

FOR SURVEY CONTROL DATA, SEE  
SURVEY DATA SHEETS

STATE OF OKLAHOMA  
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED  
**STATE HIGHWAY**  
FEDERAL AID PROJECT NO. J2-9571(004)  
GRADE, DRAIN, BRIDGE & SURFACE PLANS  
I-35 & SH 74 INTERCHANGE  
**MCCLAIN COUNTY**

CONTROL SECTION NO. 74-44-38  
STATE JOB NO. 29571(04)

BRIDGE 'A' LOCATION NO. 4405-2297X; EXIST. NBI NO. 14496, NEW NBI NO. 33402

INDEX OF SHEETS

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0002-0009	TYPICAL SECTIONS
B001-B003	GENERAL PLAN AND ELEVATION (BRIDGE 'A')
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B006	GENERAL PLAN AND ELEVATION (RETAINING WALL 'B')
B007	GENERAL PLAN AND ELEVATION (RETAINING WALL 'C')
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R016	ROUNDBOUT DETAIL & SPOT ELEVATIONS
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X001-X070	CROSS SECTIONS

PRELIMINARY EARTHWORK

	UNCLASSIFIED EXCAVATION 202(A)	FILL +15%	EXCESS EXCAVATION	UNCLASSIFIED BORROW 202(D)
	CY	CY	CY	CY
SH 74	15,230	28,042		12,812
RAMP A	225	959		734
RAMP B	2,108	606	1,502	
RAMP C	750	322	428	
RAMP D	2,780	197	2,583	
RAMP D1	620	12	608	
WEST FRONTAGE RD	1,865	2,159		294
WEST FRONTAGE RD 1	534	87	447	
TOTALS	24,112	32,384	5,568	13,840

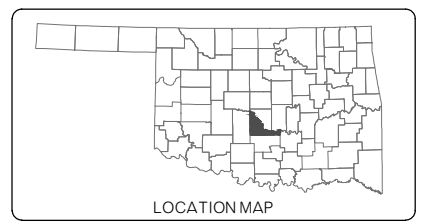
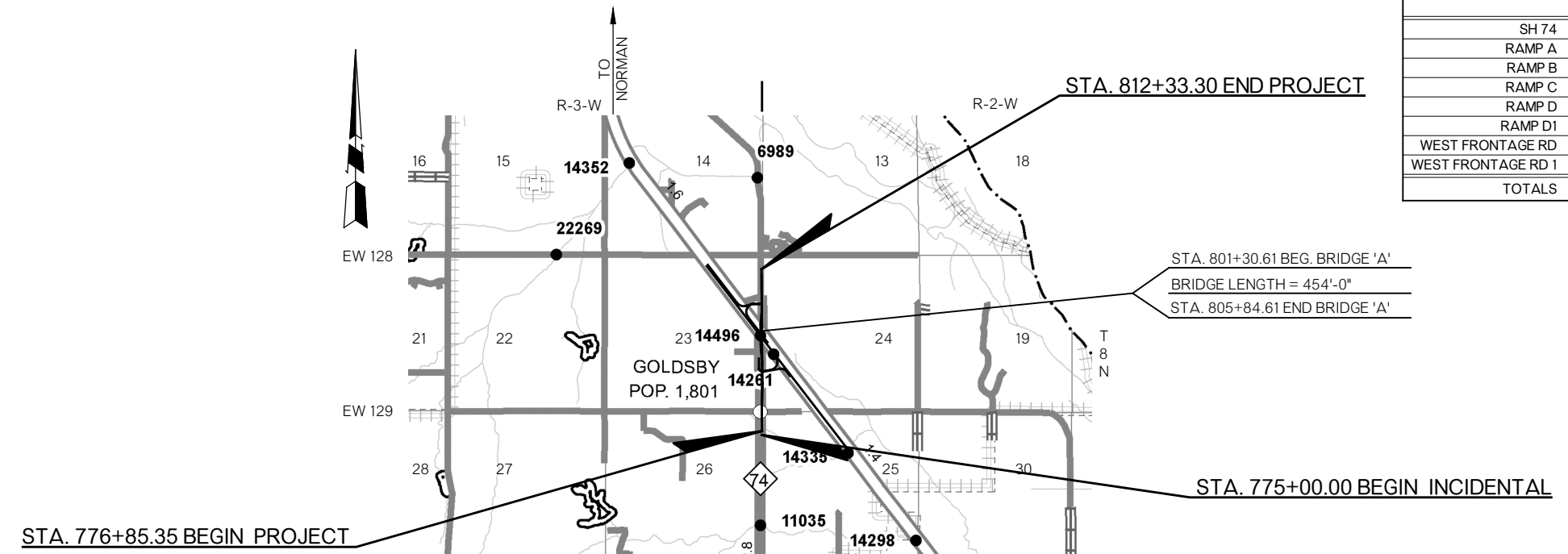
SH 74

AADT 2028	=	9,580
AADT 2048	=	12,940
DHV (2-WAY)	=	1,294
K (DHV/ADT)	=	10%
D	=	65%
T (% DHV)	=	9%
T (% ADT)	=	15%
T3 (% ADT)	=	3%
V	=	45 MPH
20YR FLEX. ESALS	=	3.8 M

SCALES

PLAN	1" = 50'
PROFILE HOR.	1" = 50'
VER.	1" = 5'
LAYOUT MAP	1" = 2,640'

- CONVENTIONAL SYMBOLS
- PROPOSED ROAD
  - RAILROADS
  - RANGE & TOWNSHIP SECTION LINES
  - QUARTER SECTION LINES
  - FENCES
  - GROUND LINE
  - EXISTING ROADS
  - BASE LINE
  - GRADE LINES
  - TELEPHONE & TELEGRAPH
  - POWER LINES
  - BUILDINGS
  - OIL WELLS
  - DRAINAGE STRUCTURES - IN PLACE
  - DRAINAGE STRUCTURES - NEW
  - RIGHT-OF-WAY LINES - EXISTING
  - RIGHT-OF-WAY LINES - NEW
  - CONTROLLED ACCESS
  - RIGHT-OF-WAY FENCE



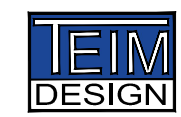
PROJECT LENGTH BASED ON SH 74 CRL STATIONING.

NOTE:

ROADWAY LENGTH	3,093.95 FT.	0.585 MI.
BRIDGE LENGTH	454.00 FT.	0.085 MI.
PROJECT LENGTH		0.670 MI.

EQUATIONS: NONE  
EXCEPTION: NONE

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



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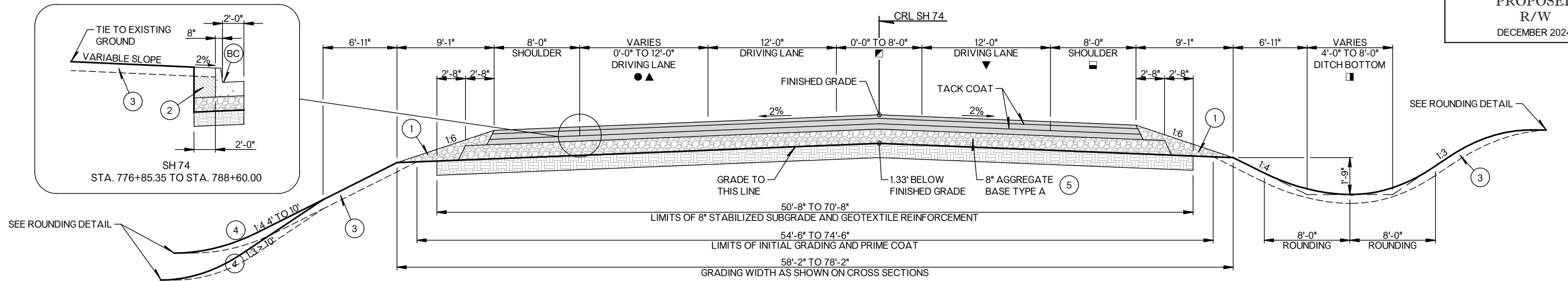


KRISTI NICOLE BRADLEY  
LICENSED PROFESSIONAL ENGINEER NO. 30409 DATE

OKLAHOMA DEPARTMENT OF TRANSPORTATION	DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION
DATE APPROVED	DATE APPROVED
BY	BY
CHIEF ENGINEER	DIVISION ADMINISTRATOR
SWO 4892(1)	F.A. PROJECT NO. 29571(04)
COUNTY McClain COUNTY	HIGHWAY SH 74 SHEET NO. 0001

P.E. NO. XXXX

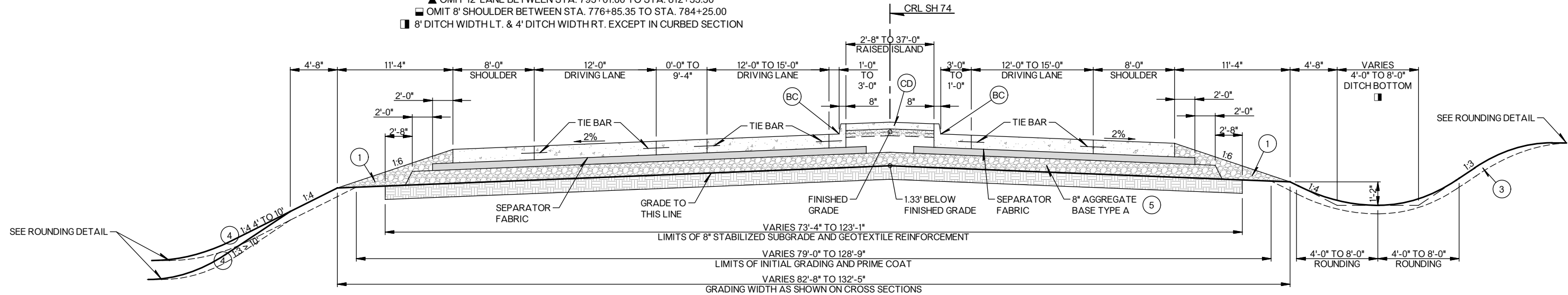
2019 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION GOVERN, APPROVED BY THE U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION, DECEMBER 18, 2019.



TYPICAL SECTION NO. 1  
SH 74

- STA. 776+85.35 TO STA. 788+60.00 - 12' LANE
- STA. 786+48.74 TO STA. 788+60.00
- STA. 793+01.00 TO STA. 794+65.09
- STA. 793+01.00 TO STA. 812+33.30
- ▼ STA. 793+01.00 TO STA. 794+85.44 - 15' TO 12' LANE
- ▲ OMIT 12' LANE BETWEEN STA. 793+01.00 TO STA. 812+33.30
- OMIT 8' SHOULDER BETWEEN STA. 776+85.35 TO STA. 784+25.00
- 8' DITCH WIDTH LT. & 4' DITCH WIDTH RT. EXCEPT IN CURBED SECTION

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	DRIVING LANES	SHOULDERS
SURFACE COURSE	2" SUPERPAVE TYPE S4 (PG 70-28 OK)	2" SUPERPAVE TYPE S4 (PG 64-22 OK)
BASE COURSE	3" SUPERPAVE TYPE S3 (PG 70-28 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)
	3" SUPERPAVE TYPE S3 (PG 64-22 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)



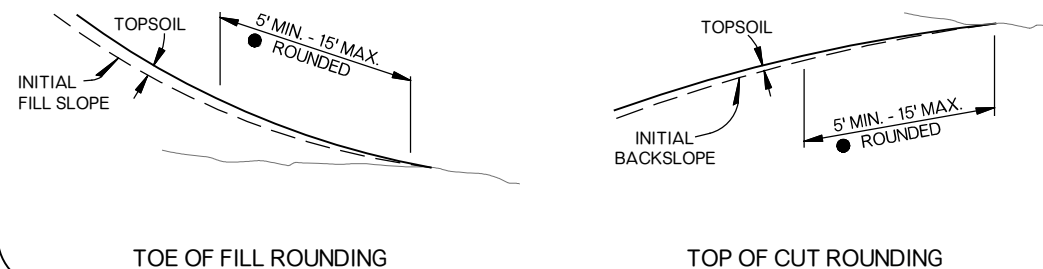
TYPICAL SECTION NO. 2  
SH 74

- STA. 788+60.00 TO STA. 790+68.32
- STA. 792+12.10 TO STA. 793+01.00
- 8' DITCH WIDTH LT. & 4' DITCH WIDTH RT.

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	DRIVING LANES	SHOULDERS
SURFACE COURSE	8" DOWEL JOINTED P.C.C. PAVEMENT	8" P.C.C. PAVEMENT
BASE COURSE	4" CEMENT TREATED BASE	4" CEMENT TREATED BASE

**ROUNDING DETAIL**

- INTERSECTION OF CUT AND/OR FILL SLOPES WITH GROUND LINE TO BE ROUNDED AS PART OF FINISHING OPERATIONS. ROUNDED SHALL BE 5' MINIMUM FOR SMALLER CUTS AND FILLS TO 15' MAXIMUM FOR LARGER CUTS AND FILLS OR AS DESIGNATED BY THE ENGINEER. COST OF ROUNDED TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF WORK.

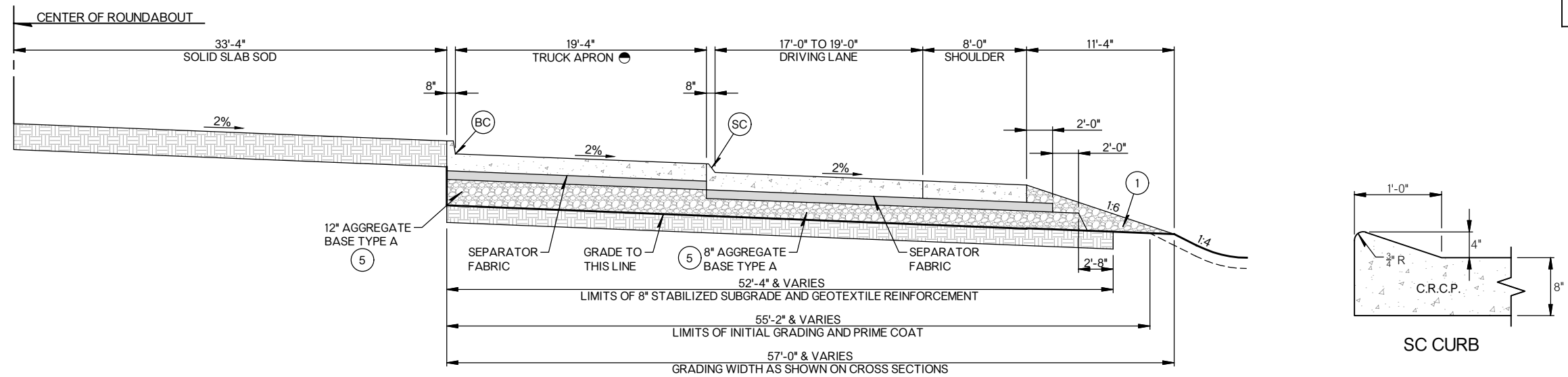


- 1 BACKFILL NOTE:  
TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURE IN TBSC TYPE E.
- 2 BACKFILL NOTE:  
TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURE UNCLASSIFIED BORROW.
- 3 TOPSOIL NOTE:  
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.  
  
THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASSLINE BALANCE.

- 4 DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.
- 5 PRIME COAT ON TOP OF AGGREGATE BASE.
- SE SAFETY EDGE
- CD 4" CONCRETE DIVIDING STRIP WITH 4" SAND BASE.
- BC CONC. CURB 6" BARRIER INTEGRAL
- FOR TYPICAL TAPERS AND VARIABLE WIDTHS, SEE SURFACING CONSTRUCTION DETAIL SHEETS.

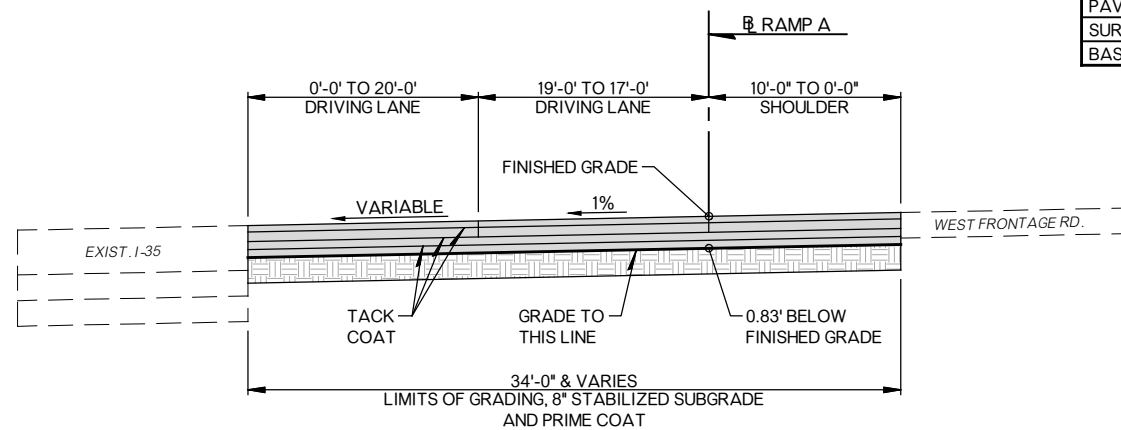
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TYPICAL SECTION



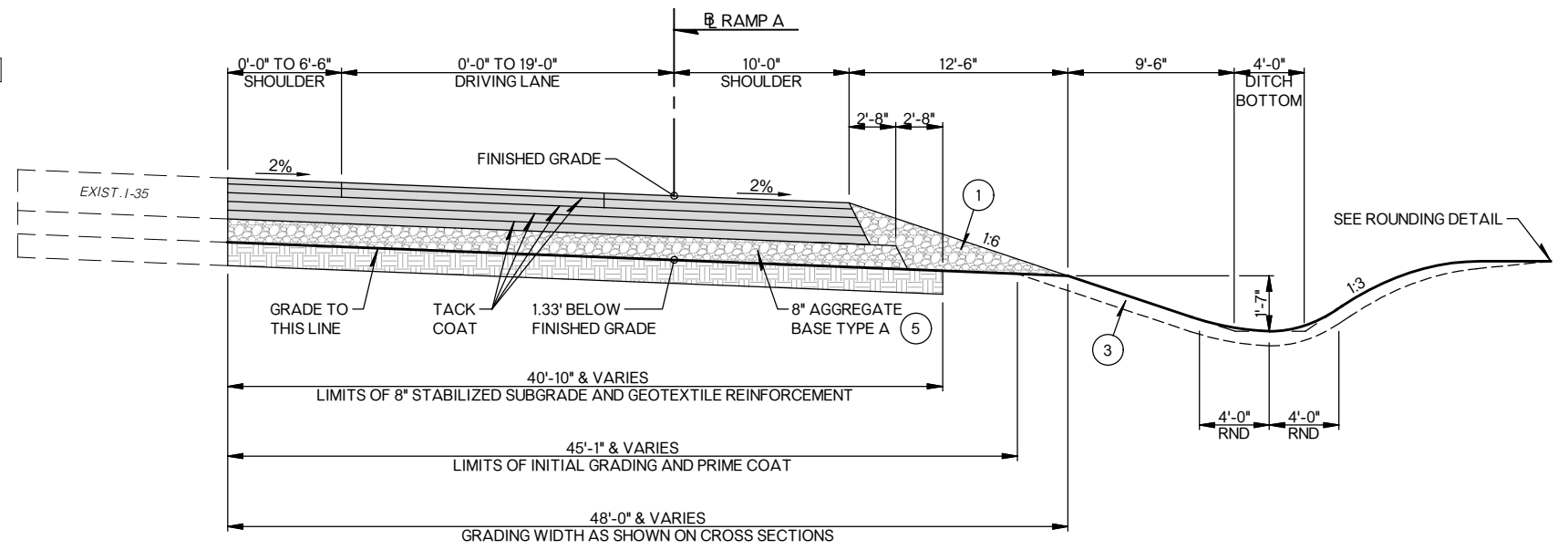
**TYPICAL SECTION NO. 3**  
SH 74 ROUNDABOUT  
STA. 790+68.32 TO STA. 792+12.10  
● SHALL HAVE A STAMPED CONCRETE FINISH

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	TRUCK APRON	DRIVING LANES
SURFACE COURSE	8" CONT. REINF. P.C.C. PAVEMENT	8" CONT. REINF. P.C.C. PAVEMENT
BASE COURSE	4" CEMENT TREATED BASE	4" CEMENT TREATED BASE



**TYPICAL SECTION NO. 5**  
RAMP A  
STA. 53+83.19 TO STA. 55+42.90

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	DRIVING LANE	SHOULDER
SURFACE COURSE	2" SUPERPAVE TYPE S4 (PG 70-28 OK)	2" SUPERPAVE TYPE S4 (PG 64-22 OK)
BASE COURSE	3" SUPERPAVE TYPE S3 (PG 70-28 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)
	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)
	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)



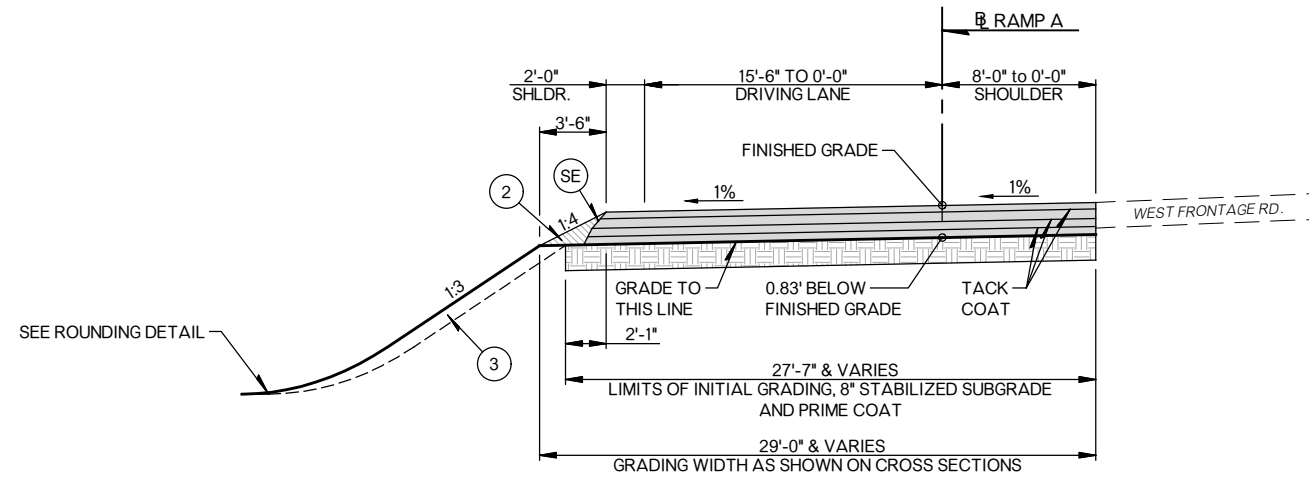
**TYPICAL SECTION NO. 4**  
RAMP A  
STA. 50+00.00 TO STA. 53+83.19

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	EDGE OF LANE	SHOULDER
SURFACE COURSE	2" SUPERPAVE TYPE S4 (PG 76-28 OK)	2" SUPERPAVE TYPE S4 (PG 64-22 OK)
BASE COURSE	3" SUPERPAVE TYPE S3 (PG 76-28 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)
	3" SUPERPAVE TYPE S3 (PG 64-22 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)
	3" SUPERPAVE TYPE S3 (PG 64-22 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)
	3" SUPERPAVE TYPE S3 (PG 64-22 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)

- ① SEE BACKFILL NOTE ON SHEET 0002
- ③ SEE TOPSOIL NOTE ON SHEET 0002
- ④ DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.
- ⑤ PRIME COAT ON TOP OF AGGREGATE BASE.
- (BC) CONC. CURB (6" BARRIER INTEGRAL)
- FOR TYPICAL TAPERS AND VARIABLE WIDTHS, SEE SURFACING CONSTRUCTION DETAIL SHEETS.

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**TYPICAL SECTION**



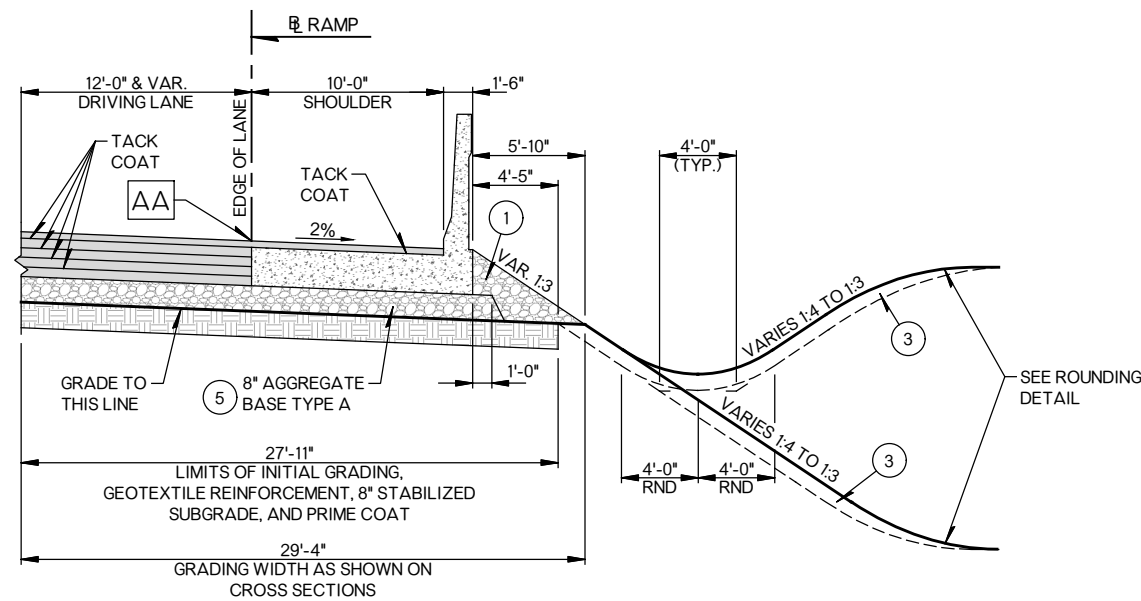
TYPICAL SECTION NO. 6

RAMP A

STA. 55+42.10 TO STA. 56+63.21

GORE VARIES FROM 8'-0" TO 0'-0" FROM STA. 55+42.10 TO STA. 55+93.06

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	DRIVING LANE	SHOULDER
SURFACE COURSE	2" SUPERPAVE TYPE S4 (PG 70-28 OK)	2" SUPERPAVE TYPE S4 (PG 64-22 OK)
BASE COURSE	3" SUPERPAVE TYPE S3 (PG 70-28 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)
	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)
	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)



TYPICAL SECTION NO. 7

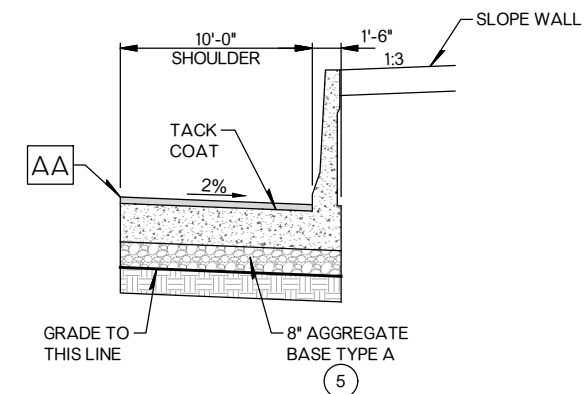
RAMP D

STA. 847+68.05 TO STA. 854+50.30

RAMP B

STA. 848+30.06 TO STA. 853+51.84

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	EDGE OF LANE	SHOULDER
SURFACE COURSE	2" SUPERPAVE TYPE S4 (PG 76-28 OK)	2" SUPERPAVE TYPE S4 (PG 64-22 OK)
BASE COURSE	3" SUPERPAVE TYPE S3 (PG 76-28 OK)	12" CLASS AA CONCRETE
	3" SUPERPAVE TYPE S3 (PG 64-22 OK)	
	3" SUPERPAVE TYPE S3 (PG 64-22 OK)	
	3" SUPERPAVE TYPE S3 (PG 64-22 OK)	



TYPICAL SECTION NO. 7A

RAMP D

STA. 849+79.30 TO STA. 850+85.31

RAMP B

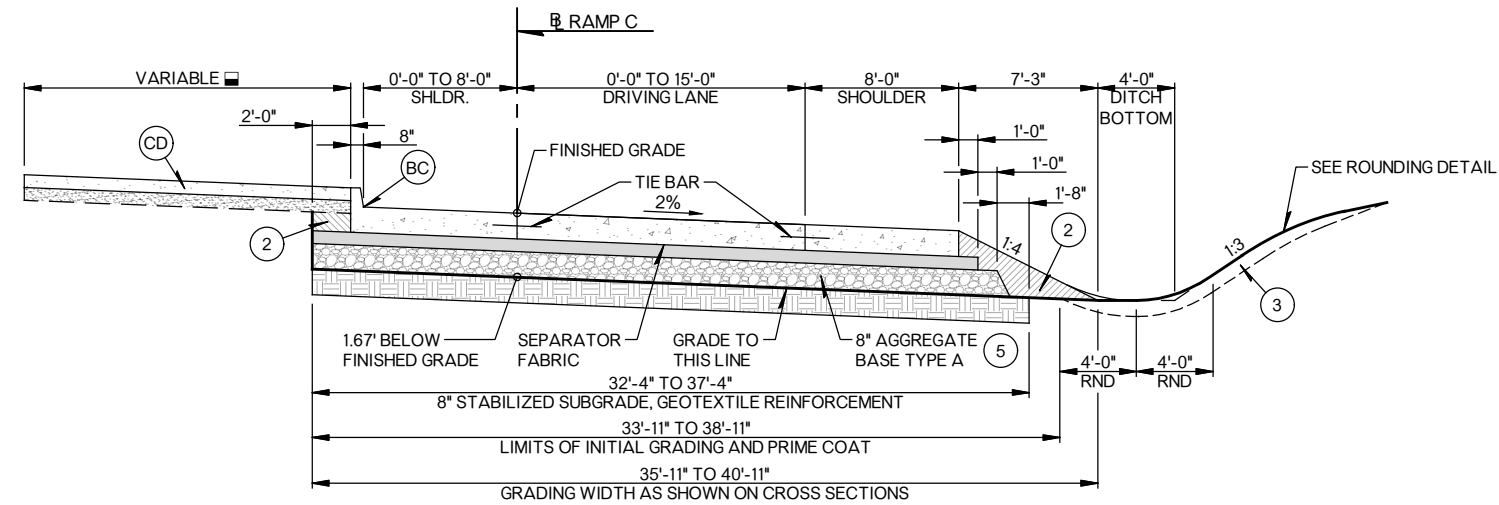
STA. 852+04.37 TO STA. 853+20.91

- ① SEE BACKFILL NOTE ON SHEET 0002
- ② SEE BACKFILL NOTE ON SHEET 0002
- ③ SEE TOPSOIL NOTE ON SHEET 0002
- ⑤ PRIME COAT ON TOP OF AGGREGATE BASE.
- (SE) SAFETY EDGE

FOR TYPICAL TAPERS AND VARIABLE WIDTHS, SEE SURFACING CONSTRUCTION DETAIL SHEETS.

