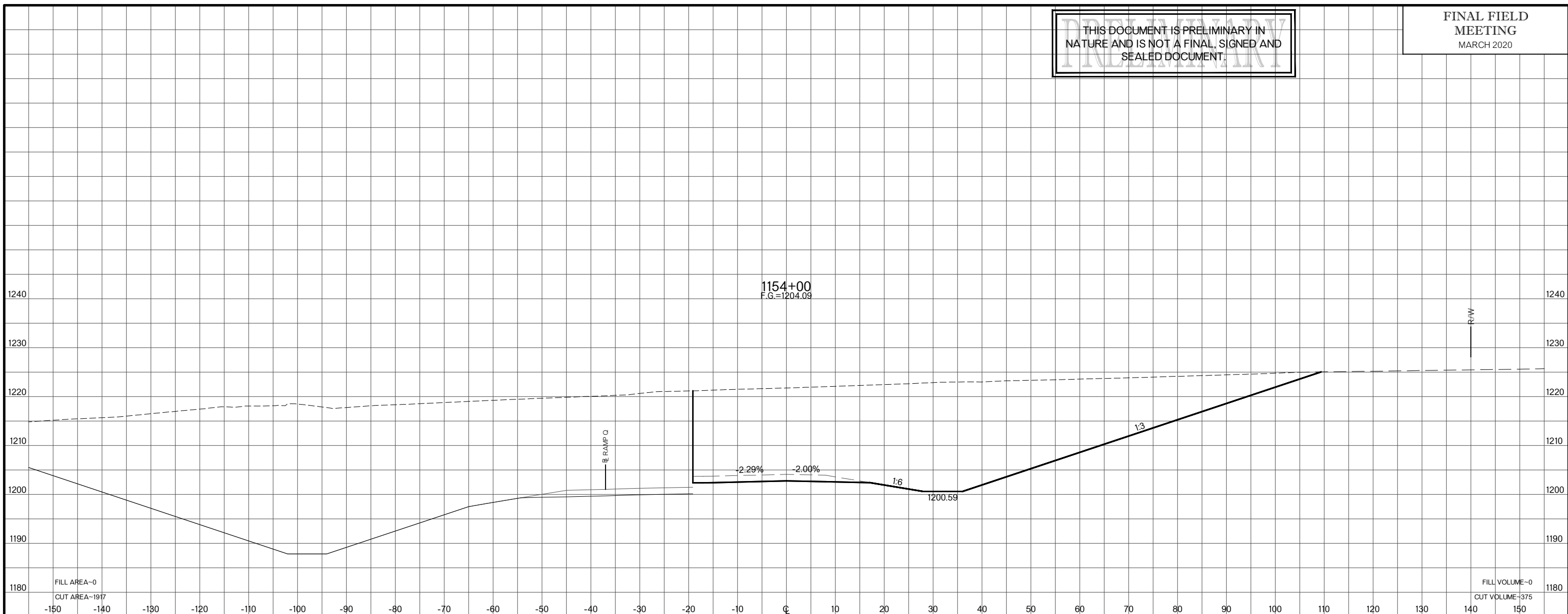
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

State Job No. 24428(04) Sheet No. X462

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

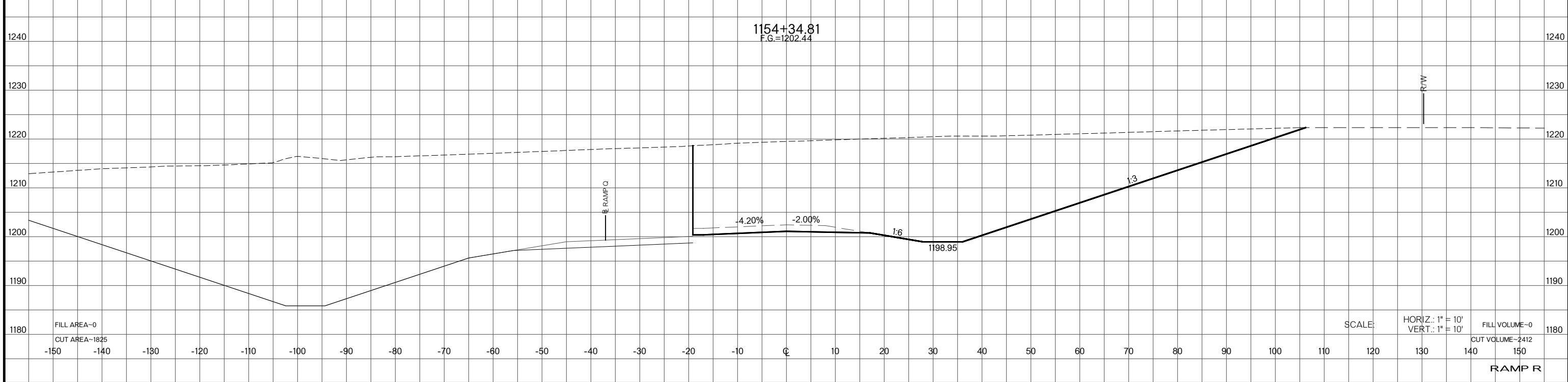
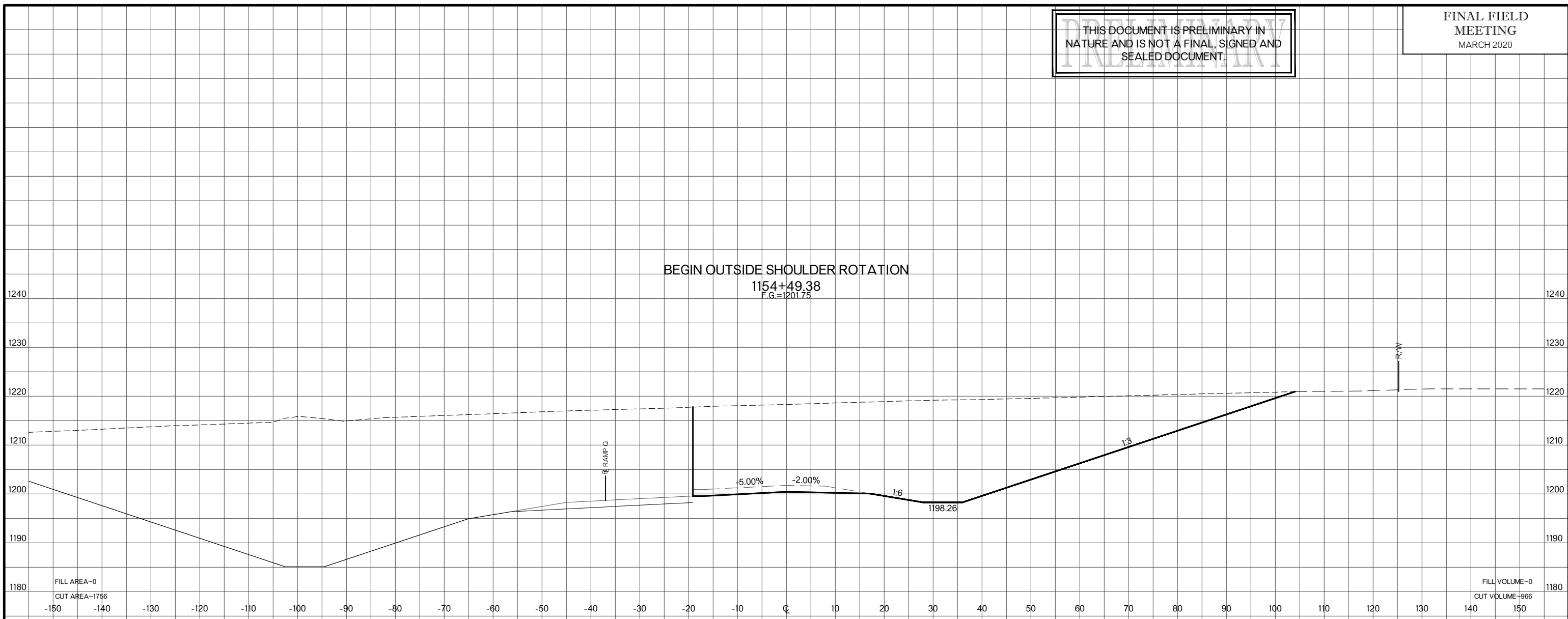
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



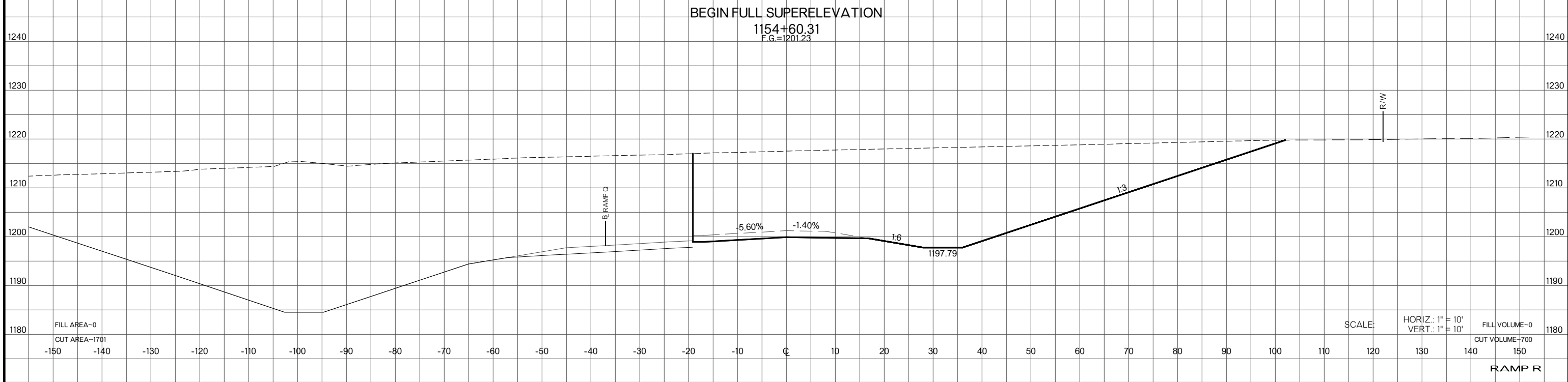
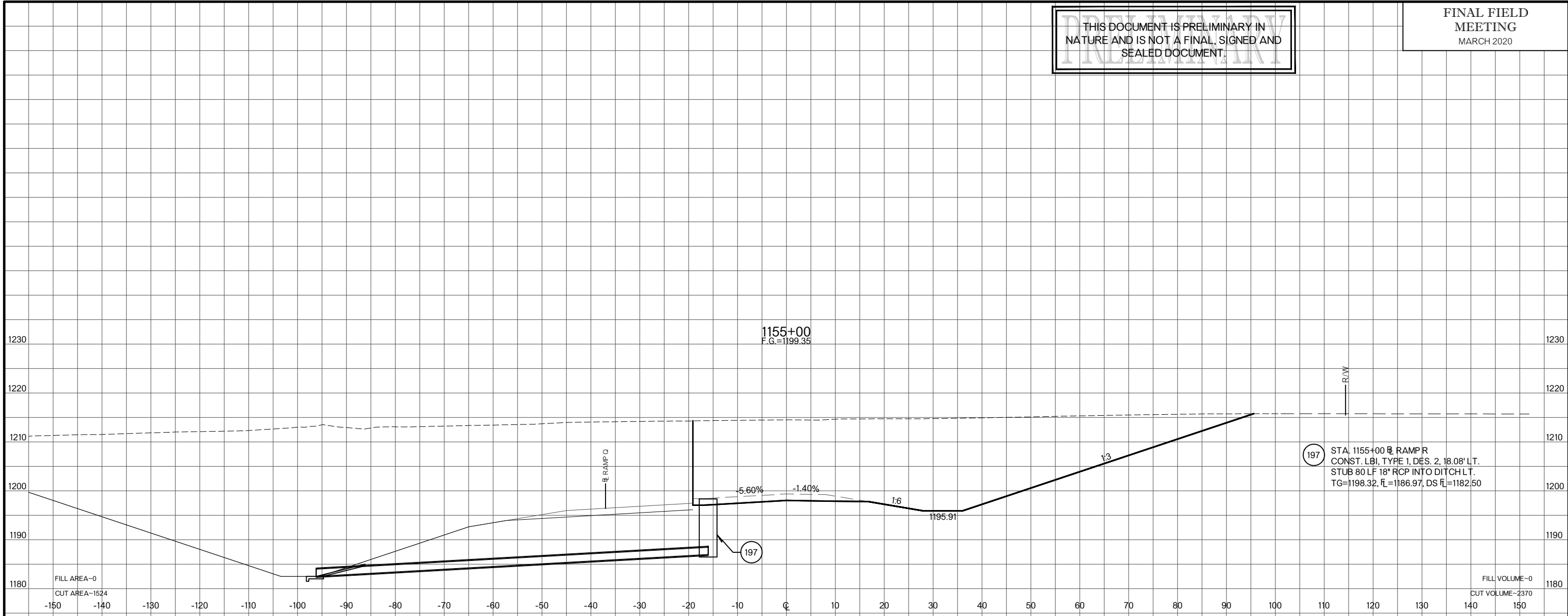
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP R

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

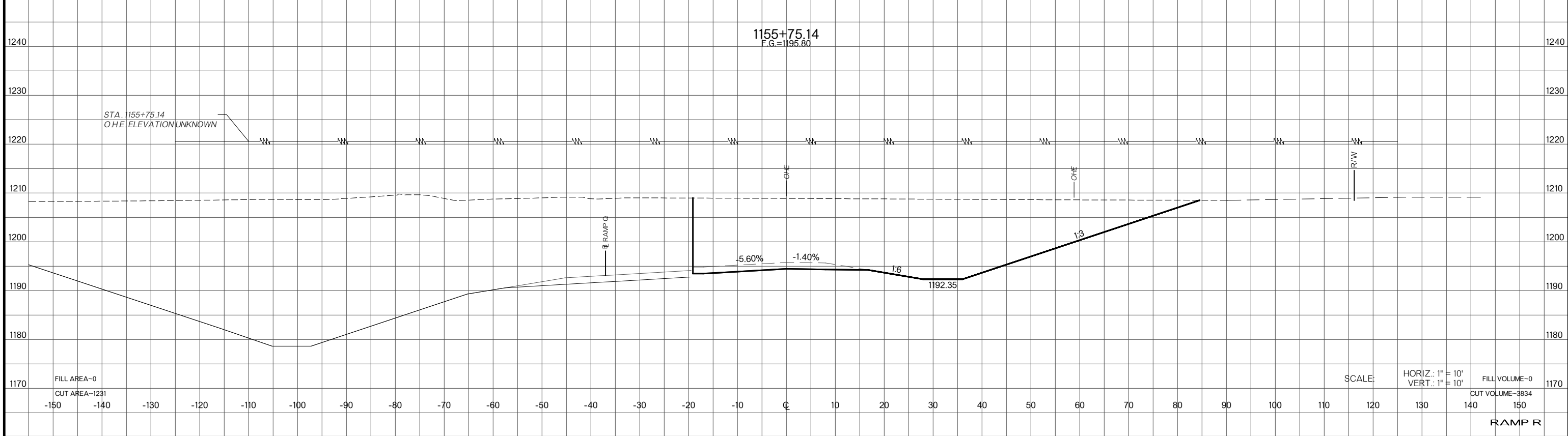
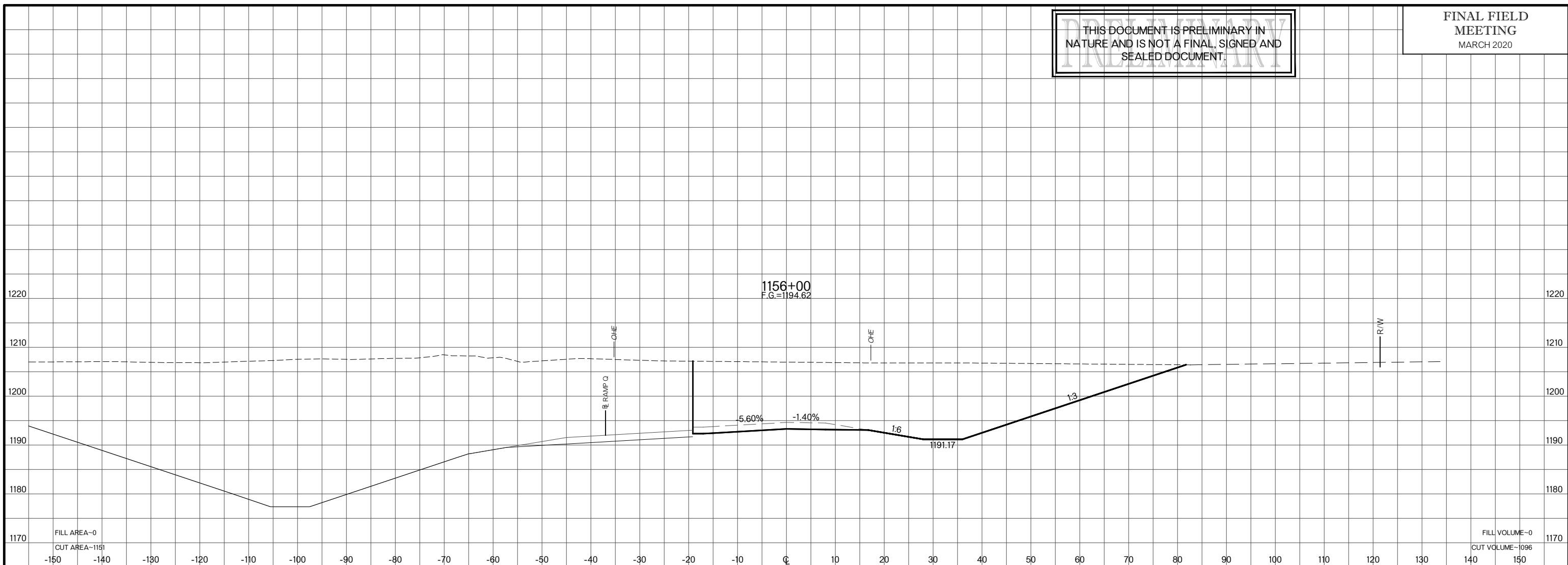
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

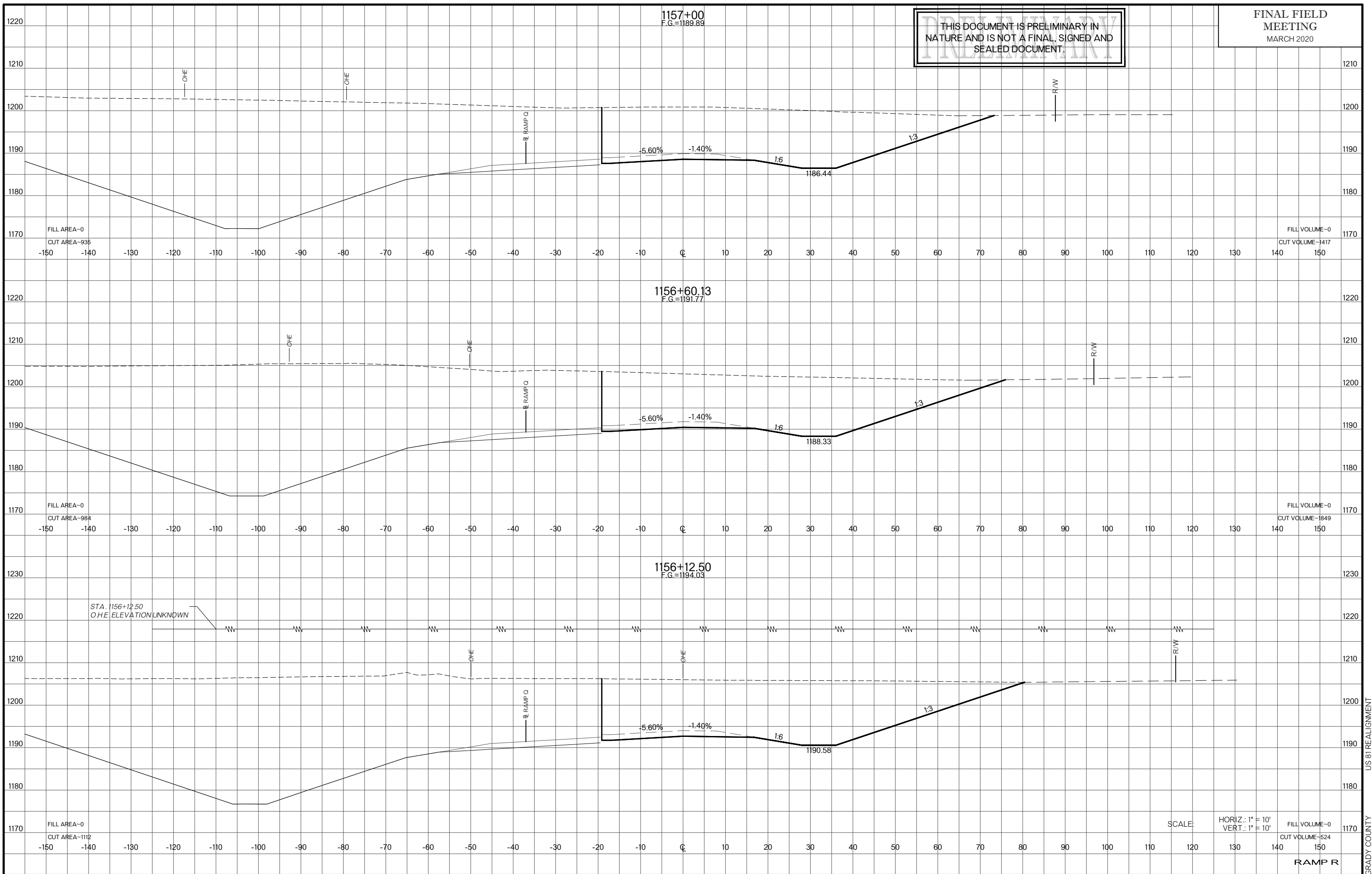
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

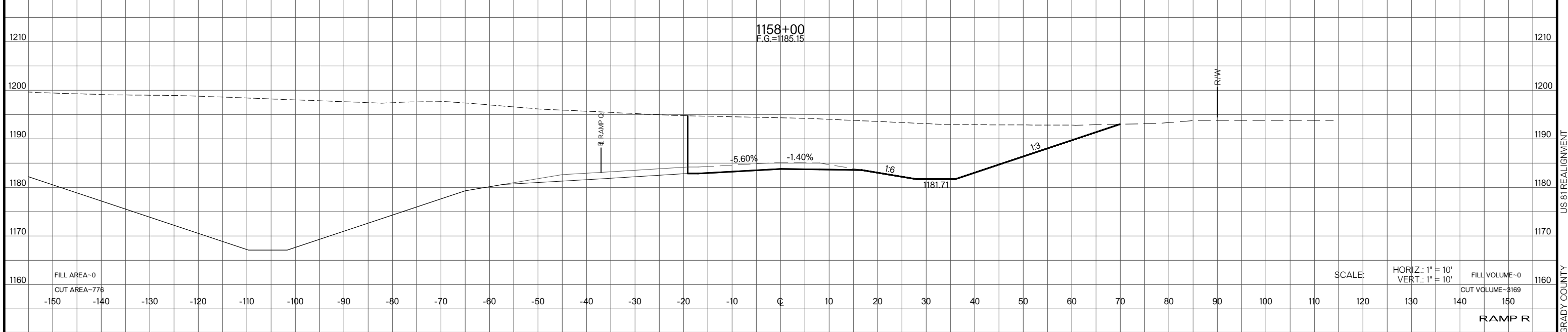
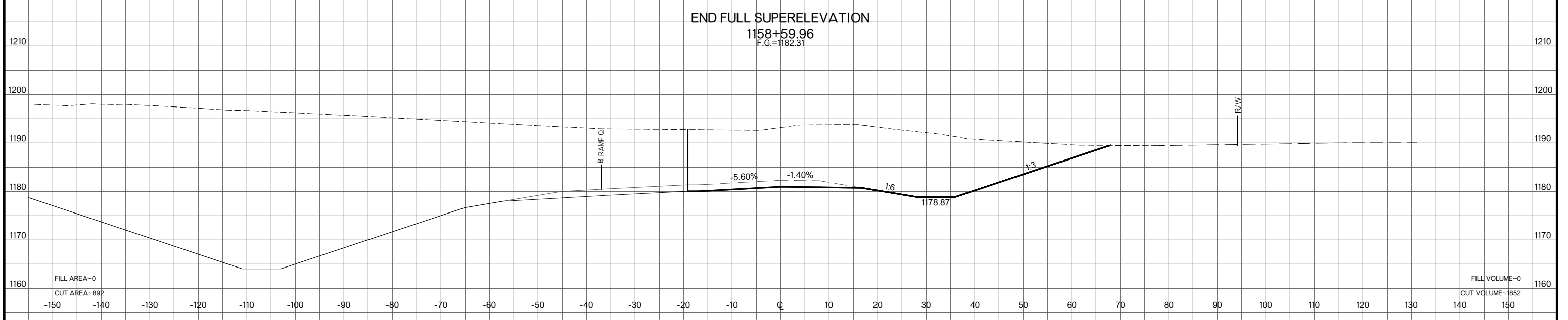
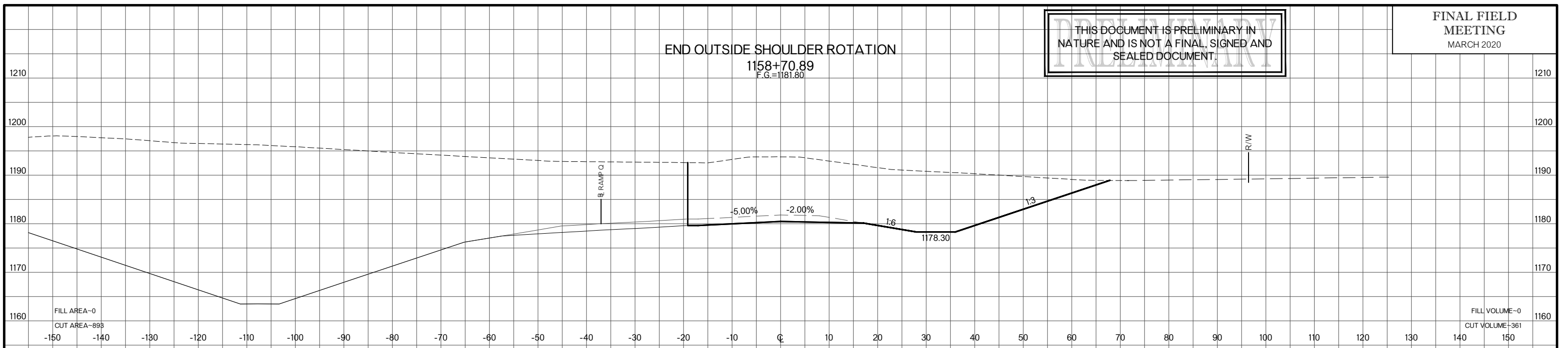


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP R

GRADY COUNTY US 81 REALIGNMENT

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

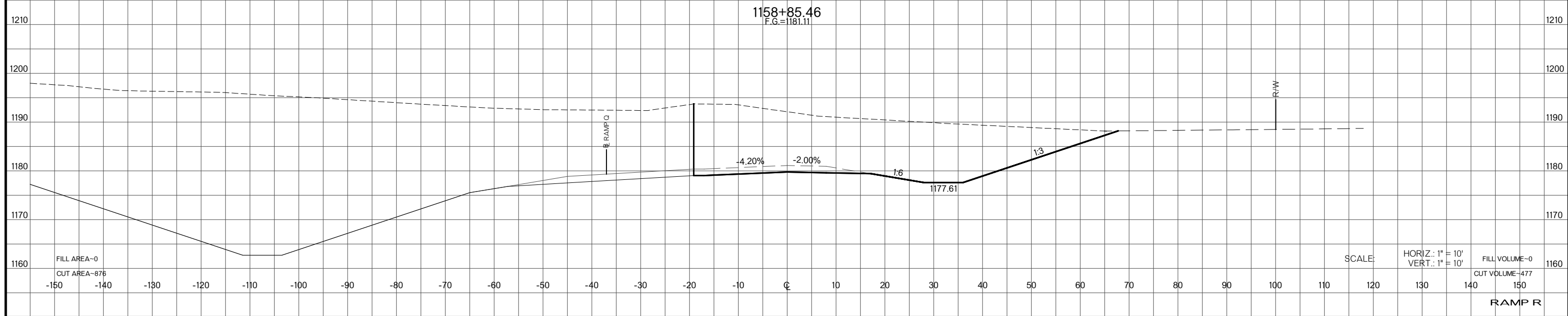
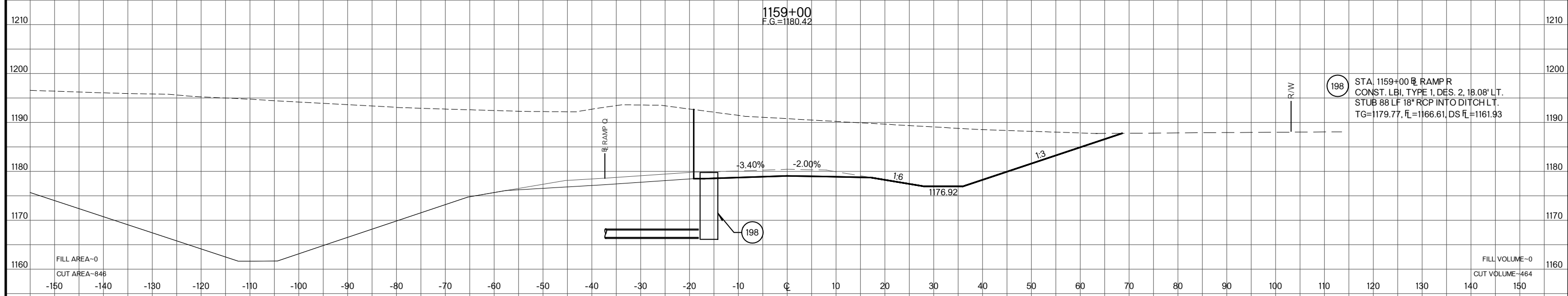
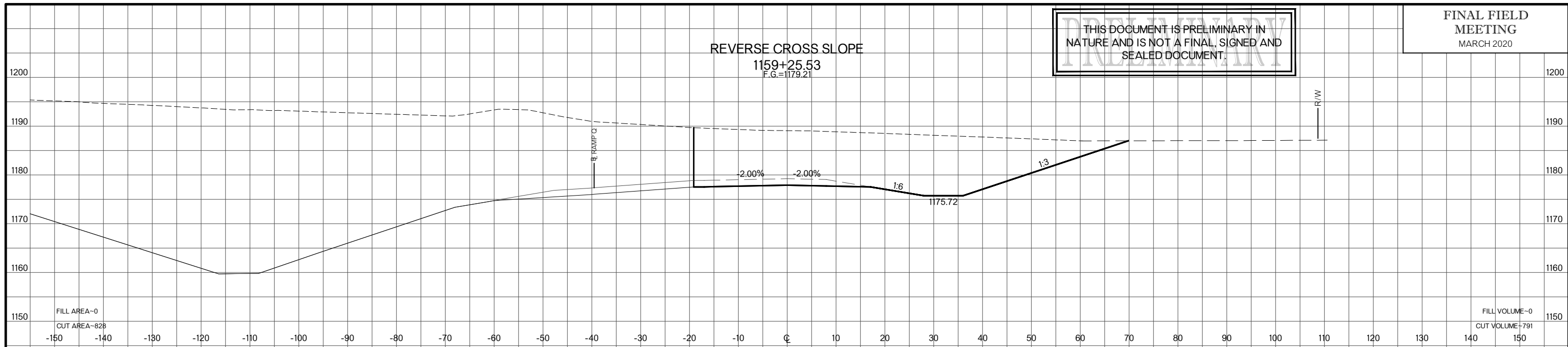


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



198 STA. 1159+00 RAMP R
CONST. LBI, TYPE 1, DES. 2, 18.08' LT.
STUB 88 LF 18" RCP INTO DITCH LT.
TG=1179.77, f_L=1166.61, DS f_L=1161.93

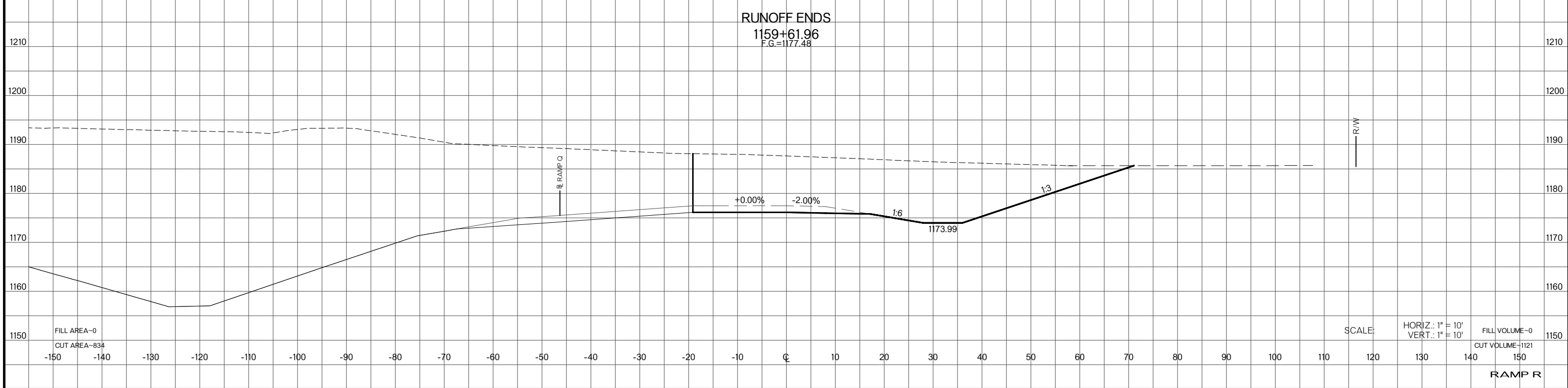
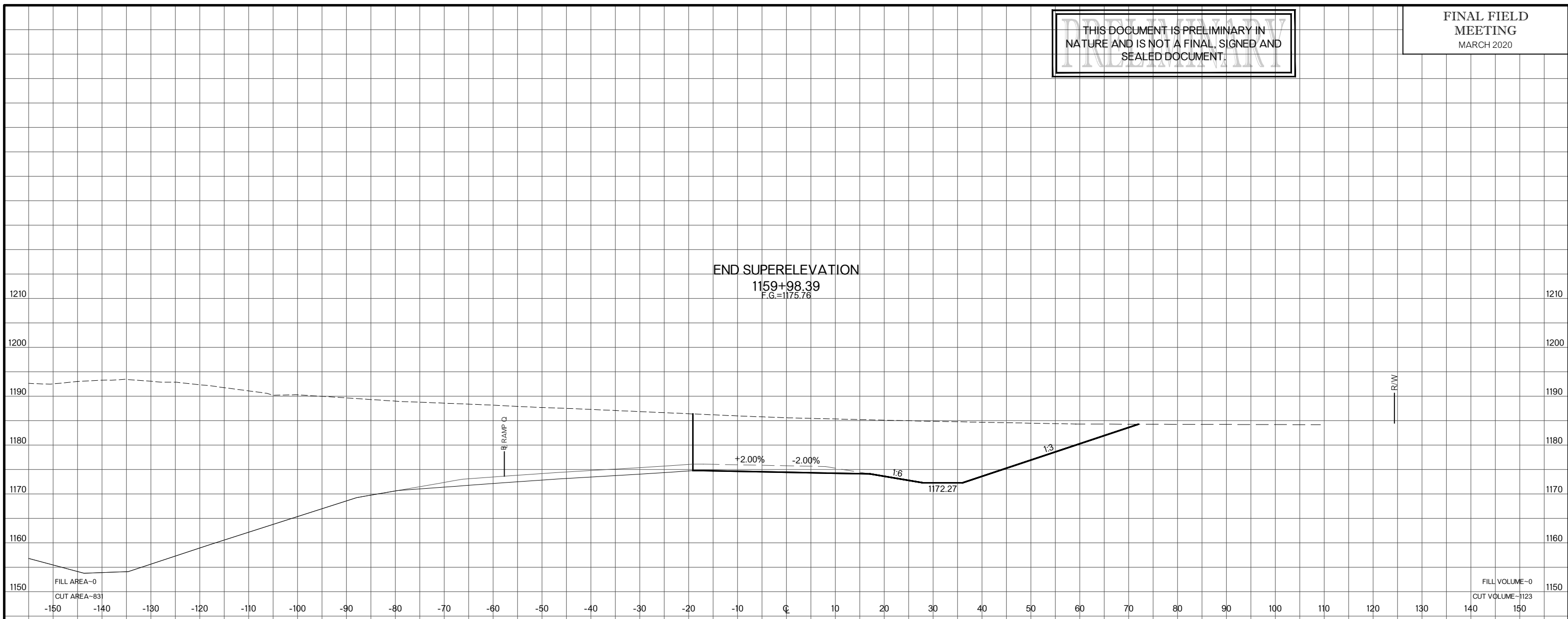
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP R

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



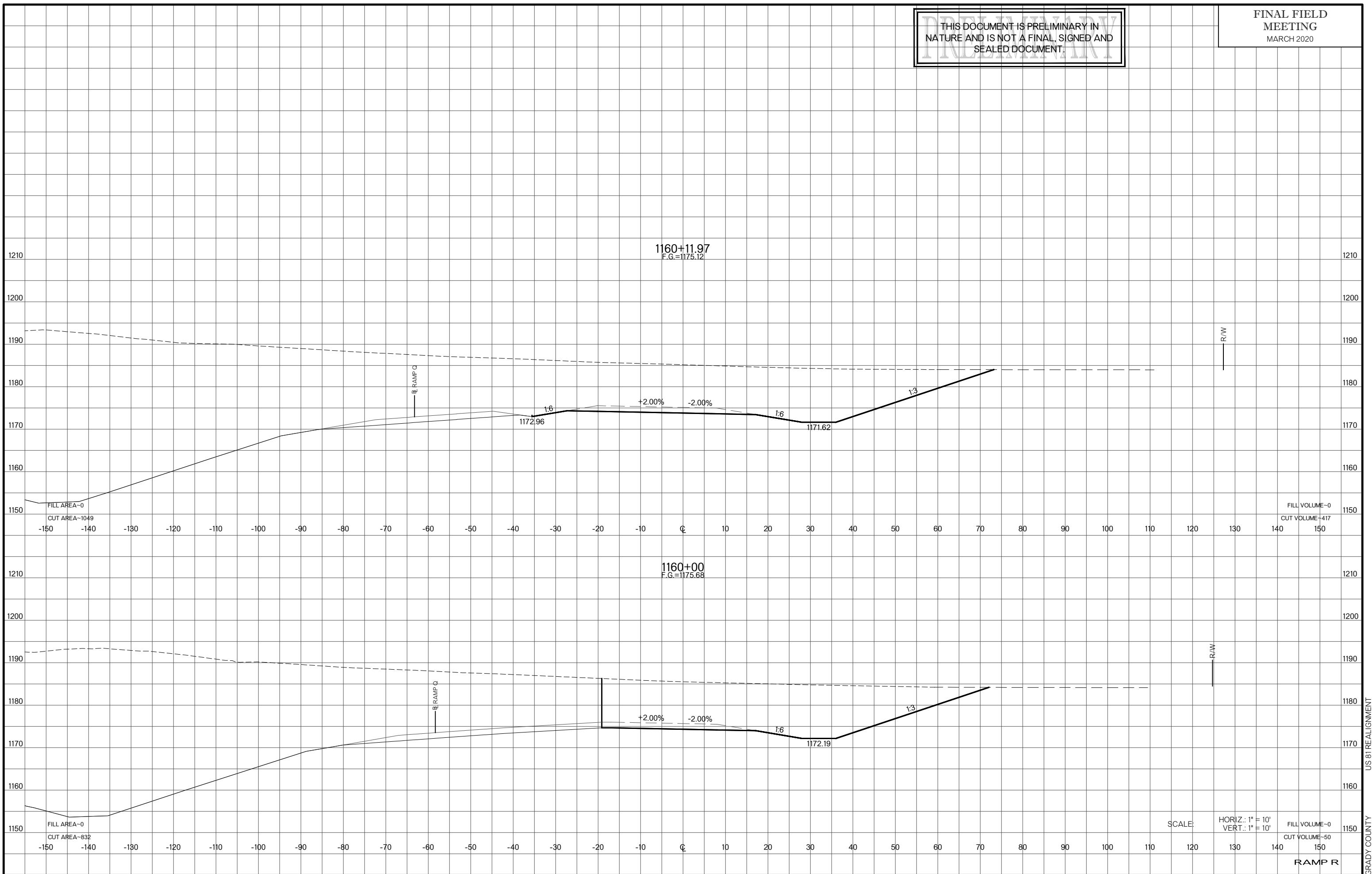
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP R

US 81 REALIGNMENT
GRADY COUNTY

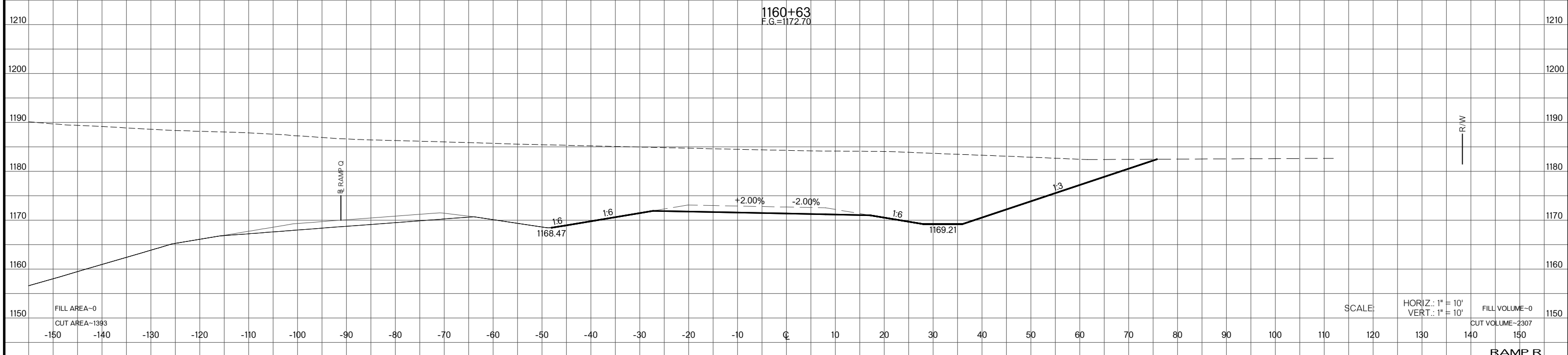
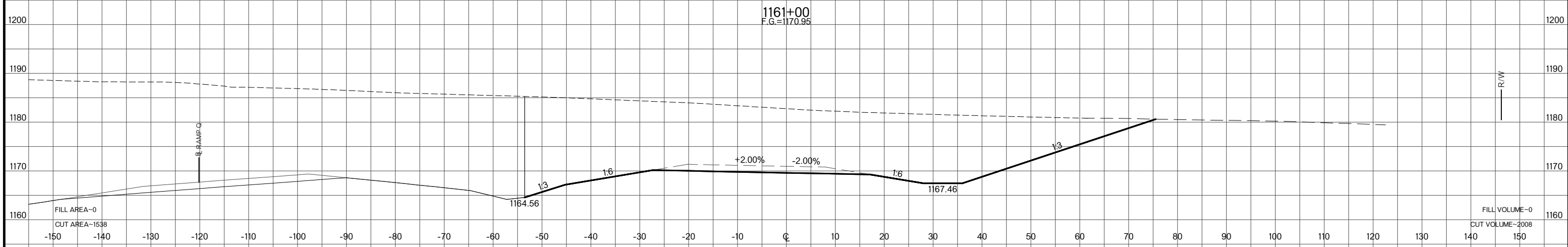
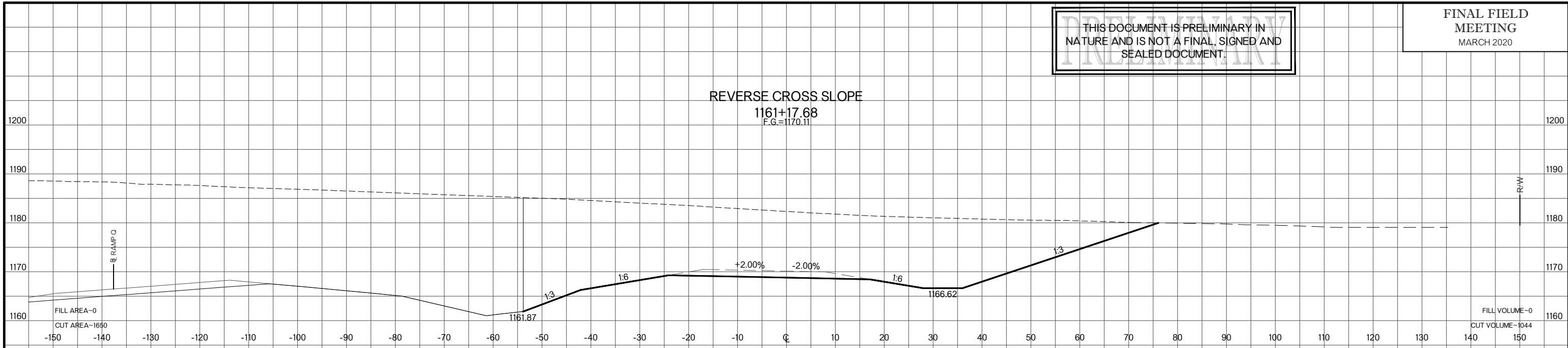
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

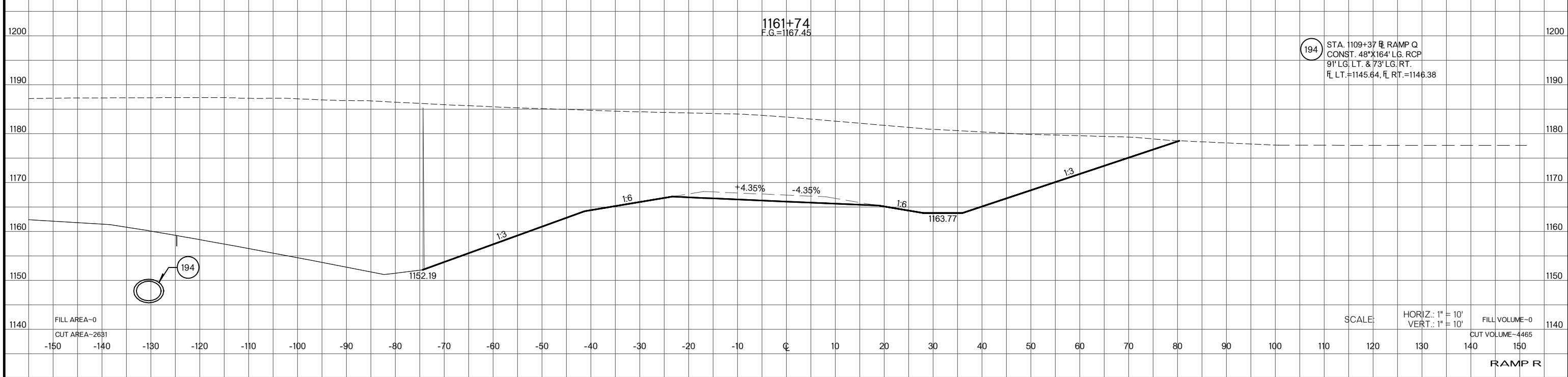
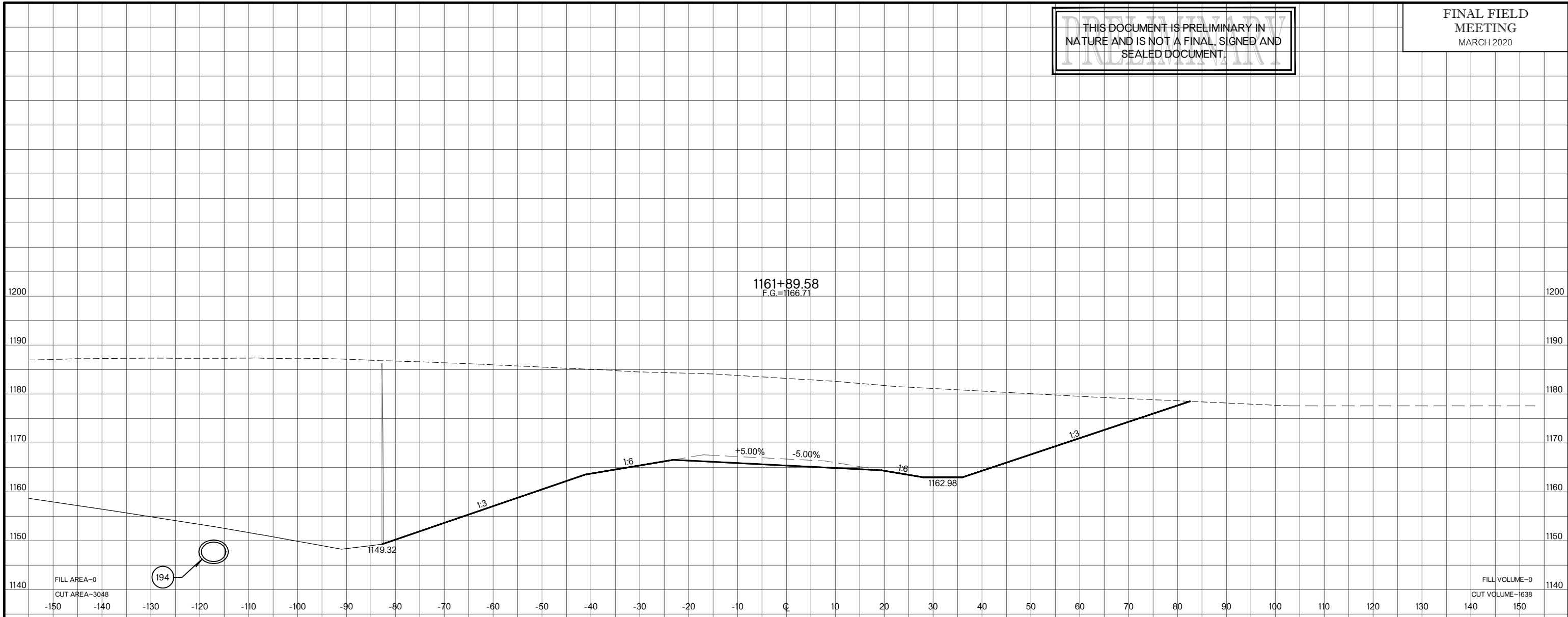
FILL VOLUME=0
CUT VOLUME=2307

RAMP R

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

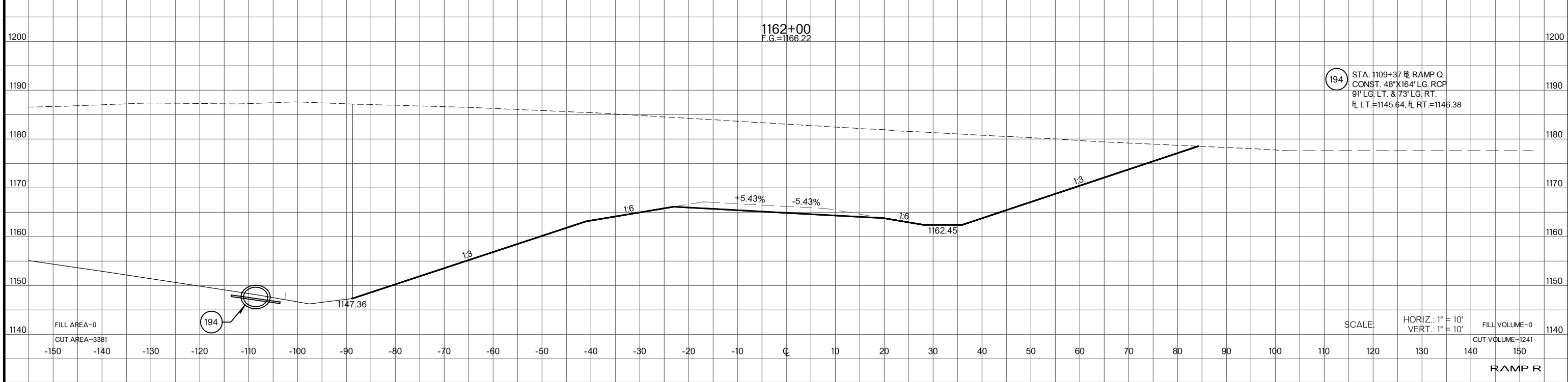
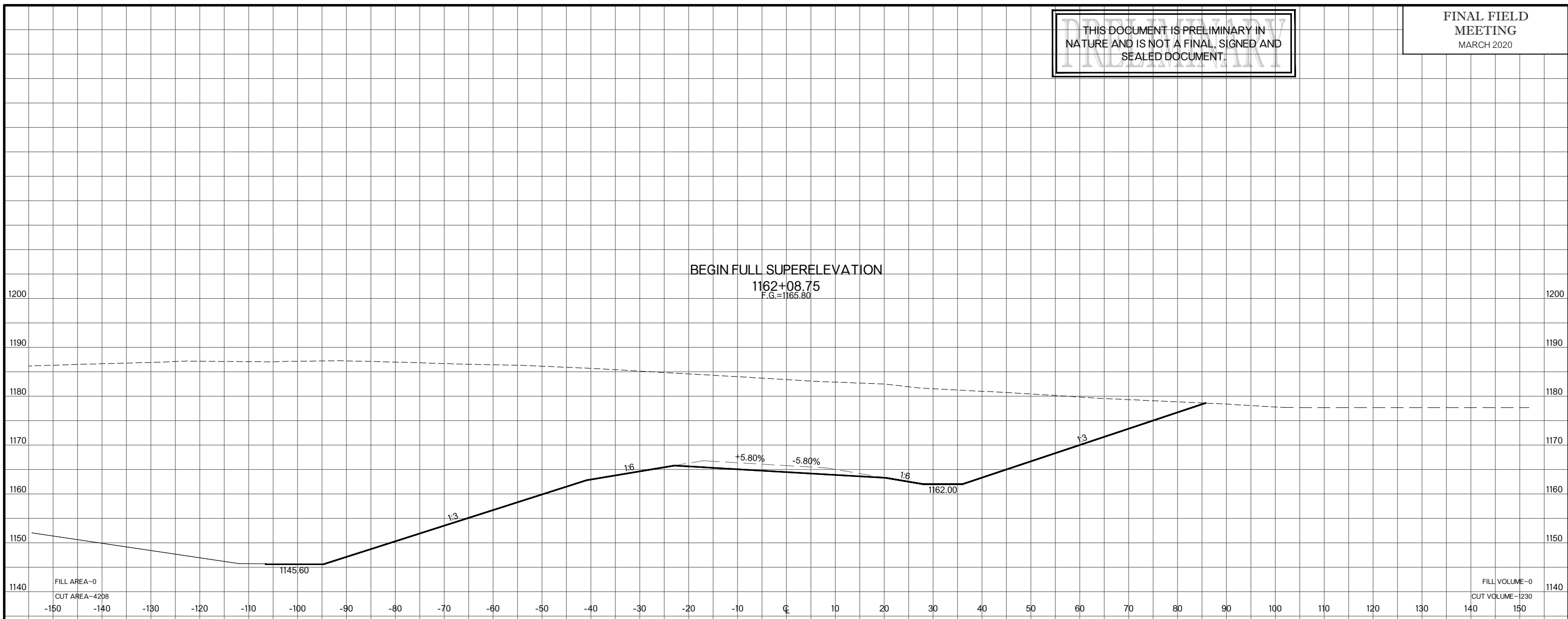
FILL VOLUME-0
CUT VOLUME-4465

RAMP R

GRADY COUNTY
US 81 REALIGNMENT

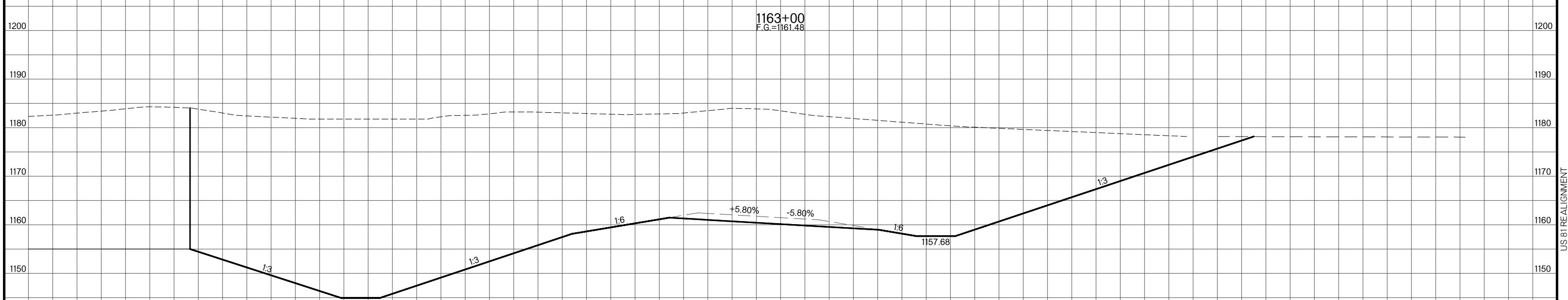
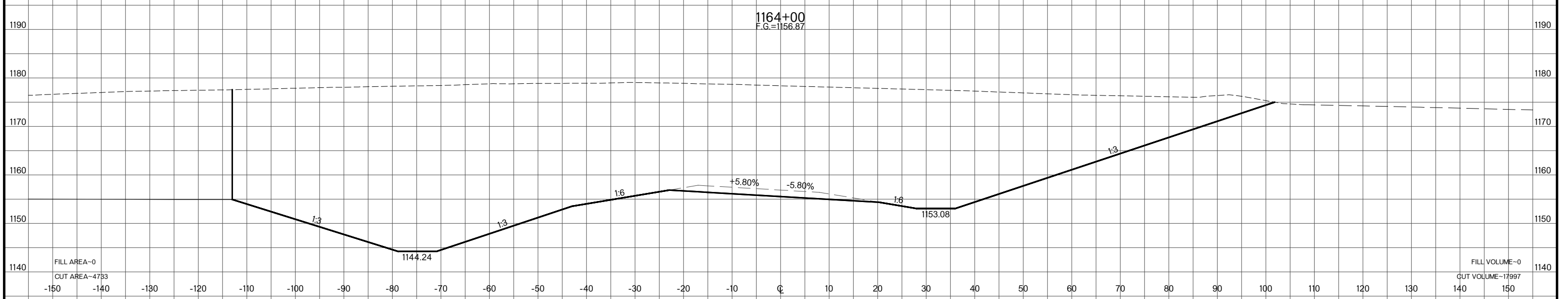
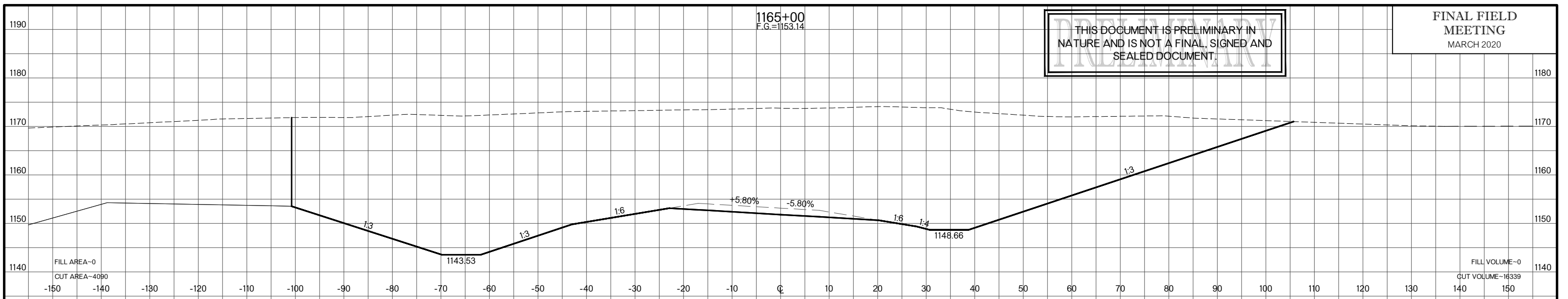
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



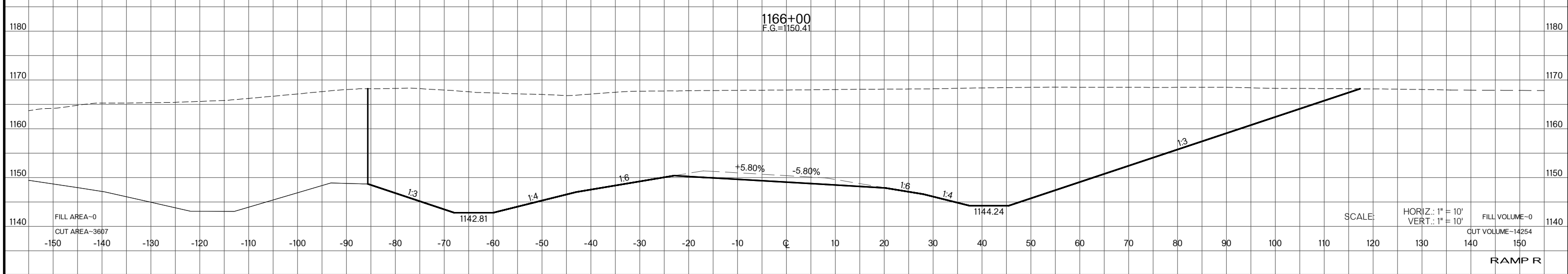
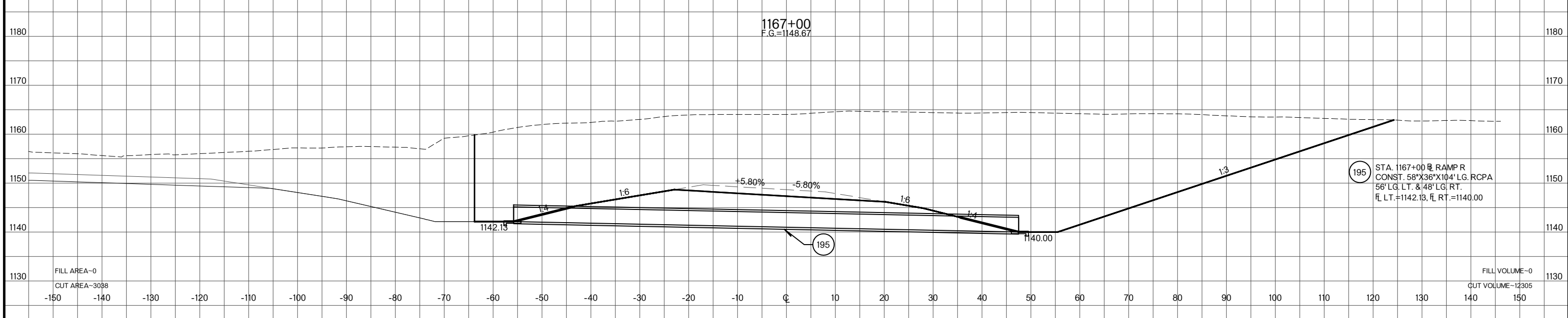
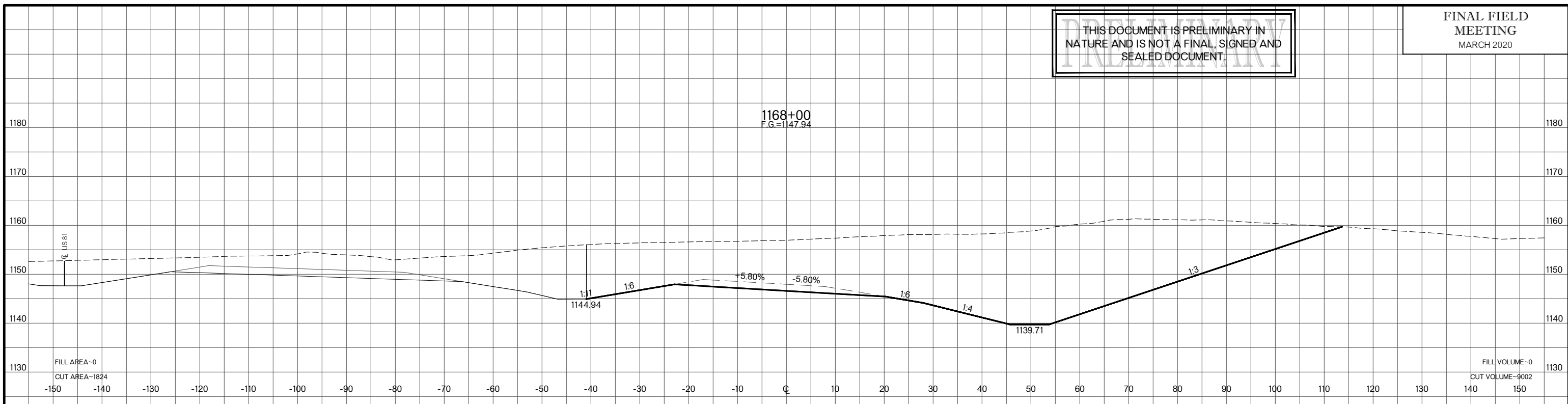
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP R

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



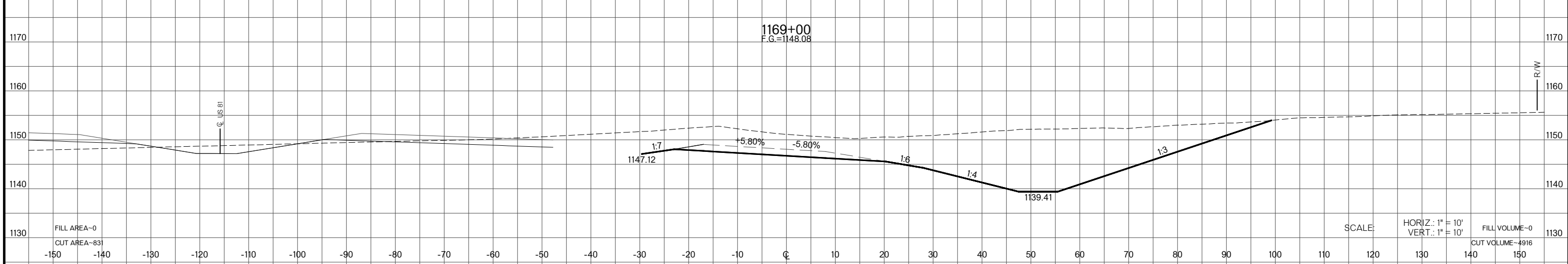
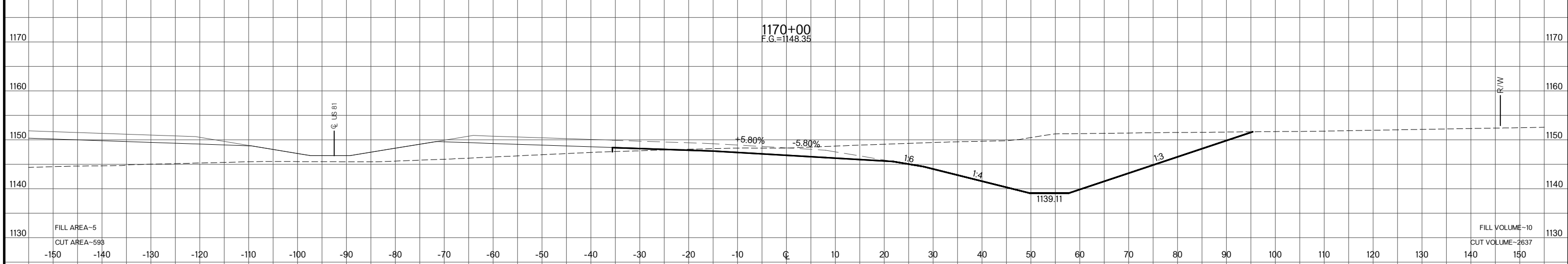
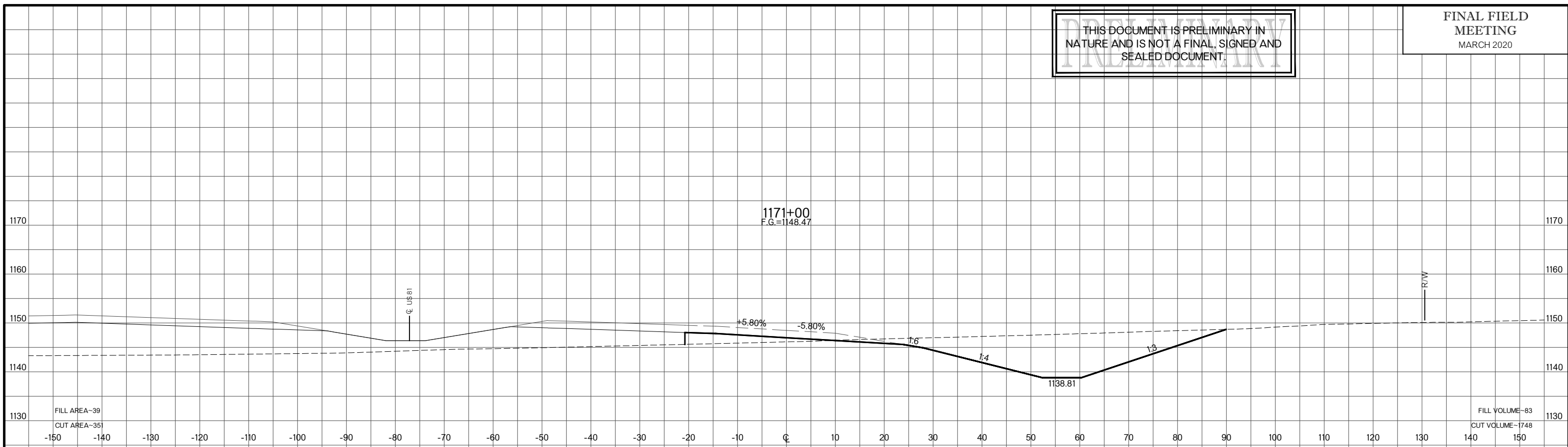
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

State Job No. 24428(04) Sheet No. X476

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



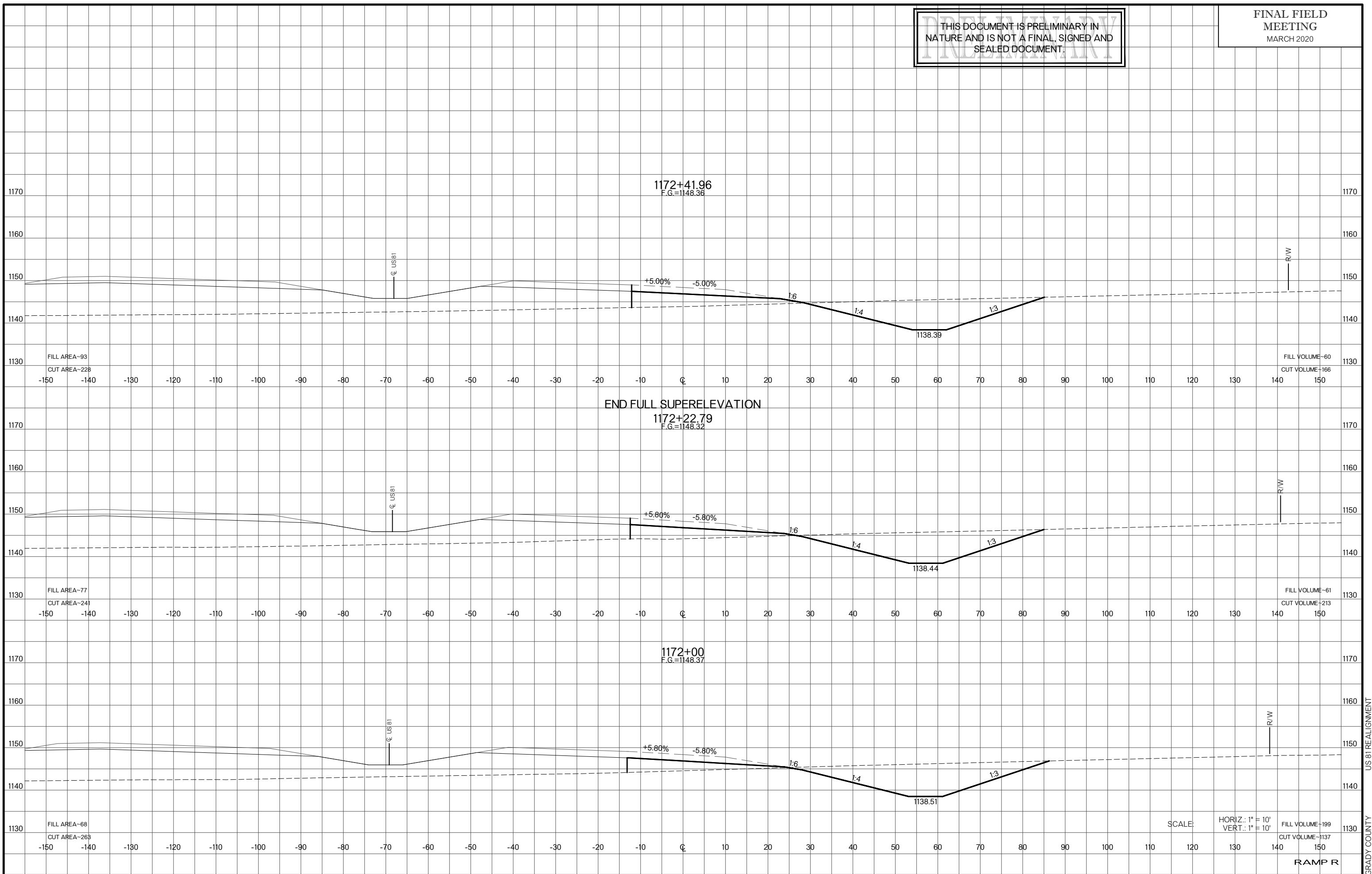
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP R

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



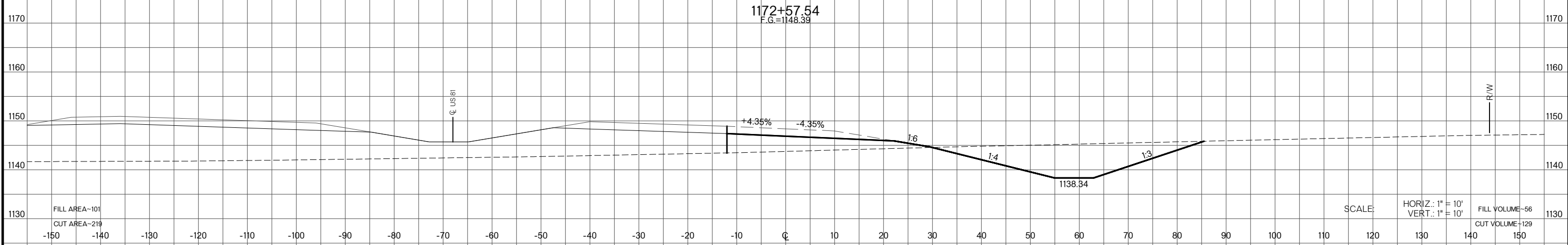
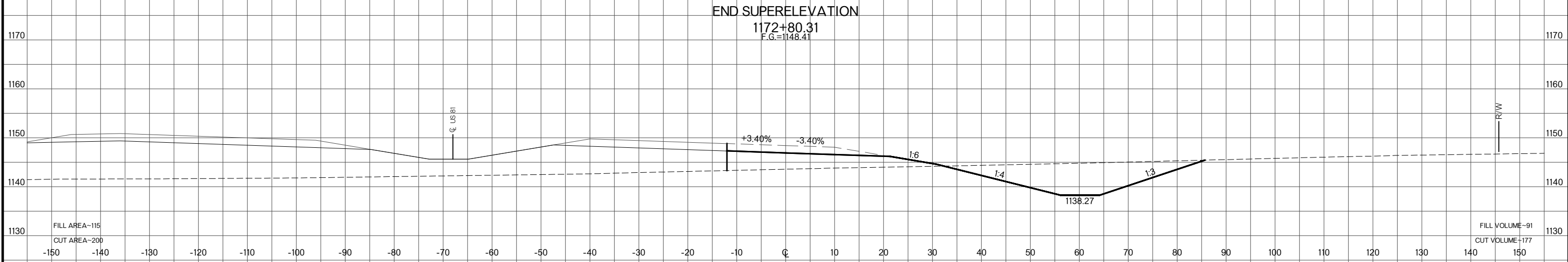
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