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TYPICAL SECTION

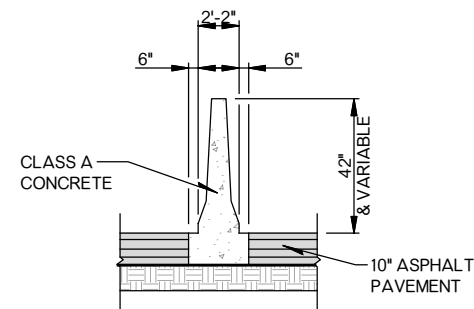


TYPICAL SECTION NO. 8  
RAMP C

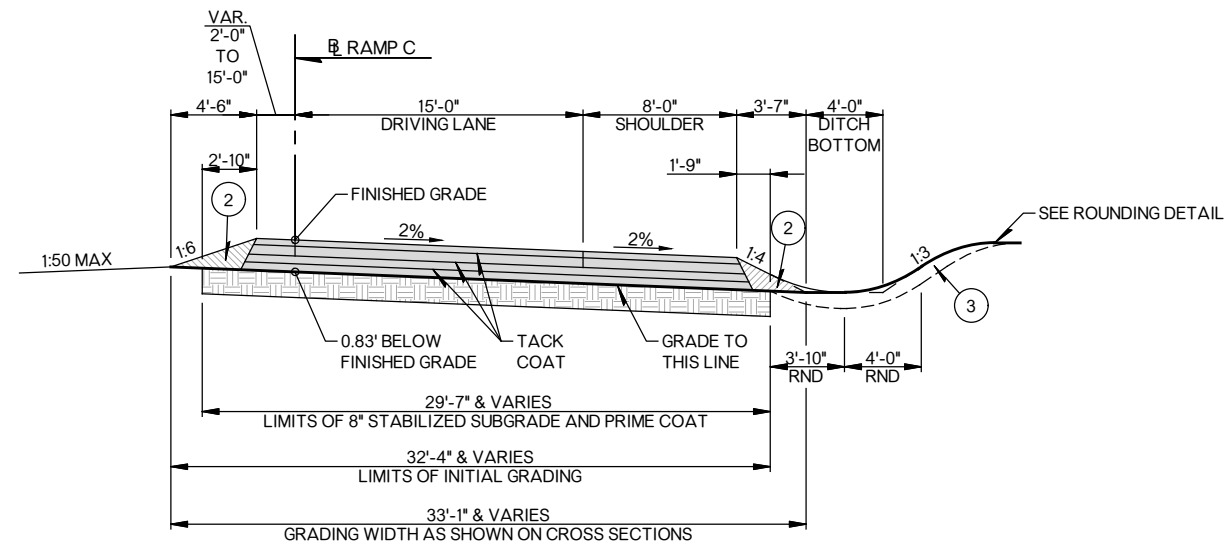
STA. 880+41.70 TO STA. 881+70.00

OMIT CONCRETE DIVIDING STRIP AND CURB FROM STA. 880+41.70 TO STA. 880+72.70

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	DRIVING LANE	SHOULDER
SURFACE COURSE	8" DOWEL JOINTED P.C.C. PAVEMENT	8" P.C.C. PAVEMENT
BASE COURSE	4" CEMENT TREATED BASE	4" CEMENT TREATED BASE



CONCRETE LONGITUDINAL BARRIER  
RAMP C & D1



TYPICAL SECTION NO. 9  
RAMP C

STA. 881+70.00 TO STA. 887+03.11

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	DRIVING LANE	SHOULDER
SURFACE COURSE	2" SUPERPAVE TYPE S4 (PG 70-28 OK)	2" SUPERPAVE TYPE S4 (PG 64-22 OK)
BASE COURSE	3" SUPERPAVE TYPE S3 (PG 70-28 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)
	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)
	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)

- 1 SEE BACKFILL NOTE ON SHEET 0002
- 2 SEE BACKFILL NOTE ON SHEET 0002
- 3 SEE TOPSOIL NOTE ON SHEET 0002
- 5 PRIME COAT ON TOP OF AGGREGATE BASE

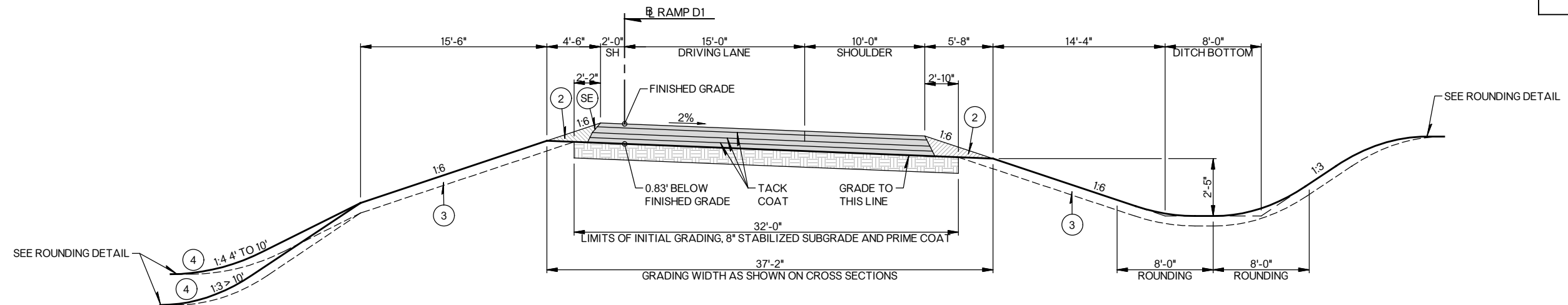
CD STAMPED 4" CONCRETE DIVIDING STRIP WITH 4" SAND BASE.

BC CONC. CURB (6" BARRIER INTEGRAL)

FOR TYPICAL TAPERS AND VARIABLE WIDTHS, SEE SURFACING CONSTRUCTION DETAIL SHEETS.

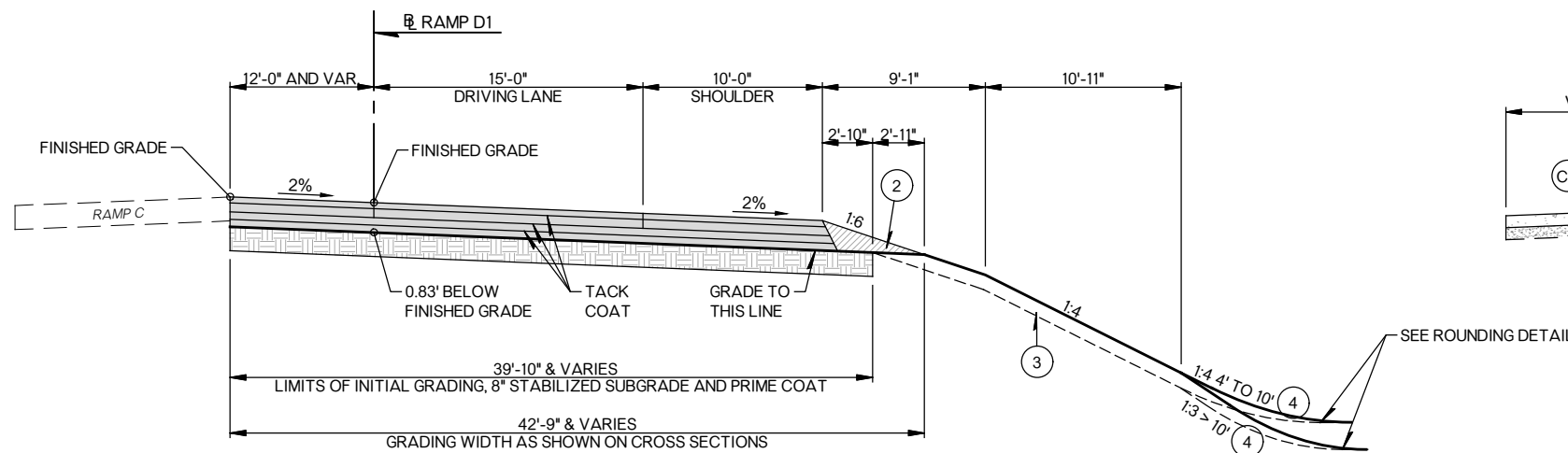
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TYPICAL SECTION



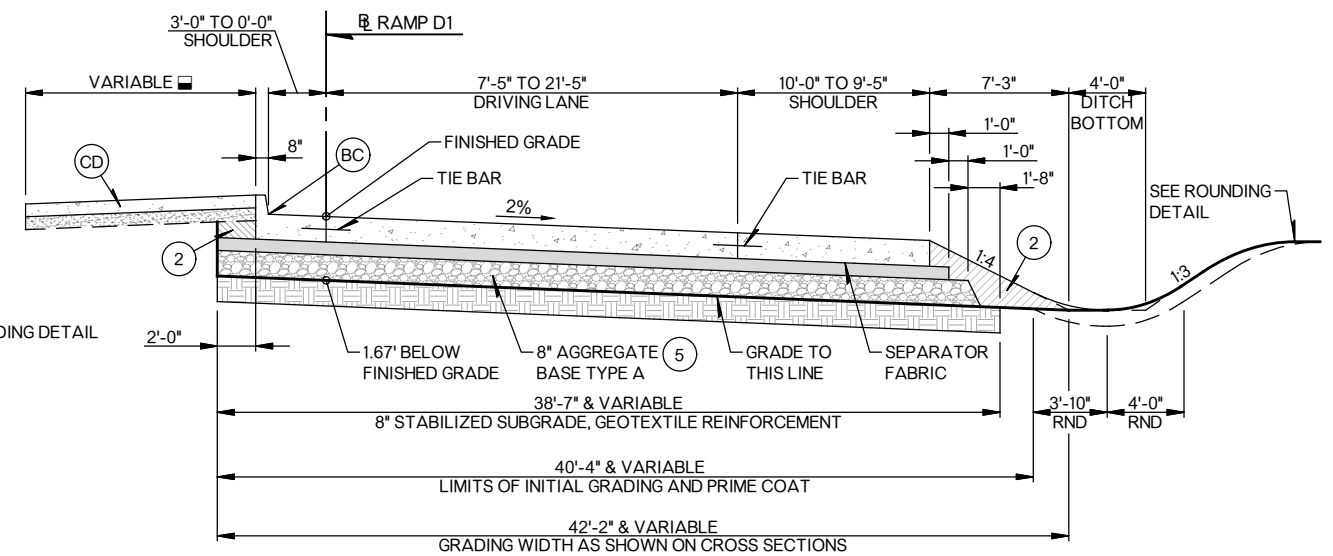
TYPICAL SECTION NO. 10  
RAMP D1  
STA. 717+56.07 TO STA. 719+05.00

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	DRIVING LANES & 2' SHOULDER	8' SHOULDER
SURFACE COURSE	2" SUPERPAVE TYPE S4 (PG 70-28 OK)	2" SUPERPAVE TYPE S4 (PG 64-22 OK)
BASE COURSE	3" SUPERPAVE TYPE S3 (PG 70-28 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)
	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)
	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)



TYPICAL SECTION NO. 11  
RAMP D1  
STA. 719+05.00 TO STA. 720+83.27

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	DRIVING LANE	SHOULDER
SURFACE COURSE	2" SUPERPAVE TYPE S4 (PG 70-28 OK)	2" SUPERPAVE TYPE S4 (PG 64-22 OK)
BASE COURSE	3" SUPERPAVE TYPE S3 (PG 70-28 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)
	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)
	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)	2.5" SUPERPAVE TYPE S3 (PG 64-22 OK)



TYPICAL SECTION NO. 12  
RAMP D1  
STA. 720+83.27 TO STA. 722+02.42  
■ OMIT CONCRETE DIVIDING STRIP AND CURB BETWEEN STA. 721+81.02 TO STA. 722+02.42

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	DRIVING LANE	SHOULDER
SURFACE COURSE	8" DOWEL JOINTED P.C.C. PAVEMENT	8" P.C.C. PAVEMENT
BASE COURSE	4" CEMENT TREATED BASE	4" CEMENT TREATED BASE

(2) SEE BACKFILL NOTE ON SHEET 0002

(3) SEE TOPSOIL NOTE ON SHEET 0002

(4) DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

(5) PRIME COAT ON TOP OF AGGREGATE BASE

(SE) SAFETY EDGE

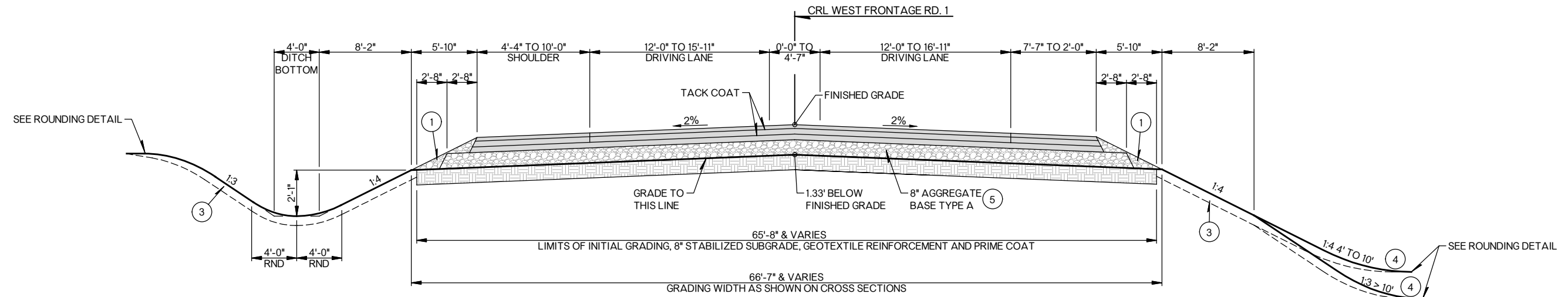
(BC) CONC. CURB (6" BARRIER INTEGRAL)

(CD) STAMPED 4" CONCRETE DIVIDING STRIP WITH 4" SAND BASE.

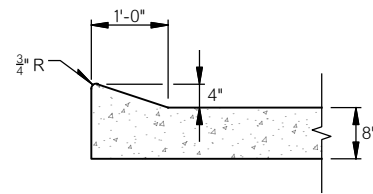
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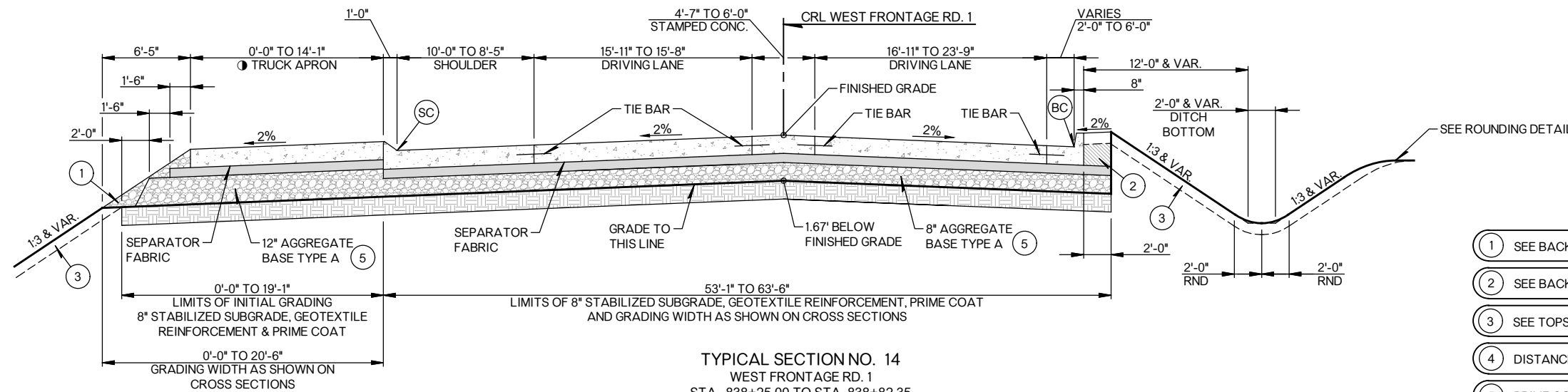


TYPICAL SECTION NO. 13  
WEST FRONTAGE RD. 1  
STA. 836+20.92 TO STA. 838+25.00



SC INTEGRAL CURB

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	DRIVING LANES	SHOULDERS
SURFACE COURSE	2" SUPERPAVE TYPE S4 (PG 64-22 OK)	2" SUPERPAVE TYPE S4 (PG 64-22 OK)
BASE COURSE	3" SUPERPAVE TYPE S3 (PG 64-22 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)
	3" SUPERPAVE TYPE S3 (PG 64-22 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)



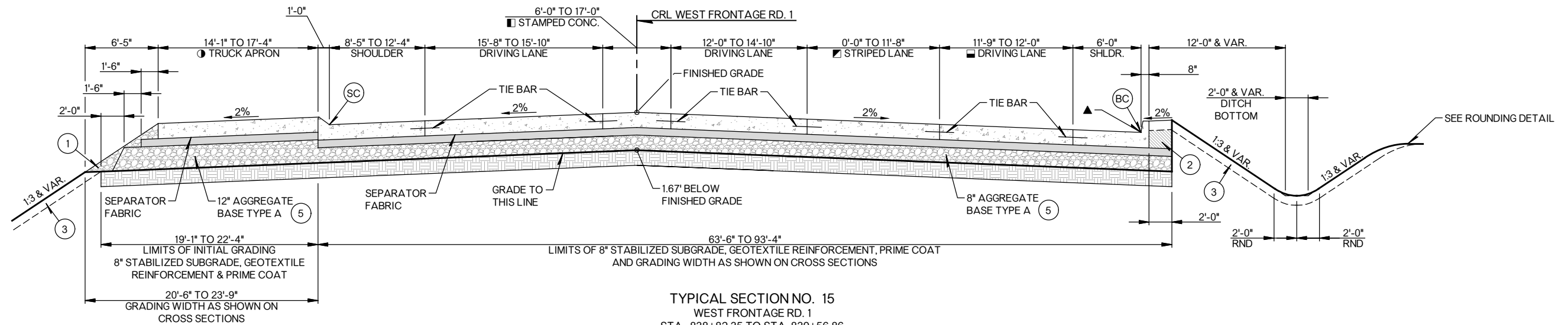
TYPICAL SECTION NO. 14  
WEST FRONTAGE RD. 1  
STA. 838+25.00 TO STA. 838+82.35  
● BEGIN TRUCK APRON AND "SC" CURB AT STA. 838+37.01. TRUCK APRON SHALL HAVE A STAMPED CONCRETE FINISH

PAVEMENT REQUIREMENTS			
PAVEMENT STRUCTURE	DRIVING LANES	SHOULDERS	TRUCK APRON
SURFACE COURSE	8" DOWEL JOINTED P.C.C. PAVEMENT	8" P.C.C. PAVEMENT	8" P.C.C. PAVEMENT
BASE COURSE	4" CEMENT TREATED BASE	4" CEMENT TREATED BASE	4" CEMENT TREATED BASE

- ① SEE BACKFILL NOTE ON SHEET 0002
- ② SEE BACKFILL NOTE ON SHEET 0002
- ③ SEE TOPSOIL NOTE ON SHEET 0002
- ④ DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.
- ⑤ PRIME COAT ON TOP OF AGGREGATE BASE.
- (SC) SEE SC INTEGRAL CURB DETAIL ON THIS SHEET
- (BC) CONCRETE CURB (6" BARRIER-INTEGRAL)
- FOR TYPICAL TAPERS AND VARIABLE WIDTHS, SEE SURFACING CONSTRUCTION DETAIL SHEETS.

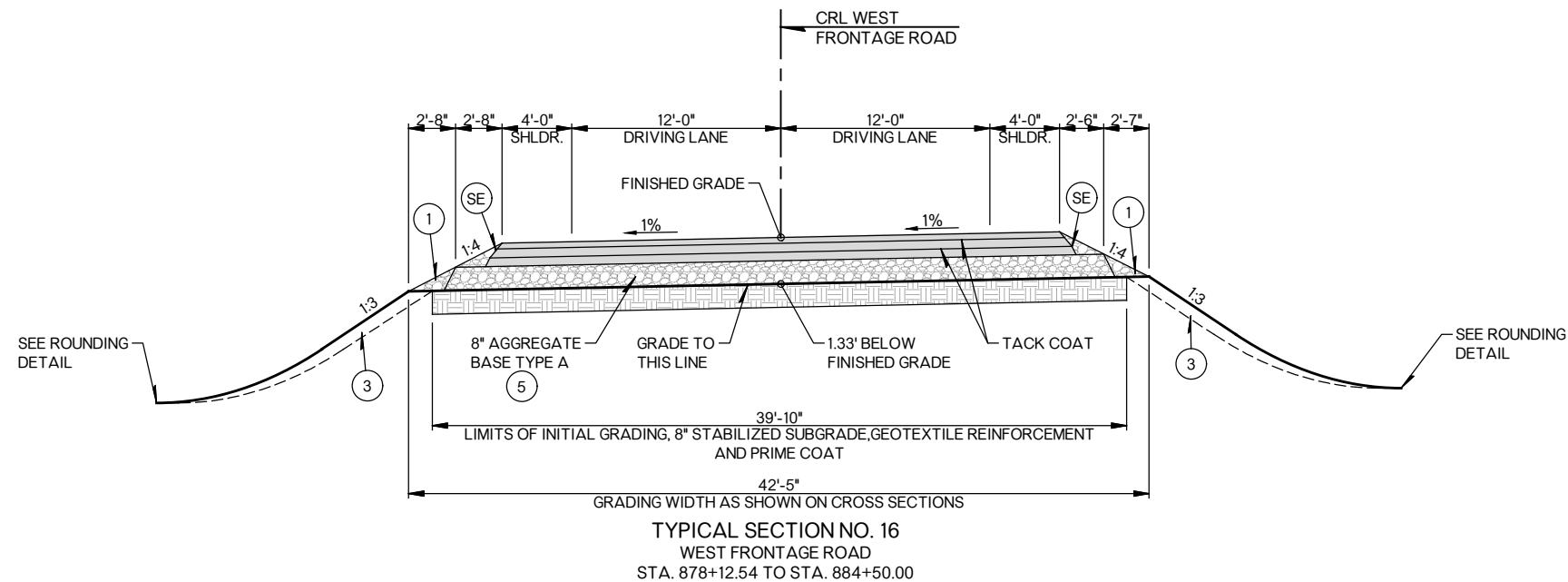
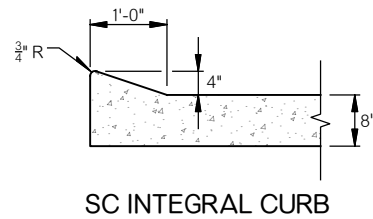
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TYPICAL SECTION



**TYPICAL SECTION NO. 15**  
WEST FRONTAGE RD. 1  
STA. 838+82.35 TO STA. 839+56.86  
END TRUCK APRON AND \*SC\* CURB AT STA. 839+67.24. TRUCK APRON SHALL HAVE A STAMPED CONCRETE FINISH  
STAMPED CONC. STA. 838+82.35 TO STA. 839+56.86  
STRIPED LANE STA. 839+12.06 TO STA. 839+62.23  
DRIVING LANE STA. 838+82.35 TO STA. 839+62.23  
END BC CURB AT STA. 839+36.82

PAVEMENT REQUIREMENTS			
PAVEMENT STRUCTURE	DRIVING LANES	SHOULDERS	TRUCK APRON
SURFACE COURSE	8" DOWEL JOINTED P.C.C. PAVEMENT	8" P.C.C. PAVEMENT	8" P.C.C. PAVEMENT
BASE COURSE	4" CEMENT TREATED BASE	4" CEMENT TREATED BASE	4" CEMENT TREATED BASE

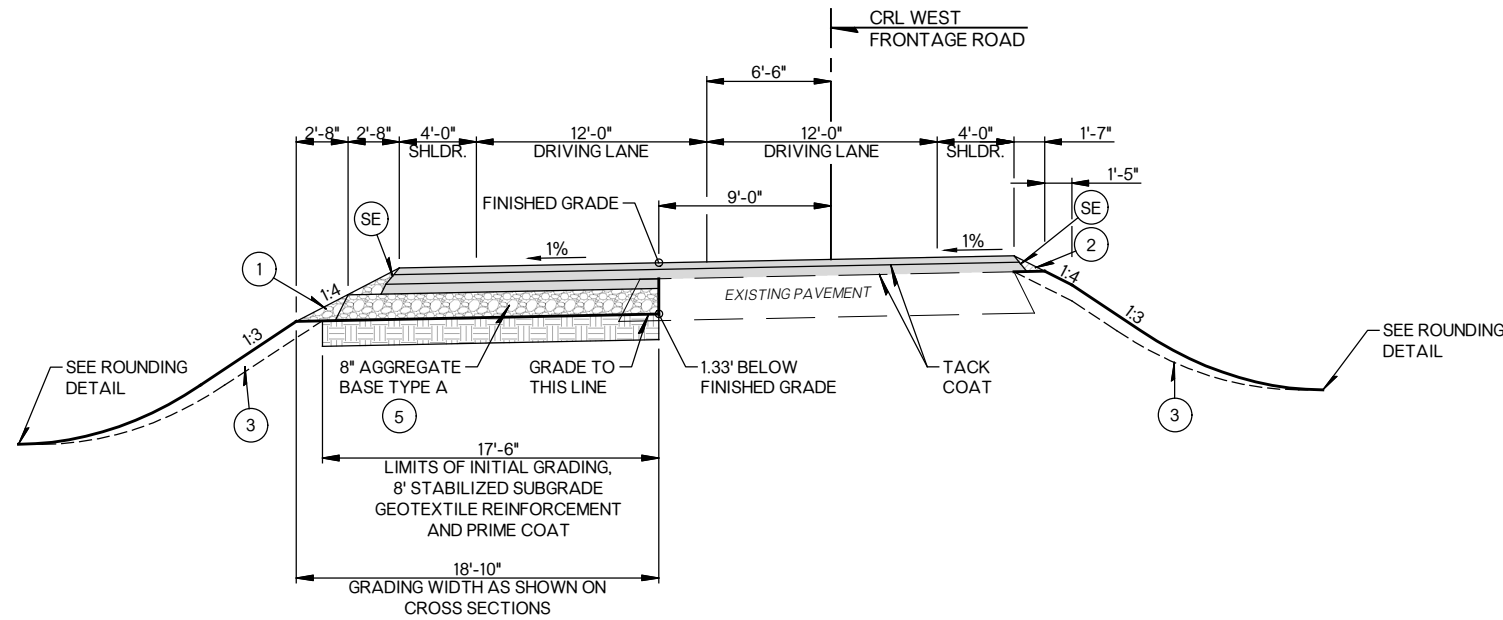


PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	OVERLAY	SHOULDERS
SURFACE COURSE	2" SUPERPAVE TYPE S4 (PG 64-22 OK)	2" SUPERPAVE TYPE S4 (PG 64-22 OK)
BASE COURSE	3" SUPERPAVE TYPE S3 (PG 64-22 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)
	3" SUPERPAVE TYPE S3 (PG 64-22 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)

- (1) SEE BACKFILL NOTE ON SHEET 0002
- (2) SEE BACKFILL NOTE ON SHEET 0002
- (3) SEE TOPSOIL NOTE ON SHEET 0002
- (4) DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.
- (5) PRIME COAT ON TOP OF AGGREGATE BASE.
- (BC) CONCRETE CURB (6" BARRIER-INTEGRAL)
- (SC) SEE SC INTEGRAL CURB DETAIL ON THIS SHEET
- (SE) SAFETY EDGE
- FOR TYPICAL TAPERS AND VARIABLE WIDTHS, SEE SURFACING CONSTRUCTION DETAIL SHEETS.