END SUPERELEVATION

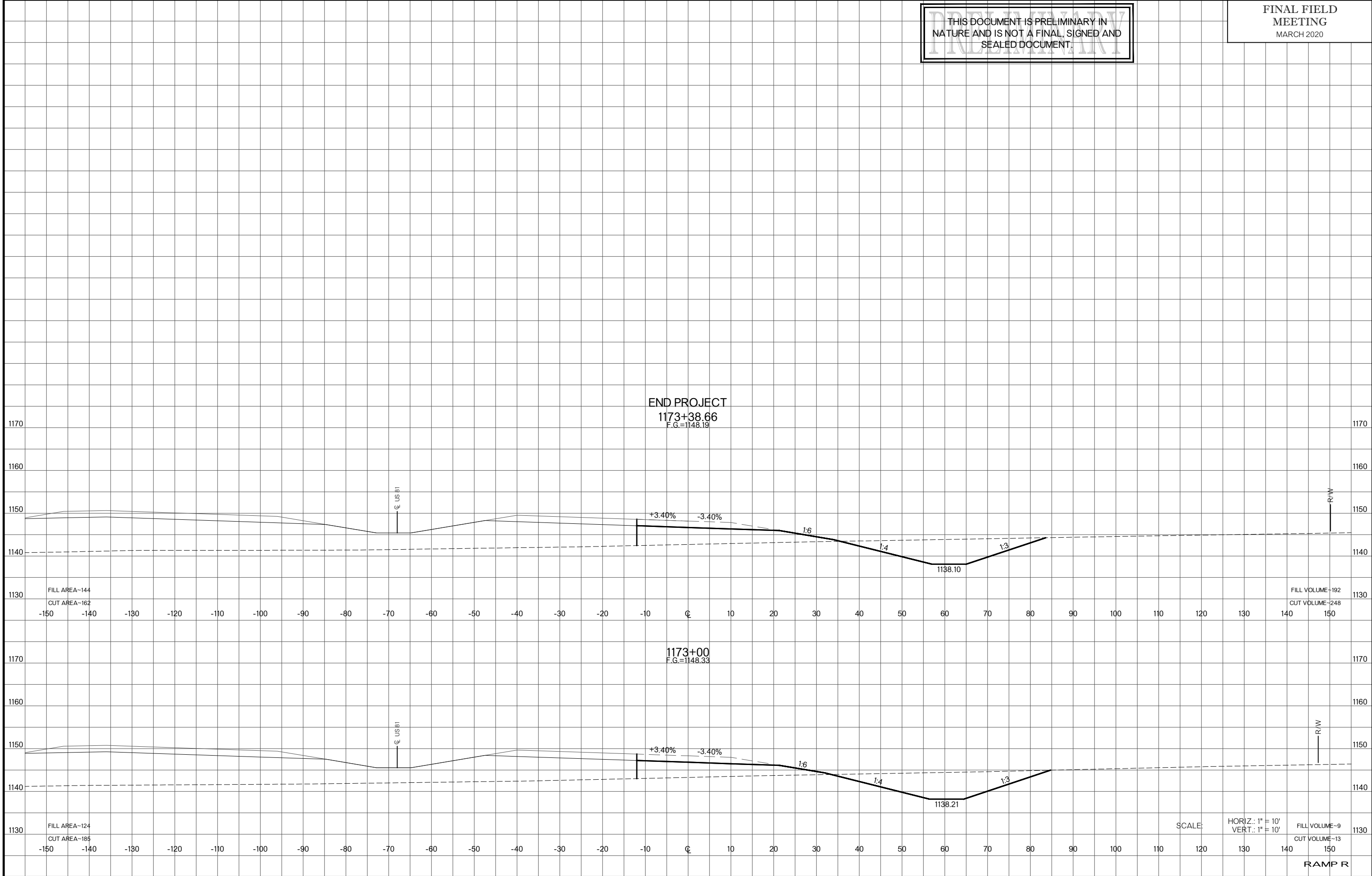


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP R

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



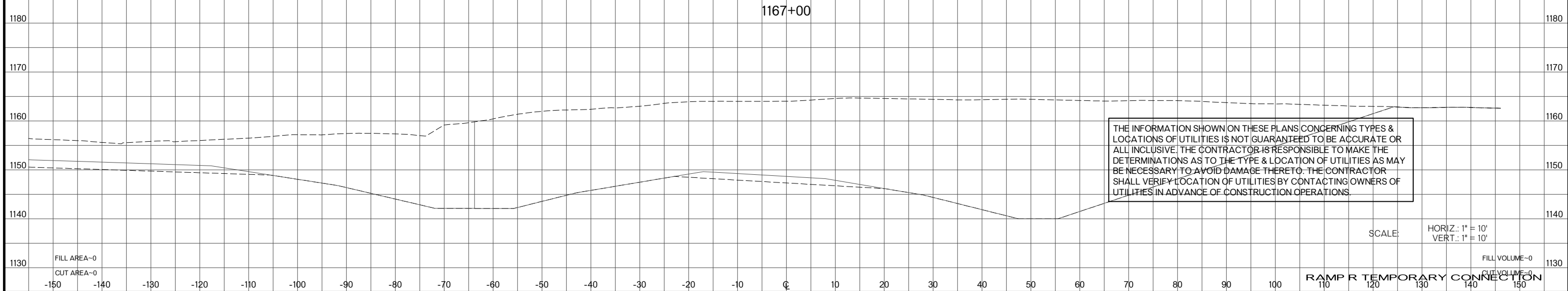
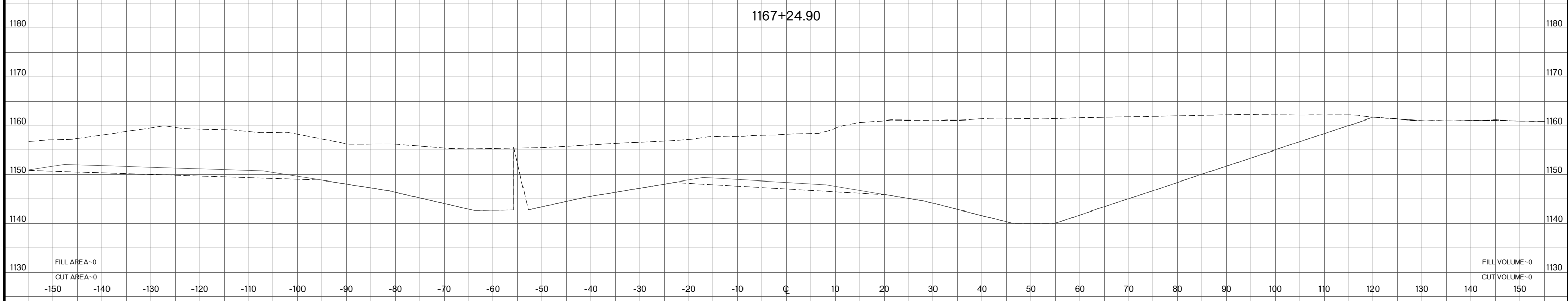
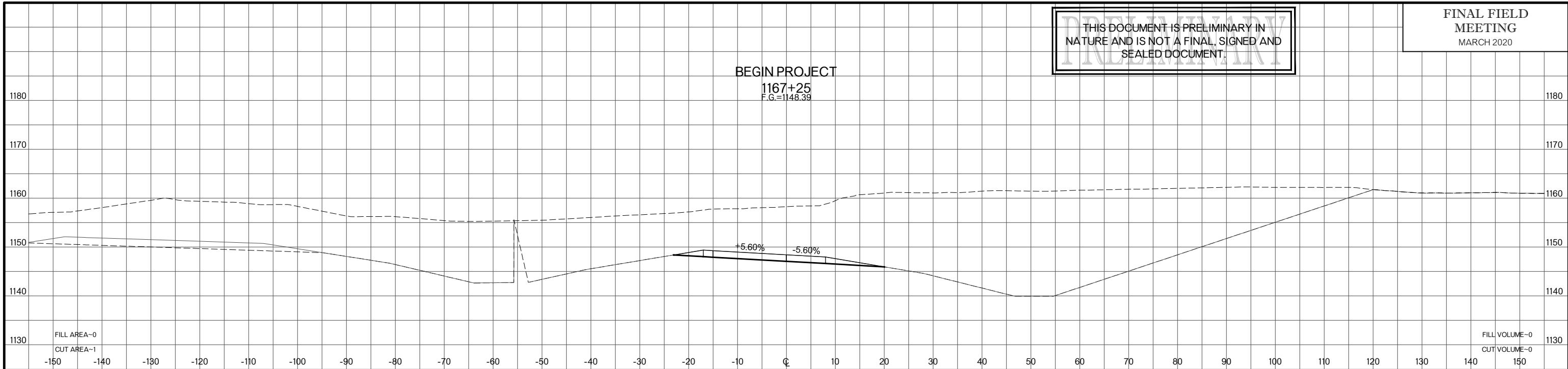
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP R

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

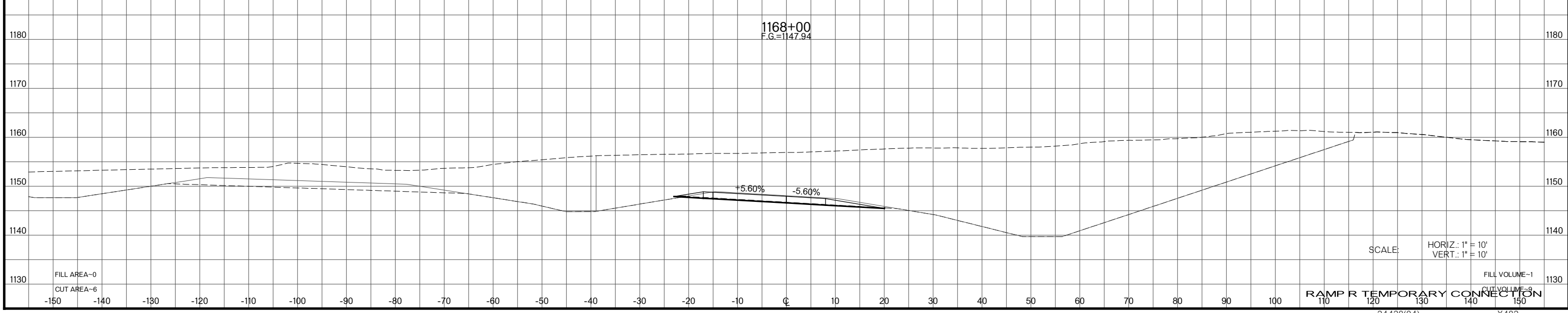
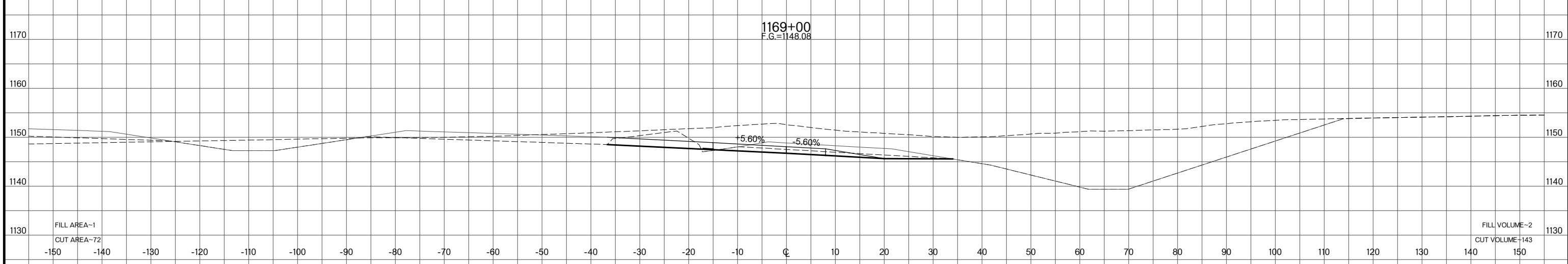
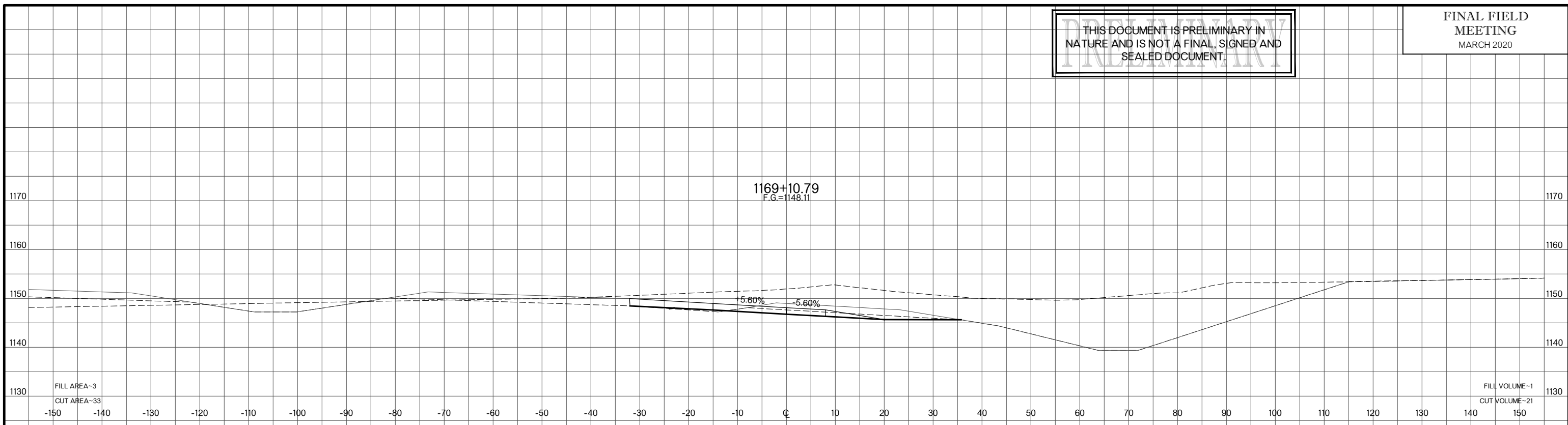
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP R TEMPORARY CONNECTION

US 81 REALIGNMENT
GRADY COUNTY

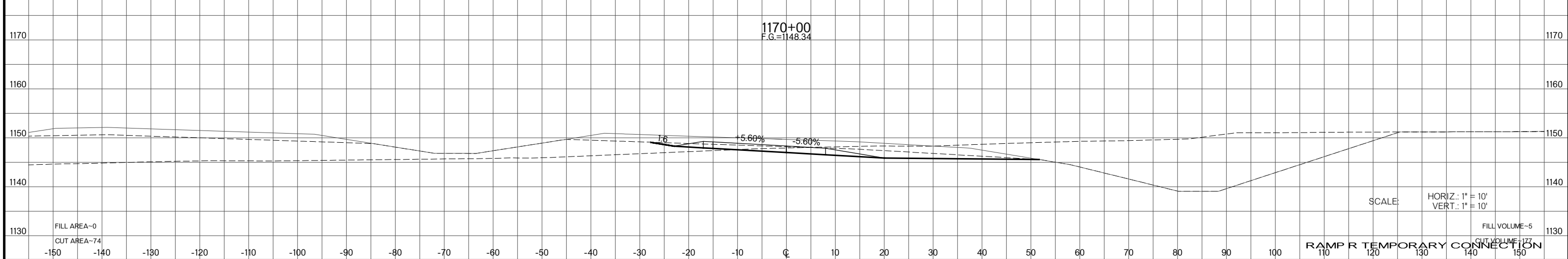
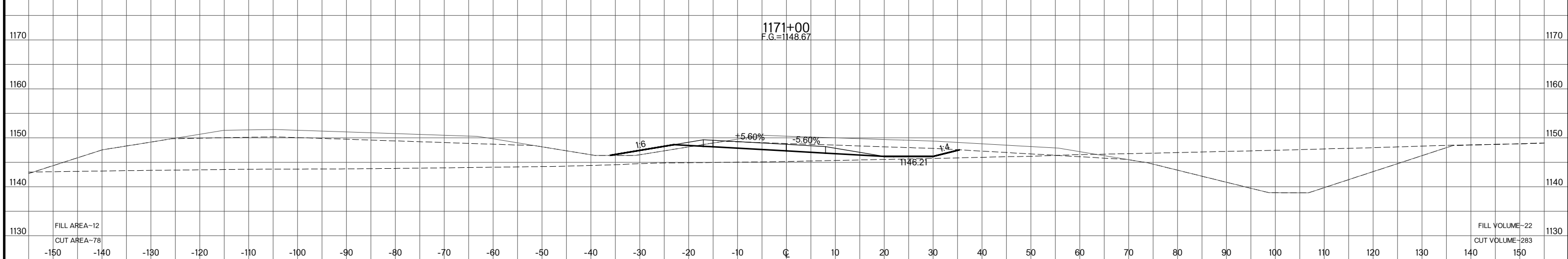
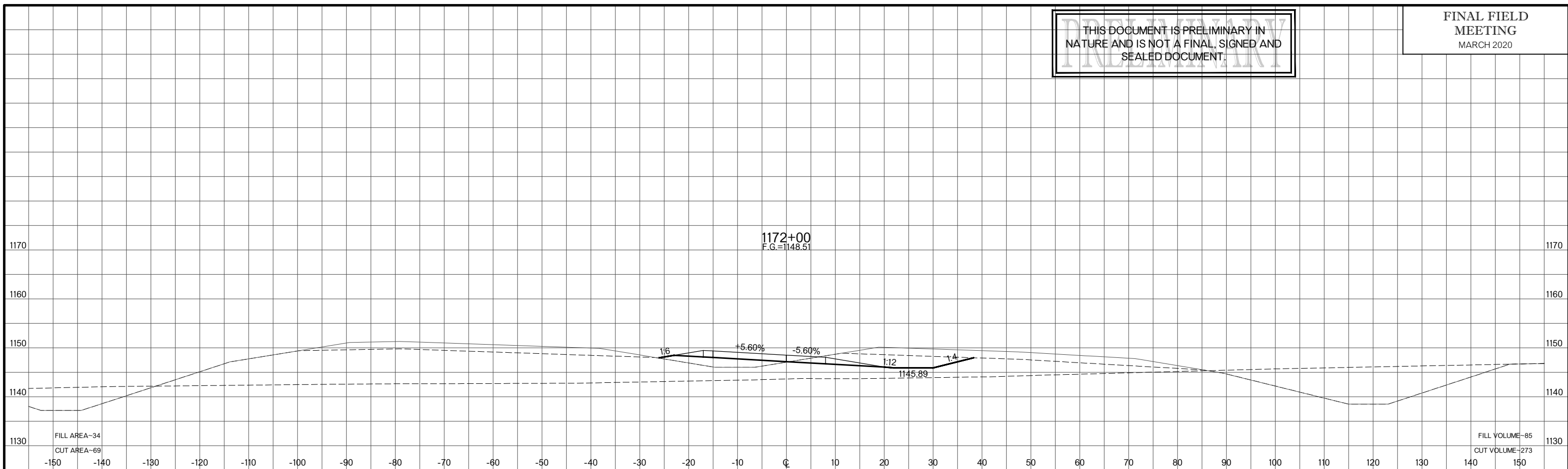
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

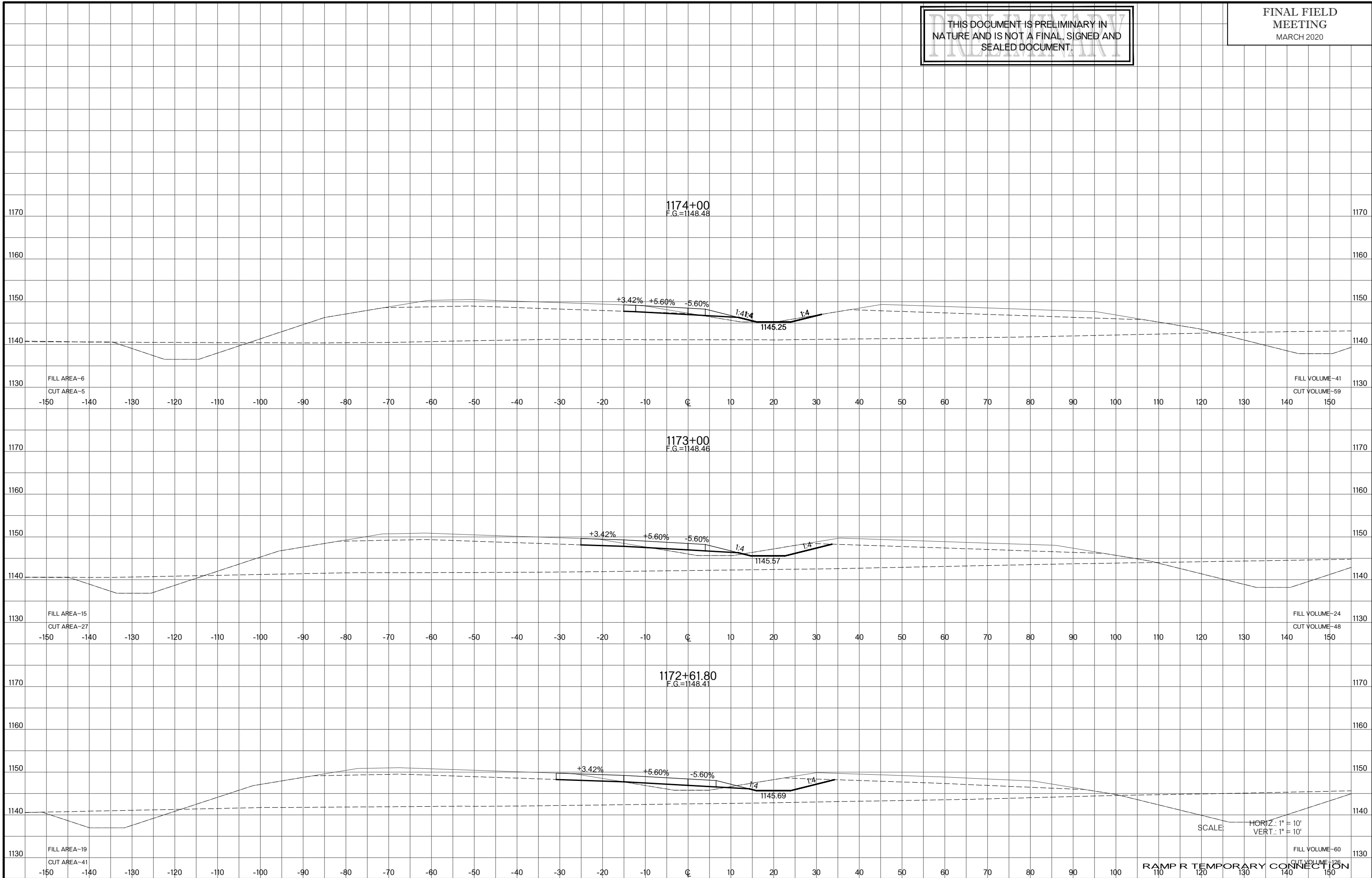
RAMP R TEMPORARY CONNECTION

State Job No. 24428(04) Sheet No. X483

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

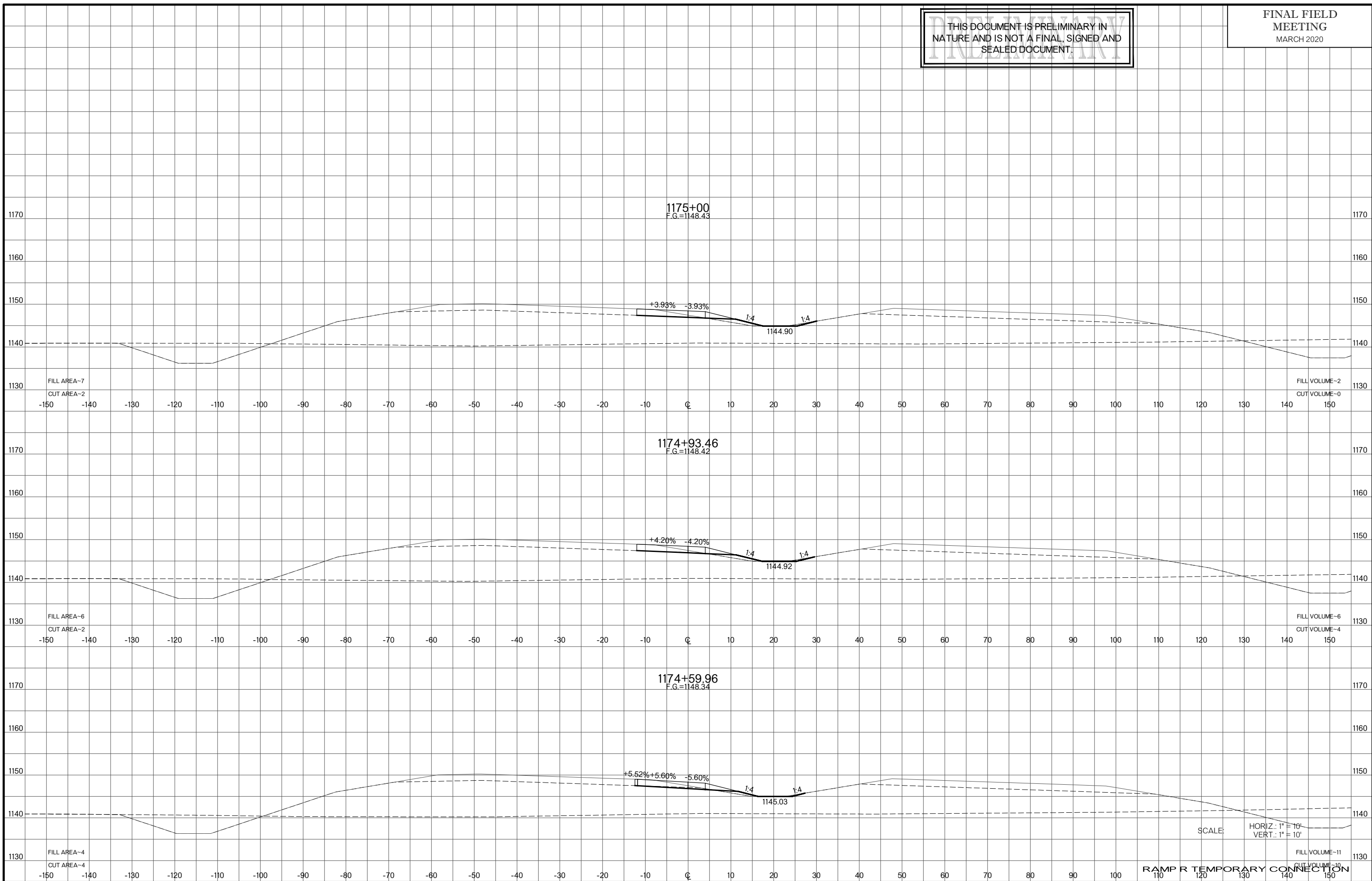
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

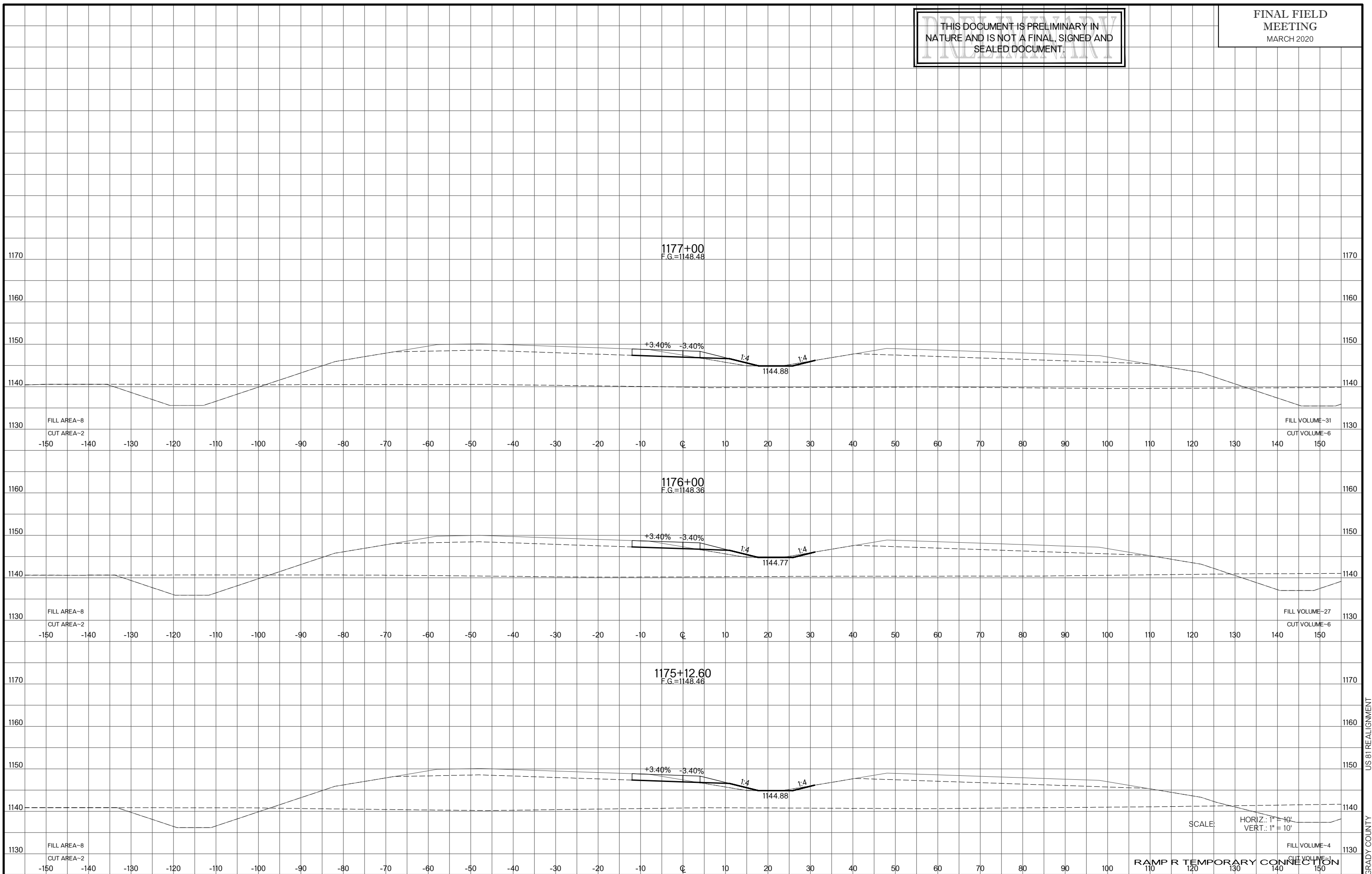
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

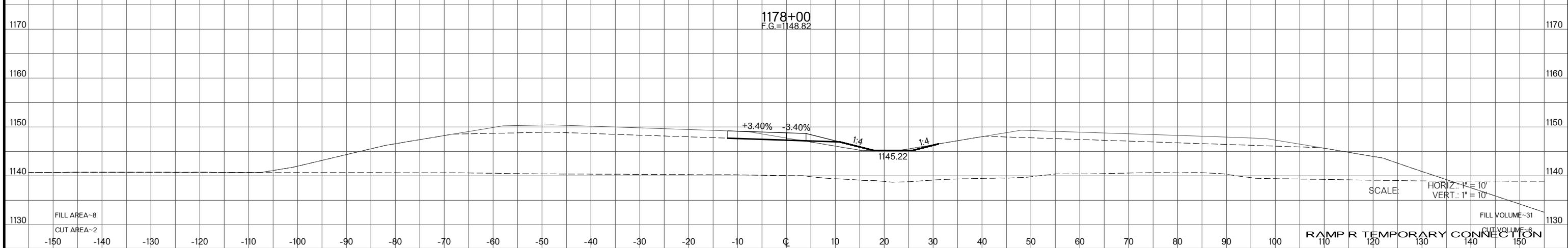
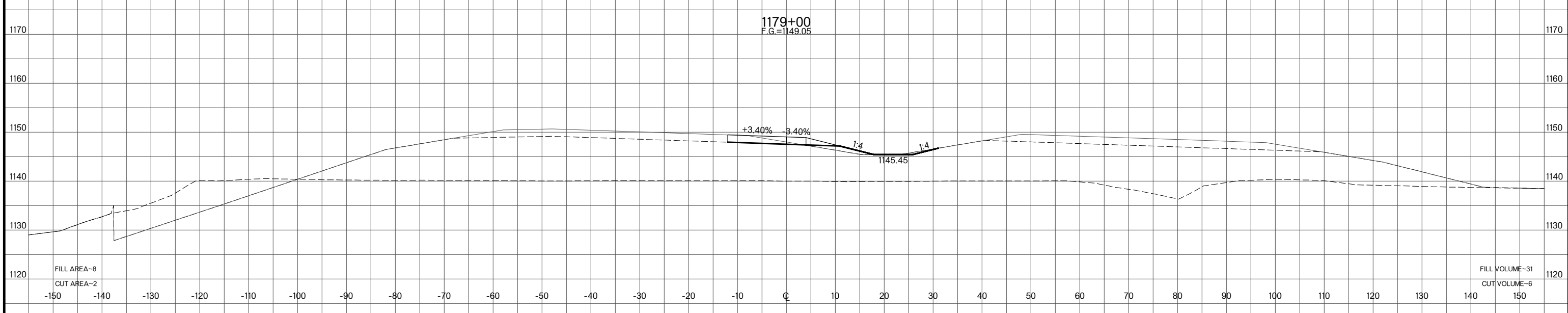
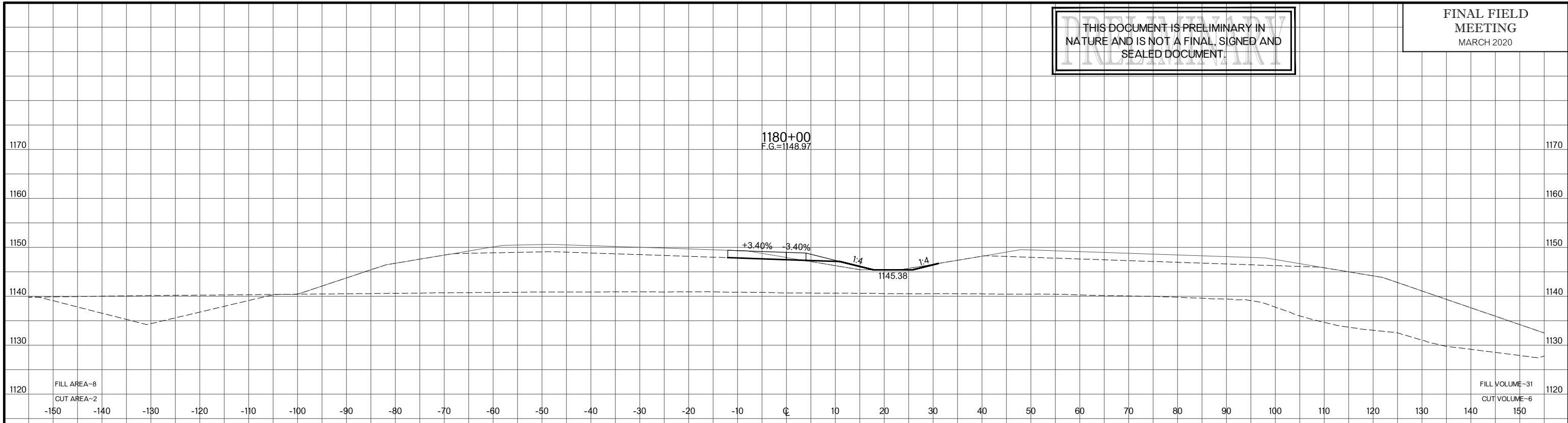
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

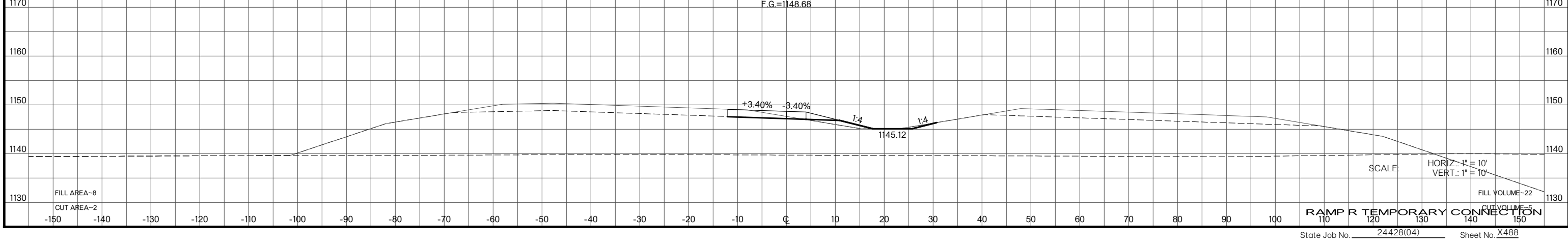
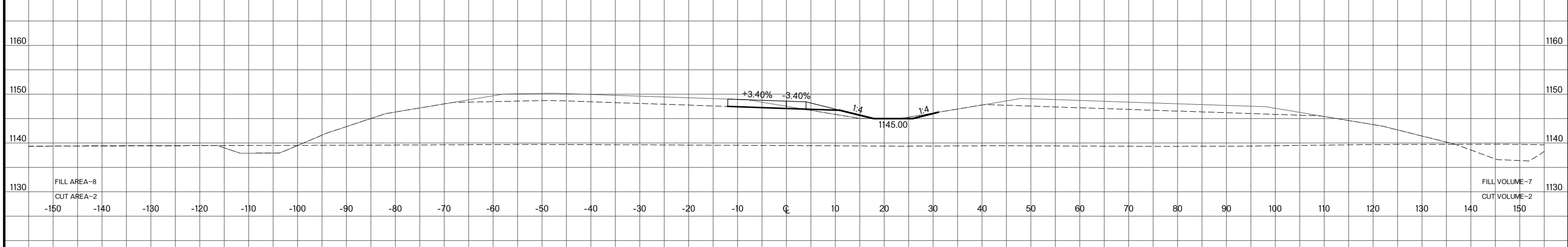
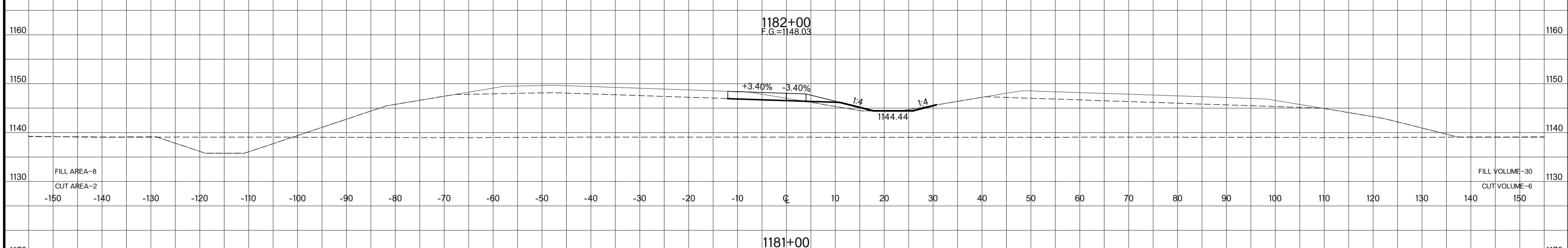
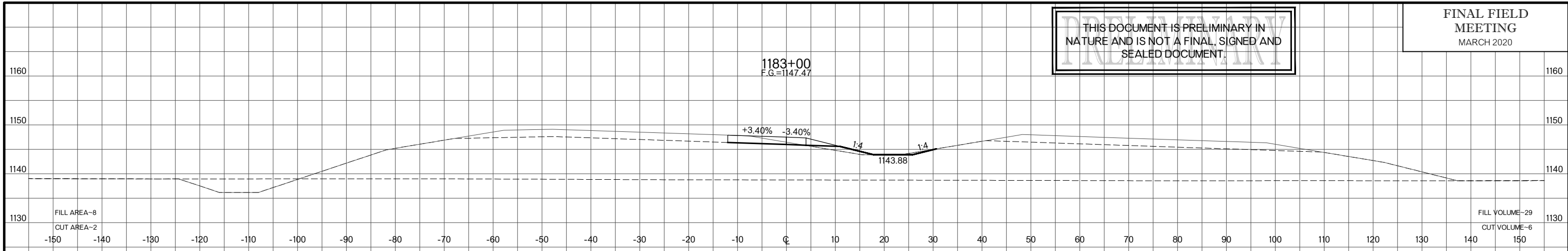
RAMP R TEMPORARY CONNECTION

State Job No. 24428(04) Sheet No. X487

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

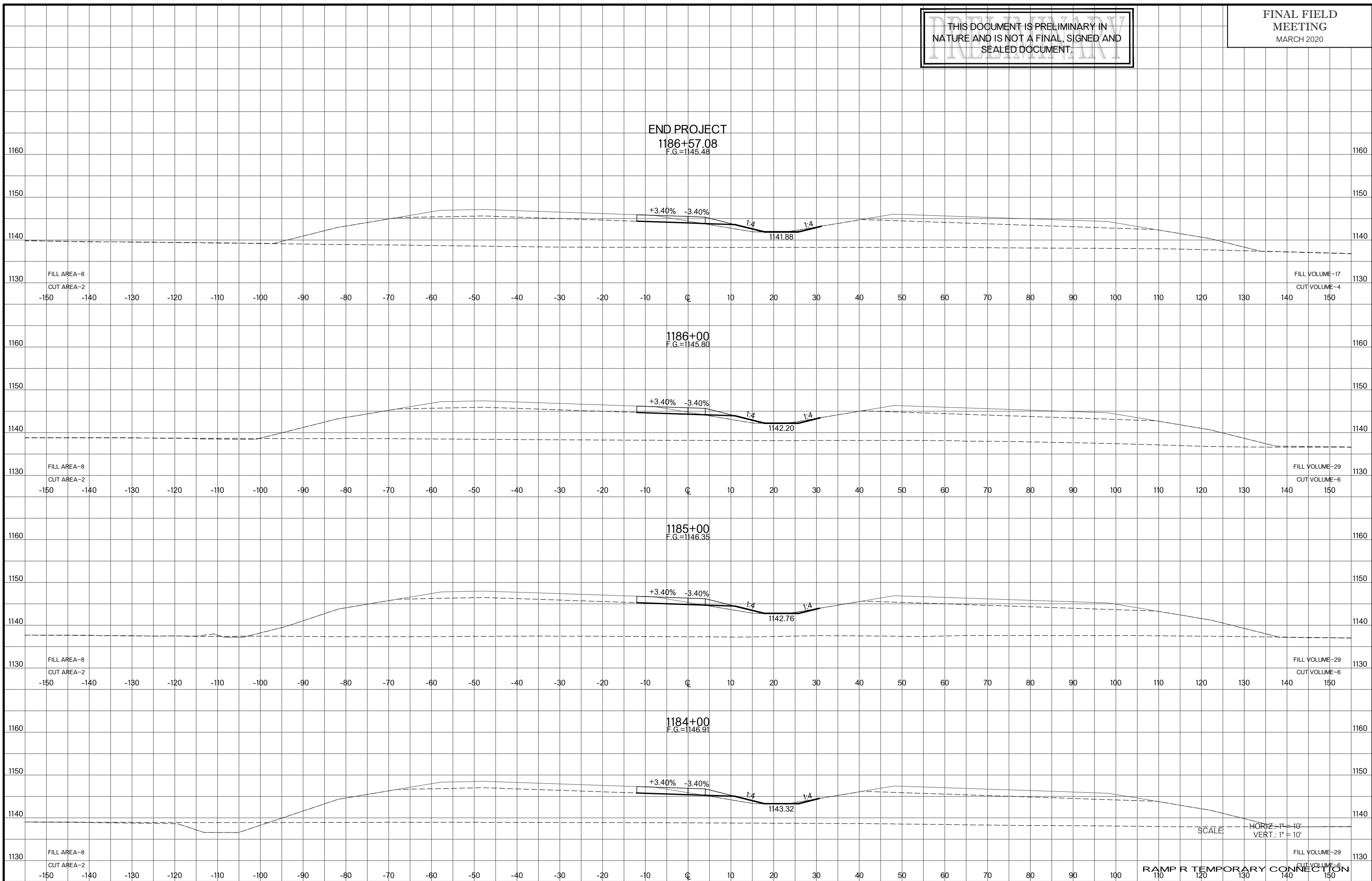
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

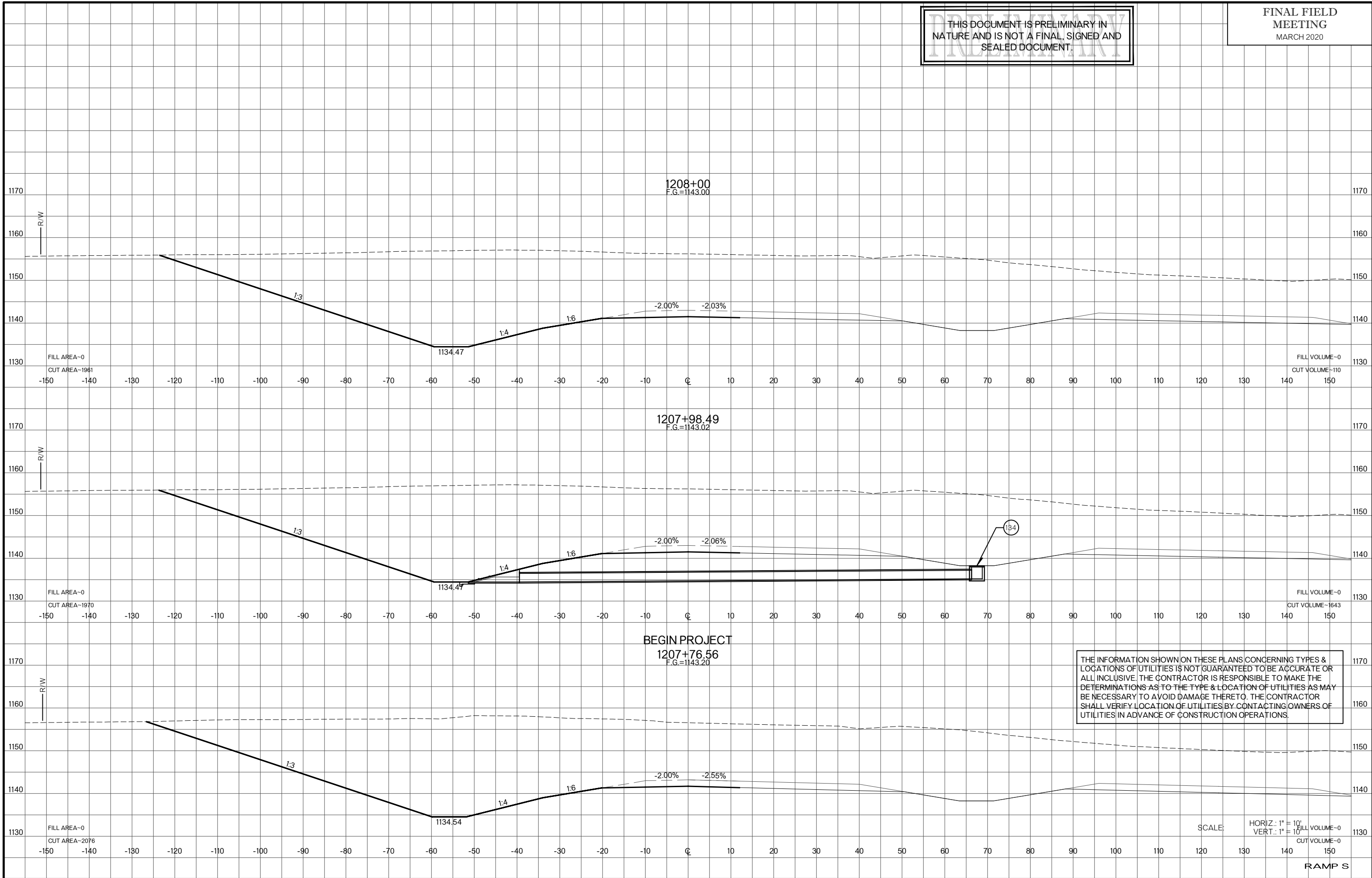
RAMP R TEMPORARY CONNECTION

FILL VOLUME-29
CUT VOLUME-6

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

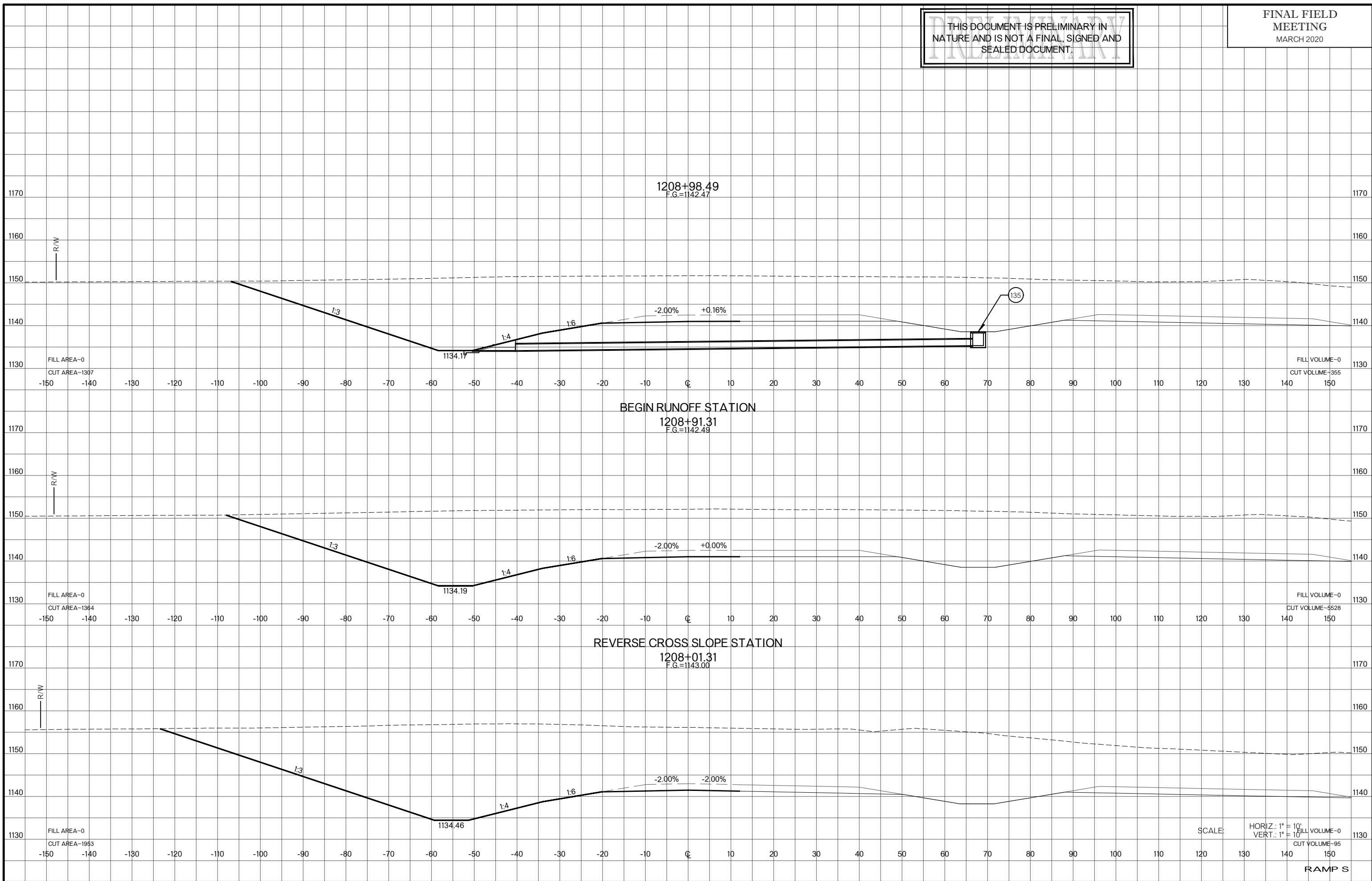
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP S

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

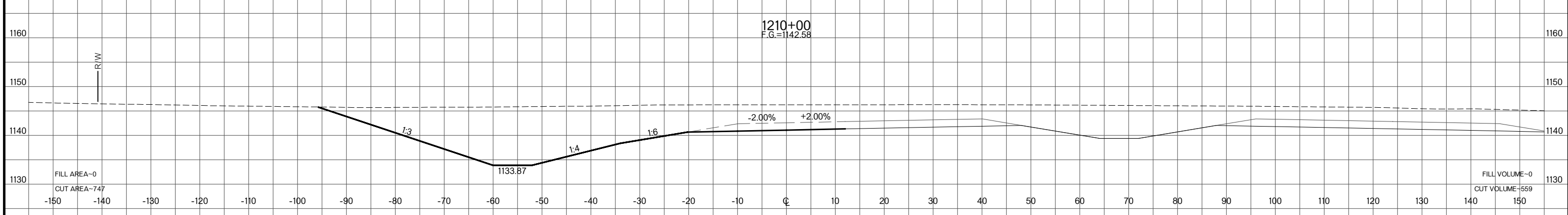
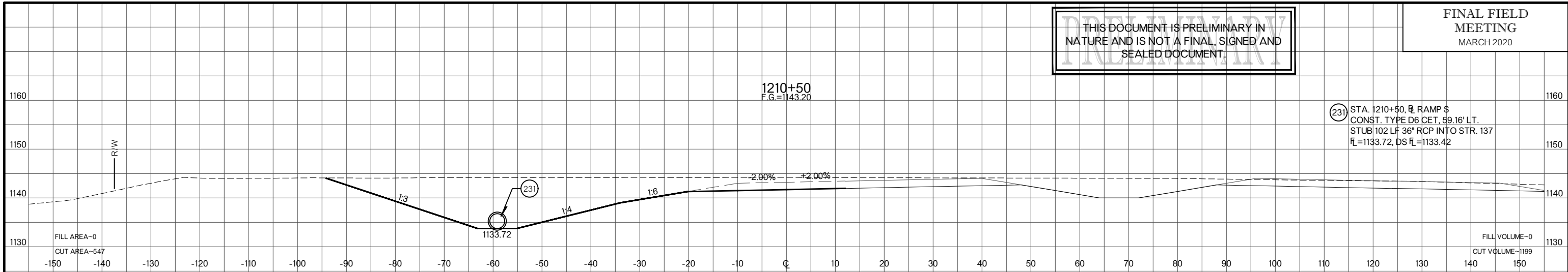
FINAL FIELD MEETING
MARCH 2020



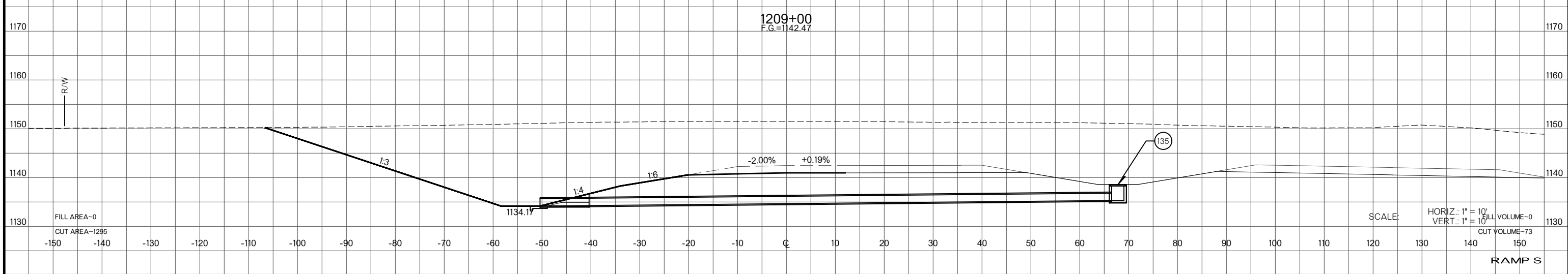
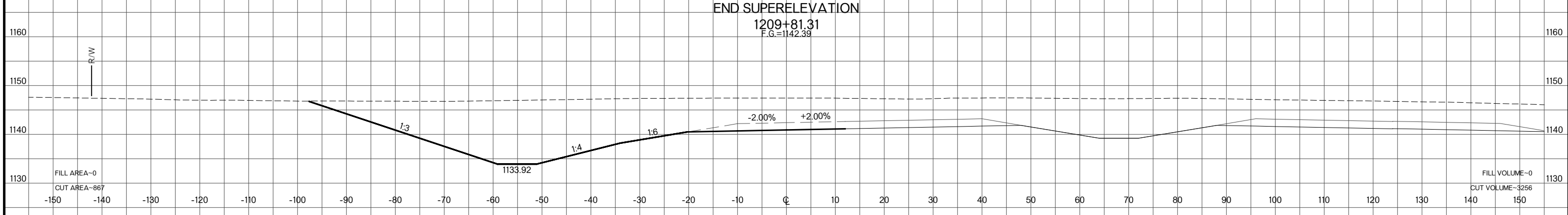
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



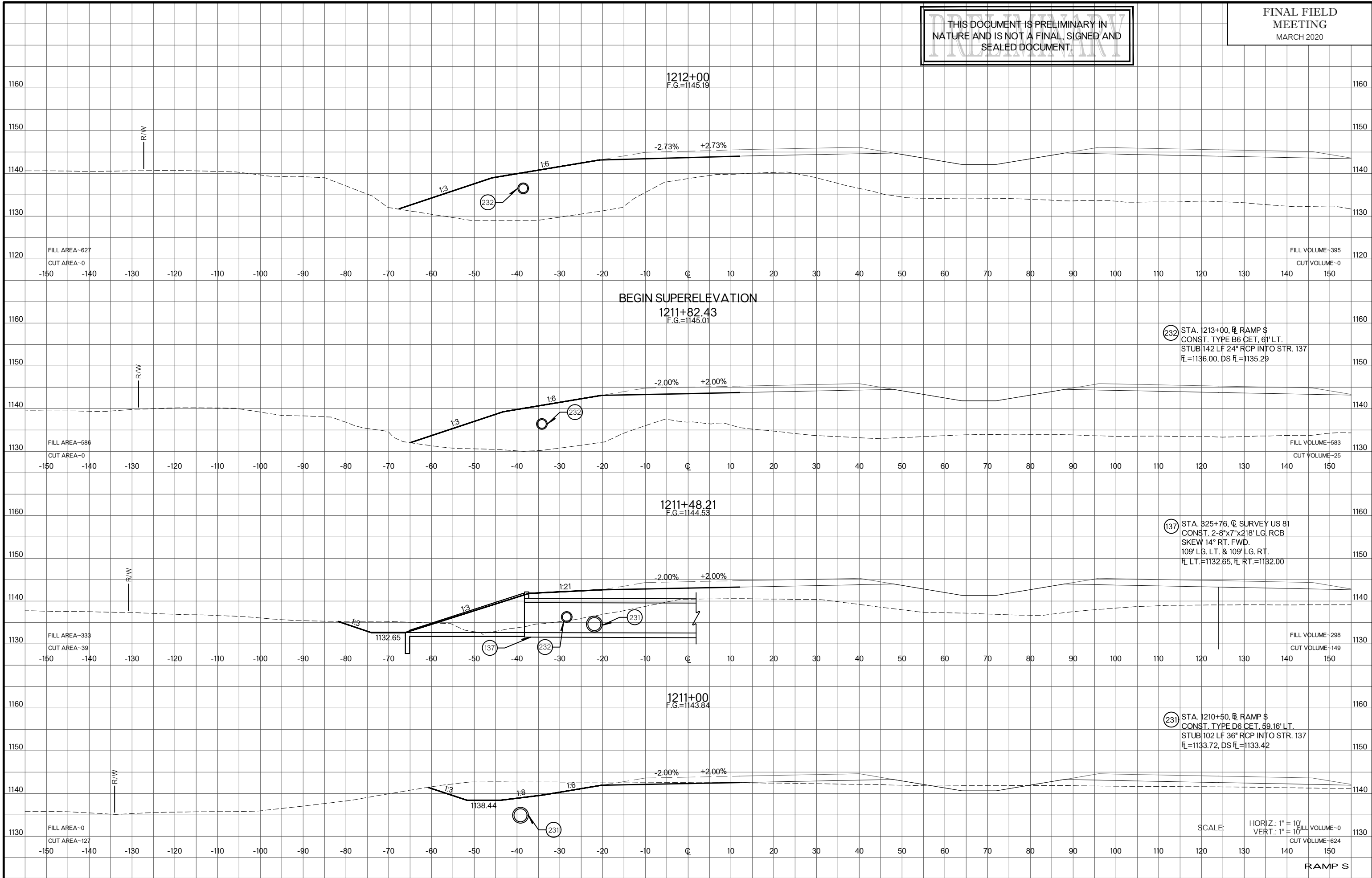
END SUPERELEVATION



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



(232) STA. 1213+00, RAMP S
CONST. TYPE B6 CET. 61' LT.
STUB 142 LF 24" RCP INTO STR. 137
FL=1136.00, DS FL=1135.29

(137) STA. 325+76, Q SURVEY US 81
CONST. 2-8"x7"x218' LG. RCB
SKEW 14° RT. FWD.
109' LG. LT. & 109' LG. RT.
FL LT.=1132.65; FL RT.=1132.00

(231) STA. 1210+50, RAMP S
CONST. TYPE D6 CET. 59.16' LT.
STUB 102 LF 36" RCP INTO STR. 137
FL=1133.72, DS FL=1133.42

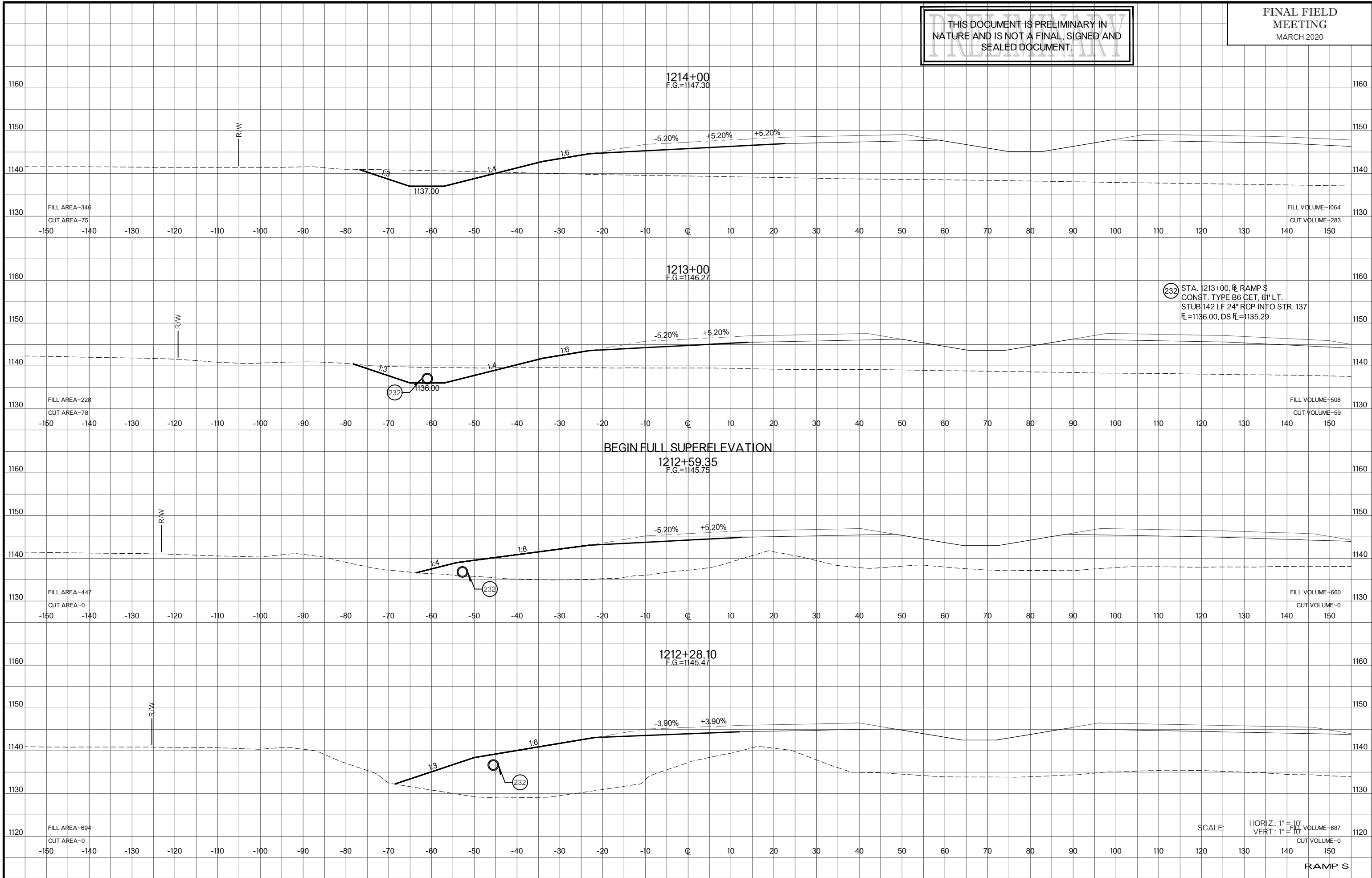
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP S

US 81 REALIGNMENT
GRADY COUNTY

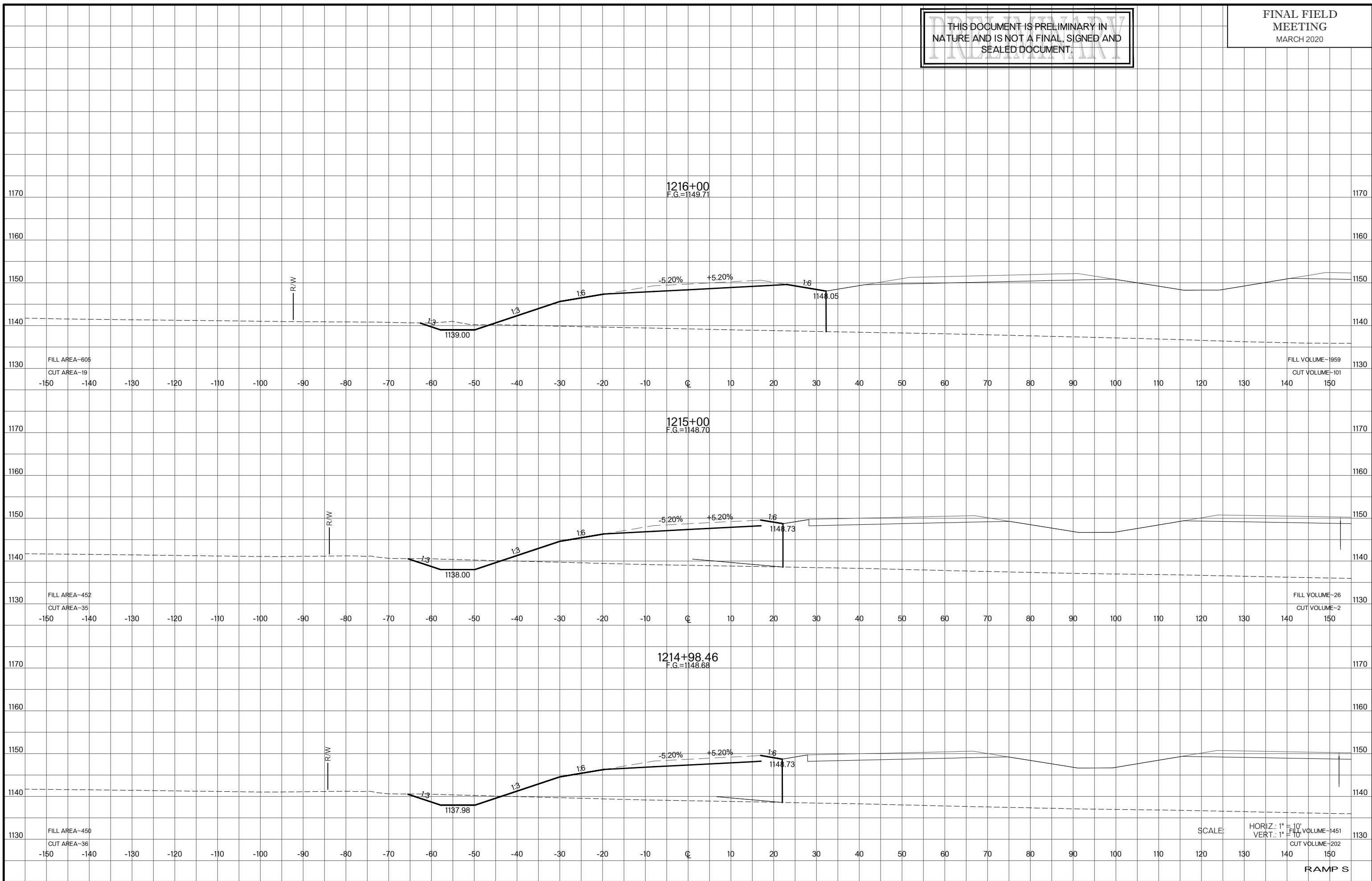
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

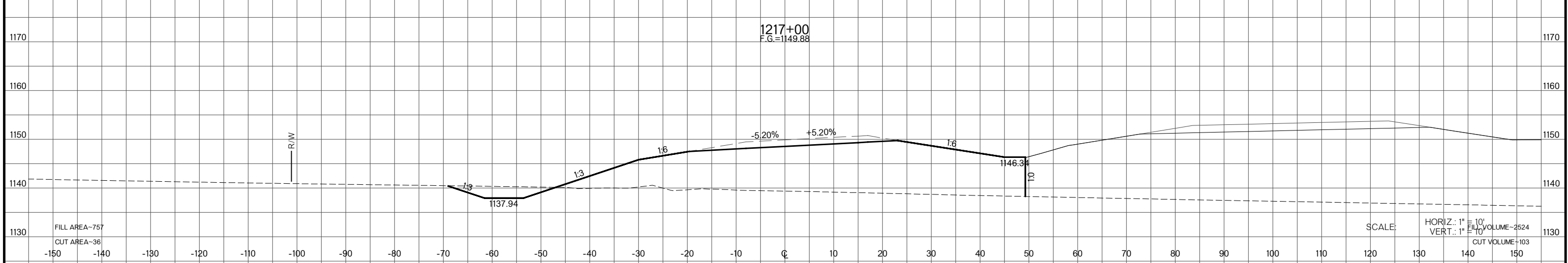
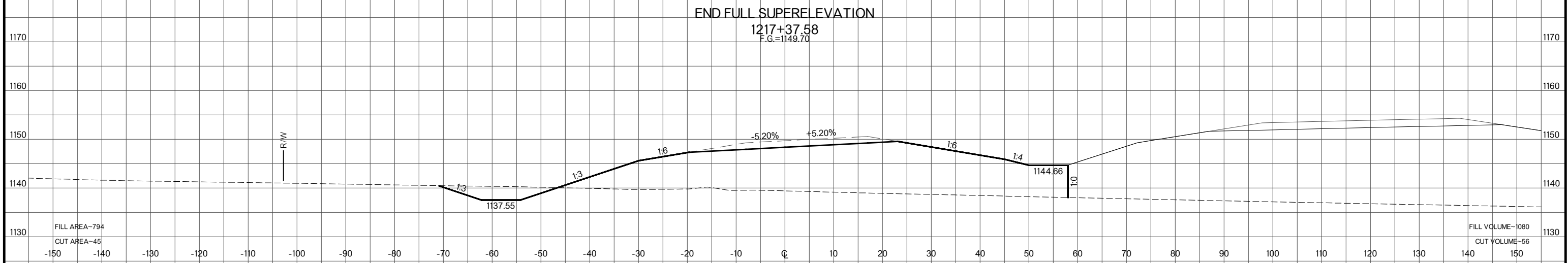
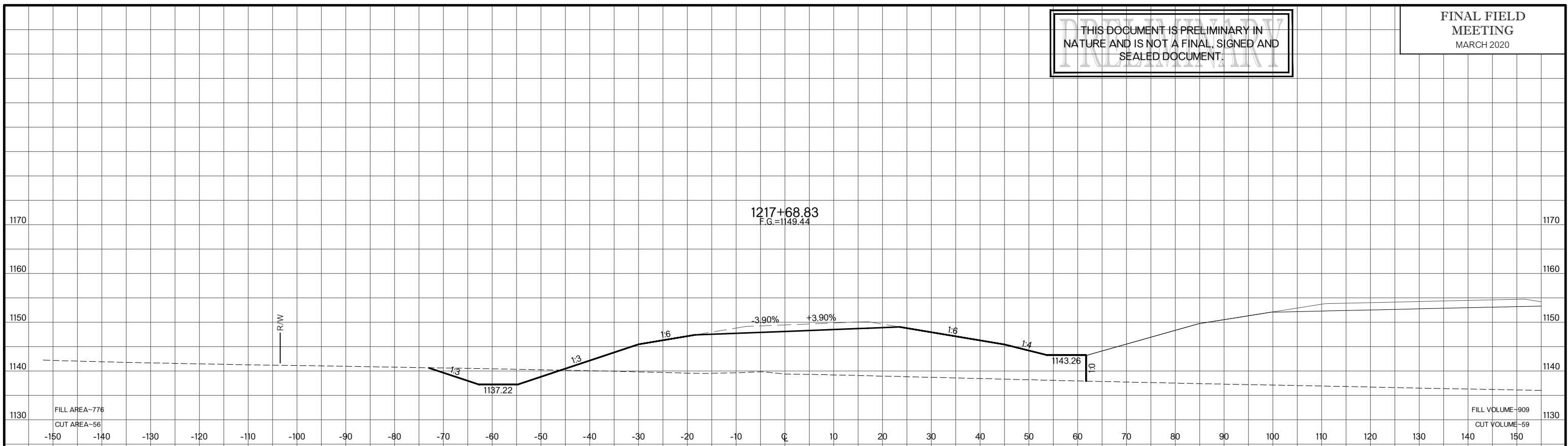
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



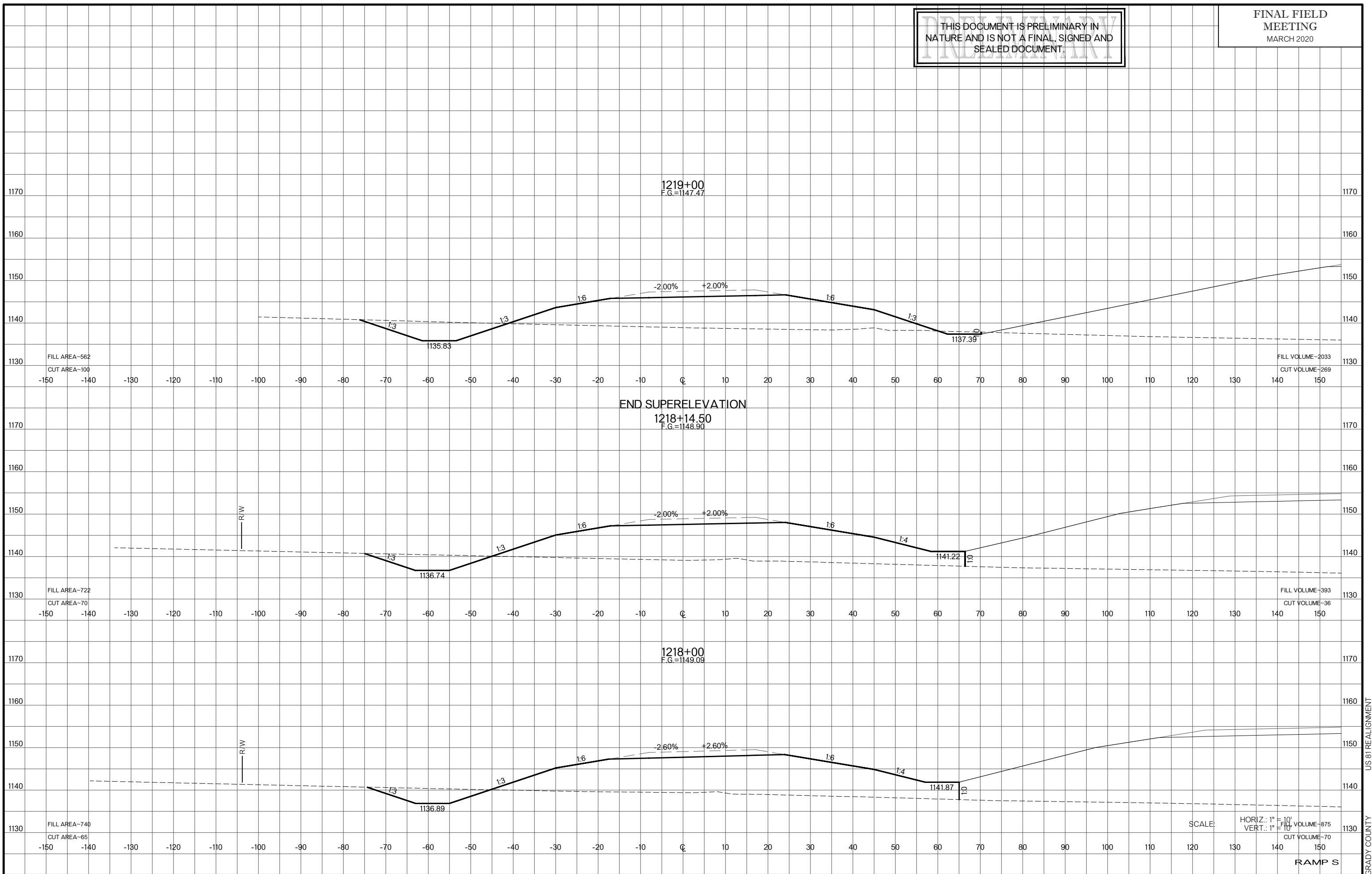
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP S

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

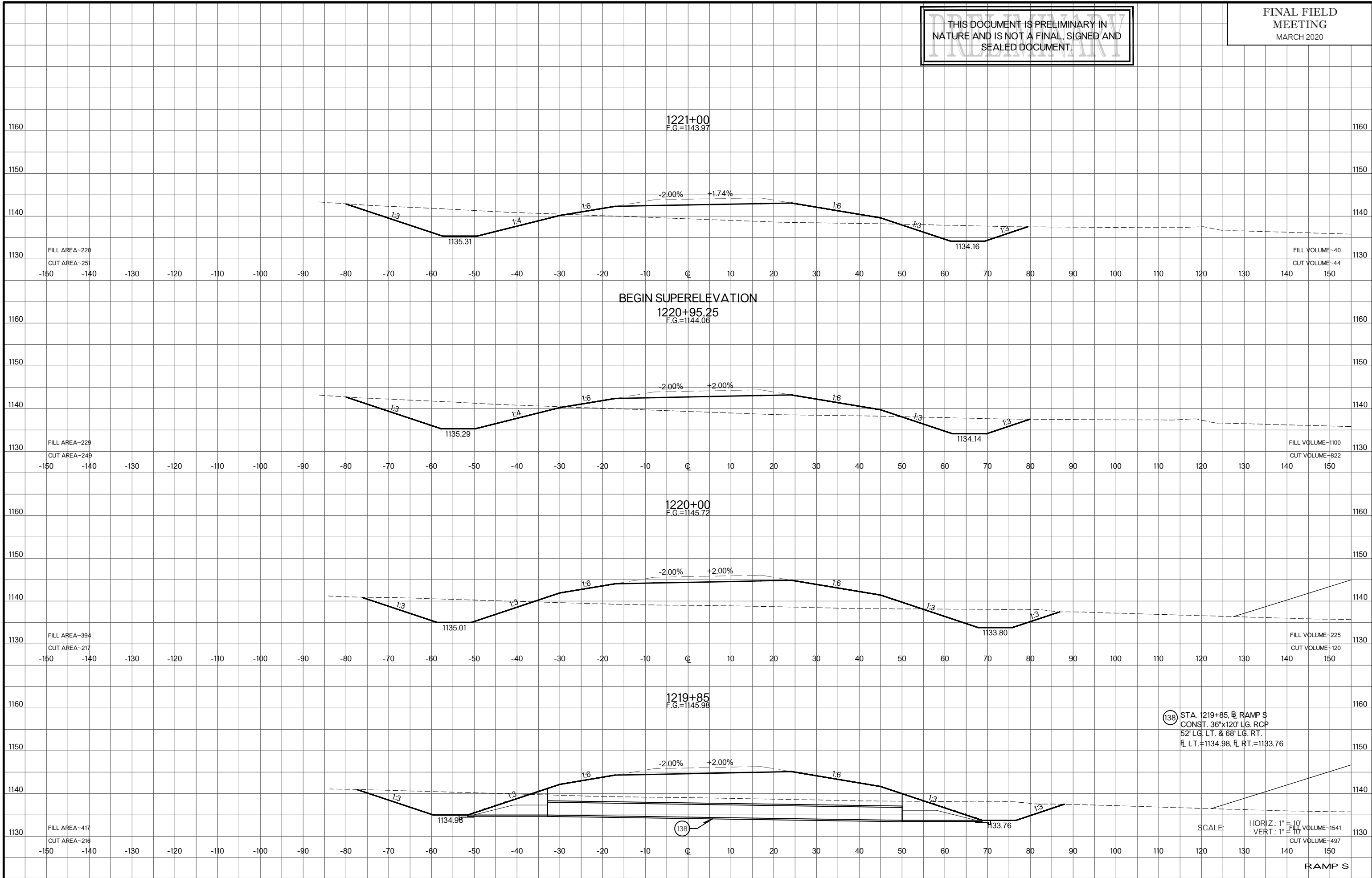
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



(138) STA. 1219+85, RAMP S
CONST. 36"x120" LG. RCP
52' LG. LT. & 68' LG. RT.
FL LT.=1134.98, FL RT.=1133.76

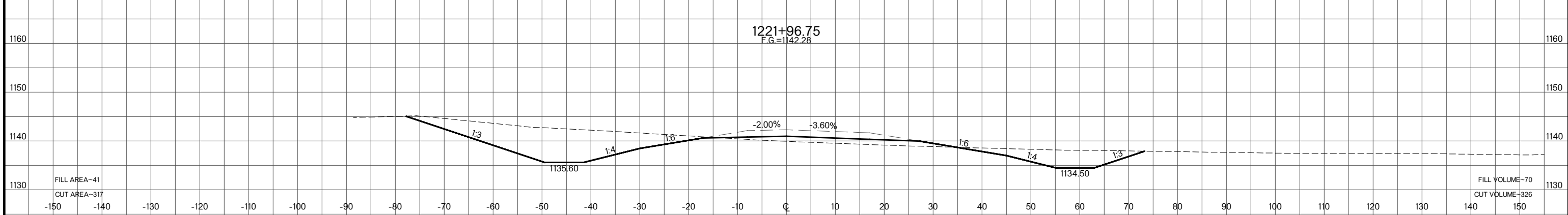
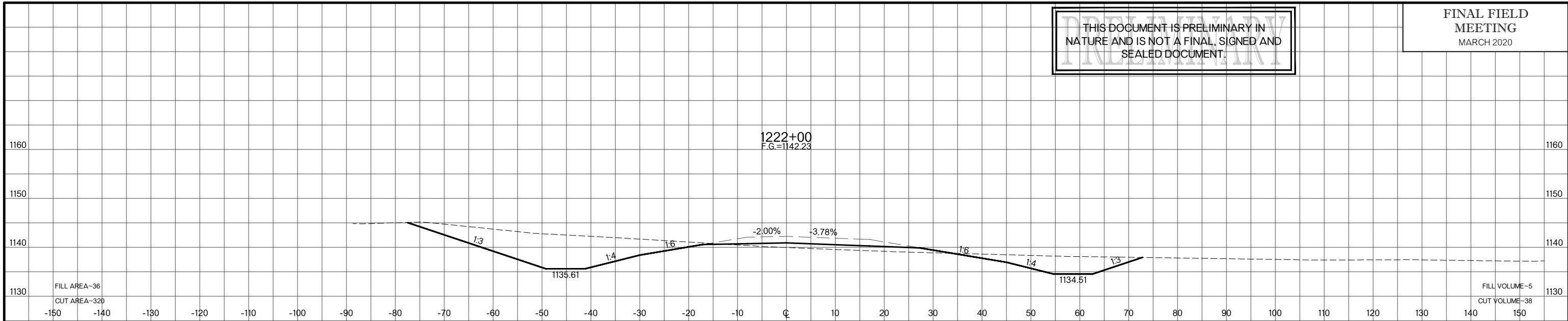
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP S

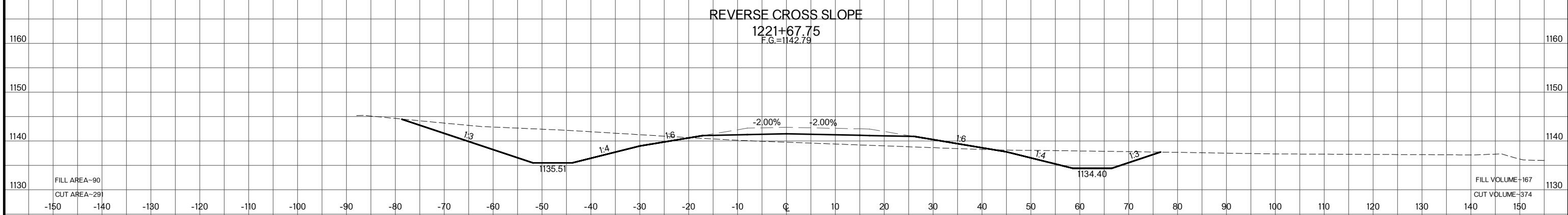
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

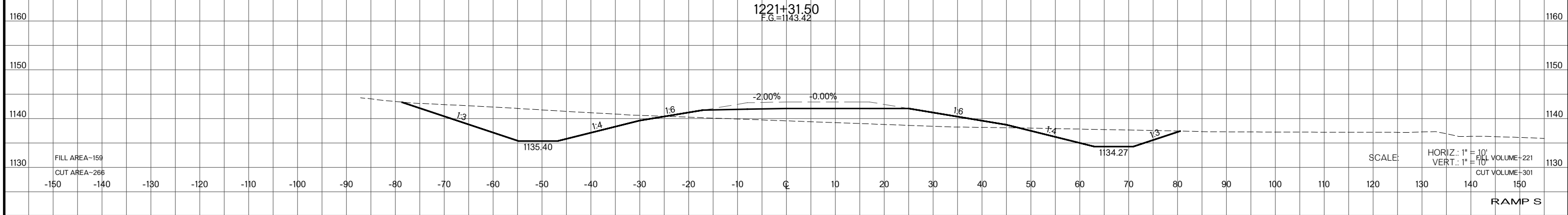
FINAL FIELD MEETING MARCH 2020



REVERSE CROSS SLOPE



RUNOFF BEGINS STATION



SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMPS

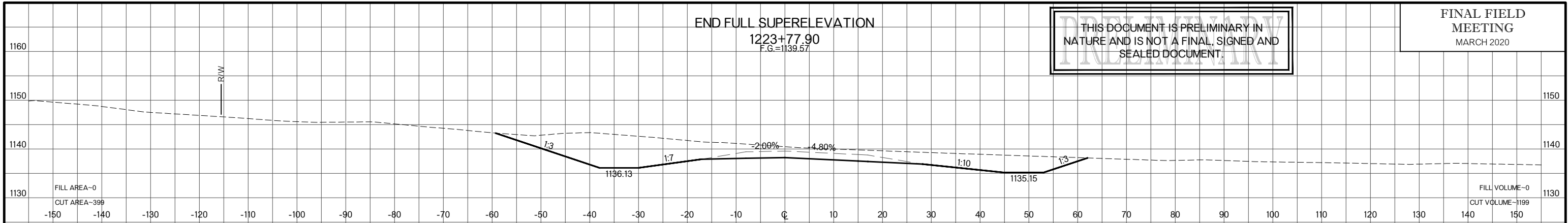
US 81 REALIGNMENT GRADY COUNTY

END FULL SUPERELEVATION

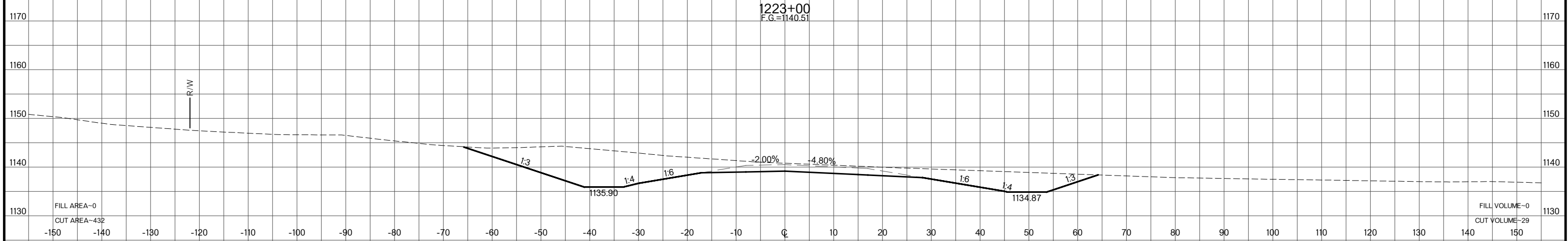
1223+77.90
F.G.=1139.57

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

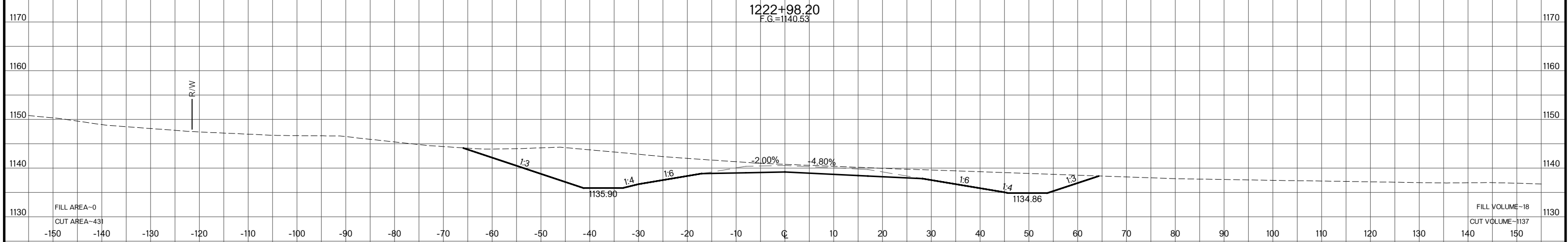
FINAL FIELD MEETING
MARCH 2020



1223+00
F.G.=1140.51

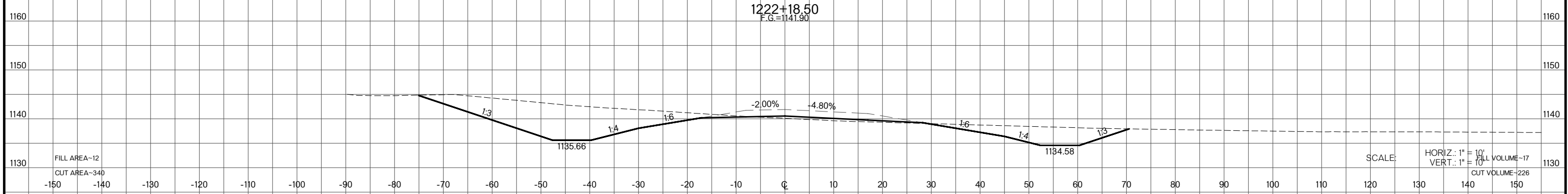


1222+98.20
F.G.=1140.53



BEGIN FULL SUPERELEVATION

1222+18.50
F.G.=1141.90

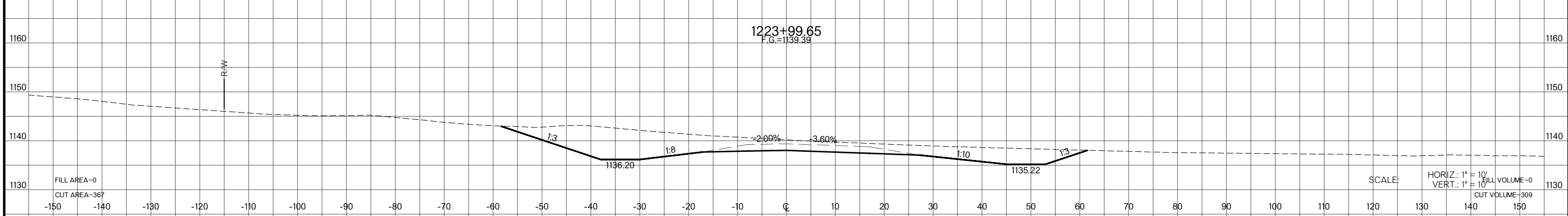
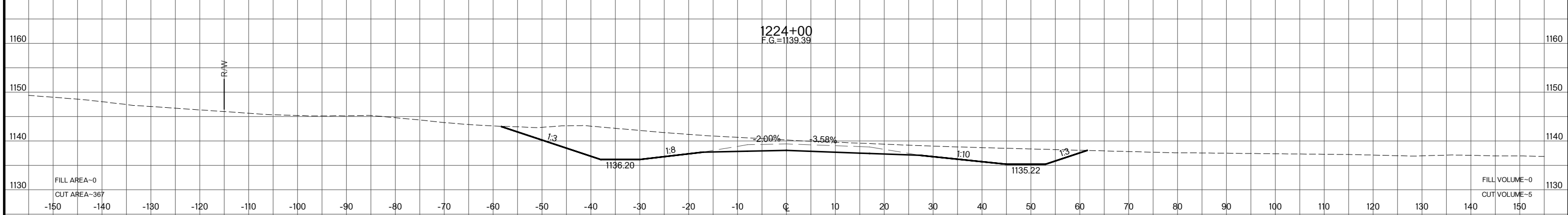
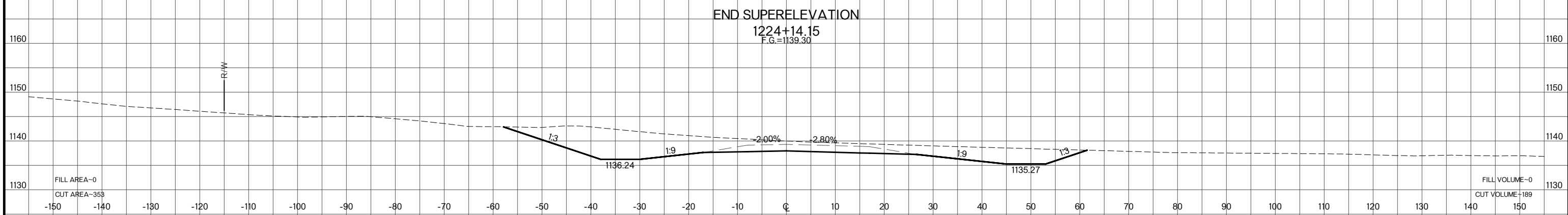
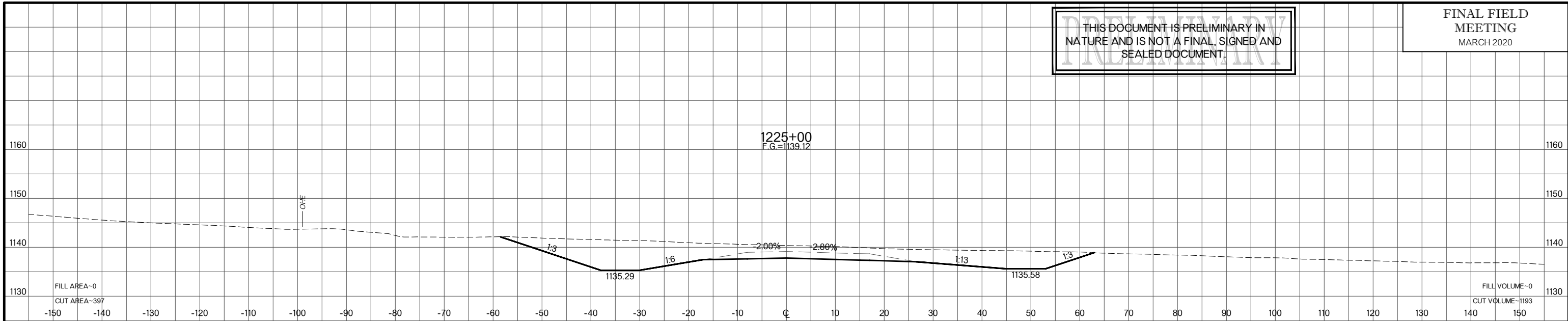


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMPS

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



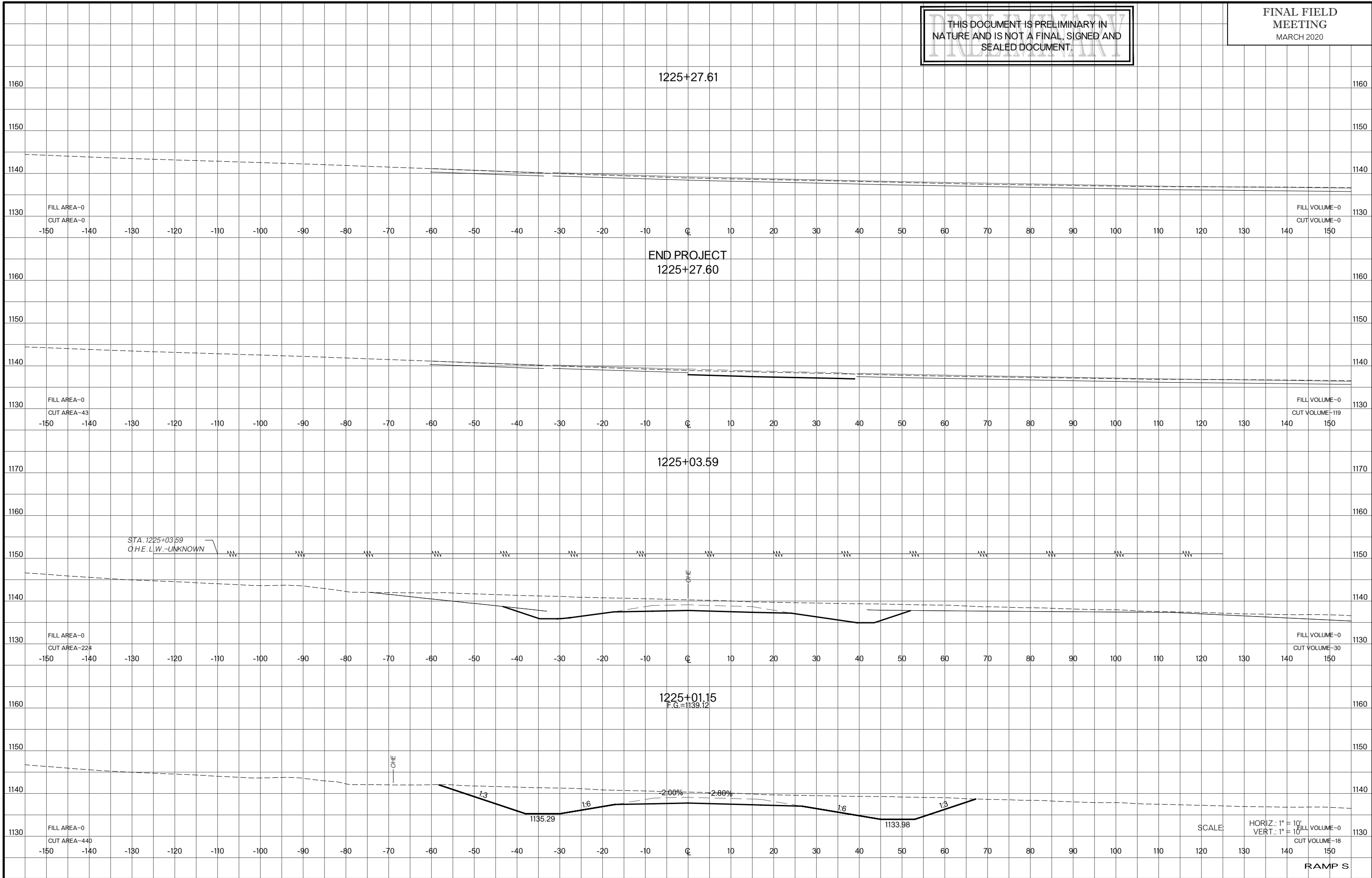
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMPS

US 81 REALIGNMENT
GRADY COUNTY

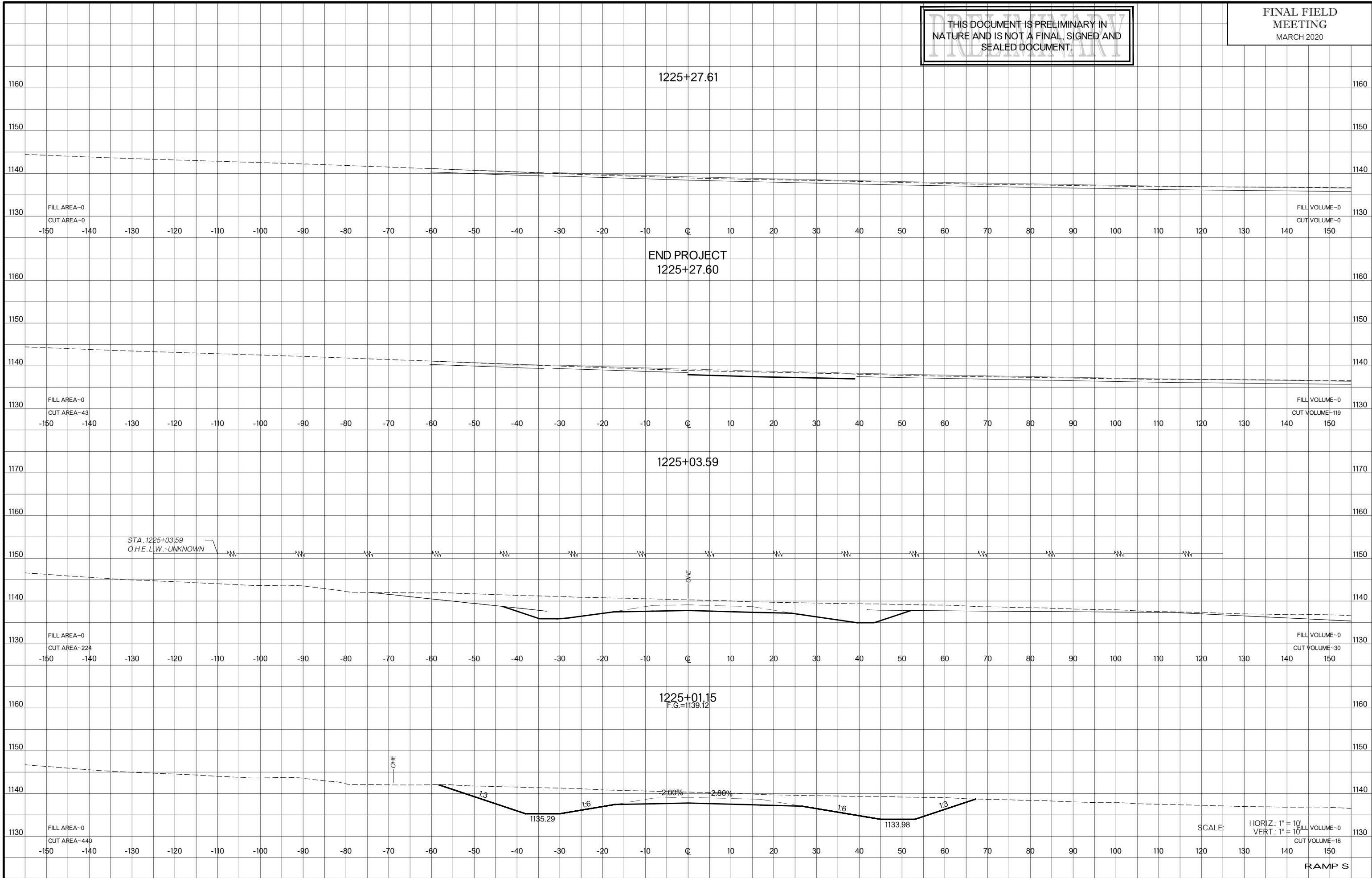
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

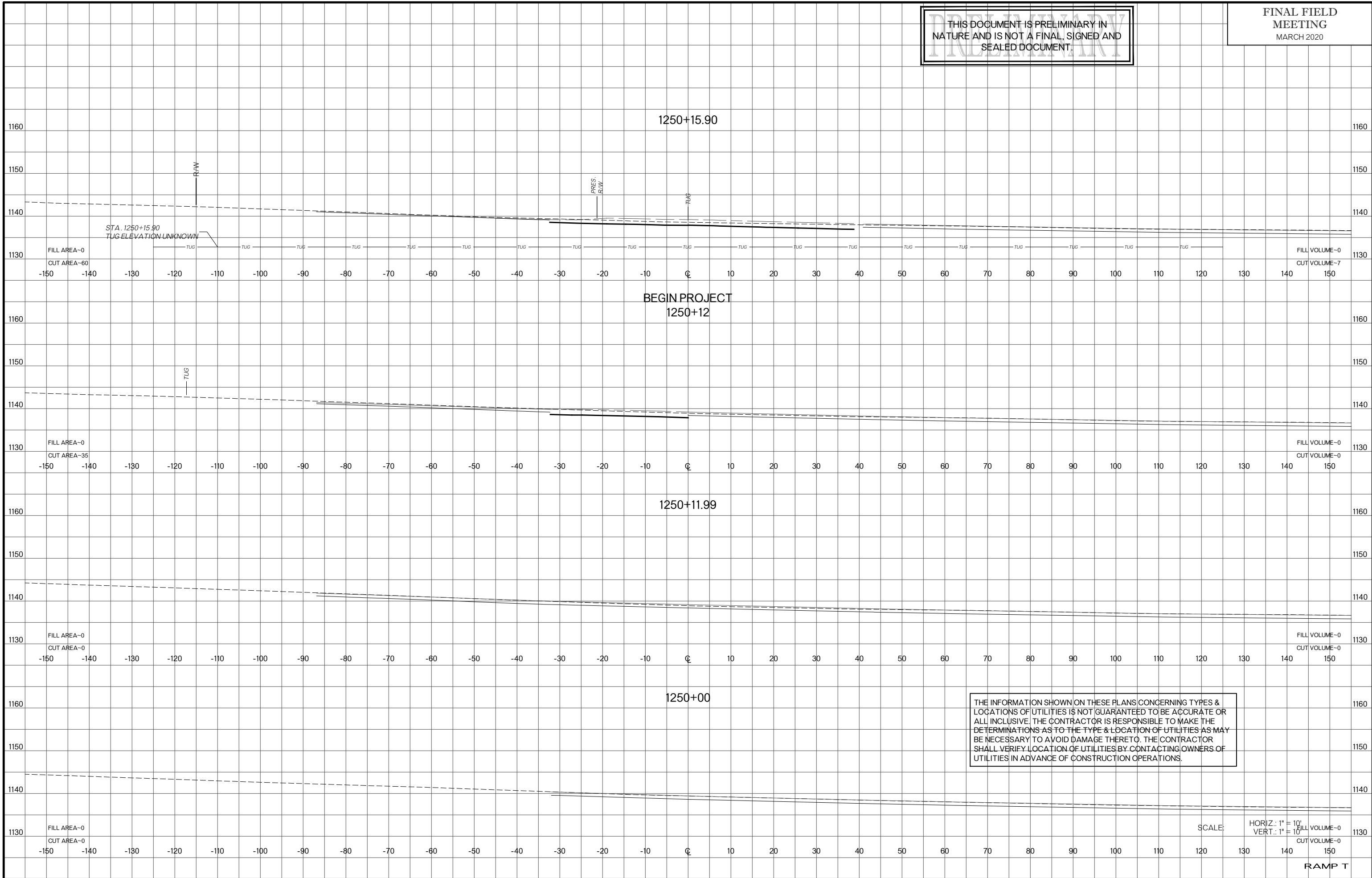
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



BEGIN PROJECT
1250+12

THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

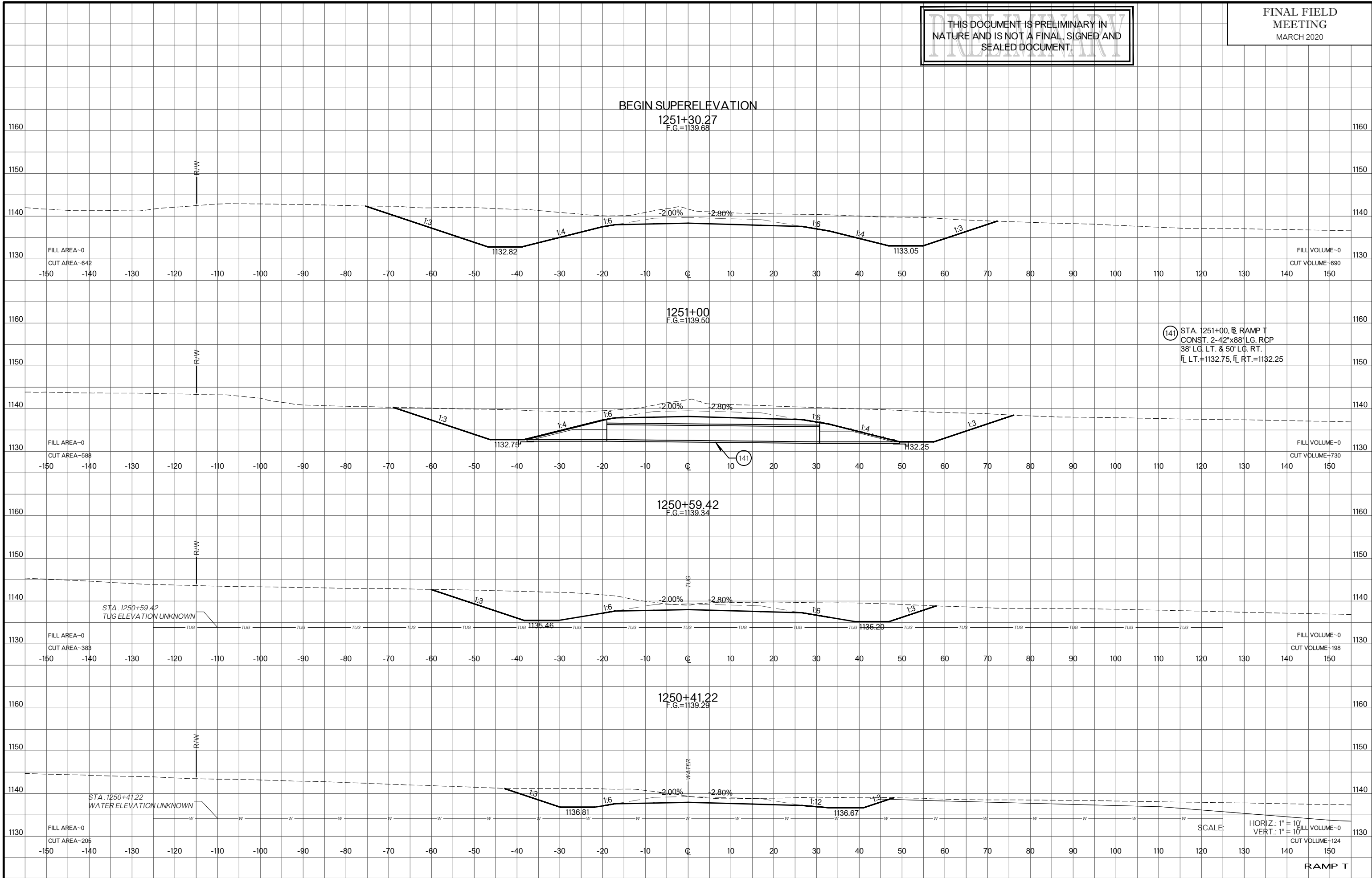
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP T

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

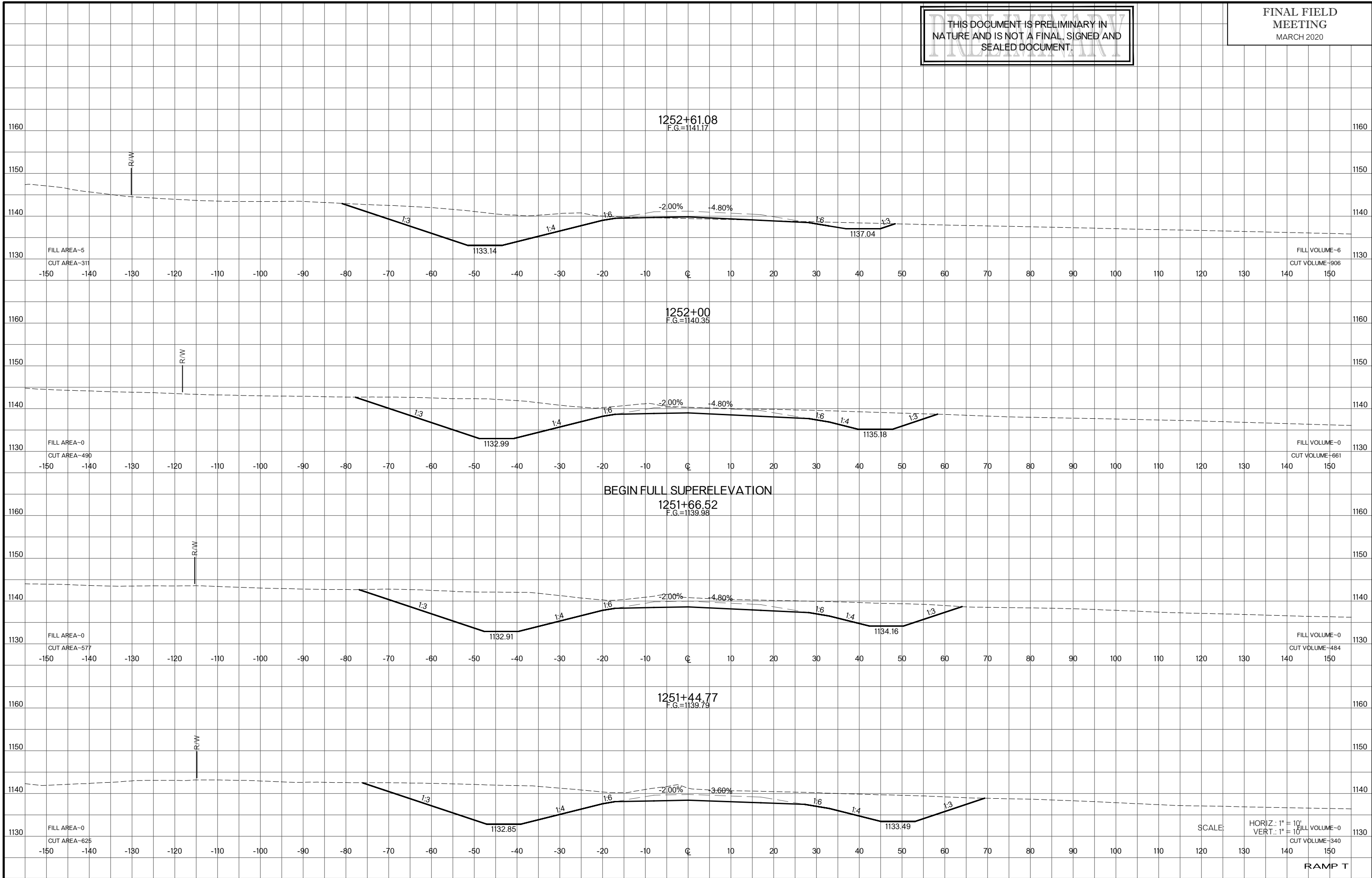
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



BEGIN FULL SUPERELEVATION

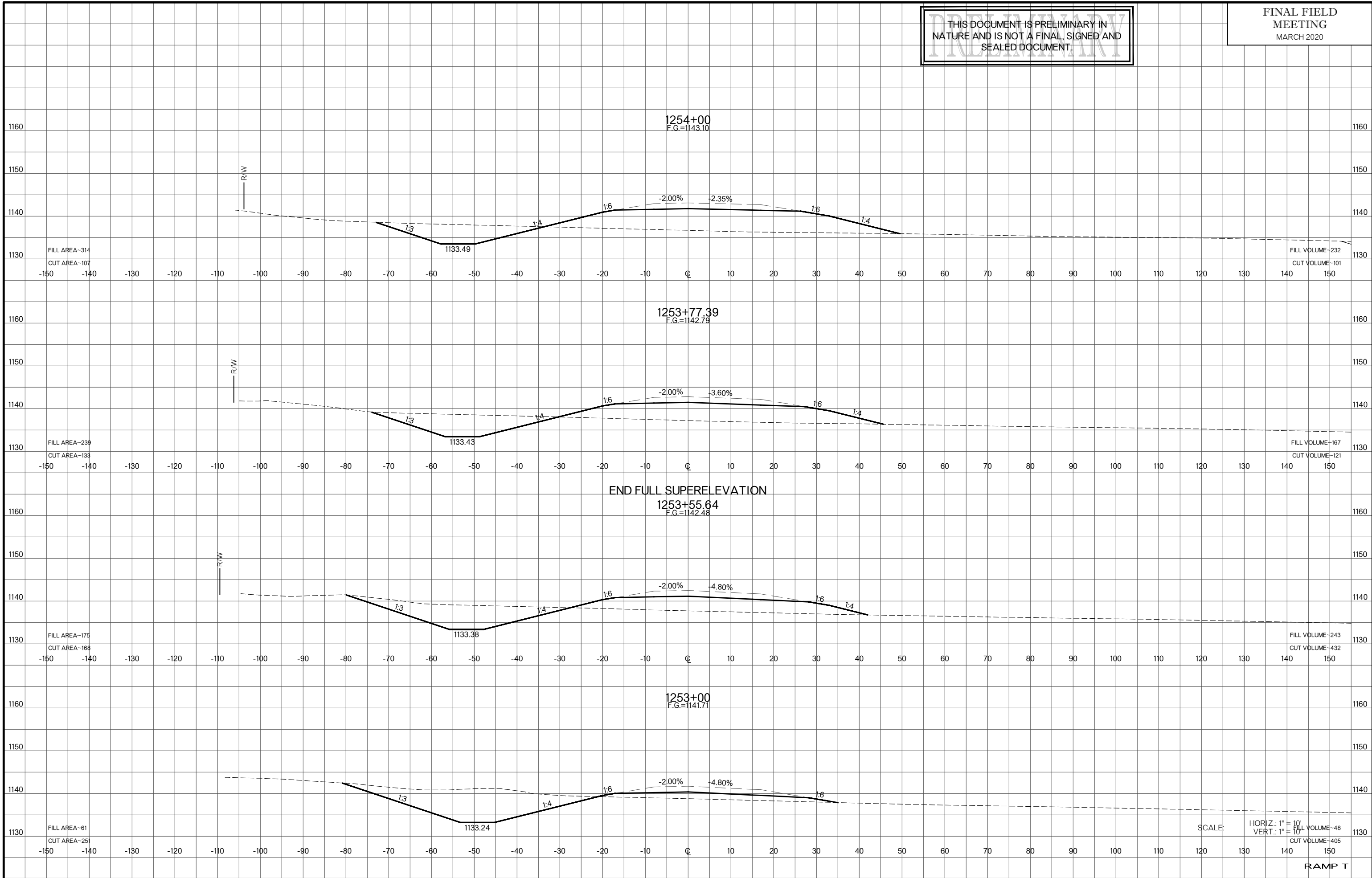
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP T

US 81 REALIGNMENT
GRADY COUNTY

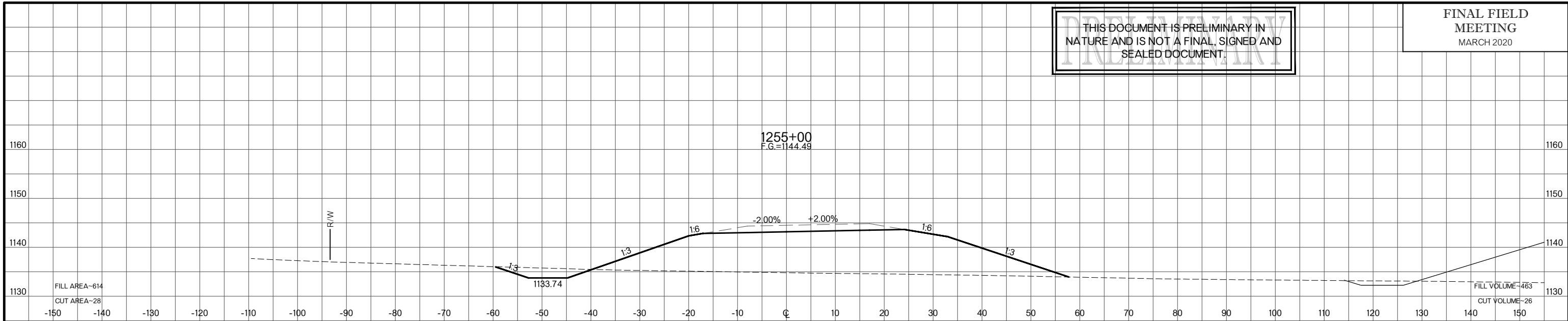
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020

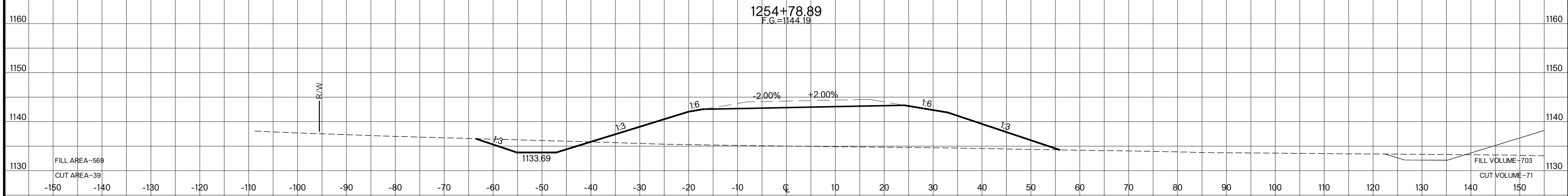


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

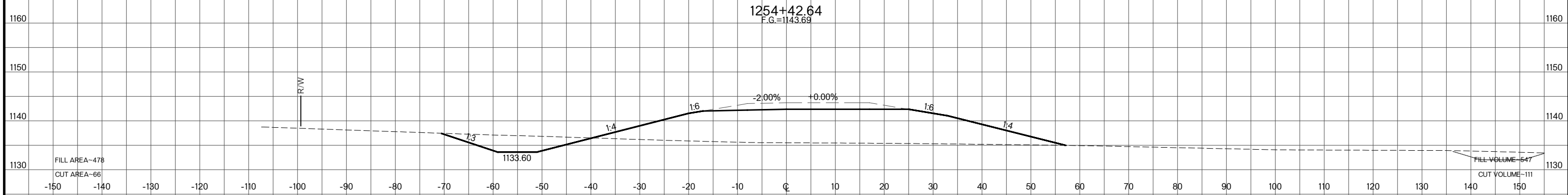
FINAL FIELD MEETING
MARCH 2020



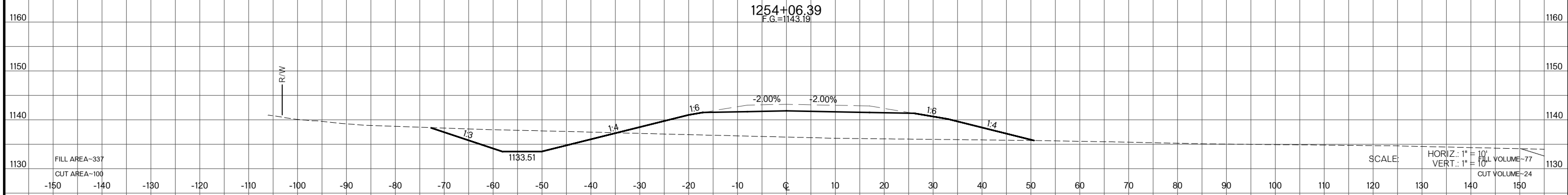
END SUPERELEVATION



RUNOFF ENDS STATION



REVERSE CROSS SLOPE



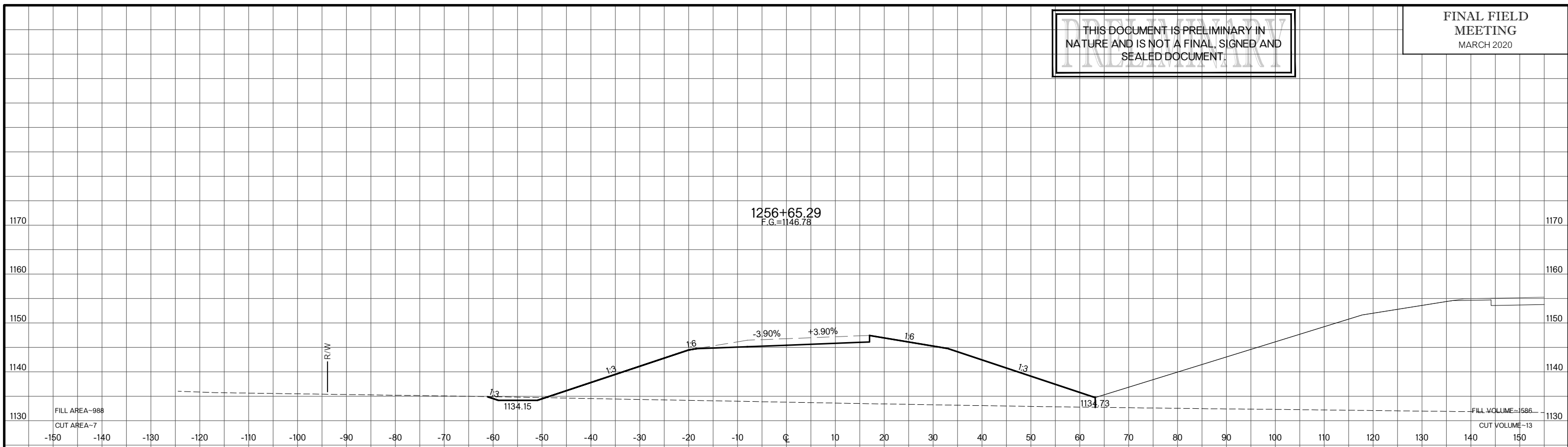
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP T

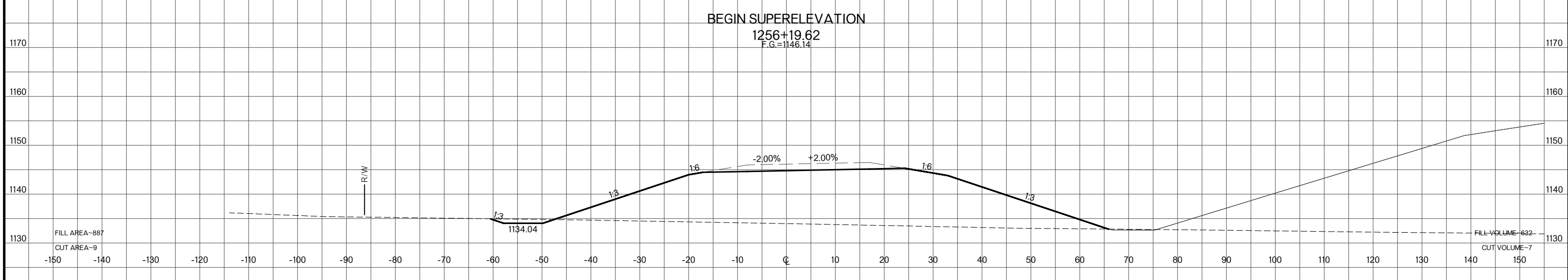
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

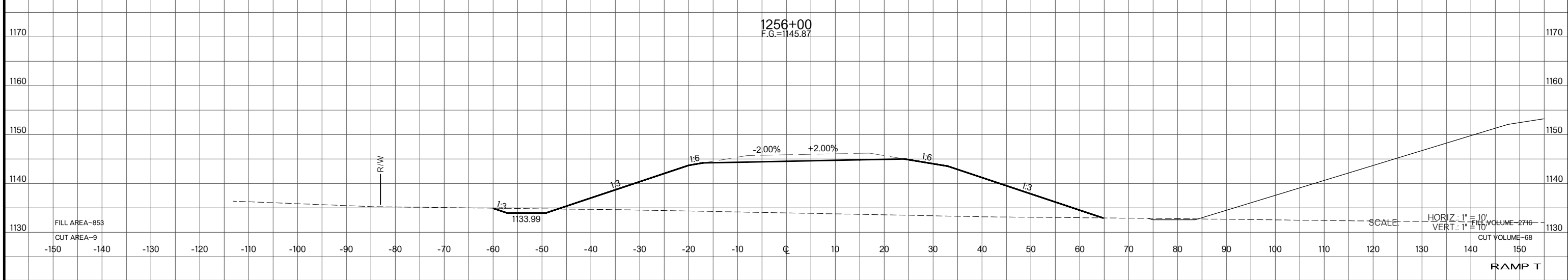
FINAL FIELD MEETING
MARCH 2020



BEGIN SUPERELEVATION
1256+19.62
F.G.=1146.14



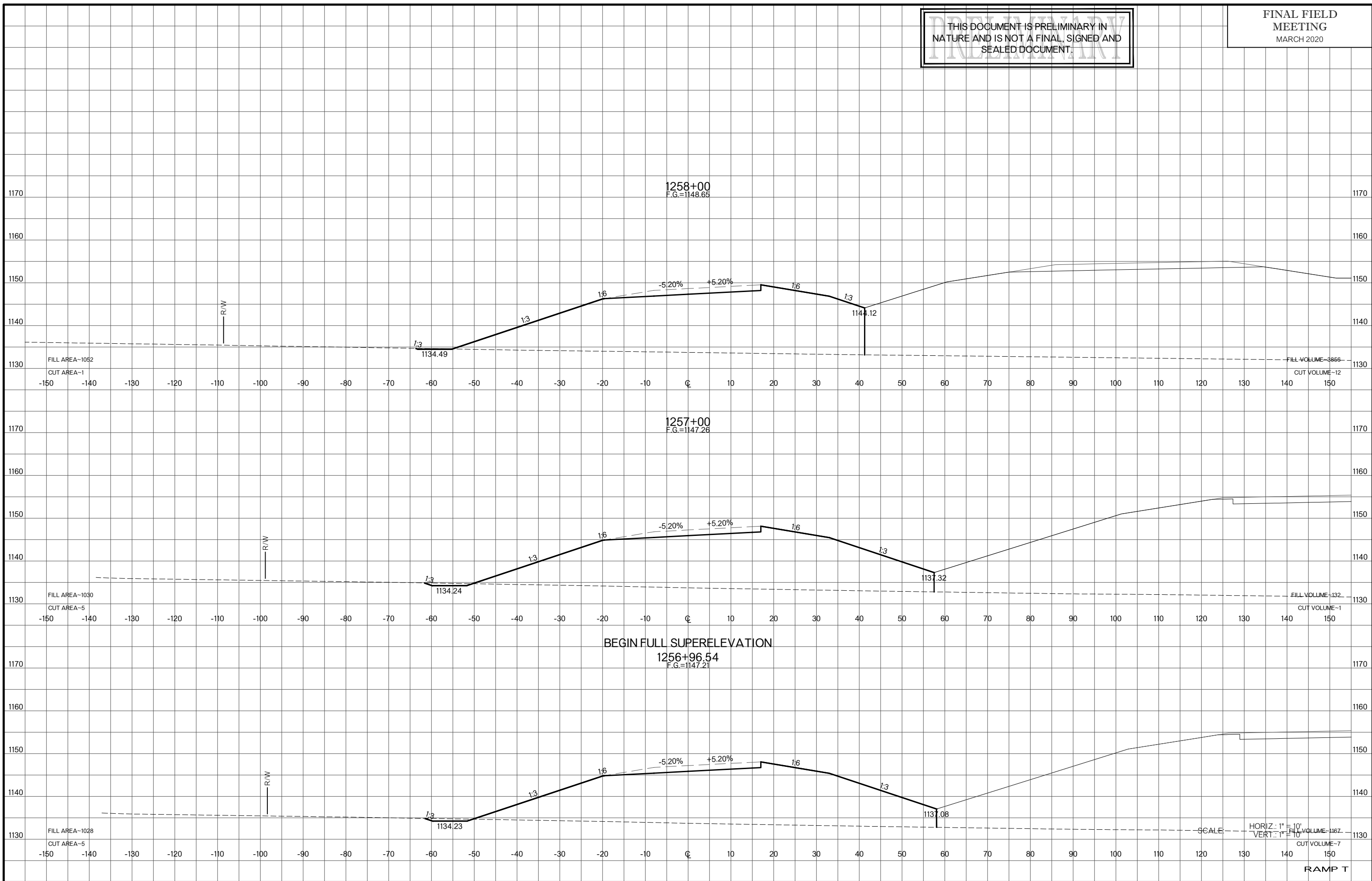
1256+00
F.G.=1145.87



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

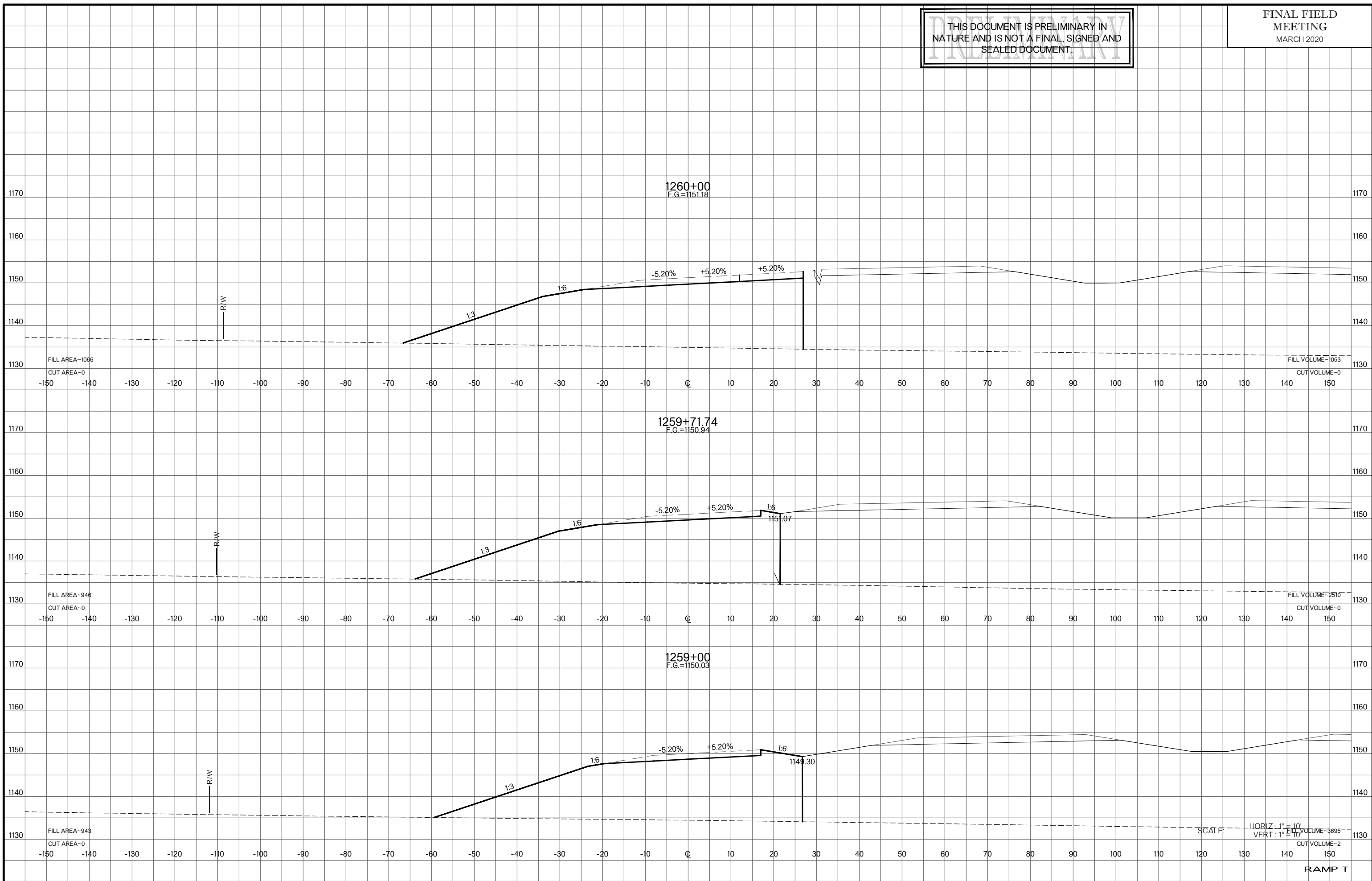
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

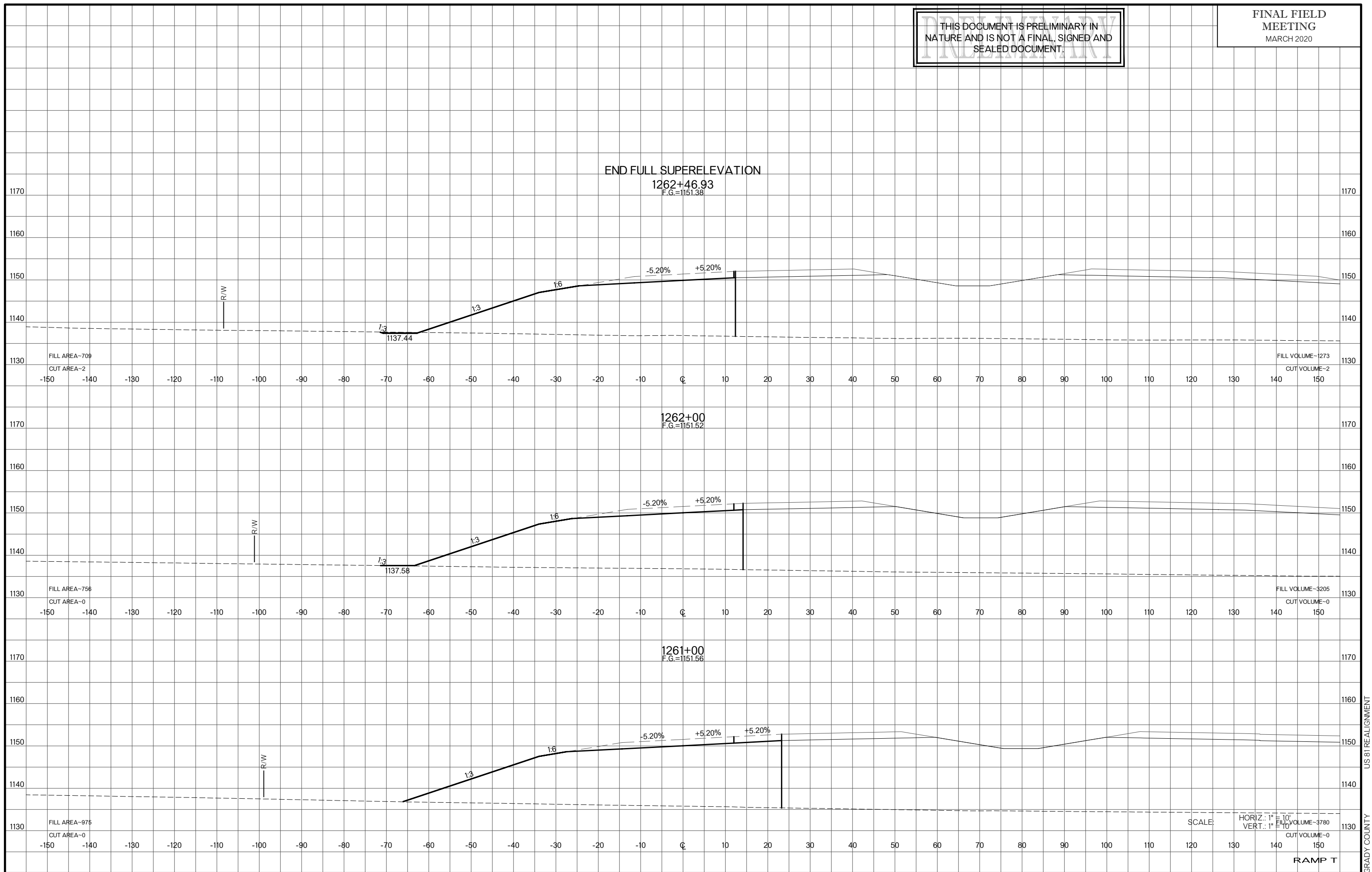
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

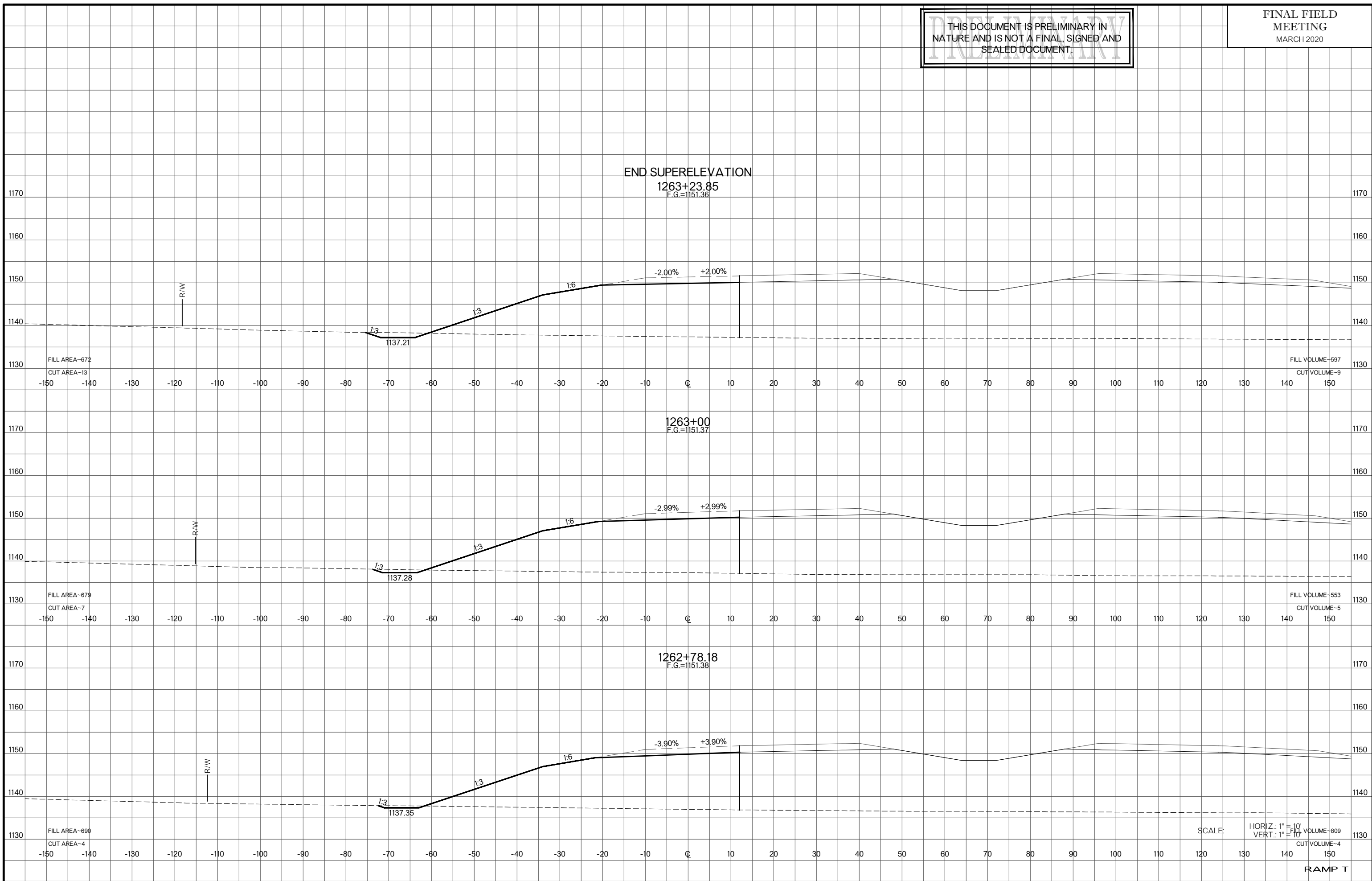
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



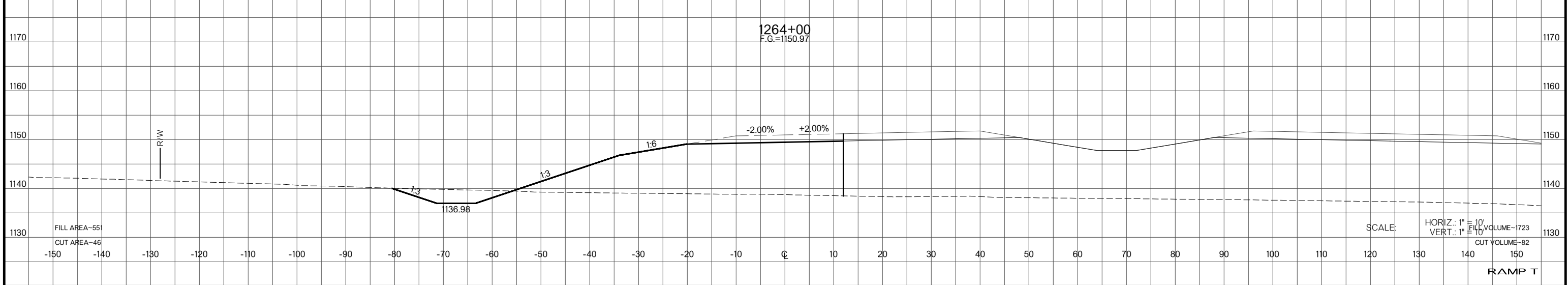
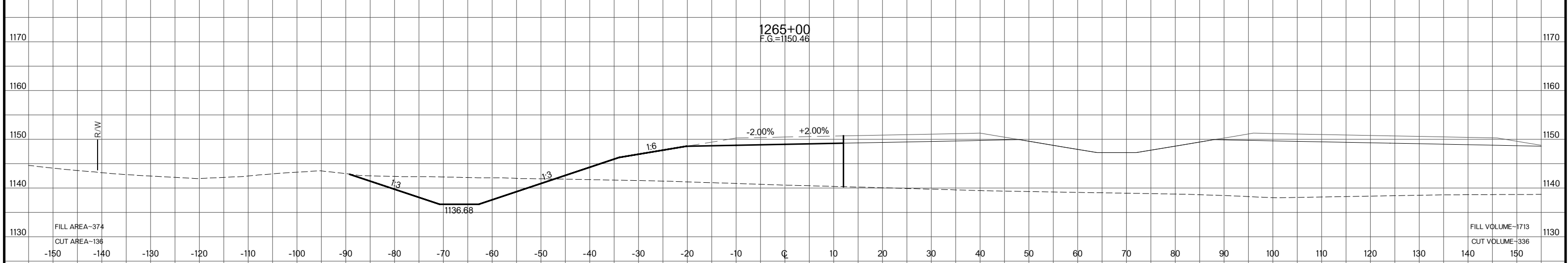
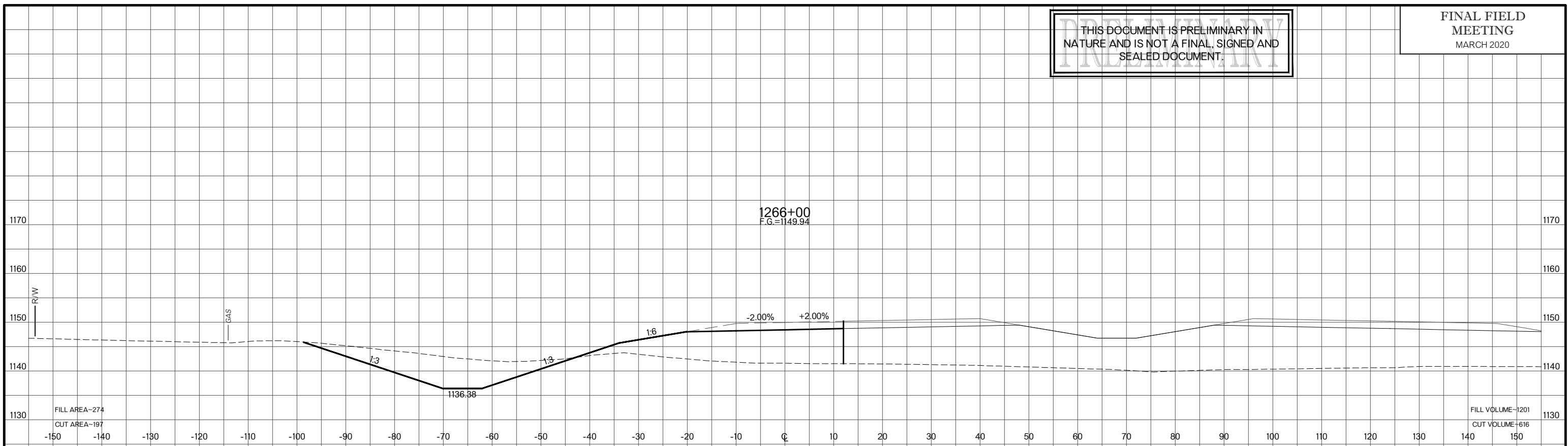
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP T

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

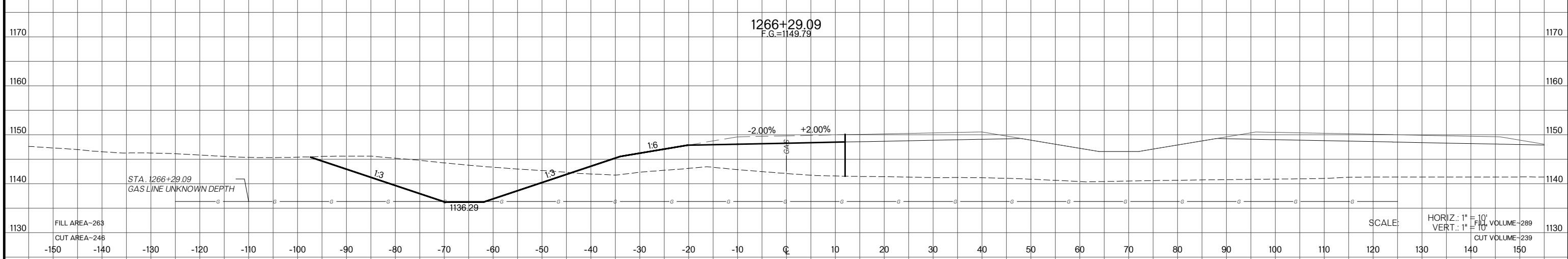
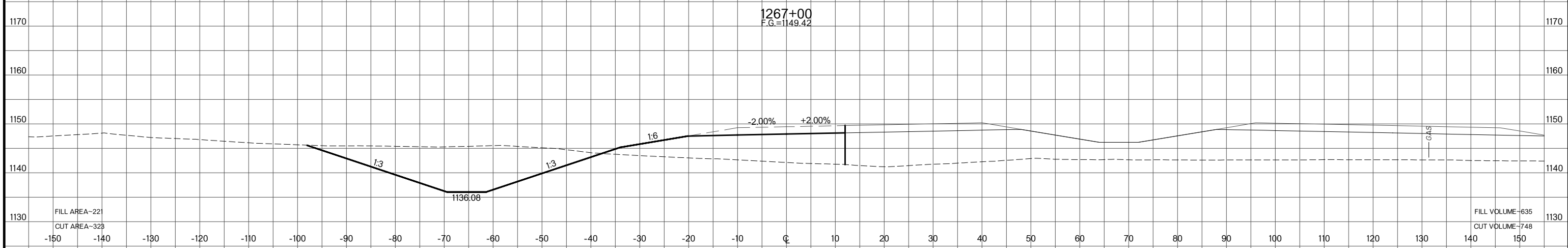
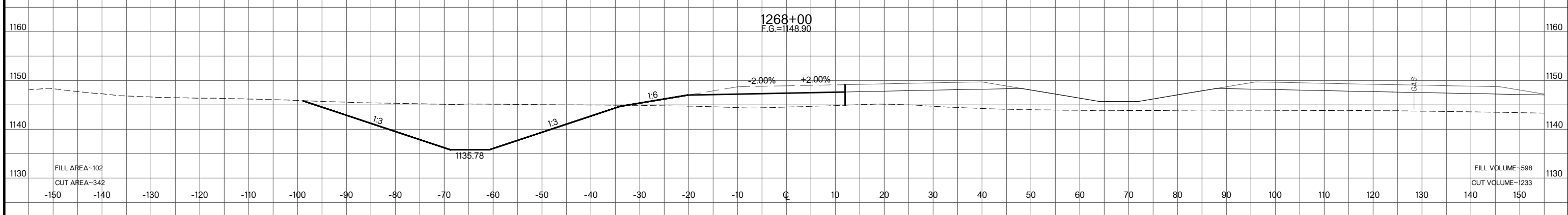
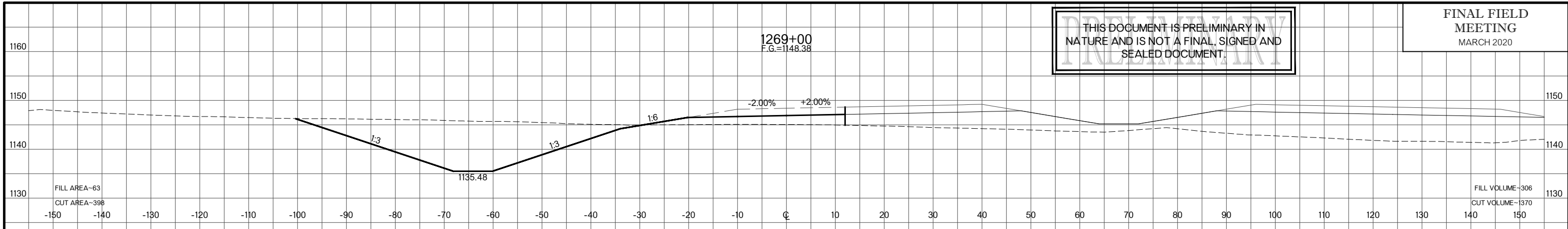
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



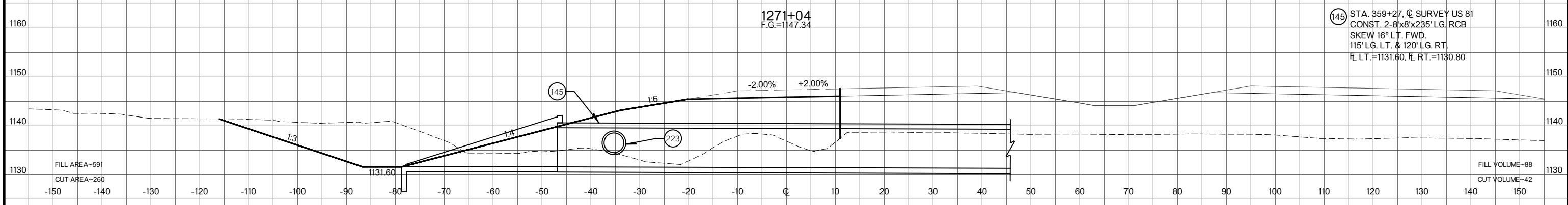
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP T

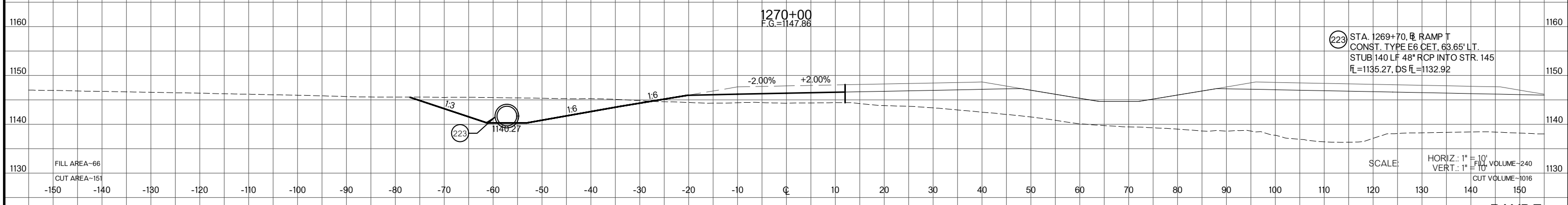
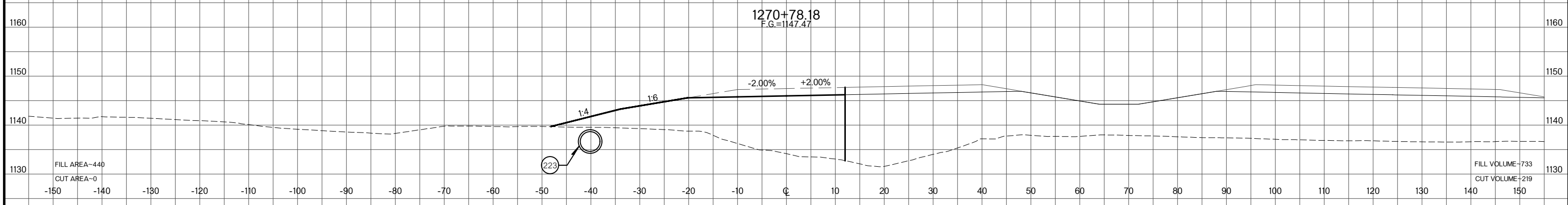
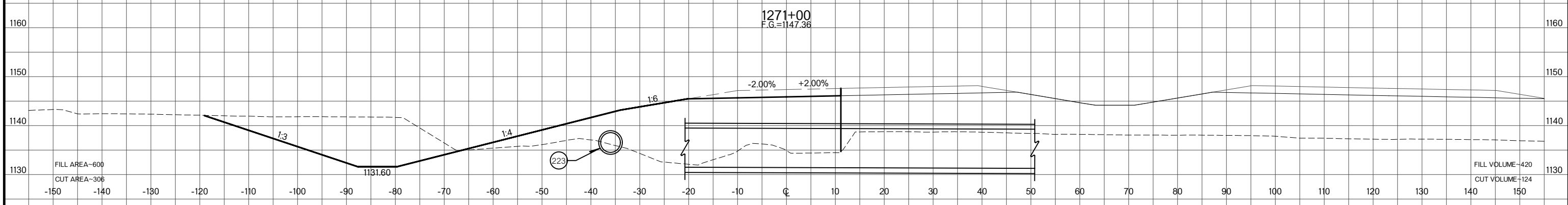
US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