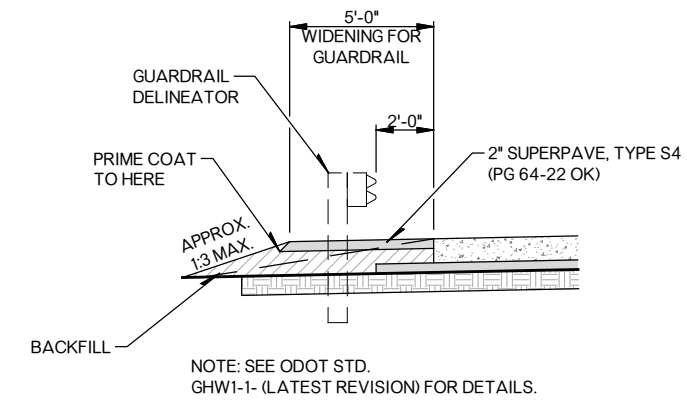
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TYPICAL SECTION

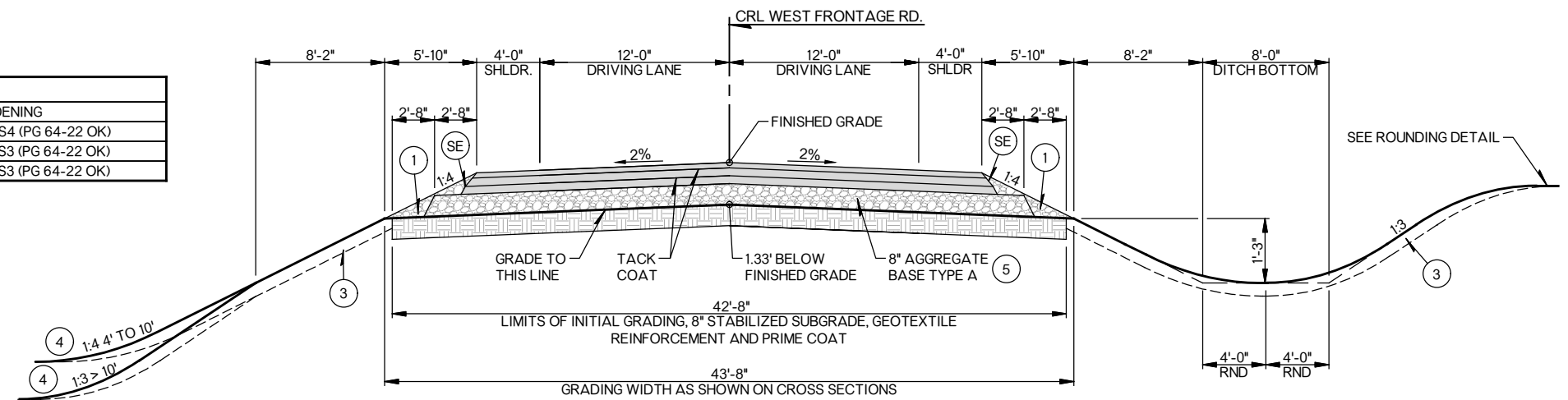


TYPICAL SECTION NO. 17  
WEST FRONTAGE ROAD  
STA. 884+50.00 TO STA. 902+33.90

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	DRIVING LANES	WIDENING
SURFACE COURSE	2" SUPERPAVE TYPE S4 (PG 64-22 OK)	2" SUPERPAVE TYPE S4 (PG 64-22 OK)
BASE COURSE	3" SUPERPAVE TYPE S3 (PG 64-22 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)

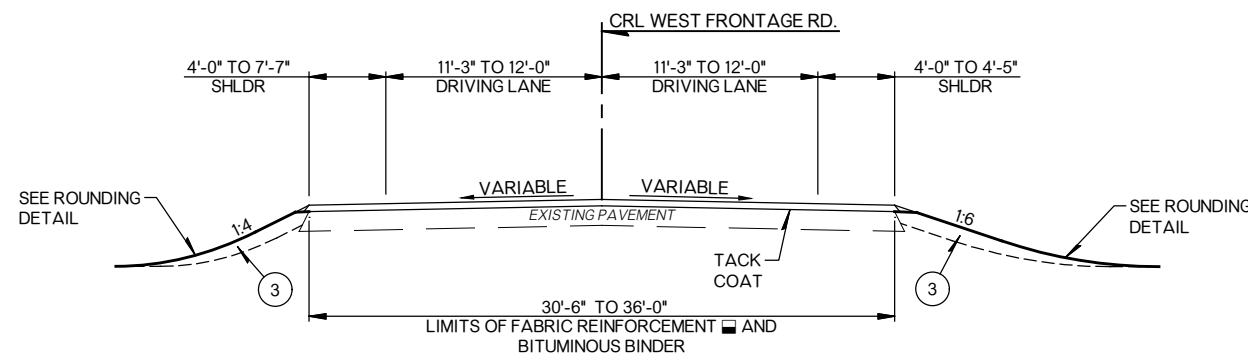


DETAIL  
GUARDRAIL WIDENING



TYPICAL SECTION NO. 18  
WEST FRONTAGE RD.  
STA. 902+33.90 TO STA. 904+83.90

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	DRIVING LANES	SHOULDERS
SURFACE COURSE	2" SUPERPAVE TYPE S4 (PG 64-22 OK)	2" SUPERPAVE TYPE S4 (PG 64-22 OK)
BASE COURSE	3" SUPERPAVE TYPE S3 (PG 64-22 OK)	3" SUPERPAVE TYPE S3 (PG 64-22 OK)



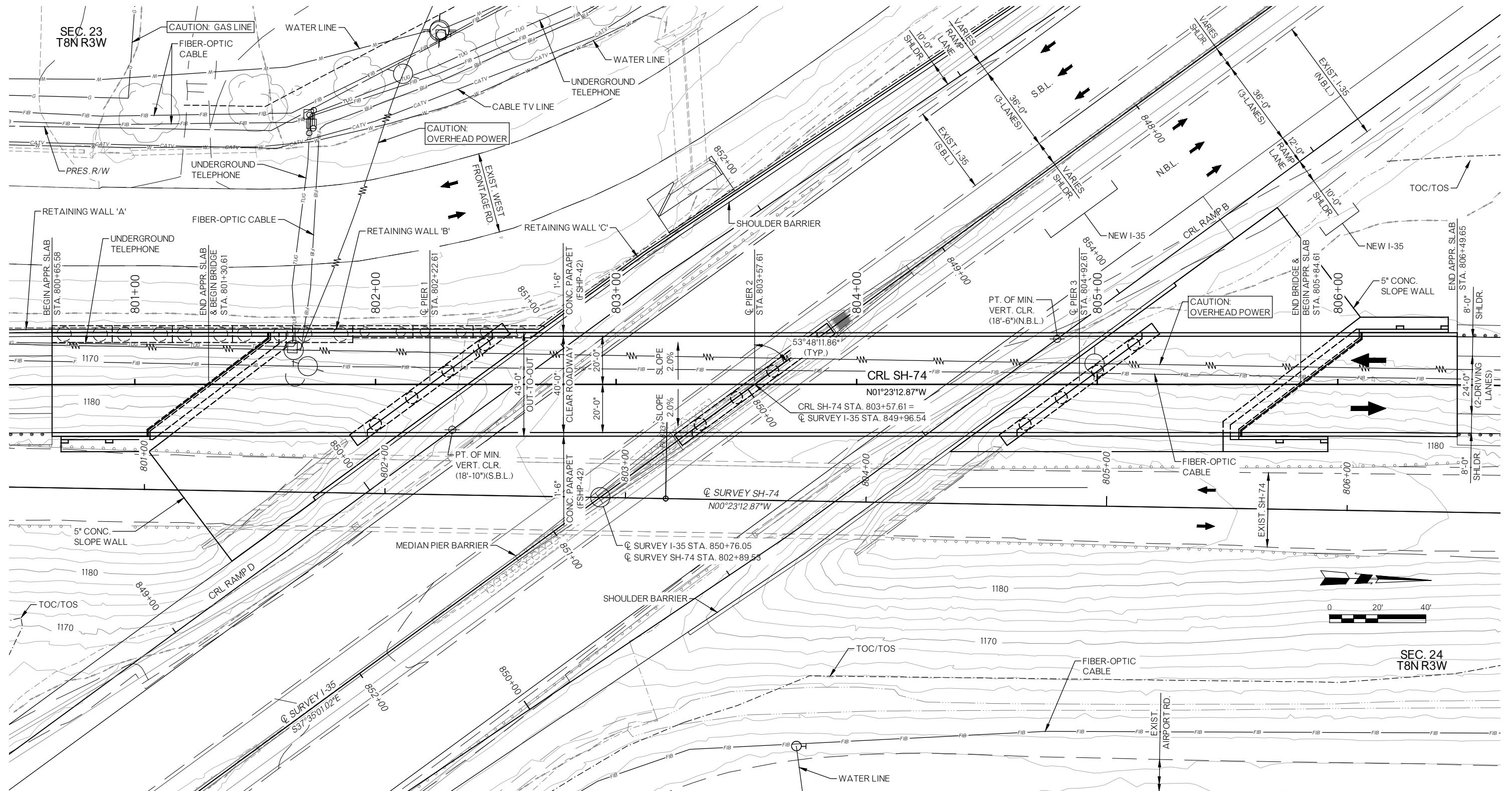
TYPICAL SECTION NO. 19  
WEST FRONTAGE RD. OVERLAY  
STA. 904+83.90 (CRL WEST FRONTAGE RD.) TO STA. 836+20.92 (CRL WEST FRONTAGE RD. 1)  
■ EXTEND FABRIC REINFORCEMENT 2 FEET INTO FULL DEPTH PAVEMENTS AT THE BEGINNING AND ENDING OF STATION EXTENTS.

PAVEMENT REQUIREMENTS		
PAVEMENT STRUCTURE	DRIVING LANES	SHOULDERS
SURFACE COURSE	2" SUPERPAVE TYPE S4 (PG 64-22 OK)	2" SUPERPAVE TYPE S4 (PG 64-22 OK)

- ① SEE BACKFILL NOTE ON SHEET 0002
- ② SEE BACKFILL NOTE ON SHEET 0002
- ③ SEE TOPSOIL NOTE ON SHEET 0002
- ④ DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.
- ⑤ PRIME COAT ON TOP OF AGGREGATE BASE.
- (SE) SAFETY EDGE
- FOR TYPICAL TAPERS AND VARIABLE WIDTHS, SEE SURFACING CONSTRUCTION DETAIL SHEETS.

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TYPICAL SECTION



BM#2-NE CORNER OF RT. HEADWALL CULVERT  
 @ SURVEY SH-74 STA. 796+78.3, 42.1' RT.  
 ELEV. = 1166.27

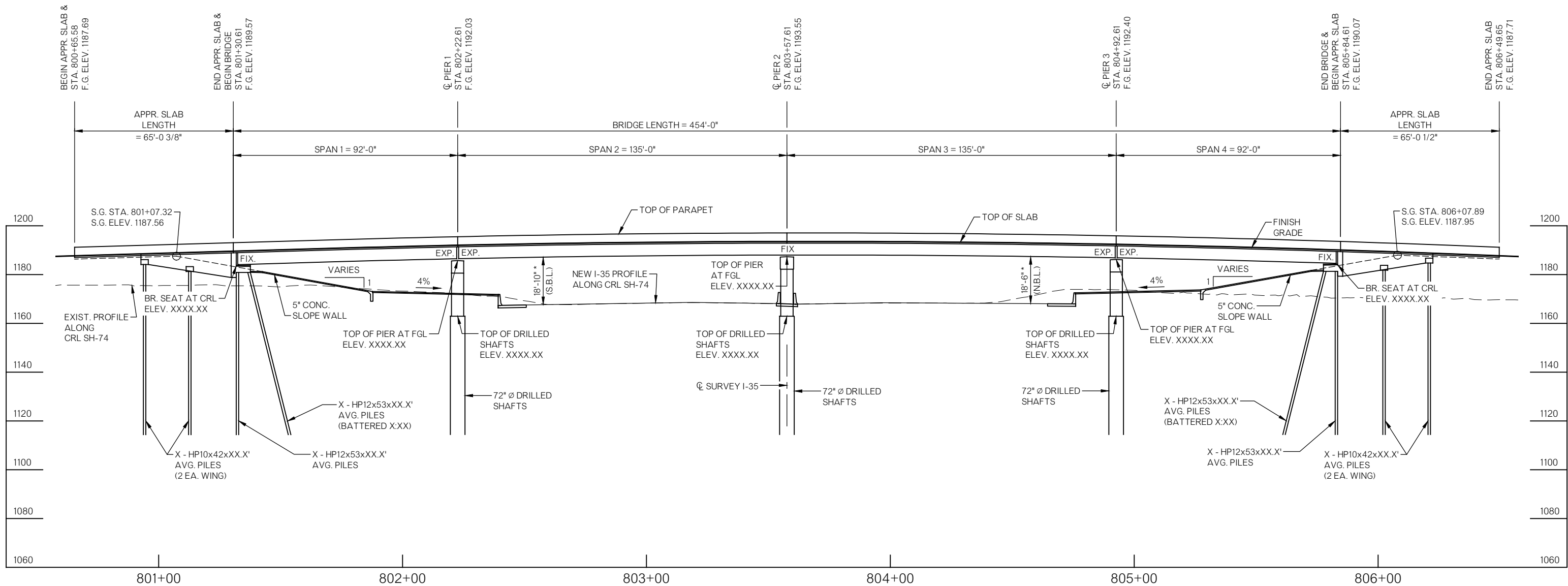
NOTES  
 ALL STATIONING FOLLOWS CRL SH-74, UNLESS  
 NOTED OTHERWISE.

FOR GENERAL BRIDGE ELEVATION, SEE SHEET  
 NO. B002.

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BM#M-44-859 TOP OF NO.4 REBAR WITH ODOT CAP  
 @ SURVEY SH-74 STA. 816+61.6, 103.5' RT.  
 ELEV. = 1167.82

**SH-74 OVER I-35  
 BRIDGE 'A'**  
**GENERAL PLAN AND ELEVATION (BRIDGE 'A')**  
 (SHEET NO. 1 OF 3)  
 92' SIMPLE, 135'-135' CONT., 92' SIMPLE PLATE GIRDER SPANS, SKEWED 53°48'11.86" L.F.,  
 40'-0" CLEAR ROADWAY AND 42" F-SHAPED PARAPETS AT CRL SH-74 STA. 803+57.61  
 State Job No. 29571(04) Sheet No. B001



ELEVATION  
SCALE 1" = 20'

BM#2-NE CORNER OF RT. HEADWALL CULVERT  
CL SURVEY SH-74 STA. 796+78.3, 42.1' RT.  
ELEV. = 1166.27

BM#44-859 TOP OF NO. 4 REBAR WITH ODOT CAP  
CL SURVEY SH-74 STA. 816+61.6, 103.5' RT.  
ELEV. = 1167.82

NOTES  
\* MINIMUM THEORETICAL VERTICAL CLEARANCE.  
SEE PLAN VIEW FOR LOCATIONS.

NOTES  
ALL STATIONING FOLLOWS CRL SH-74, UNLESS NOTED OTHERWISE.  
FOR PLAN VIEW, SEE SHEET NO. B001.

CONST. 92' SIMPLE, 135'-135' CONT., 92' SIMPLE  
PLATE GIRDER SPANS; 40'-0" CLR. RDWY. W/ CONC.  
PARAPETS (FSHP-42) SKEWED 53°48'11.86" L.F.

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SH-74 OVER I-35  
BRIDGE 'A'  
**GENERAL PLAN AND ELEVATION (BRIDGE 'A')**  
(SHEET NO. 2 OF 3)  
92' SIMPLE, 135'-135' CONT., 92' SIMPLE PLATE GIRDER SPANS, SKEWED 53°48'11.86" L.F.,  
40'-0" CLEAR ROADWAY AND 42" F-SHAPED PARAPETS AT CRL SH-74 STA. 803+57.61  
State Job No. 29571(04) Sheet No. B002

DESIGN DATA  
(LOAD AND RESISTANCE FACTOR DESIGN)

CLASS "AA" CONCRETE F'C = 4,000 PSI  
CLASS "A" CONCRETE F'C = 3,000 PSI  
REINFORCING STEEL (GRADE 60) FY = 60,000 PSI  
STRUCTURAL STEEL M270 (GRADE 50W) FY = 50,000 PSI  
STAINLESS STEEL A240 (TYPE 316) FY = 30,000 PSI

LOADING: HL-93 AND ODOT OVERLOAD TRUCK  
20 PSF FUTURE WEARING SURFACE  
5 PSF STAY-IN-PLACE FORMS

DESIGN: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 9TH EDITION.  
ANSI/AASHTO/AWS D1.5 BRIDGE WELDING CODE.  
ANSI/AWS D1.6 STRUCTURAL STEEL WELDING CODE.

LRFR INVENTORY RATING FACTOR: X.XX  
LFD OPERATING RATING: HS XX.X

SUMMARY OF QUANTITIES (BRIDGE 'A')

DESCRIPTION	UNIT	ABUTS.	PIERS	SUPSTR.	APPR. SLABS	SLOPE WALLS	TOTAL
SUBSTRUCTURE EXCAVATION COMMON	CY	1.00					1.00
CLSM BACKFILL	CY	1.00					1.00
APPROACH SLAB	SY				1.00		1.00
SAW-CUT GROOVING	SY			1.00	1.00		1.00
42" F-SHAPED PARAPET	LF			1.00	1.00		1.00
STRUCTURAL STEEL	LB			1.00			1.00
STAINLESS STEEL FIXED BEARING ASSEMBLY	EA			1.00			1.00
STAINLESS STEEL EXP. BEARING ASSEMBLY	EA			1.00			1.00
ELASTOMERIC COATING	SF			1.00			1.00
CLASS AA CONCRETE	CY			1.00			1.00
CLASS A CONCRETE	CY	1.00	1.00				1.00
CLASS C CONCRETE	CY						1.00
SLOPE WALL (5")	SY					1.00	1.00
REINFORCING STEEL	LB		1.00				1.00
EPOXY COATED REINFORCING STEEL	LB	1.00	1.00	1.00			1.00
PILES, FURNISHED (HP 10x42)	LF	1.00					1.00
PILES, FURNISHED (HP 12x53)	LF	1.00					1.00
PILES, DRIVEN (HP 10x42)	LF	1.00					1.00
PILES, DRIVEN (HP 12x53)	LF	1.00					1.00
PILE SPLICE, (NON-BIDDABLE)	EA	1.00					1.00
WATER REPELLENT (VISUALLY INSPECTED)	SY	1.00	1.00	1.00	1.00		1.00
DRILLED SHAFTS 72" DIAMETER	LF		1.00				1.00
CROSSHOLE SONIC LOGGING	EA		1.00				1.00
SEALED EXPANSION JOINTS	LF			1.00			1.00
SEALER CRACK PREPARATION	LF			1.00			1.00
SEALER RESIN	GAL			1.00			1.00
6" PERFORATED PIPE UNDERDRAIN ROUND	LF	1.00				1.00	1.00
6" NON-PERF. PIPE UNDERDRAIN RND.	LF	1.00				1.00	1.00
OUTLET LATERAL HEADWALL	EA	1.00					1.00
REMOVAL OF EXISTING BRIDGE STRUCTURE	LSUM						1.00

INDEX OF SHEETS (BRIDGE 'A')

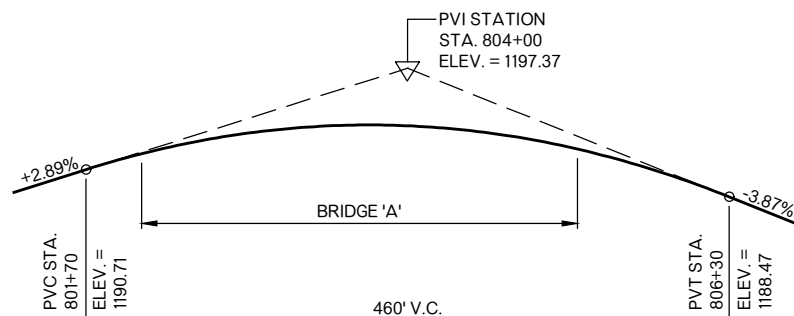
NO.	DESCRIPTION
ABXX	PAY QUANTITIES AND GENERAL NOTES (BRIDGE)
B001	GENERAL PLAN AND ELEVATION (SHEET 1 OF 3)
B002	GENERAL PLAN AND ELEVATION (SHEET 2 OF 3)
B003	GENERAL PLAN AND ELEVATION (SHEET 3 OF 3)
BXXX	SUBSURFACE PROFILE (SHEET 1 OF 3)
BXXX	SUBSURFACE PROFILE (SHEET 2 OF 3)
BXXX	SUBSURFACE PROFILE (SHEET 3 OF 3)
BXXX	SUBSTRUCTURE STAKING DIAGRAM
BXXX	ABUTMENT 1 DETAILS (SHEET 1 OF 4)
BXXX	ABUTMENT 1 DETAILS (SHEET 2 OF 4)
BXXX	ABUTMENT 1 DETAILS (SHEET 3 OF 4)
BXXX	ABUTMENT 1 DETAILS (SHEET 4 OF 4)
BXXX	ABUTMENT 1 WINGWALL DETAILS
BXXX	ABUTMENT 2 DETAILS (SHEET 1 OF 4)
BXXX	ABUTMENT 2 DETAILS (SHEET 2 OF 4)
BXXX	ABUTMENT 2 DETAILS (SHEET 3 OF 4)
BXXX	ABUTMENT 2 DETAILS (SHEET 4 OF 4)
BXXX	ABUTMENT 2 WINGWALL DETAILS
BXXX	SUBSTRUCTURE EXCAVATION AND PIPE UNDERDRAIN ASSEMBLY DETAILS
BXXX	PIER 1 DETAILS (SHEET 1 OF 2)
BXXX	PIER 1 DETAILS (SHEET 2 OF 2)
BXXX	PIER 2 DETAILS (SHEET 1 OF 2)
BXXX	PIER 2 DETAILS (SHEET 2 OF 2)
BXXX	PIER 3 DETAILS (SHEET 1 OF 2)
BXXX	PIER 3 DETAILS (SHEET 2 OF 2)
BXXX	MISCELLANEOUS PIER DETAILS (SHEET 1 OF 2)
BXXX	MISCELLANEOUS PIER DETAILS (SHEET 2 OF 2)
BXXX	BEARING DETAILS
BXXX	STRUCTURAL STEEL DETAILS (SHEET 1 OF 4)
BXXX	STRUCTURAL STEEL DETAILS (SHEET 2 OF 4)
BXXX	STRUCTURAL STEEL DETAILS (SHEET 3 OF 4)
BXXX	STRUCTURAL STEEL DETAILS (SHEET 4 OF 4)
BXXX	BRIDGE DECK FORMWORK BRACING DETAILS
B004	TYPICAL CROSS SECTION
BXXX	TYPICAL LONGITUDINAL SECTION
BXXX	BOTTOM SLAB REINFORCING PLAN (SHEET 1 OF 2)
BXXX	BOTTOM SLAB REINFORCING PLAN (SHEET 2 OF 2)
BXXX	TOP SLAB REINFORCING PLAN (SHEET 1 OF 2)
BXXX	TOP SLAB REINFORCING PLAN (SHEET 2 OF 2)
BXXX	EXPANSION JOINT DETAILS
BXXX	APPROACH SLAB DETAILS (SHEET 1 OF 4)
BXXX	APPROACH SLAB DETAILS (SHEET 2 OF 4)
BXXX	APPROACH SLAB DETAILS (SHEET 3 OF 4)
BXXX	APPROACH SLAB DETAILS (SHEET 4 OF 4)
BXXX	SLOPE WALL DETAILS (SHEET 1 OF 2)
BXXX	SLOPE WALL DETAILS (SHEET 2 OF 2)

ABUTMENTS (HP12x53 PILING)	ABUT. 1	ABUT. 2
(1) FACTORED PILE REACTION (TONS/PILE)	= 0.0	0.0
PILE LENGTH (AVG.) (BR. SEAT, VERTICAL) (FT.)	= 0.0	0.0
PILE LENGTH (AVG.) (BR. SEAT, BATTERED) (FT.)	= 0.0	0.0
(2) PILE LENGTH (AVG.) (WINGS) (FT.)	= 0.0	0.0

PIERS (72" DIAMETER DRILLED SHAFTS)	PIER 1	PIER 1	PIER 3
MINIMUM DEPTH INTO ROCK (FT.)	= 0.0	0.0	0.0
DEPTH OF ROCK NEGLECTED FOR FRICTION (FT.)	= 0.0	0.0	0.0
UNIT BEARING RESISTANCE (TSF)	= 0.0	0.0	0.0
BEARING RESISTANCE FACTOR	= 0.7	0.7	0.7
FACTORED BEARING RESISTANCE (TONS/SHAFT)	= 0.0	0.0	0.0
NOMINAL UNIT FRICTION RESISTANCE (TSF)	= 0.0	0.0	0.0
FRICTION RESISTANCE FACTOR	= 0.45	0.45	0.45
FACTORED FRICTION RESISTANCE (TONS/SHAFT)	= 0.0	0.0	0.0
TOTAL FACTORED RESISTANCE (TONS/SHAFT)	= 0.0	0.0	0.0
TOTAL FACTORED REACTION (TONS/SHAFT)	= 0.0	0.0	0.0

- (1) ABUTMENT PILING SHALL BE DRIVEN THROUGH THE COMPACTED FILL. PILING SHALL BE DRIVEN TO POINT BEARING ON SOLID FOUNDATION MATERIAL AT THE APPROXIMATE ELEVATION SHOWN ON THE PLANS. IF THE FACTORED PILE CAPACITY IS NOT OBTAINED AT THIS ELEVATION, DRIVING SHALL CONTINUE UNTIL THE FACTORED PILE CAPACITY IS OBTAINED. THE LENGTH OF STEEL PILING SHOWN ON THE PLANS IS FOR ESTIMATING PURPOSES ONLY.
- (2) HP10x42 PILES, WINGS.

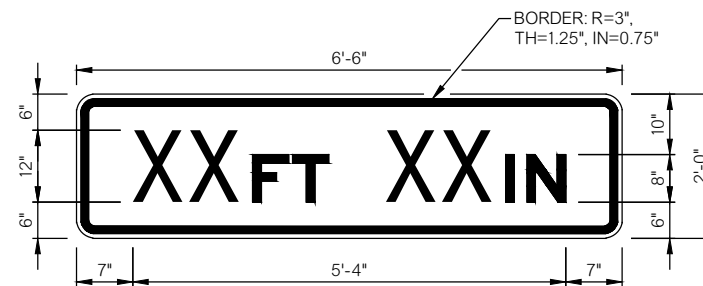
NOTE  
SEE SHEET NO. BXXX FOR INDIVIDUAL PILE LENGTHS.



FINISH GRADE DATA  
CRL SH-74

NOTES  
STATIONING FOLLOWS CRL SH-74.

FINISH GRADE IS ALONG FINISH GRADE LINE (FGL).



SIGN NUMBER	W12-2p
WIDTH x HEIGHT	6'-6" x 2'-0"
BORDER WIDTH	1.25"
CORNER RADIUS	3"
MOUNTING	BRIDGE PARAPET
BACKGROUND	TYPE: REFLECTIVE COLOR: YELLOW
LEGEND & BORDER	TYPE: REFLECTIVE COLOR: BLACK

VERTICAL CLEARANCE SIGN DETAILS

NOTES

INSTALL ONE (1) PERMANENT SIGN ON THE OUTSIDE OF THE BRIDGE DECK AT THE MIDPOINT OF THE S.B. LANES IN SPAN 2, AND INSTALL ONE (1) PERMANENT SIGN ON THE OUTSIDE OF THE BRIDGE DECK AT THE MIDPOINT OF THE N.B. LANES IN SPAN 3.

SIGNS SHOULD INDICATE A VERTICAL CLEARANCE OF 3" MIN. LESS THAN THE FINAL MEASURED CLEARANCE. THE CONTRACTOR SHALL CONTACT ODOT DIV. 3 FOR FINAL MEASUREMENT OF VERTICAL CLEARANCES.

BRACKETS SHALL PROJECT VERTICAL CLEARANCE SIGN PERPENDICULAR TO I-35 TRAFFIC. HARDWARE AND CONNECTION DETAILS TO THE DECK SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO INSTALLATION.

ALL COSTS FOR FABRICATION, LABOR, MATERIALS, HARDWARE, AND INSTALLATION OF THE VERTICAL CLEARANCE SIGNS, COMPLETE-IN-PLACE, SHALL BE INCLUDED IN THE PRICE BID FOR "INSTALL G.M. SHEET ALUMINUM SIGNS", PER SQ. FT. (TRAFFIC ITEM).

THE INSTALLATION OF THE PERMANENT VERTICAL CLEARANCE SIGNS DOES NOT RELIEVE THE CONTRACTOR OF MAINTAINING APPROPRIATE VERTICAL CLEARANCE SIGNS DURING CONSTRUCTION. COSTS TO BE INCLUDED IN OTHER ITEMS OF WORK.

THE FOLLOWING STANDARDS SHALL BE REQUIRED:

- FSHP-42-2-00E
- EJ-SK-04E
- EJ-DTL-02E
- HP1-2-01E
- LECS-5-2
- PUD-4-1
- PED-4-1

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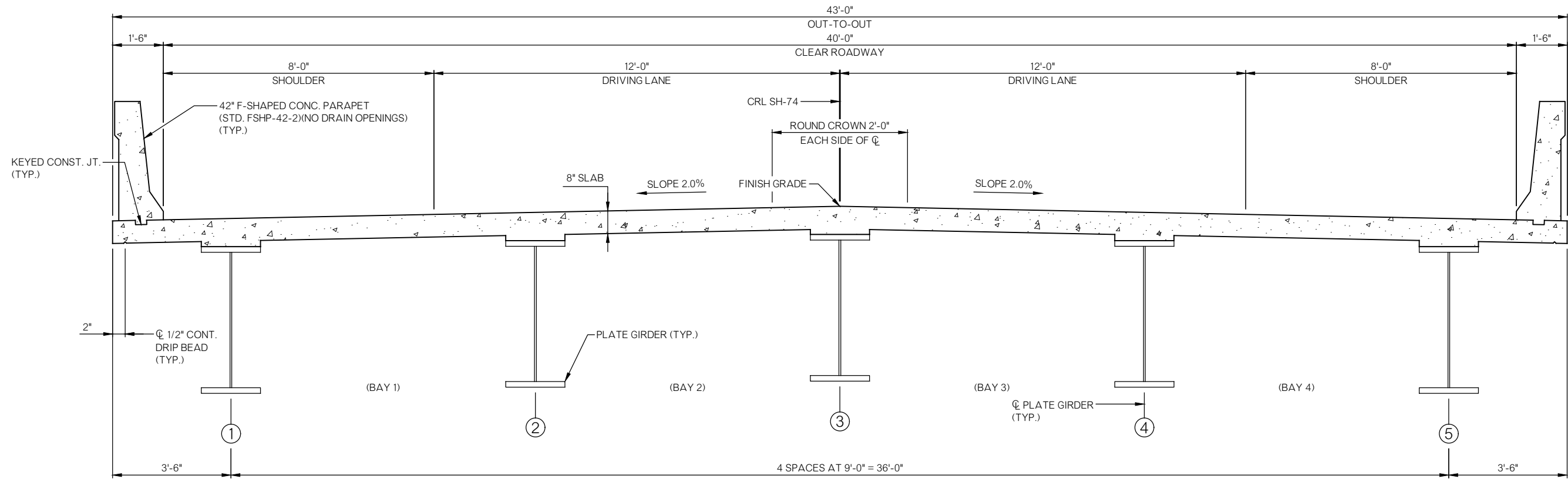
SH-74 OVER I-35  
BRIDGE 'A'

GENERAL PLAN AND ELEVATION (BRIDGE 'A')

(SHEET NO. 3 OF 3)

92' SIMPLE, 135'-135' CONT., 92' SIMPLE PLATE GIRDER SPANS, SKEWED 53°48'11.86" L.F., 40'-0" CLEAR ROADWAY AND 42" F-SHAPED PARAPETS AT CRL SH-74 STA. 803+57.61