145 STA. 359+27, C SURVEY US 81
CONST. 2-8"x8"x235' LG. RCB
SKEW 16° LT. FWD.
115' LG. LT. & 120' LG. RT.
RL LT.=1131.60, RL RT.=1130.80



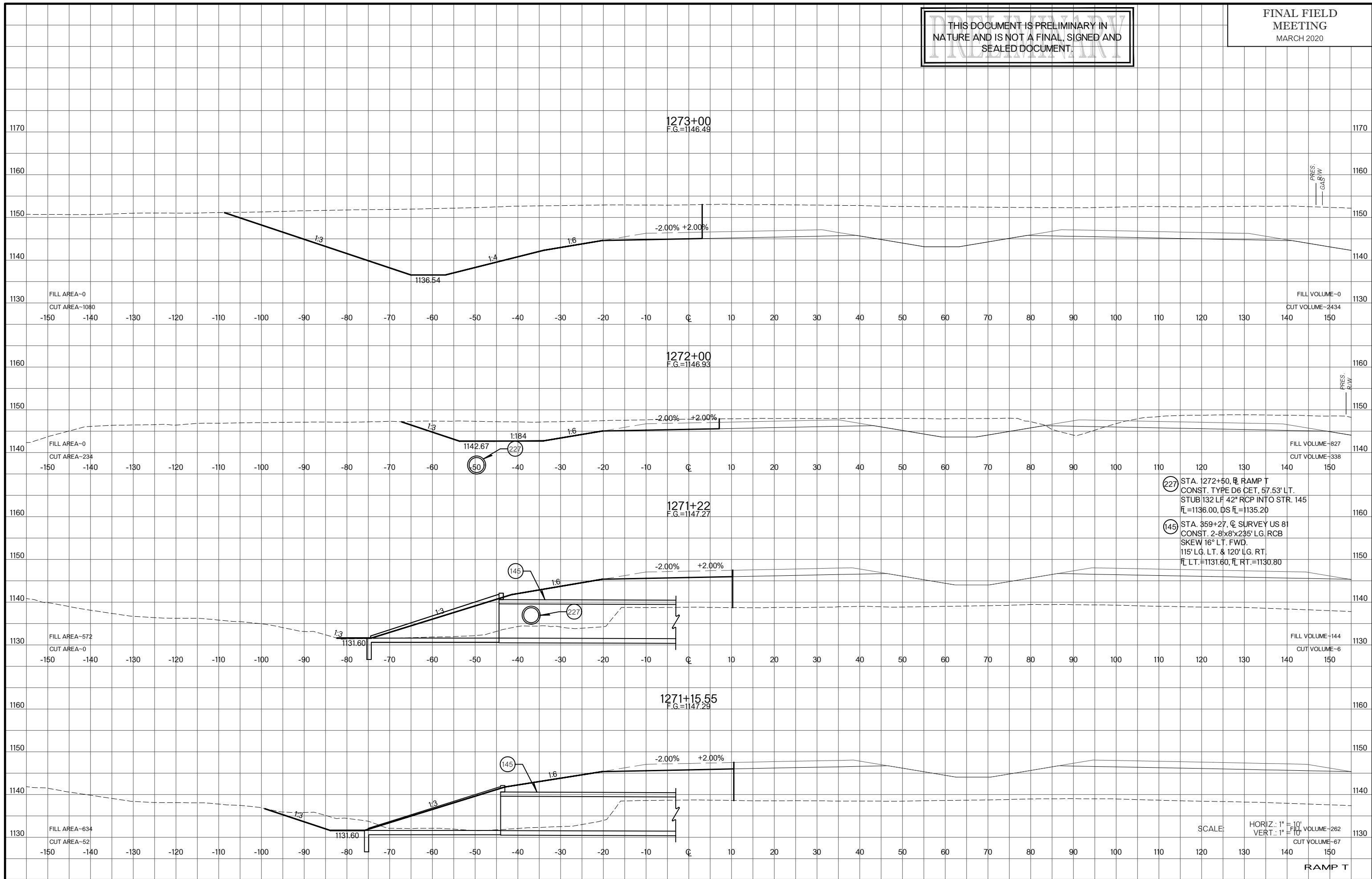
223 STA. 1269+70, RAMP T
CONST. TYPE E6 CET, 63.65' LT.
STUB 140 LF 48" RCP INTO STR. 145
RL=1135.27, DS RL=1132.92

SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

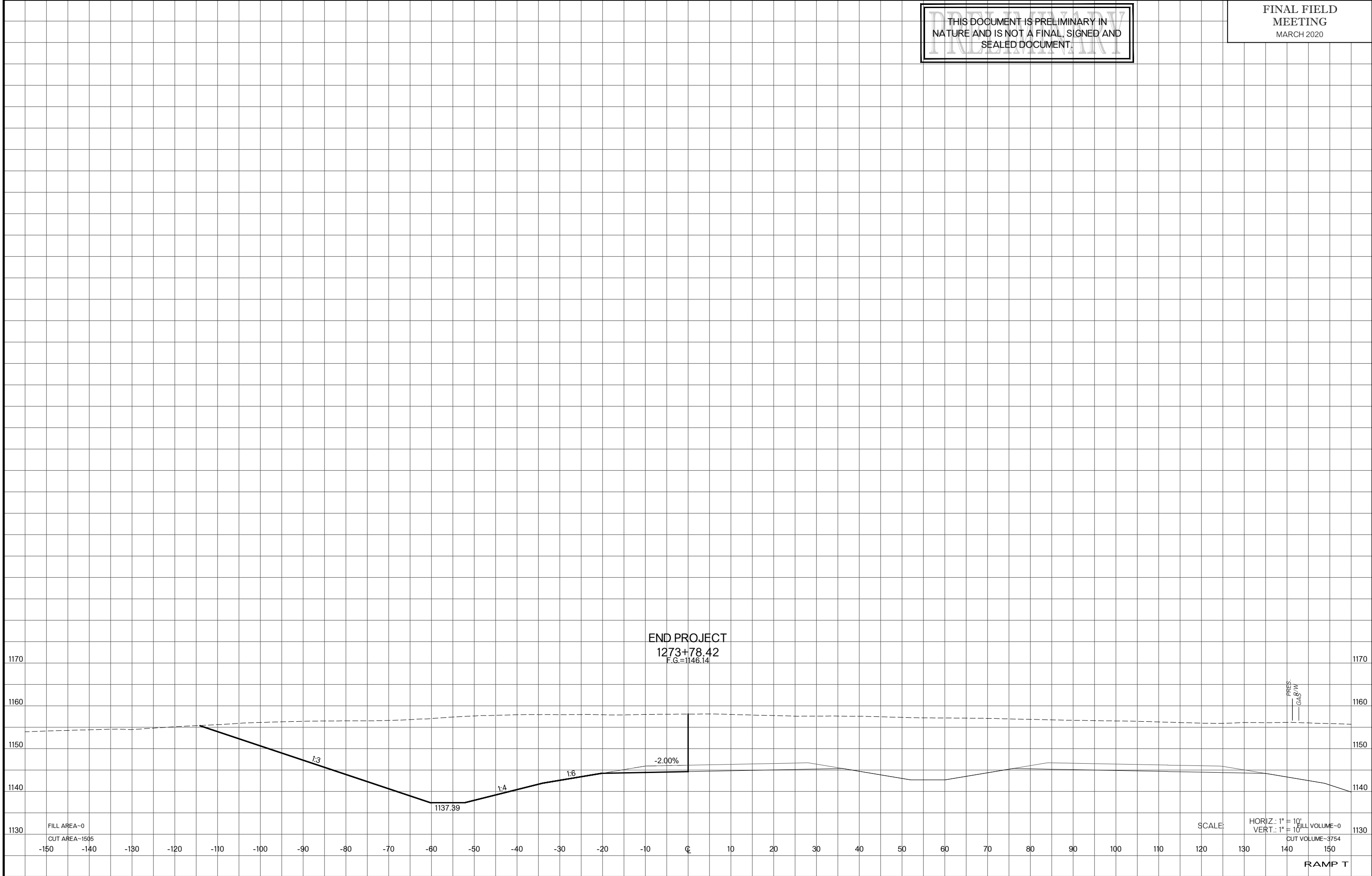
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

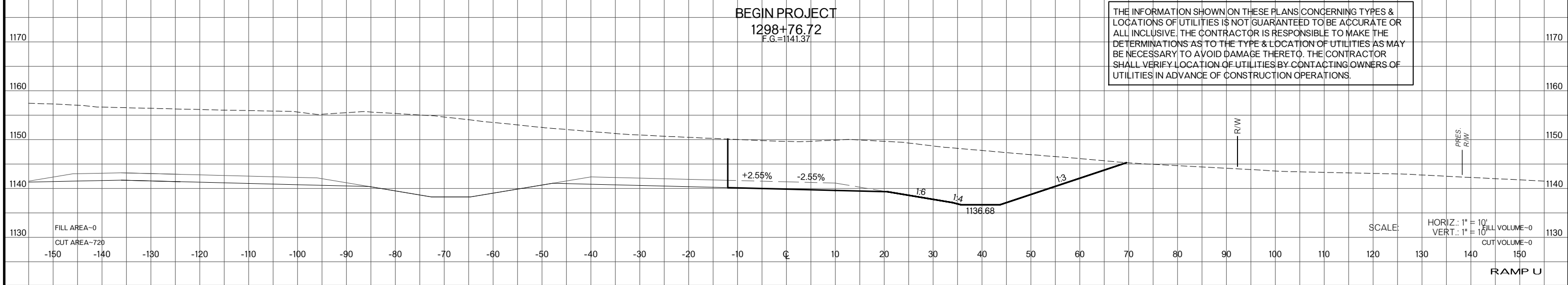
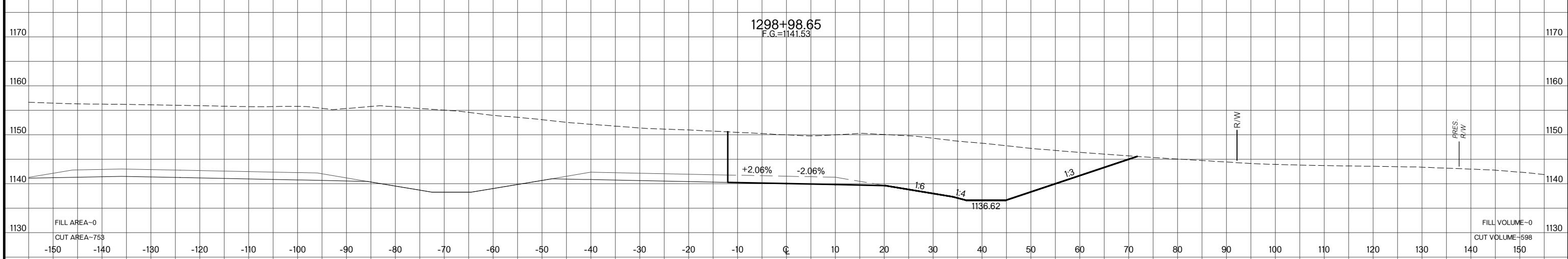
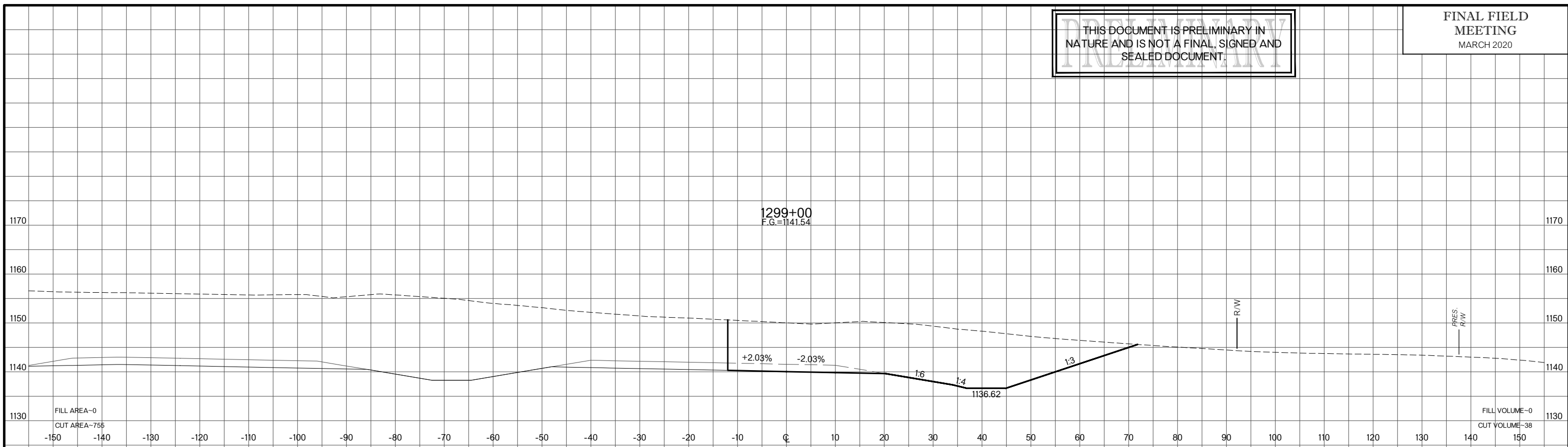
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



BEGIN PROJECT

THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

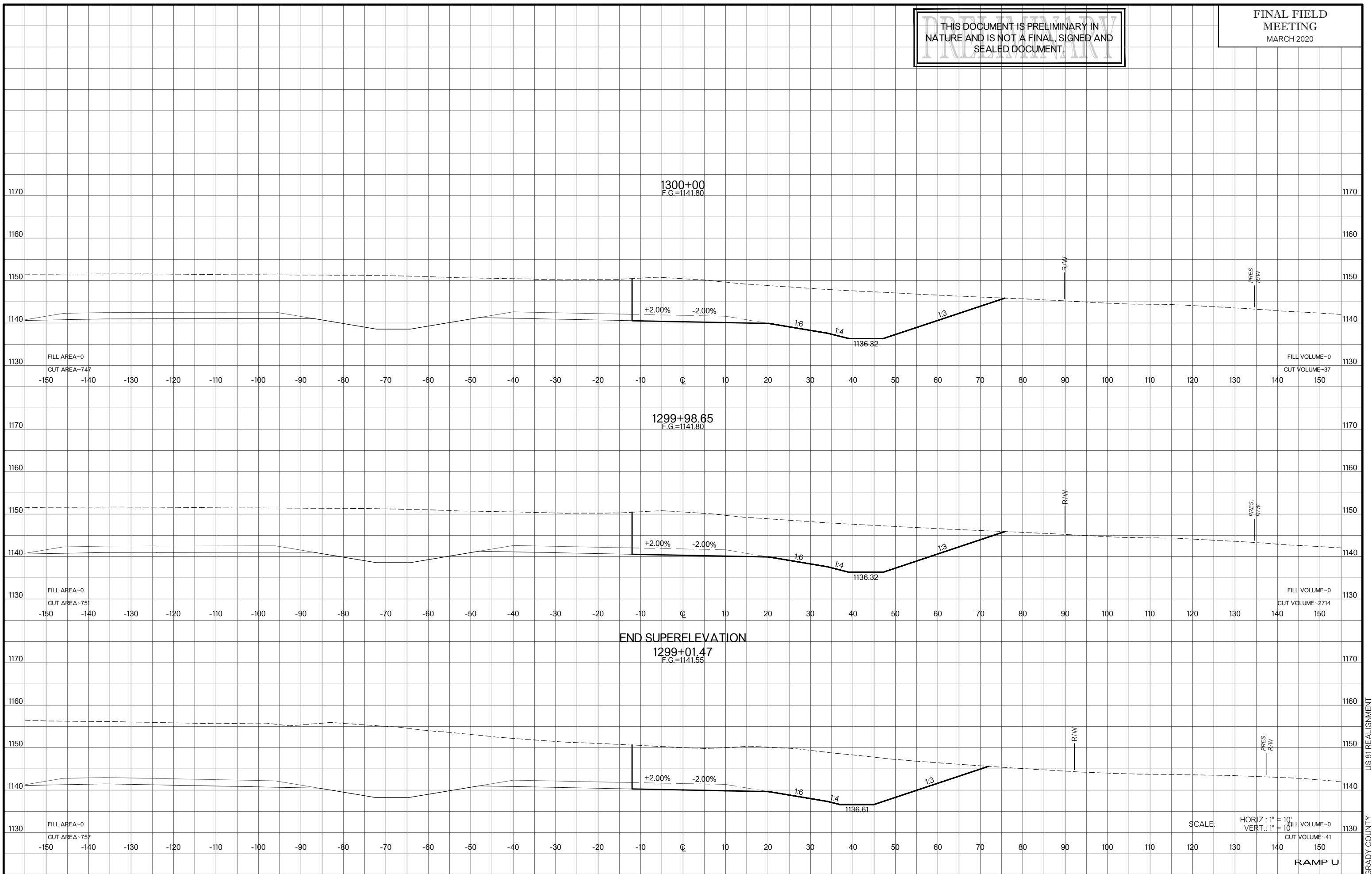
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP U

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



END SUPERELEVATION
1299+01.47
F.G.=1141.55

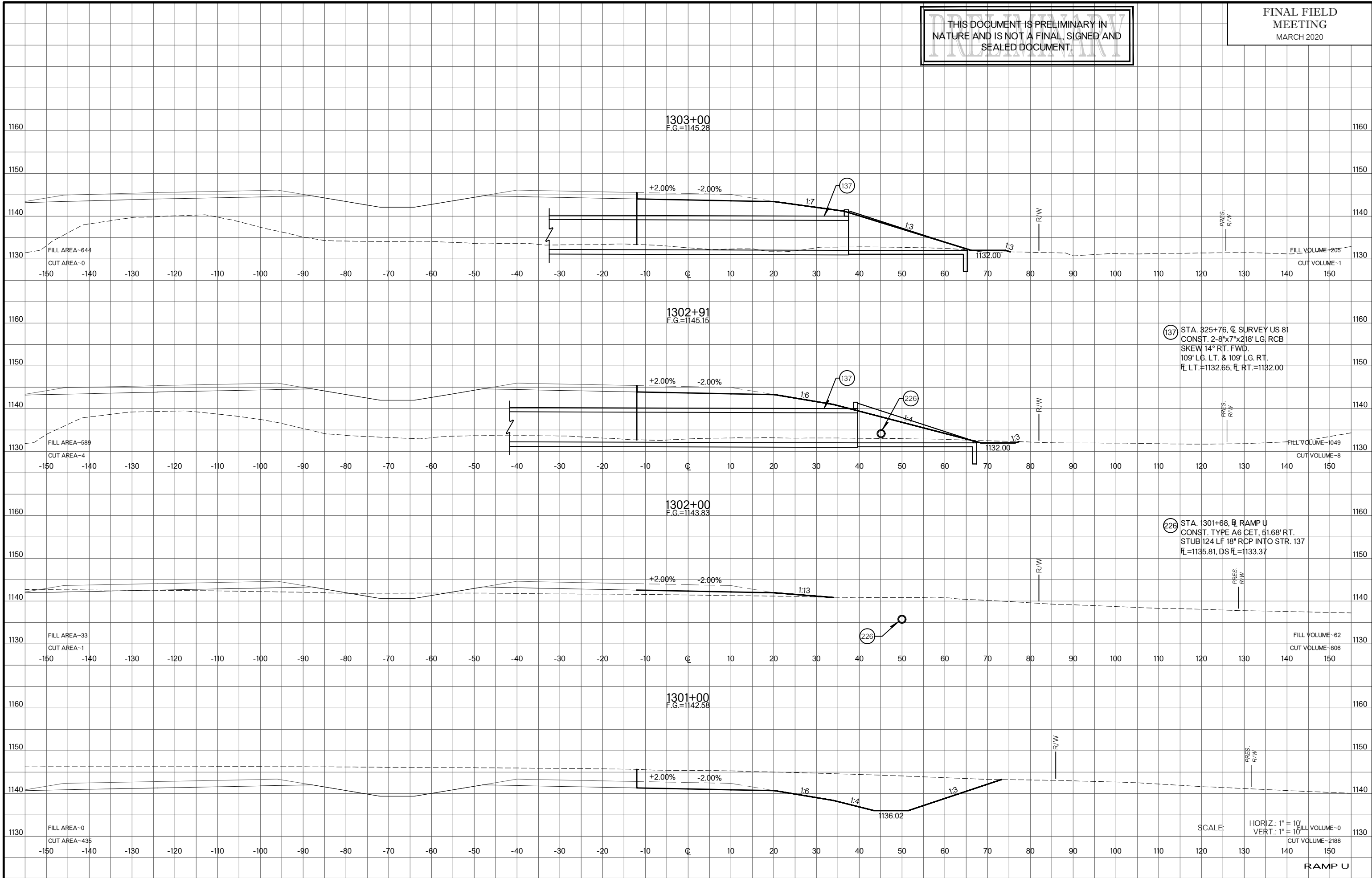
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP U

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



137 STA. 325+76, C SURVEY US 81
CONST. 2-8"x7"x218' LG. RCB
SKEW 14° RT. FWD.
109' LG. LT. & 109' LG. RT.
FL LT.=1132.65, FL RT.=1132.00

226 STA. 1301+68, R RAMP U
CONST. TYPE A6 CET, 51.68' RT.
STUB 124 LF 18" RCP INTO STR. 137
FL=1135.81, DS FL=1133.37

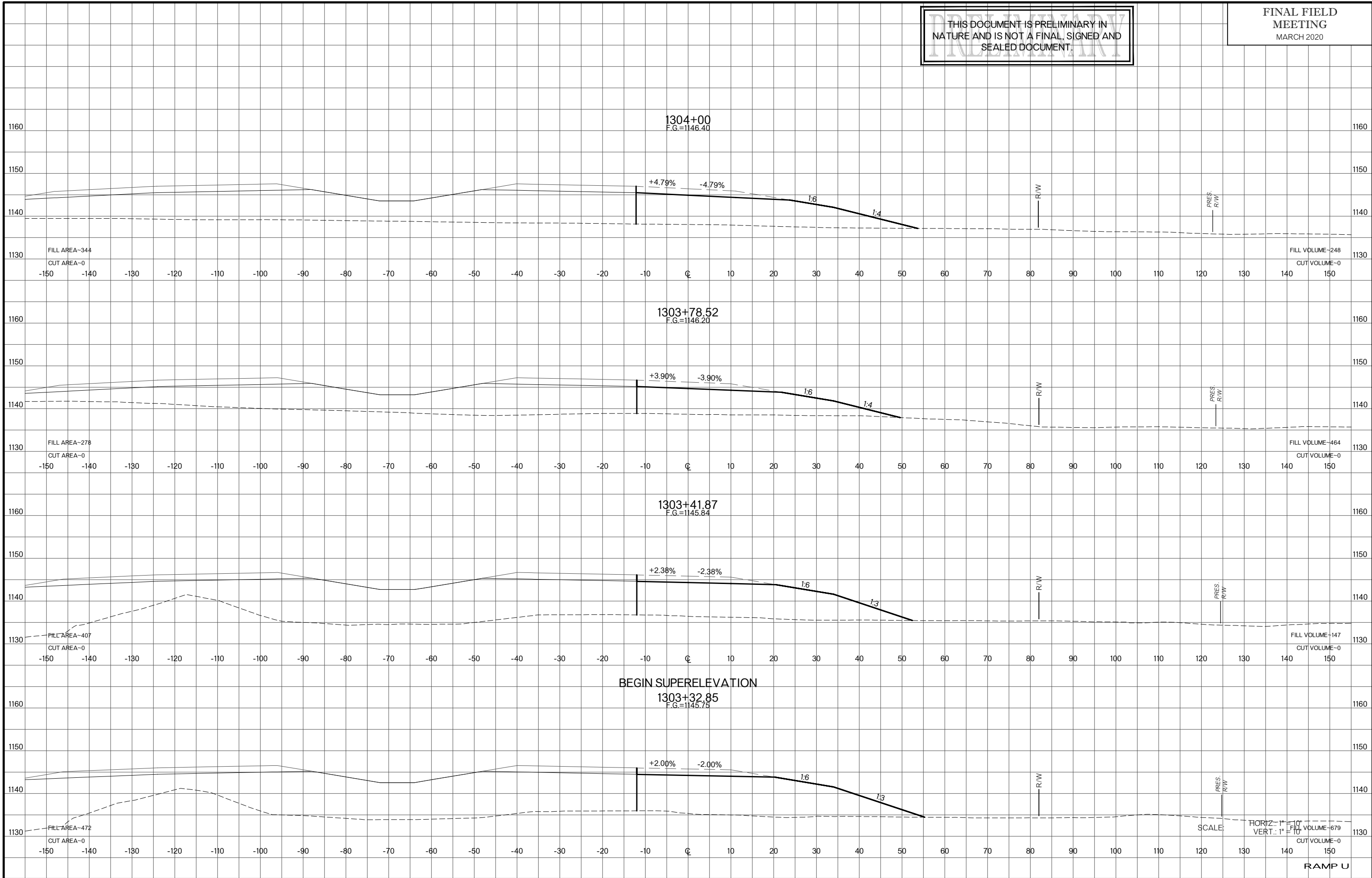
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP U

US 81 REALIGNMENT
GRADY COUNTY

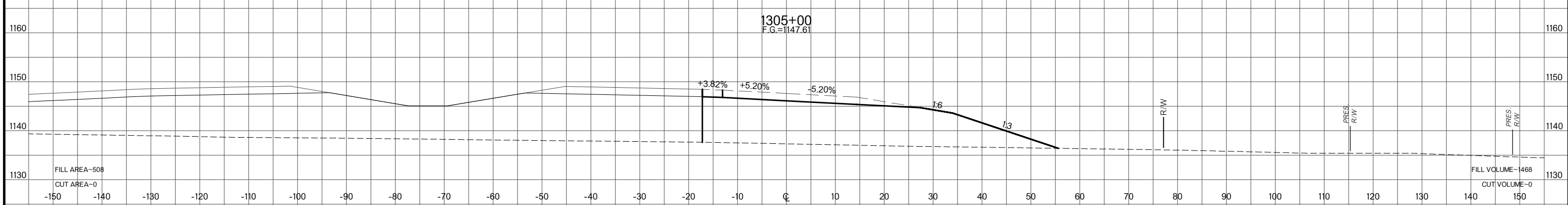
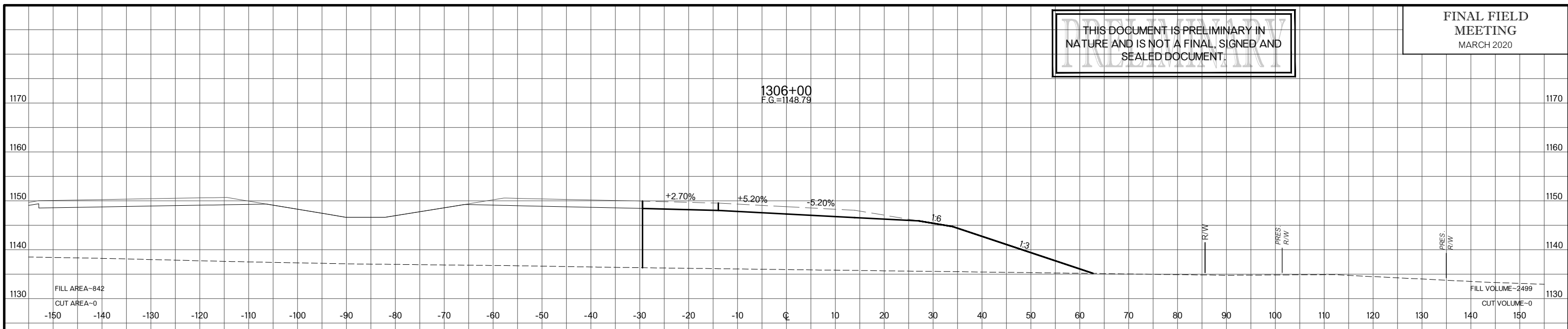
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

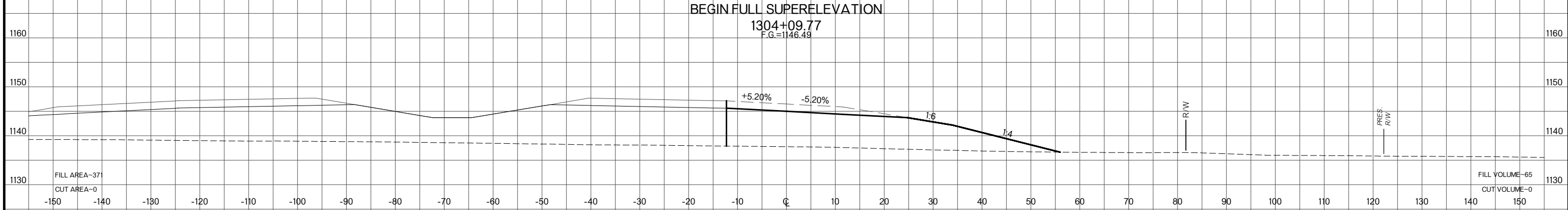


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

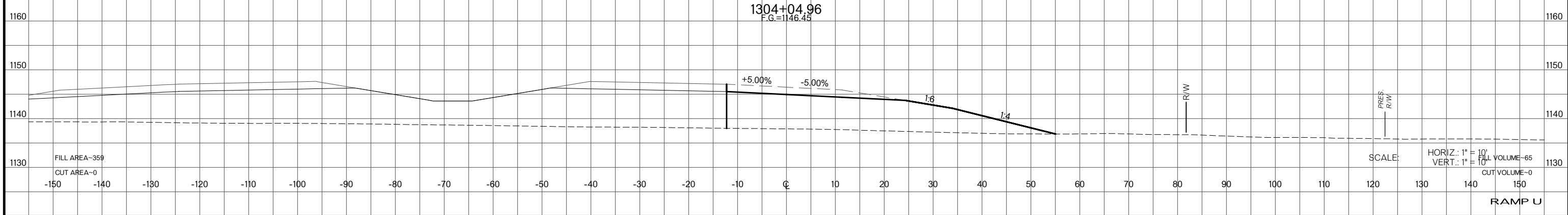
FINAL FIELD MEETING
MARCH 2020



BEGIN FULL SUPERELEVATION



BEGIN OUTSIDE SHOULDER ROTATION

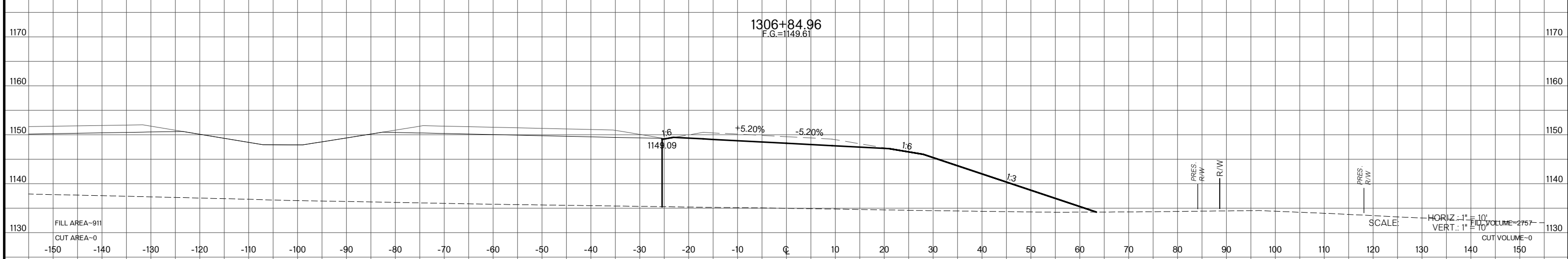
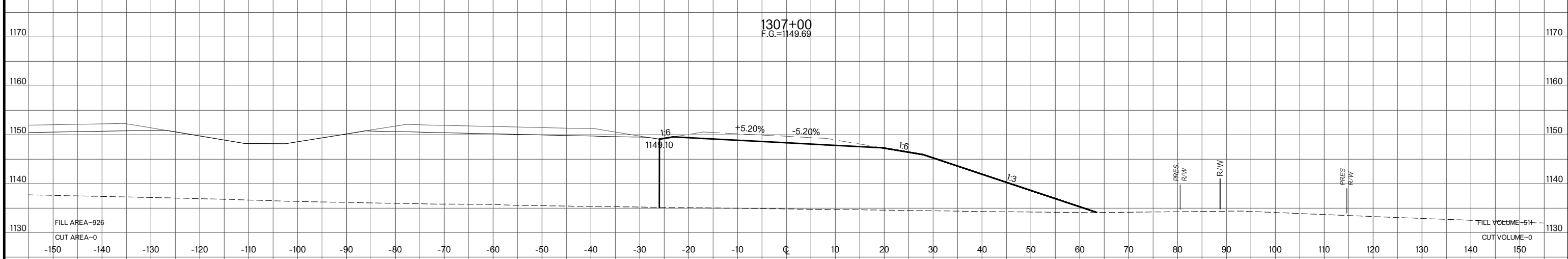


RAMP U

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

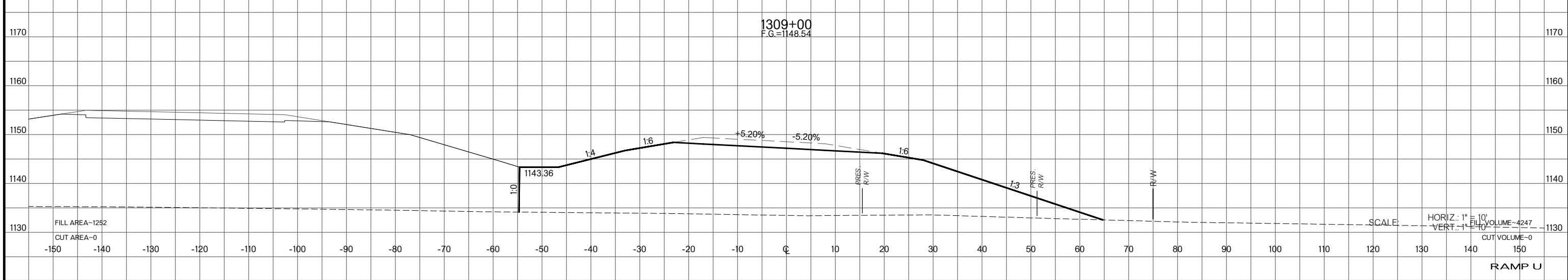
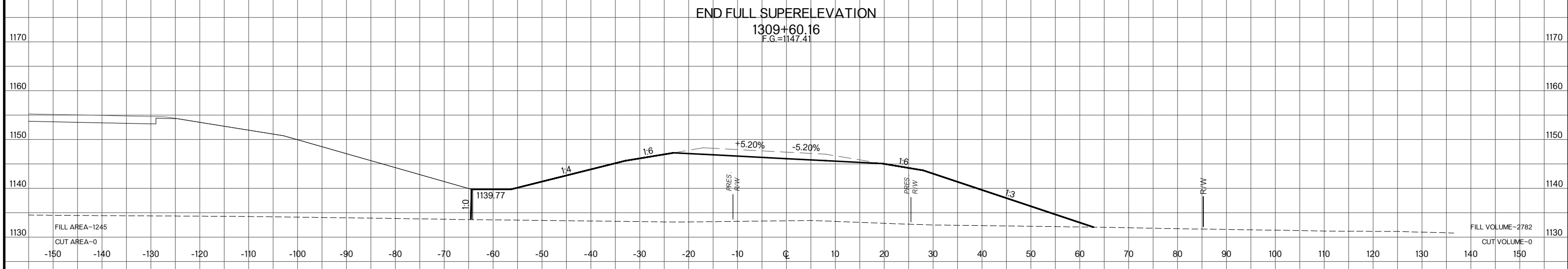
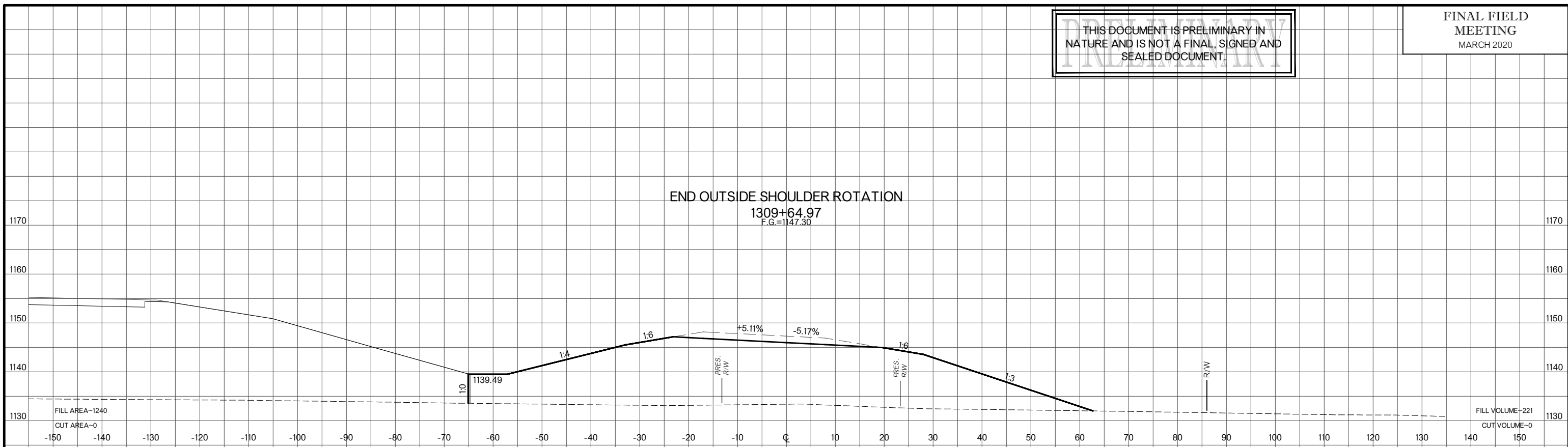


US 81 REALIGNMENT
GRADY COUNTY

RAMP U

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

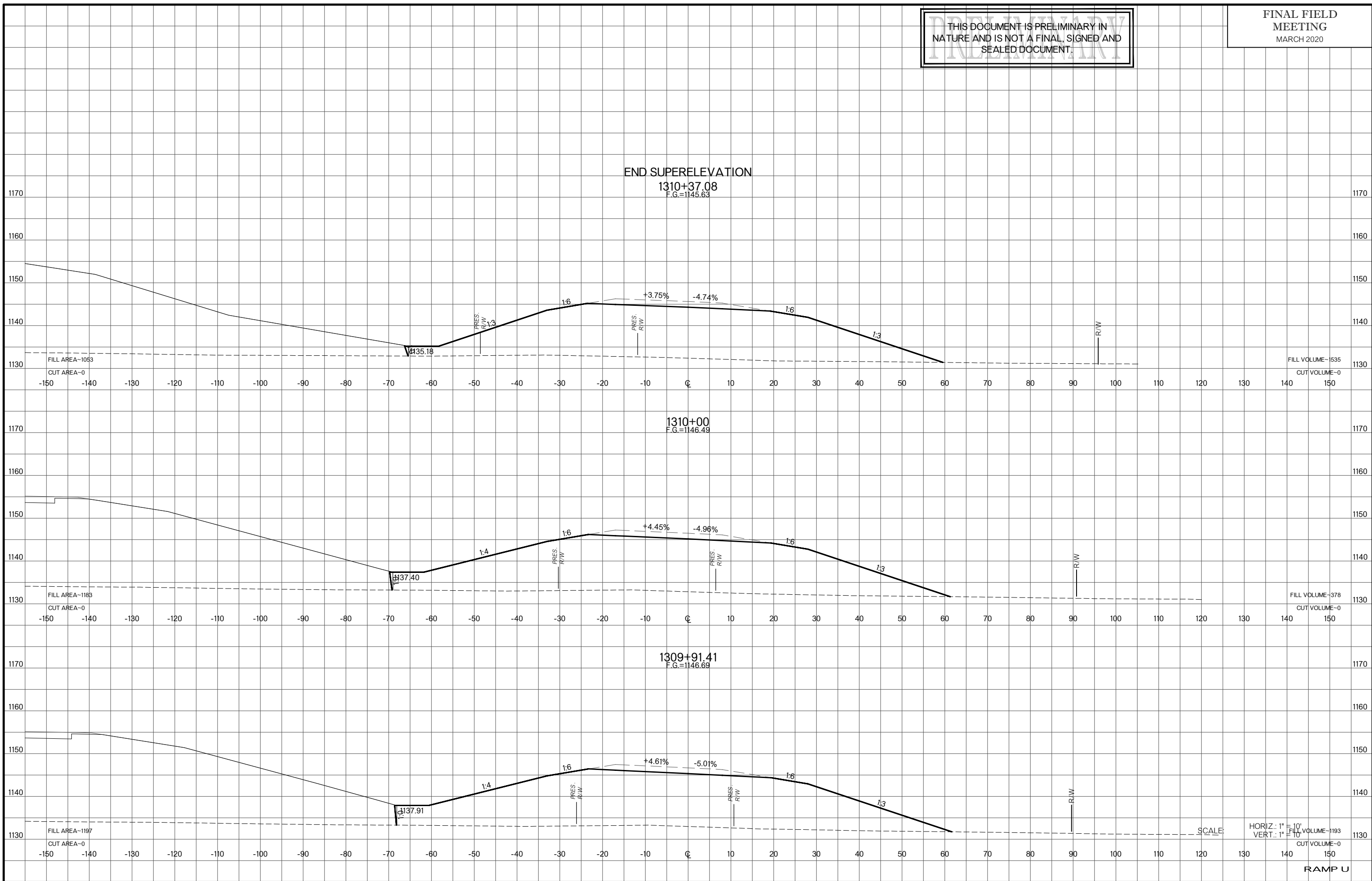


US 81 REALIGNMENT
GRADY COUNTY

RAMP U

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



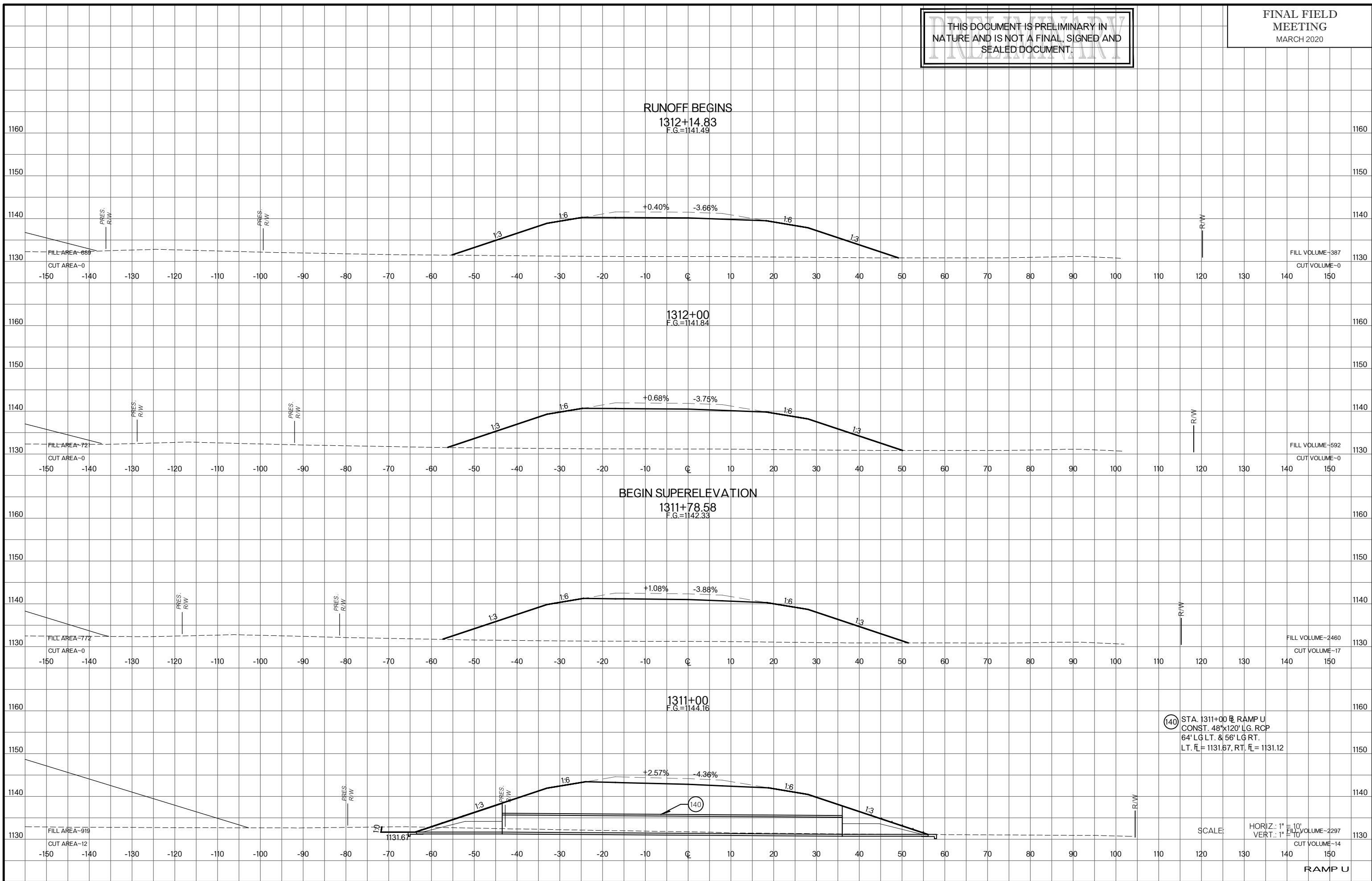
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

RAMP U

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

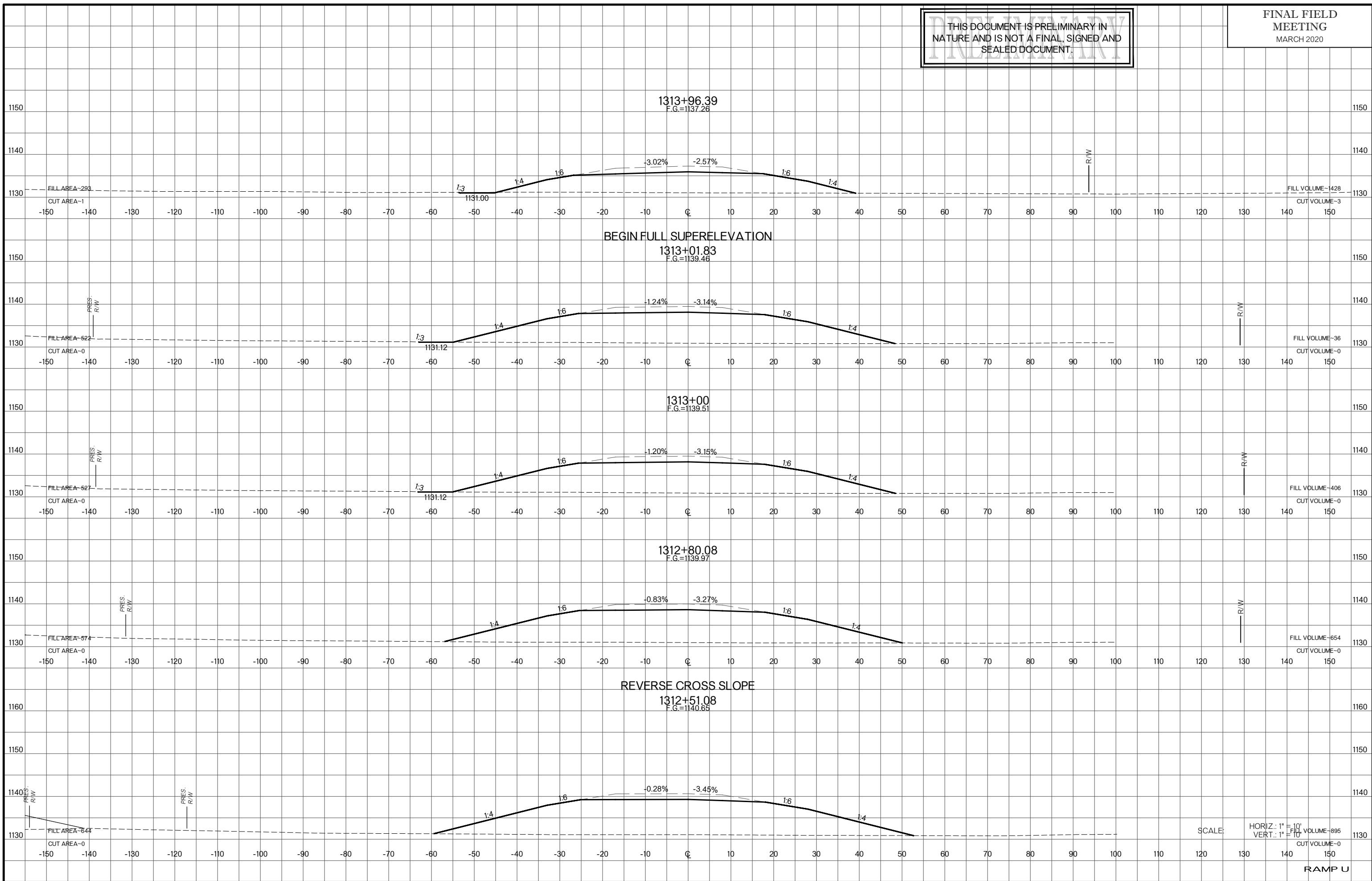
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

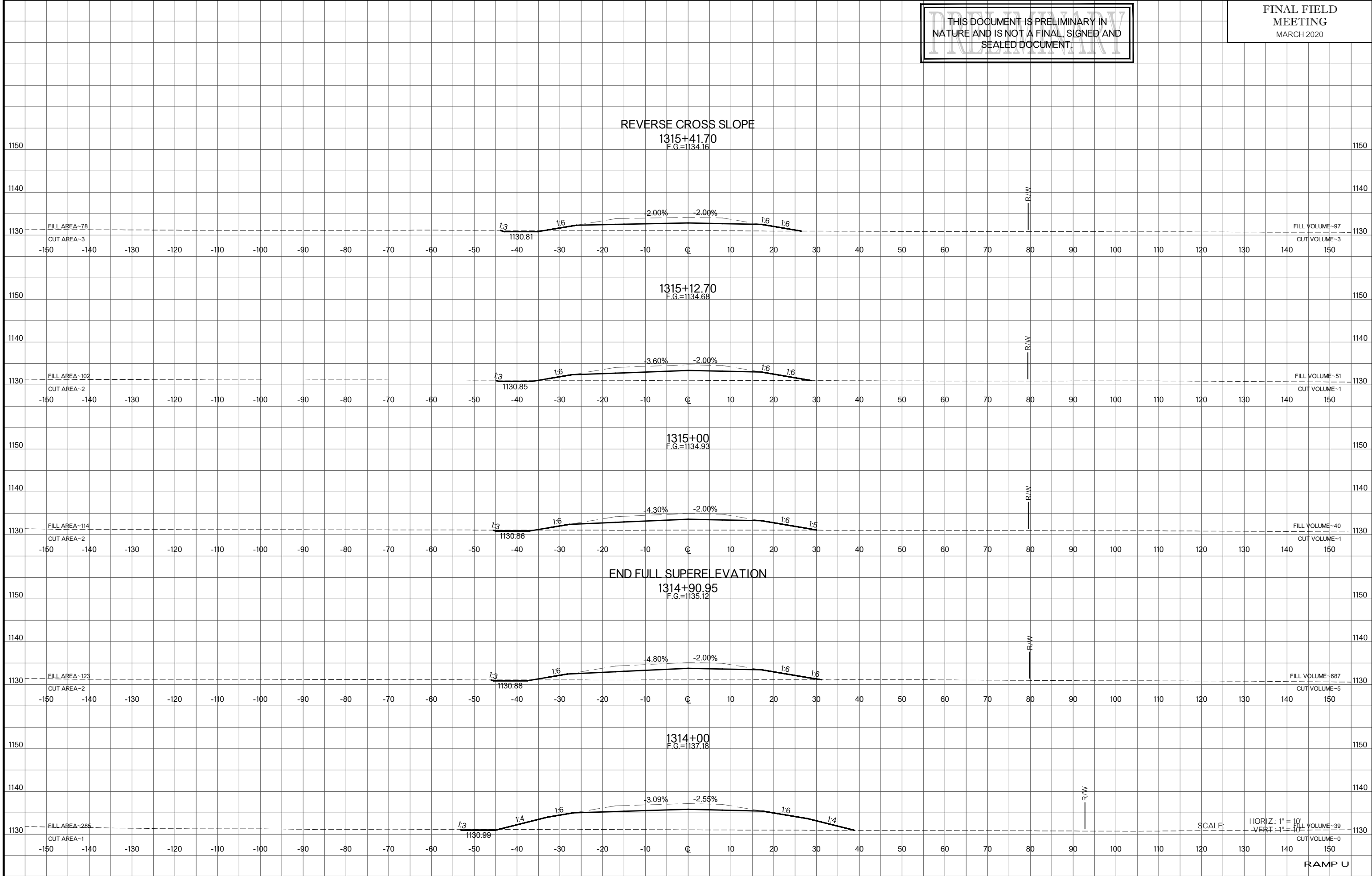
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



REVERSE CROSS SLOPE

1315+41.70
F.G.=1134.16

1315+12.70
F.G.=1134.68

1315+00
F.G.=1134.93

END FULL SUPERELEVATION

1314+90.95
F.G.=1135.12

1314+00
F.G.=1137.18

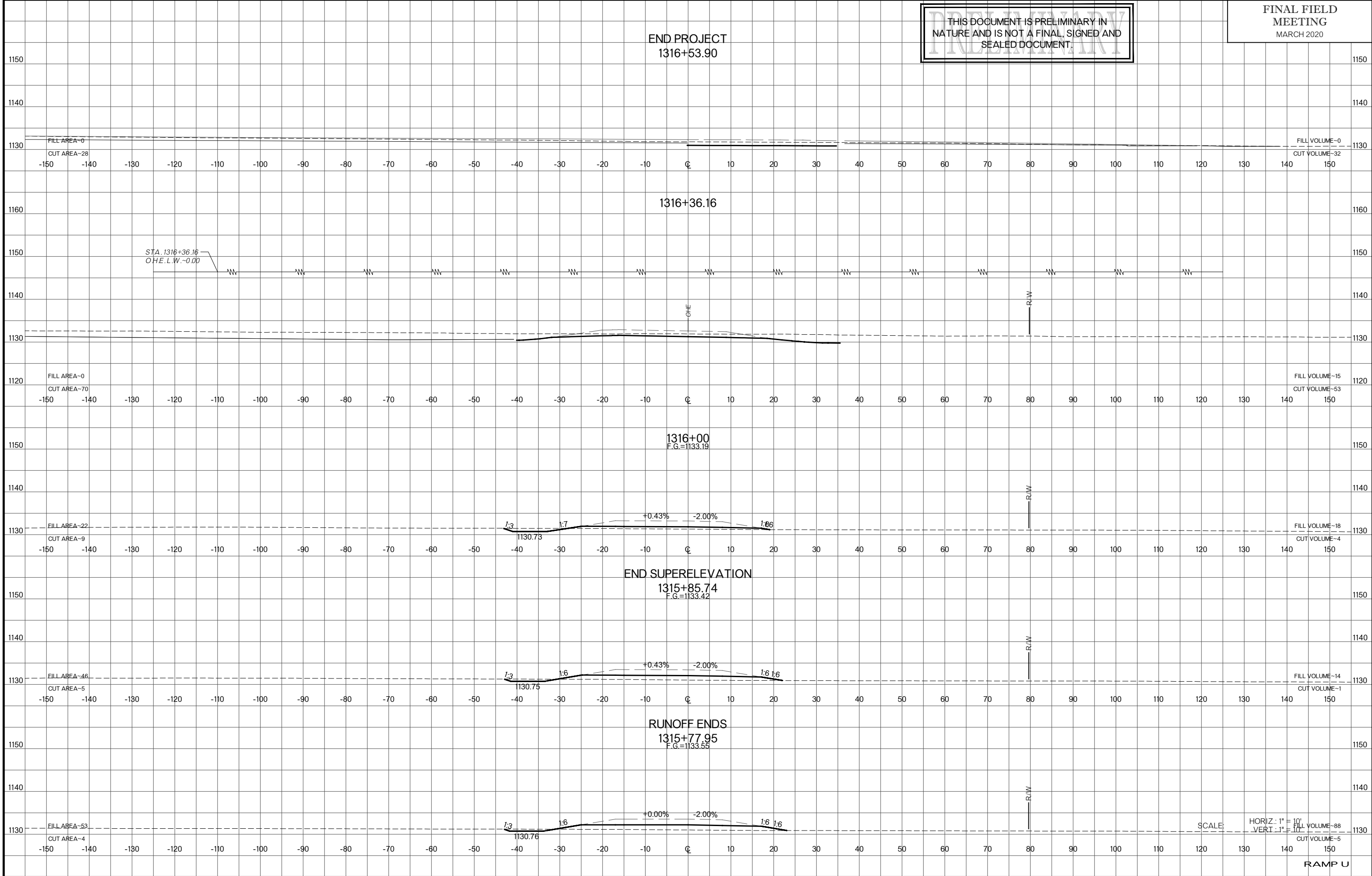
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP U

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



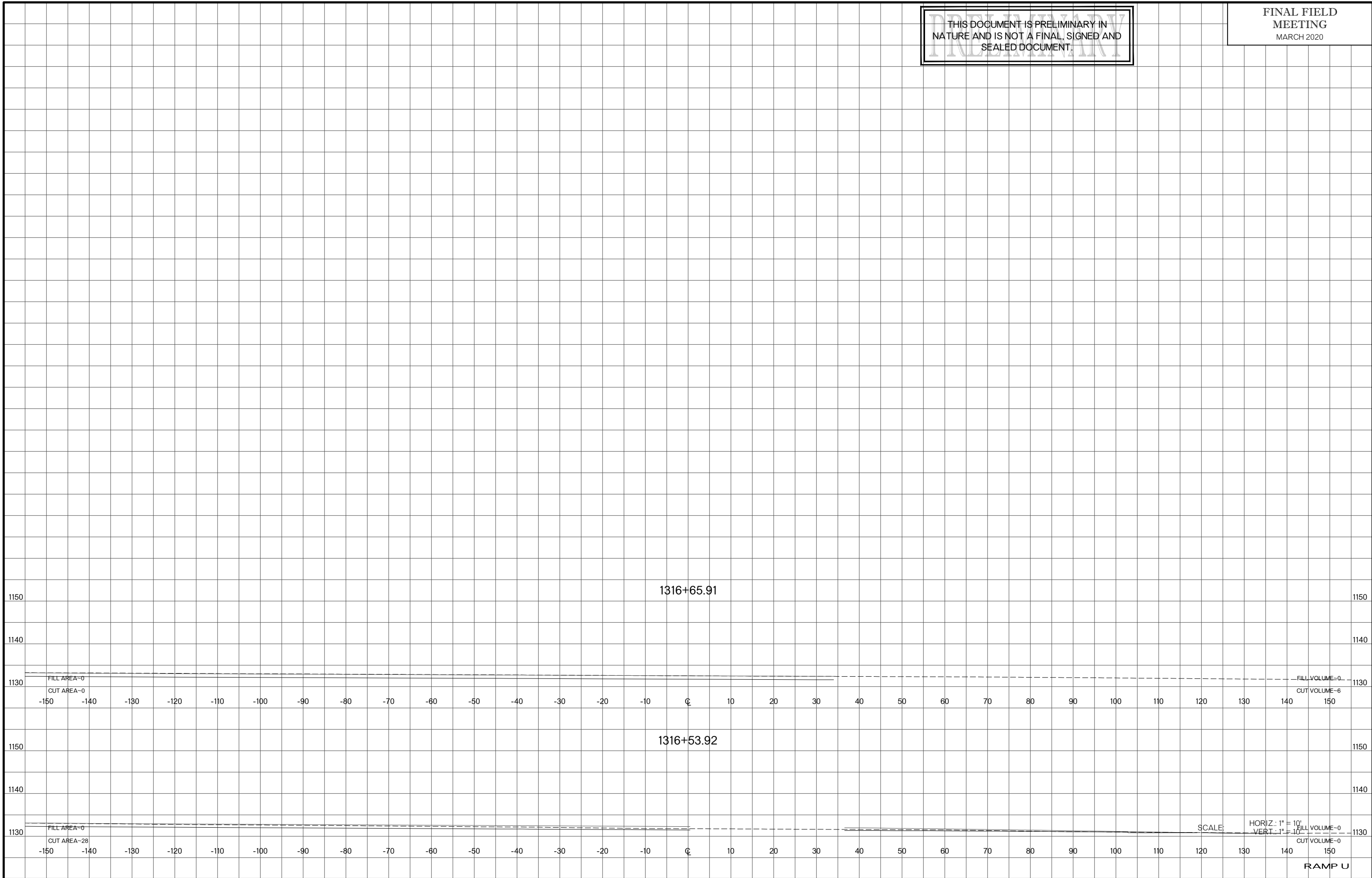
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP U

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

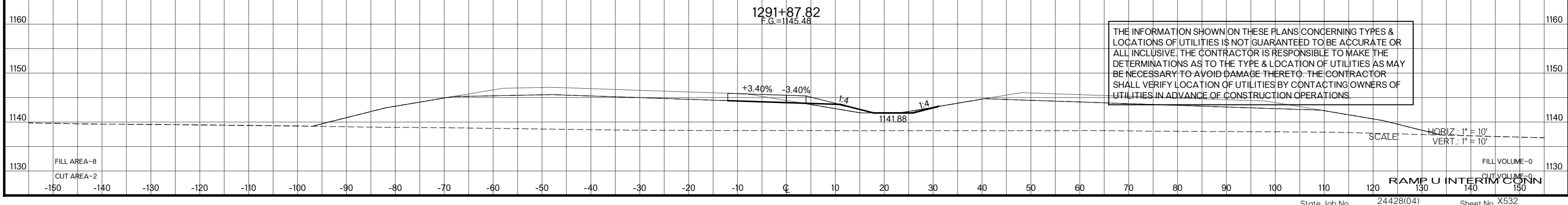
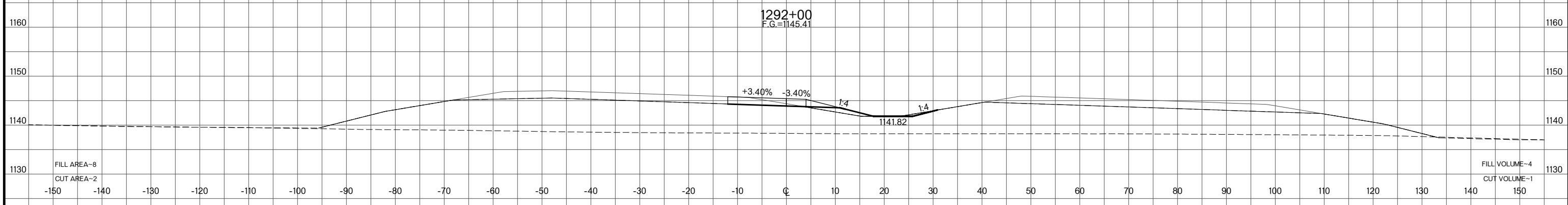
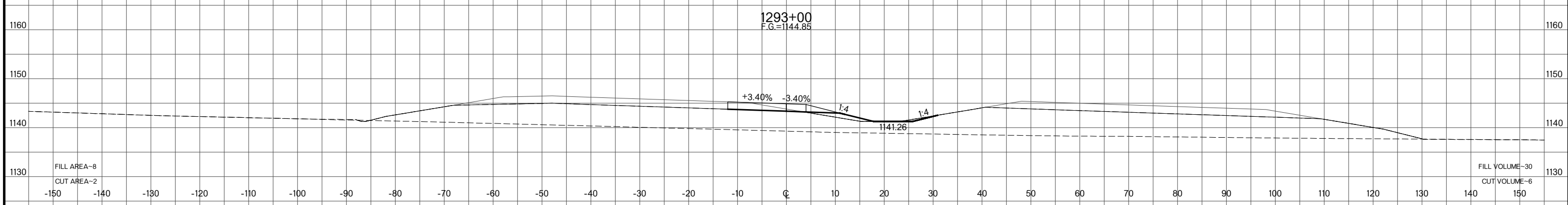
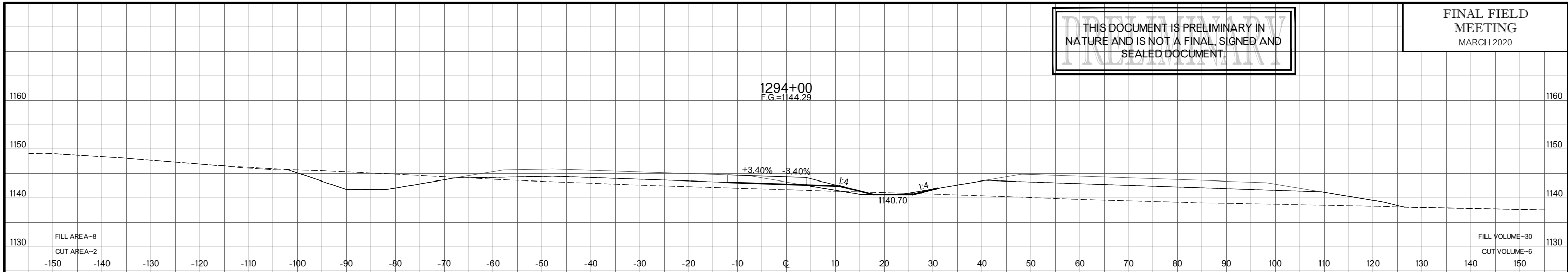
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



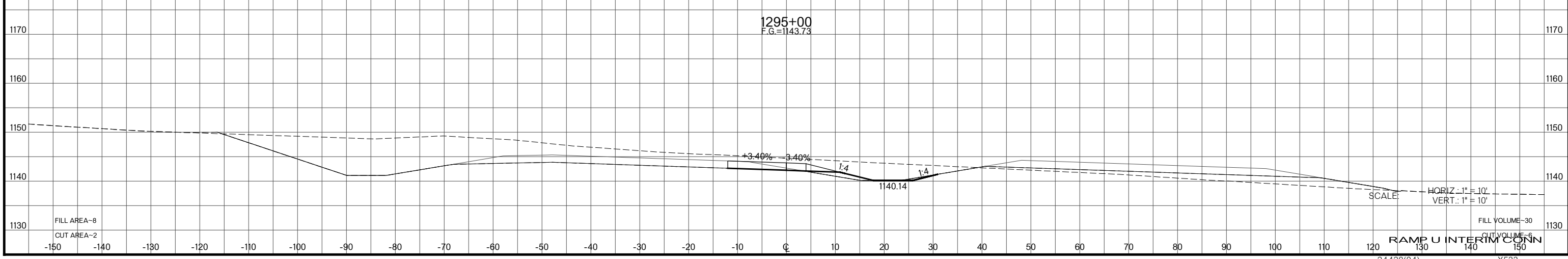
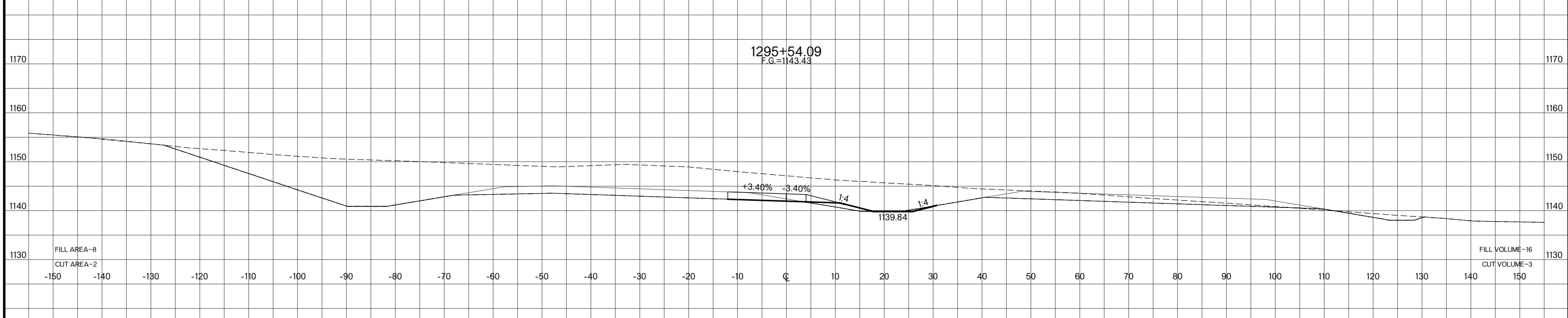
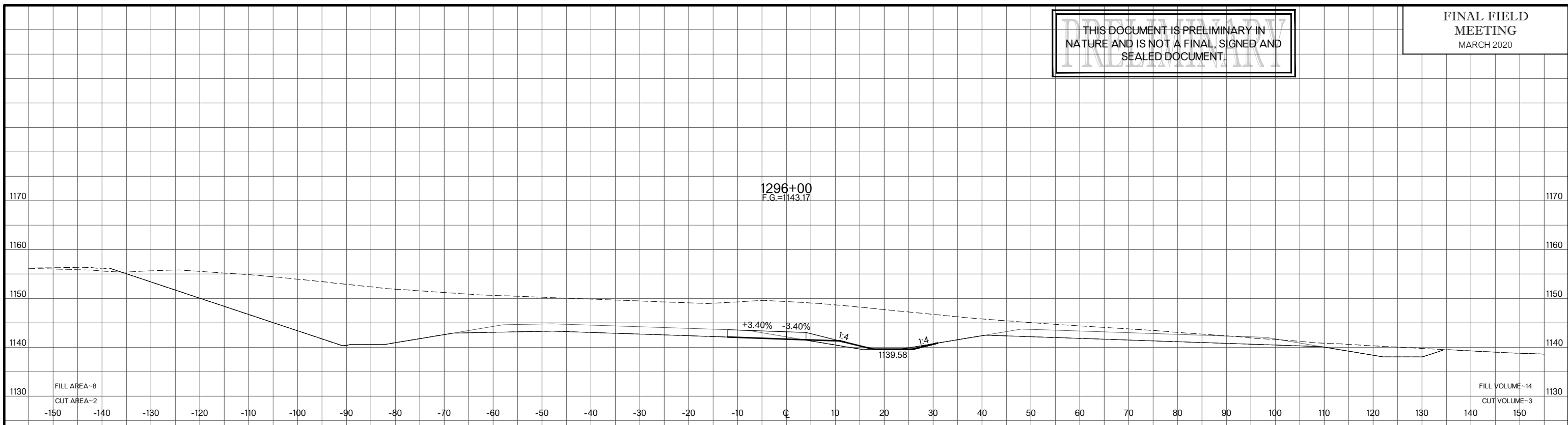
THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

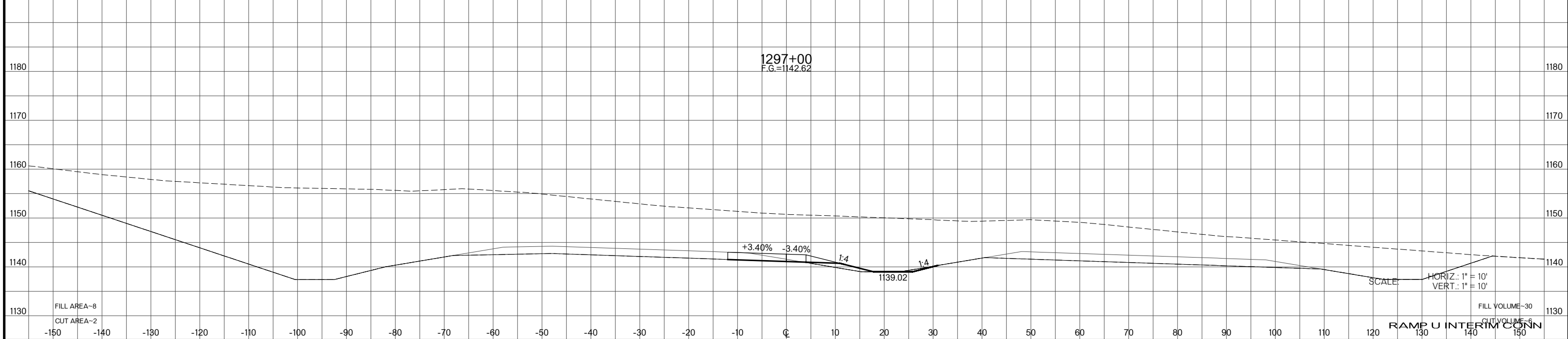
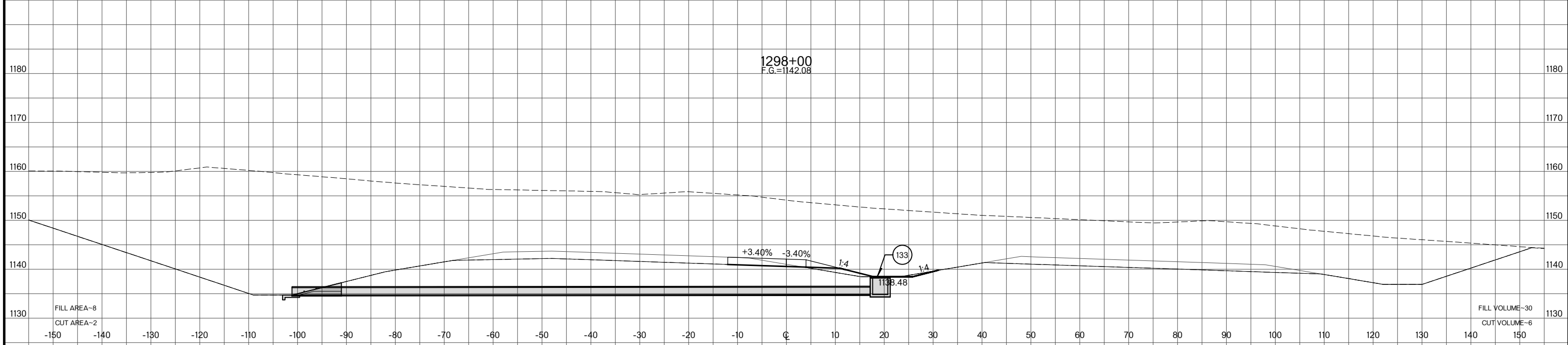
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



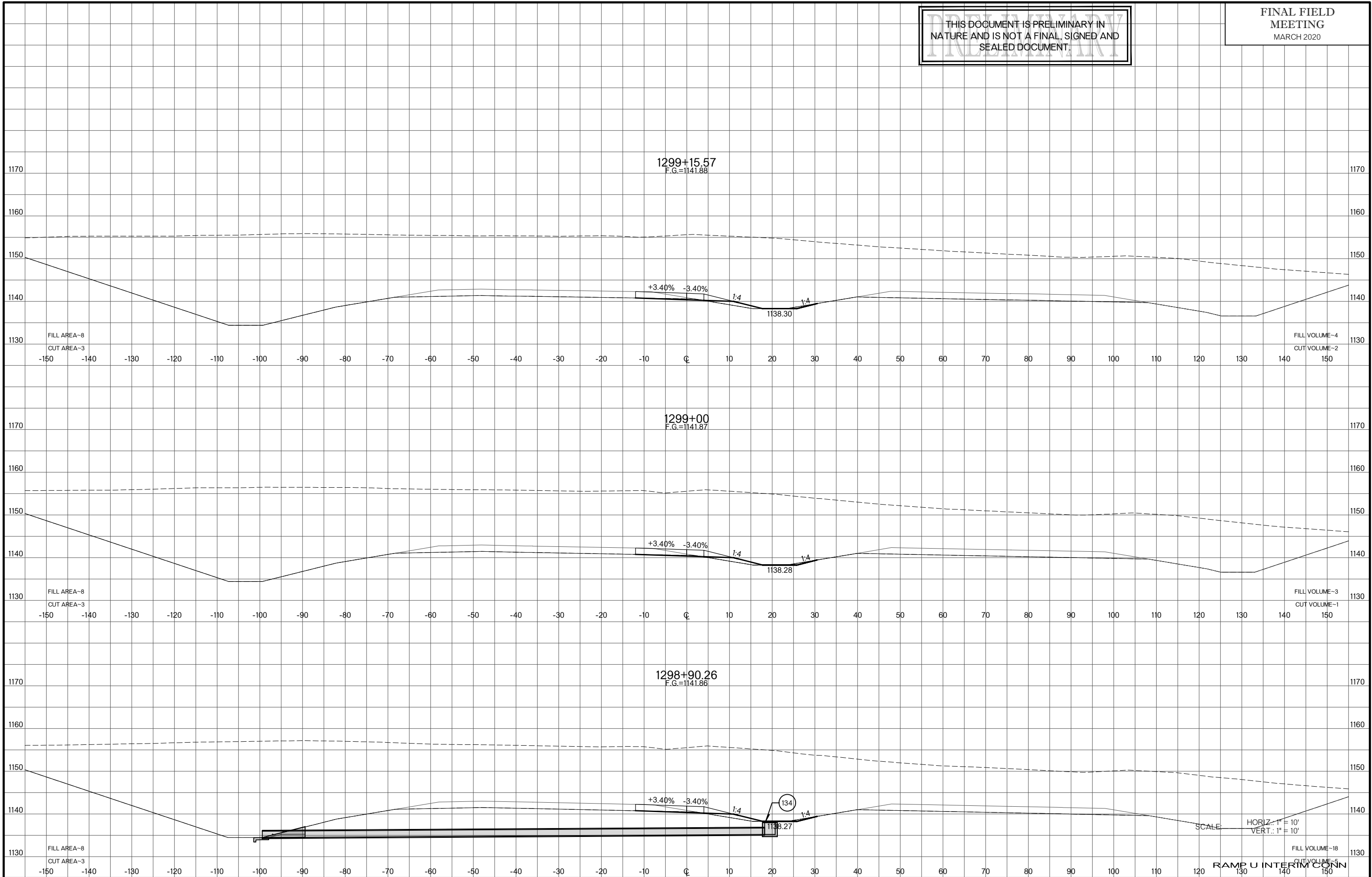
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

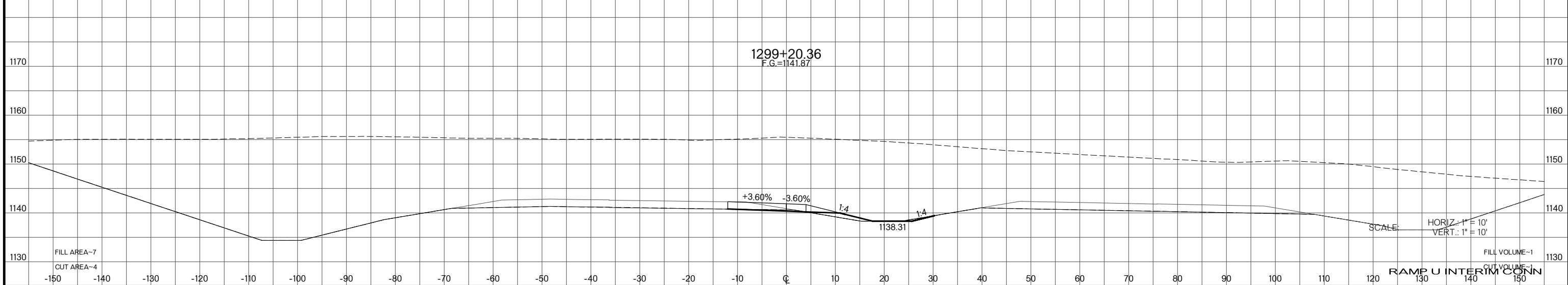
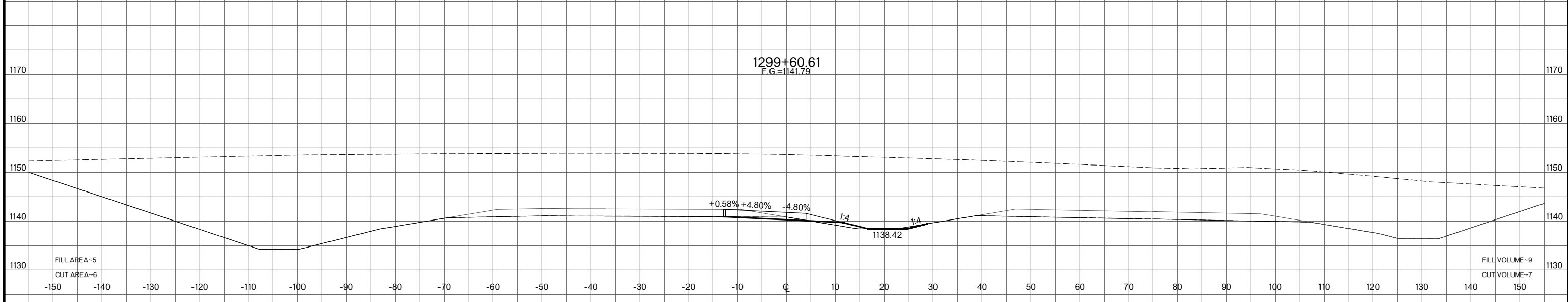
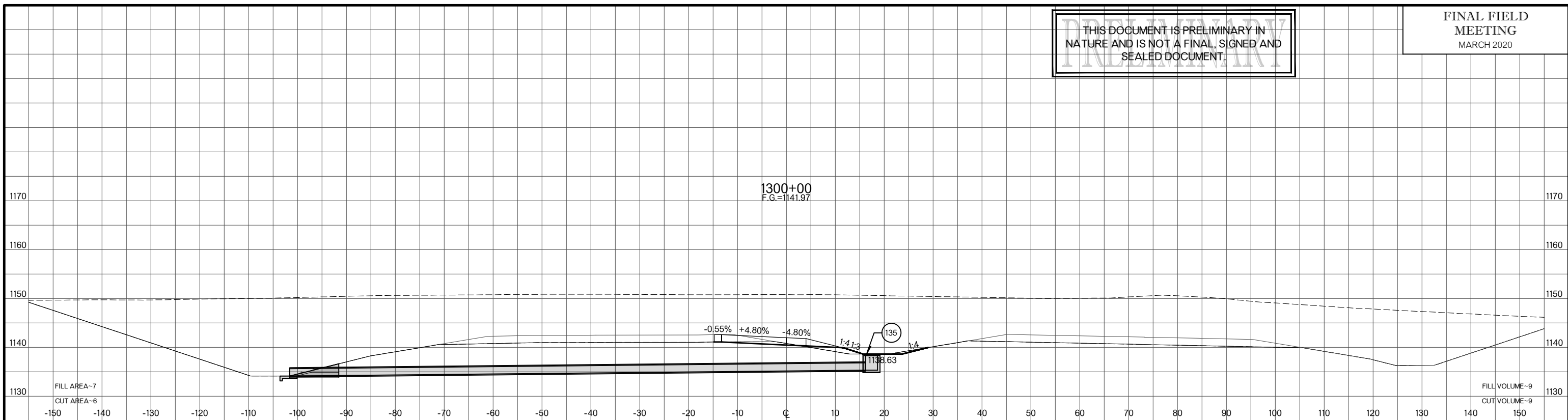
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

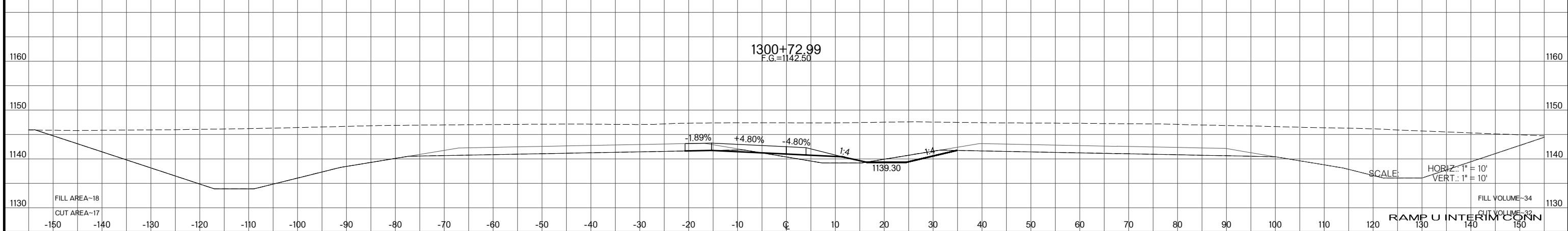
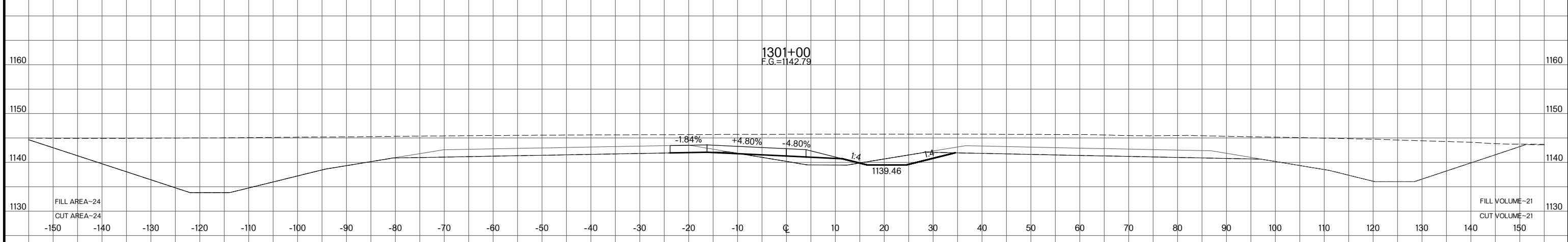
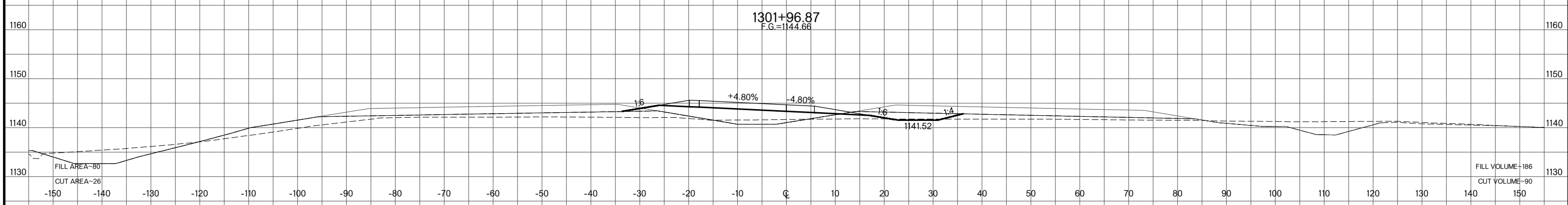
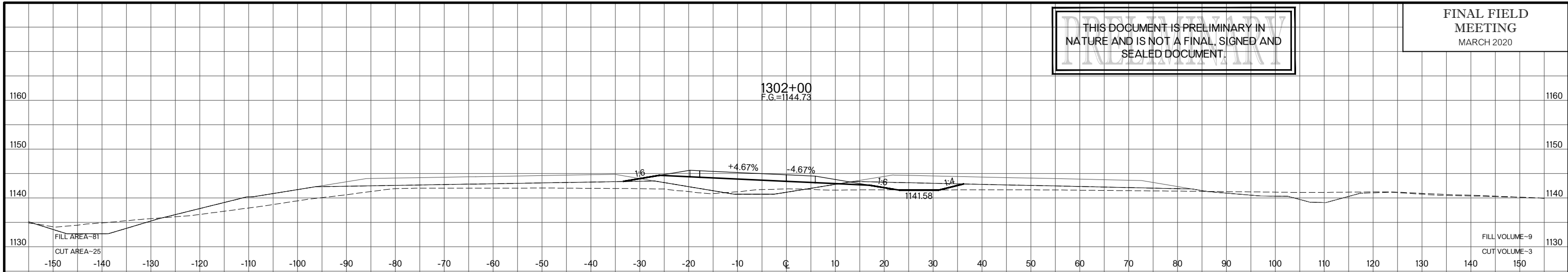
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

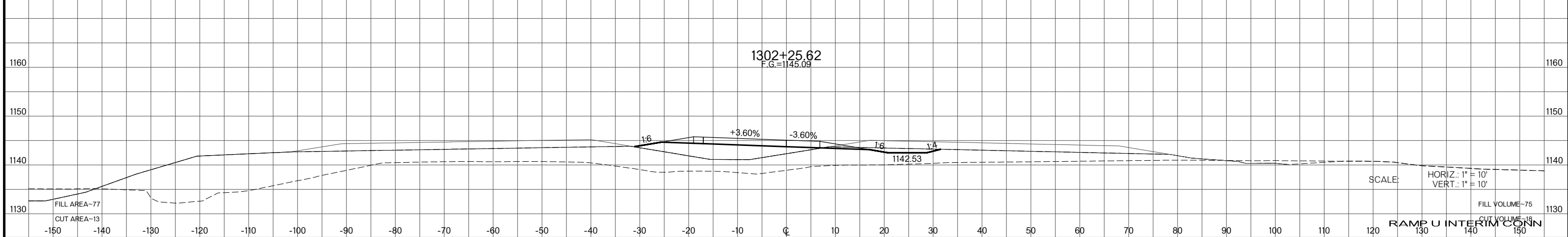
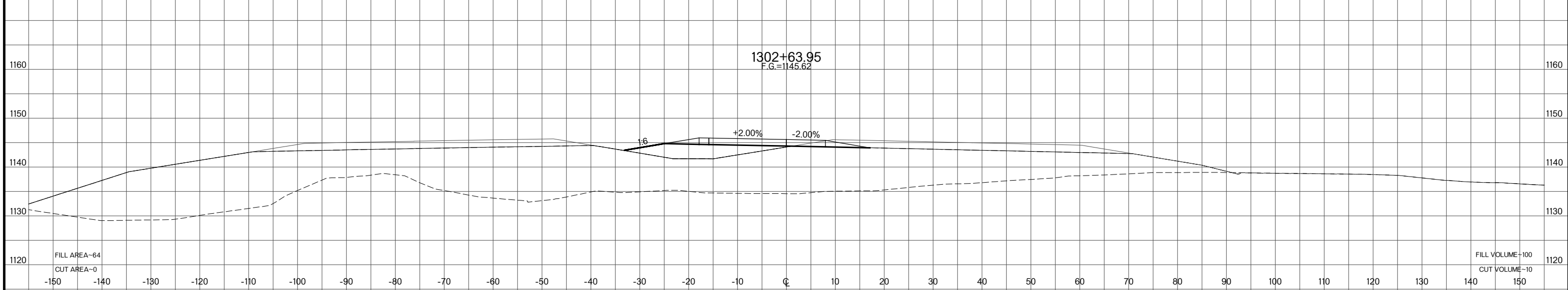
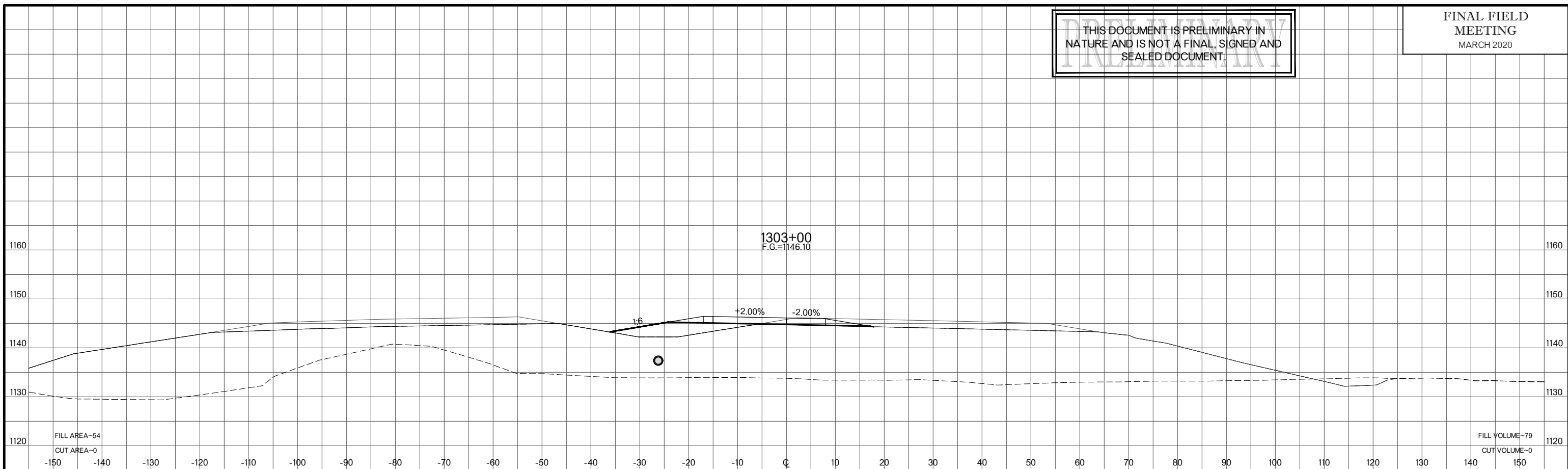
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

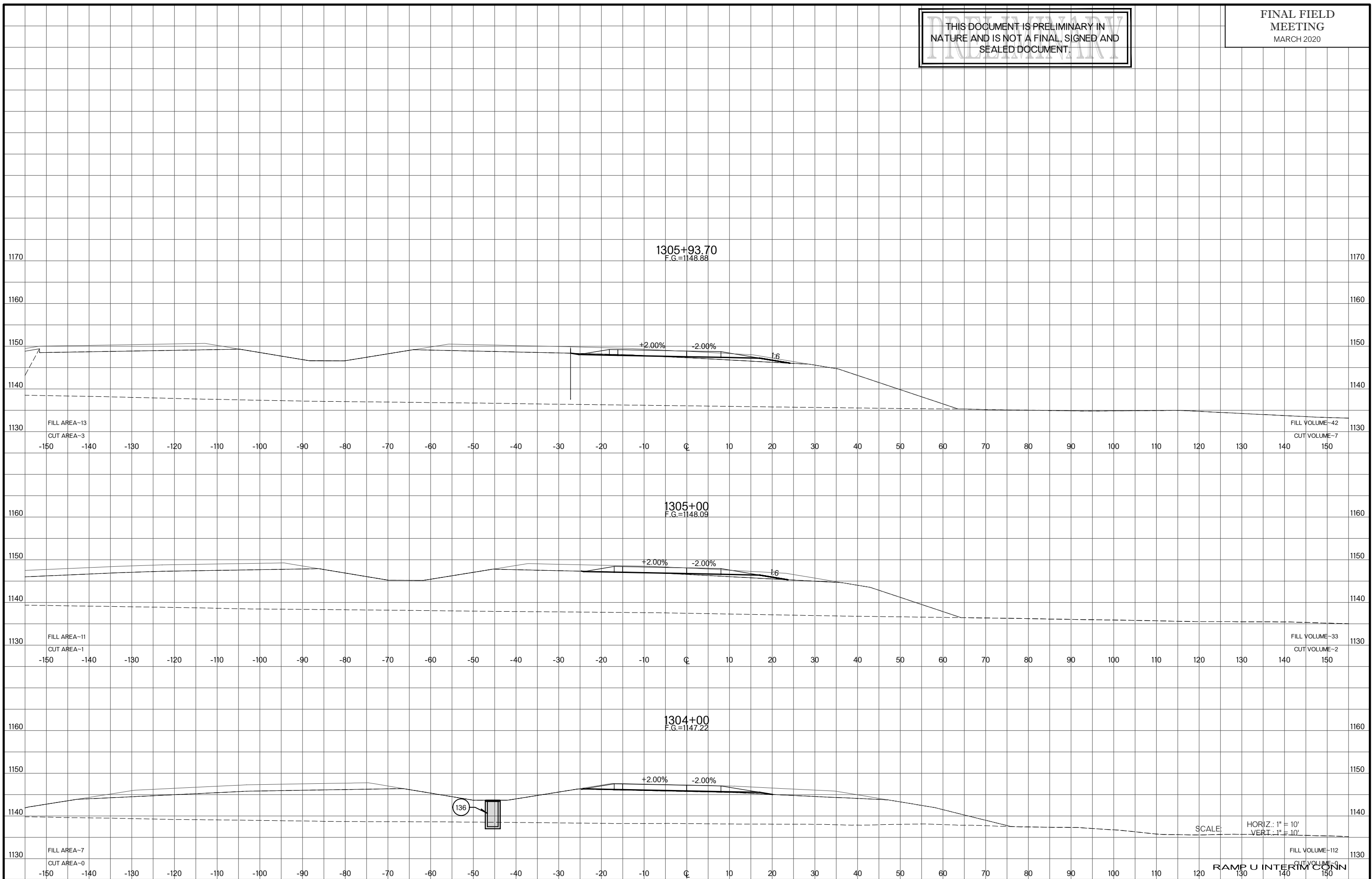
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

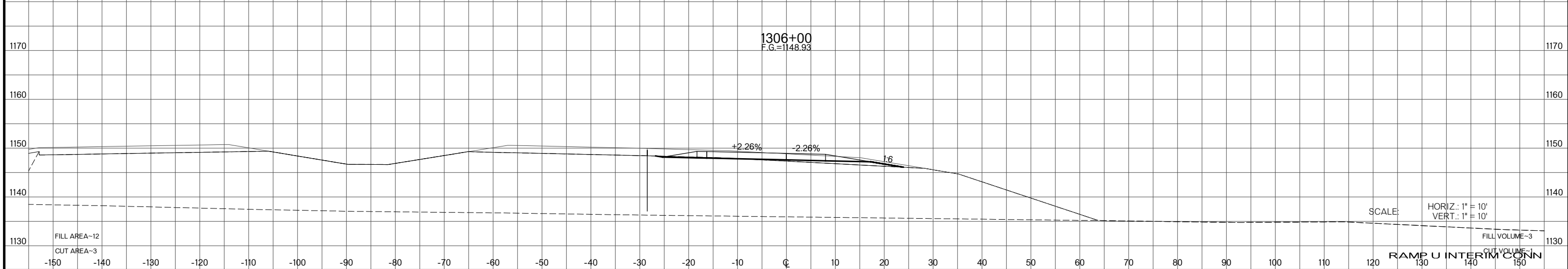
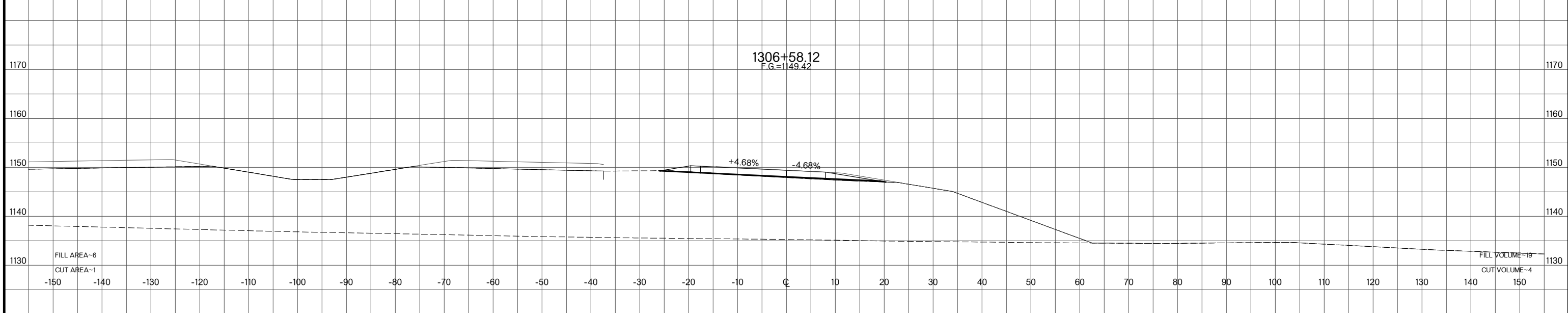
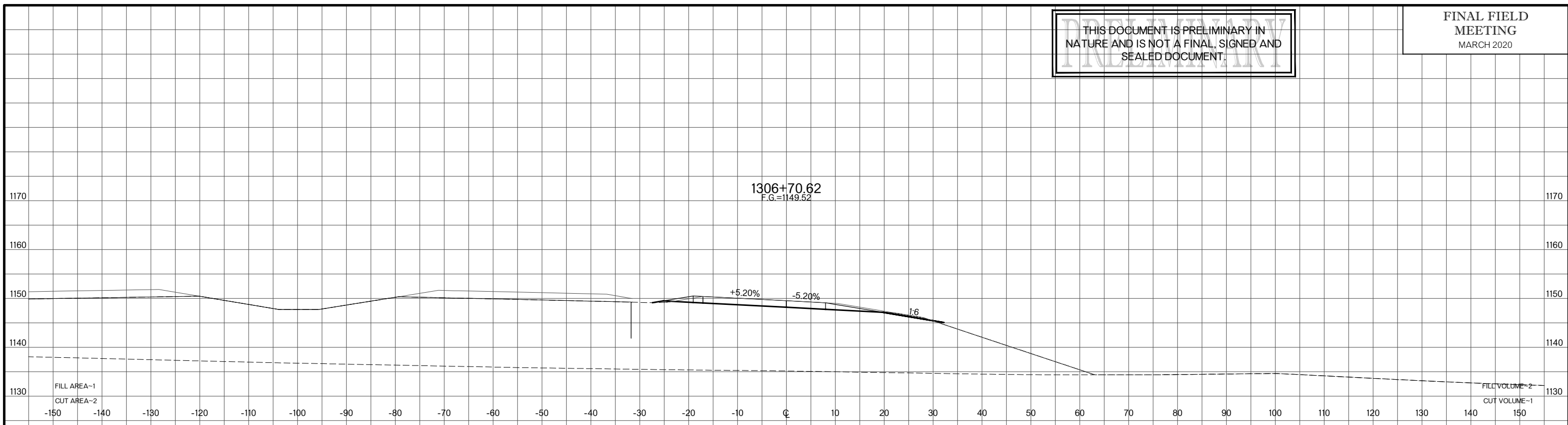
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



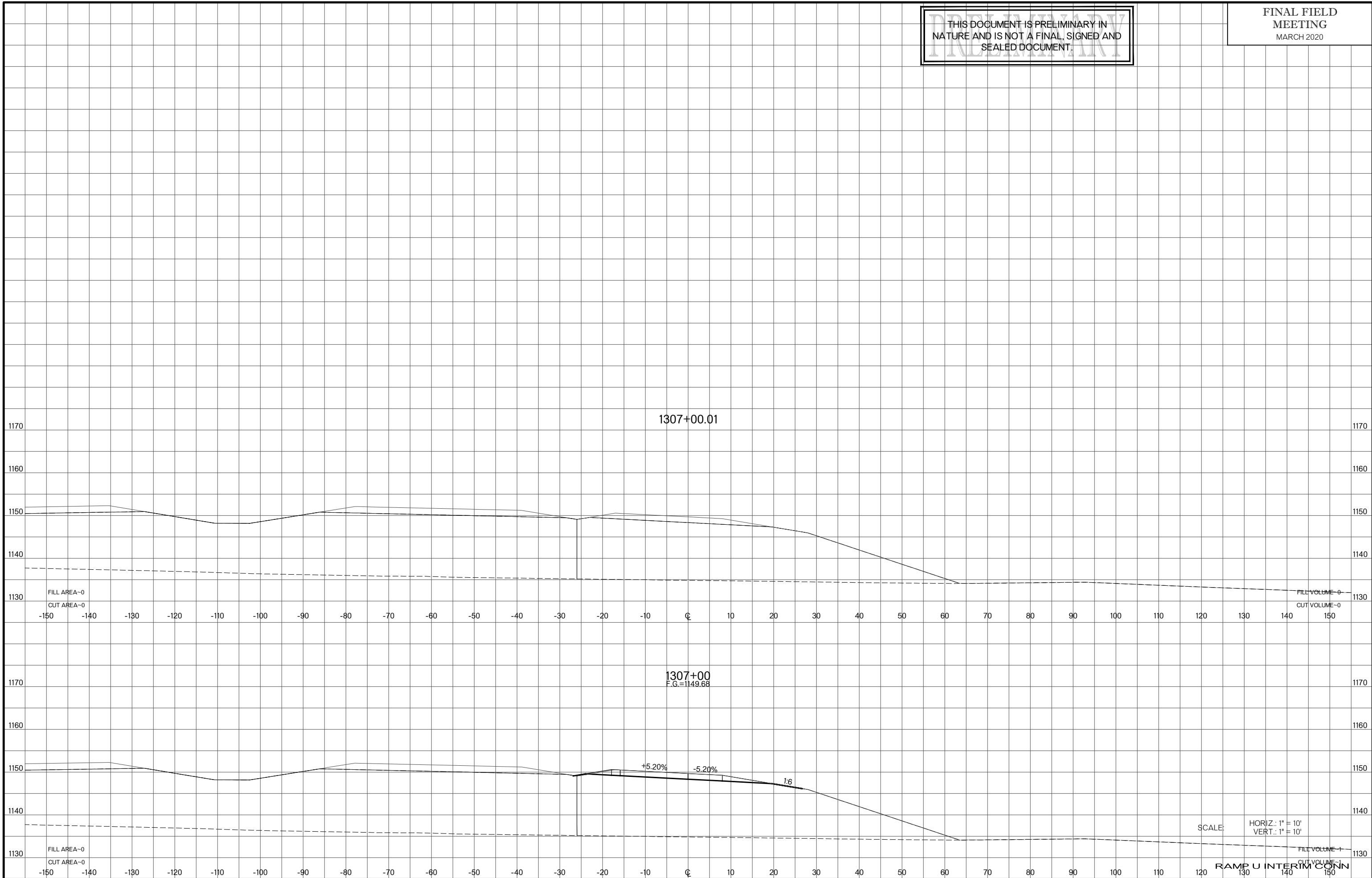
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP U INTERIM CONN

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

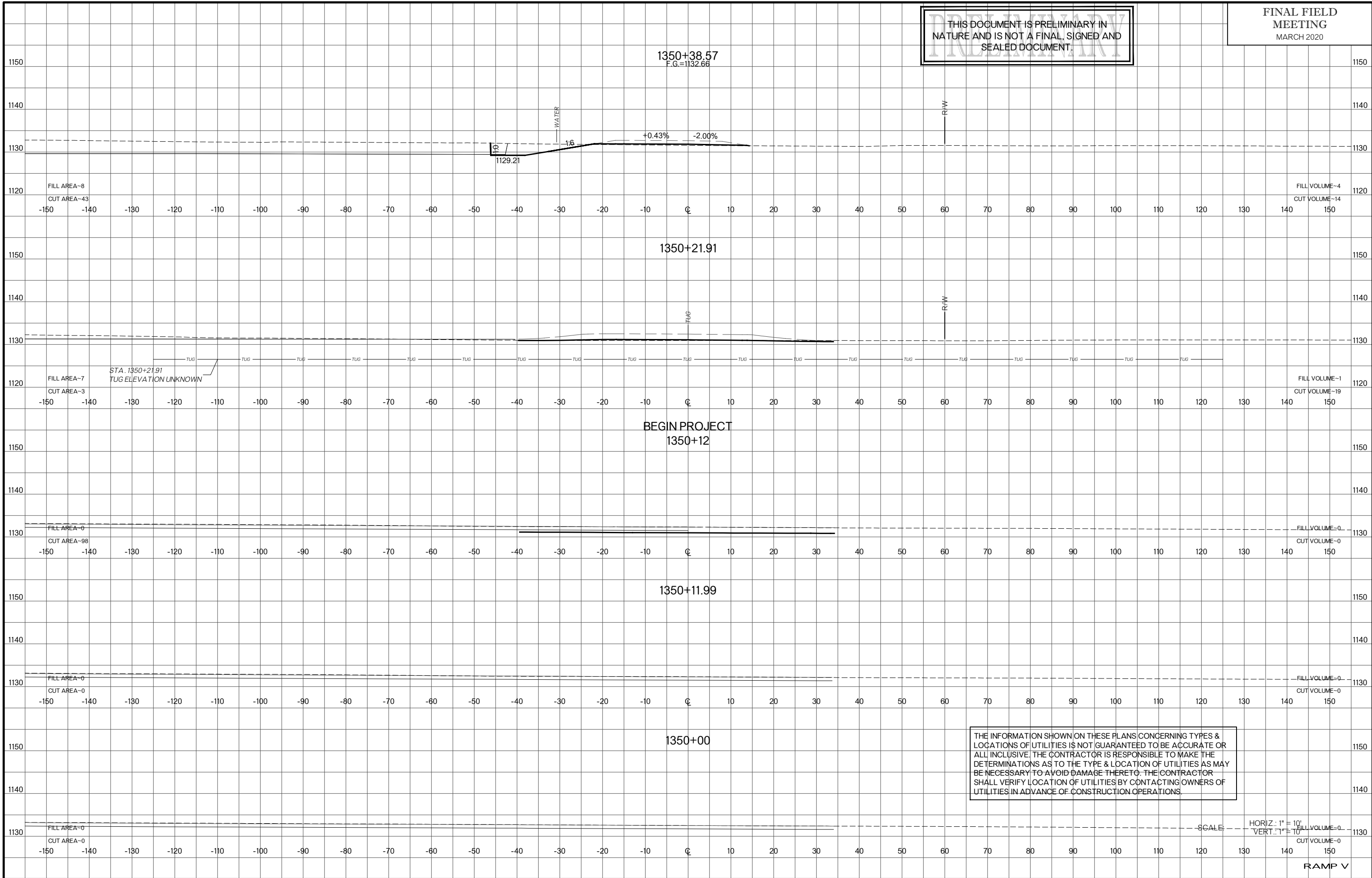
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



1350+38.57
F.G.=1132.66

1350+21.91

BEGIN PROJECT
1350+12

1350+11.99

1350+00

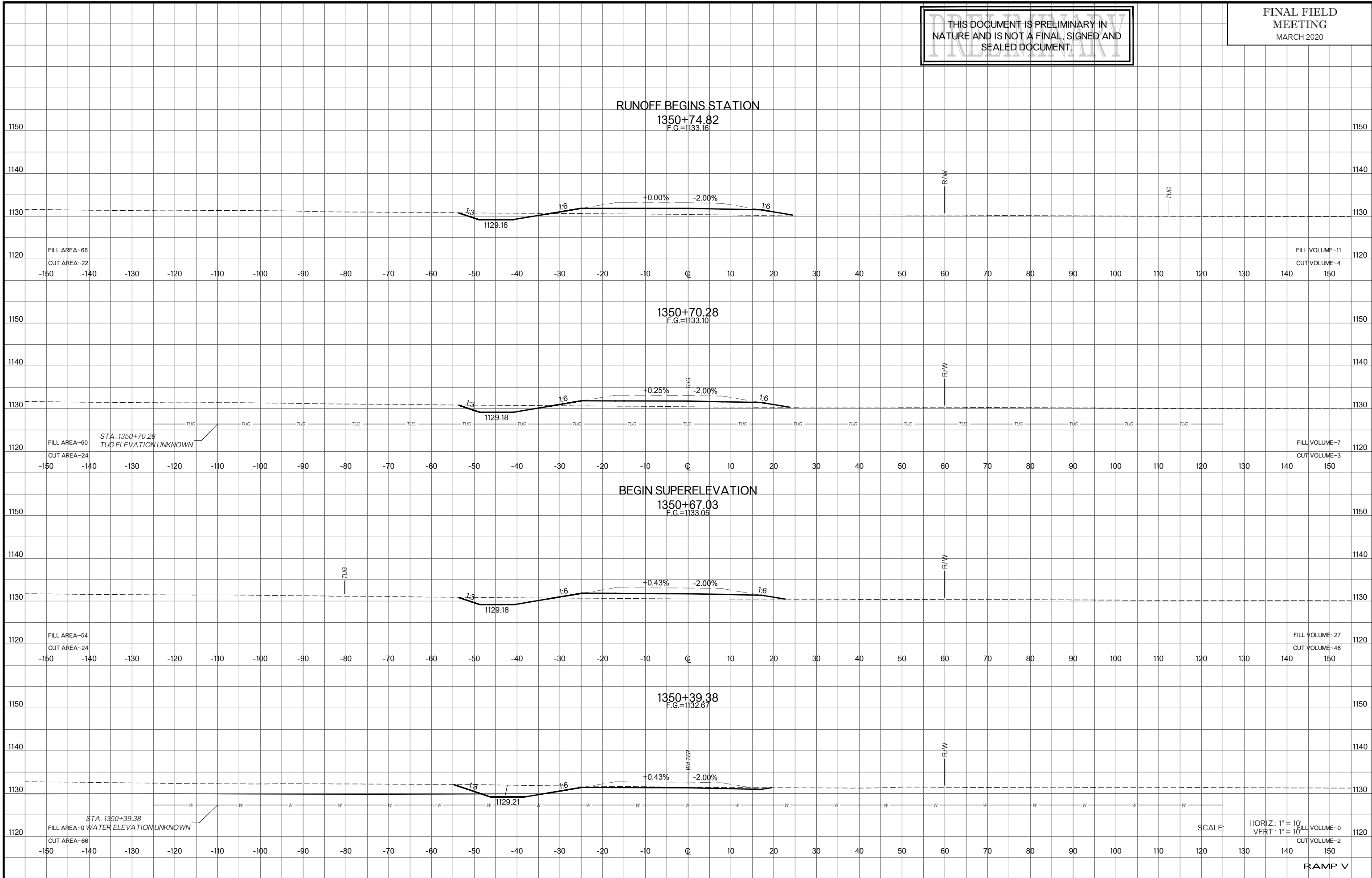
THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP V

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



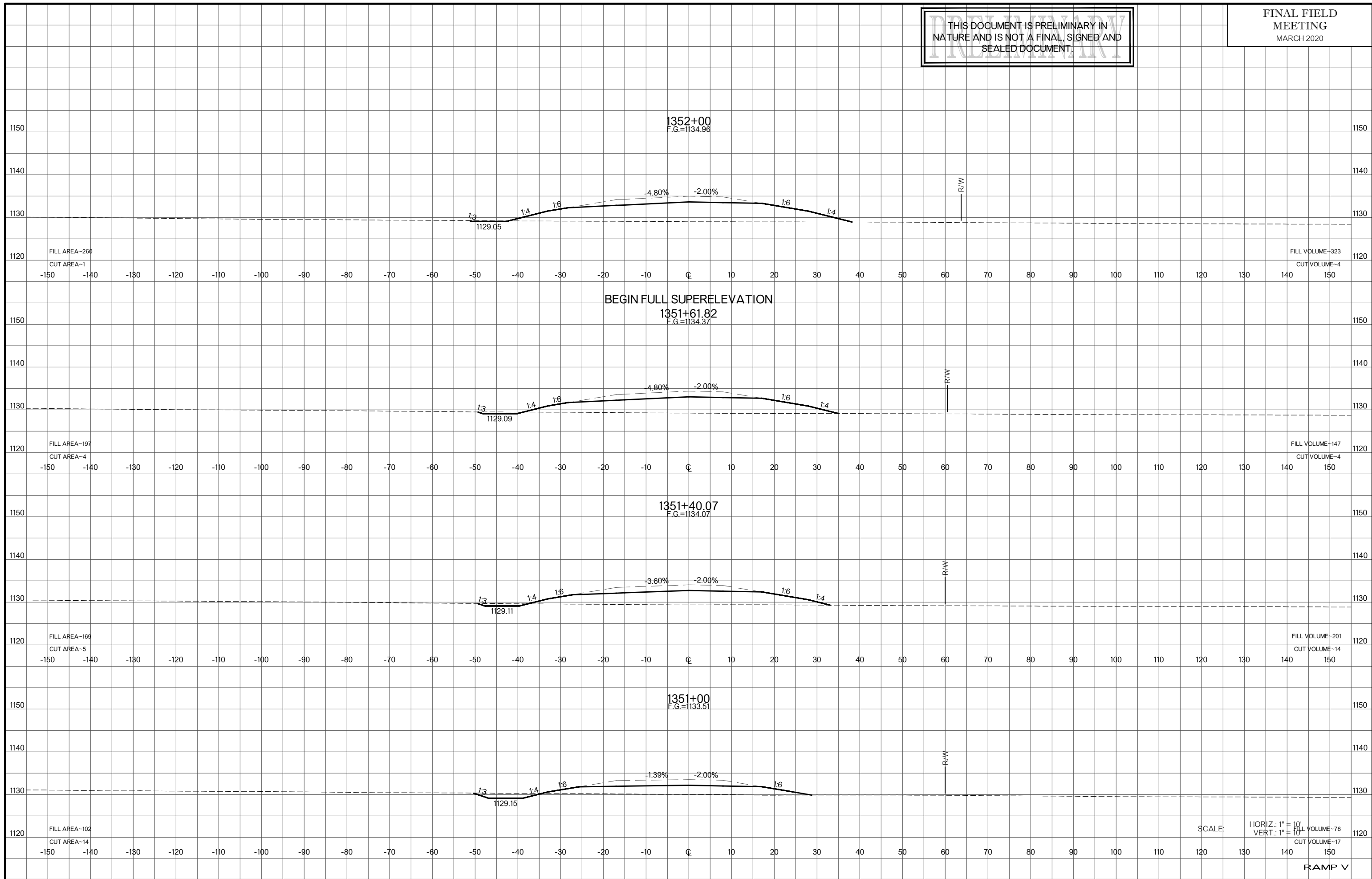
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP V

US 81 REALIGNMENT GRADY COUNTY

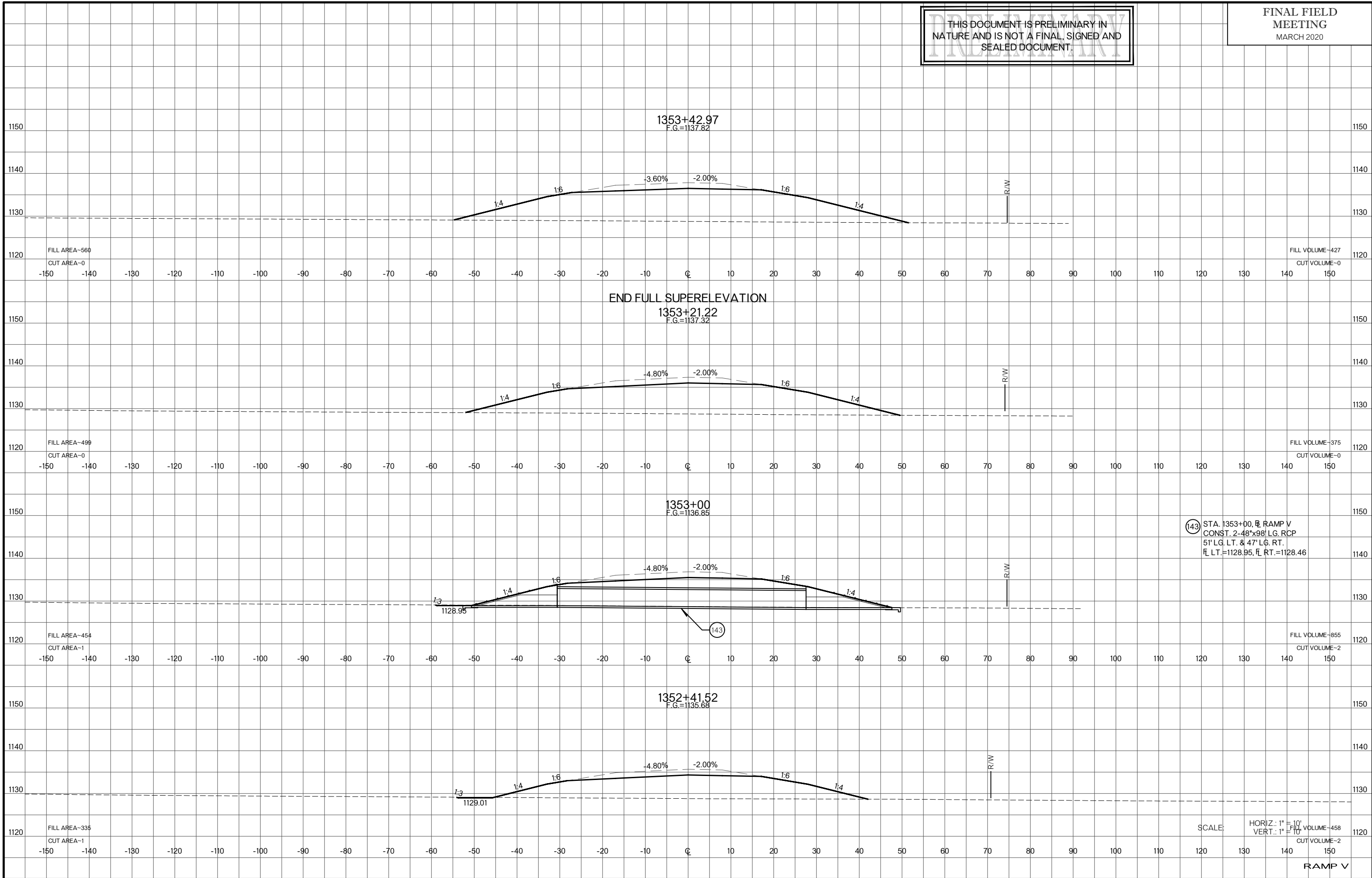
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



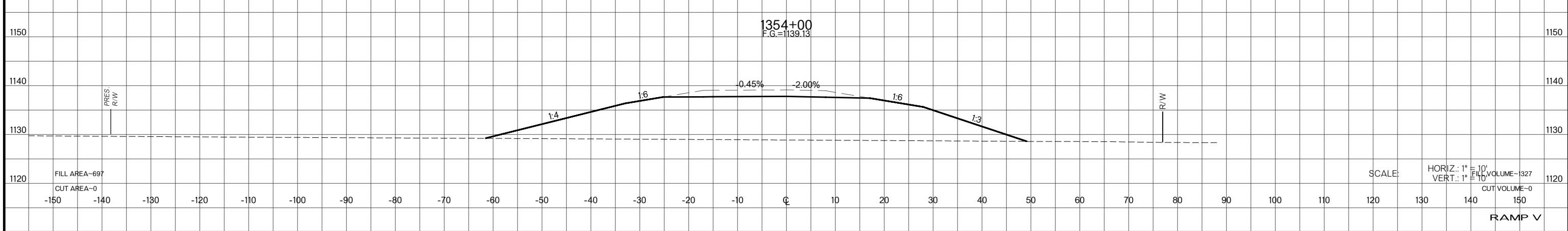
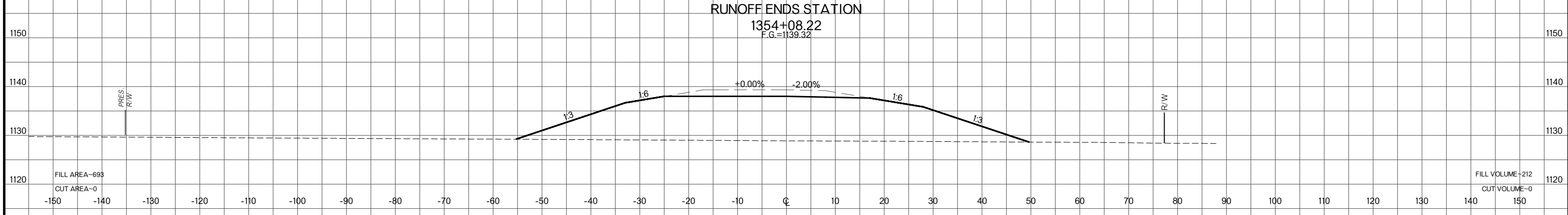
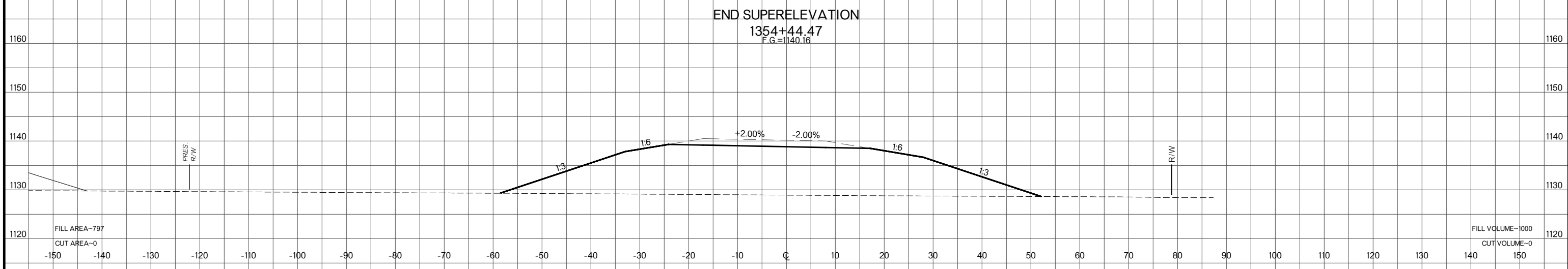
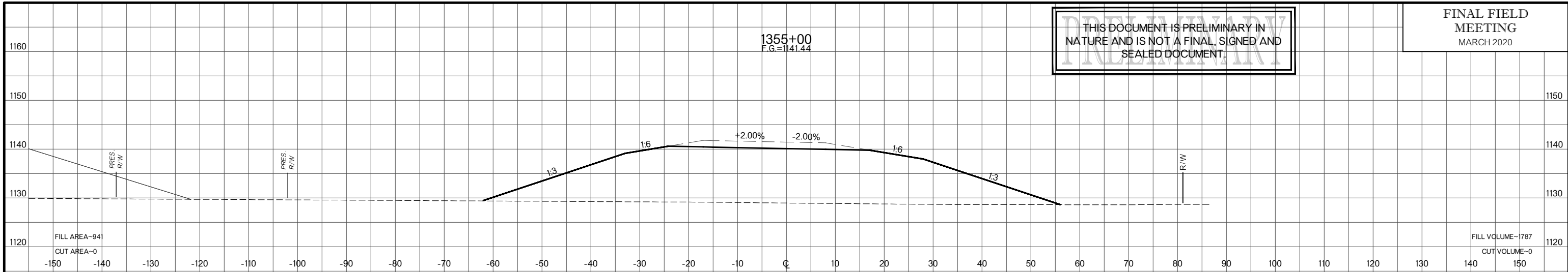
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



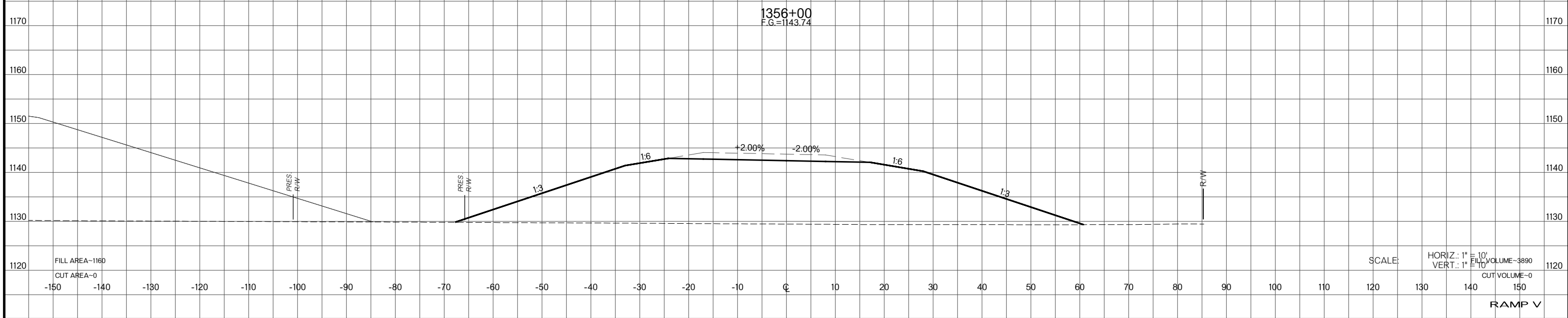
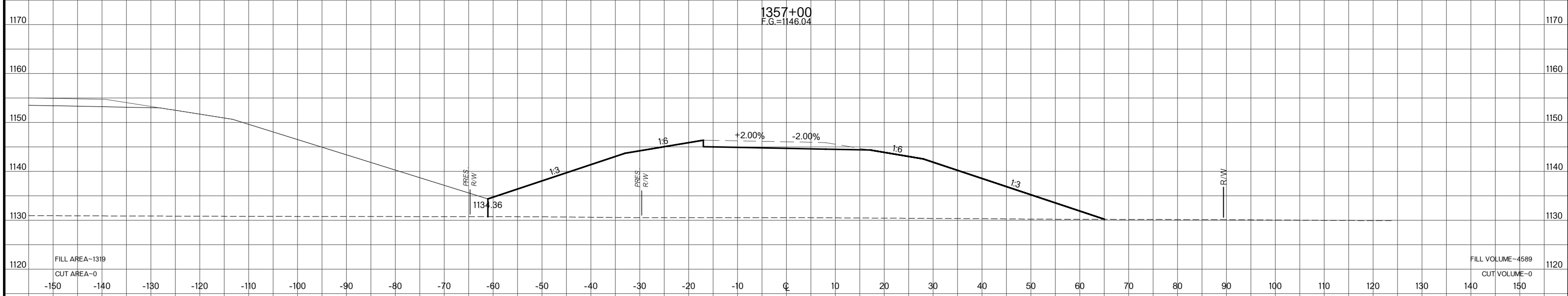
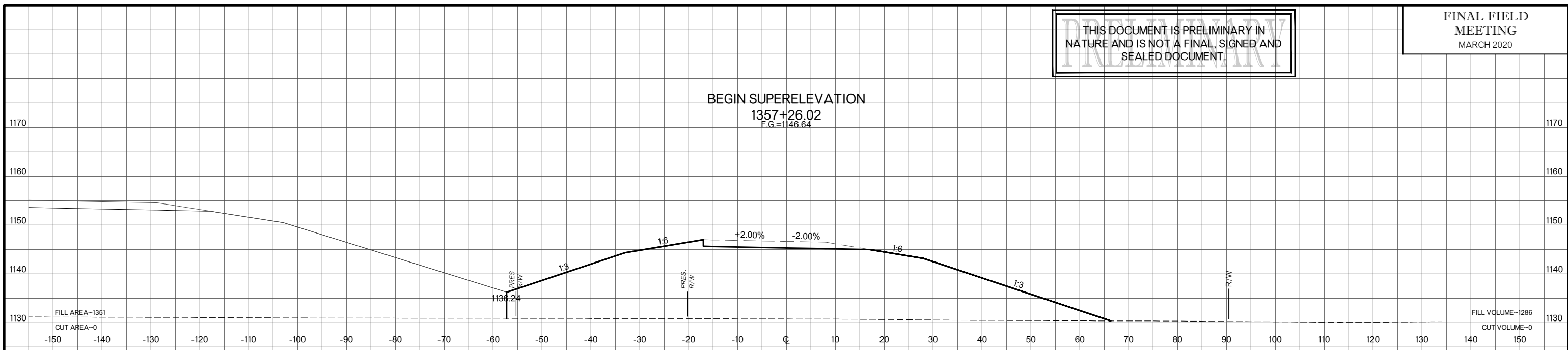
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

US 81 REALIGNMENT GRADY COUNTY

RAMP V

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



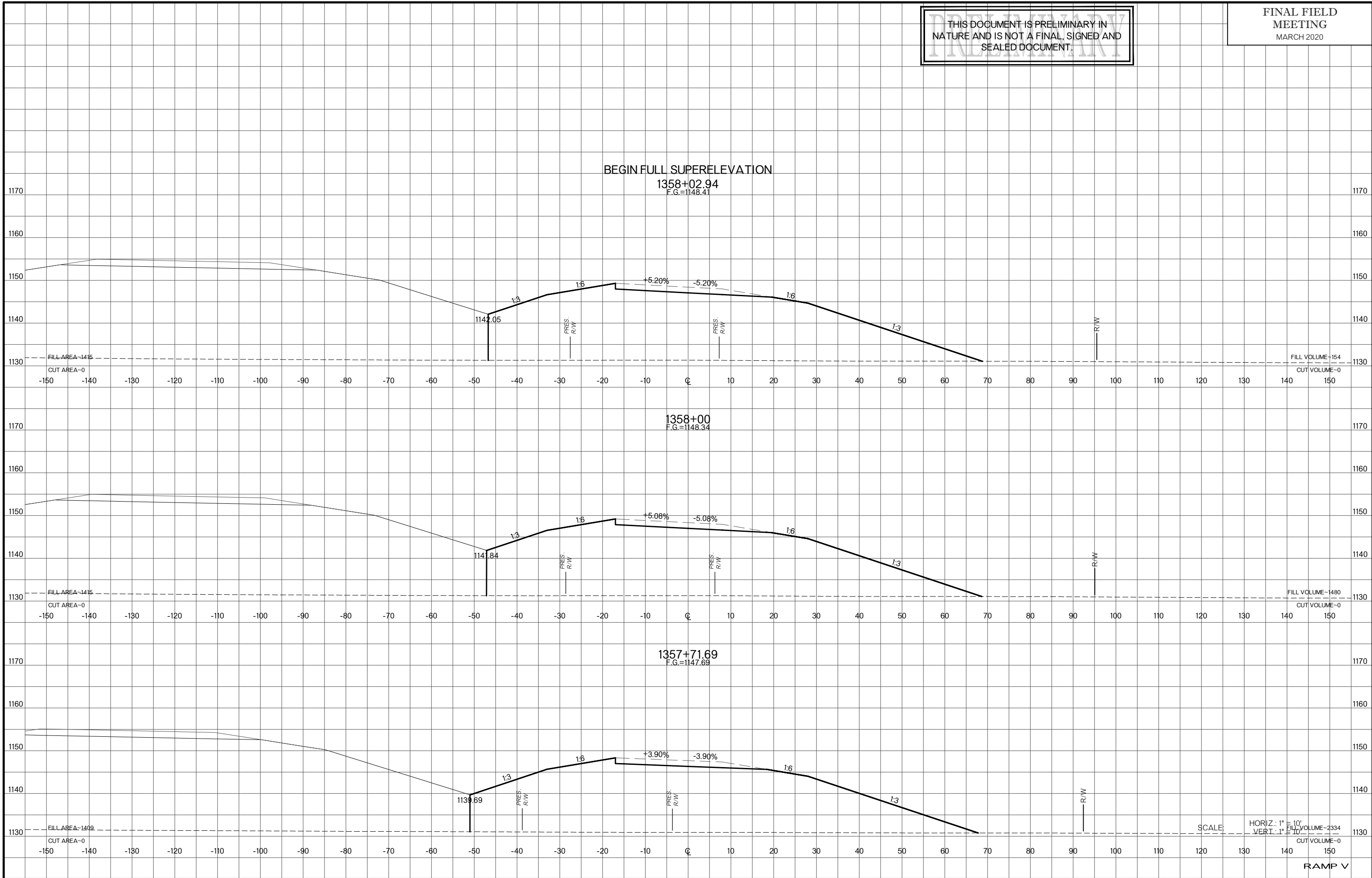
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP V

US 81 REALIGNMENT
GRADY COUNTY

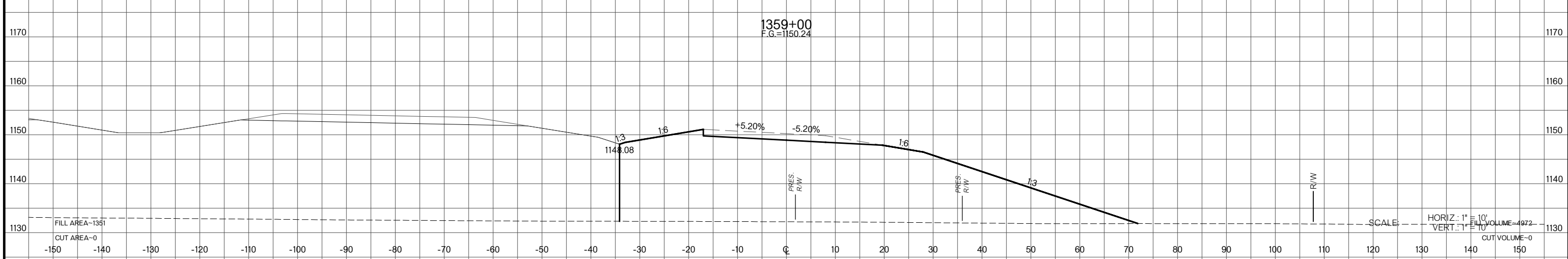
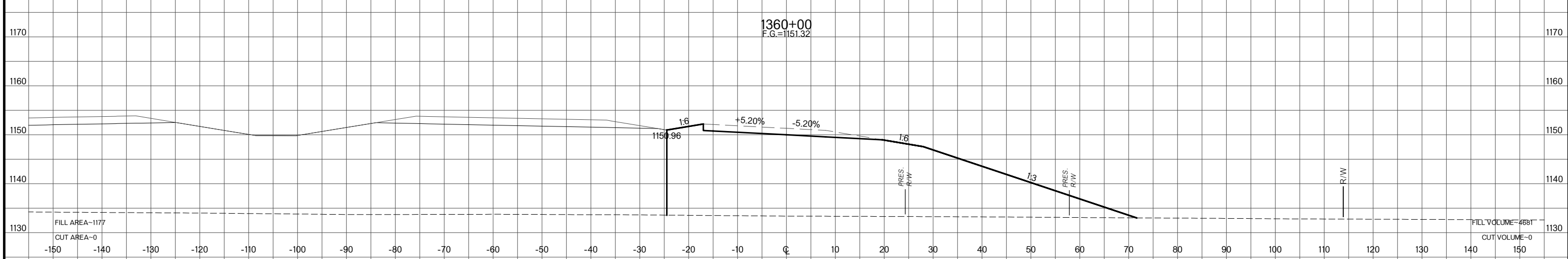
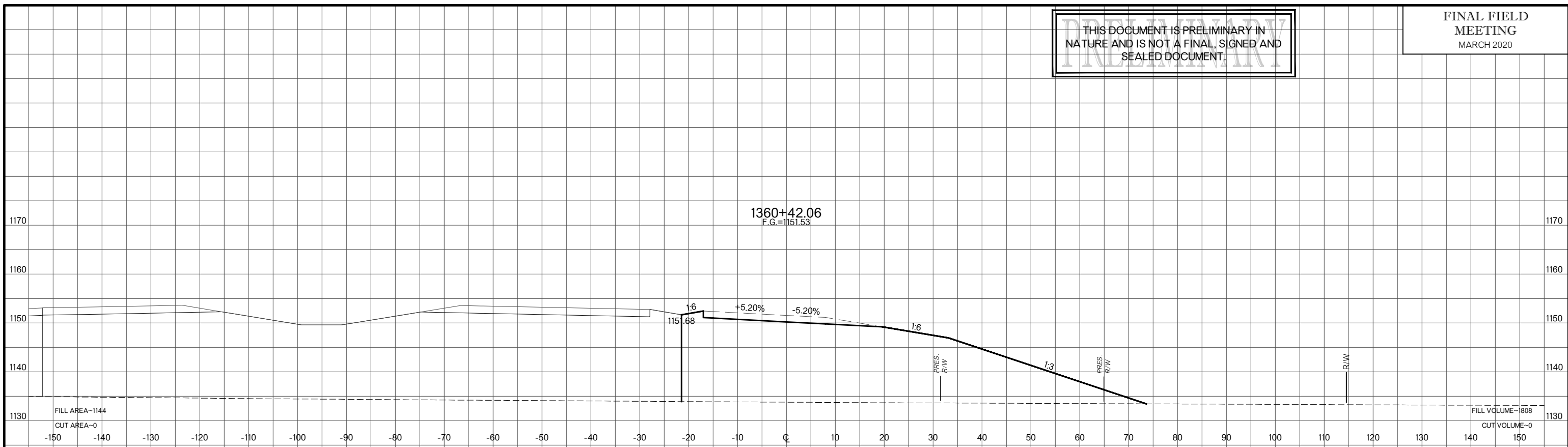
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



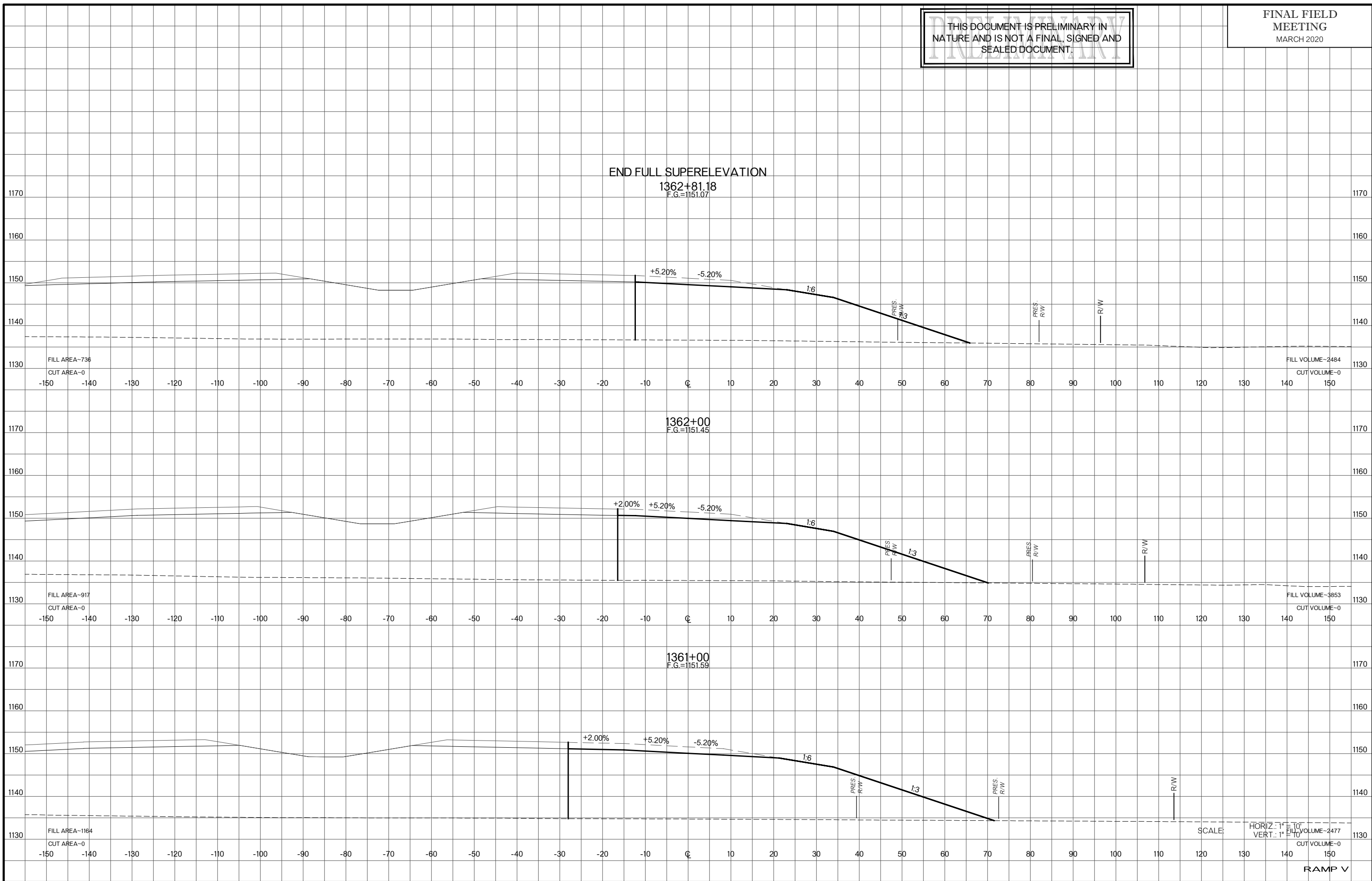
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP V

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



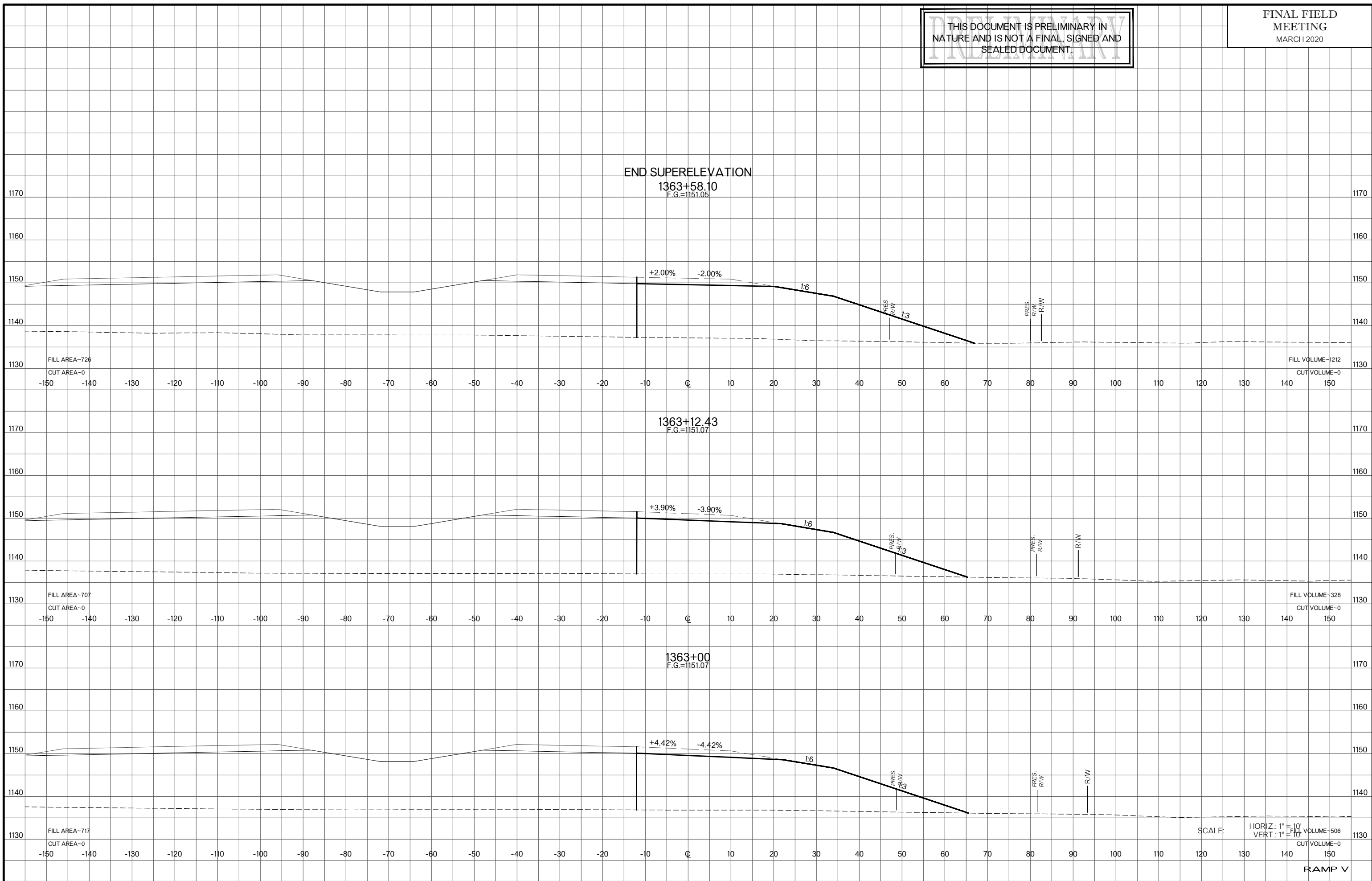
SCALE: HORIZ.: 1"=10' VERT.: 1"=10'

RAMP V

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



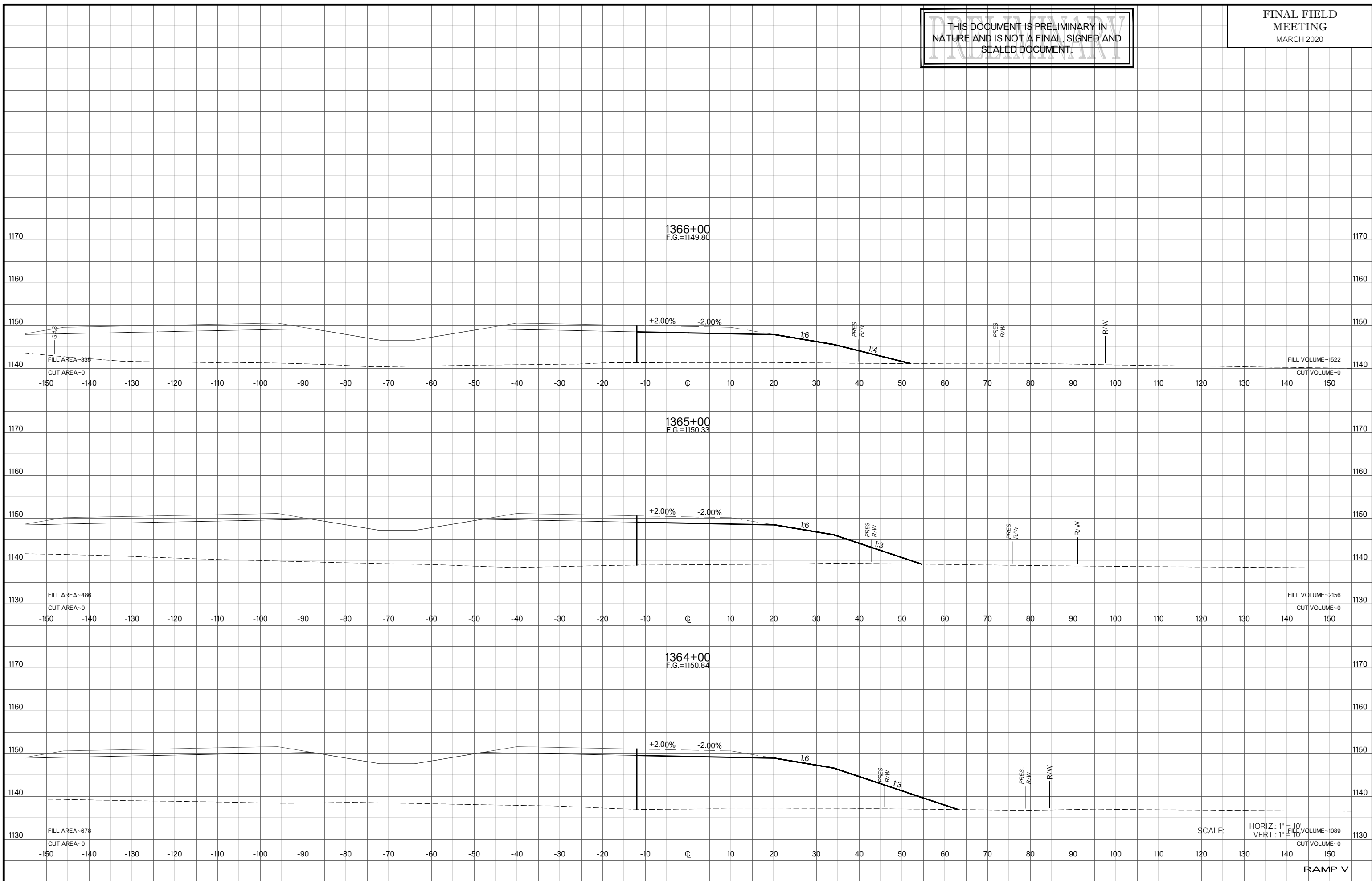
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP V

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

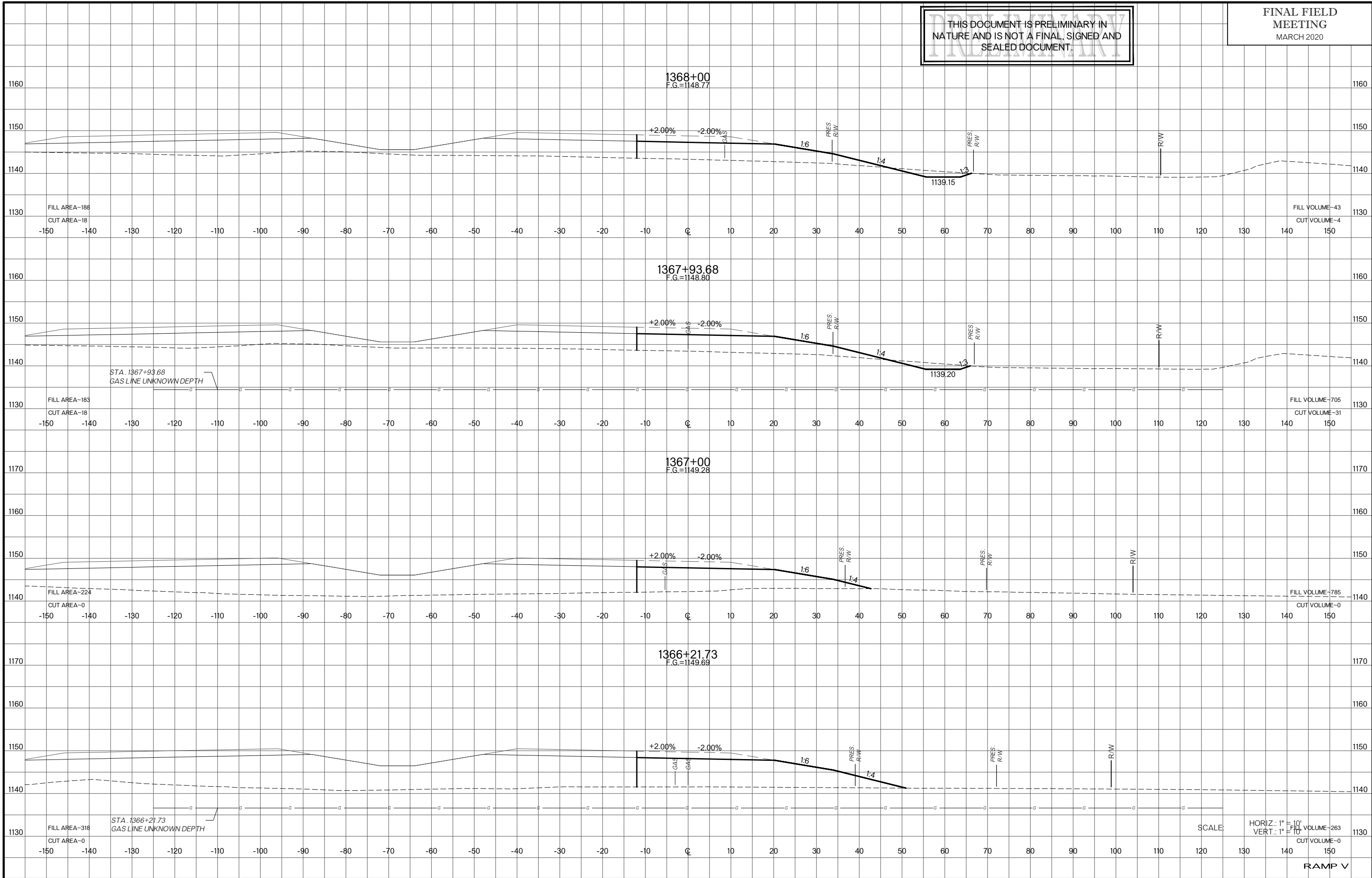


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

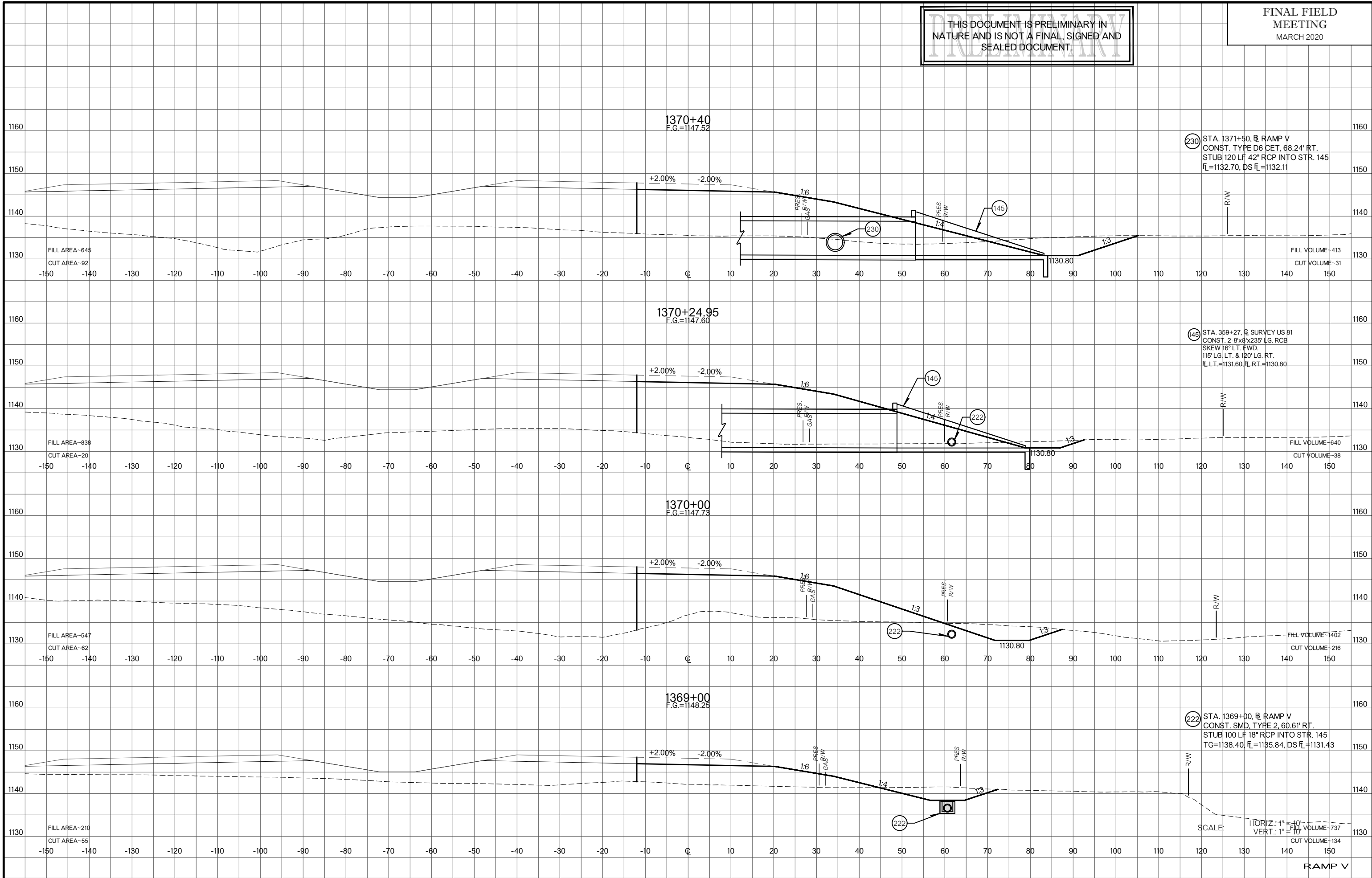


SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



230 STA. 1371+50, RAMP V
CONST. TYPE D6 CET, 68.24' RT.
STUB 120 LF 42" RCP INTO STR. 145
FL=1132.70, DS FL=1132.11

145 STA. 359+27, SURVEY US 81
CONST. 2-8"x8"x235' LG. RCB
SKEW 16° LT. FWD.
115' LG. LT. & 120' LG. RT.
FL LT=1131.60, FL RT=1130.80