State Job No. 29571(04) Sheet No. B003



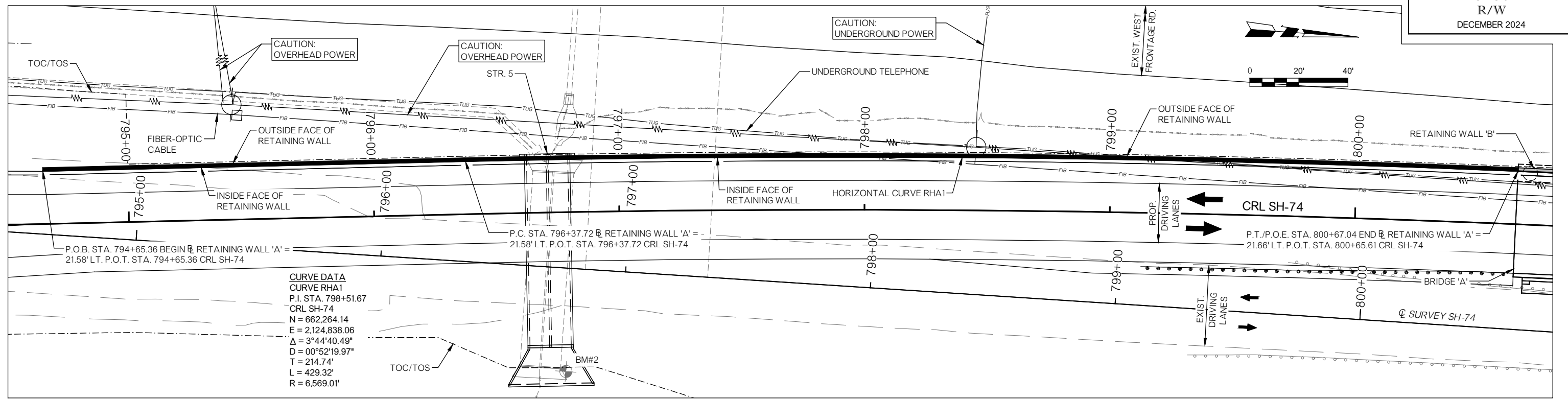
TYPICAL CROSS SECTION

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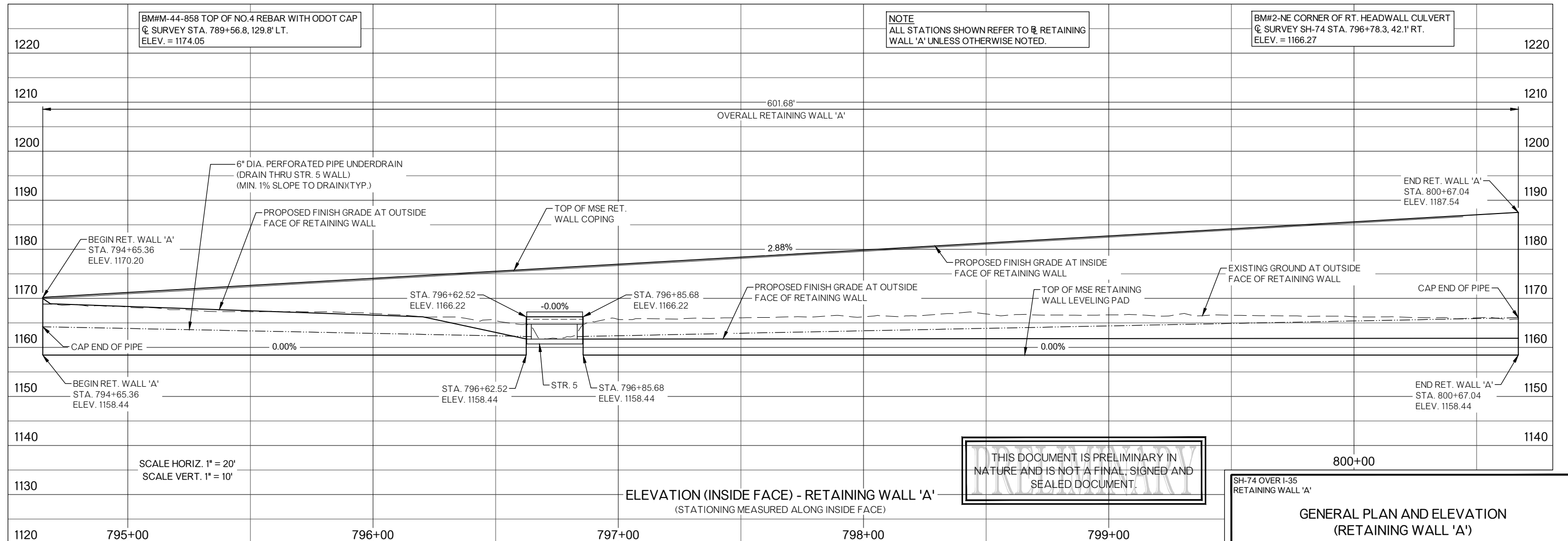
SH-74 OVER I-35  
BRIDGE 'A'

TYPICAL CROSS SECTION  
CRL SH-74 STA. 803+57.61

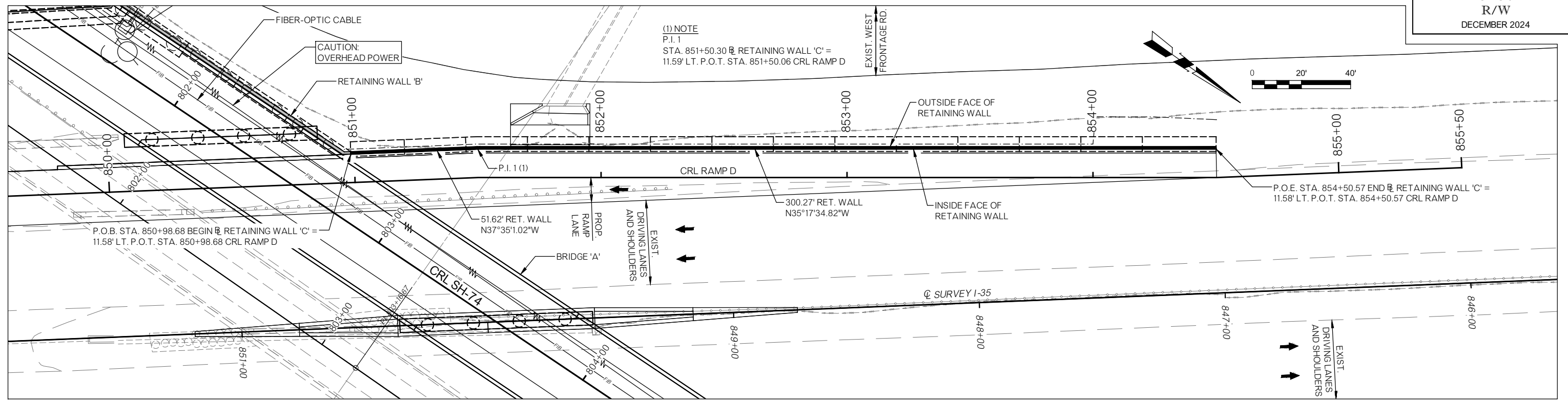
State Job No. 29571(04) Sheet No. B004



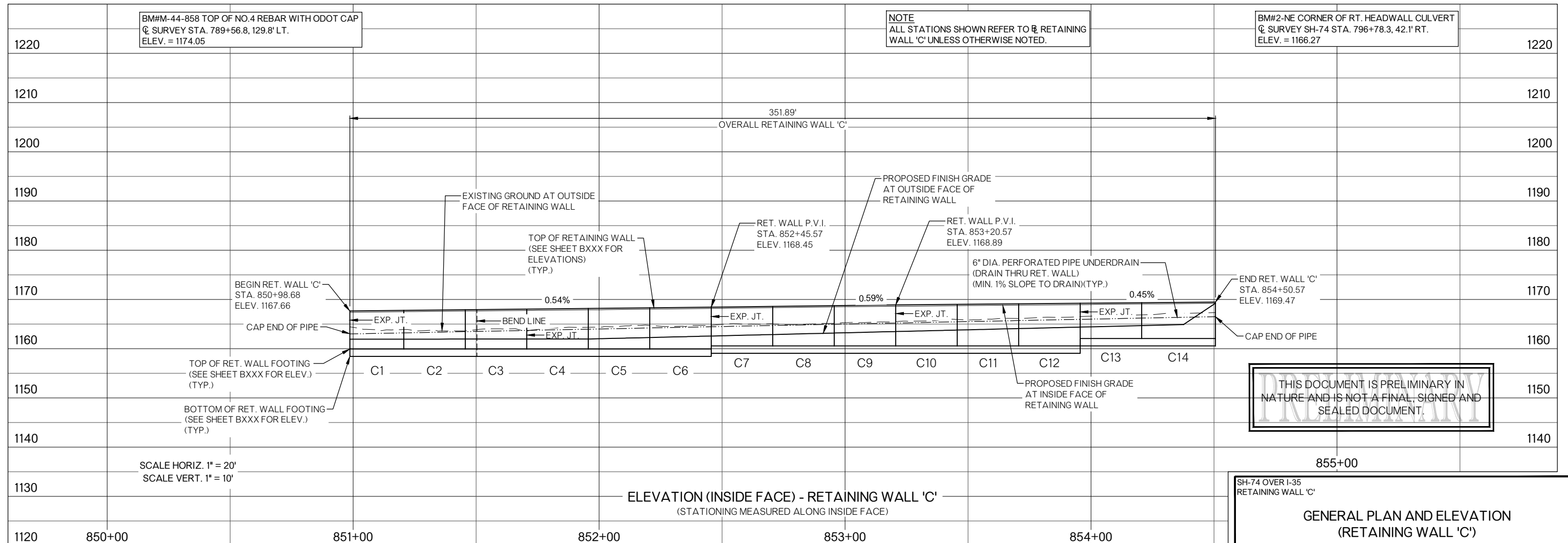
PLAN - RETAINING WALL 'A'  
SCALE 1" = 20'







PLAN - RETAINING WALL 'C'  
SCALE 1" = 20'

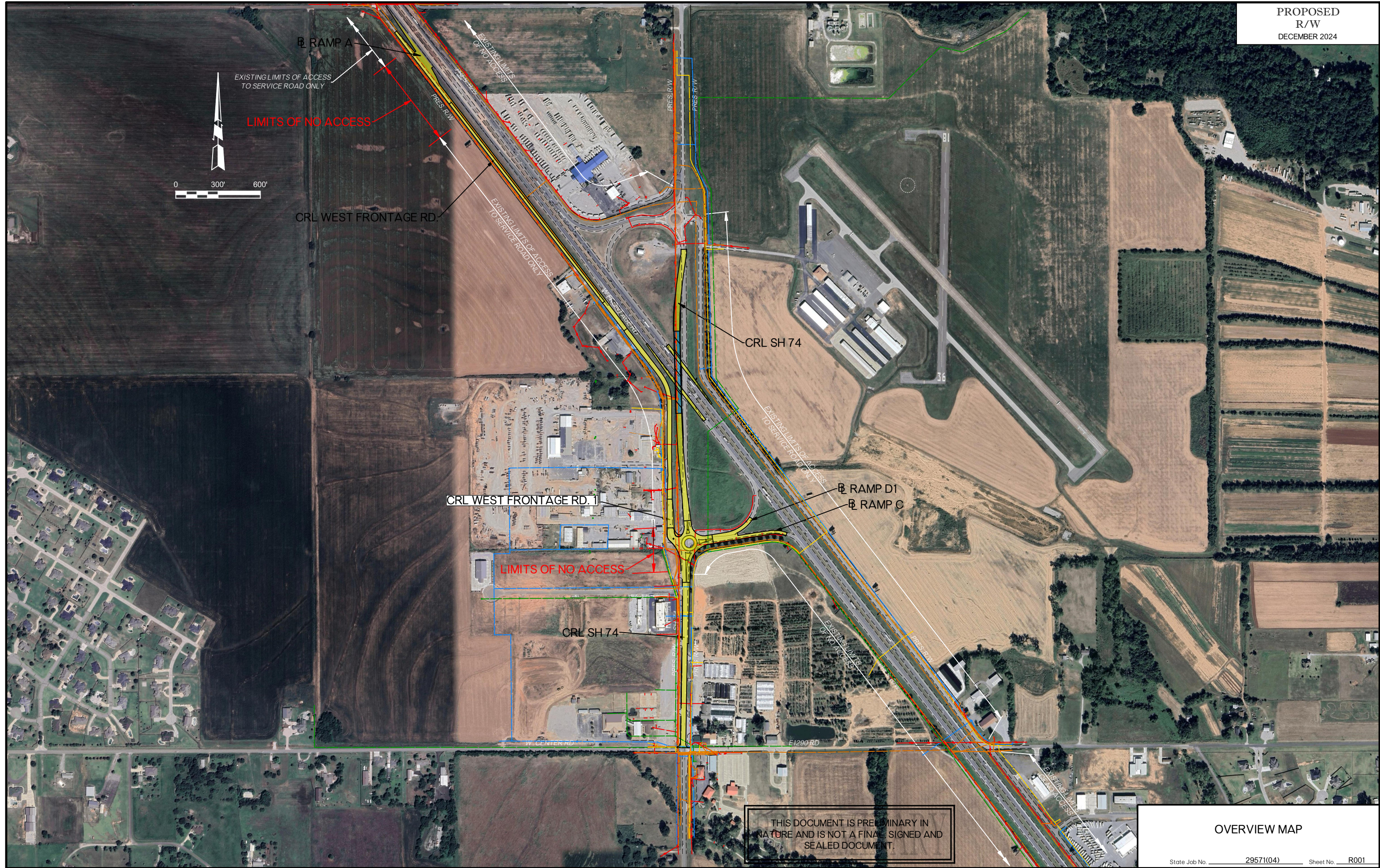
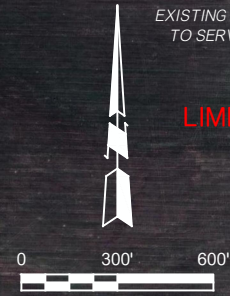


ELEVATION (INSIDE FACE) - RETAINING WALL 'C'  
(STATIONING MEASURED ALONG INSIDE FACE)

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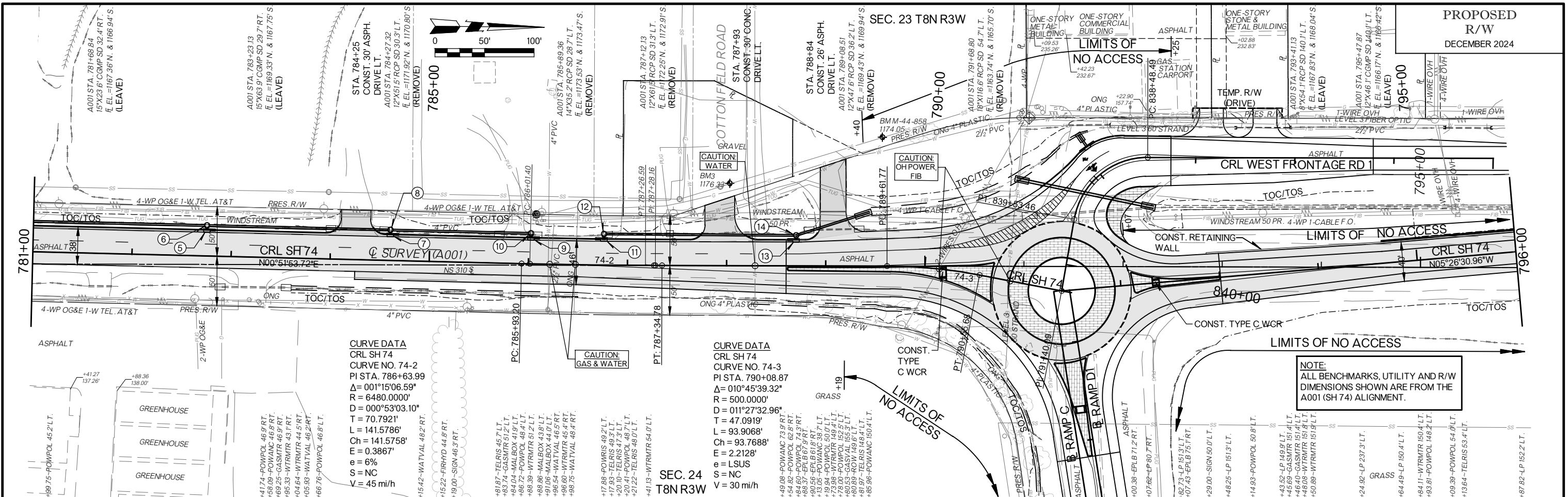
SH-74 OVER I-35  
RETAINING WALL 'C'

GENERAL PLAN AND ELEVATION  
(RETAINING WALL 'C')



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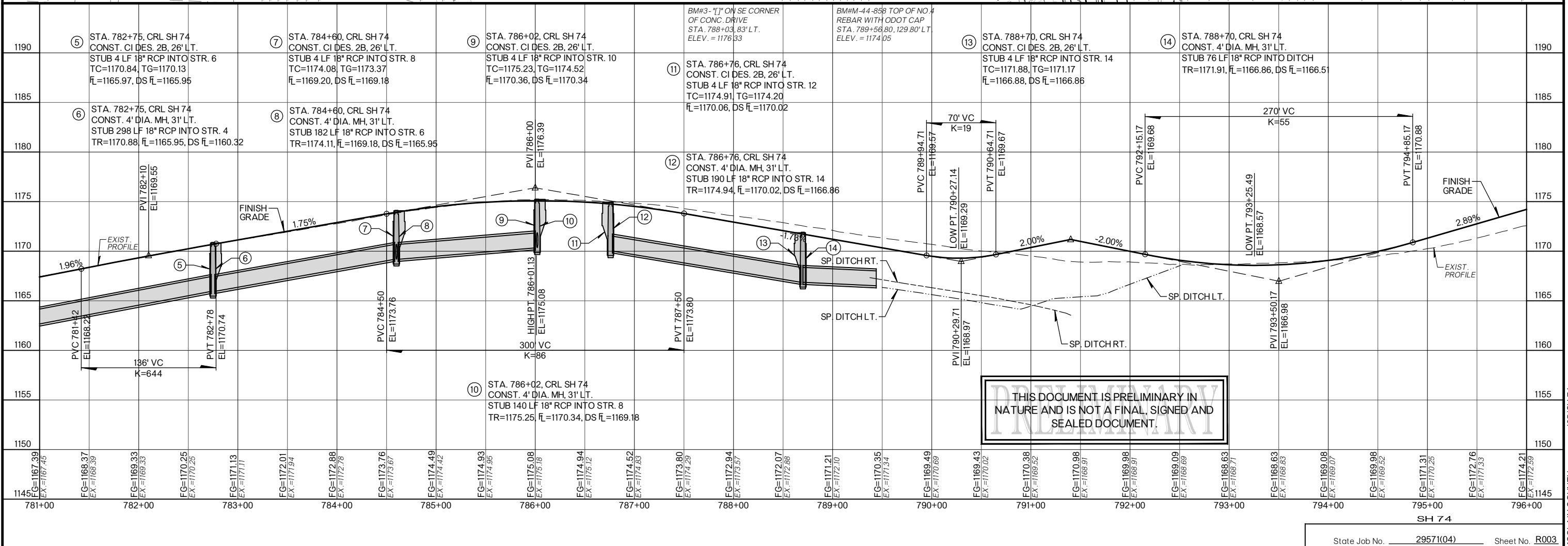




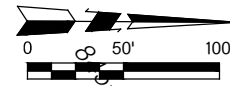
**CURVE DATA**  
CRL SH 74  
CURVE NO. 74-2  
PI STA. 786+63.99  
 $\Delta = 001^{\circ}15'06.59''$   
 $R = 6480.0000'$   
 $D = 000^{\circ}53'03.10''$   
 $T = 70.7921'$   
 $L = 141.5786'$   
 $Ch = 141.5758'$   
 $E = -0.3867'$   
 $e = 6\%$   
 $s = NC$   
 $V = 45 \text{ mi/h}$

**CURVE DATA**  
CRL SH 74  
CURVE NO. 74-3  
PI STA. 790+08.87  
 $\Delta = 010^{\circ}45'39.32''$   
 $R = 500.0000'$   
 $D = 011^{\circ}27'32.96''$   
 $T = 47.0919'$   
 $L = 93.9068'$   
 $Ch = 93.7688'$   
 $E = 2.2128'$   
 $e = LSUS$   
 $s = NC$   
 $V = 30 \text{ mi/h}$

**NOTE:**  
ALL BENCHMARKS, UTILITY AND R/W  
DIMENSIONS SHOWN ARE FROM THE  
A001 (SH 74) ALIGNMENT.

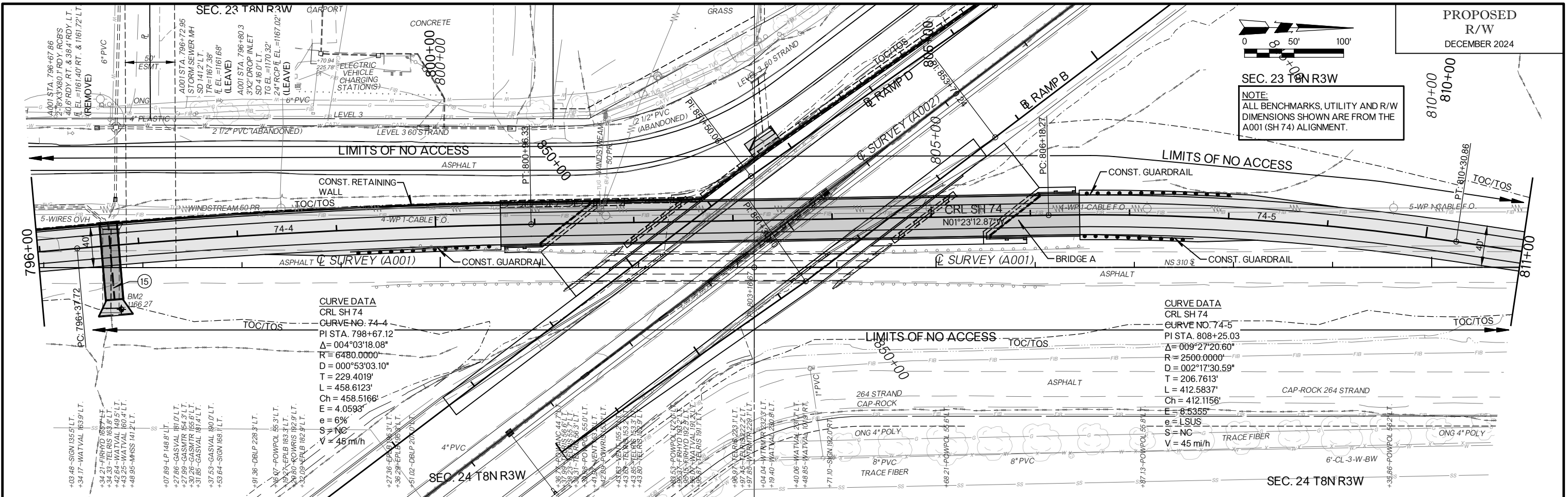


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SEC. 23 T8N R3W

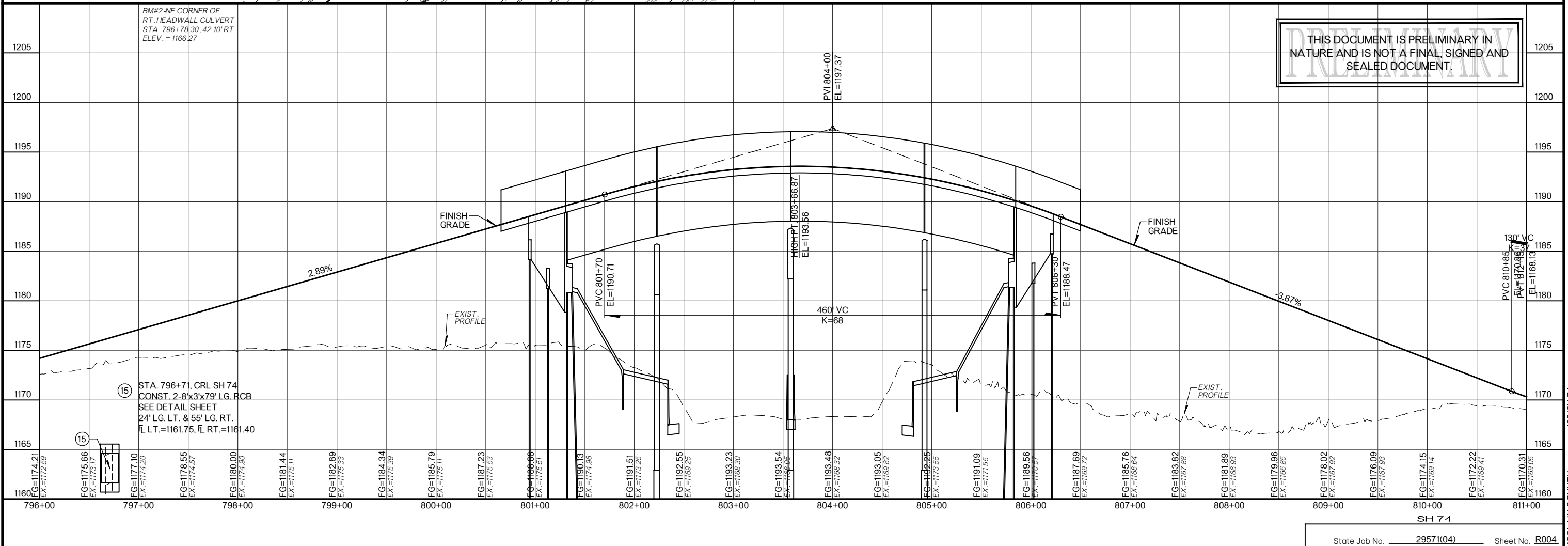
NOTE:  
ALL BENCHMARKS, UTILITY AND R/W  
DIMENSIONS SHOWN ARE FROM THE  
A001 (SH 74) ALIGNMENT.



**CURVE DATA**  
CRL SH 74  
CURVE NO. 74-4  
PI STA. 798+67.12  
 $\Delta = 004^{\circ}03'18.08''$   
 $R = 6480.0000'$   
 $D = 000^{\circ}53'03.10''$   
 $T = 229.4019'$   
 $L = 458.6123'$   
 $Ch = 458.5166'$   
 $E = 4.0593'$   
 $e = 6\%$   
 $S = NC$   
 $V = 45 \text{ mi/h}$

**CURVE DATA**  
CRL SH 74  
CURVE NO. 74-5  
PI STA. 808+25.03  
 $\Delta = 009^{\circ}27'20.60''$   
 $R = 2500.0000'$   
 $D = 002^{\circ}17'30.59''$   
 $T = 206.7613'$   
 $L = 412.5837'$   
 $Ch = 412.1156'$   
 $E = 8.5355'$   
 $e = LSUS$   
 $S = NC$   
 $V = 45 \text{ mi/h}$

THIS DOCUMENT IS PRELIMINARY IN  
NATURE AND IS NOT A FINAL, SIGNED AND  
SEALED DOCUMENT.



BM#2-NE CORNER OF  
RT. HEADWALL CULVERT  
STA. 796+78.30, 42.10' RT.  
ELEV. = 1166.27

15 STA. 796+71, CRL SH 74  
CONST. 2'-8"x3"x79' LG. RCB  
SEE DETAIL SHEET  
24' LG. LT. & 55' LG. RT.  
FL LT. = 1161.75, FL RT. = 1161.40

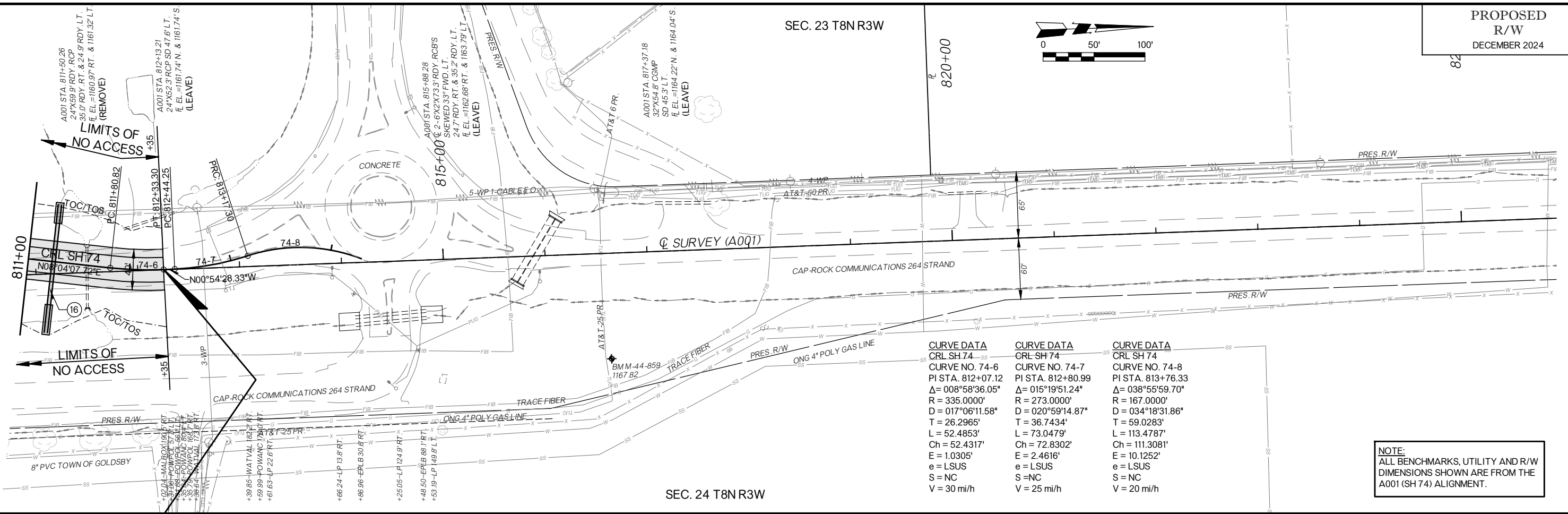
15 PVC 810+85  
AVT B (2) REL  
EL = 1168.13



SEC. 23 T8N R3W

820+00

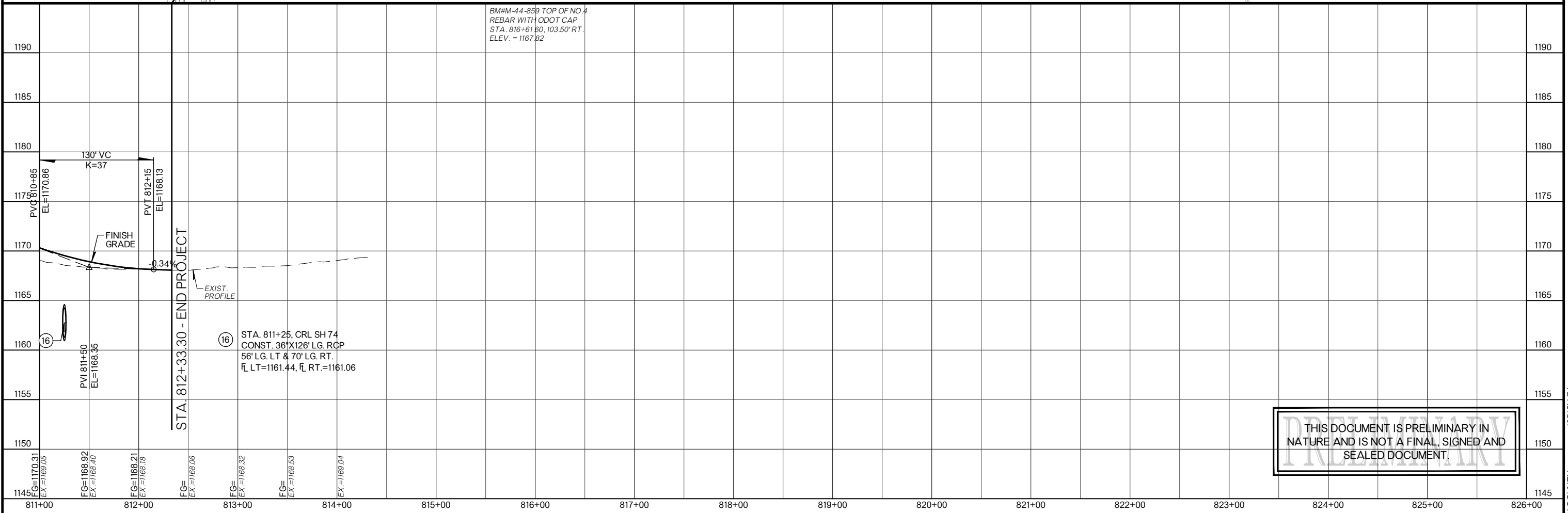
82



CURVE DATA	CURVE DATA	CURVE DATA
CRL SH 74	CRL SH 74	CRL SH 74
CURVE NO. 74-6	CURVE NO. 74-7	CURVE NO. 74-8
PI STA. 812+07.12	PI STA. 812+80.99	PI STA. 813+76.33
$\Delta = 008^{\circ}58'36.05''$	$\Delta = 015^{\circ}19'51.24''$	$\Delta = 038^{\circ}55'59.70''$
R = 335.0000'	R = 273.0000'	R = 167.0000'
D = 017^{\circ}06'11.58''	D = 020^{\circ}59'14.87''	D = 034^{\circ}18'31.86''
T = 26.2965'	T = 36.7434'	T = 59.0283'
L = 52.4853'	L = 73.0479'	L = 113.4787'
Ch = 52.4317'	Ch = 72.8302'	Ch = 111.3081'
E = 1.0305'	E = 2.4616'	E = 10.1252'
e = LSUS	e = LSUS	e = LSUS
S = NC	S = NC	S = NC
V = 30 mi/h	V = 25 mi/h	V = 20 mi/h