222 STA. 1369+00, RAMP V
CONST. SMD, TYPE 2, 60.61' RT.
STUB 100 LF 18" RCP INTO STR. 145
TG=1138.40, FL=1135.84, DS FL=1131.43

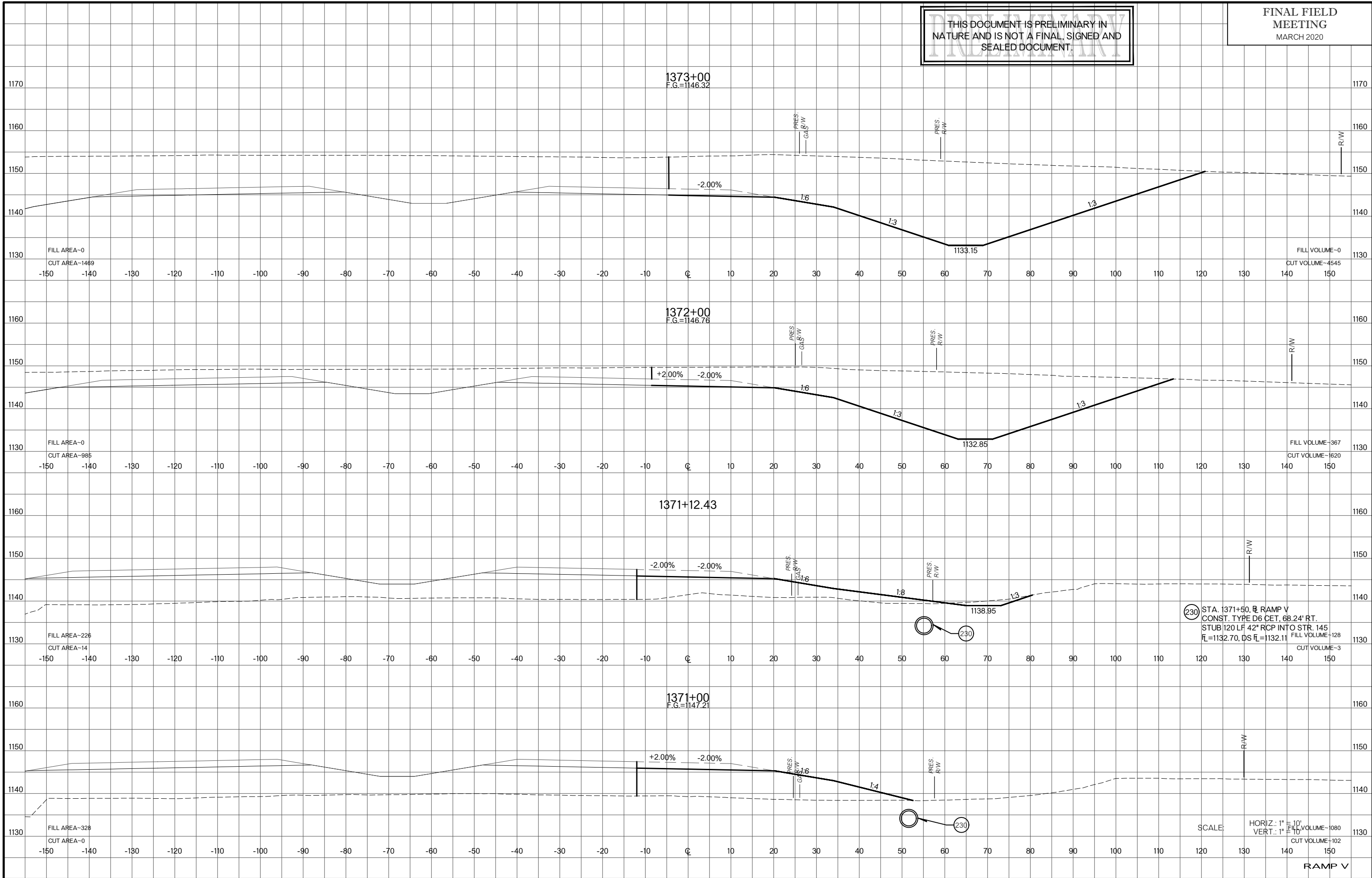
SCALE: HORIZ.: 1"=10'
VERT.: 1"=10'

RAMP V

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

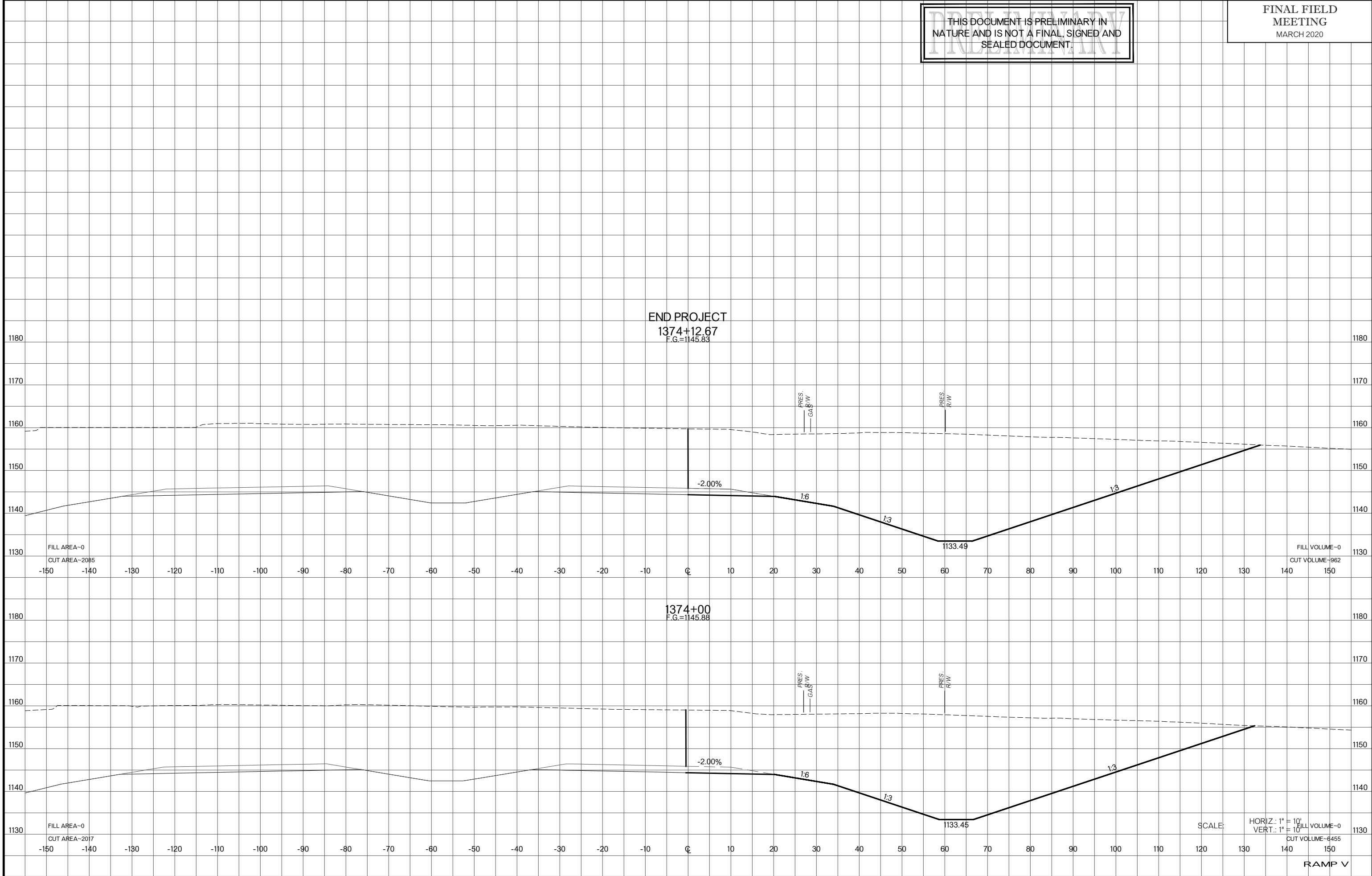


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

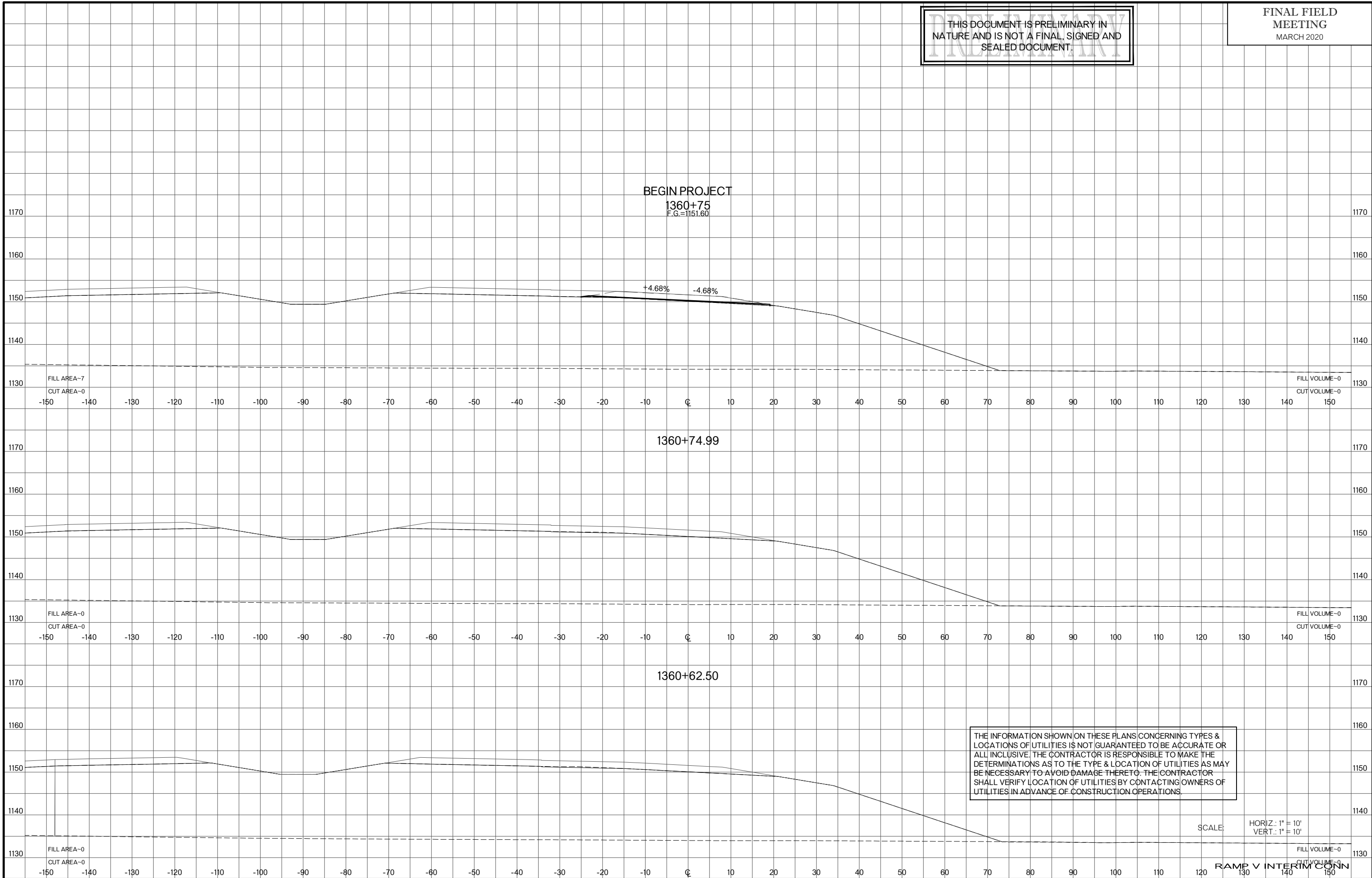
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



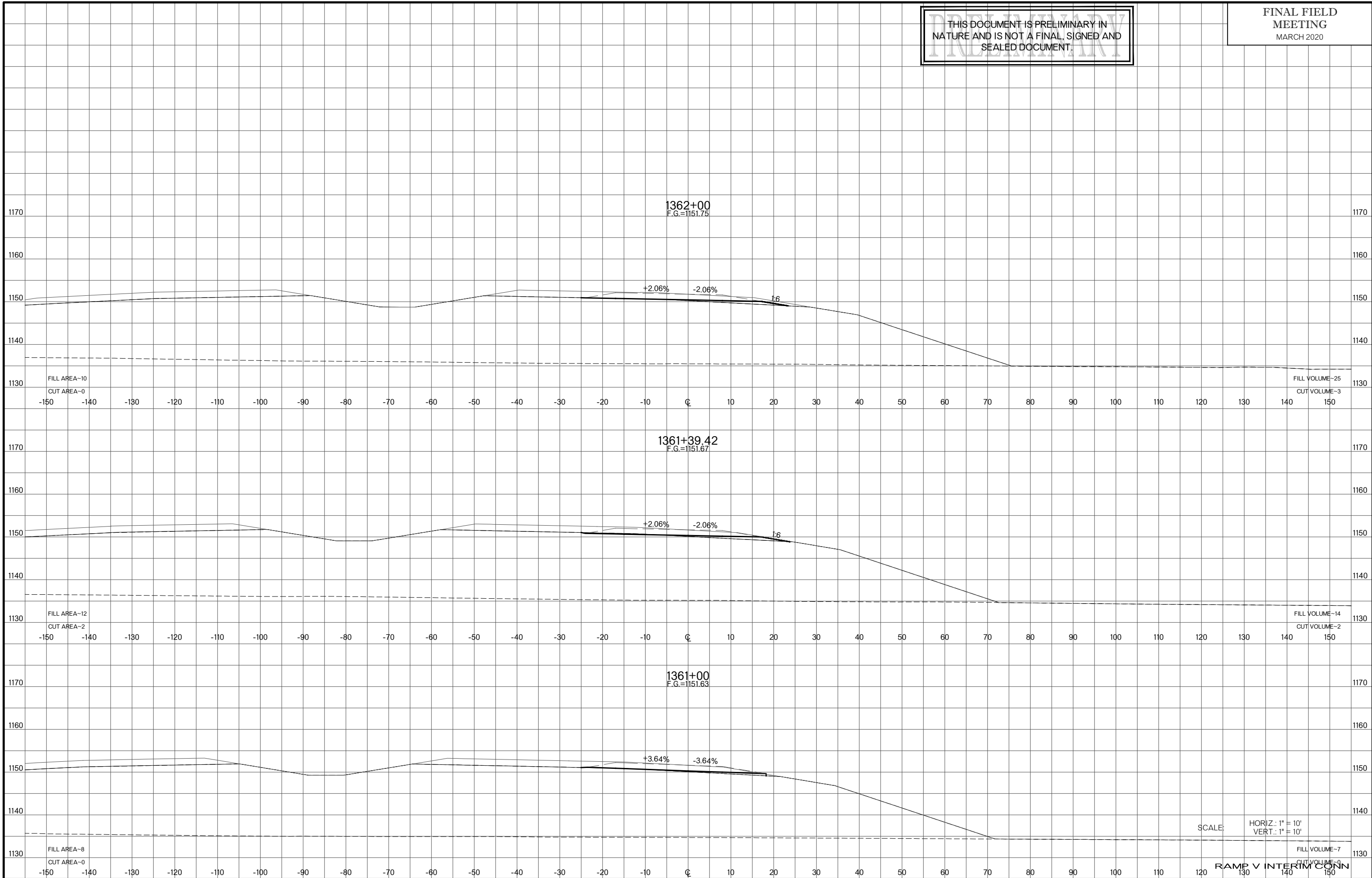
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP V INTERIM CONN

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



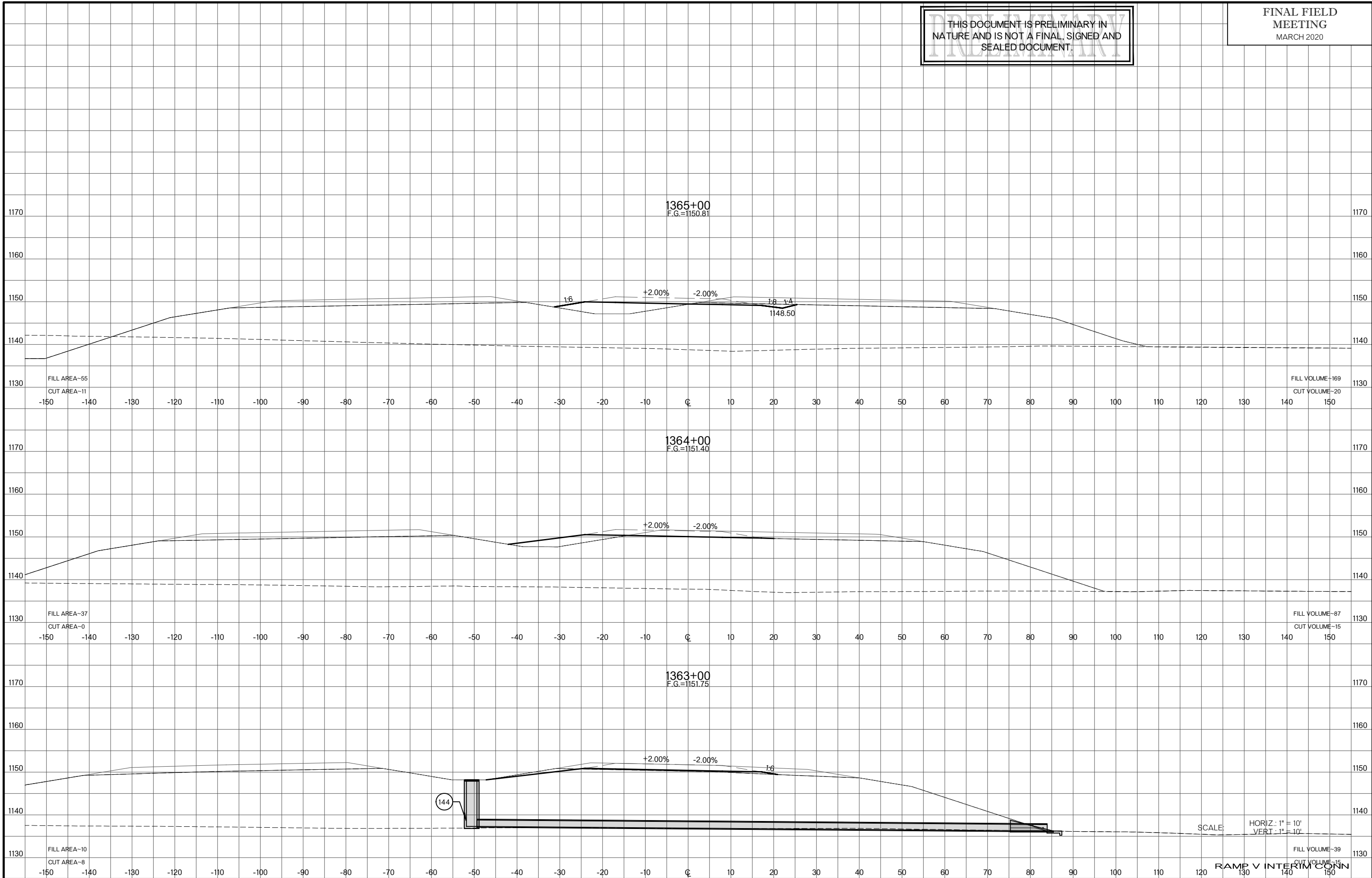
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP V INTERIM CONN

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



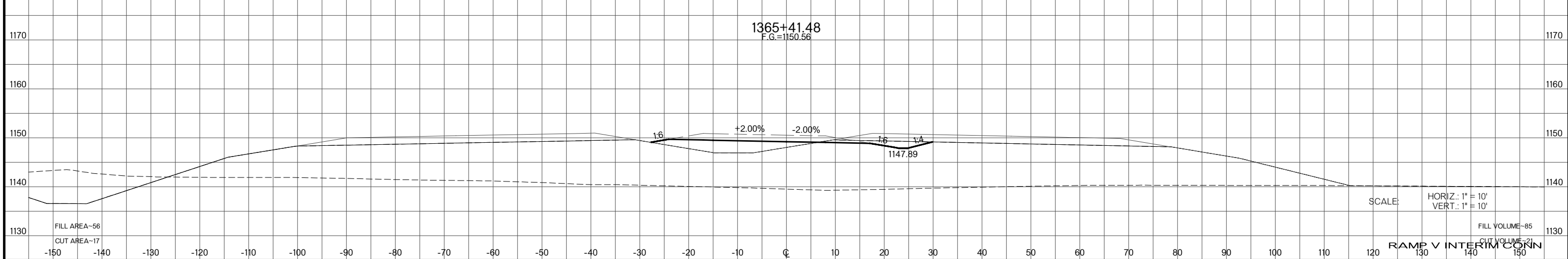
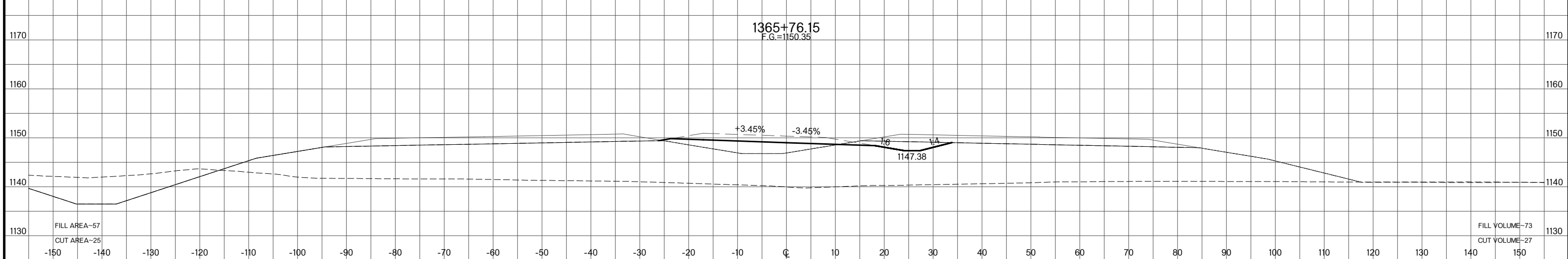
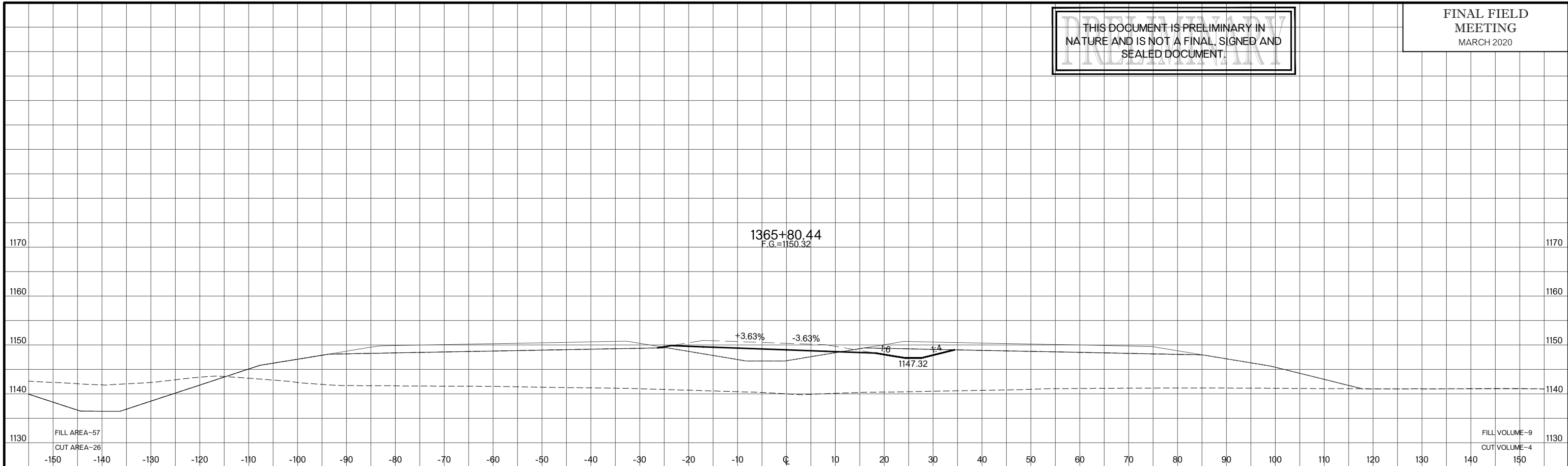
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP V INTERIM CONN

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



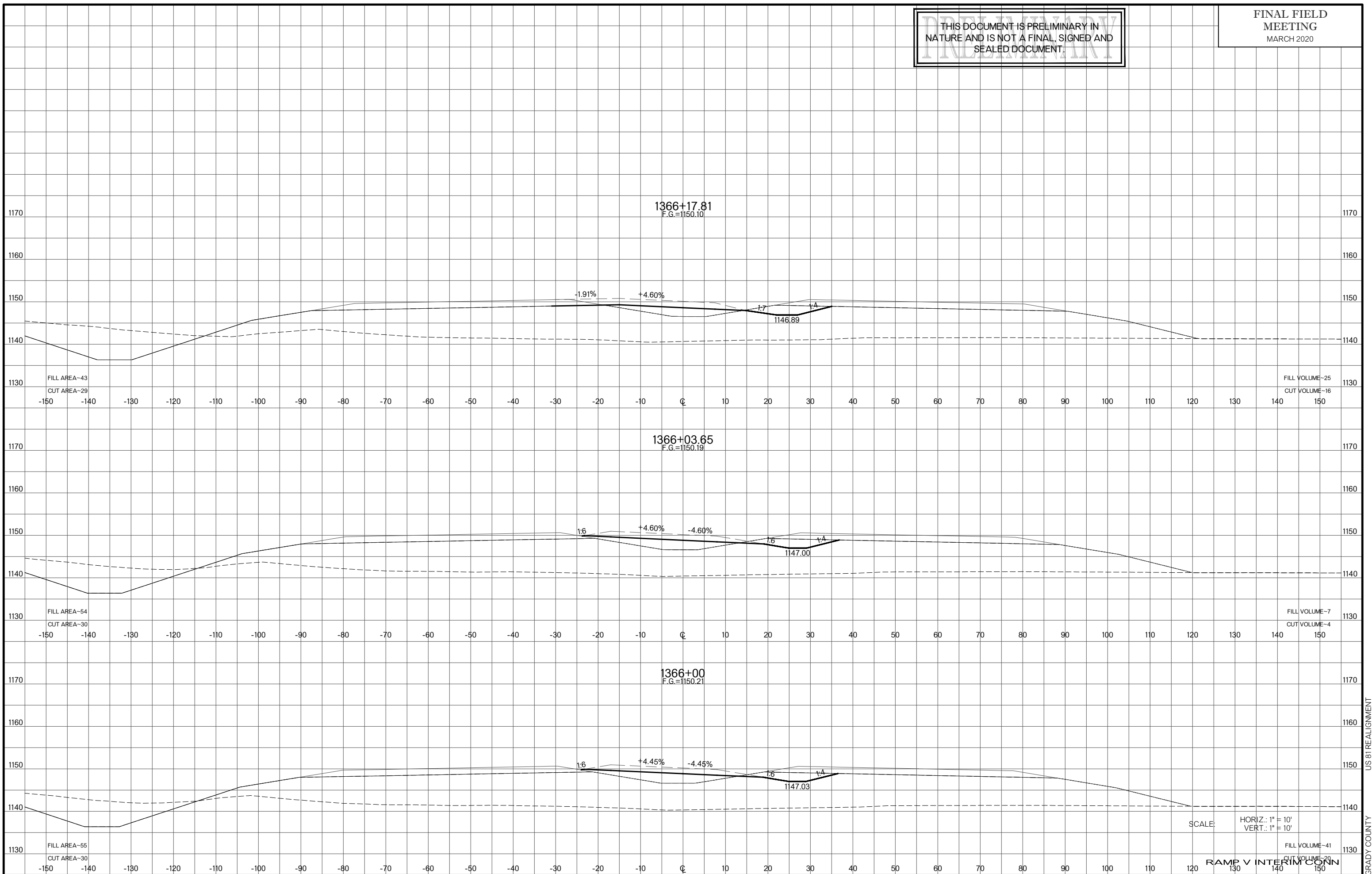
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP V INTERM CONN

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



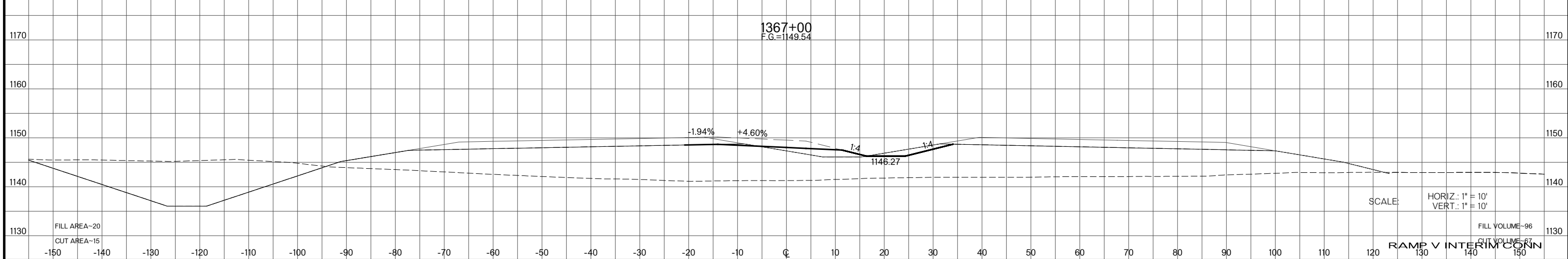
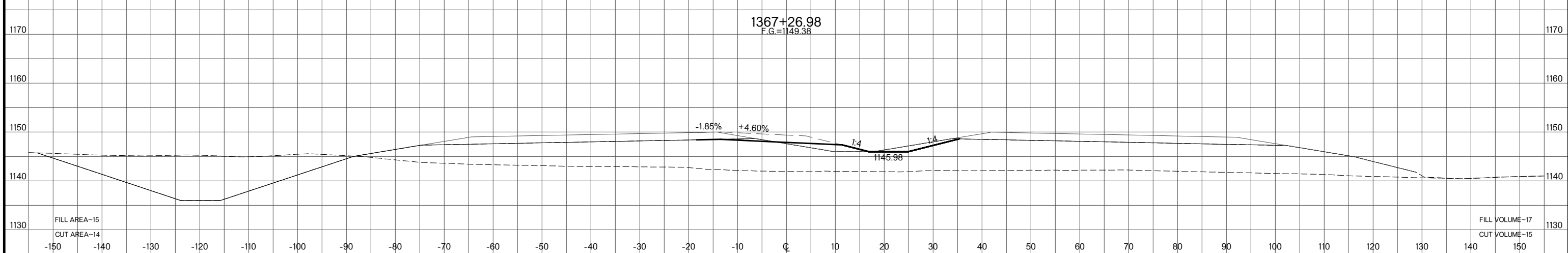
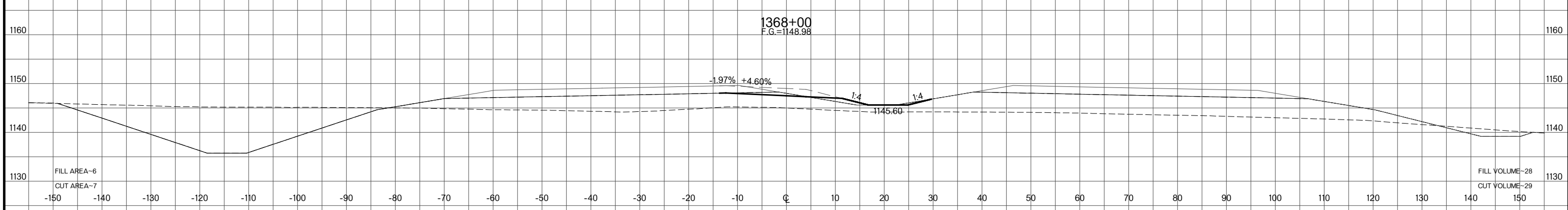
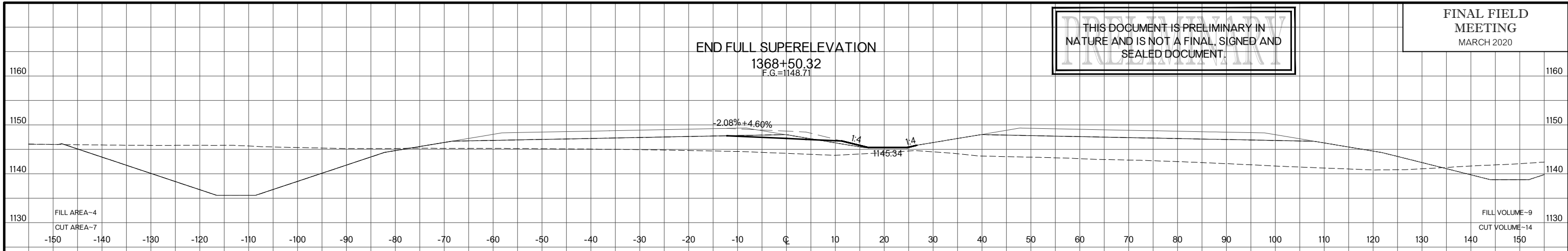
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP V INTERIM CONN

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



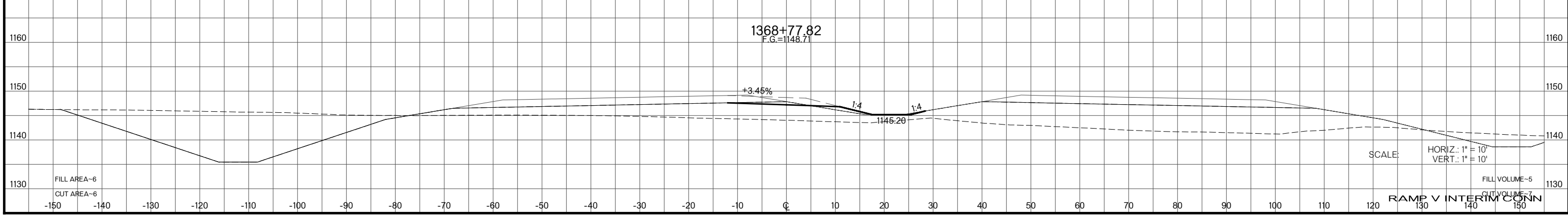
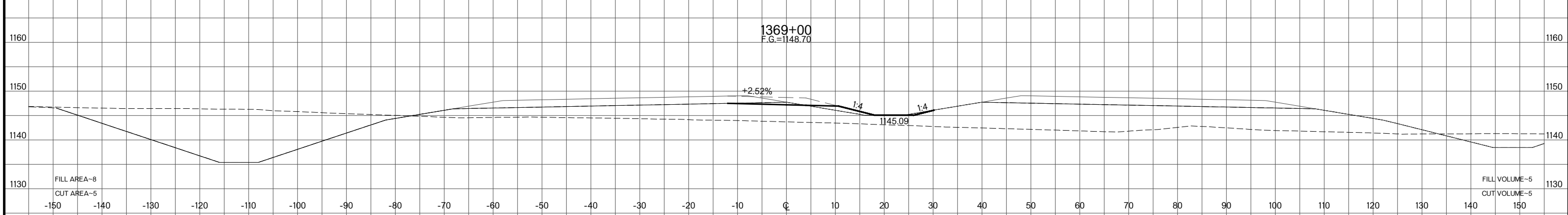
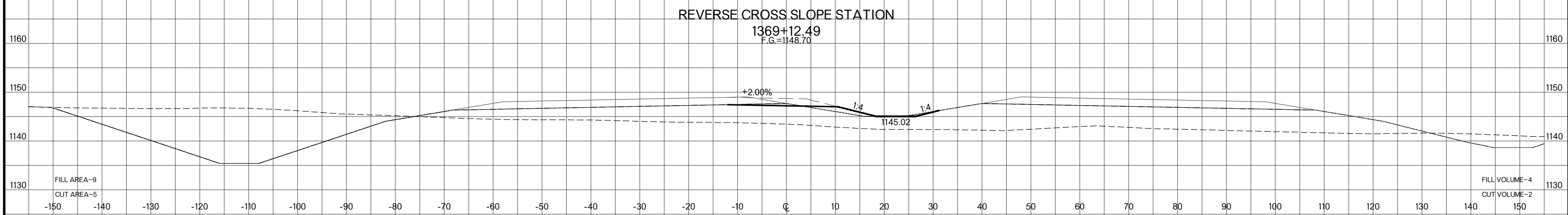
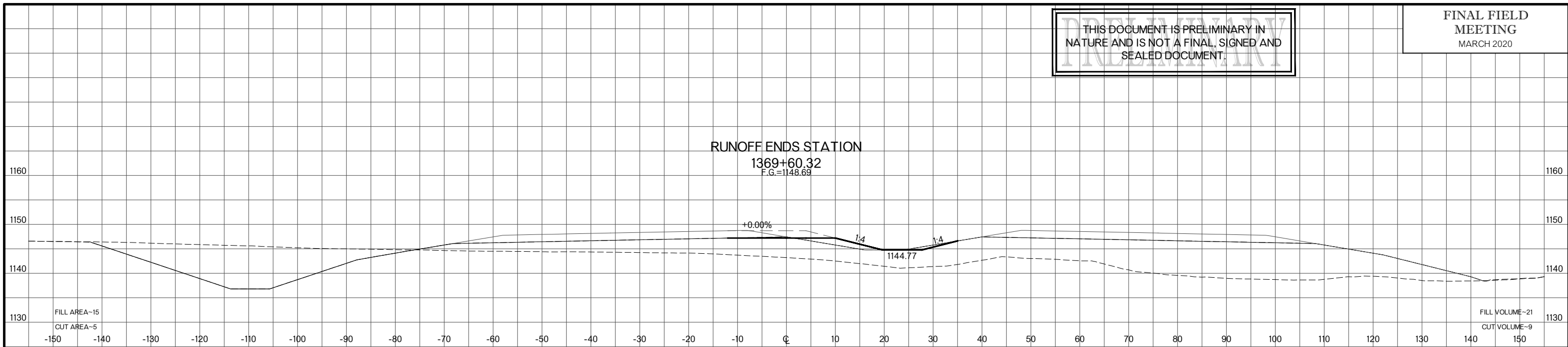
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP V INTERM CONN

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



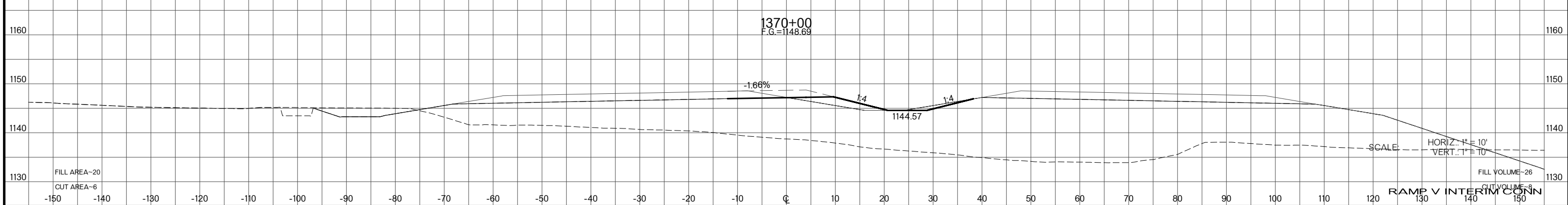
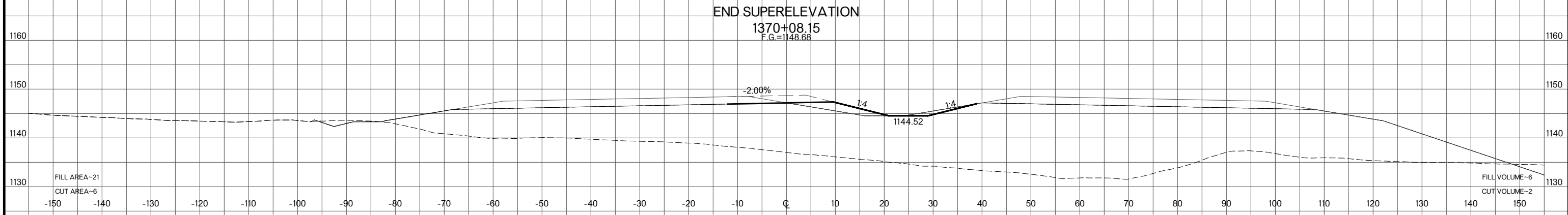
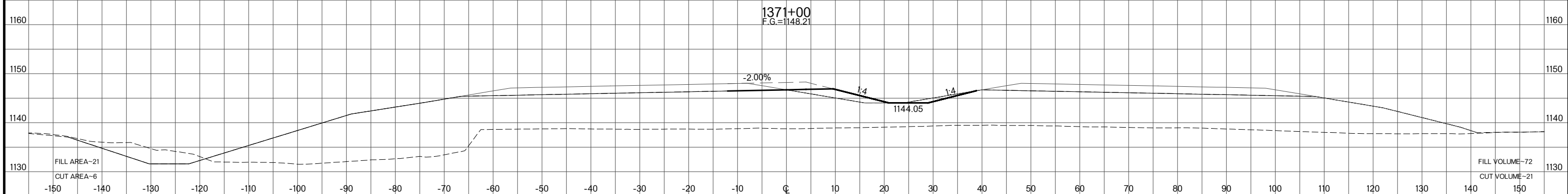
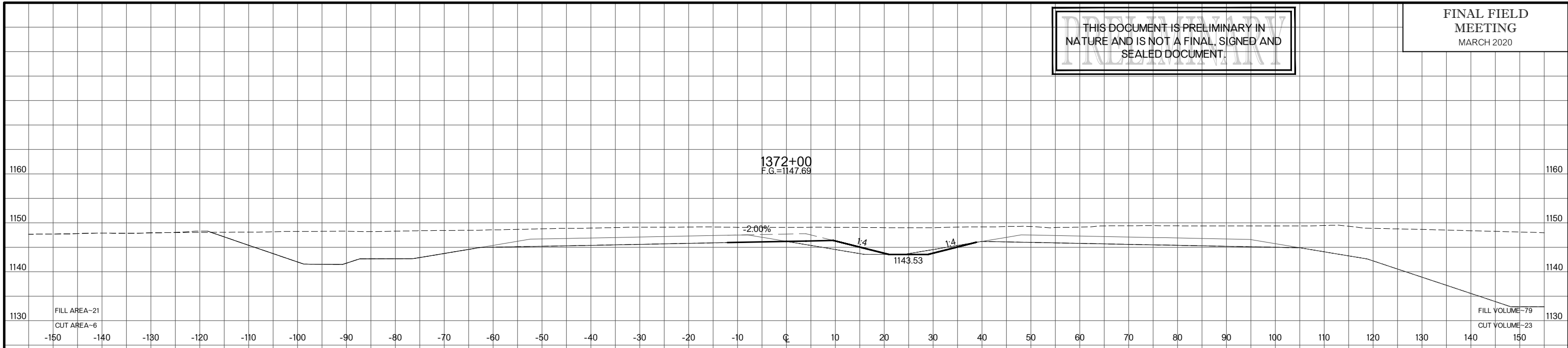
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP V INTERIM CONN

US 81 REALIGNMENT
GRADY COUNTY

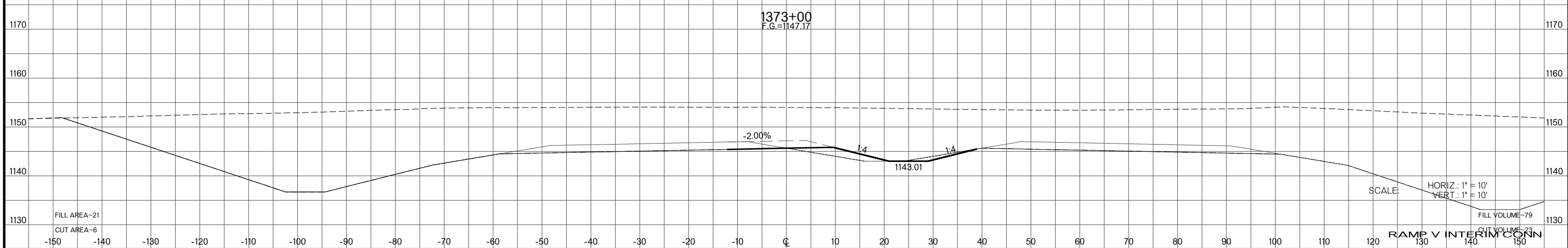
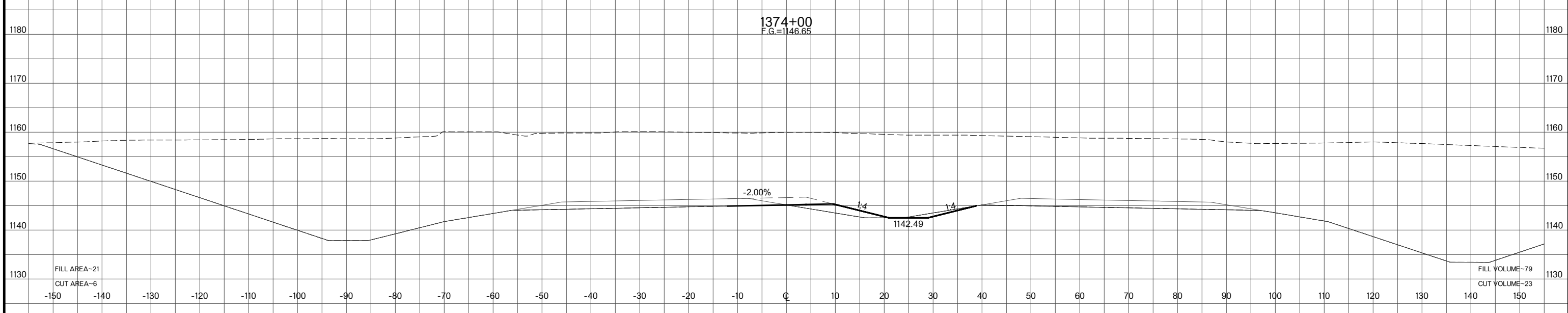
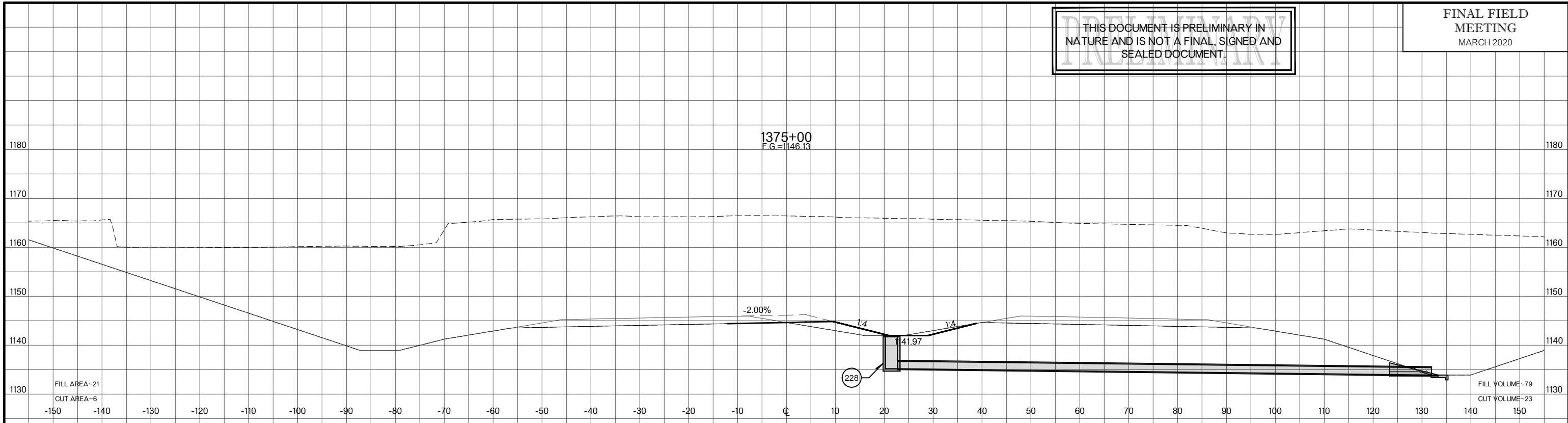
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



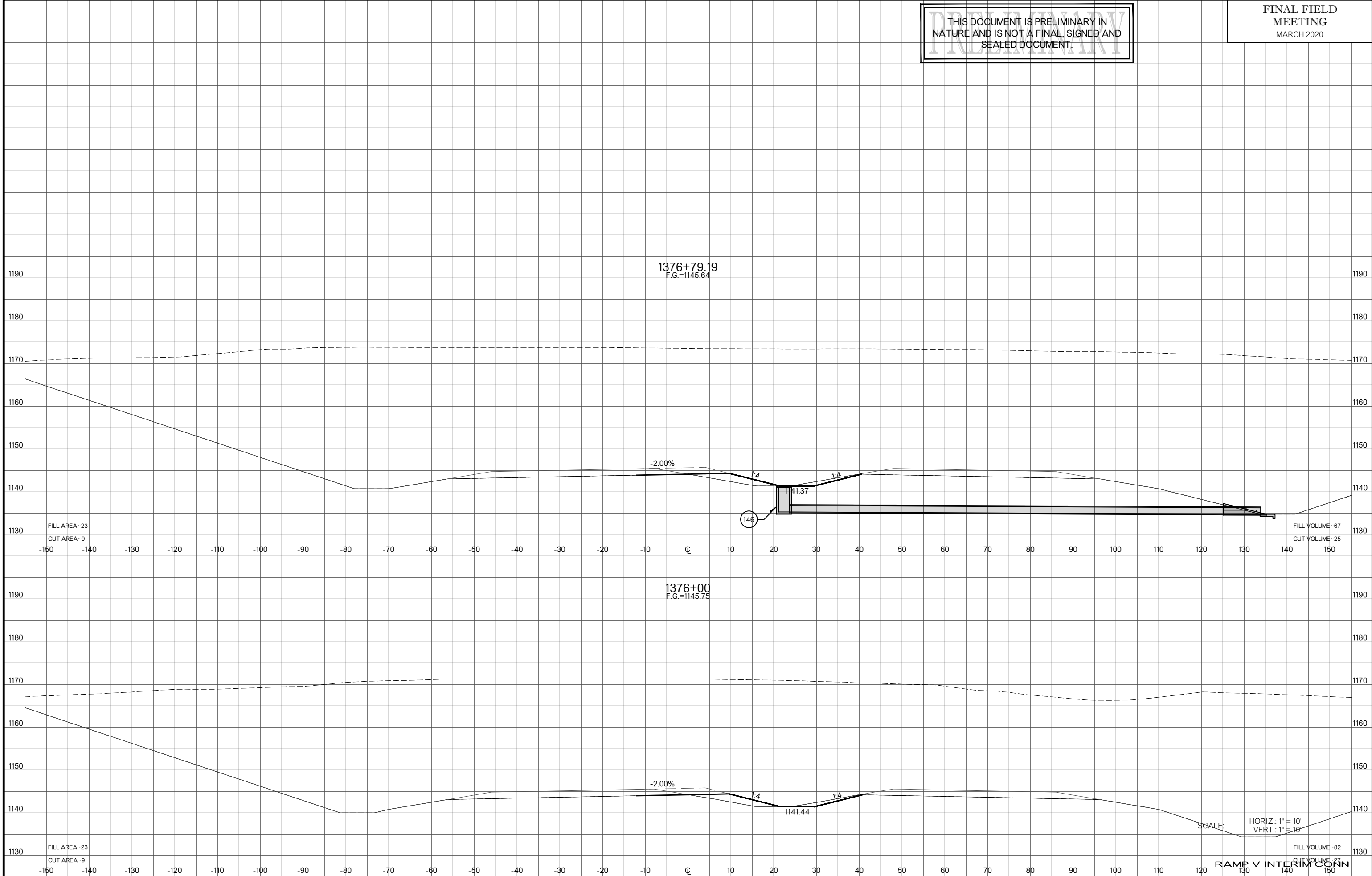
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

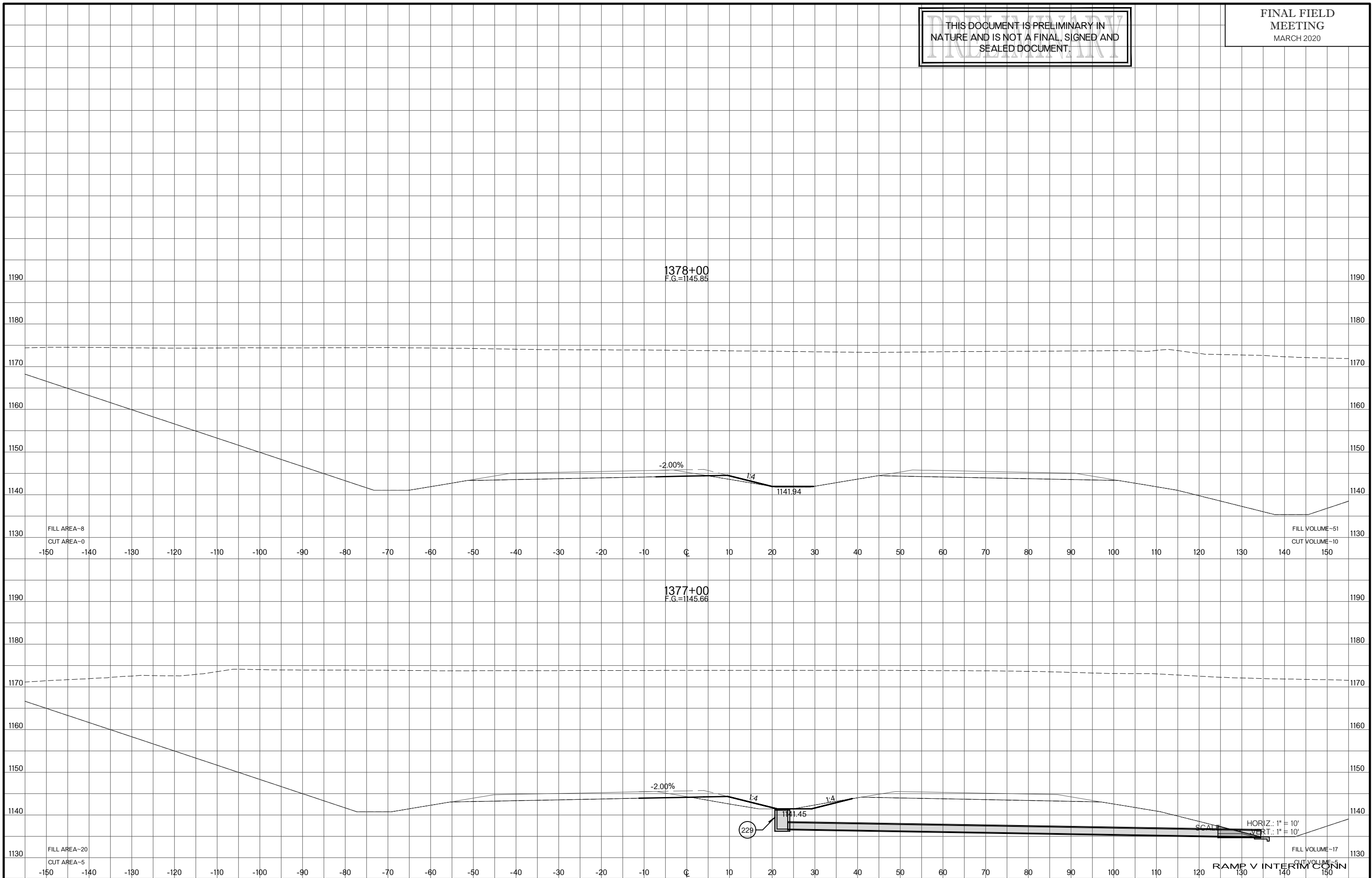
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

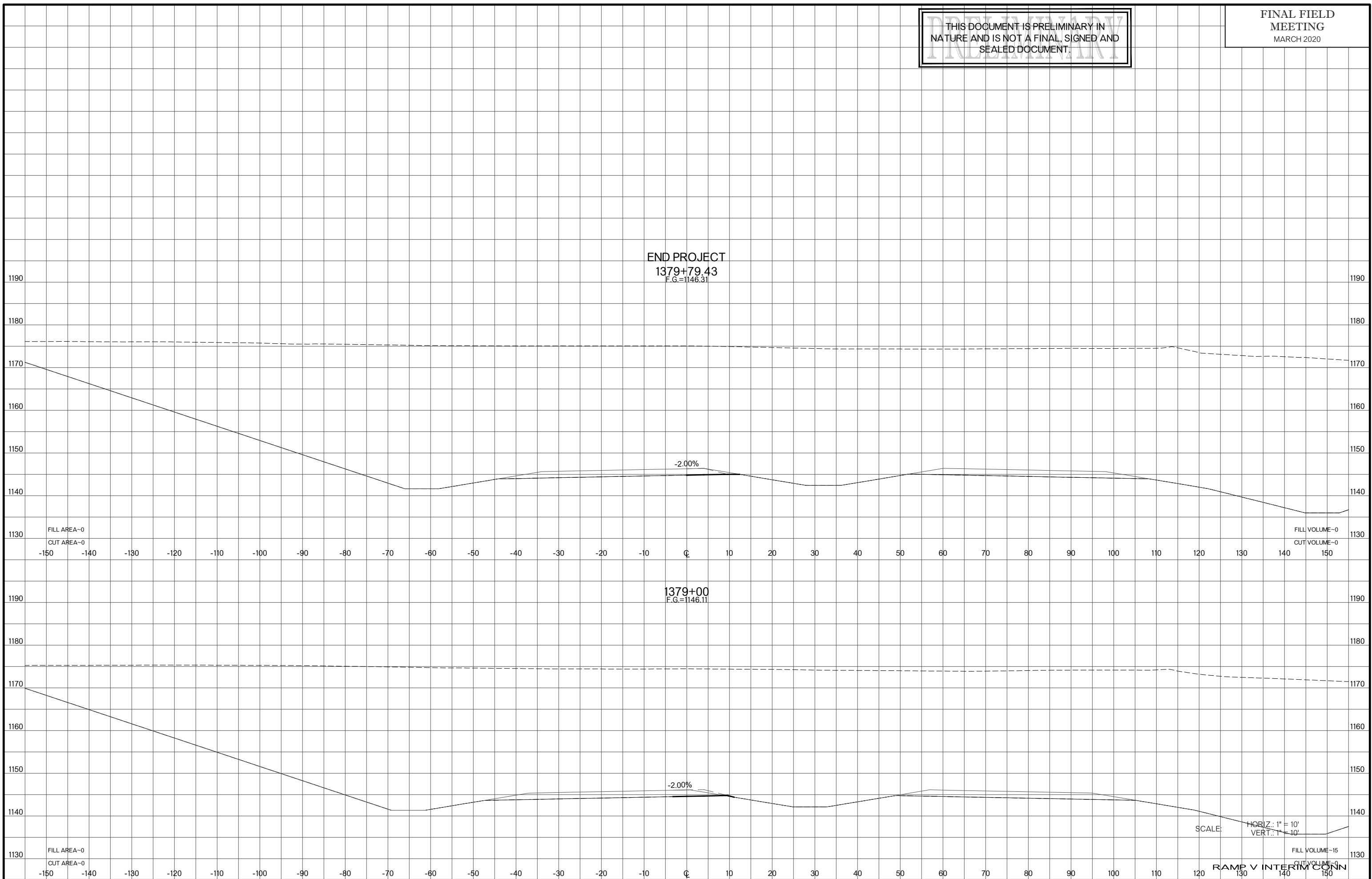
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

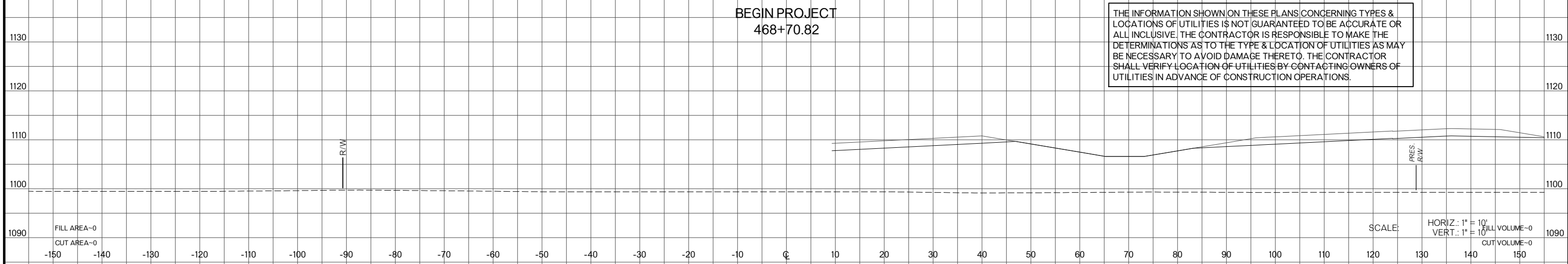
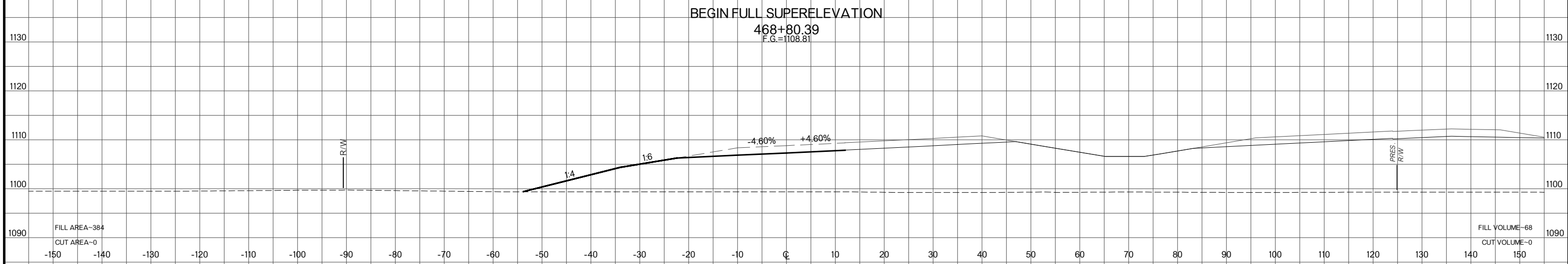
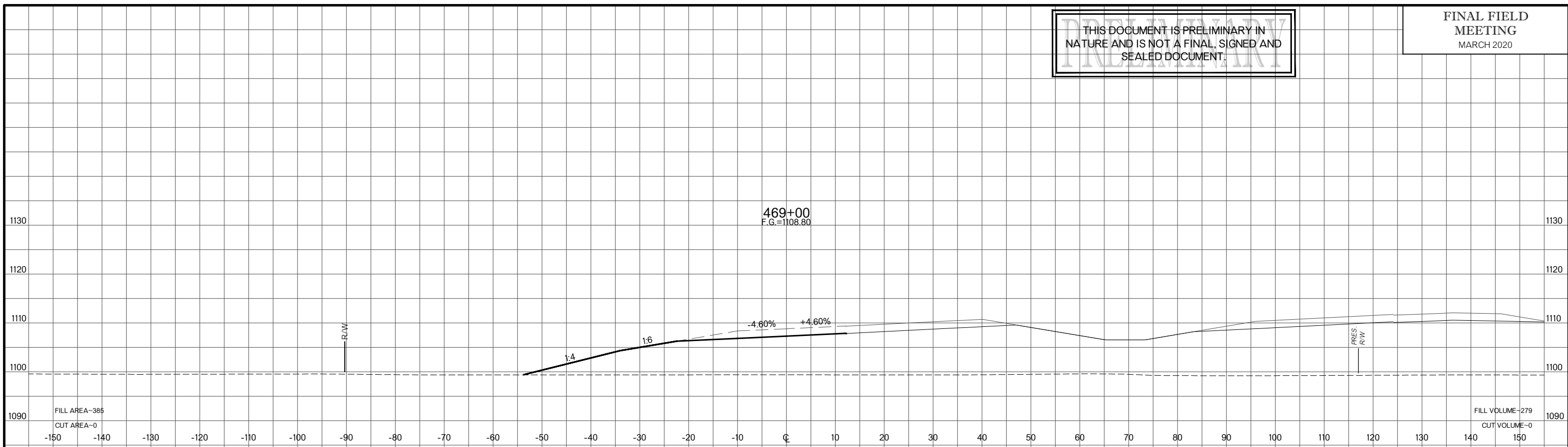
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

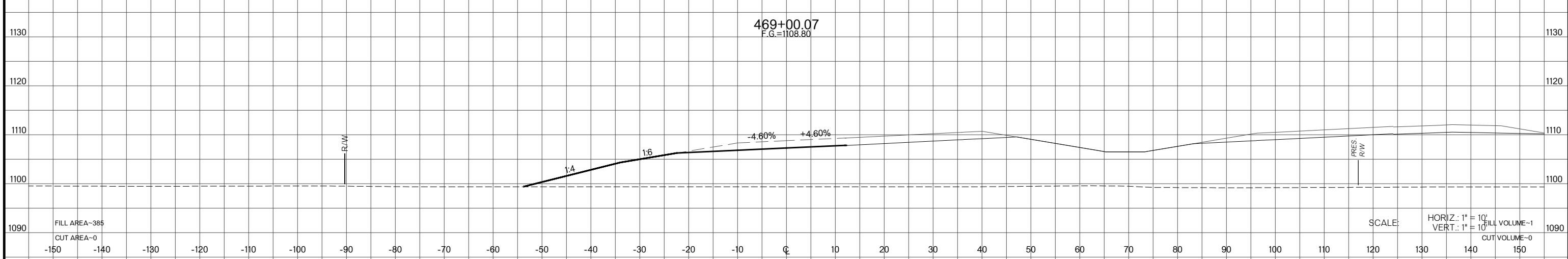
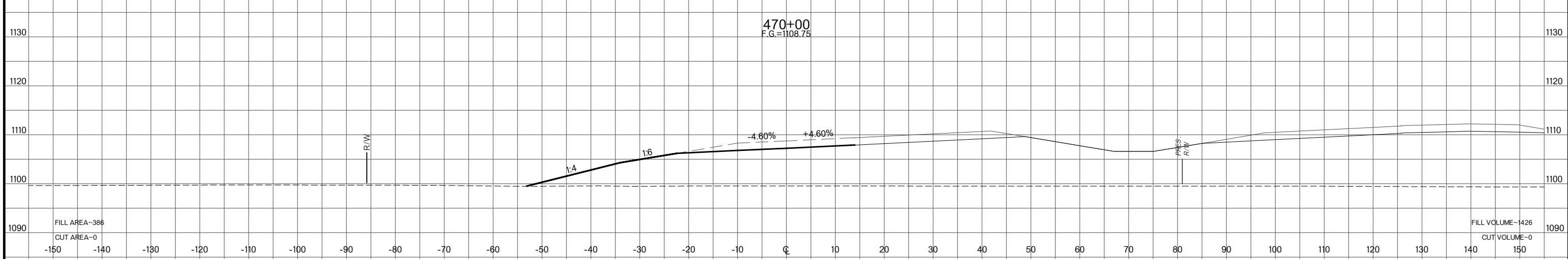
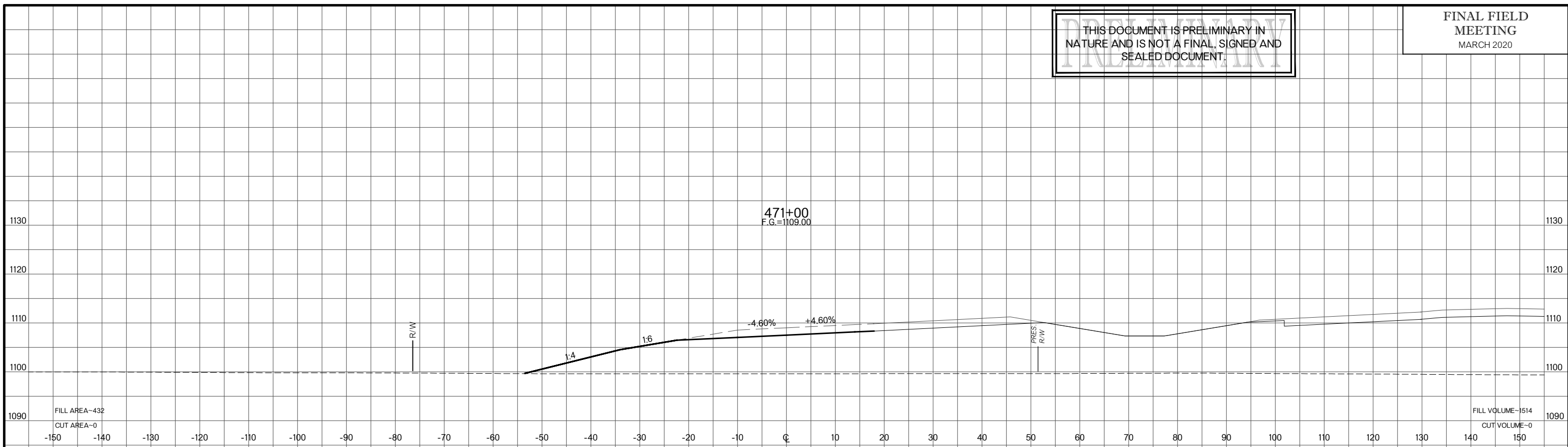
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP AA

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



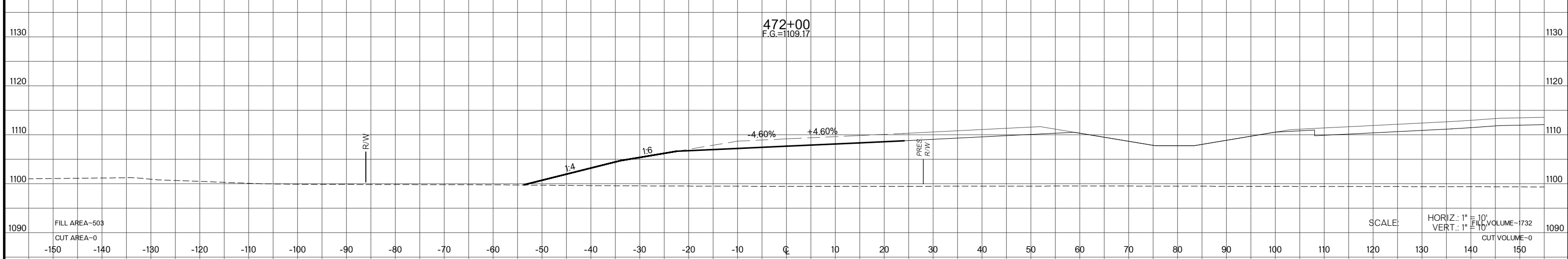
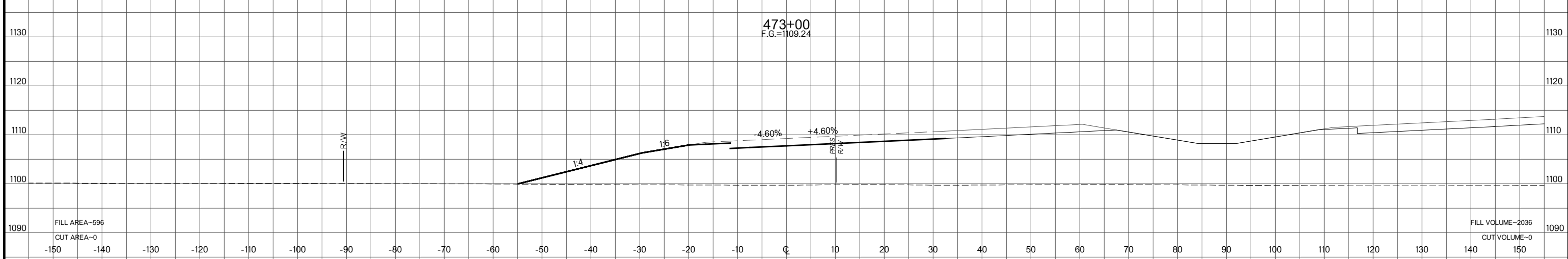
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP AA

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



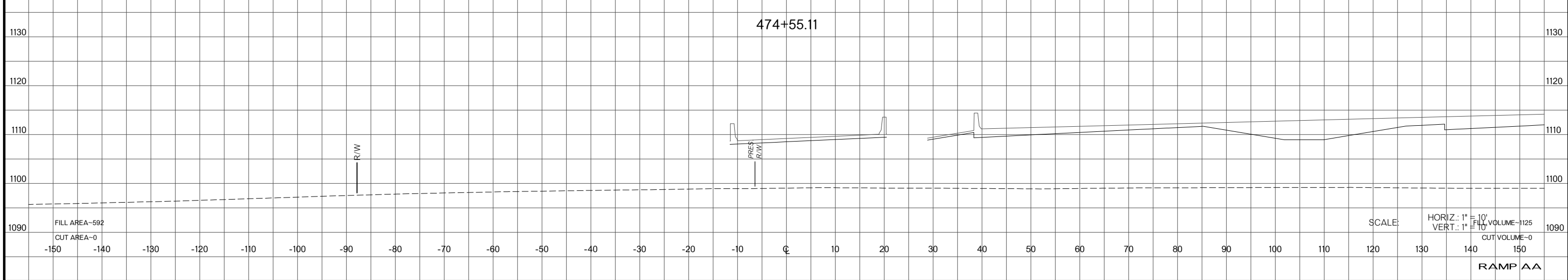
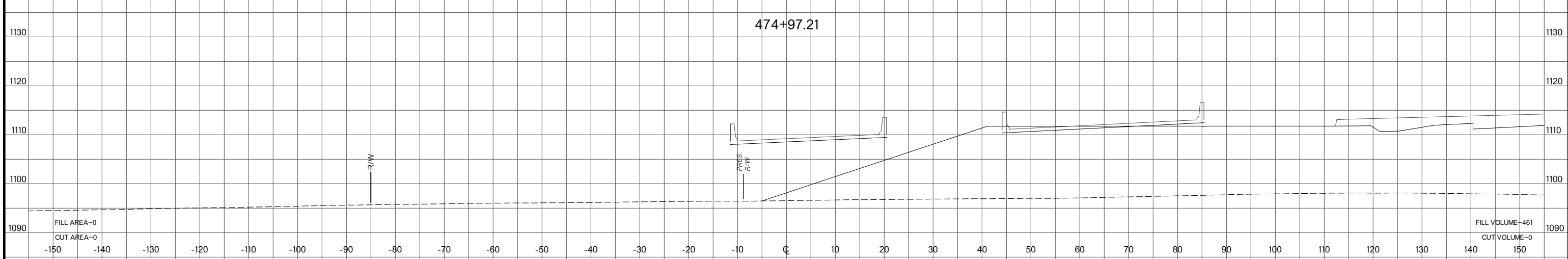
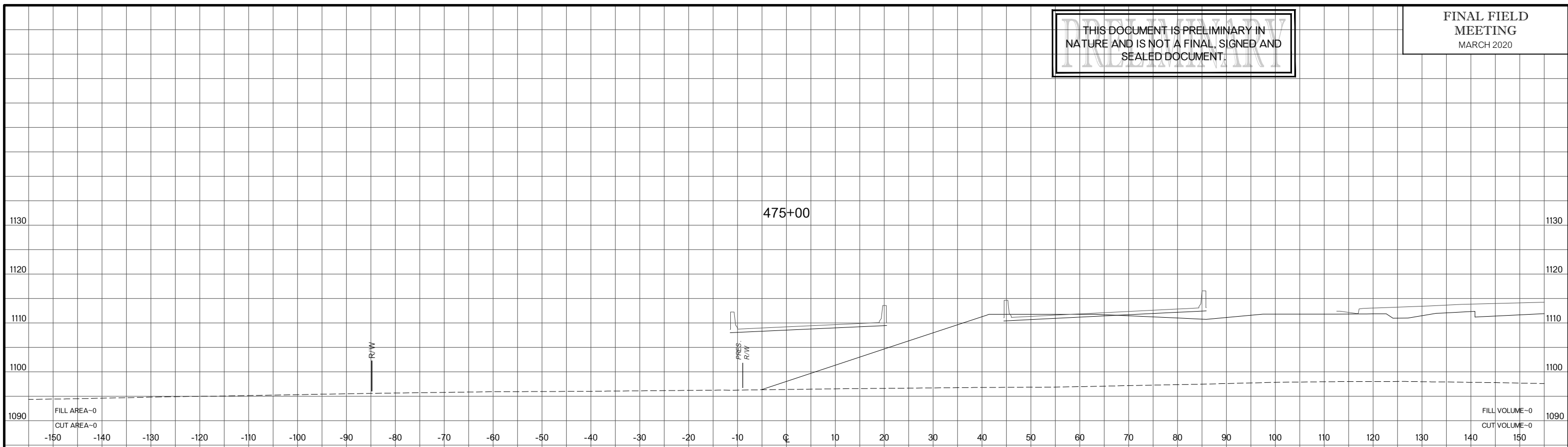
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP AA