**NOTE:**  
ALL BENCHMARKS, UTILITY AND R/W  
DIMENSIONS SHOWN ARE FROM THE  
A001 (SH 74) ALIGNMENT.

SEC. 24 T8N R3W

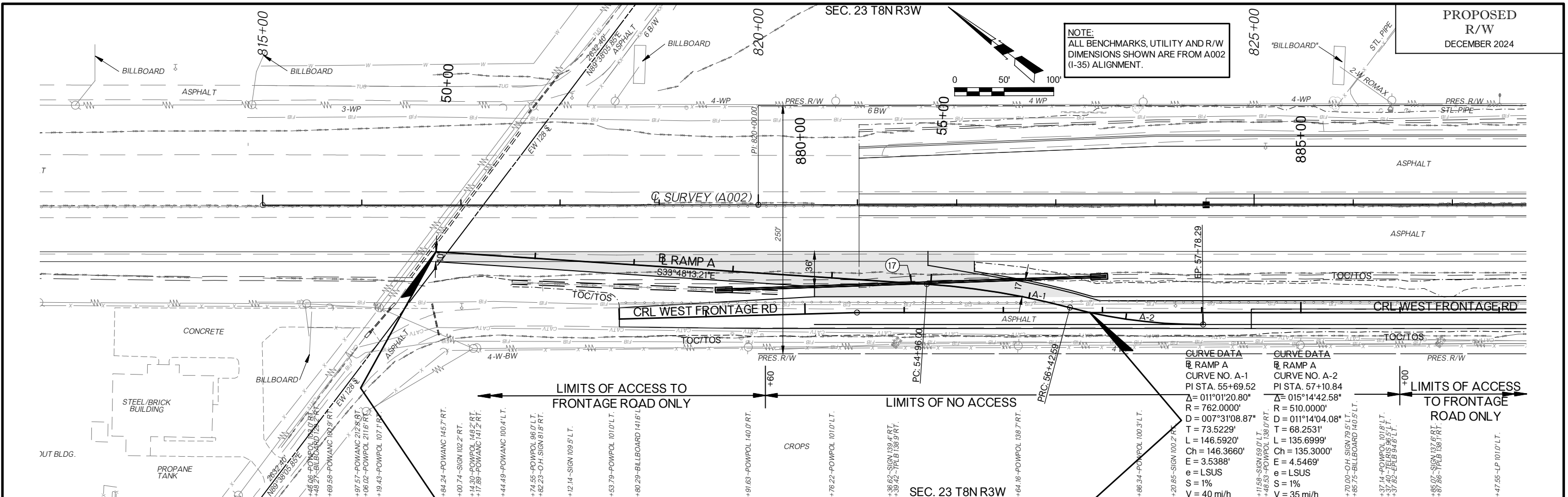
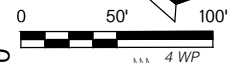


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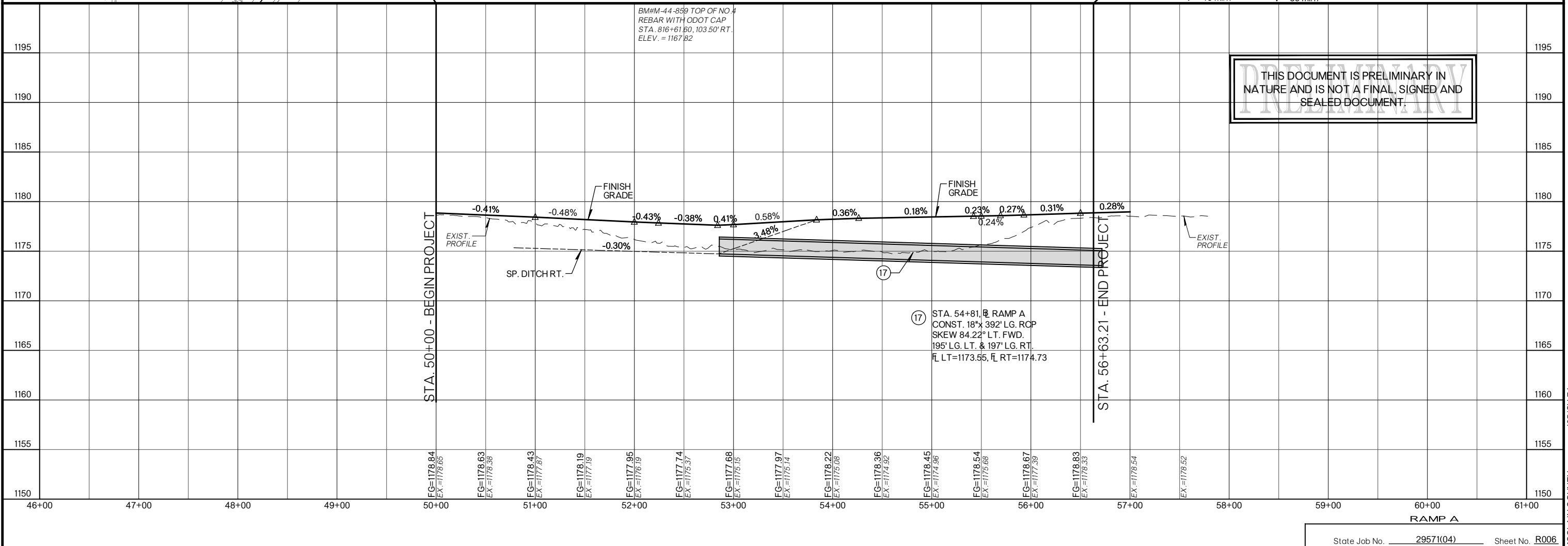
SH 74

MOCLAIN COUNTY 135/SH 74

NOTE:  
ALL BENCHMARKS, UTILITY AND R/W  
DIMENSIONS SHOWN ARE FROM A002  
(1-35) ALIGNMENT.

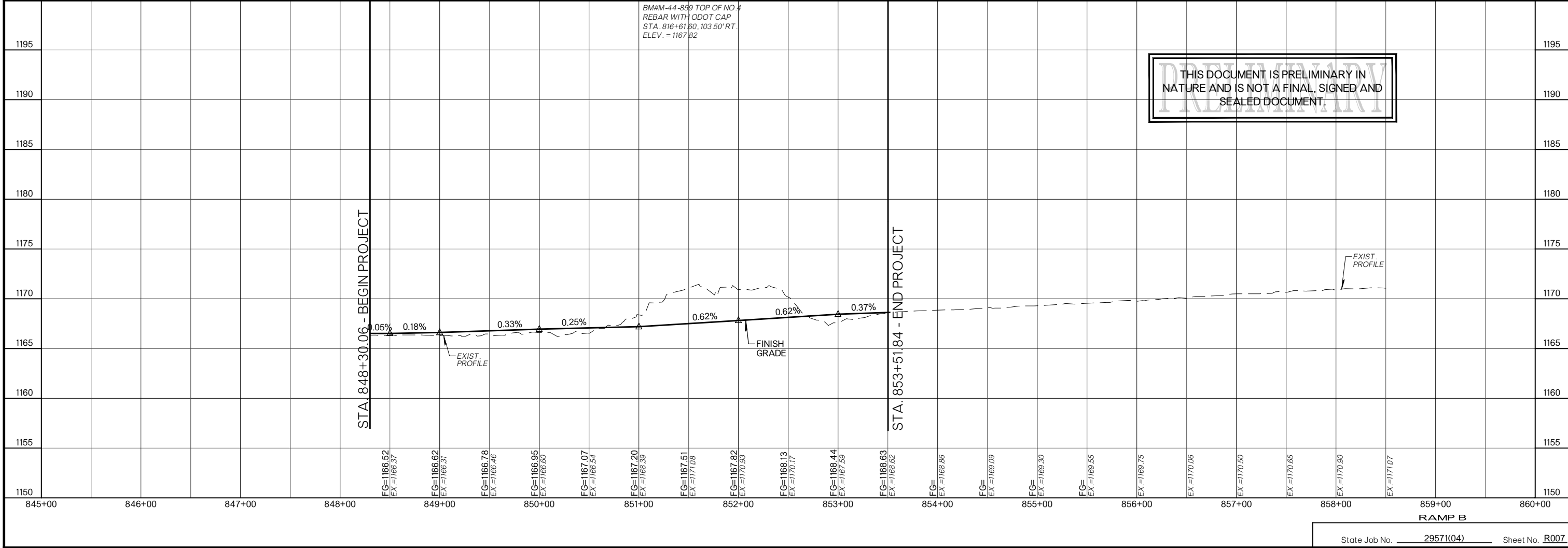
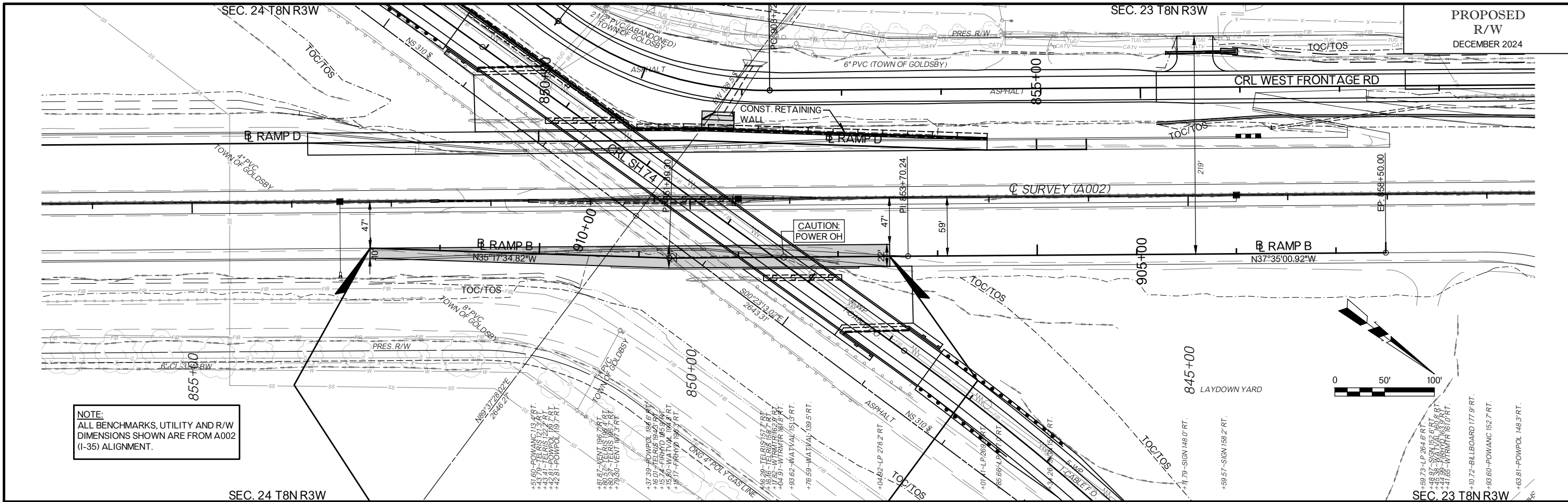


Curve No.	Station	Delta	Radius	Length	Chord	Elevation	Grade	Speed
RAMP A	55+69.52	011°01'20.80"	762.0000'	73.5229'	146.5920'	3.5388'	LSUS	40 mi/h
RAMP A	57+10.84	015°14'42.58"	510.0000'	68.2531'	135.6999'	4.5469'	LSUS	35 mi/h

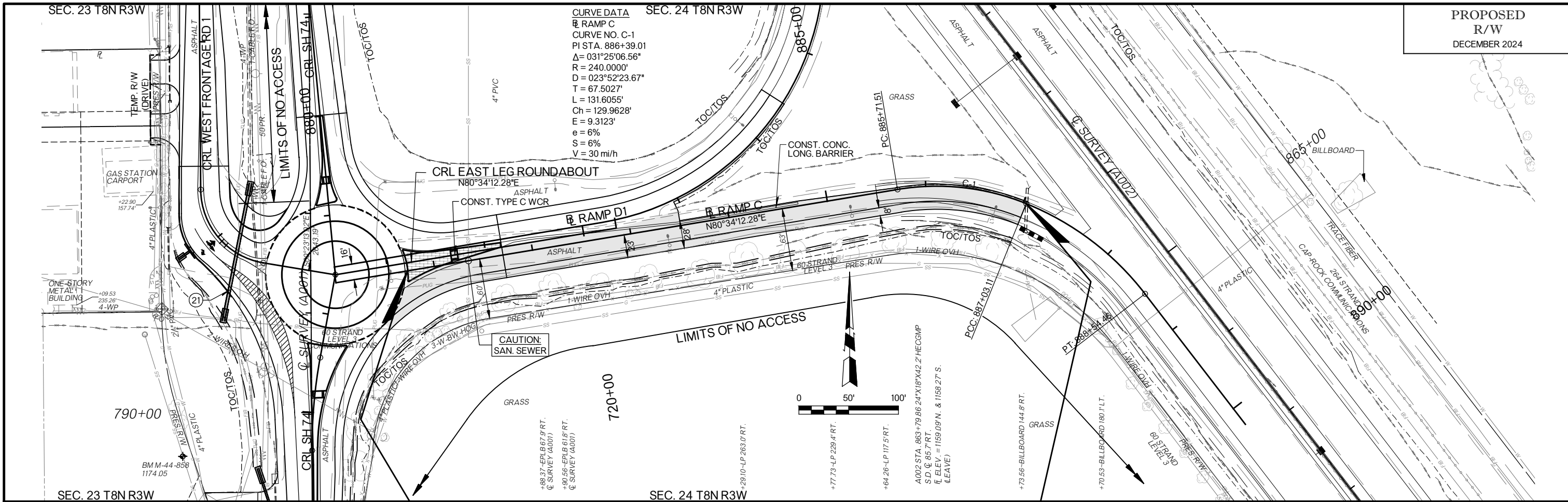


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

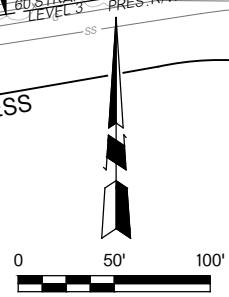
(17) STA. 54+81.1 RAMP A  
CONST. 18' x 392' LG. ROP  
SKEW 84.22° LT. FWD.  
195' LG. LT. & 197' LG. RT.  
FL LT=1173.55, FL RT=1174.73



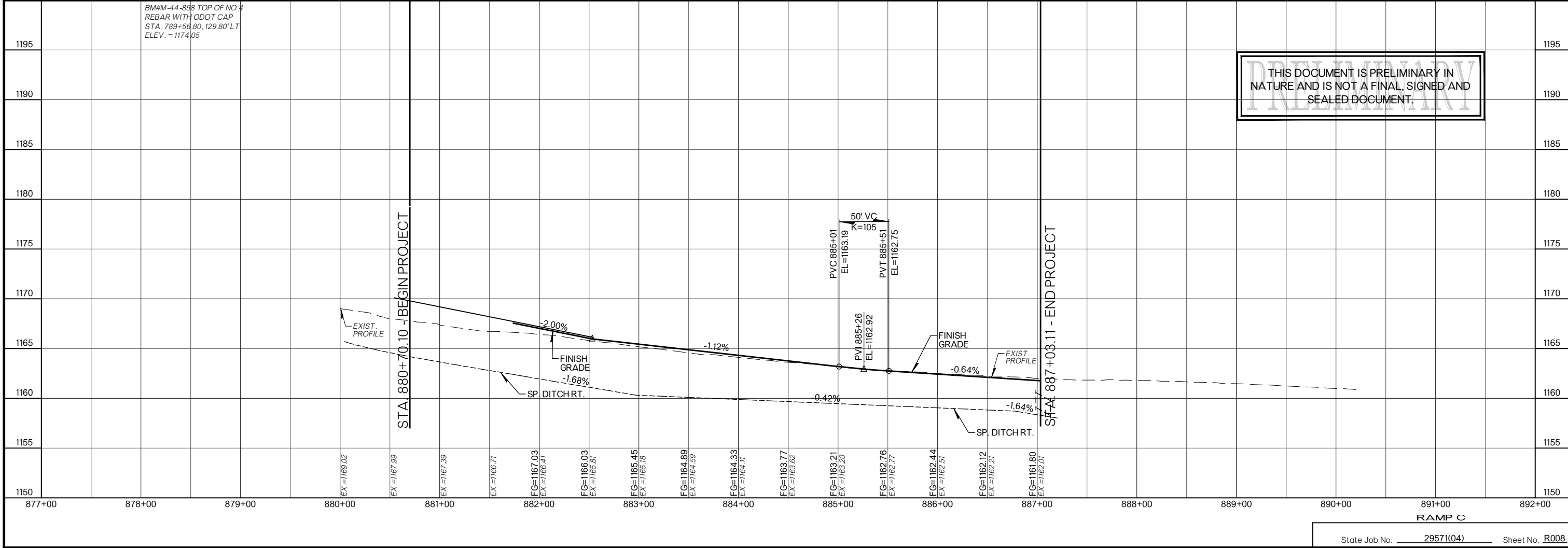
THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.



CURVE DATA  
 RAMP C  
 CURVE NO. C-1  
 PI STA. 886+39.01  
 $\Delta = 031^{\circ}25'06.56''$   
 $R = 240.000'$   
 $D = 023^{\circ}52'23.67''$   
 $T = 67.5027'$   
 $L = 131.6055'$   
 $Ch = 129.9628'$   
 $E = 9.3123'$   
 $e = 6\%$   
 $S = 6\%$   
 $V = 30 \text{ mi/h}$

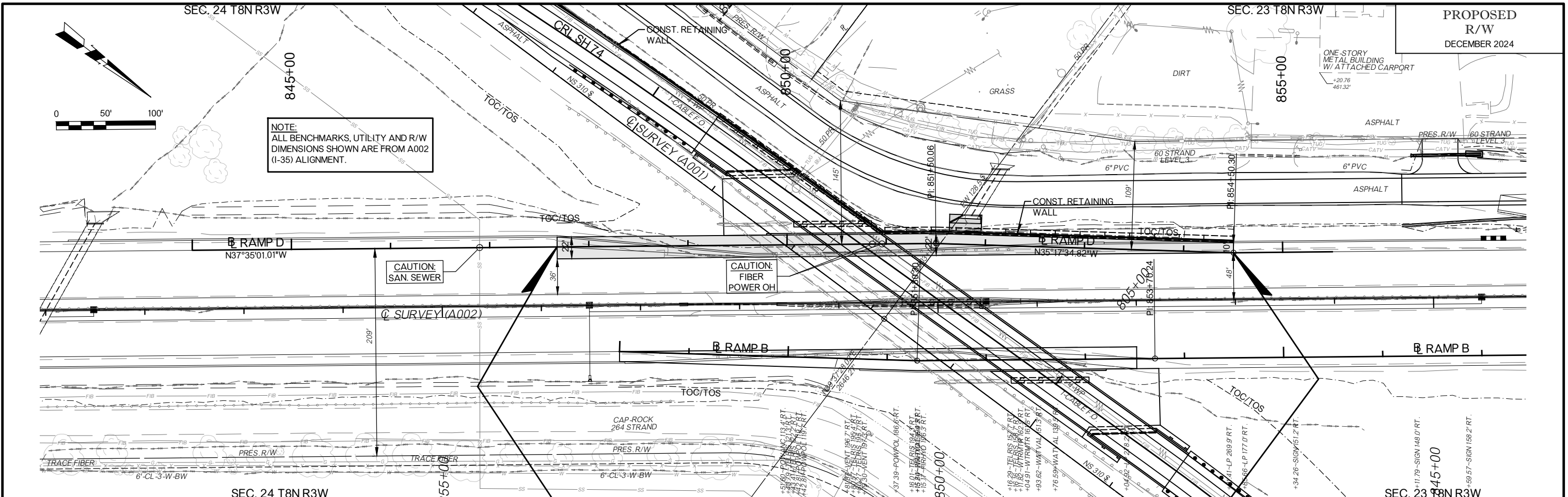


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

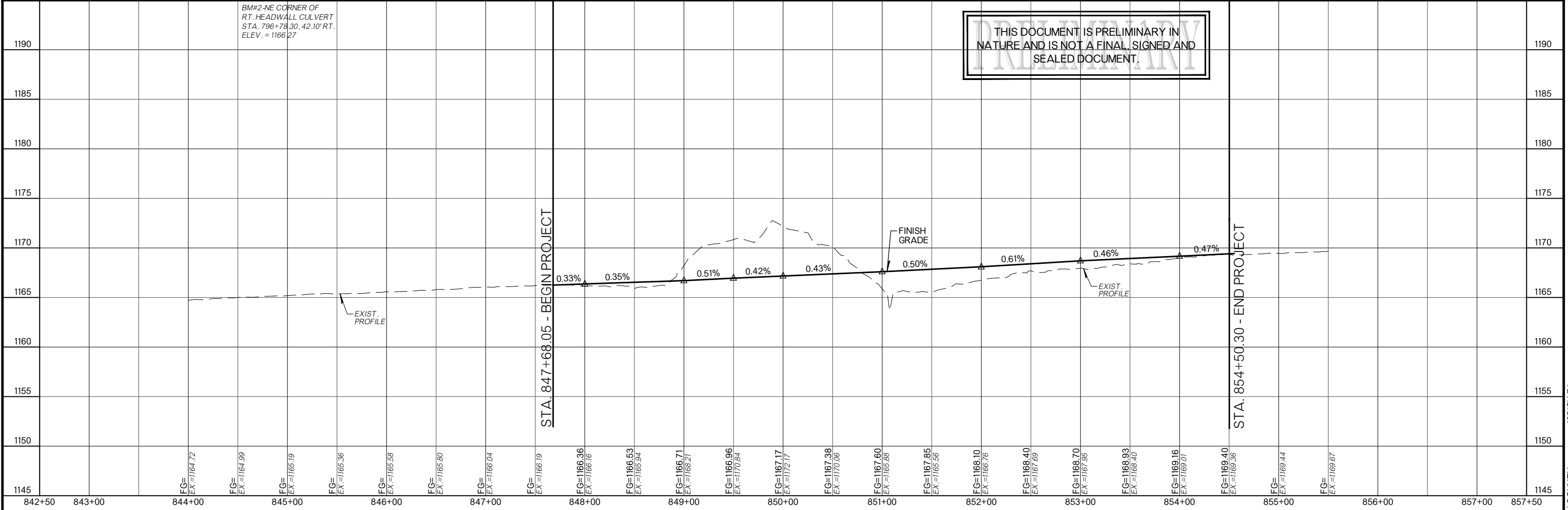


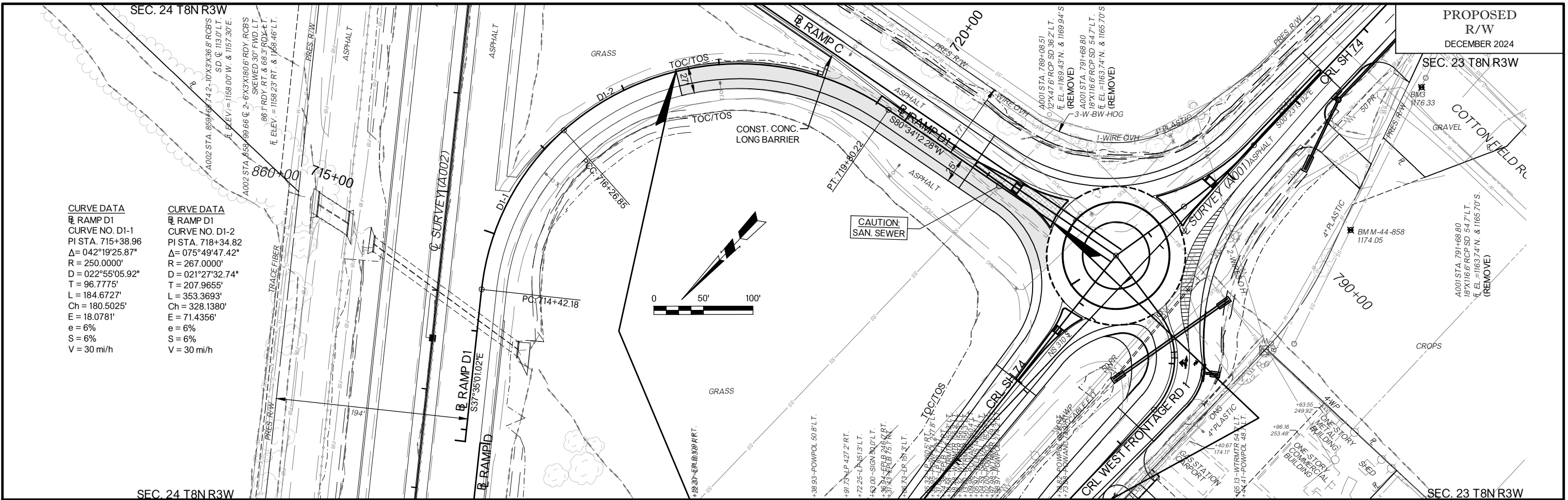
0 50' 100'

NOTE:  
ALL BENCHMARKS, UTILITY AND R/W  
DIMENSIONS SHOWN ARE FROM A002  
(I-35) ALIGNMENT.

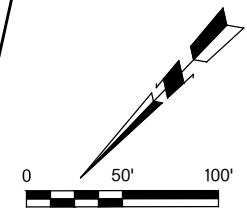


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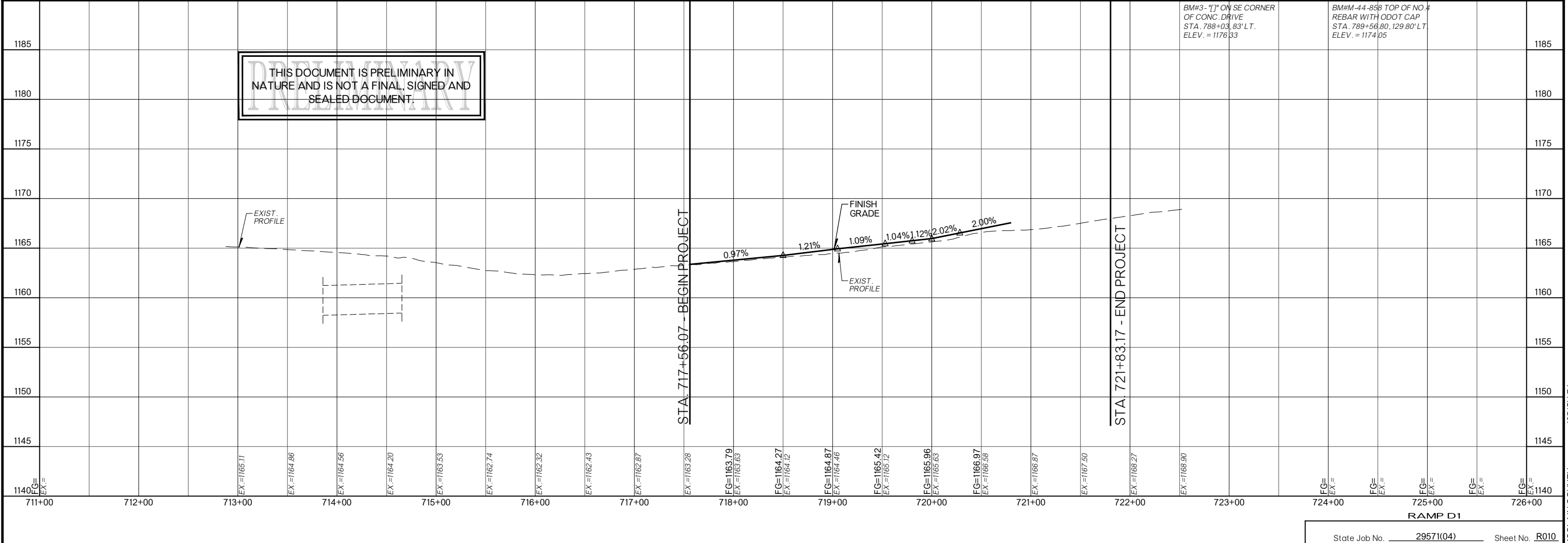
CURVE DATA	
RAMP D1	RAMP D1
CURVE NO. D1-1	CURVE NO. D1-2
PI STA. 715+38.96	PI STA. 718+34.82
$\Delta = 042^{\circ}19'25.87''$	$\Delta = 075^{\circ}49'47.42''$
R = 250.0000'	R = 267.0000'
D = 022^{\circ}55'05.92''	D = 021^{\circ}27'32.74''
T = 96.7775'	T = 207.9655'
L = 184.6727'	L = 353.3693'
Ch = 180.5025'	Ch = 328.1380'
E = 18.0781'	E = 71.4356'
e = 6%	e = 6%
S = 6%	S = 6%
V = 30 mi/h	V = 30 mi/h

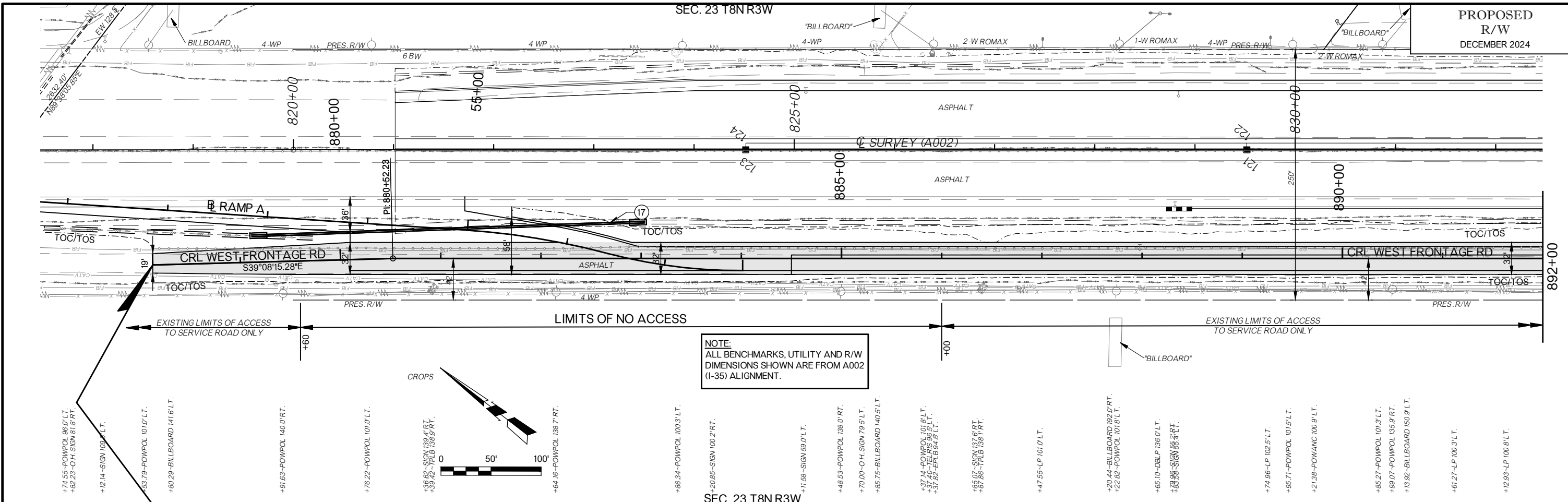


SEC. 24 T8N R3W

SEC. 23 T8N R3W

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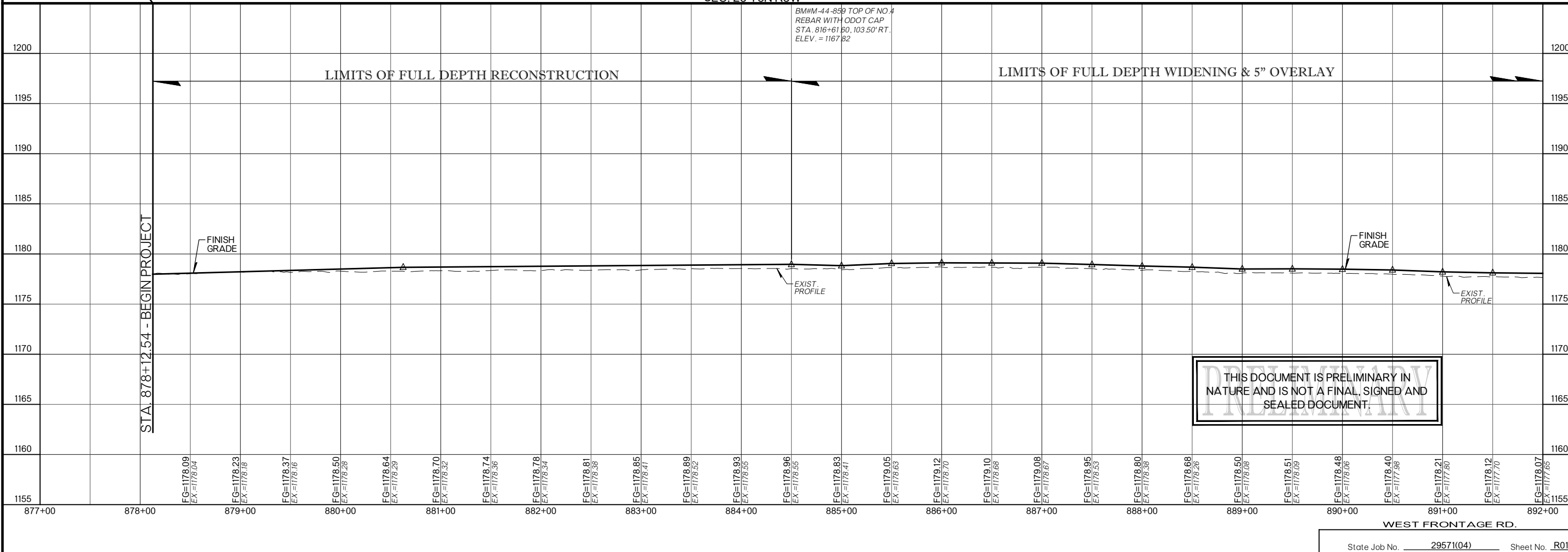




NOTE:  
ALL BENCHMARKS, UTILITY AND R/W  
DIMENSIONS SHOWN ARE FROM A002  
(I-35) ALIGNMENT.

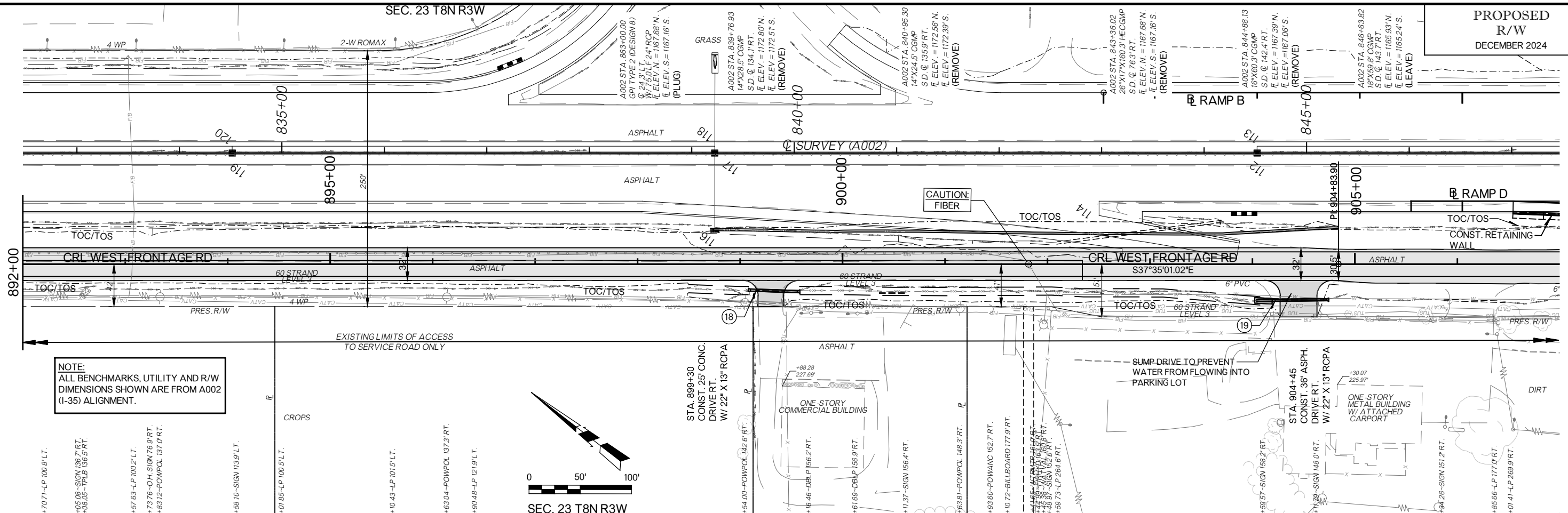


SEC. 23 T8N R3W

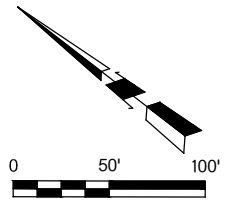


THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

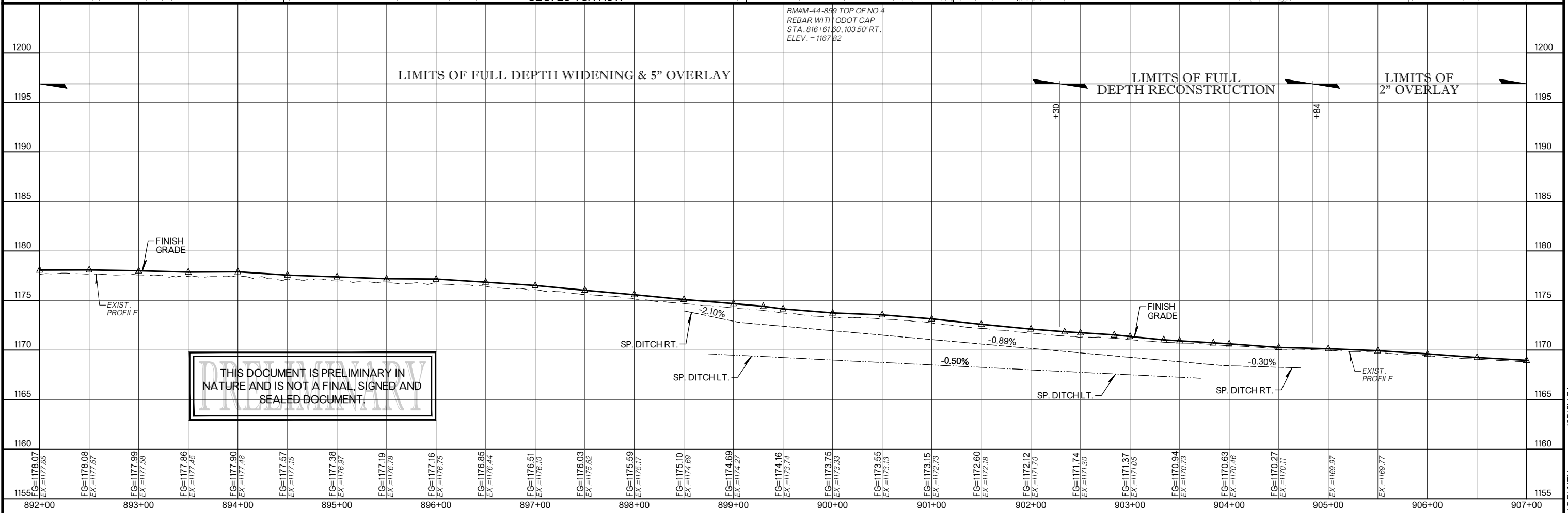
SEC. 23 T8N R3W



NOTE:  
ALL BENCHMARKS, UTILITY AND R/W  
DIMENSIONS SHOWN ARE FROM A002  
(I-35) ALIGNMENT.



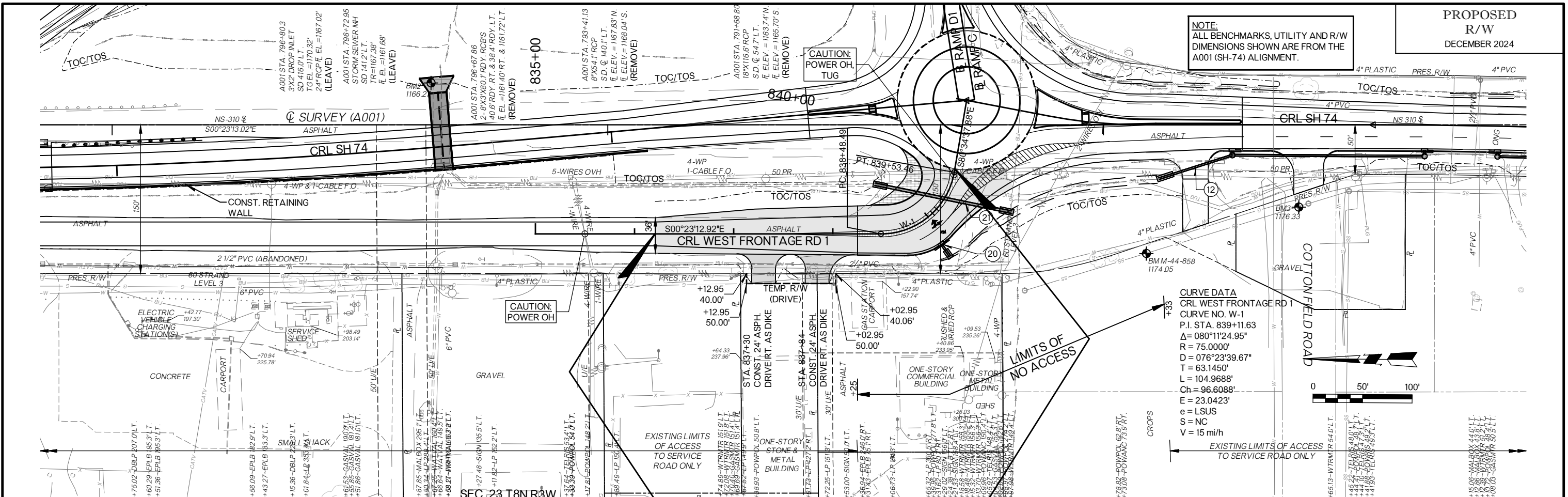
SEC. 23 T8N R3W



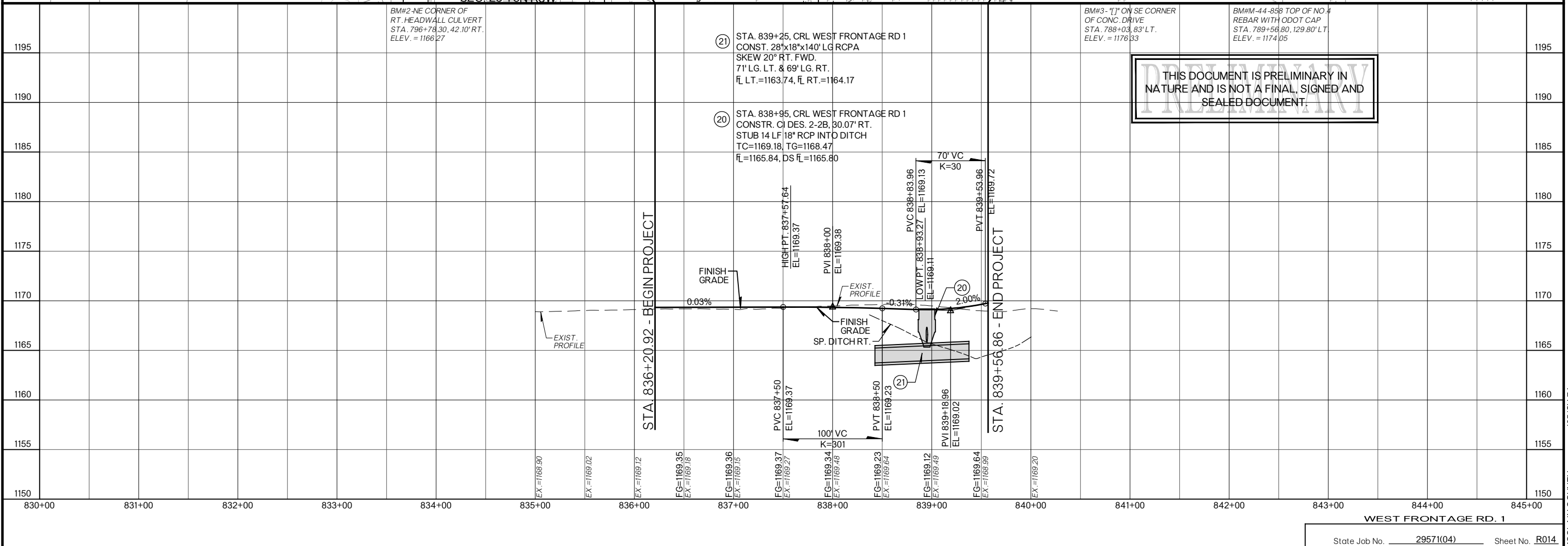
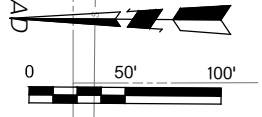
THIS DOCUMENT IS PRELIMINARY IN  
NATURE AND IS NOT A FINAL, SIGNED AND  
SEALED DOCUMENT.



NOTE:  
ALL BENCHMARKS, UTILITY AND R/W  
DIMENSIONS SHOWN ARE FROM THE  
A001 (SH-74) ALIGNMENT.



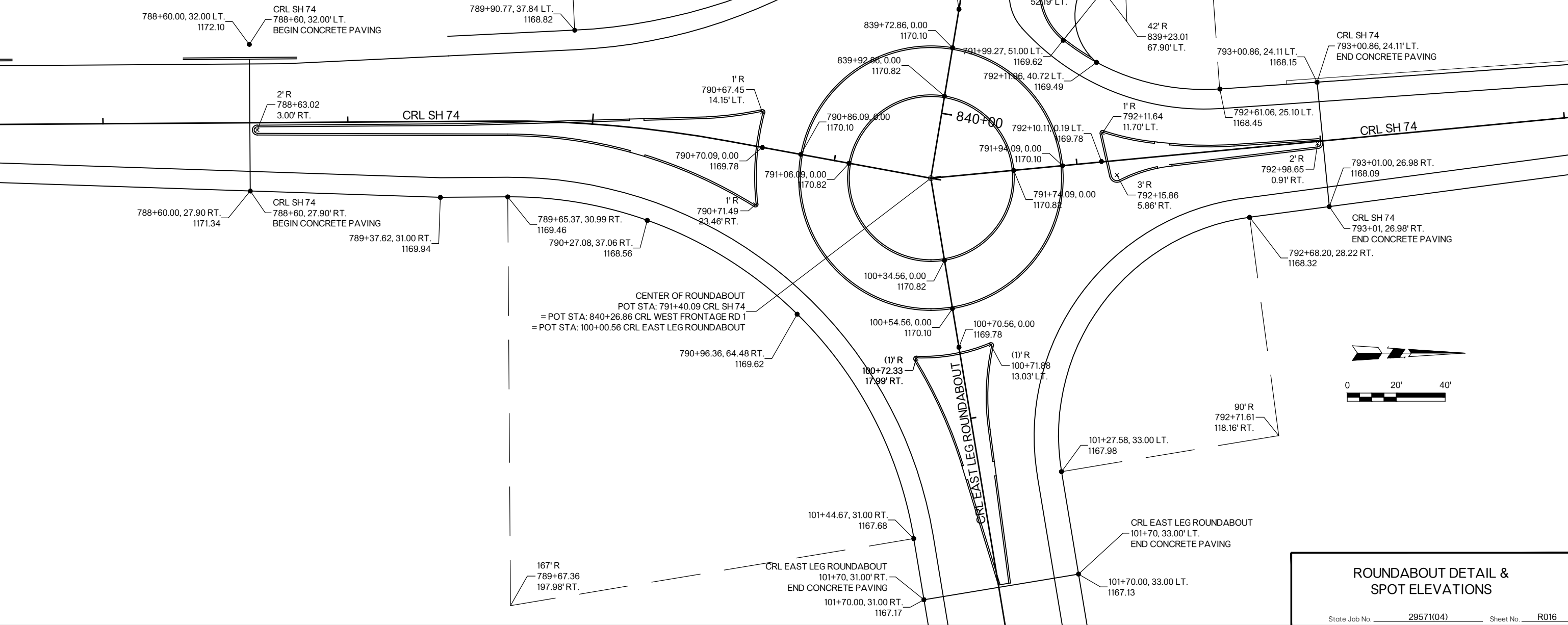
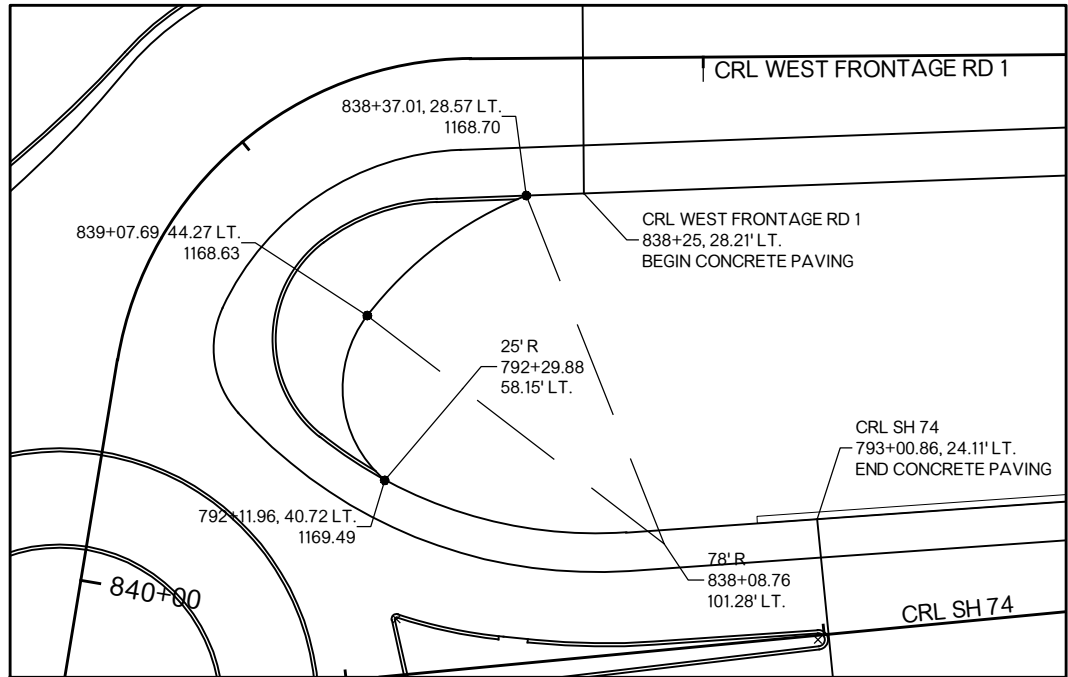
CURVE DATA  
CRL WEST FRONTAGE RD 1  
CURVE NO. W-1  
P.I. STA. 839+11.63  
 $\Delta = 080^{\circ}11'24.95''$   
R = 75.0000'  
D =  $076^{\circ}23'39.67''$   
T = 63.1450'  
L = 104.9688'  
Ch = 96.6088'  
E = 23.0423'  
e = LSUS  
S = NC  
V = 15 mi/h



THIS DOCUMENT IS PRELIMINARY IN  
NATURE AND IS NOT A FINAL, SIGNED AND  
SEALED DOCUMENT.

- (21) STA. 839+25, CRL WEST FRONTAGE RD 1  
CONST. 28"x18"x140" LG RCPA  
SKEW 20° RT. FWD.  
71' LG. LT. & 69' LG. RT.  
FL LT.=1163.74, FL RT.=1164.17
- (20) STA. 838+95, CRL WEST FRONTAGE RD 1  
CONST. CH DES. 2-2B, 30.07' RT.  
STUB 14 LF 18" RCP INTO DITCH  
TC=1169.18, TG=1168.47  
FL=1165.84, DS FL=1165.80





CENTER OF ROUNDABOUT  
POT STA: 791+40.09 CRL SH 74  
= POT STA: 840+26.86 CRL WEST FRONTAGE RD 1  
= POT STA: 100+00.56 CRL EAST LEG ROUNDABOUT

**ROUNDABOUT DETAIL & SPOT ELEVATIONS**

State Job No. 29571(04) Sheet No. R016

SURVEY CONTROL DATA

1. POSITIONAL CONTROL:

- A. POSITIONAL CONTROL FOR THIS SURVEY IS THE NGS OKLAHOMA STATE PLANE COORDINATE SYSTEM, NAD83 (2011), LAMBERT PROJECTION (SOUTH ZONE).
- B. ACCURACY - THE POSITIONAL CONTROLS FOR THIS SURVEY MEETS OR EXCEEDS THE FOLLOWING ACCURACY CRITERIA:
1. NETWORK ACCURACY: 0.10 FOOT
  2. LOCAL ACCURACY: 0.05 FOOT

2. BEARINGS:

THE BEARINGS SHOWN HEREIN OR HEREON ARE GRID BEARINGS DERIVED FROM THE NGS OKLAHOMA STATE PLANE COORDINATE SYSTEM AND ARE NOT ASTRONOMICAL. THE ANGLE OF VARIANCE BETWEEN GRID NORTH (GN) AND THE ASTRONOMICAL TRUE NORTH (TN) IS DEPICTED DIAGRAMMATICALLY.

3. VERTICAL CONTROLS:

- A. LEVEL DATUM IS NAVD 88 TAKEN FROM SW04892(1) ORIGINAL SURVEY.
- B. ACCURACY - VERTICAL CONTROL FOR THIS SURVEY MEETS OR EXCEEDS THE FOLLOWING ACCURACY CRITERIA:
1. NETWORK ACCURACY (FROM GPS OR LEVELING): 0.10 FOOT
  2. LOCAL ACCURACY (CONFIRMED BY LEVELING): 0.02 FOOT

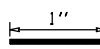
SURVEY BEGAN: January 25, 2021  
 SURVEY COMPLETED: May 27, 2021

SURVEY CREW MEMBERS:

Derrick Anderson, Land Surveyor Manager  
 Charles Pauley, Transportation Specialist Lv. VI  
 Kristy Henderson, Transportation Specialist Lv. IV  
 Jimmie Wallace, Transportation Specialist Lv. IV  
 Robert Barthelemy, Transportation Specialist Lv. IV  
 Corey Goshe, Transportation Specialist Lv. IV

EQUIPMENT:

LEICA GPS EQUIPMENT (4) GS 15  
 LEICA MS 60 MULTISTATION  
 LEICA CS 20 & CS 15 CONTROLLERS

SCALES   
 SURVEY DATA SHEETS 1" = 100'  
 GEOMETRIC DATA SHEETS 1" = 500'

# SURVEY DATA SHEETS

## McCLAIN COUNTY SH-74

SWO 4892(1)  
STATE JOB NO. 29571(04)

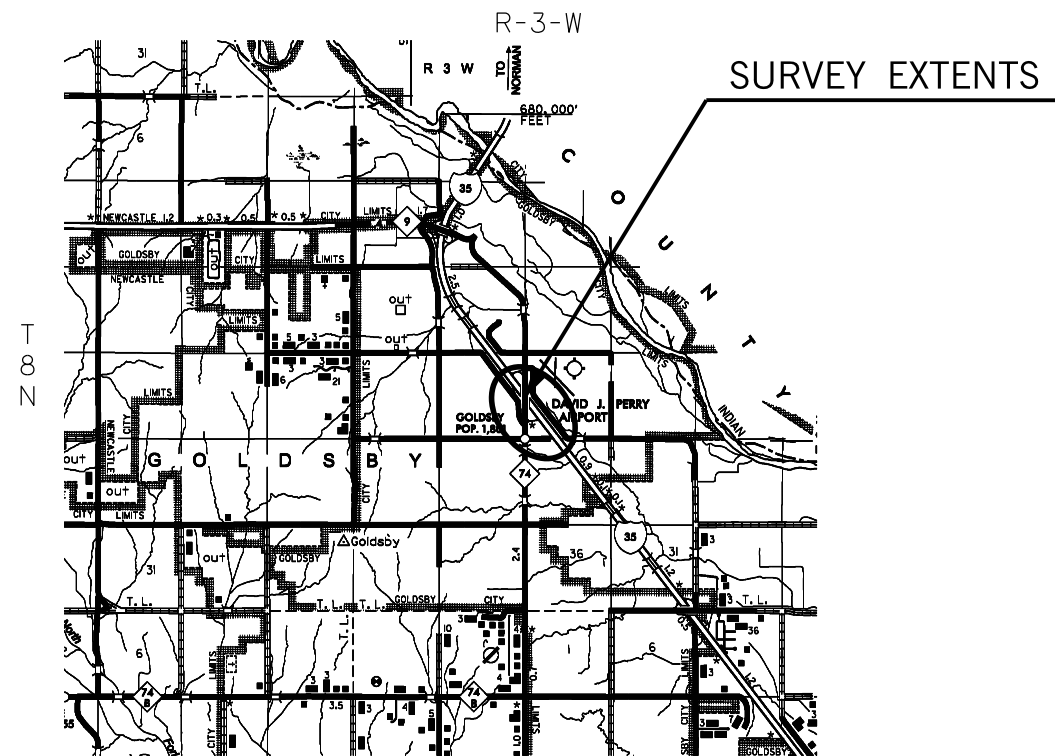
Project Location

INDEX OF SURVEY SHEETS

S001.	TITLE SHEET
S002-S006.	SURVEY REPORTS
S007-S011.	SURVEY DATA SHEETS
S012-S013.	LAND TIE DATA SHEETS

Utility Companies:

Windstream	- Oklahoma City, Ok. - 405-228-2530
Oklahoma Electric Cooperative	- Norman, Ok. - 800-778-9140
DCP Midstream	- Washington, Ok. - 405-288-2387
Goldsby Water Authority	- Goldsby, Ok. - 405-288-6697
ONG	- Oklahoma City, Ok. - 800-778-9140
Cap Rock Communications	- Dallas, Tx. - 214-720-0385
Level 3 Communications	- Tulsa, Ok. - 877-366-8344



PROJECT LENGTH 8,000.00 (I-35) 1.52 (I-35)  
 5,516.67 (SH 74) Ft. 1.04 (SH 74) MI.

BEGINNING STATION : 820+00.00 (I-35) & 771+00.00 (SH 74)  
 ENDING STATION : 900+00.00 (I-35) & 826+16.67 (SH 74)

STATE OF OKLAHOMA  
DEPARTMENT OF TRANSPORTATION

SWO 4892(1) Job/Piece 29571 (04) Engr.Contract No. \_\_\_\_\_

LAND SURVEYOR'S CERTIFICATION

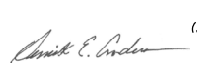
I hereby certify that all land and property sub-division distances, angles, corners and monumentation made or used in conjunction with this survey and depicted or recorded herein or hereon were recovered, established or re-established in substantial conformity with:

- applicable instructions contained in the U.S. Government Bureau of Land Management publication "Manual of Surveying Instructions";
- its supplement, "Restoration of Lost or Obliterated Corners and Subdivision of Sections";
- "Oklahoma Minimum Standards for the Practice of Land Surveying" as adopted by the State Board of Registration for Professional Engineers and Land Surveyors; and
- sound land surveying practices;

including a thorough search, study, analysis and consideration of all existing records and field evidence.

I further certify that all survey monuments depicted exist and that all land survey work was done by me or under my direct supervision and that it is true, accurate and correct to the best of my knowledge and belief.

Dated this 27th day of May, 2021.

Land Surveyor  (seal)  
 Signature  
 Derrick E. Anderson  
 Printed Name

Oklahoma Registered Land Surveyor No. 1551  
 Certificate of Authorization No. \_\_\_\_\_ Exp. Date \_\_\_\_\_



THIS SURVEY MEETS THE OKLAHOMA MINIMUM STANDARDS FOR THE PRACTICE OF LAND SURVEYING AS ADOPTED BY THE OKLAHOMA STATE BOARD OF REGISTRATION FOR PROFESSIONAL ENGINEERS AND LAND SURVEYORS, JULY 25, 2013.

"CALL BEFORE YOU DIG"  
THE NEW NATIONAL LOCATE NUMBER  
\*\*811\*\*



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SPECIFICATIONS FOR SURVEYS FOR PRIMARY AND SECONDARY HIGHWAYS DATED MONTH, 20XX GOVERN.