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



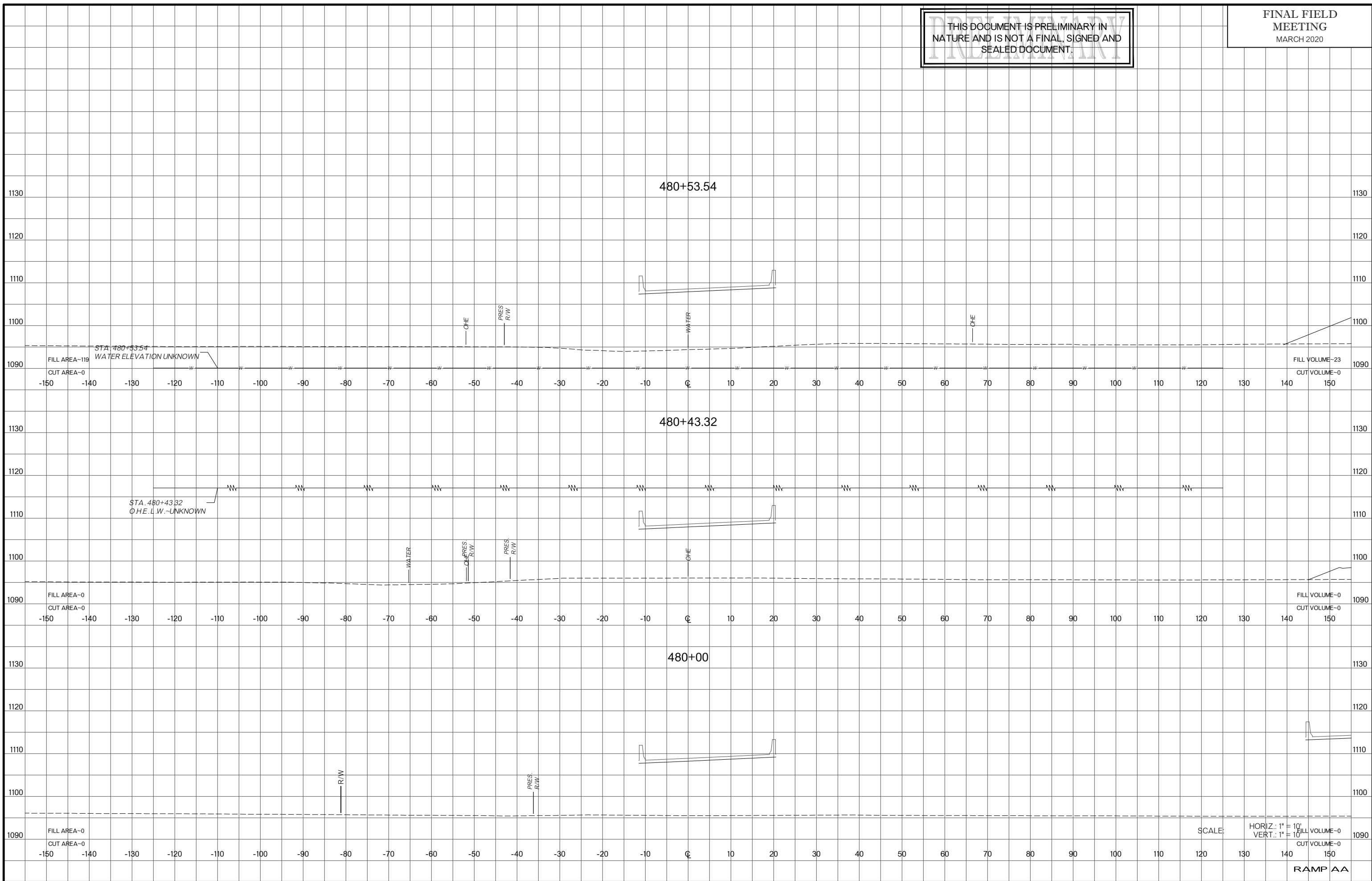
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP AA

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



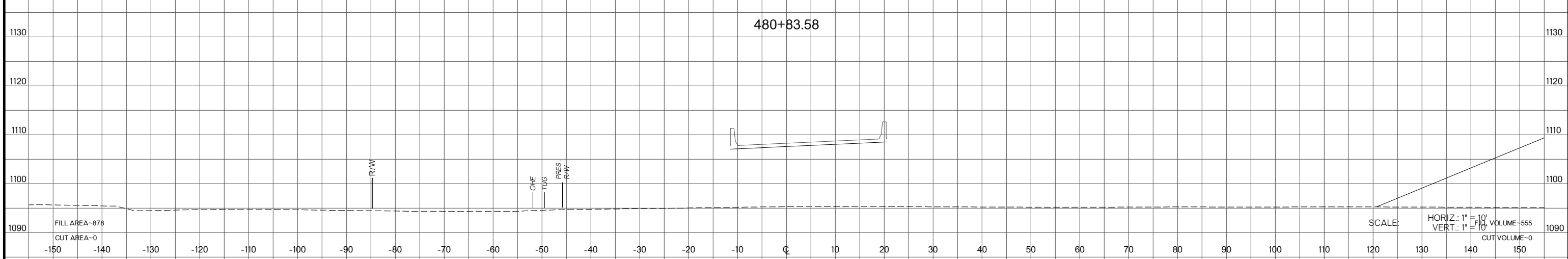
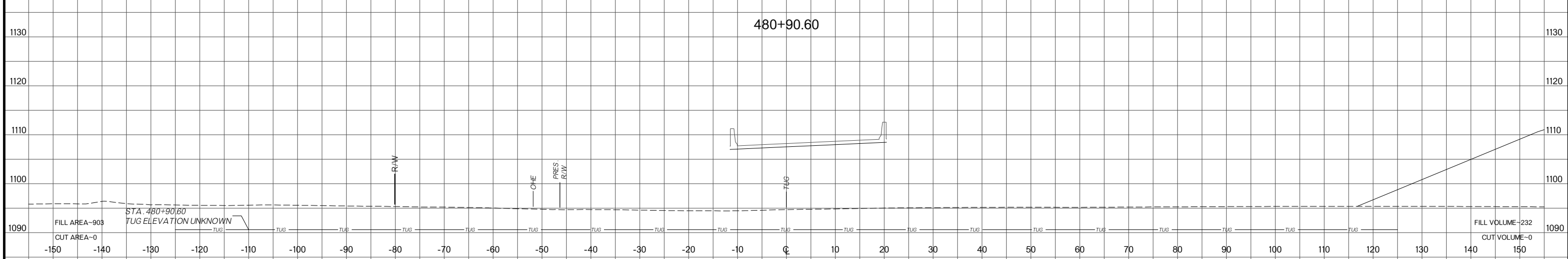
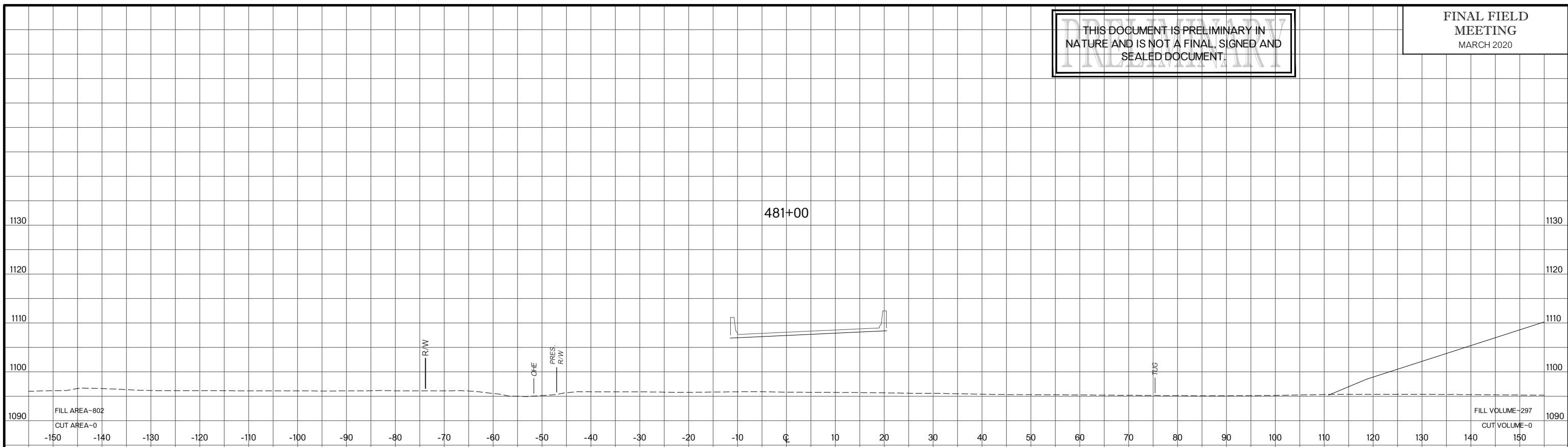
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP AA

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



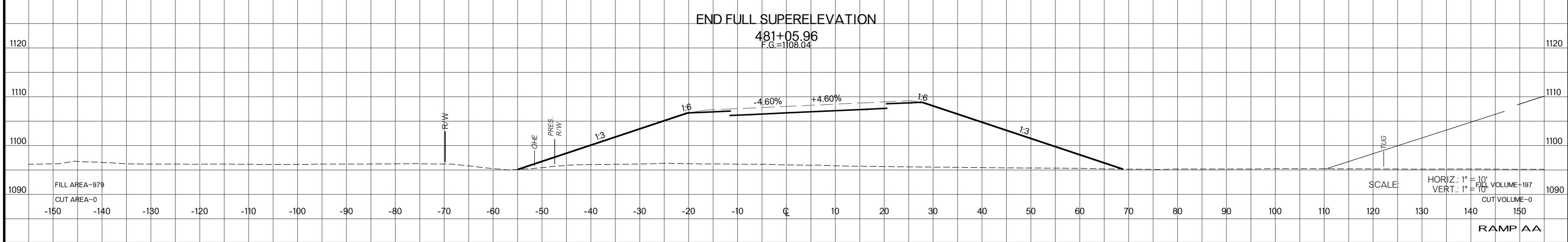
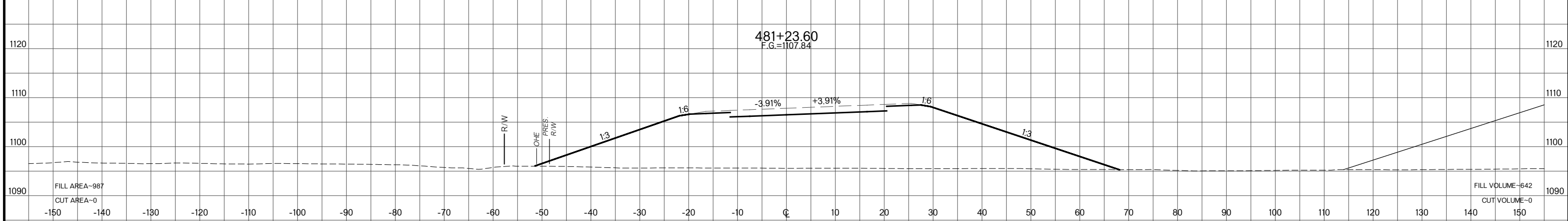
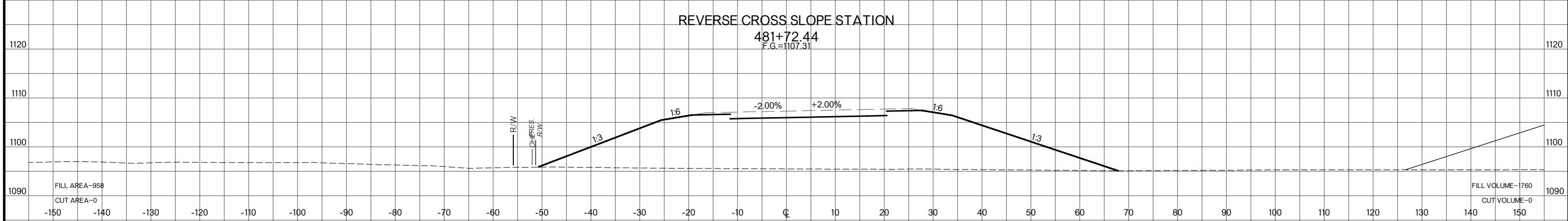
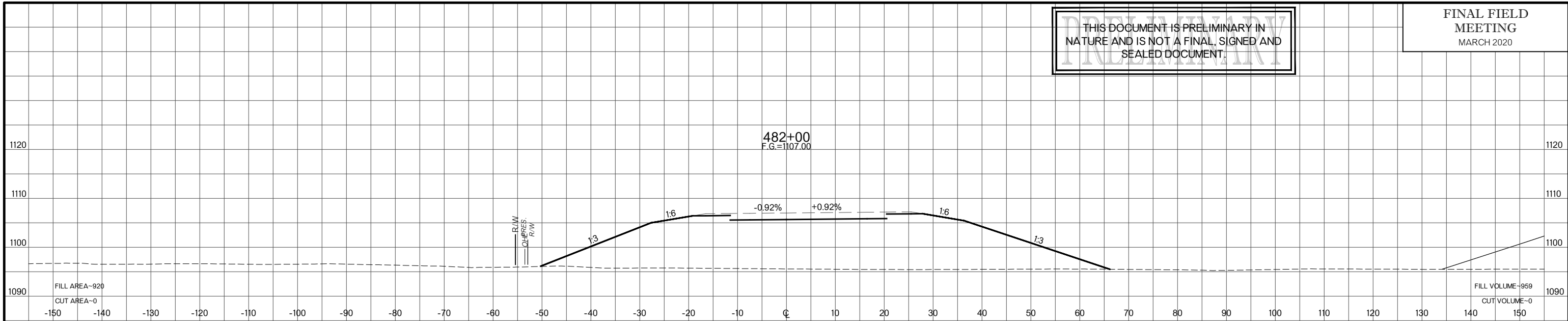
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP AA

US 81 REALIGNMENT
GRADY COUNTY

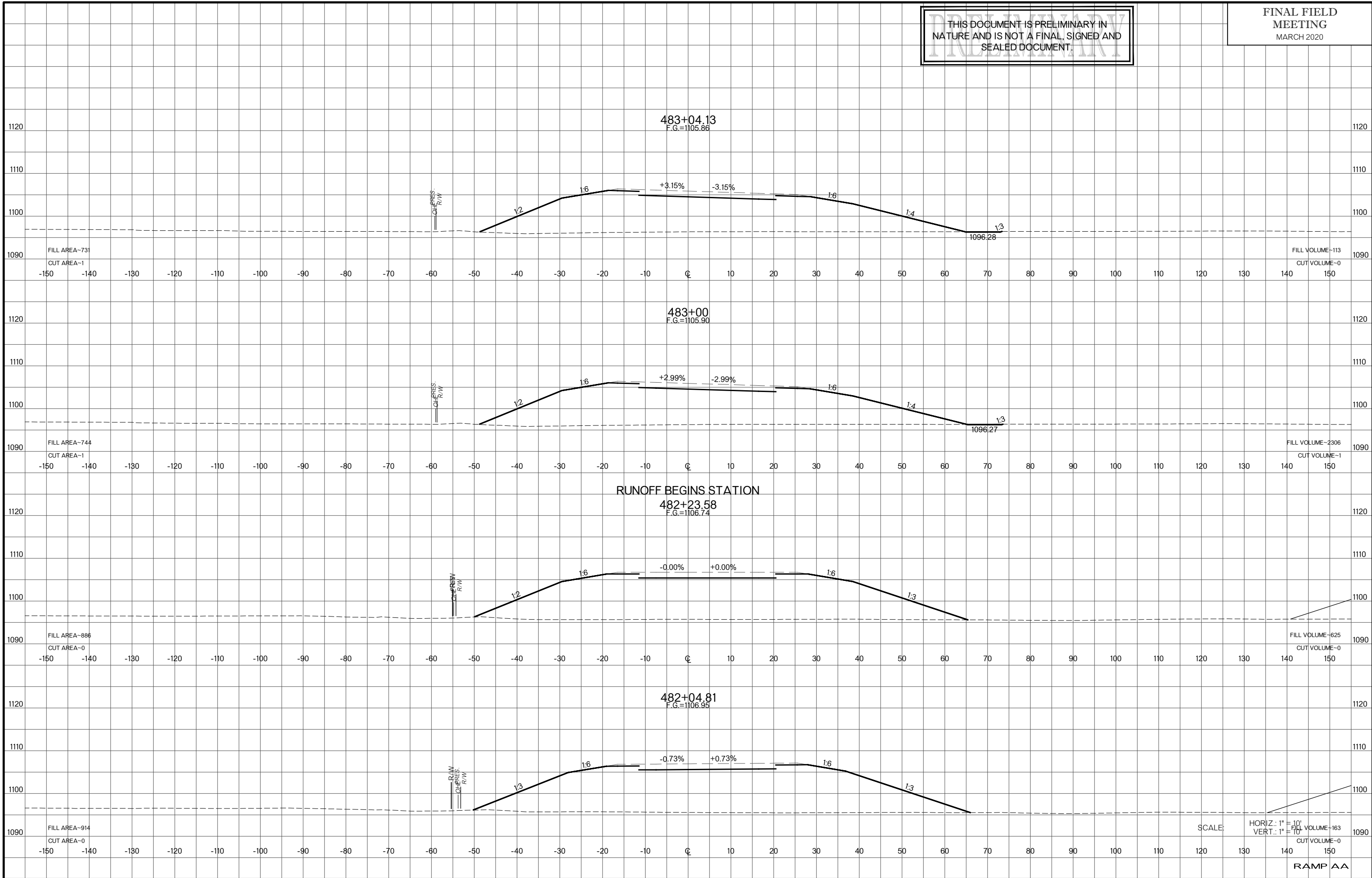
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



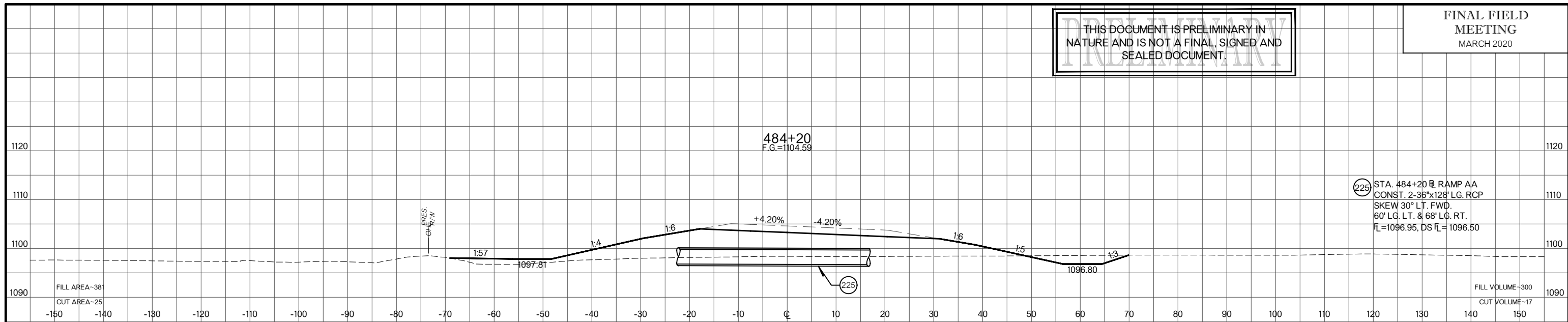
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP AA

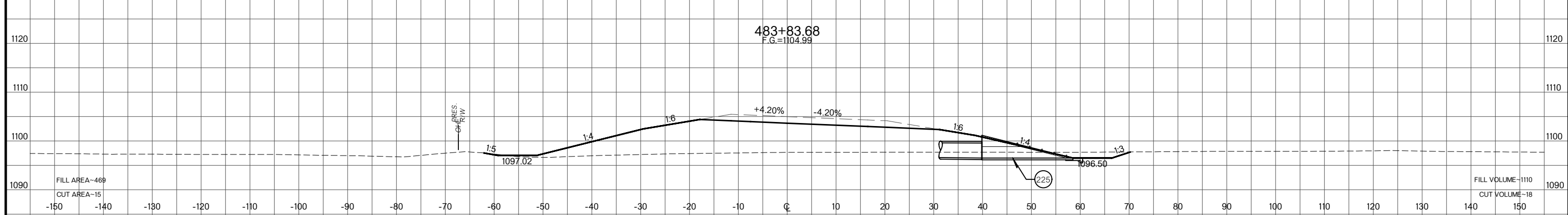
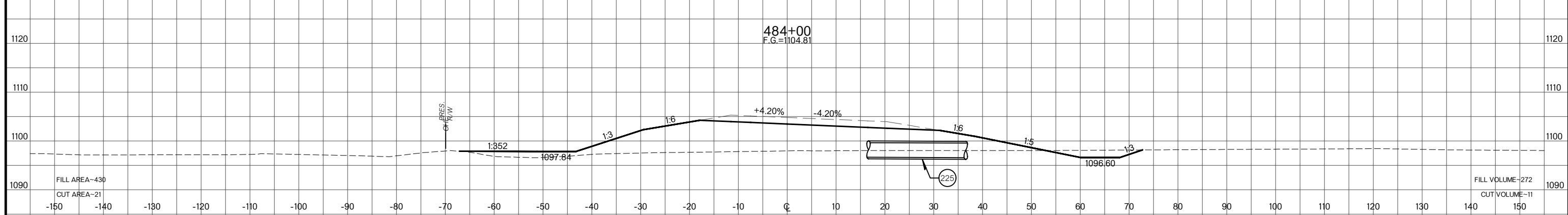
US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

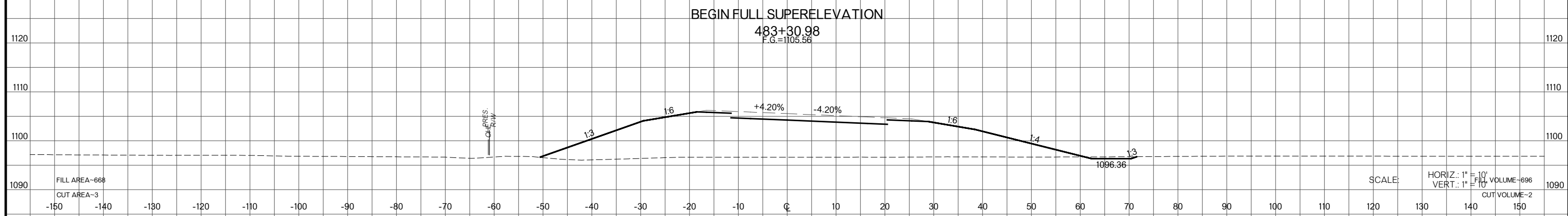
FINAL FIELD MEETING MARCH 2020



225 STA. 484+20 RAMP AA
CONST. 2-36"x128" LG. RCP
SKEW 30° LT. FWD.
60' LG. LT. & 68' LG. RT.
FL=1096.95, DS FL=1096.50



BEGIN FULL SUPERELEVATION



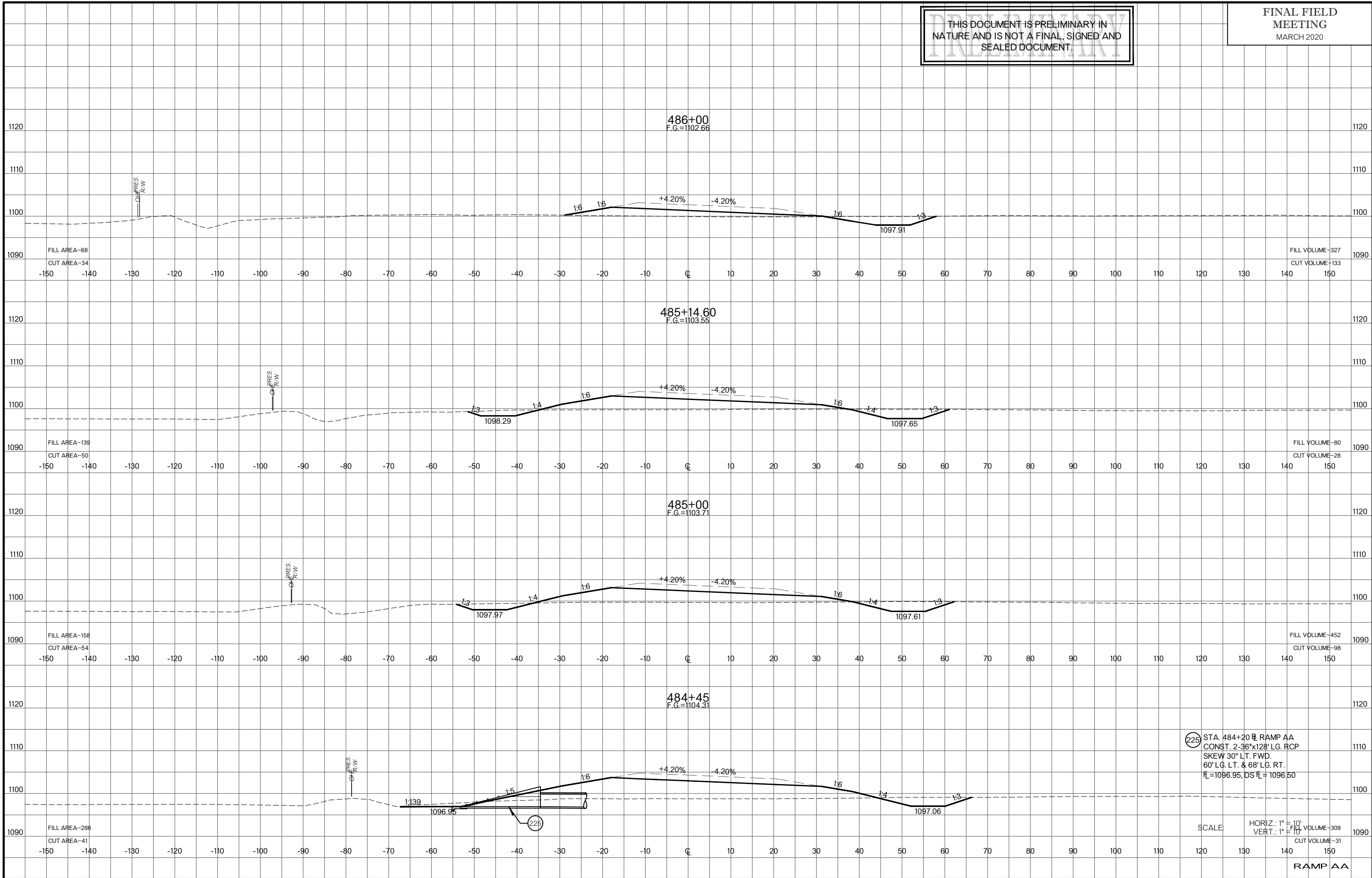
SCALE: HORIZ.: 1"=10' VERT.: 1"=10'

RAMP AA

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



225 STA. 484+20 RAMP AA
CONST. 2'-36"x128' LG. RCP
SKEW 30° LT. FWD.
60' LG. LT. & 68' LG. RT.
FL=1096.95, DS FL=1096.50

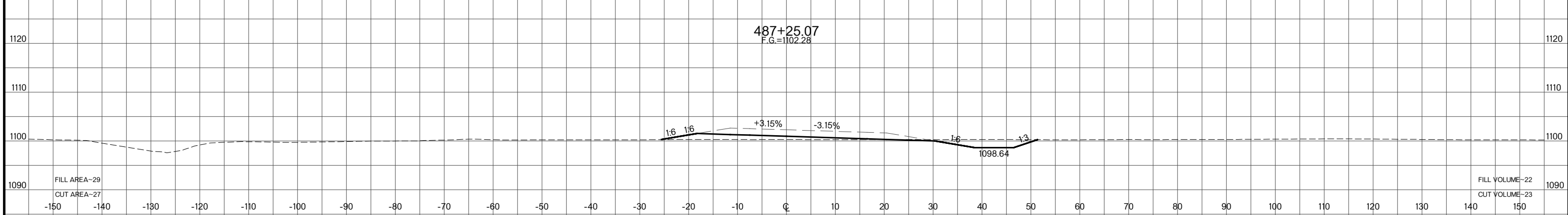
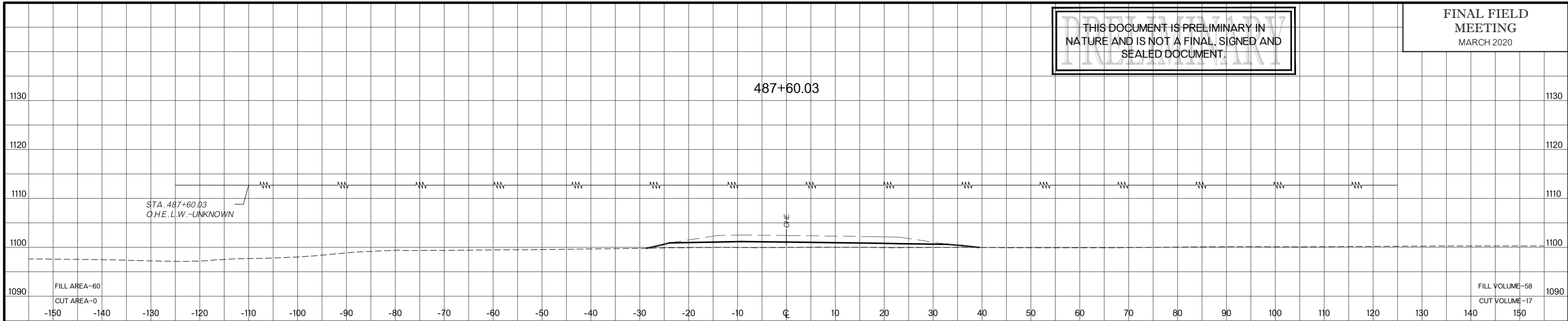
SCALE: HORIZ.: 1"=10'
VERT.: 1"=10'

RAMP AA

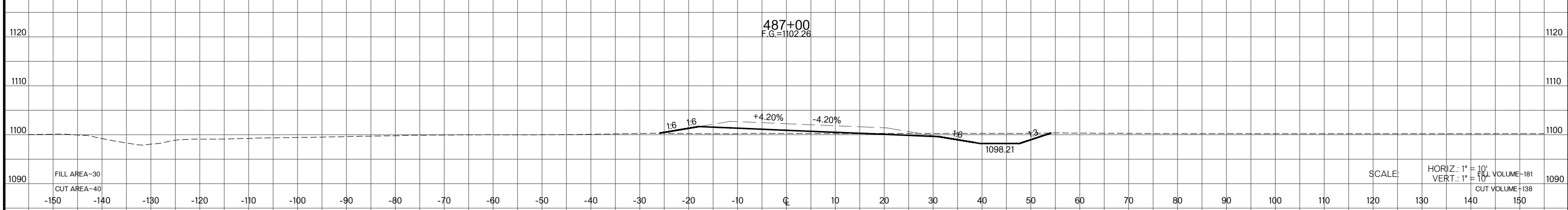
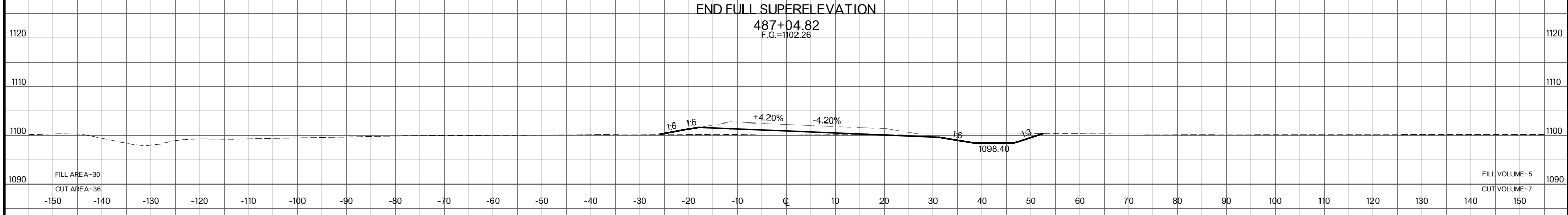
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



END FULL SUPERELEVATION



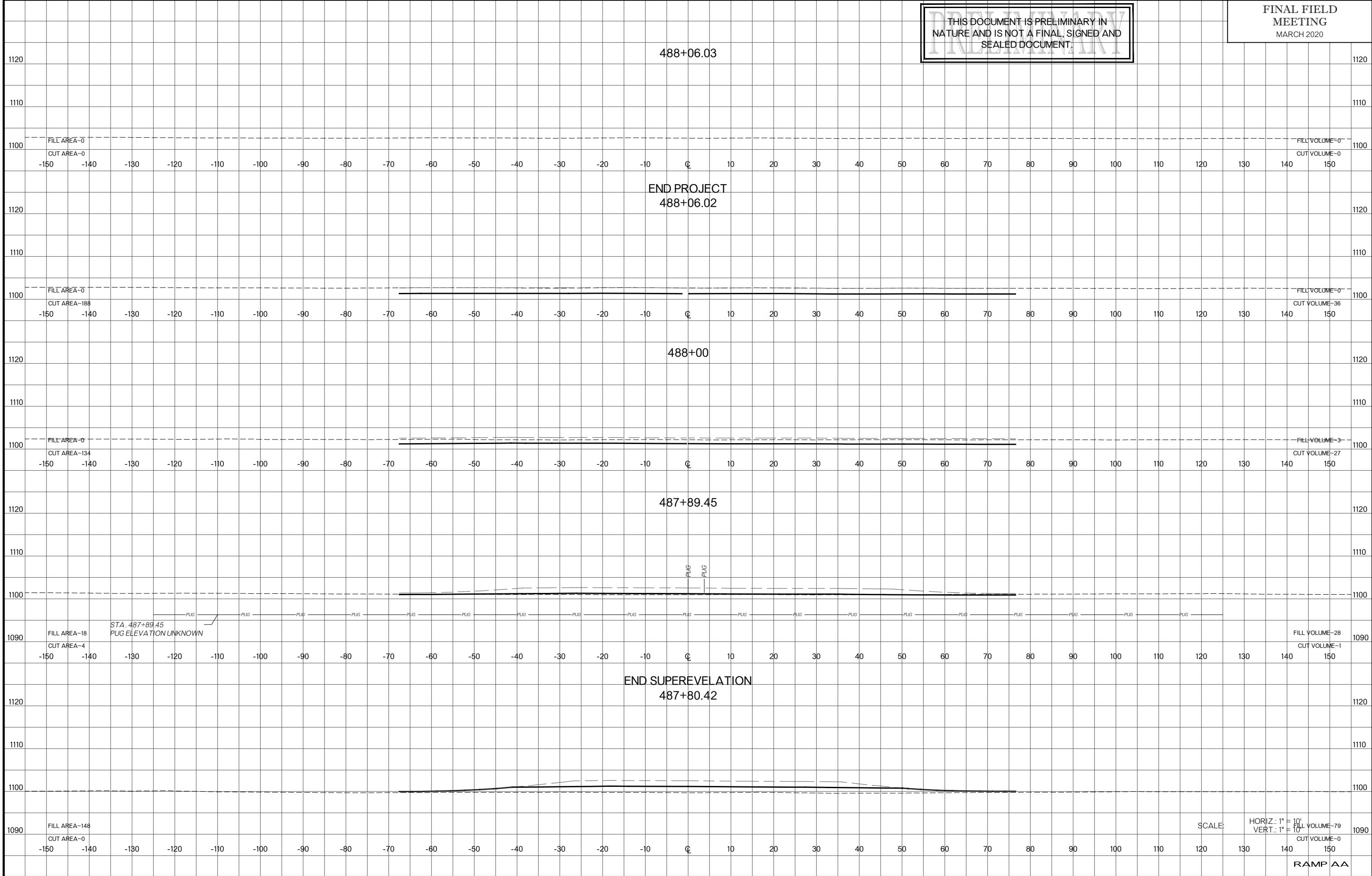
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP AA

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



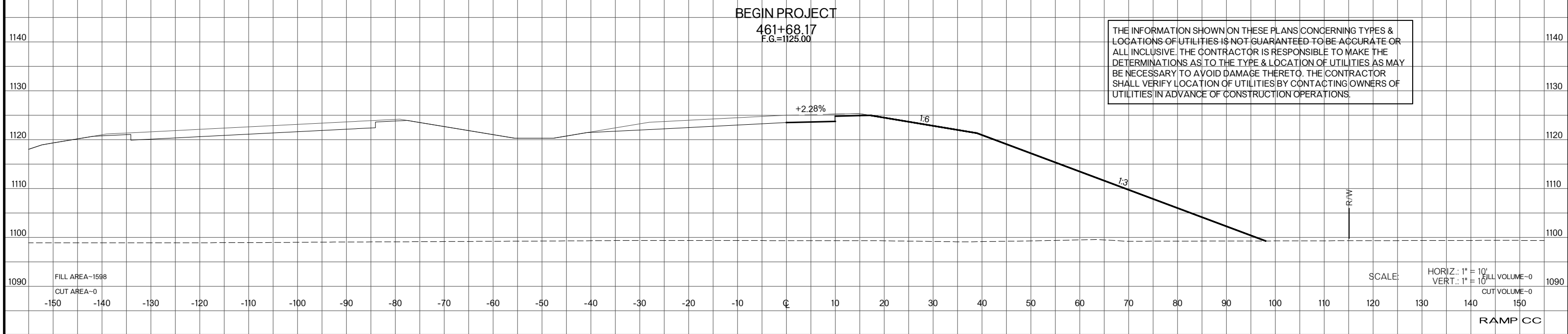
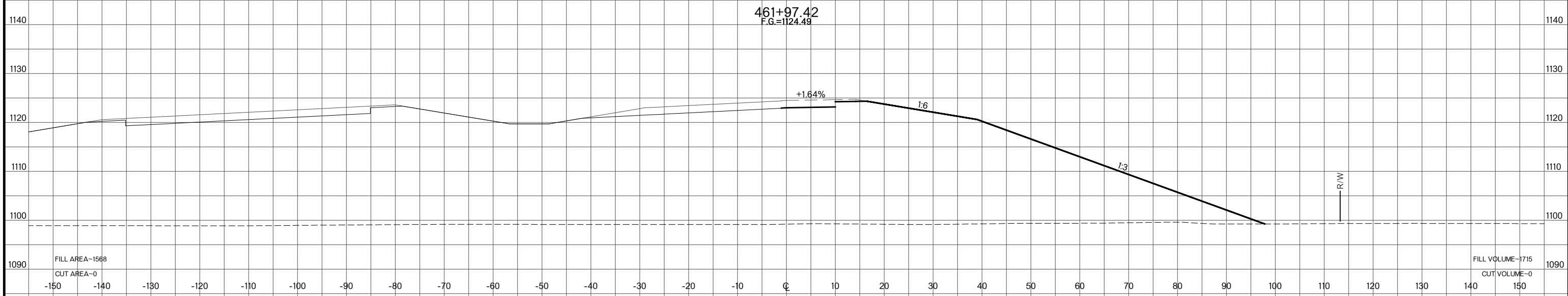
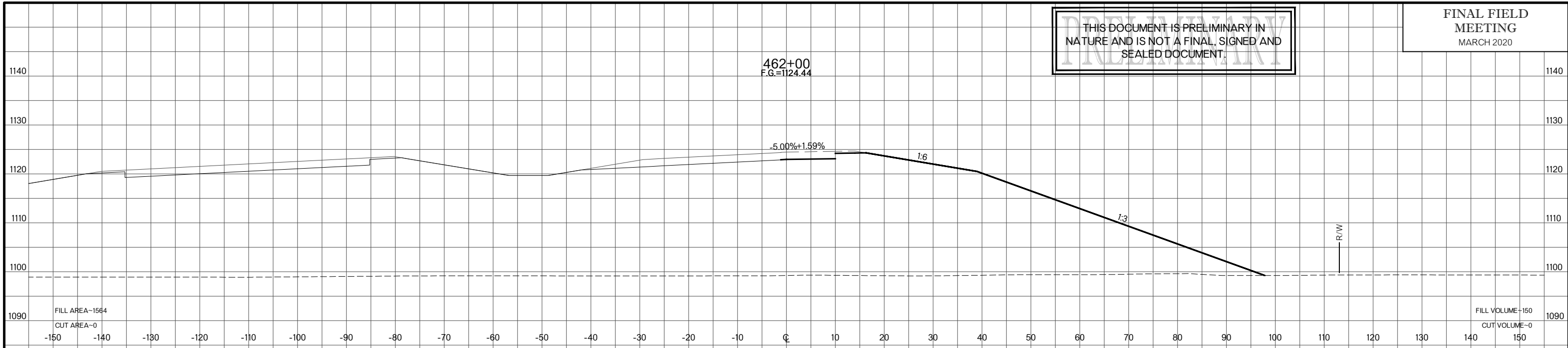
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP AA

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP CC

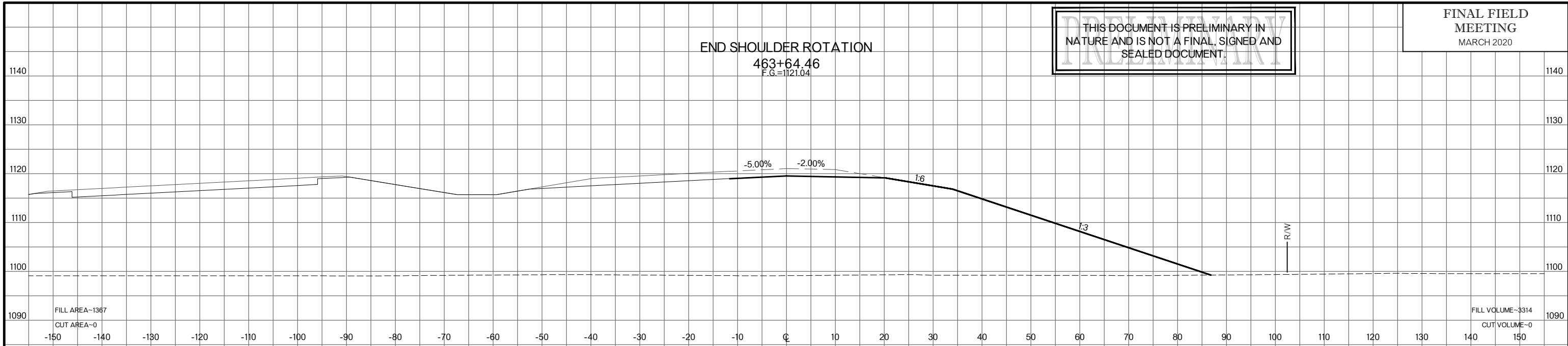
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

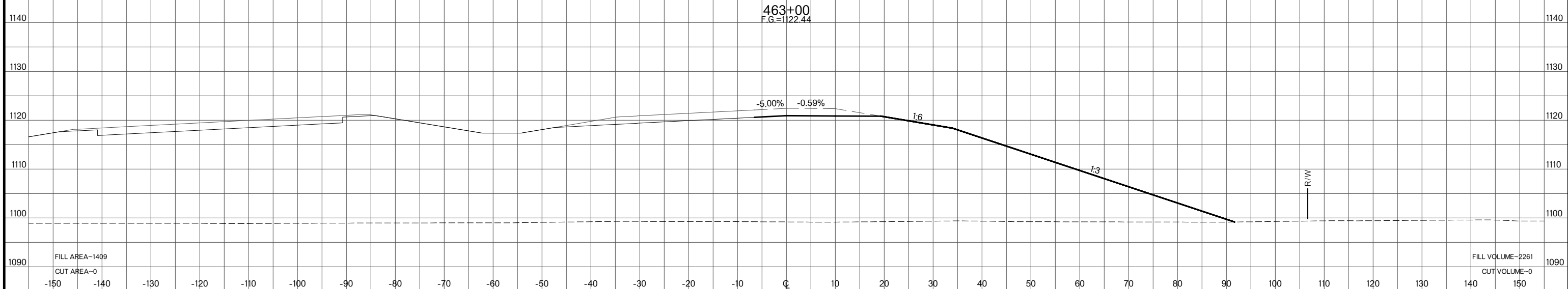
FINAL FIELD MEETING
MARCH 2020

END SHOULDER ROTATION

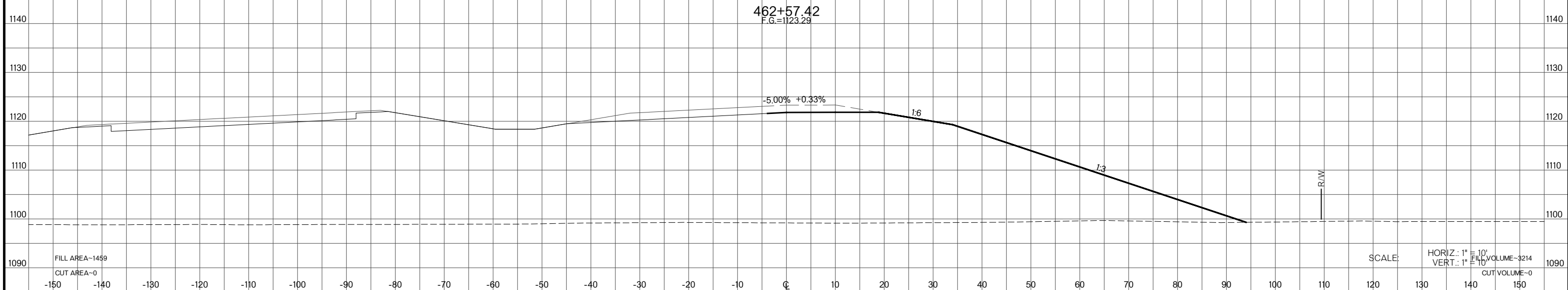
463+64.46
F.G.=1121.04



463+00
F.G.=1122.44



462+57.42
F.G.=1123.29

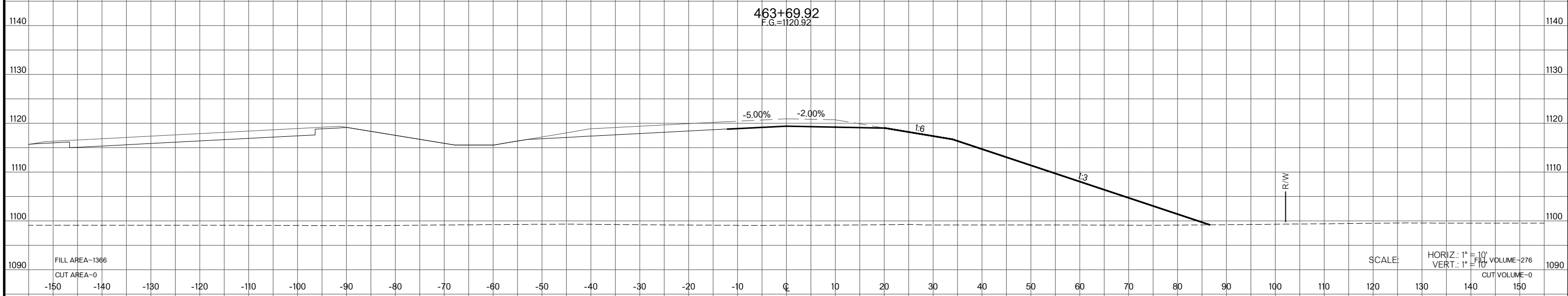
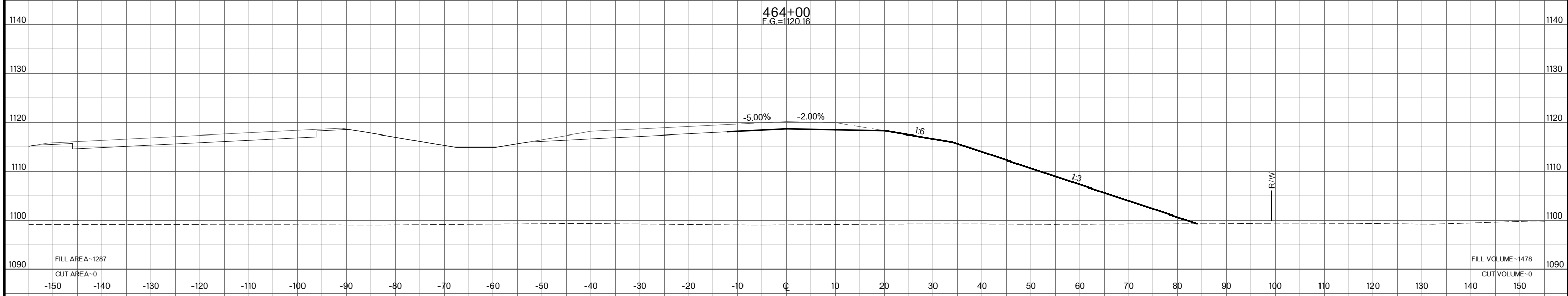
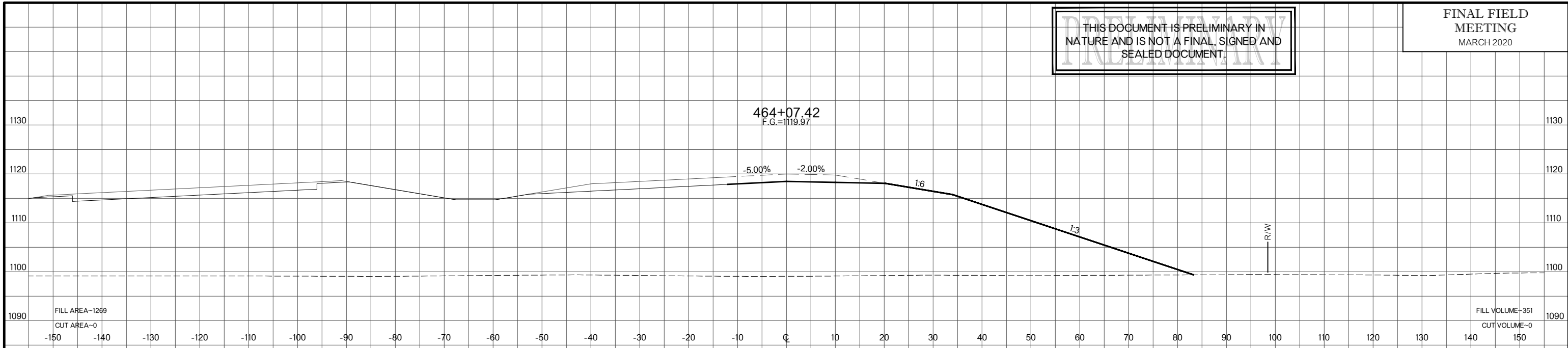


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP CC

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



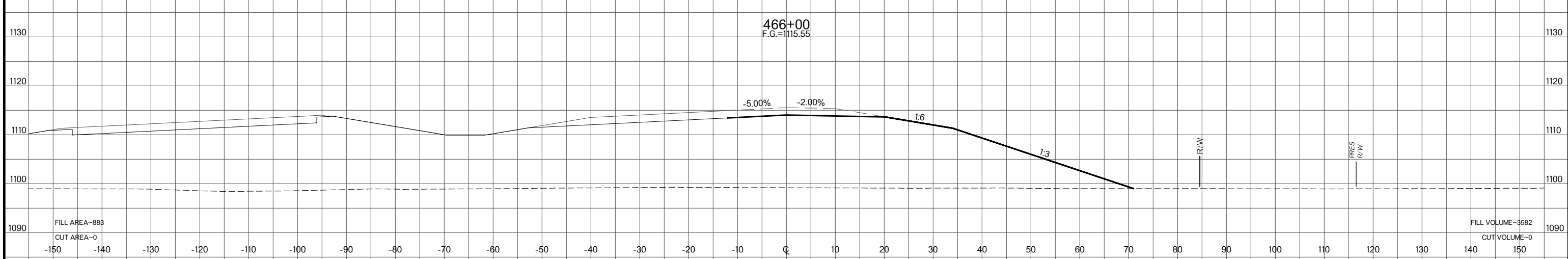
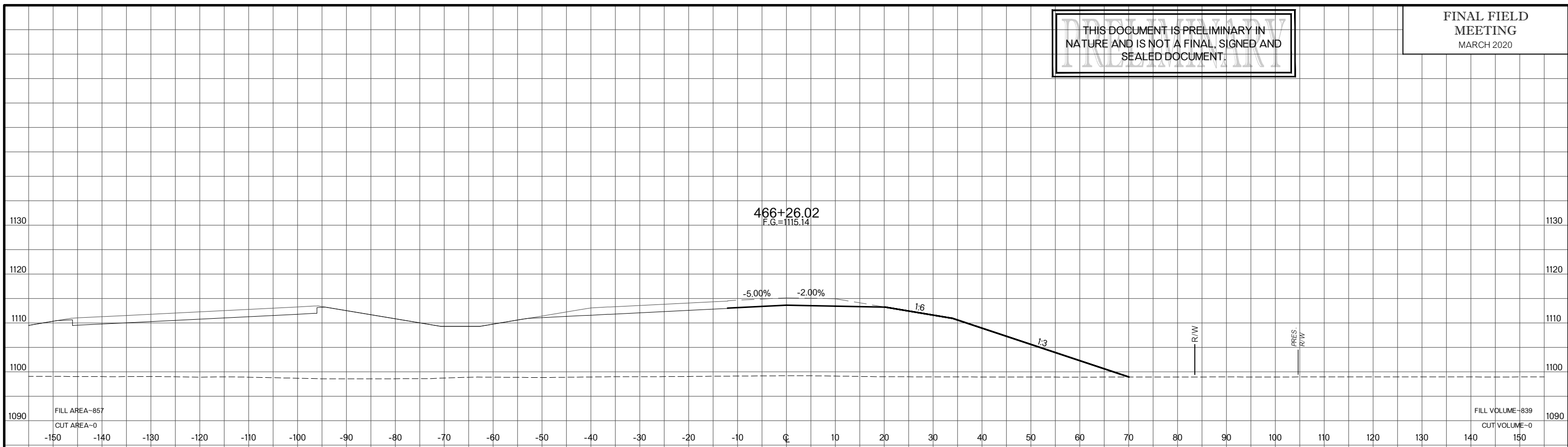
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP CC

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



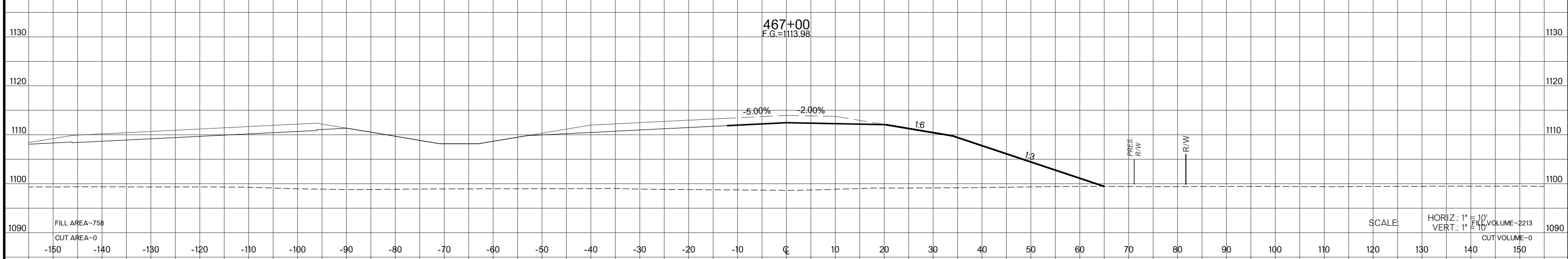
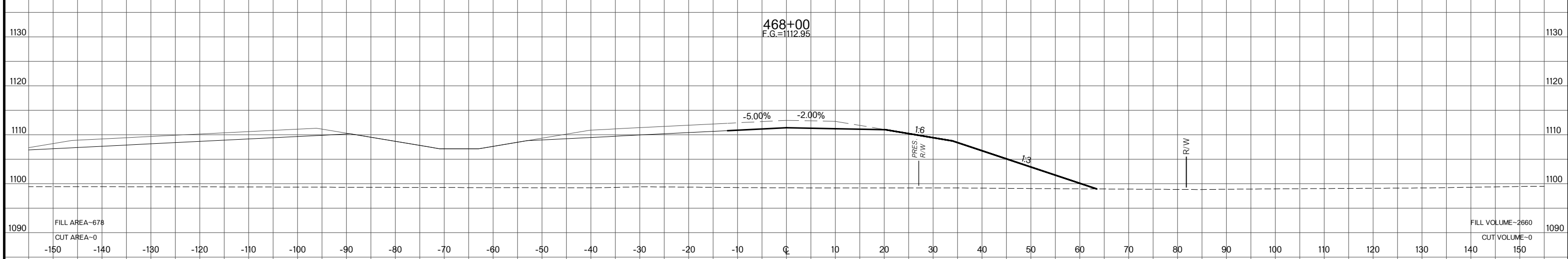
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP CC

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



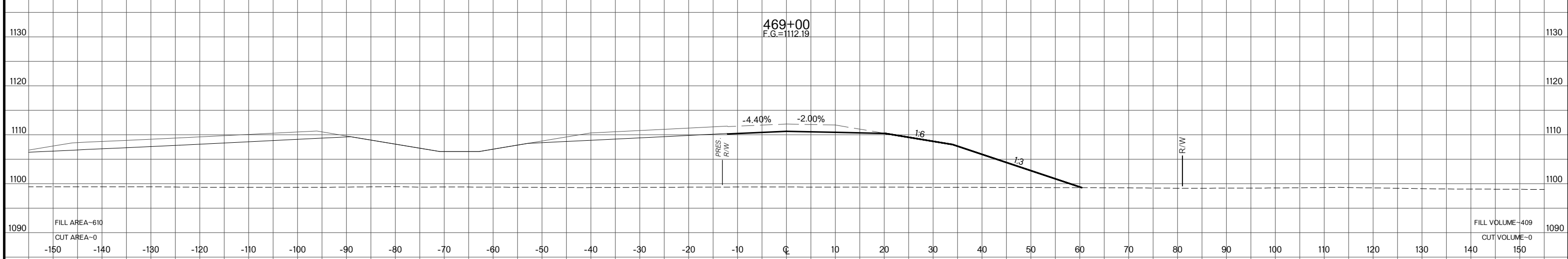
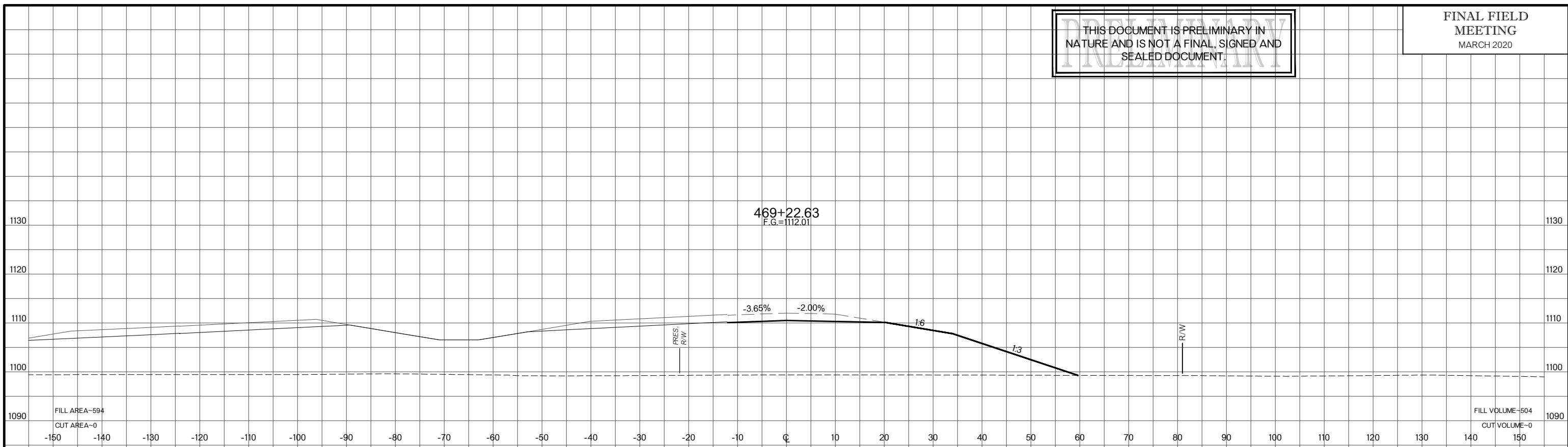
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP CC

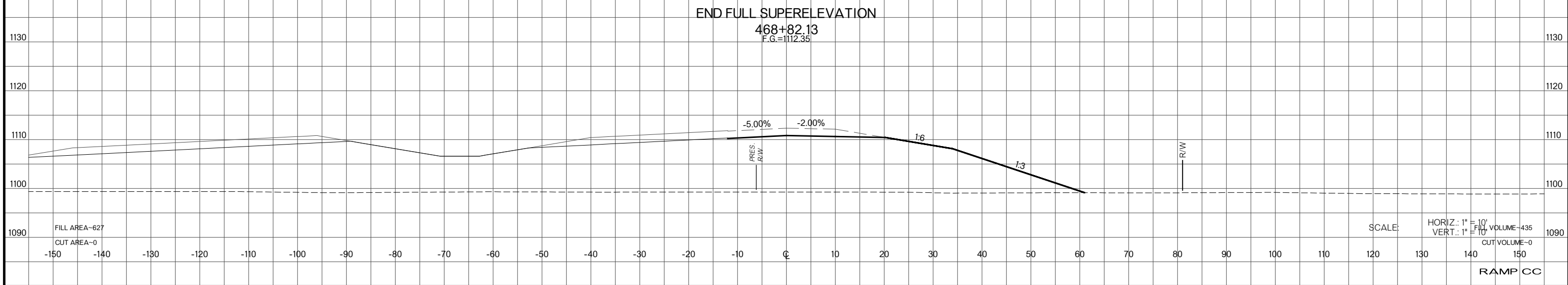
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



END FULL SUPERELEVATION



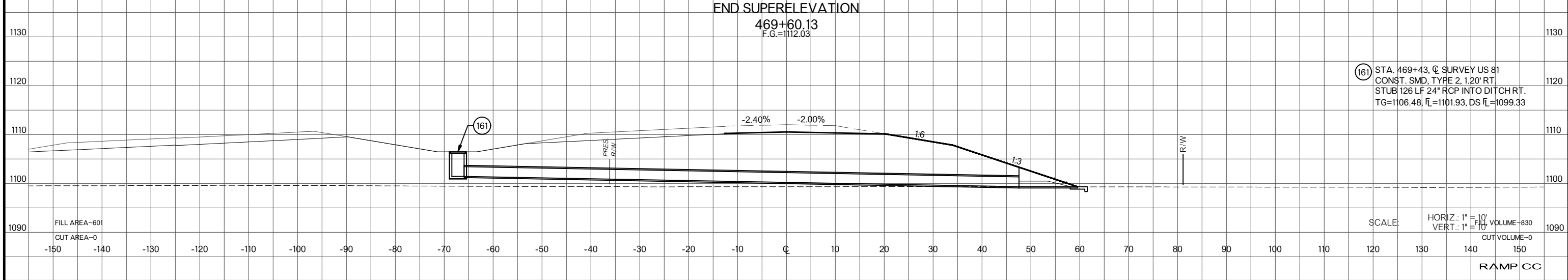
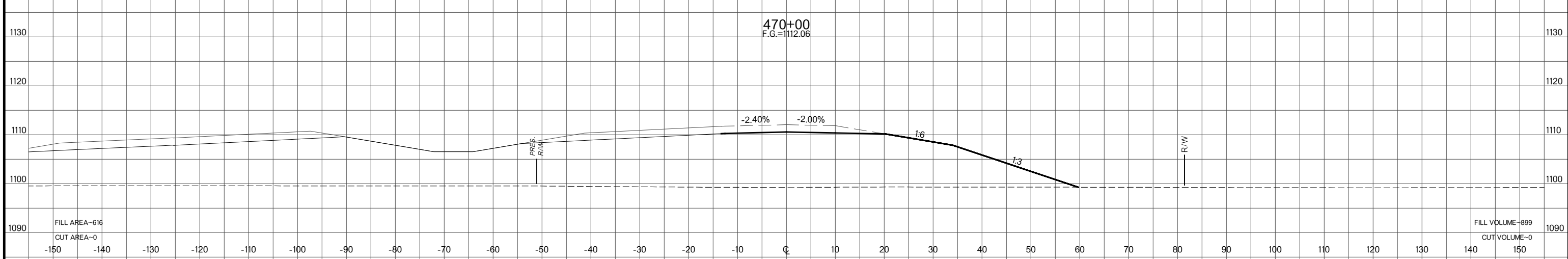
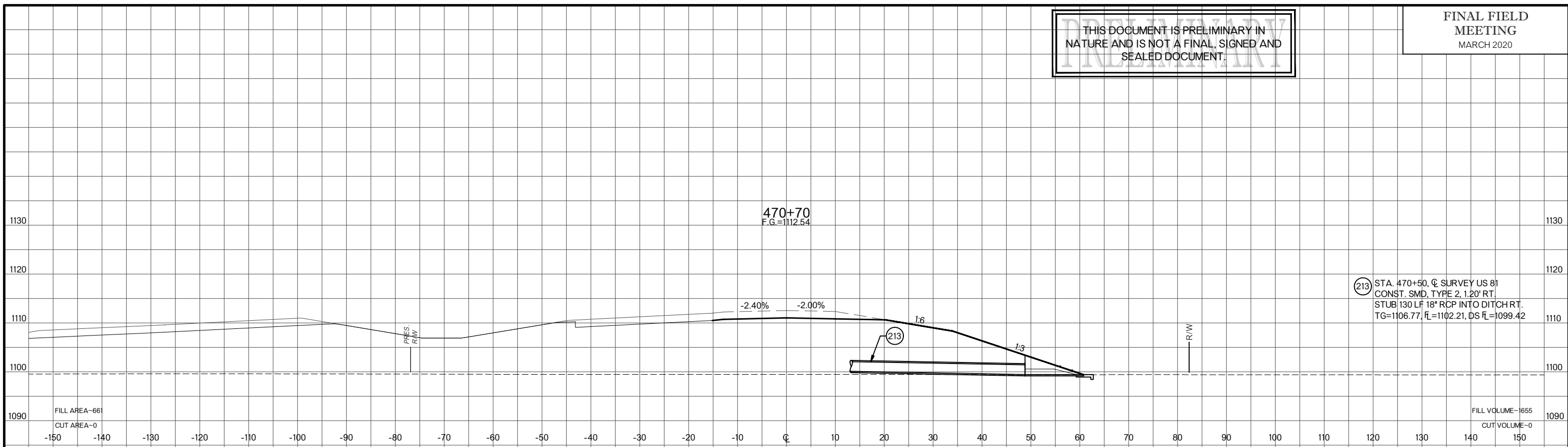
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP CC

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

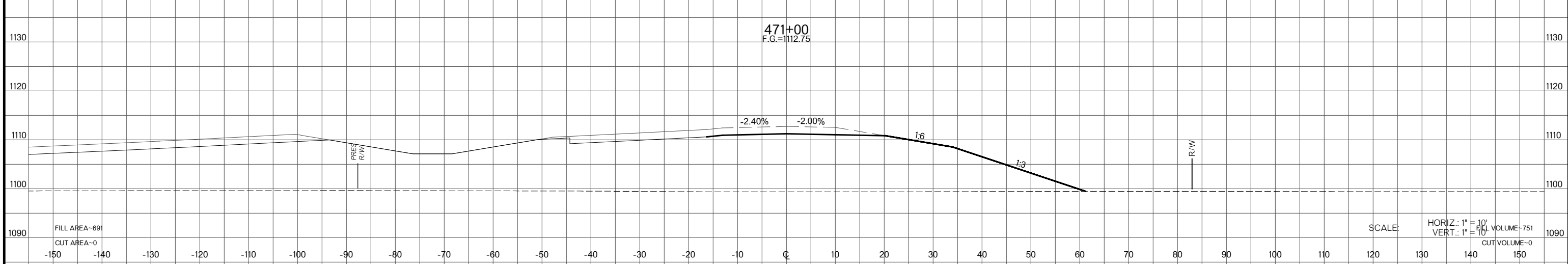
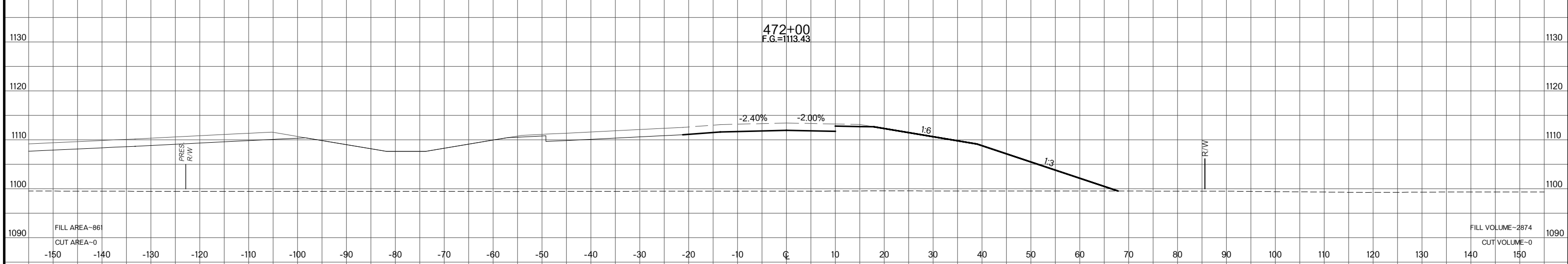
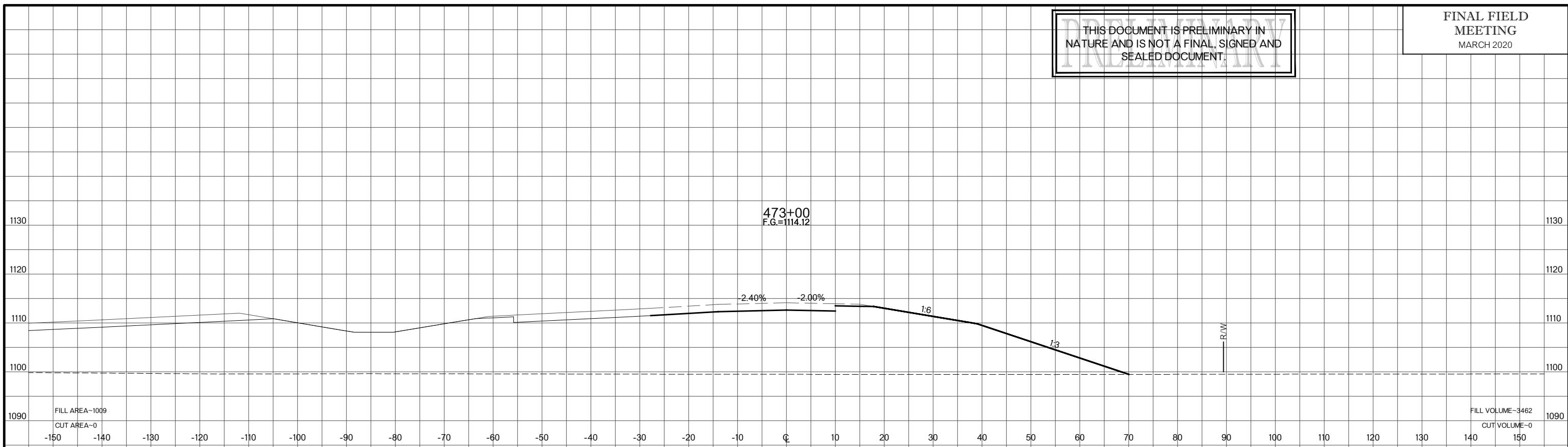
FINAL FIELD MEETING
MARCH 2020



US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



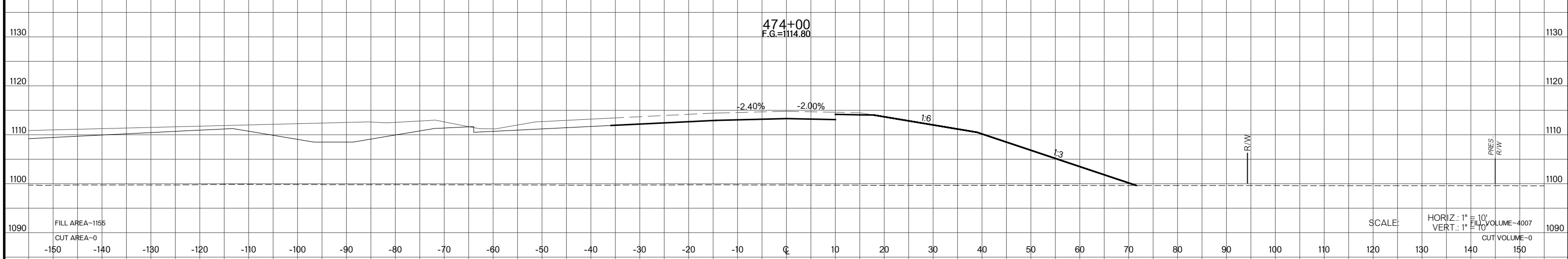
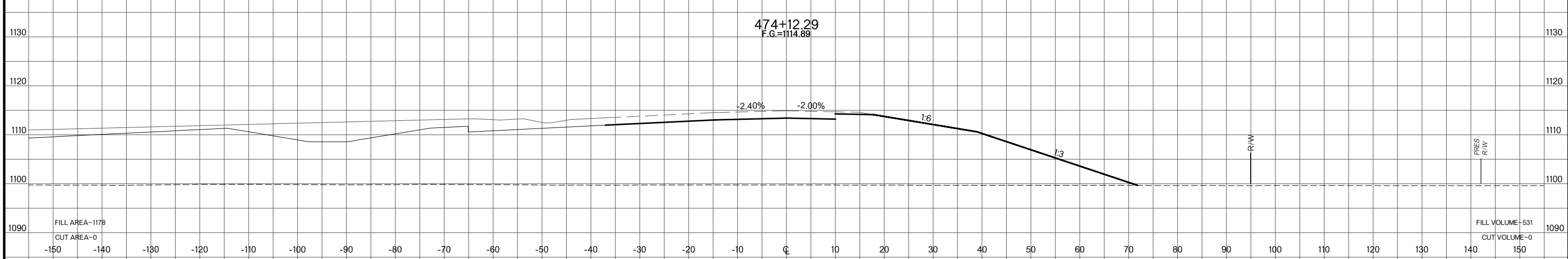
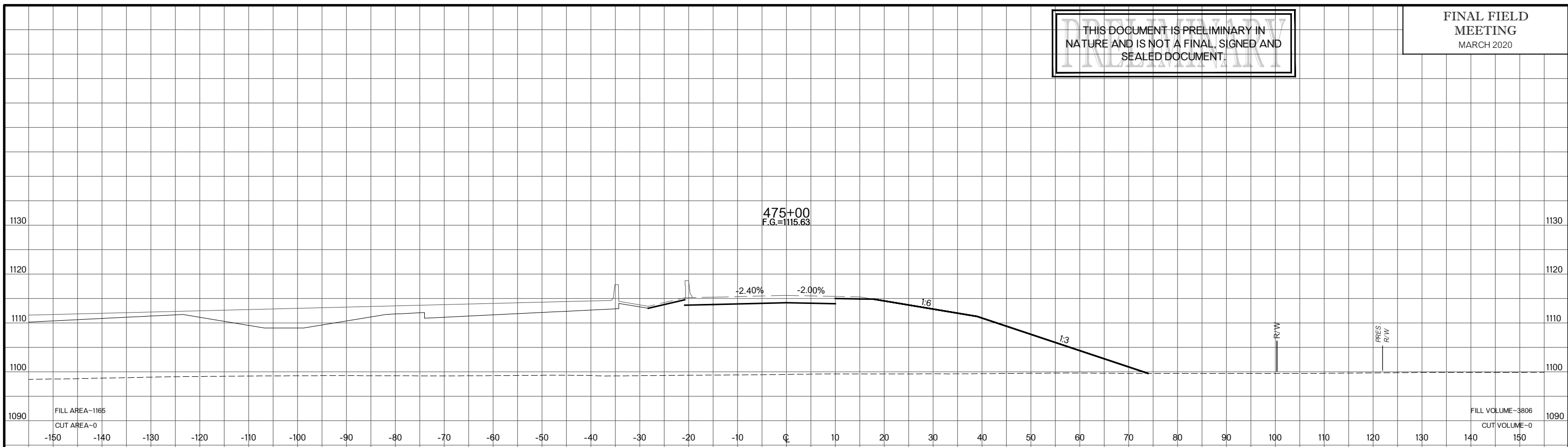
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP CC