PLS	DEA		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	DEA		
CHECKED	DEA		
APPROVED	DEA		
CREW	Tecumseh		
COUNTY McClain			OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
HIGHWAY I-35 SH 74			SURVEY DATA SHEET
STATE JOB NO. 29571(04)			
SHEET NO. S001			SWO 4892(1)

Date: May 27, 2021

To: Mr. Kyle K. King, Chief of Surveys  
From: Derrick E. Anderson, Professional Land Surveyor  
Subject: SWO 4892(1) - J/P 29571(04) - SH-74 - McClain County  
SH-74: At I-35, 2.76 miles south of the Cleveland County Line.  
(Interchange Project)

1. General:

Method of Survey - Combination Lidar & Field Conventional  
Units of Measurement - U.S. Survey Foot

Survey Began: January 25, 2021  
Survey Completed: May 27, 2021

Survey Personnel:

Derrick E. Anderson, PLS, Land Surveyor Manager  
Charles Pauley, Transportation Specialist VI  
Jimmie Wallace, Transportation Specialist V  
Kristy Henderson, Transportation Specialist IV  
Robert Barthelemy, Transportation Specialist IV  
Cory Goshe, Transportation Specialist IV

2. Survey Assignment:

This Survey was assigned to the Tecumseh-based survey crew under the direction of Charles Pauley, verbally on January 25, 2021 from Mr. Derrick Anderson, Land Surveyor Manager.

3. Purpose of Survey:

The purpose of this survey is to develop plans to re-construct the interchange at I-35 & SH-74 (Goldsby Exit).

4. Limits of Survey

Survey on SH-74 begins 0.60 mile south of the intersection of SH-74 and I-35 and extends north along SH-74 for 1.04 miles.

Survey on I-35 begins 0.58 mile northwest of the intersection of SH-74 and I-35 and extends southeasterly along I-35 for 1.52 miles.

5. Alignment:

Centerline for I-35 survey was taken from is the exact as SWO 4892(1) J/P 23283(04) survey. The alignment was extended southerly to the limits of this survey.

Centerline for SH-74 was taken from and is the exact as the original SWO 4892(1) J/P 23283(04) survey.

6. Stationing:

Stationing for both I-35 and SH-74 surveys were taken from the original SWO 4892(1) J/P No. 23283(04) survey on I-35 and SH 74.

7. Horizontal Control:

Horizontal Control for this survey is Oklahoma State Plane Coordinate System, NAD 83 (2011), Lambert Projection, South Zone, derived by double occupation on M-44-858 and M-44-859 using Static GPS and processed using OPUS Projects.

8. Vertical Control:

Level Datum for this survey was taken from the original SWO 4892(1) J/P No. 23283(04) survey.

Bench marks were placed in the Bench Mark and Check Level List.

9. Topography:

Topography was obtained using a combination of Aerial and Field methods. ODOT Aerial Survey Branch provided the topography design file to Heartland Surveying which was completed under CIN-2327A Engineering Contract.

10. DTM/Surface Features:

Lidar data was obtained using Aerial methods. Obscure areas were obtained by Heartland Surveying using conventional methods and sent to ODOT Aerial Survey Branch to be merged into the surface. The OSSDA Check Points were obtained by the Tecumseh Survey Crew. The Horizontal Check Points were obtained by Heartland Surveying under CIN-2327A Engineering Contract.

11. Land & Property Ties:

No Land Ties needed this survey. Land ties shown on this survey was taken from the original SWO 4892(1) J/P No. 23283(04) survey.

12. Utilities:

Utilities shown on the survey was taken from the original SWO 4892(1) J/P No. 23283(04) survey. A check was made to see if any new utilities have been added since the last updated survey was completed and none had been.

13. Existing Right of Way:

Right of Way shown was taken from the original SWO 4892(1) J/P No. 23283(04) survey.

14. Environmental Concerns:

None encountered this survey.

15. Drainage Information:

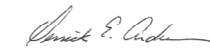
No Drainage information was obtained on this survey.

17. Submitted Survey Data:

In addition to the computer files submitted. The following hard copies were submitted to the Central Survey Office:

- a. Historical Letter and Written Report
- b. Form SD #7 - Public & Private Owned Utility List
- c. Form SD #9 - Final Cost of Survey
- d. Form SD #11 - Position and Description of Survey Mon. (2)
- e. Form SD #20 - Survey Control Data Statement
- f. Form SD #41 - Surveyors Certificate
- g. Bench Mark and Check Level List
- h. Cogo Point List
- i. Alignment Table

Respectfully,



Derrick E. Anderson, PLS  
Land Surveyor Manager

PLS	DEA		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	DEA		
CHECKED	DEA		
APPROVED	DEA		
CREW	Tecumseh		
COUNTY <b>McClain</b> HIGHWAY <b>I-35&amp;SH74</b> STATE JOB NO. <b>29571(04)</b> SHEET NO. <b>S002</b>			

**SURVEY DATA SHEET**  
SWO 4892(1)

PT NO.	X-COORD.	Y-COORD.	ELEV.
BM1	2124777.78565	665218.05219	1170.72
BM2	2124940.96553	662097.38295	1166.27
BM3	2124821.77911	661221.25814	1176.33
M-44-858	2124773.94160	661374.73860	1174.05
M-44-859	2124988.97131	664081.05236	1167.82
300	2124032.16050	663829.05650	
301	2124213.95360	663592.85360	
302	2124341.24970	663427.45840	
303	2124451.90650	663283.68280	
304	2124873.93390	662735.34470	
305	2124894.73790	662708.31420	
306	2125336.28840	662134.60980	
307	2125448.40180	661988.94150	
308	2125898.02340	661404.75040	
309	2124895.92640	662532.32670	
310	2124898.38160	662168.79650	
311	2124898.66160	662127.32710	
312	2124900.03320	661924.24870	
313	2124900.96810	661785.81490	
314	2124901.48990	661708.54670	
315	2124902.75100	661521.82090	
316	2124904.34910	661285.20190	
317	2124905.28260	661146.97980	
318	2124894.55570	662735.45590	
319	2123018.59780	665145.97332	
320	2126983.06855	659994.95679	
321	2123506.53267	664512.00205	
322	2126434.14185	660708.17444	
323	2126897.16154	660106.57533	
324	2124894.22248	659518.93553	
325	2124879.02427	665035.40346	
326	2124890.76841	660092.70524	
327	2124905.71594	661082.81226	
328	2127897.94643	658806.26067	
400	2123111.28822	665091.12365	
401	2124159.76266	663728.84619	
402	2124550.16530	663856.72860	
403	2124748.65650	663857.69870	
404	2124887.06690	663955.82250	
405	2124886.03386	664060.23739	
406	2124795.95483	663900.30576	
407	2124753.04600	663851.97360	
408	2124068.55550	663847.35110	
409	2124788.56428	662911.84739	
410	2124798.87907	663816.12072	
411	2124804.61547	663906.69081	
412	2124886.67920	663995.00500	
413	2124863.58100	663918.71060	
414	2124881.34930	663885.22246	
415	2124886.77890	663738.41360	
416	2124888.68960	663617.83874	
417	2124796.36585	663818.20977	
418	2125003.32280	663867.44110	
419	2124894.60250	663867.44110	
420	2124877.55741	663891.35891	
421	2125003.84298	663803.78323	
422	2125003.37180	663861.44110	
423	2124897.03080	663861.44110	
424	2124880.67812	663832.43283	
425	2124887.51730	663691.81670	
426	2124860.73620	663783.76560	
427	2124883.50975	663838.22573	
7300	2347279.32100	536244.07100	
7301	1894926.55200	634442.06000	
7302	2297554.90500	703043.37200	
7303	2126460.72500	663626.03200	
7400	2124986.38500	664078.38400	
7401	2124873.56800	661335.40300	
7600	2123810.44110	663871.20090	
7601	2124095.08400	663501.36580	
7602	2124222.38010	663335.97060	
7603	2124272.07020	663271.40860	
7604	2124294.64150	663225.68620	
7605	2124599.57560	662829.48690	
7606	2124652.78480	662734.17710	
7607	2124745.68360	662567.77440	
7608	2124745.92830	662531.53520	
7609	2124748.67080	662125.45600	
7610	2124750.04240	661922.37750	
7611	2124750.97730	661783.94380	
7612	2124751.49920	661706.67550	
7613	2124752.76060	661519.89830	
7614	2124770.83630	661412.78810	
7615	2124814.40410	661284.59440	
7616	2124855.16200	661164.66890	
7617	2124855.41030	661114.06930	
7618	2124955.28360	661146.99990	
7619	2124954.11480	661320.06230	
7620	2124964.75480	661375.14410	
7621	2125082.00620	661513.42670	
7622	2125482.11810	661576.80030	
7623	2125708.90600	661486.51350	
7624	2125818.77700	661343.75860	
7625	2126016.89310	661496.23820	

PT NO.	X-COORD.	Y-COORD.
7626	2125567.27140	662080.42930
7627	2125097.75610	662690.46810
7628	2125043.80270	662847.29110
7629	2125035.88420	664019.76430
7630	2124334.45000	663863.34440
7631	2124069.61000	663944.35490
7632	2124759.50150	664004.74220
7633	2124820.40630	664089.79310
7634	2124818.31530	664399.40350
7635	2124942.84900	664468.87140
7636	2124936.70160	665379.10540
7637	2124811.70430	665378.28600
7638	2122899.72819	665054.48553
7639	2123097.84421	665206.96517
7640	2126903.82215	659933.96494
7641	2126759.15558	660121.92946
7642	2126534.06233	660120.46750
7643	2126534.30145	660087.46391
7644	2126784.29642	660089.26409
7645	2127062.31496	660055.94865
7646	2126842.72279	660423.24179
7647	2126899.15101	660308.93581
7648	2127007.91779	660167.61569
7649	2127048.46326	660147.84121
7650	2127138.04313	660124.80727
7651	2127285.04351	660125.85977
7652	2127285.28037	660092.86062
7653	2127035.28949	660091.06271
7654	2123725.06524	664392.02007
7655	2124944.22157	659519.23653
7656	2124941.13167	660032.51332
7657	2124941.03642	660076.03383
7658	2125441.02354	660079.62269
7659	2125441.17364	660112.62462
7660	2124941.18652	660109.03575
7661	2124941.67588	660152.43760
7662	2124955.37360	661082.60512
7663	2124955.29209	661145.74334
7664	2124844.22338	659518.63454
7665	2124841.13348	660031.91133
7666	2124841.03552	660075.54288
7667	2124341.03952	660073.54370
7668	2124341.18421	660106.54454
7669	2124841.18021	660108.54372
7670	2124841.68728	660153.94711
7671	2124854.77676	661020.97930
7672	2124855.29437	661145.06803
7673	2124954.76536	661019.46979
7674	2127208.78146	659537.73287
7675	2127242.63839	659469.14928
7676	2127806.81306	658736.12003
7677	2127977.19283	658867.25252
7678	2126888.55285	660330.40438
8000	2123440.60610	663586.55800
8001	2123725.24900	663216.72290
8002	2123929.69120	663374.07150
8003	2124056.98730	663208.67630
8004	2124484.93010	662733.29210
8005	2124240.32350	662732.00140
8006	2123657.64710	662728.92680
8007	2124241.69540	662528.87450
8008	2124486.30200	662530.16520
8009	2123672.09770	661693.20360
8010	2124089.06530	661698.40530
8011	2124086.05590	661914.09430
8012	2124002.61230	661913.05330
8013	2124000.12790	662091.11600
8014	2124214.61120	662093.79170
8015	2124214.26240	662118.78920
8016	2124217.09560	661915.72900
8017	2124528.77960	661919.61730
8018	2124529.71450	661781.18350
8019	2124530.23640	661703.91530
8020	2124571.60310	661704.43130
8021	2124368.85890	661701.90210
8022	2124370.12000	661515.17640
8023	2124572.86420	661517.70560
8024	2124756.59550	661284.20400
8025	2124718.67760	661204.82750
8026	2124719.29440	661113.15010
8027	2126220.57440	662083.46370
8028	2126216.26080	662744.11700
8029	2123669.08840	661908.89260
8030	2127549.12232	661427.92810
8031	2127226.78168	660108.94115
8032	2127222.58441	660769.36350
8033	2126891.90914	660767.04148
8034	2127553.25969	660771.68553
9000	2119611.25800	665345.82190
9001	2122244.35370	665361.92880
9002	2124876.70300	665378.70020
9003	2127524.11800	665393.94210
9004	2130172.30150	665409.63350
9005	2130187.34090	662770.14620

PT NO.	X-COORD.	Y-COORD.
9006	2127540.76950	662752.79870
9007	2124894.55460	662735.45360
9008	2122259.88110	662721.54330
9009	2119622.74200	662707.63600
9010	2119637.21140	660072.05630
9011	2122272.38200	660081.77250
9012	2124912.29180	660092.32790
9013	2127557.42310	660111.31450
9014	2130202.47840	660130.20620

CHECK LEVELS						BENCHMARK LIST		NAVD 88 DATUM SHEET 1 OF 1	
BM NO.	RUN 1	RUN 2	MEAN DIFF.	ADJ. DIFF.	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION		
BM 1						1170.720	NE corner bottom conc. step 100' Lt. Sta. 829+58 = BM 1, NE corner bottom conc. step 100' Lt. Sta. 829+58 swo4892(1) SH 74(Lemke Surveying) Taken from SAP 1013 (4) Plans - Source Benchmark		
TO	-2.886	-2.896	-2.891	-2.900					
M-44-859					1167.820		Top of No. 4 Rebar with ODOT cap PLS 1663 - 103.3' Rt. Sta. 816+16.6 [Set this Survey]		
TO	-1.537	-1.541	-1.549	-1.550					
BM 2					1166.270	1166.270	NE corner of Rt. Headwall culvert 42.1' Rt. Sta. 796+78.3 = BM 2, NE corner of Rt. Headwall culvert Sta. 796+68 (Lemke Surveying) Taken from SAP 1013 (4) Plans - Source Benchmark		
TO	7.782	7.782	7.782	7.780					
M-44-858					1174.050		Top of No. 4 Rebar with ODOT cap PLS 1663 - 119.8' Lt. Sta. 789+56.8 [Set this Survey]		
TO	2.280	2.280	2.283	2.280					
BM 3					1176.330	1176.330	"I" on SE corner of conc. Drive, this survey = BM 104 "I" on SE corner of conc. drive Previous swo 4892(1), done by consultant (Lemke Surveying) 83' Lt. Sta. 788+03		

PLS	DEA		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	DEA		
CHECKED	DEA		
APPROVED	DEA		
CREW	Tecumseh		
COUNTY			McClain
HIGHWAY			1-358SH74
STATE JOB NO.			295711041
SHEET NO.			S003

SURVEY DATA SHEET

SWO 4892111

Project Name: SW04892 I V1 ODOT  
 Description: I-35 & SH 74 Interchange at Goldsby  
 Horizontal Alignment Name: A001  
 Description: SH 74  
 Style: Centerline

	STATION	EASTING	NORTHING
Element: Linear			
POB ( 324)	771+00.00	2124894.2225	659518.9355
PC ( 1)	776+13.29	2124891.1326	660032.2123
Tangent Direction:		N 0°20'41.69" W	
Tangent Length:		513.286'	
Element: Circular			
PC ( 1)	776+13.29	2124891.1326	660032.2123
PI ( )	776+73.78	2124890.7684	660092.7052
CC ( 2)	2130620.6067	660066.7035	
PT ( 3)	777+34.27	2124891.6816	660153.1924
Radius:		5729.578'	
Delta:		1°12'35.41" Right	
Degree of Curvature(Arc):		1°00'00.00"	
Length:		120.984'	
Tangent:		60.494'	
Chord:		120.981'	
Middle Ordinate:		0.319'	
External:		0.319'	
Tangent Direction:		N 0°20'41.69" W	
Radial Direction:		N 89°39'18.31" E	
Chord Direction:		N 0°15'36.01" E	
Radial Direction:		S 89°08'06.28" E	
Tangent Direction:		N 0°51'53.72" E	
Element: Linear			
PT ( 3)	777+34.27	2124891.6816	660153.1924
PC ( 4)	786+01.40	2124904.7711	661020.2245
Tangent Direction:		N 0°51'53.72" E	
Tangent Length:		867.131'	
Element: Circular			
PC ( 4)	786+01.40	2124904.7711	661020.2245
PI ( )	786+64.00	2124905.7159	661082.8123
CC ( 5)	2119175.8459	661106.7134	
PT ( 6)	787+26.59	2124905.2932	661145.4057
Radius:		5729.578'	
Delta:		1°15'06.65" Left	
Degree of Curvature(Arc):		1°00'00.00"	
Length:		125.185'	
Tangent:		62.595'	
Chord:		125.182'	
Middle Ordinate:		0.342'	
External:		0.342'	
Tangent Direction:		N 0°51'53.72" E	
Radial Direction:		S 89°08'06.28" E	
Chord Direction:		N 0°14'20.39" E	
Radial Direction:		N 89°36'47.07" W	
Tangent Direction:		N 0°23'12.93" W	
Element: Linear			
PT ( 6)	787+26.59	2124905.2932	661145.4057
PI ( 317)	787+28.16	2124905.2826	661146.9798
Tangent Direction:		N 0°23'12.93" W	
Tangent Length:		1.574'	
Element: Linear			
PI ( 317)	787+28.16	2124905.2826	661146.9798
PI ( 318)	803+16.67	2124894.5557	662735.4559
Tangent Direction:		N 0°23'12.87" W	
Tangent Length:		1588.512'	
Element: Linear			
PI ( 318)	803+16.67	2124894.5557	662735.4559
POE ( 325)	826+16.67	2124879.0243	665035.4035
Tangent Direction:		N 0°23'12.87" W	
Tangent Length:		2300.000'	

Project Name: SW04892 I V1  
 Description: I-35 & SH 74 Interchange at Goldsby  
 Horizontal Alignment Name: A002  
 Description: I-35  
 Style: Centerline

	STATION	EASTING	NORTHING
Element: Linear			
POB ( 319)	820+00.00	2123018.5978	665145.9733
POE ( 328)	900+00.00	2127897.9464	658806.2607
Tangent Direction:		S 37°35'01.02" E	
Tangent Length:		8000.000'	

Project Name: SW04892 I V1  
 Description: I-35 & SH 74 Interchange at Goldsby  
 Horizontal Alignment Name: A003  
 Description: Ramp A  
 Style: Centerline

	STATION	EASTING	NORTHING
Element: Linear			
POB ( 400)	821+00.00	2123111.2882	665091.1237
PC ( 7)	836+30.16	2124044.5591	663878.5296
Tangent Direction:		S 37°35'01.02" E	
Tangent Length:		1530.156'	
Element: Circular			
PC ( 7)	836+30.16	2124044.5591	663878.5296
PI ( )	838+19.04	2124159.7627	663728.8462
CC ( 8)	2124256.1470	664041.3779	
PT ( 9)	839+58.94	2124339.2615	663787.6438
Radius:		267.000'	
Delta:		70°33'12.21" Left	
Degree of Curvature(Arc):		21°27'32.74"	
Length:		328.781'	
Tangent:		188.884'	
Chord:		308.399'	
Middle Ordinate:		49.029'	
External:		60.057'	
Tangent Direction:		S 37°35'01.02" E	
Radial Direction:		S 52°24'58.98" W	
Chord Direction:		S 72°51'37.12" E	
Radial Direction:		S 18°08'13.23" E	
Tangent Direction:		N 71°51'46.77" E	
Element: Linear			
PT ( 9)	839+58.94	2124339.2615	663787.6438
PC ( 10)	841+38.92	2124510.3023	663843.6709
Tangent Direction:		N 71°51'46.77" E	
Tangent Length:		179.983'	
Element: Circular			
PC ( 10)	841+38.92	2124510.3023	663843.6709
PI ( )	841+80.87	2124550.1653	663856.7286
CC ( 11)	2124593.4168	663589.9368	
PT ( 12)	842+22.13	2124592.1119	663856.9336
Radius:		267.000'	
Delta:		17°51'25.14" Right	
Degree of Curvature(Arc):		21°27'32.74"	
Length:		83.214'	
Tangent:		41.947'	
Chord:		82.878'	
Middle Ordinate:		3.235'	
External:		3.275'	
Tangent Direction:		N 71°51'46.77" E	
Radial Direction:		S 18°08'13.23" E	
Chord Direction:		N 80°47'29.34" E	
Radial Direction:		S 0°16'48.08" E	
Tangent Direction:		N 89°43'11.92" E	
Element: Linear			
PT ( 12)	842+22.13	2124592.1119	663856.9336
PC ( 13)	843+15.02	2124684.9980	663857.3876
Tangent Direction:		N 89°43'11.92" E	
Tangent Length:		92.887'	
Element: Circular			
PC ( 13)	843+15.02	2124684.9980	663857.3876
PI ( )	843+78.68	2124748.6565	663857.6987
CC ( 14)	2124684.1819	664024.3856	
PT ( 406)	844+36.66	2124795.9548	663900.3058
Radius:		167.000'	
Delta:		41°43'58.72" Left	
Degree of Curvature(Arc):		34°18'31.86"	
Length:		121.639'	
Tangent:		63.659'	
Chord:		118.968'	
Middle Ordinate:		10.953'	
External:		11.722'	
Tangent Direction:		N 89°43'11.92" E	
Radial Direction:		S 0°16'48.08" E	
Chord Direction:		N 68°51'12.56" E	
Radial Direction:		S 42°00'46.80" E	
Tangent Direction:		N 47°59'13.20" E	

Project Name: SW04892 I V1  
 Description: I-35 & SH 74 Interchange at Goldsby  
 Horizontal Alignment Name: A004  
 Description: Ramp B  
 Style: Centerline

	STATION	EASTING	NORTHING
Element: Circular			
PC ( 410)	834+14.12	2124798.8791	663816.1207
PI ( )	834+72.31	2124753.0460	663851.9736
CC ( 15)	2124695.9849	663684.5844	
PT ( 16)	835+26.11	2124694.8571	663851.5806
Radius:		167.000'	
Delta:		38°25'16.30" Left	
Degree of Curvature(Arc):		34°18'31.86"	
Length:		111.986'	
Tangent:		58.190'	
Chord:		109.900'	
Middle Ordinate:		9.299'	
External:		9.848'	
Tangent Direction:		N 51°57'56.62" W	
Radial Direction:		N 38°02'03.38" E	
Chord Direction:		N 71°10'34.77" W	
Radial Direction:		N 0°23'12.93" W	
Tangent Direction:		S 89°36'47.07" W	
Element: Linear			
PT ( 16)	835+26.11	2124694.8571	663851.5806
PC ( 17)	836+14.59	2124606.3722	663850.9831
Tangent Direction:		S 89°36'47.07" W	
Tangent Length:		88.487'	
Element: Circular			
PC ( 17)	836+14.59	2124606.3722	663850.9831
PI ( )	841+52.42	2124068.5555	663847.3511
CC ( 18)	2124608.1753	663583.9892	
PT ( 19)	842+07.33	2124396.5874	663421.1409
Radius:		267.000'	
Delta:		127°11'48.09" Left	
Degree of Curvature(Arc):		21°27'32.74"	
Length:		592.740'	
Tangent:		537.829'	
Chord:		478.303'	
Middle Ordinate:		148.275'	
External:		333.457'	
Tangent Direction:		S 89°36'47.07" W	
Radial Direction:		N 0°23'12.93" W	
Chord Direction:		S 26°00'53.03" W	
Radial Direction:		S 52°24'58.98" W	
Tangent Direction:		S 37°35'01.02" E	
Element: Linear			
PT ( 19)	842+07.33	2124396.5874	663421.1409
POE ( 409)	848+50.00	2124788.5643	662911.8474
Tangent Direction:		S 37°35'01.02" E	
Tangent Length:		642.671'	

Project Name: SW04892 I V1  
 Description: I-35 & SH 74 Interchange at Goldsby  
 Horizontal Alignment Name: A005  
 Description: C/L North Entrance  
 Style: Centerline

	STATION	EASTING	NORTHING
Element: Linear			
POB ( 405)	700+00.00	2124886.0339	664060.2374
PC ( 20)	700+08.44	2124886.1174	664051.7978
Tangent Direction:		S 0°34'00.63" E	
Tangent Length:		8.440'	
Element: Circular			
PC ( 20)	700+08.44	2124886.1174	664051.7978
PI ( )	701+04.42	2124887.0669	663955.8225
CC ( 21)	2124719.1326	664050.1457	
PT ( 411)	701+82.66	2124804.6155	663906.6908
Radius:		166.993'	
Delta:		59°46'36.12" Right	
Degree of Curvature(Arc):		34°18'37.11"	
Length:		174.224'	
Tangent:		95.980'	
Chord:		166.429'	
Middle Ordinate:		22.210'	
External:		25.618'	
Tangent Direction:		S 0°34'00.63" E	
Radial Direction:		S 89°25'59.37" W	
Chord Direction:		S 29°19'17.43" W	
Radial Direction:		N 30°47'24.51" W	
Tangent Direction:		S 59°12'35.49" W	

Project Name: SW04892 I V1  
 Description: I-35 & SH 74 Interchange at Goldsby  
 Horizontal Alignment Name: A006  
 Description: C/L North Exit  
 Style: Centerline

	STATION	EASTING	NORTHING
Element: Linear			
POB ( 405)	600+00.00	2124886.0339	664060.2374
PC ( 22)	600+23.44	2124886.2657	664036.8035
Tangent Direction: S 0°34'00.49" E			
Tangent Length: 23.435'			
Element: Circular			
PC ( 22)	600+23.44	2124886.2657	664036.8035
PI ( )	600+65.24	2124886.6792	663995.0050
CC ( 23)	2124613.2790	664034.1029	
PT ( 24)	601+06.39	2124874.5670	663954.9977
Radius: 273.000'			
Delta: 17°24'37.94" Right			
Degree of Curvature(Arc): 20°59'14.87"			
Length: 82.957'			
Tangent: 41.801'			
Chord: 82.638'			
Middle Ordinate: 3.145'			
External: 3.182'			
Tangent Direction: S 0°34'00.49" E			
Radial Direction: S 89°25'59.51" W			
Chord Direction: S 8°08'18.48" W			
Radial Direction: N 73°09'22.55" W			
Tangent Direction: S 16°50'37.45" W			
Element: Linear			
PT ( 24)	601+06.39	2124874.5670	663954.9977
PC ( 25)	601+06.40	2124874.5659	663954.9942
Tangent Direction: S 16°50'37.47" W			
Tangent Length: 0.004'			
Element: Circular			
PC ( 25)	601+06.40	2124874.5659	663954.9942
PI ( )	601+44.31	2124863.5810	663918.7106
CC ( 26)	2124962.6107	663928.3385	
PT ( 414)	601+78.31	2124881.3493	663885.2225
Radius: 91.991'			
Delta: 44°47'36.53" Left			
Degree of Curvature(Arc): 62°17'01.81"			
Length: 71.918'			
Tangent: 37.910'			
Chord: 70.101'			
Middle Ordinate: 6.939'			
External: 7.505'			
Tangent Direction: S 16°50'37.45" W			
Radial Direction: N 73°09'22.55" W			
Chord Direction: S 5°33'10.81" E			
Radial Direction: S 62°03'00.92" W			
Tangent Direction: S 27°56'59.08" E			

Project Name: SW04892 I V1  
 Description: I-35 & SH 74 Interchange at Goldsby  
 Horizontal Alignment Name: A008  
 Description: East Ent.  
 Style: Centerline

	STATION	EASTING	NORTHING
Element: Linear			
POB ( 421)	500+00.00	2125003.8430	663803.7832
PC ( 30)	500+20.02	2125003.6794	663823.8026
Tangent Direction: N 0°28'05.45" W			
Tangent Length: 20.020'			
Element: Circular			
PC ( 30)	500+20.02	2125003.6794	663823.8026
PI ( )	500+63.66	2125003.3228	663867.4411
CC ( 31)	2124959.6828	663823.4430	
PT ( 32)	500+88.77	2124959.6828	663867.4411
Radius: 43.998'			
Delta: 89°31'54.55" Left			
Degree of Curvature(Arc): 130°13'24.32"			
Length: 68.752'			
Tangent: 43.640'			
Chord: 61.968'			
Middle Ordinate: 12.760'			
External: 17.972'			
Tangent Direction: N 0°28'05.45" W			
Radial Direction: N 89°31'54.55" E			
Chord Direction: N 45°14'02.72" W			
Radial Direction: N 0°00'00.00" E			
Tangent Direction: S 90°00'00.00" W			
Element: Linear			
PT ( 32)	500+88.77	2124959.6828	663867.4411
PC ( 33)	501+24.48	2124923.9725	663867.4411
Tangent Direction: S 90°00'00.00" W			
Tangent Length: 35.710'			
Element: Circular			
PC ( 33)	501+24.48	2124923.9725	663867.4411
PI ( )	501+53.85	2124894.6025	663867.4411
CC ( 34)	2124923.9725	663924.4367	
PT ( 420)	501+78.72	2124877.5574	663891.3589
Radius: 56.996'			
Delta: 54°31'27.60" Right			
Degree of Curvature(Arc): 100°31'35.75"			
Length: 54.239'			
Tangent: 29.370'			
Chord: 52.215'			
Middle Ordinate: 6.331'			
External: 7.122'			
Tangent Direction: S 90°00'00.00" W			
Radial Direction: N 0°00'00.00" E			
Chord Direction: N 62°44'16.20" W			
Radial Direction: N 54°31'27.60" E			
Tangent Direction: N 35°28'32.40" W			

Project Name: SW04892 I V1  
 Description: I-35 & SH 74 Interchange at Goldsby  
 Horizontal Alignment Name: A009  
 Description: East Exit  
 Style: Centerline

	STATION	EASTING	NORTHING
Element: Linear			
POB ( 421)	400+00.00	2125003.8430	663803.7832
PC ( 35)	400+14.02	2125003.7284	663817.8026
Tangent Direction: N 0°28'05.55" W			
Tangent Length: 14.020'			
Element: Circular			
PC ( 35)	400+14.02	2125003.7284	663817.8026
PI ( )	400+57.66	2125003.3718	663861.4411
CC ( 36)	2124959.7318	663817.4430	
PT ( 37)	400+82.77	2124959.7318	663861.4411
Radius: 43.998'			
Delta: 89°31'54.45" Left			
Degree of Curvature(Arc): 130°13'24.09"			
Length: 68.752'			
Tangent: 43.640'			
Chord: 61.968'			
Middle Ordinate: 12.760'			
External: 17.972'			
Tangent Direction: N 0°28'05.55" W			
Radial Direction: N 89°31'54.45" E			
Chord Direction: N 45°14'02.78" W			
Radial Direction: N 0°00'00.00" E			
Tangent Direction: S 90°00'00.00" W			
Element: Linear			
PT ( 37)	400+82.77	2124959.7318	663861.4411
PC ( 38)	401+12.17	2124930.3308	663861.4411
Tangent Direction: S 90°00'00.00" W			
Tangent Length: 29.401'			
Element: Circular			
PC ( 38)	401+12.17	2124930.3308	663861.4411
PI ( )	401+45.47	2124897.0308	663861.4411
CC ( 39)	2124930.3308	663804.4424	
PT ( 424)	401+72.45	2124880.6781	663832.4328
Radius: 56.999'			
Delta: 60°35'20.30" Left			
Degree of Curvature(Arc): 100°31'16.26"			
Length: 60.275'			
Tangent: 33.300'			
Chord: 57.505'			
Middle Ordinate: 7.784'			
External: 9.014'			
Tangent Direction: S 90°00'00.00" W			
Radial Direction: N 0°00'00.00" E			
Chord Direction: S 59°42'19.85" W			
Radial Direction: N 60°35'20.30" W			
Tangent Direction: S 29°24'39.70" W			