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



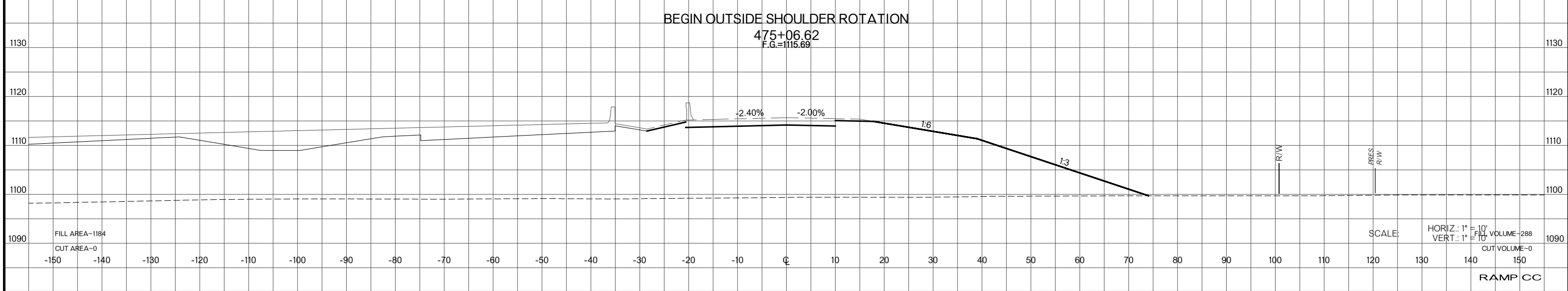
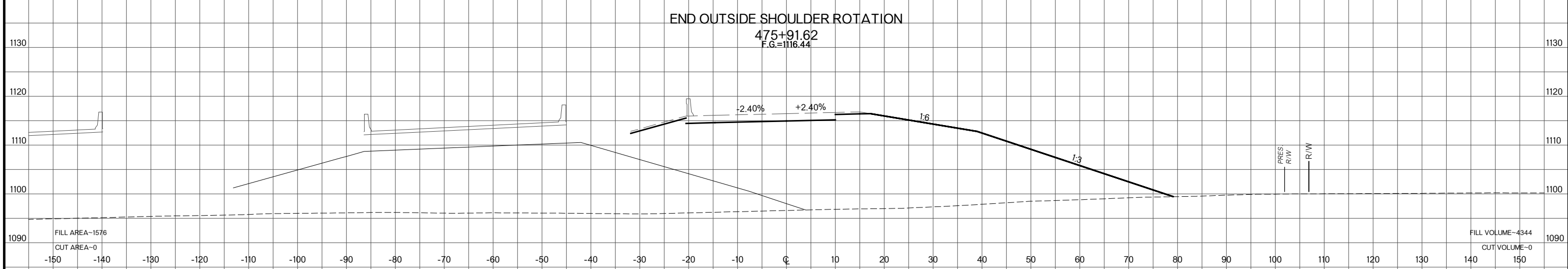
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP CC

US 81 REALIGNMENT
GRADY COUNTY

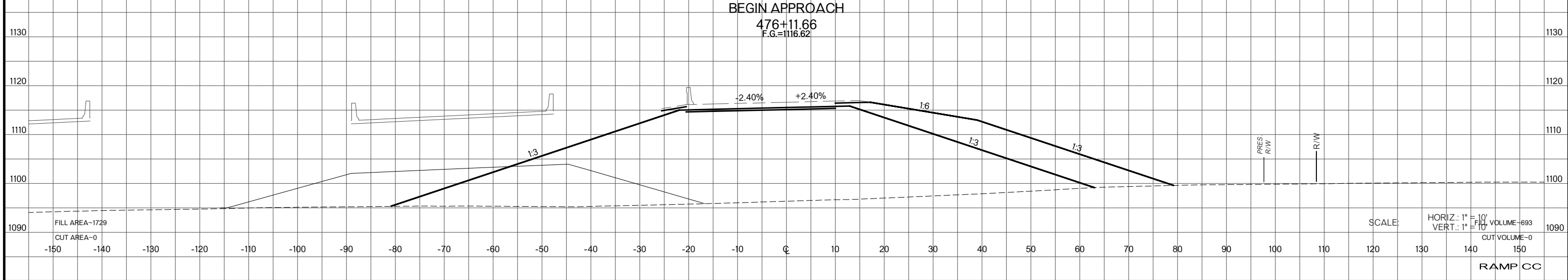
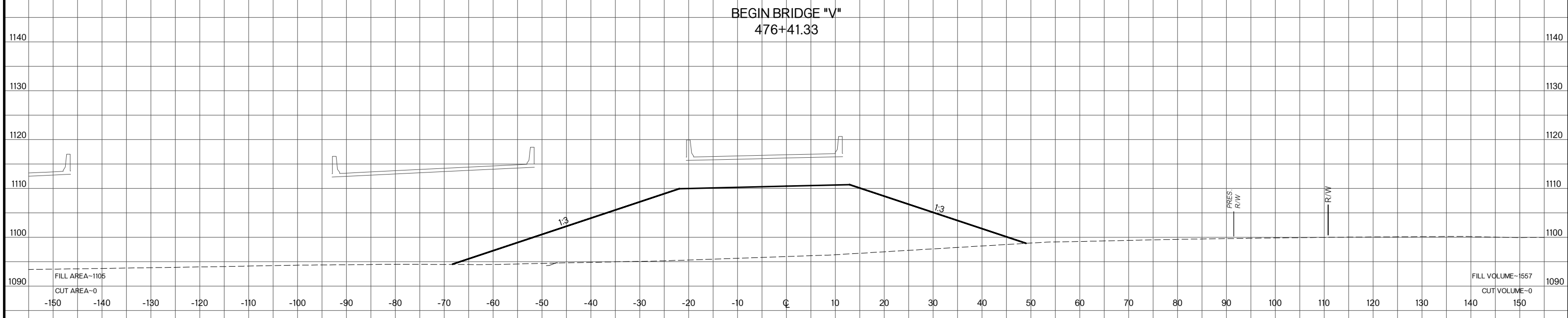
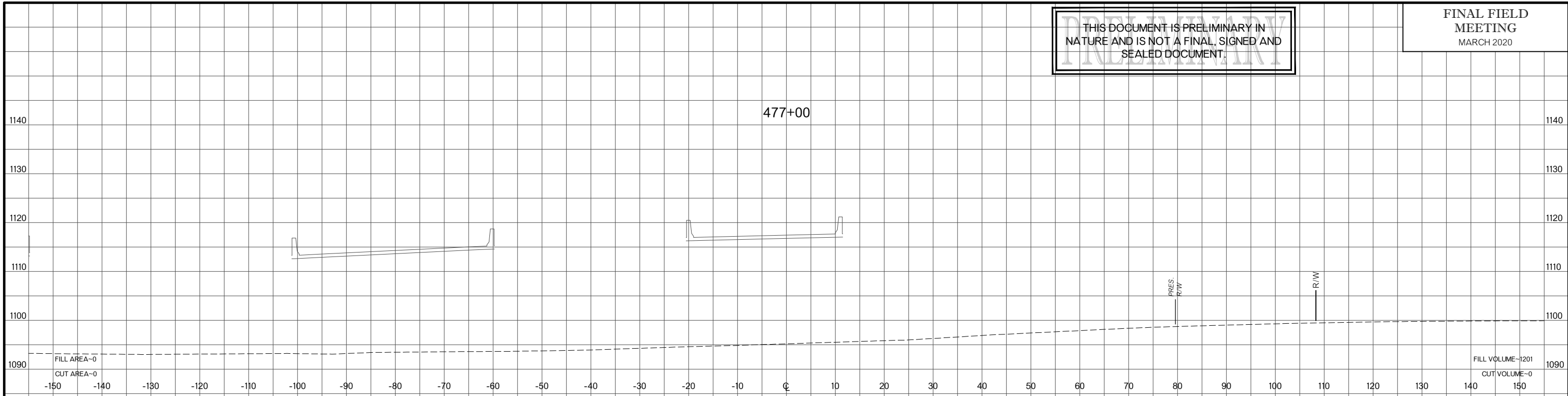
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

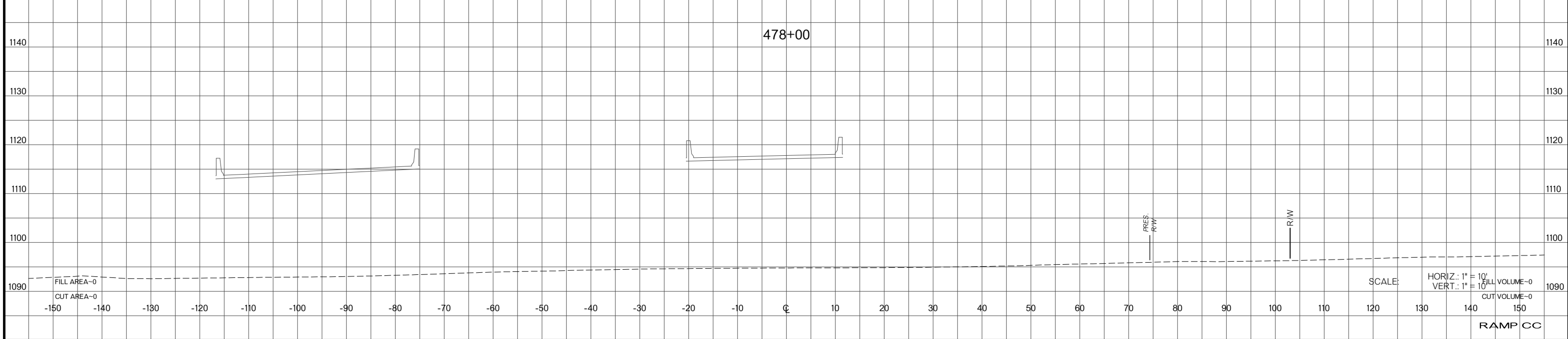
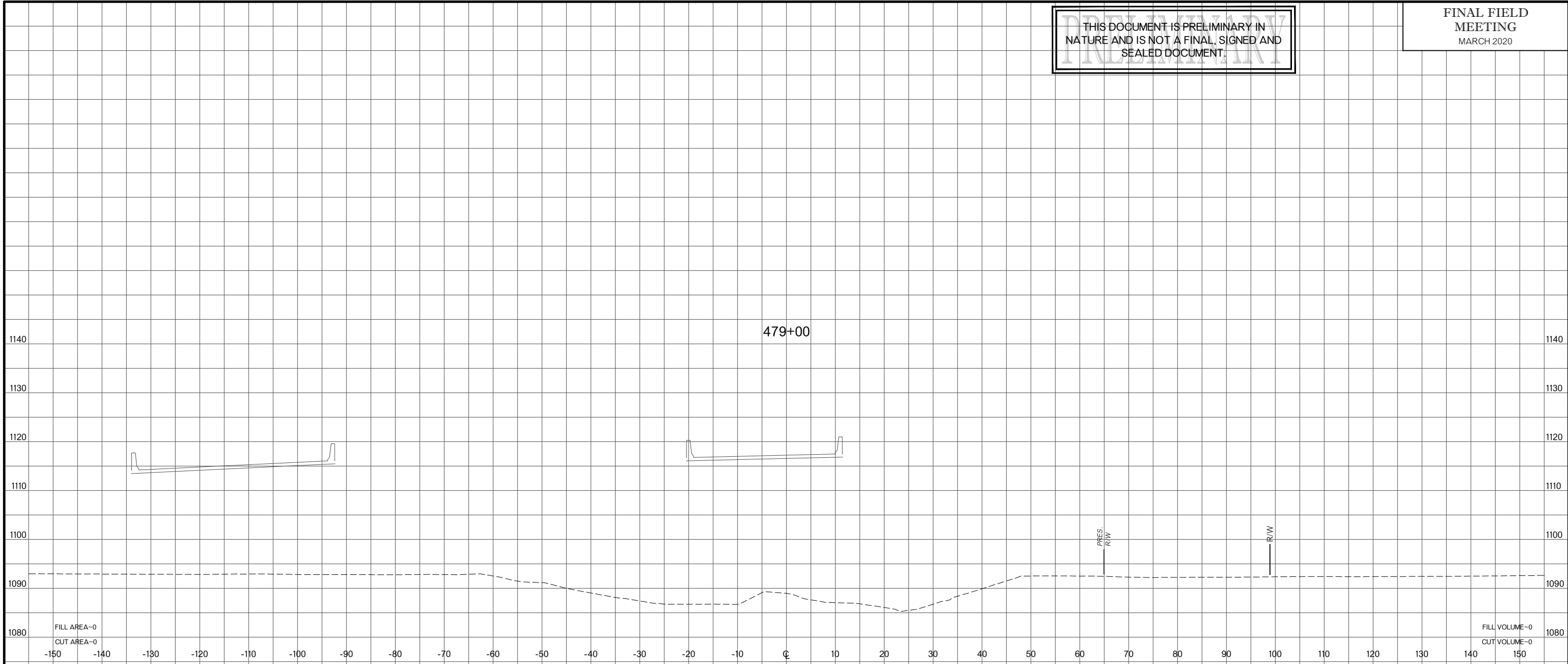


US 81 REALIGNMENT
GRADY COUNTY

RAMP CC

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



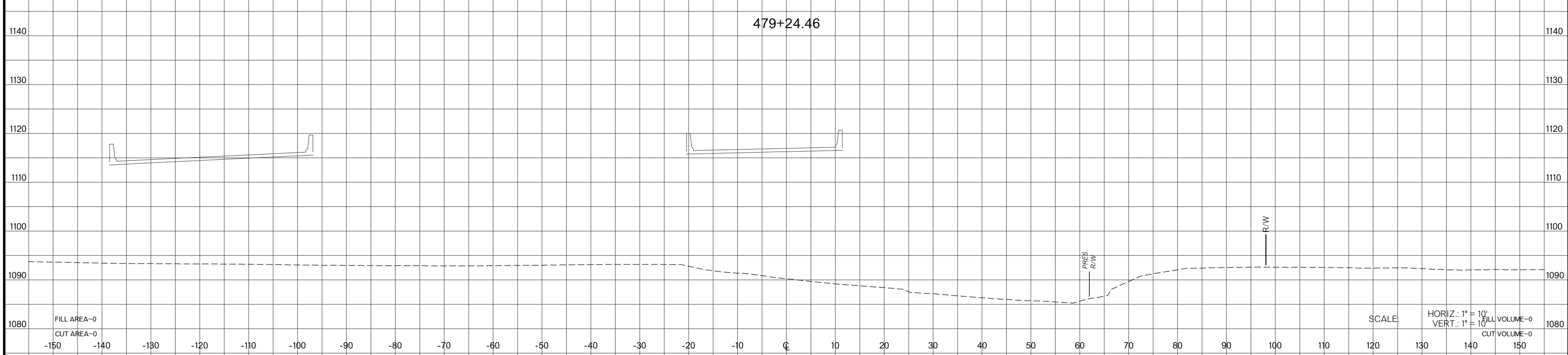
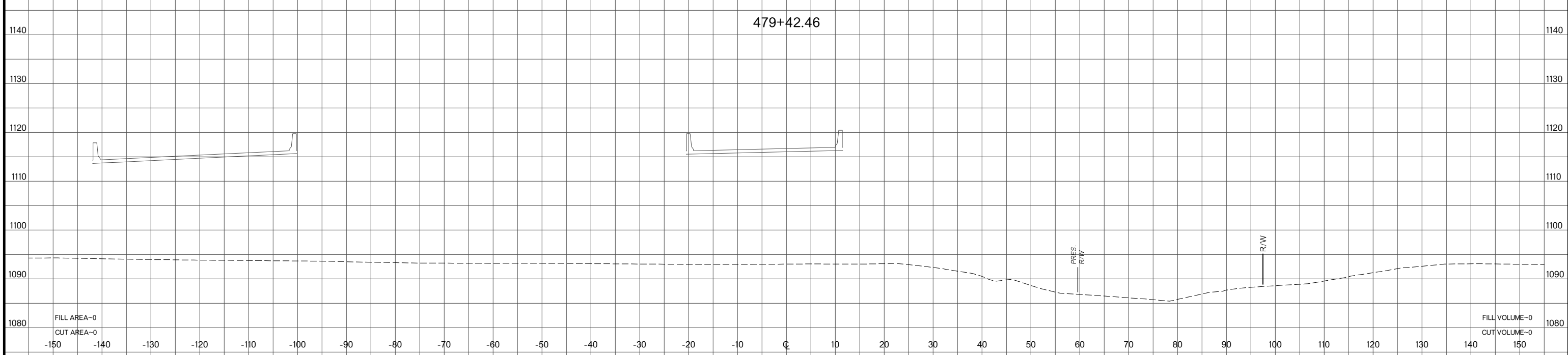
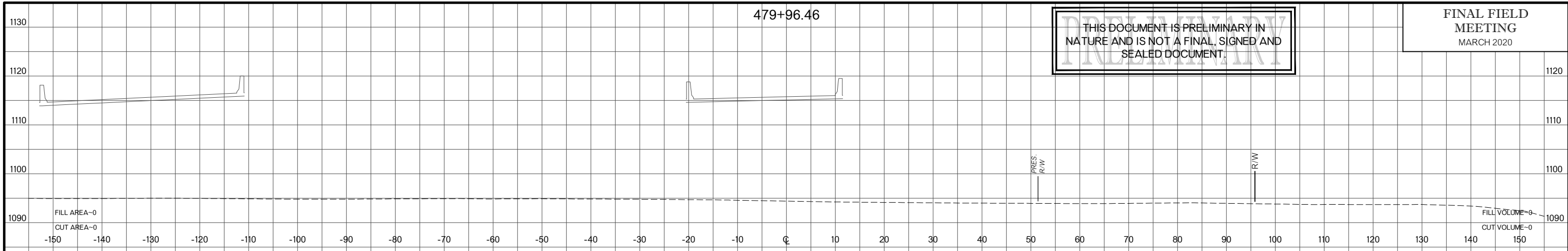
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP CC

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



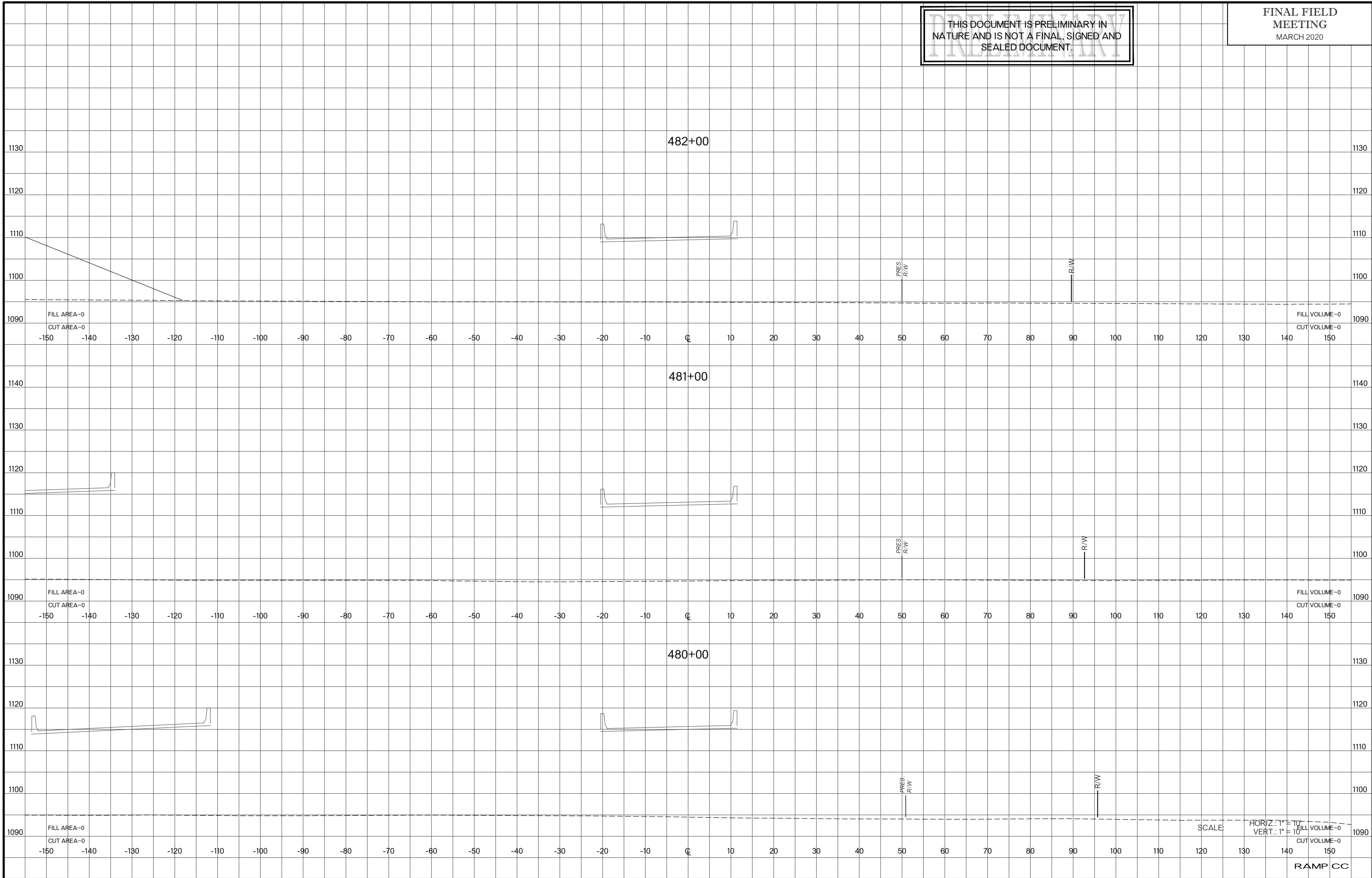
SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP CC

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



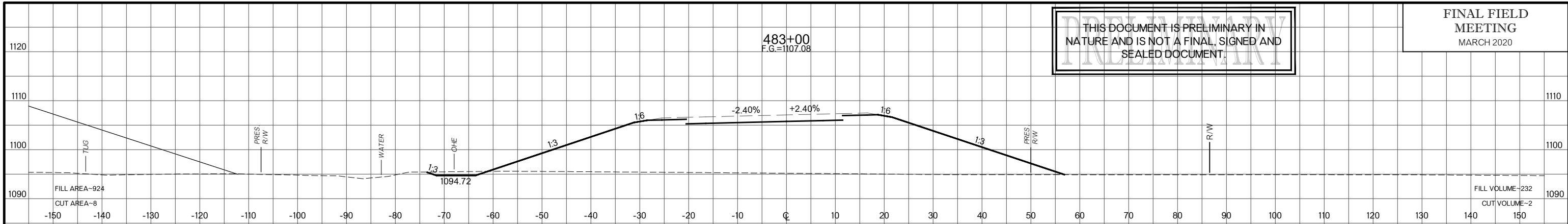
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP CC

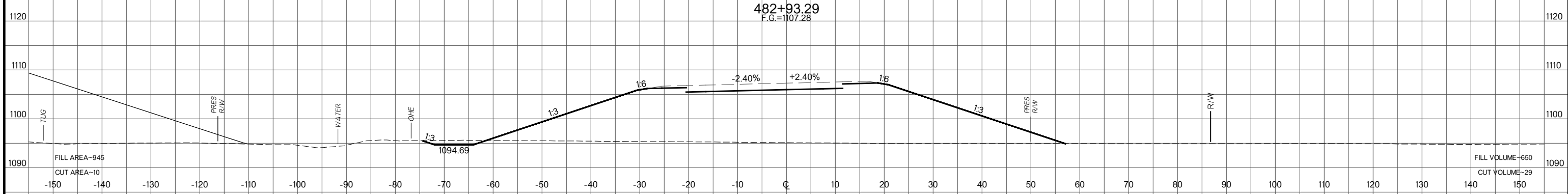
US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

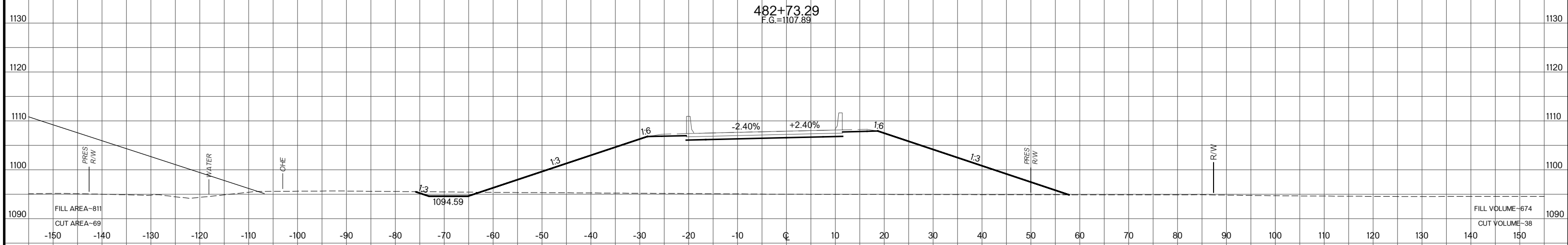
FINAL FIELD MEETING
MARCH 2020



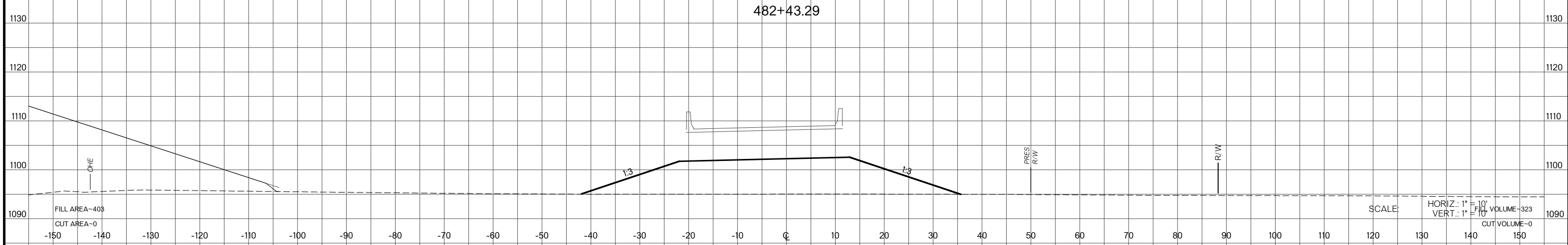
BEGIN OUTSIDE SHOULDER ROTATION



END APPROACH



END BRIDGE "V"



SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

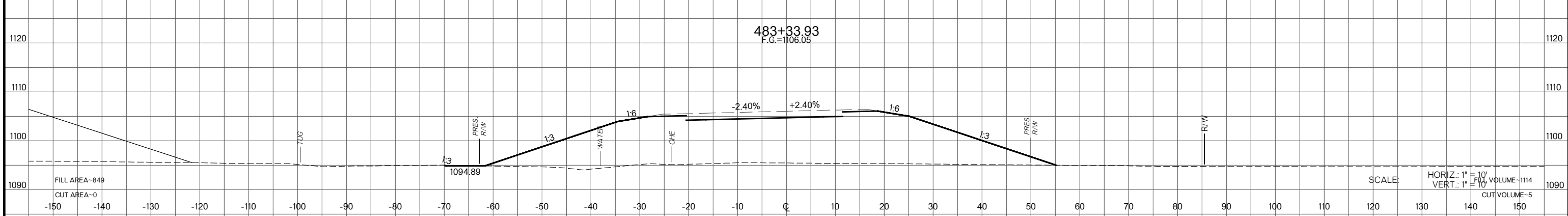
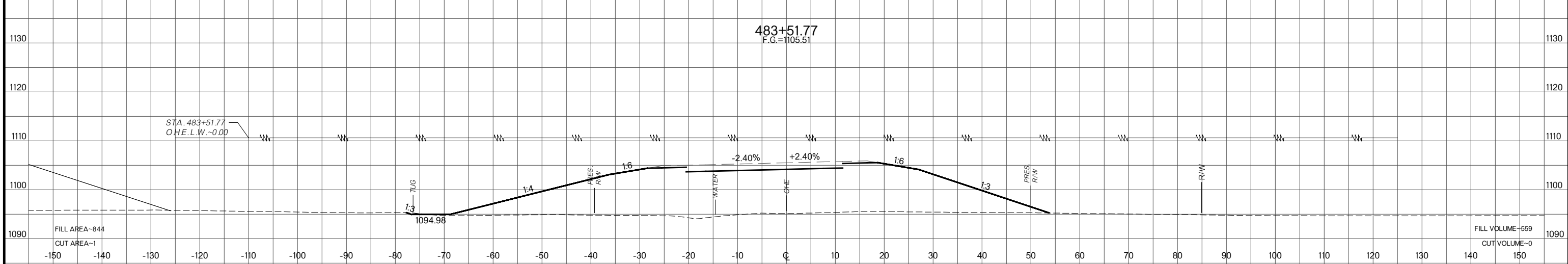
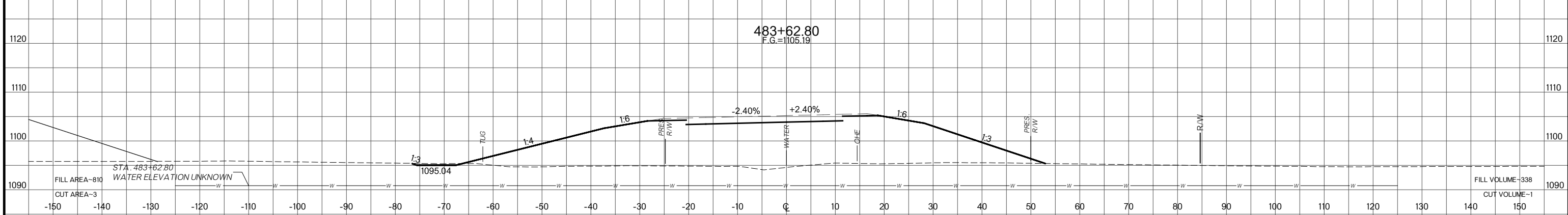
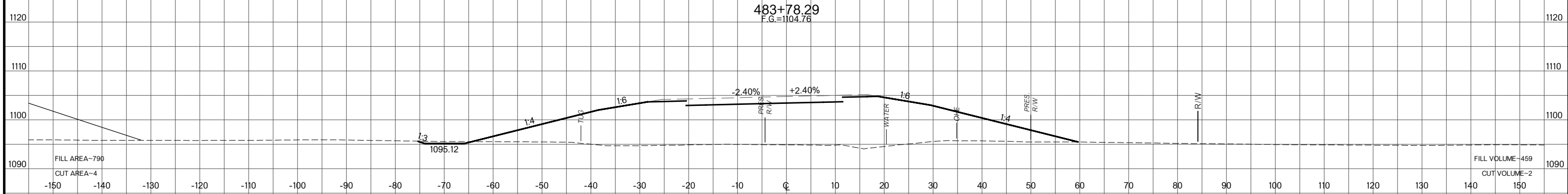
RAMP CC

US 81 REALIGNMENT
GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020

END OUTSIDE SHOULDER ROTATION



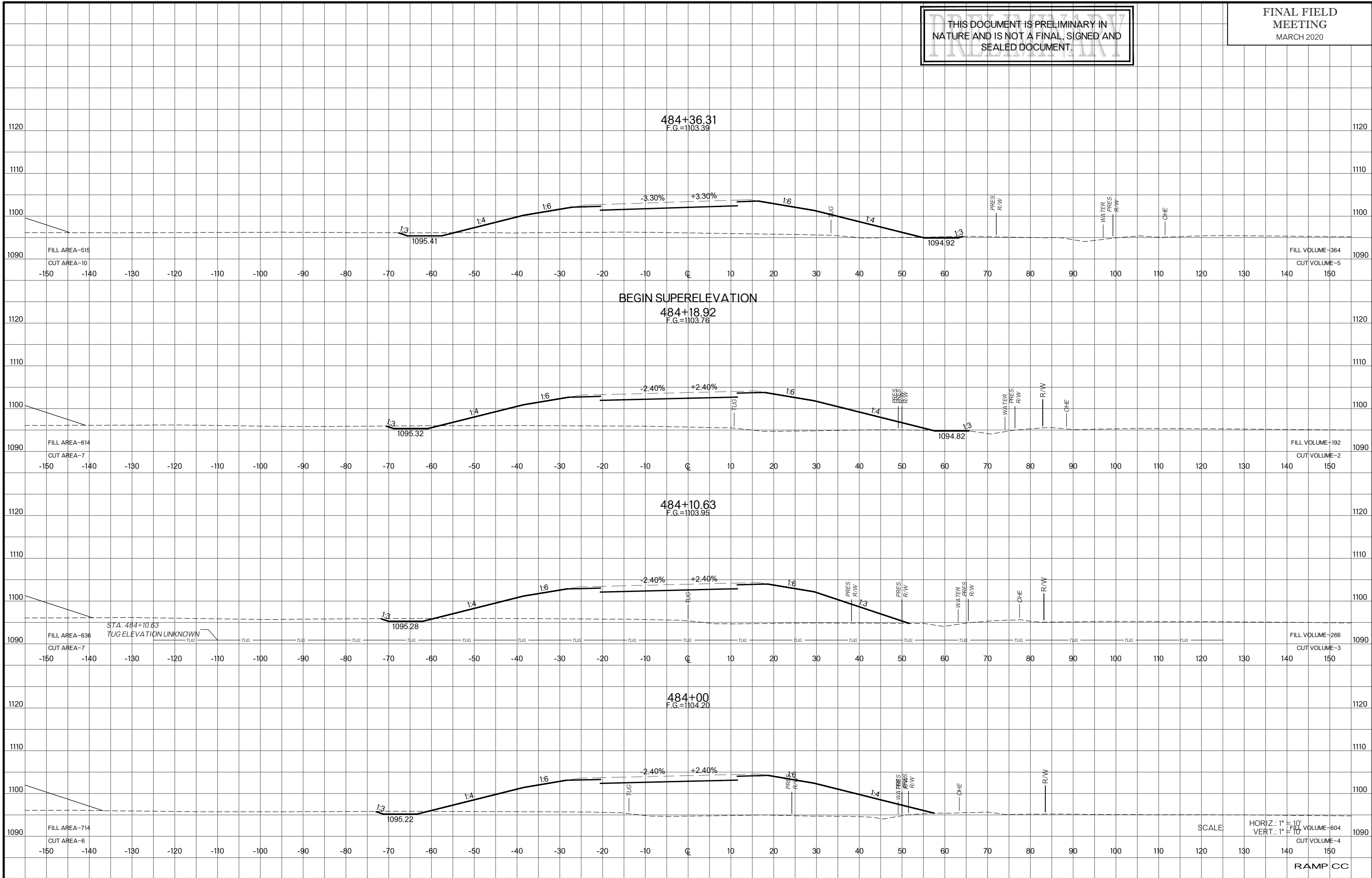
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP CC

US 81 REALIGNMENT GRADY COUNTY

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING MARCH 2020



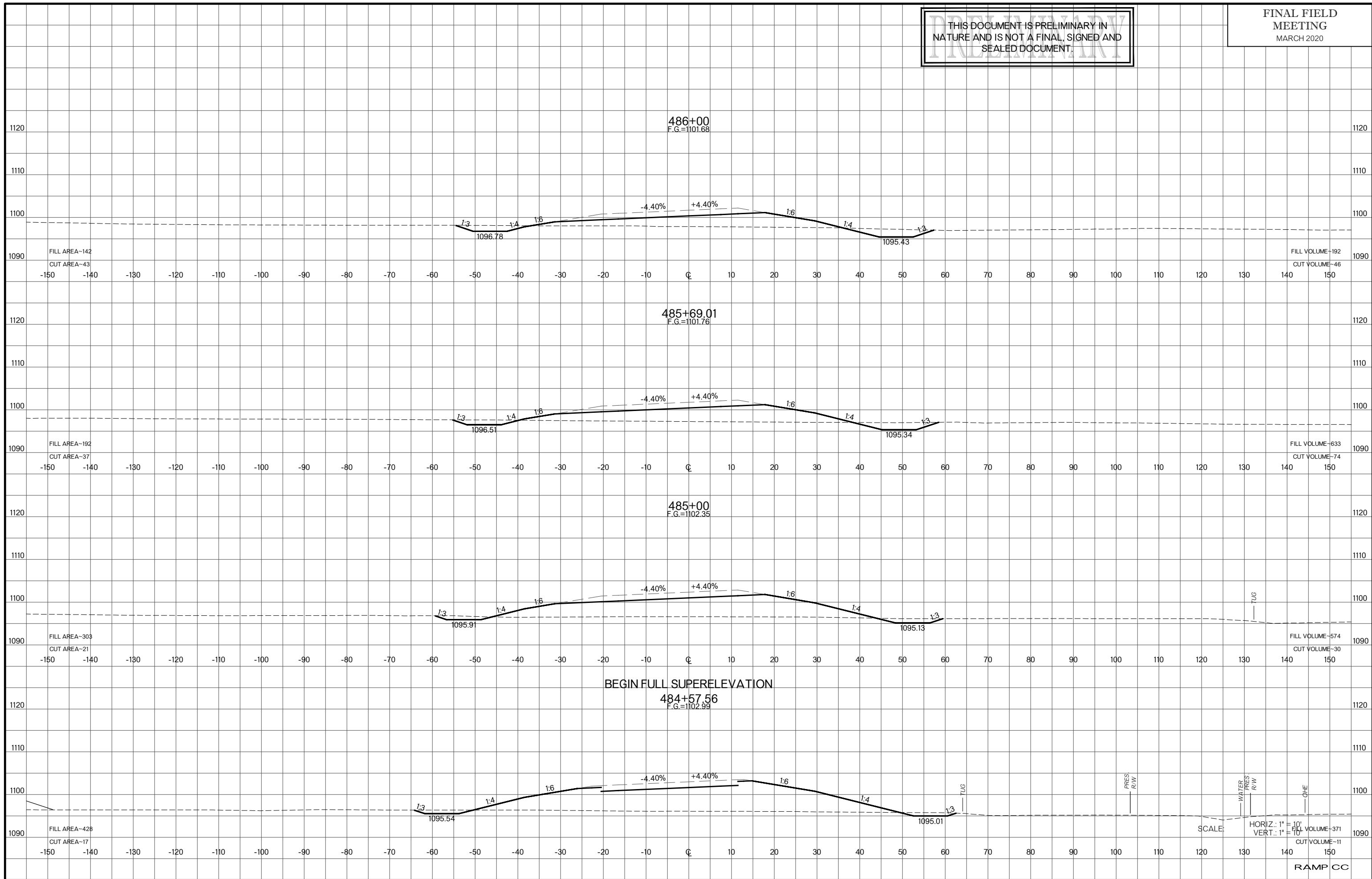
SCALE: HORIZ.: 1" = 10' VERT.: 1" = 10'

RAMP CC

US 81 REALIGNMENT GRADY COUNTY

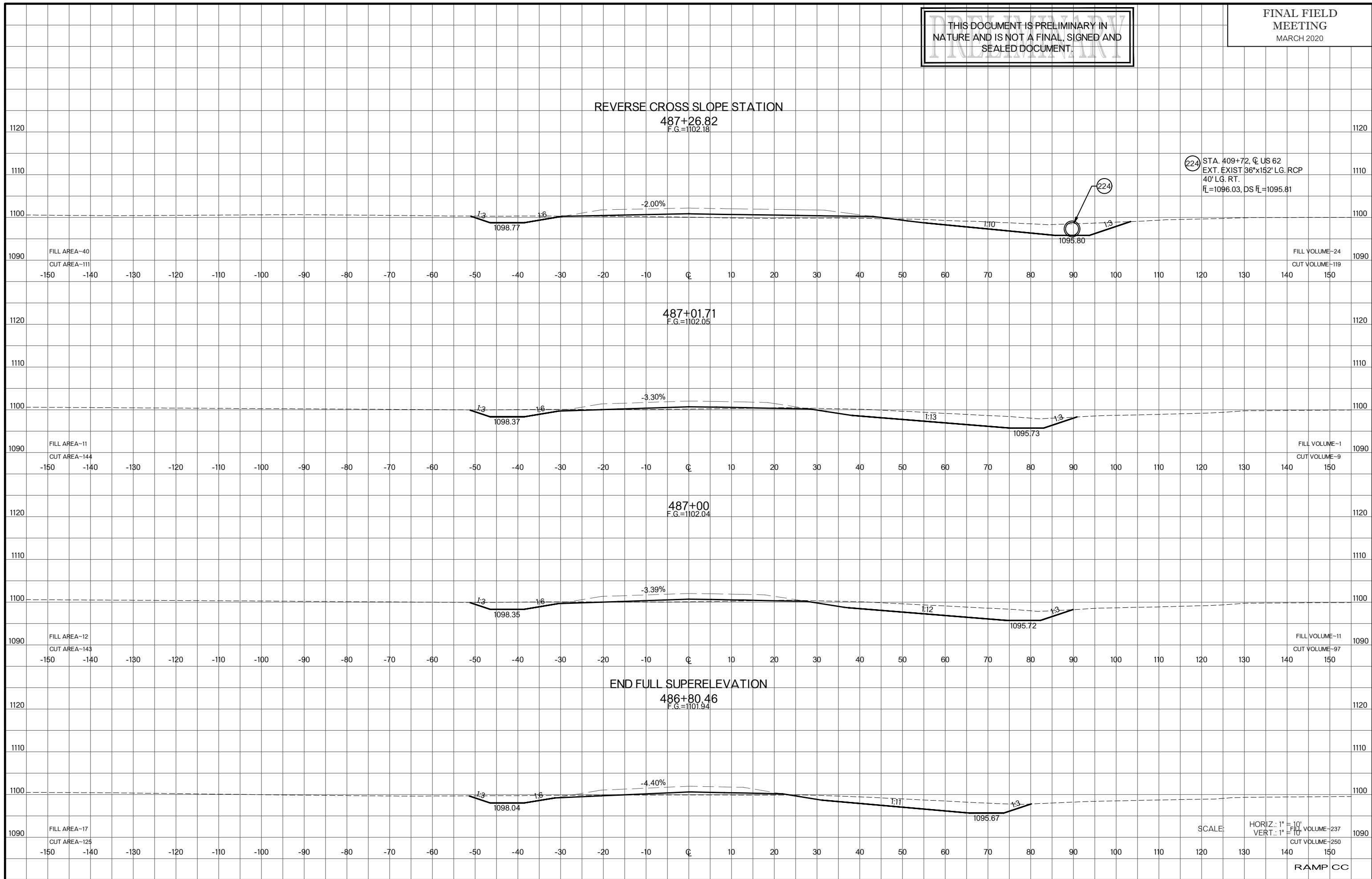
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



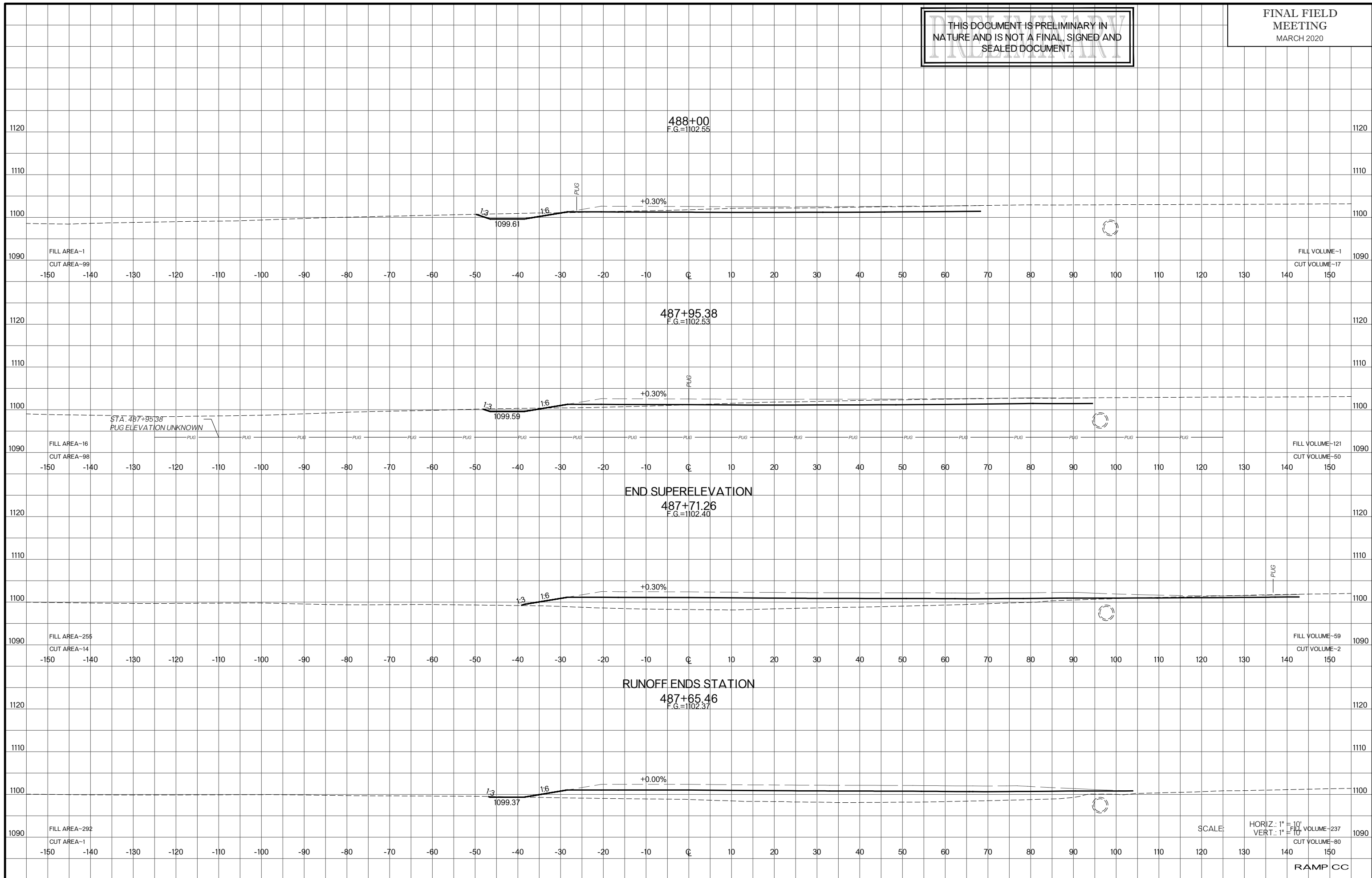
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



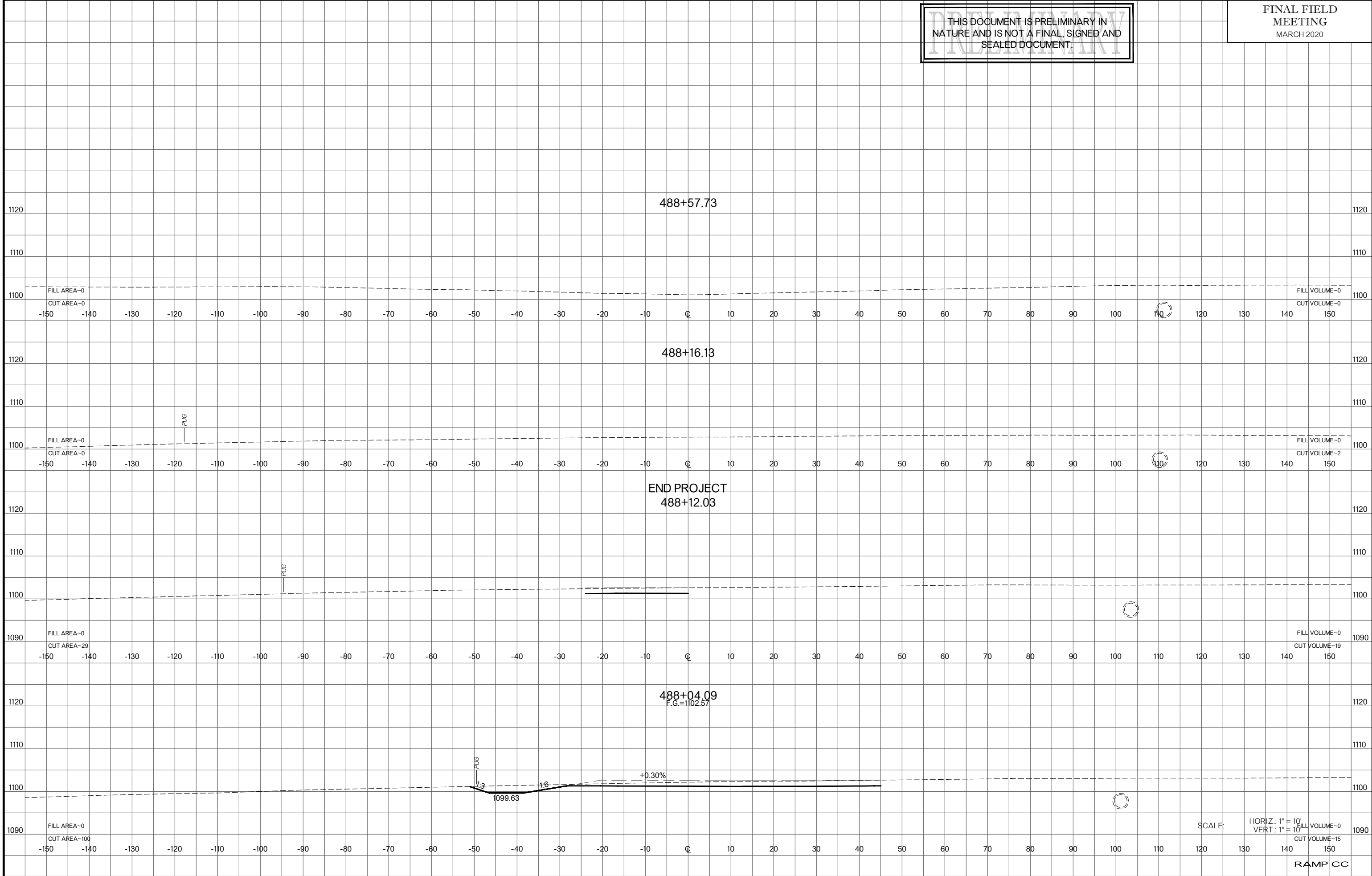
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020



THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

FINAL FIELD MEETING
MARCH 2020

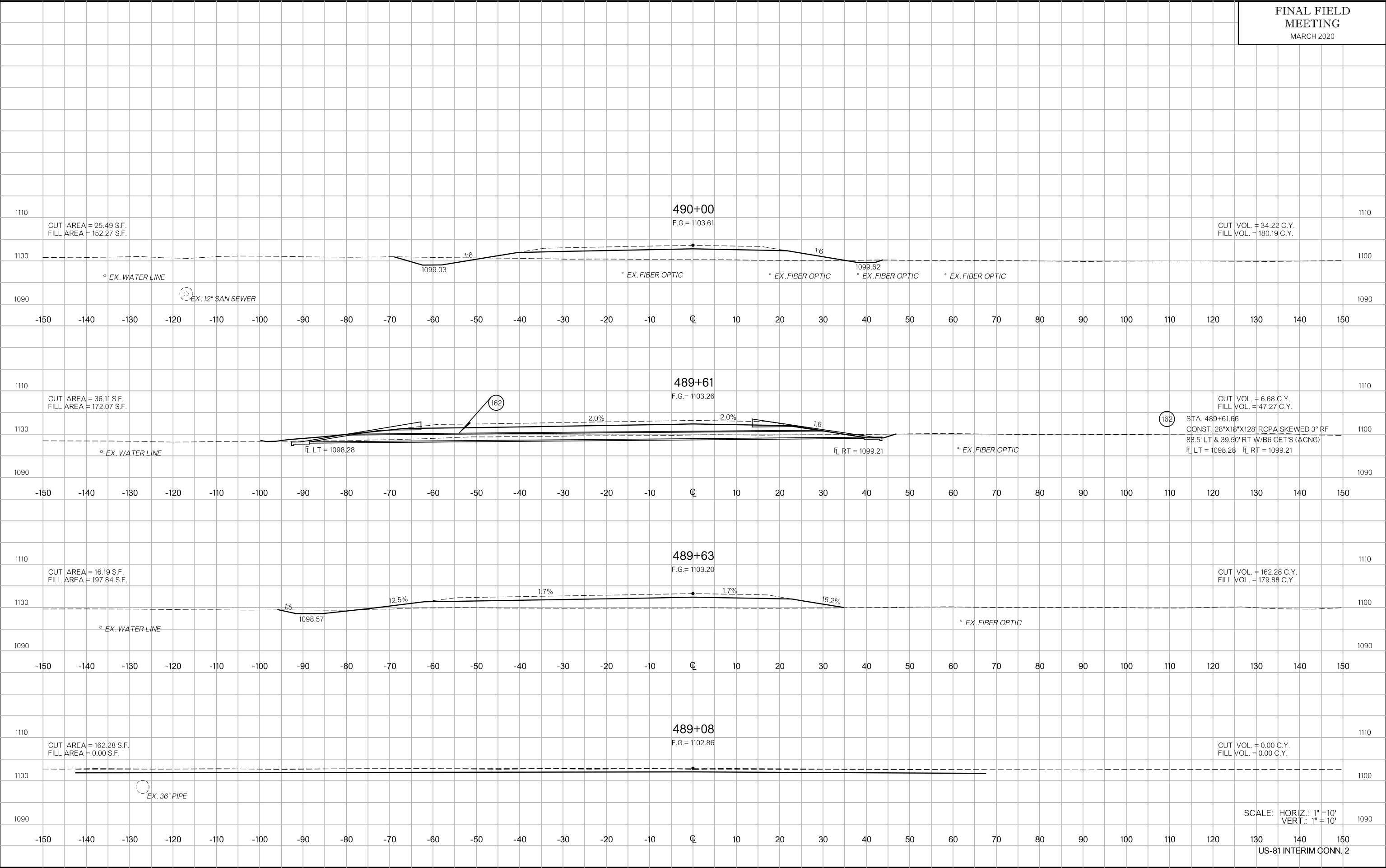


SCALE: HORIZ.: 1" = 10'
VERT.: 1" = 10'

RAMP CC

US 81 REALIGNMENT
GRADY COUNTY

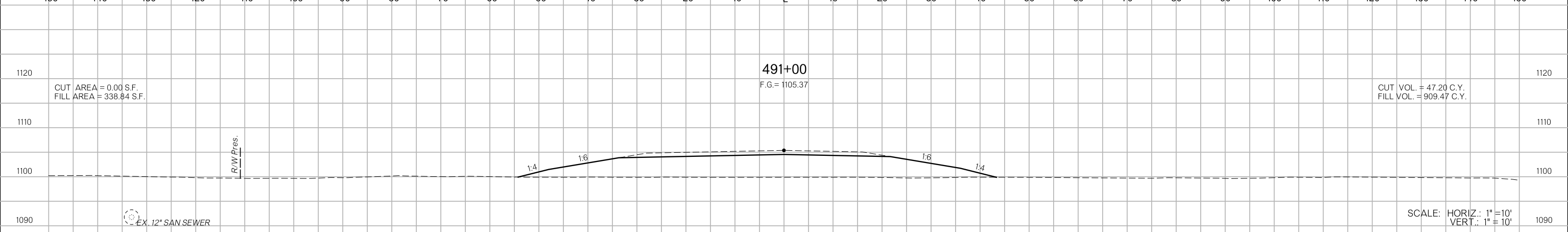
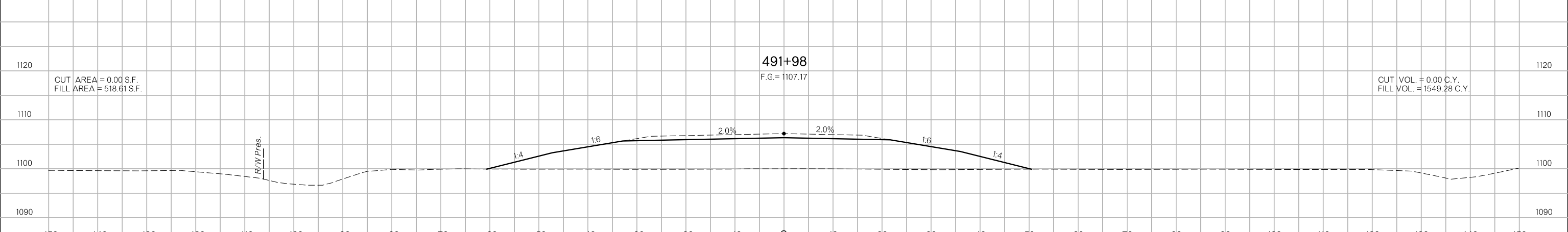
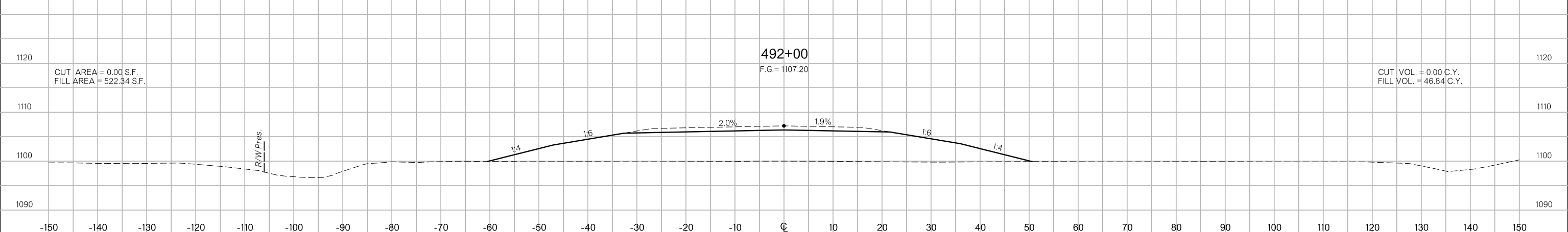
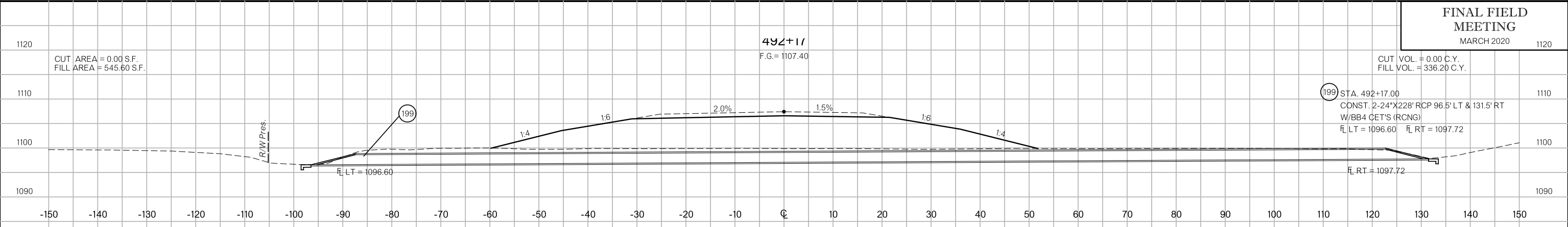
FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1"=10'
VERT.: 1"=10'

US-81 INTERIM CONN. 2

FINAL FIELD MEETING
MARCH 2020



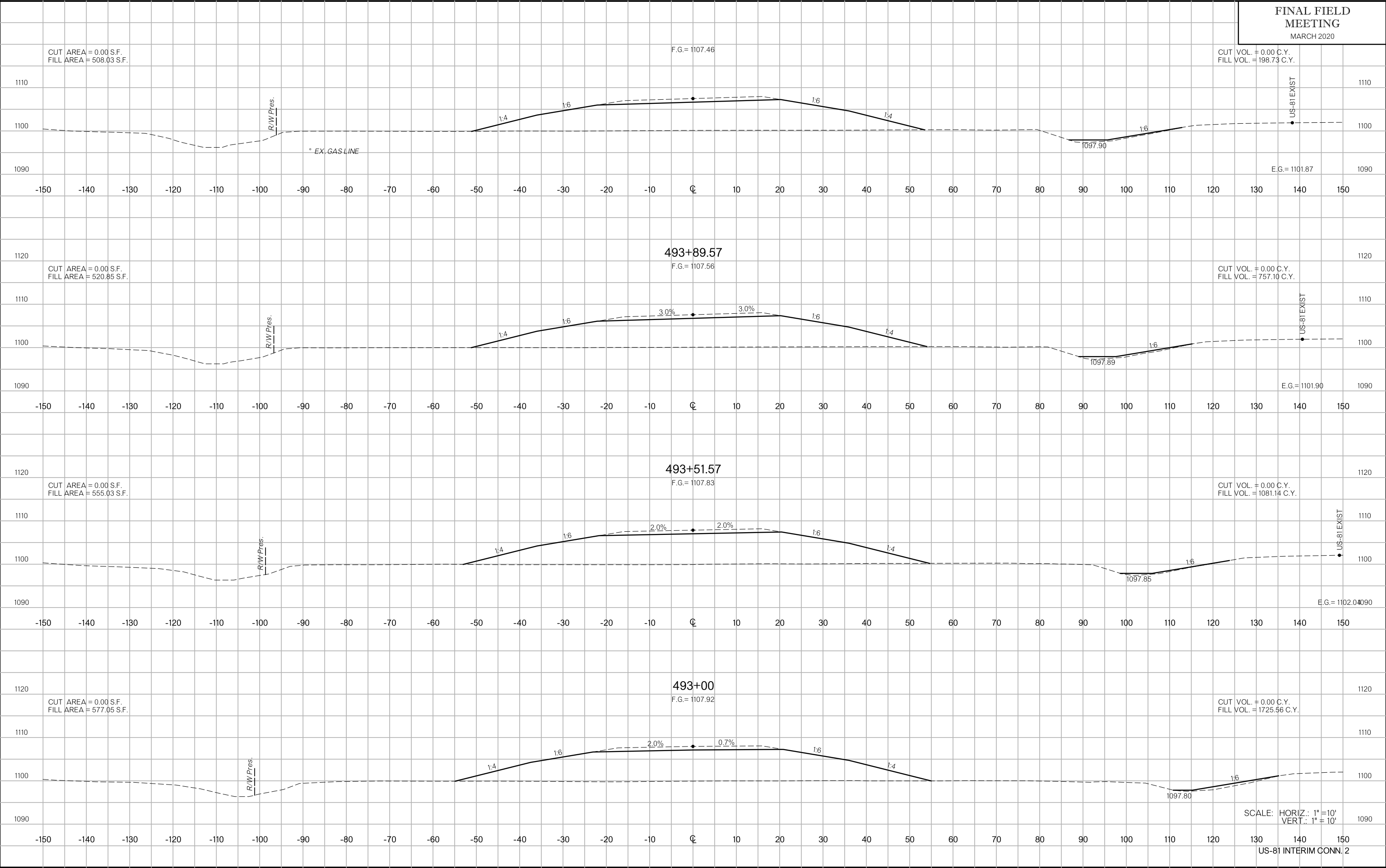
EX. 12" SAN SEWER

SCALE: HORIZ.: 1"=10'
VERT.: 1"=10'

US-81 INTERIM CONN. 2

GRADY COUNTY US-81 INTERIM CONN. 2

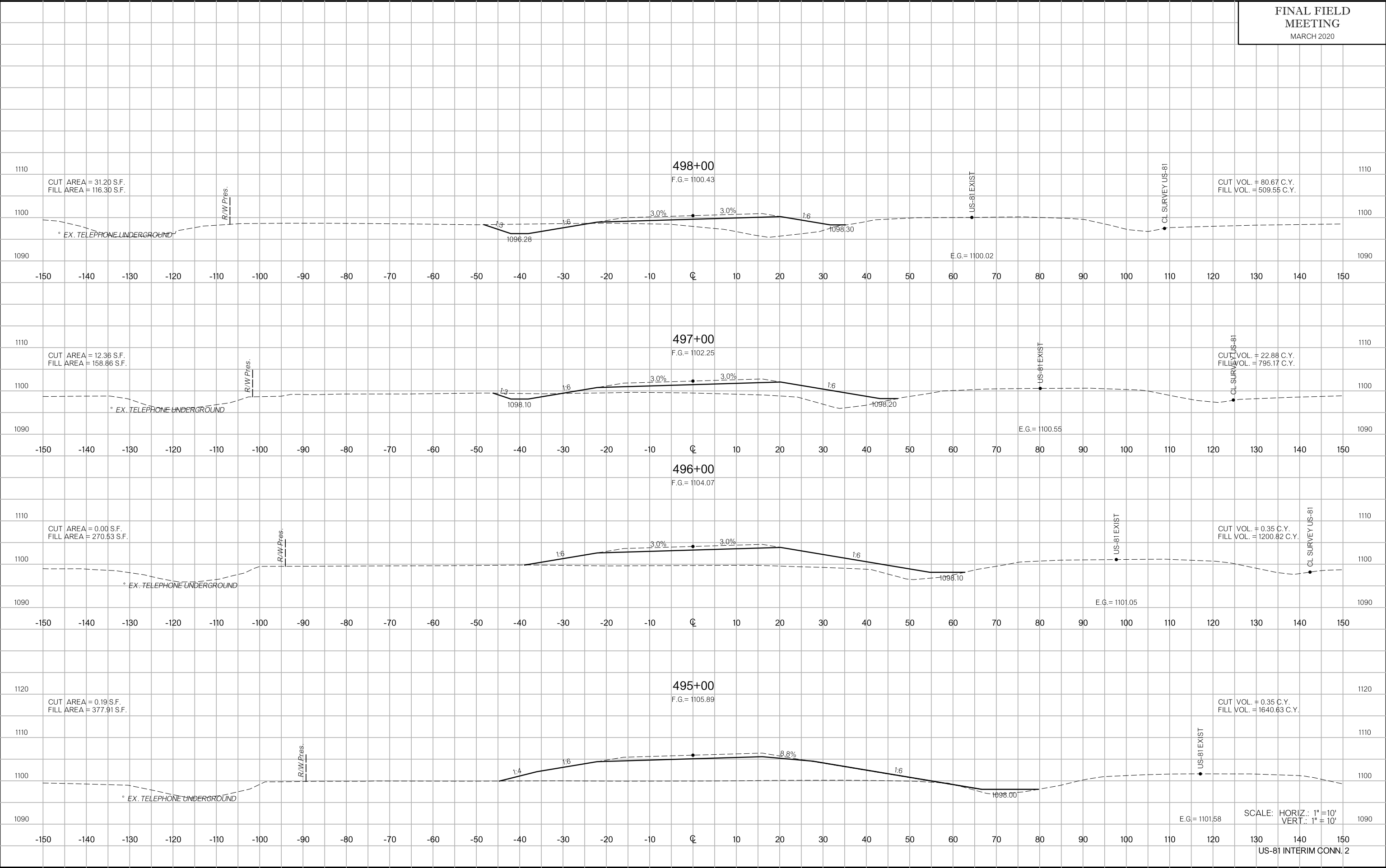
FINAL FIELD MEETING
MARCH 2020



SCALE: HORIZ.: 1"=10'
VERT.: 1"=10'

US-81 INTERIM CONN. 2

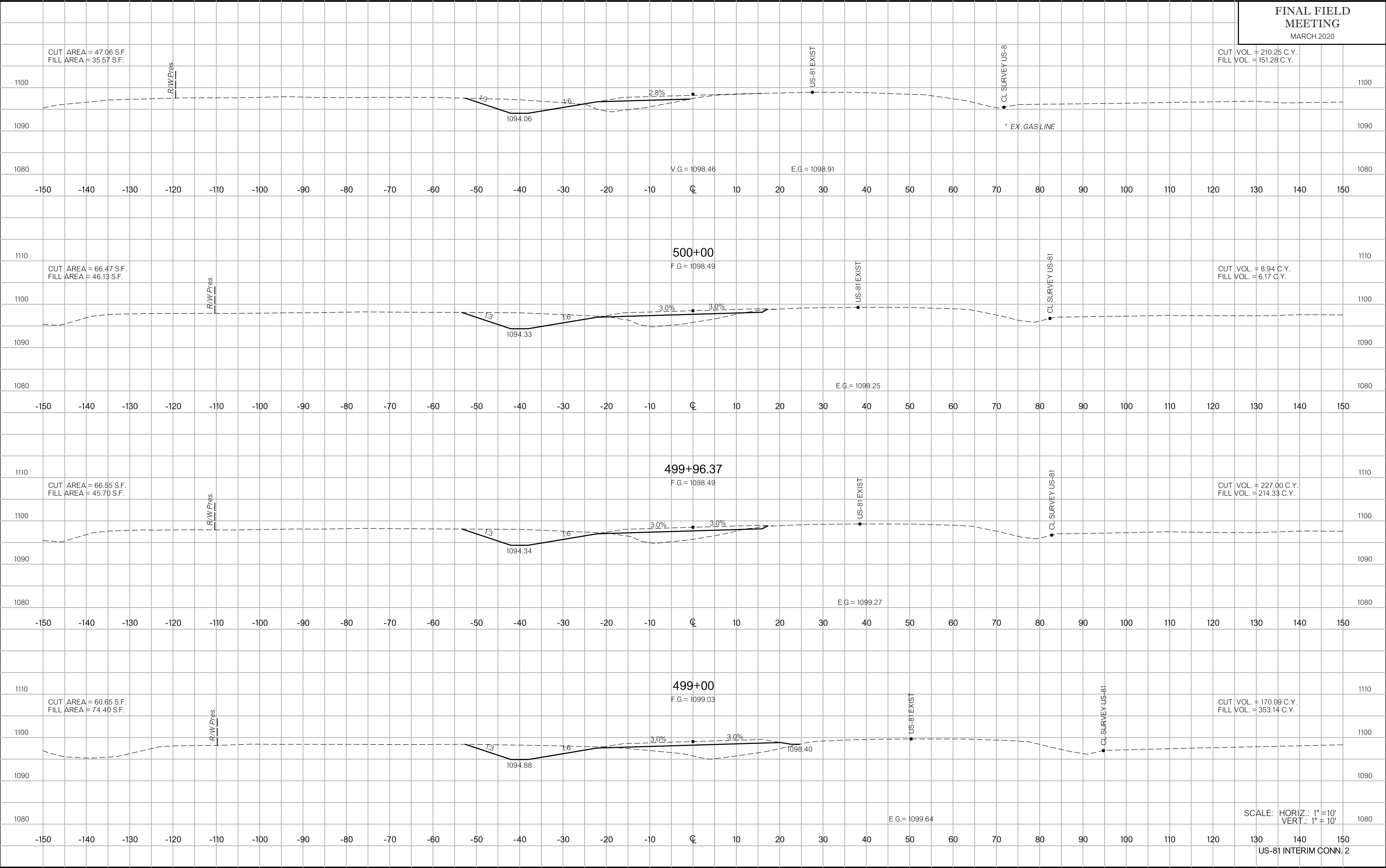
FINAL FIELD MEETING MARCH 2020



SCALE: HORIZ.: 1"=10' VERT.: 1"=10'

US-81 INTERIM CONN. 2

FINAL FIELD MEETING
MARCH 2020

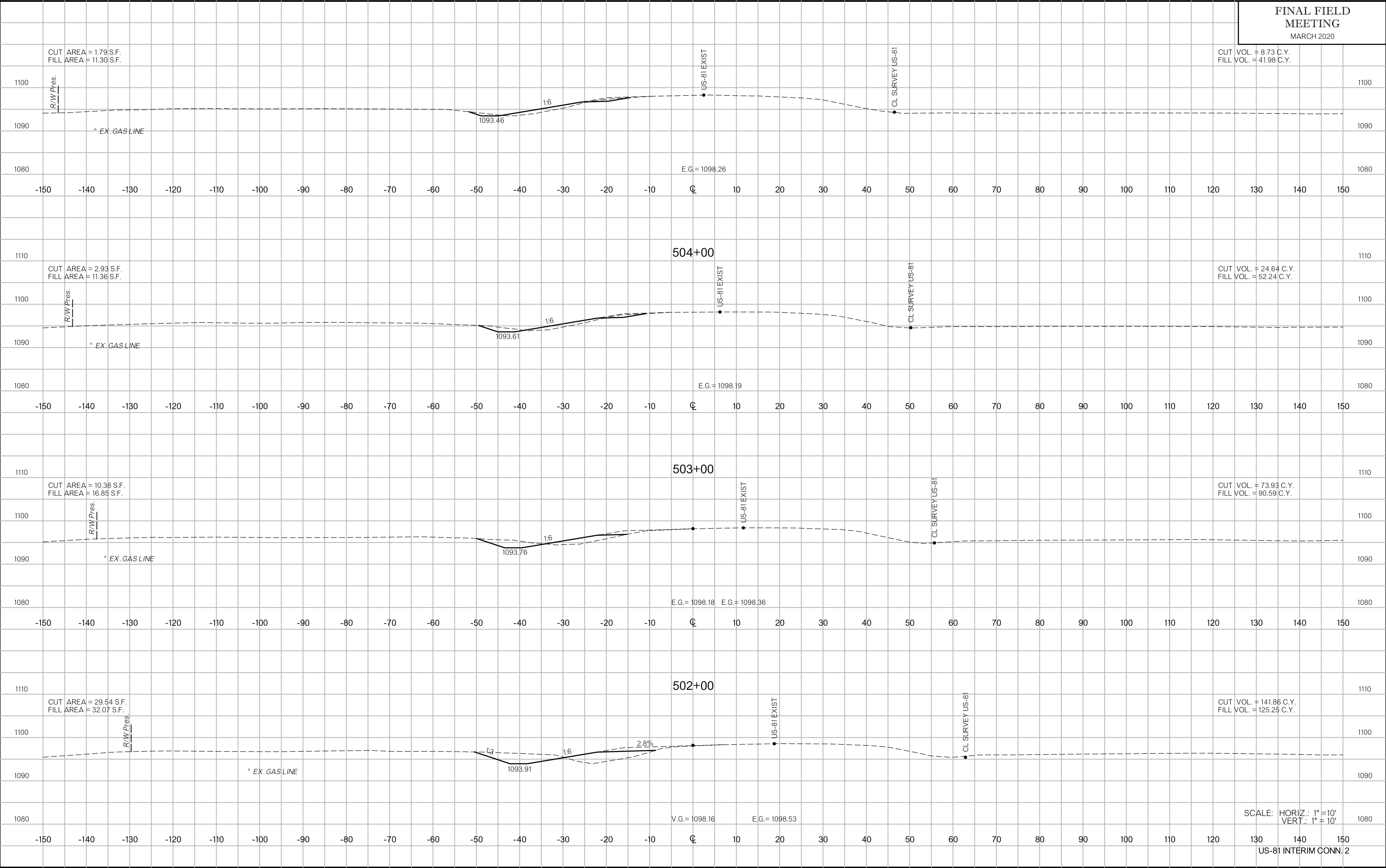


SCALE: HORIZ.: 1"=10'
VERT.: 1"=10'

US-81 INTERIM CONN. 2

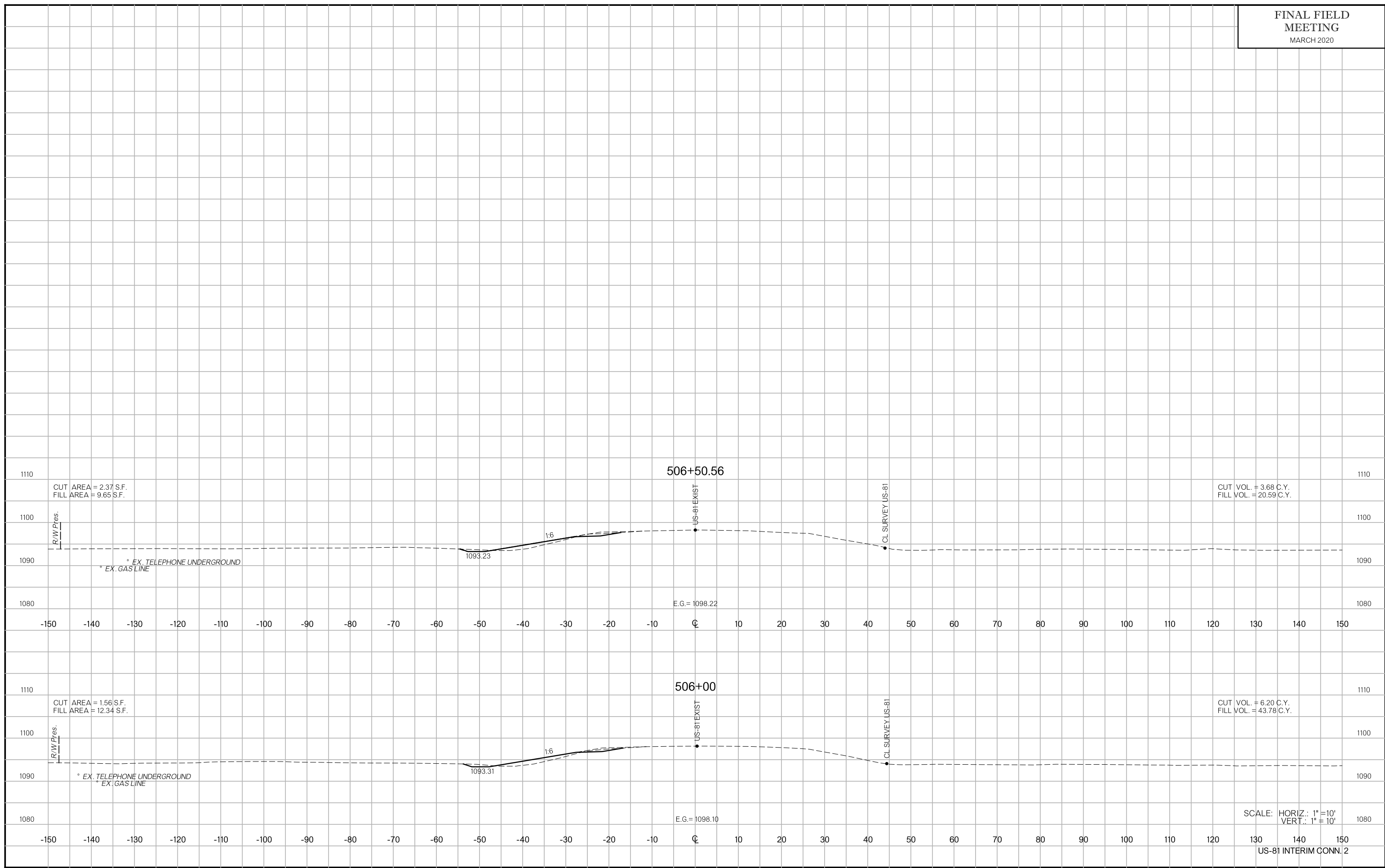
GRADY COUNTY
US-81 INTERIM CONN. 2

FINAL FIELD MEETING
MARCH 2020



FINAL FIELD MEETING
MARCH 2020

F:\2016\1001-1500\016-1182-A\40-Design\Microsoft\000T\DCN\2442804_XSEC07.dwg 6/18/20 3:4 AM



SCALE: HORIZ.: 1"=10'
VERT.: 1"=10'

US-81 INTERIM CONN. 2

GRADY COUNTY
US-81 INTERIM CONN. 2