Project Name: SW04892 I V1  
 Description: I-35 & SH 74 Interchange at Goldsby  
 Horizontal Alignment Name: A010  
 Description: South Ent.  
 Style: Centerline

	STATION	EASTING	NORTHING
Element: Linear			
POB ( 416)	300+00.00	2124888.6896	663617.8387
PC ( 40)	300+37.25	2124888.0994	663655.0816
Tangent Direction: N 0°54'28.33" W			
Tangent Length: 37.248'			
Element: Circular			
PC ( 40)	300+37.25	2124888.0994	663655.0816
PI ( )	300+73.99	2124887.5173	663691.8167
CC ( 41)	2124615.1614	663650.7565	
PT ( 42)	301+10.29	2124877.2434	663727.0907
Radius: 272.972'			
Delta: 15°19'51.23" Left			
Degree of Curvature(Arc): 20°59'22.53"			
Length: 73.040'			
Tangent: 36.740'			
Chord: 72.823'			
Middle Ordinate: 2.439'			
External: 2.461'			
Tangent Direction: N 0°54'28.33" W			
Radial Direction: N 89°05'31.67" E			
Chord Direction: N 8°34'23.94" W			
Radial Direction: N 73°45'40.45" E			
Tangent Direction: N 16°14'19.55" W			
Element: Circular			
PT ( 42)	301+10.29	2124877.2434	663727.0907
PI ( )	301+69.32	2124860.7362	663783.7656
CC ( 43)	2125037.5912	663773.7936	
PT ( 427)	302+23.77	2124883.5098	663838.2257
Radius: 167.011'			
Delta: 38°55'54.96" Right			
Degree of Curvature(Arc): 34°18'23.93"			
Length: 113.482'			
Tangent: 59.030'			
Chord: 111.312'			
Middle Ordinate: 9.546'			
External: 10.125'			
Tangent Direction: N 16°14'19.50" W			
Radial Direction: N 73°45'40.50" E			
Chord Direction: N 3°13'37.98" E			
Radial Direction: S 67°18'24.54" E			
Tangent Direction: N 22°41'35.46" E			

PLS	DEA		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	DEA		
CHECKED	DEA		
APPROVED	DEA		
CREW	Tecumseh		
COUNTY	McCain		
SURVEY DATA SHEET			
SWO 4892111			
HIGHWAY I-35&SH74 STATE JOB NO. 29571041			SHEET NO. S005

STATE OF OKLAHOMA  
DEPARTMENT OF HIGHWAYS  
SURVEY DIVISION  
POSITION AND DESCRIPTION OF SURVEY MONUMENTS

S.D. FORM NO. 11  
REVISED 03/01/2016

COUNTY McClain Monument Number M-44-859 SWO 4892(1) DATE 12/15/20

TYPE OF MONUMENT No.4 Rebar/w cap ODOT PLS1663 MONUMENT SET FOR Primary Control

WRITTEN DESCRIPTION OF LOCATION: This monument is located approx. 0.25 mi. south of SH 74 and I-35 Int. and approximately 102' west of the west edge of SH 74 and approx. 5' west of existing top road slope. control is set approx. 3-4" below ground surface

ESTABLISHED BY: Shawn Smith, PLS

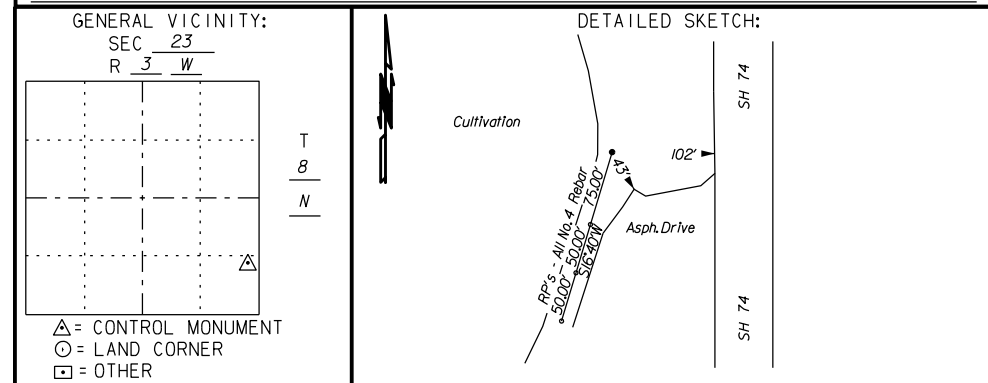
COORDINATE SYSTEM: NAD83(2011) (2010.0000) ZONE: South  
COORDINATES (FEET) X 2124773.9312 Y 661374.7189

GEODETIC POSITION  
LATITUDE 35° 08' 58.3137" NORTH  
LONGITUDE 97° 28' 38.2270" WEST  
ELLIPSOIDAL HEIGHT 331.514m  
METHOD USED TO ESTABLISHED: Static GPS

SOURCE: OPUS Solution

ORTHOMETRIC HEIGHT  
1174.05 FEET GEOID MODEL: Geoid18 GEOID SEPARATION: -86.408 FEET  
METHOD USED TO ESTABLISHED: Static GPS

SOURCE: Differential leveling



STATE OF OKLAHOMA  
DEPARTMENT OF HIGHWAYS  
SURVEY DIVISION  
POSITION AND DESCRIPTION OF SURVEY MONUMENTS

S.D. FORM NO. 11  
REVISED 03/01/2016

COUNTY McClain Monument Number M-44-858 SWO 4892(1) DATE 12/15/20

TYPE OF MONUMENT No.4 Rebar/w cap ODOT PLS1663 MONUMENT SET FOR Primary Control

WRITTEN DESCRIPTION OF LOCATION: This monument is located approx. 0.25 mi. north of SH 74 and I-35 Int. and approximately 92' east of the east edge of SH 74 and approx. 44' west of existing R/W fence. control is set approx. 3-4" below ground surface

ESTABLISHED BY: Shawn Smith, PLS

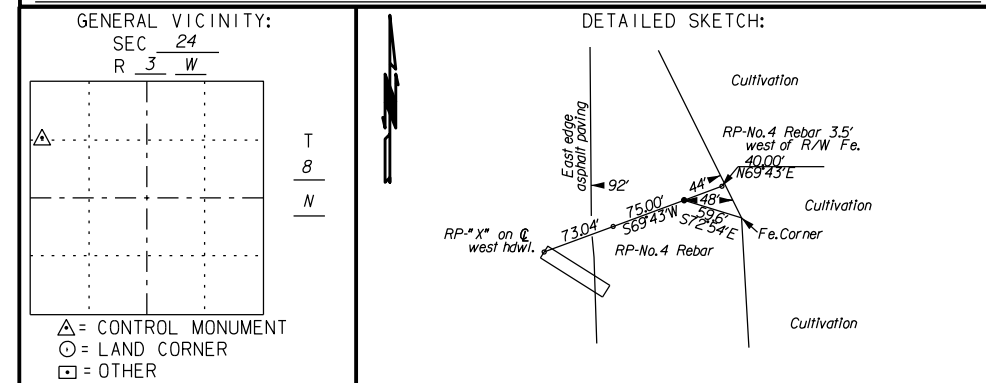
COORDINATE SYSTEM: NAD83(2011) (2010.0000) ZONE: South  
COORDINATES (FEET) X 2124988.9472 Y 664081.0062

GEODETIC POSITION  
LATITUDE 35° 09' 25.06918" NORTH  
LONGITUDE 97° 28' 35.36447" WEST  
ELLIPSOIDAL HEIGHT 329.593m  
METHOD USED TO ESTABLISHED: Static GPS

SOURCE: OPUS Solution

ORTHOMETRIC HEIGHT  
1167.82 FEET GEOID MODEL: Geoid18 GEOID SEPARATION: -86.4803 FEET  
METHOD USED TO ESTABLISHED: Static GPS

SOURCE: Differential leveling



PLS	DEA		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION  <b>SURVEY DATA SHEET</b> SWO <u>4892(1)</u>
DRAWN	DEA		
CHECKED	DEA		
APPROVED	DEA		
CREW	Tecumseh		
COUNTY <u>McClain</u> HIGHWAY <u>I-35&amp;SH74</u> STATE JOB NO. <u>295711041</u> SHEET NO. <u>S006</u>			

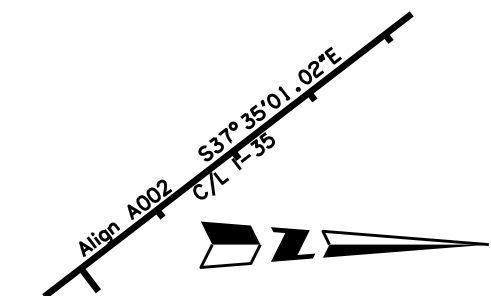
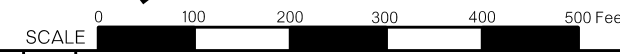
SECTION 23  
T-8-N, R-3-W

SECTION 24  
T-8-N, R-3-W

POT BEC. STA. 771+00.00  
BEG. SWO 4892(1) - J/P 29571(04)  
S.H. 74 SURVEY  
NOTHING SET

**CURVE DATA - C1**  
P.I. Sta. 776+73.78  
X=2124890.7684  
Y= 660092.7052  
Δ= 112°35.4' Ri.  
D=100'00"  
T=60.494'  
L=120.984'  
R=5729.578'  
E=0.319'

**CURVE DATA - C2**  
P.I. Sta. 786+64.00  
X=2124905.7159  
Y= 661082.8123  
Δ= 115°06.65' Li.  
D=100'00"  
T=62.595'  
L=125.185'  
R=5729.578'  
E=0.342'



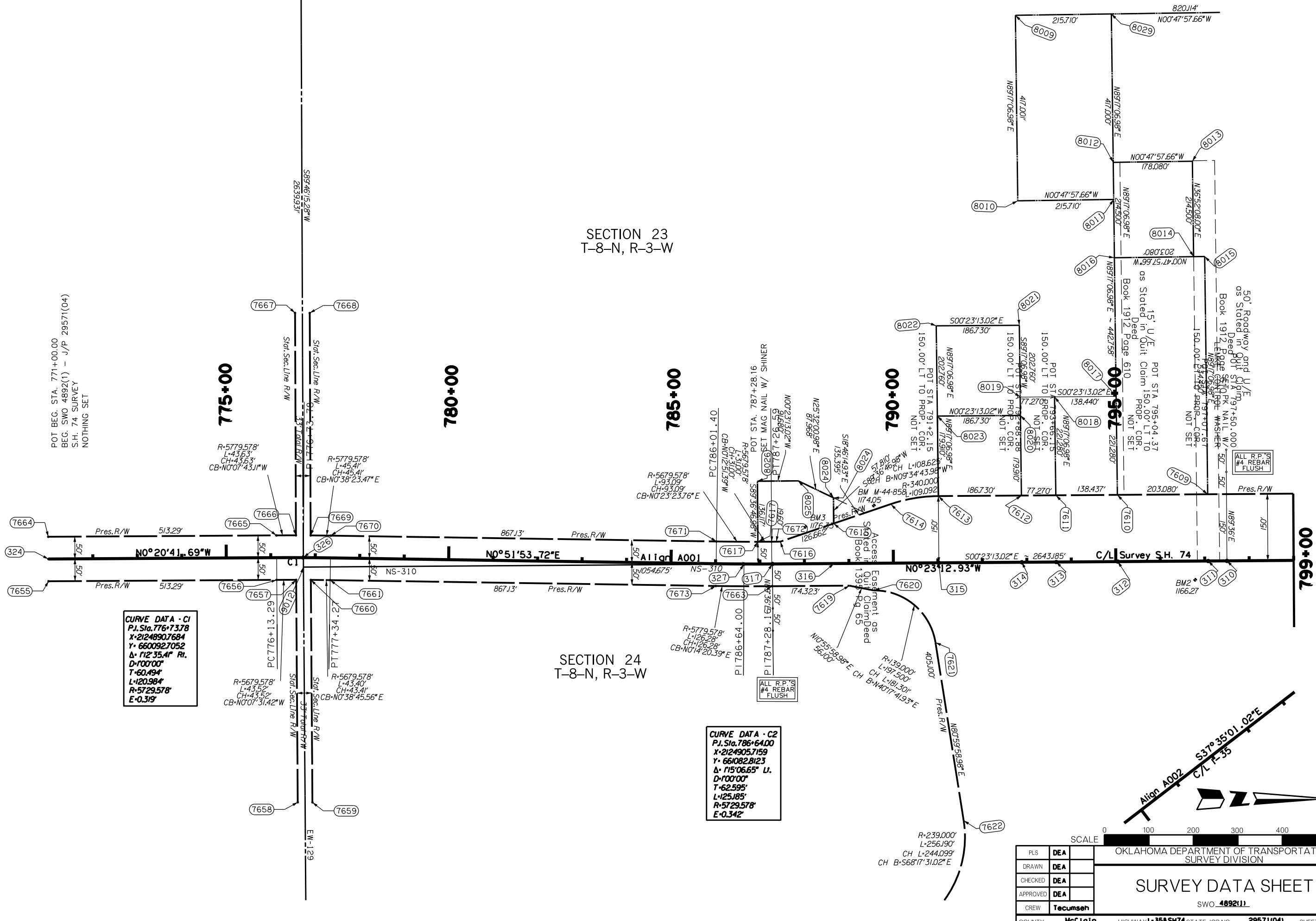
PLS	DEA	
DRAWN	DEA	
CHECKED	DEA	
APPROVED	DEA	
CREW	Tecumseh	
COUNTY	McCain	

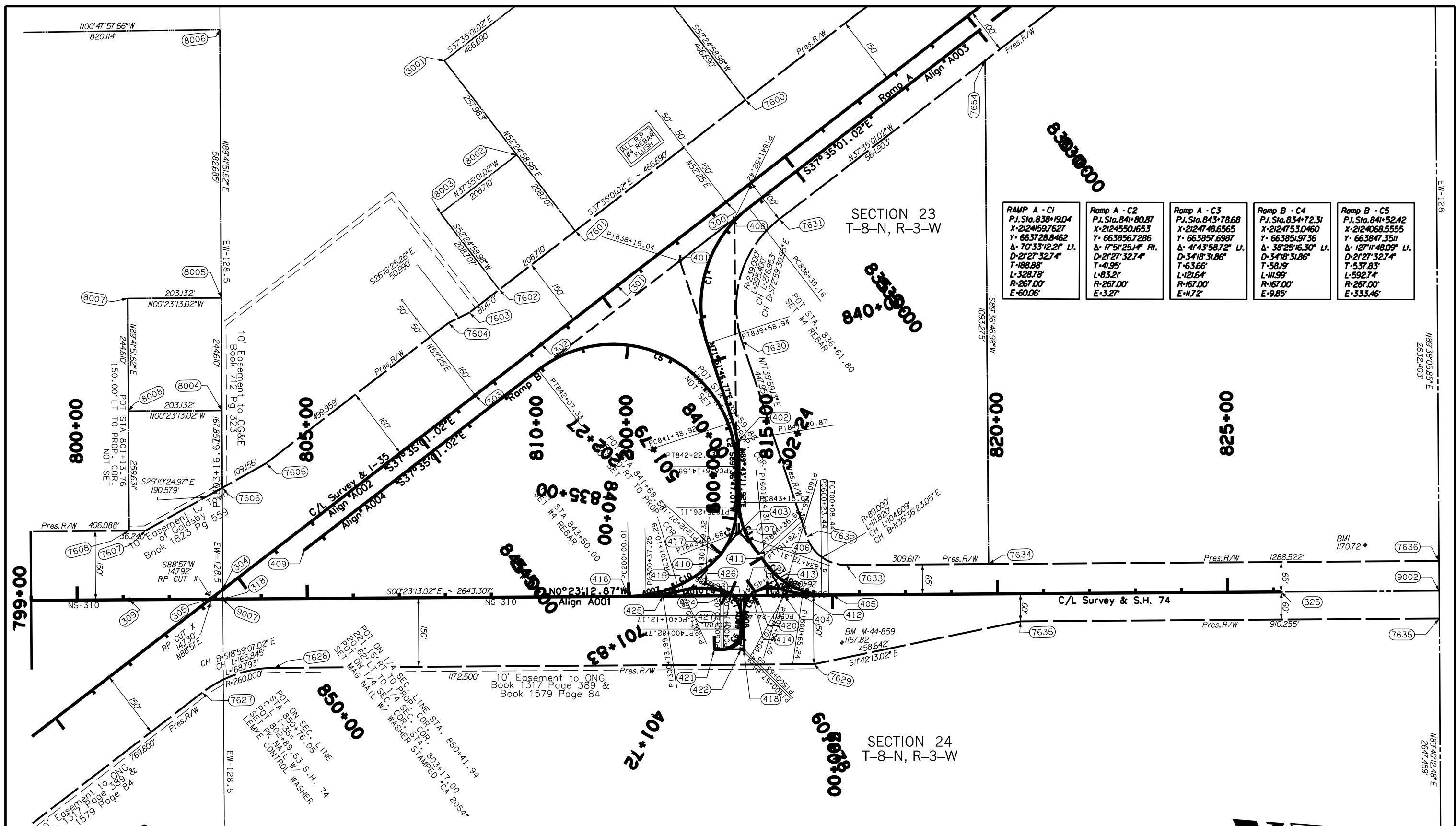
OKLAHOMA DEPARTMENT OF TRANSPORTATION  
SURVEY DIVISION

**SURVEY DATA SHEET**

SWO 4892(1)

COUNTY McCain HIGHWAY 1-35 SH 74 STATE JOB NO. 29571(04) SHEET NO. S007





<b>RAMP A - C1</b> P.I. Sta. 838+19.04 X=2124159.7627 Y= 663728.8462 Δ= 70°33'12.2" LI. D=2127'32.74" T=188.88' L=328.78' R=267.00' E=60.06'	<b>Ramp A - C2</b> P.I. Sta. 841+80.87 X=2124550.653 Y= 663856.7286 Δ= 17°51'25.14" Ri. D=2127'32.74" T=4.95' L=83.2' R=167.00' E=3.27'	<b>Ramp A - C3</b> P.I. Sta. 843+78.68 X=2124748.6565 Y= 663857.6987 Δ= 41°43'58.72" LI. D=3418'31.86" T=63.66' L=121.64' R=167.00' E=11.72'	<b>Ramp B - C4</b> P.I. Sta. 834+72.31 X=2124753.0460 Y= 663851.9736 Δ= 38°25'16.30" LI. D=3418'31.86" T=58.19' L=111.99' R=167.00' E=9.85'	<b>Ramp B - C5</b> P.I. Sta. 841+52.42 X=2124068.5555 Y= 663847.3511 Δ= 127°11'48.09" LI. D=2127'32.74" T=537.83' L=592.74' R=267.00' E=333.46'
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<b>NORTH Exit - C2</b> P.I. Sta. 600+65.24 X=2124886.6792 Y= 663995.0050 Δ= 17°24'37.94" Ri. D=2059'14.87" T=41.80' L=82.957' R=273.000' E=3.182'	<b>NORTH Exit - C3</b> P.I. Sta. 601+44.31 X=2124863.5810 Y= 663918.7106 Δ= 44°47'36.53" LI. D=6217'01.81" T=37.910' L=71.918' R=91.991' E=7.505'	<b>SOUTH Exit - C10</b> P.I. Sta. 201+20.59 X=2124886.7789 Y= 663738.4136 Δ= 47°39'40.89" LI. D=2059'16.13" T=43.640' L=120.580' L=227.091' R=272.995' E=25.444'	<b>EAST ENT. - C4</b> P.I. Sta. 500+63.66 X=2125003.3228 Y= 663867.4411 Δ= 89°31'54.55" LI. D=13013'24.32" T=43.640' L=68.752' R=43.998' E=17.972'	<b>EAST ENT. - C5</b> P.I. Sta. 501+53.85 X=2125003.3718 Y= 663861.4411 Δ= 54°31'27.60" Ri. D=100°31'35.75" T=29.370' L=54.239' R=56.996' E=7.122'	<b>EAST EXIT - C6</b> P.I. Sta. 400+57.66 X=2125003.3718 Y= 663861.4411 Δ= 89°31'54.45" LI. D=13013'24.09" T=43.640' L=68.752' R=43.998' E=17.972'	<b>EAST EXIT - C7</b> P.I. Sta. 401+45.47 X=2124897.0308 Y= 663861.4411 Δ= 60°35'20.30" LI. D=100°31'16.26" T=36.740' L=60.275' R=56.999' E=9.014'	<b>SOUTH ENT. - C8</b> P.I. Sta. 300+73.99 X=2124887.5173 Y= 663691.8167 Δ= 1519'51.23" LI. D=2059'22.53" T=36.740' L=73.040' R=272.972' E=2.461'	<b>SOUTH ENT. - C9</b> P.I. Sta. 301+69.32 X=2124860.7362 Y= 663783.7656 Δ= 38°55'54.96" Ri. D=3418'23.93" T=59.030' L=113.482' R=167.011' E=10.25'	<b>NORTH ENT. - C1</b> P.I. Sta. 701+04.42 X=2124887.0669 Y= 663955.8225 Δ= 59°46'36.12" Ri. D=3418'37.11" T=95.98' L=174.224' R=166.993' E=25.618'
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SCALE 0 100 200 300 400 500 Feet

PLS	DEA	
DRAWN	DEA	
CHECKED	DEA	
APPROVED	DEA	
CREW	Tecumseh	
COUNTY	McCain	

OKLAHOMA DEPARTMENT OF TRANSPORTATION  
SURVEY DIVISION

**SURVEY DATA SHEET**

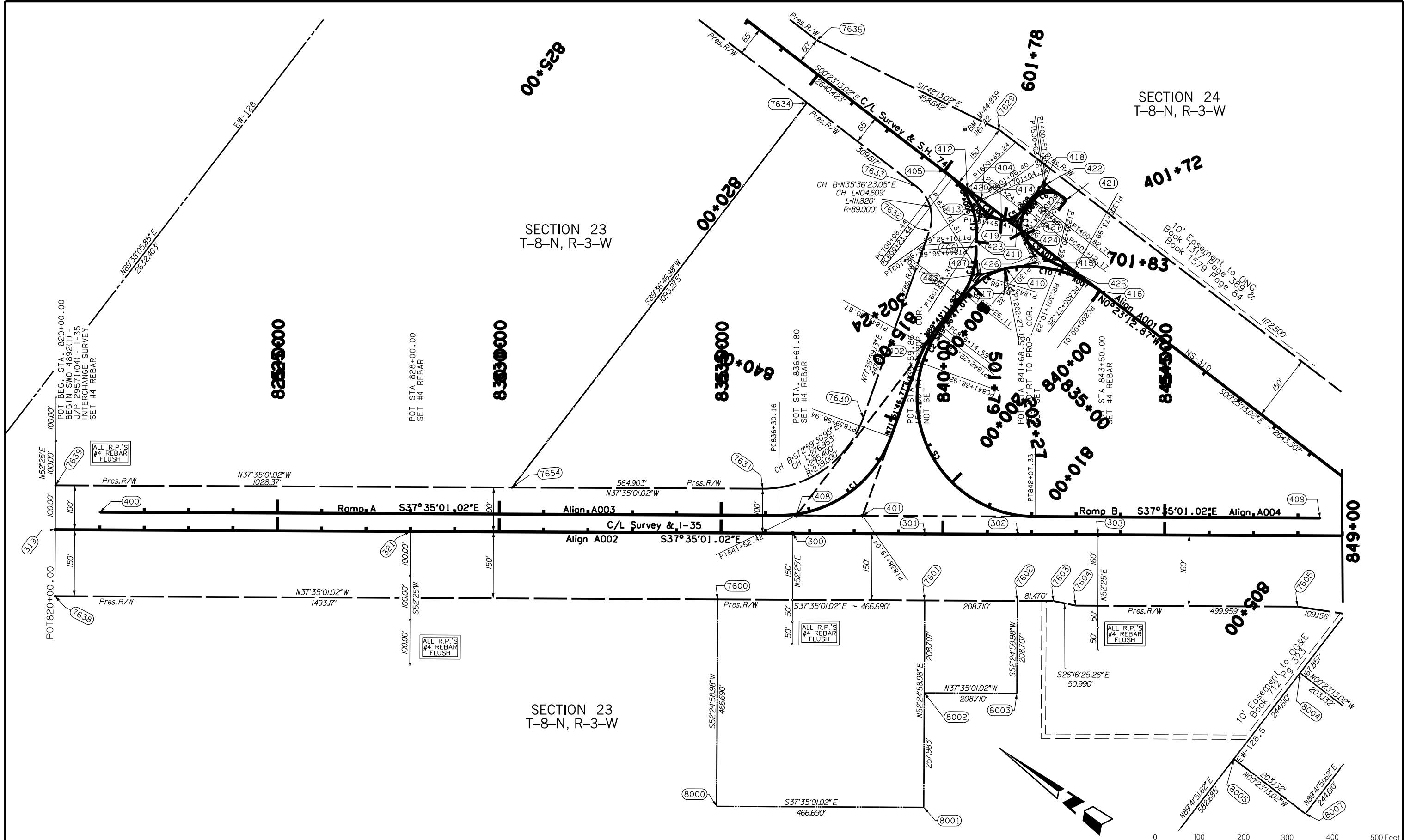
SWO 4892111

COUNTY McCain HIGHWAY 1-354 SH74 STATE JOB NO. 29571041 SHEET NO. S008

SECTION 24  
T-8-N, R-3-W

SECTION 23  
T-8-N, R-3-W

SECTION 23  
T-8-N, R-3-W



10' Easement to ONG &  
Book 1377 Page 389 &  
Book 1579 Page 84

10' Easement to ONG &  
Book 712 pg 323 &  
Book 2031 pg 32

SCALE 0 100 200 300 400 500 Feet

PLS	DEA		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	DEA		
CHECKED	DEA		
APPROVED	DEA		
CREW	Tecumseh		
SURVEY DATA SHEET			
SWO 4692111			
COUNTY	McCain	HIGHWAY	I-35A SH74 STATE JOB NO. 29571041 SHEET NO. S009

SECTION 24  
T-8-N, R-3-W

POT ON 1/4 SEC. LINE STA. 850+41.94  
221.15' RT TO PROP. COR.  
20.62' LT TO 1/4 SEC. COR.  
POT ON 1/4 SEC. COR. STA. 803+17.00  
SET MAG NAIL W/ WASHER STAMPED "CA 2054"

850+00

855+00

860+00

865+00

870+00

875+00

876+00.00

879+00

849+00

Align A002  
C/L Survey 35  
S37°35'01.02"E

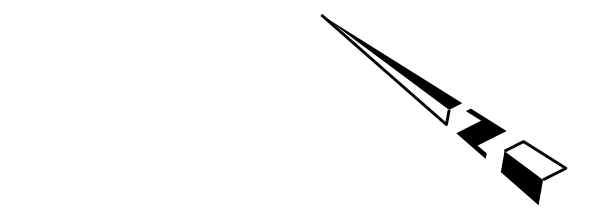
Align A002

SECTION 24  
T-8-N, R-3-W

SECTION 23  
T-8-N, R-3-W

00+008

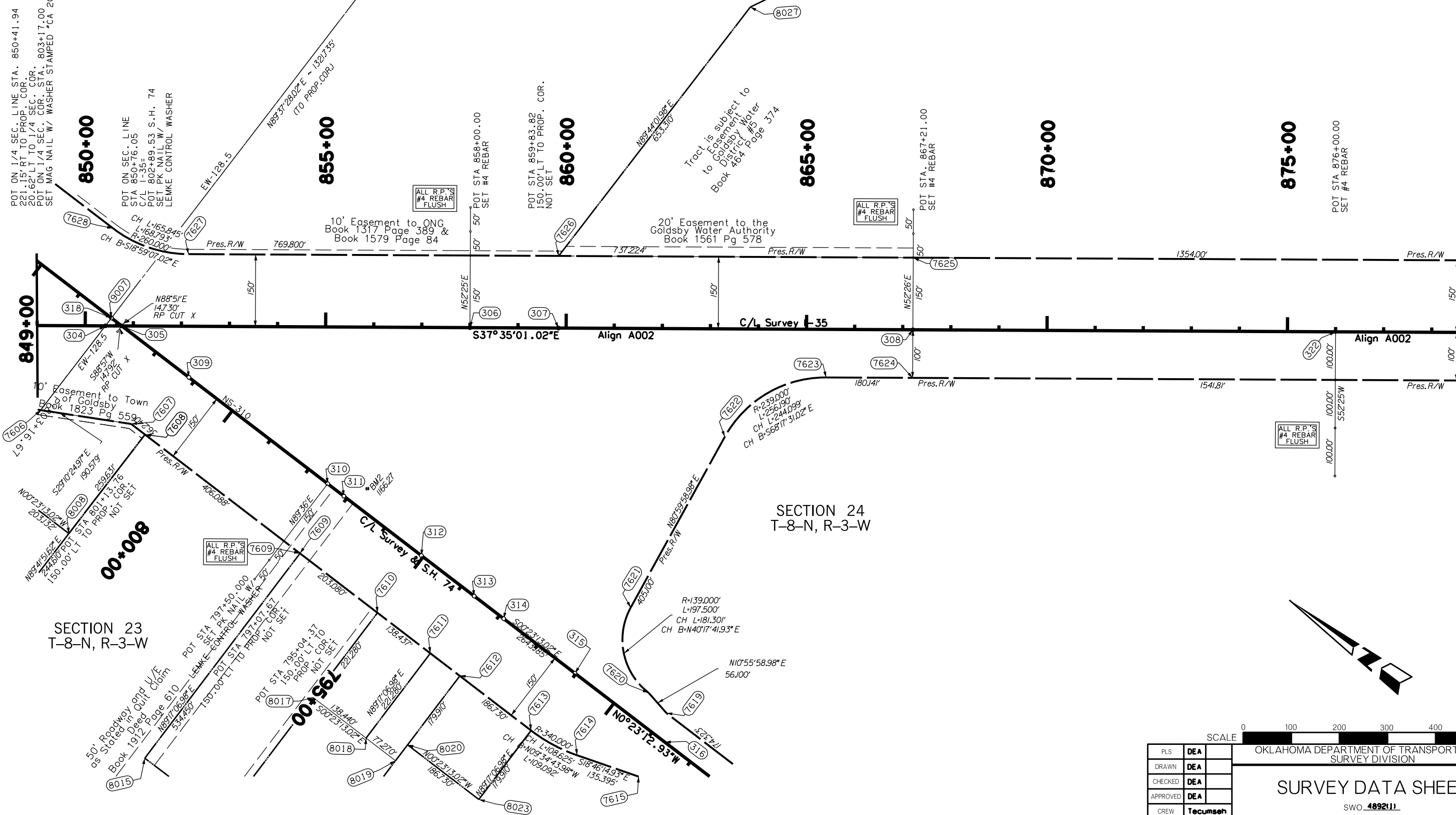
00+961

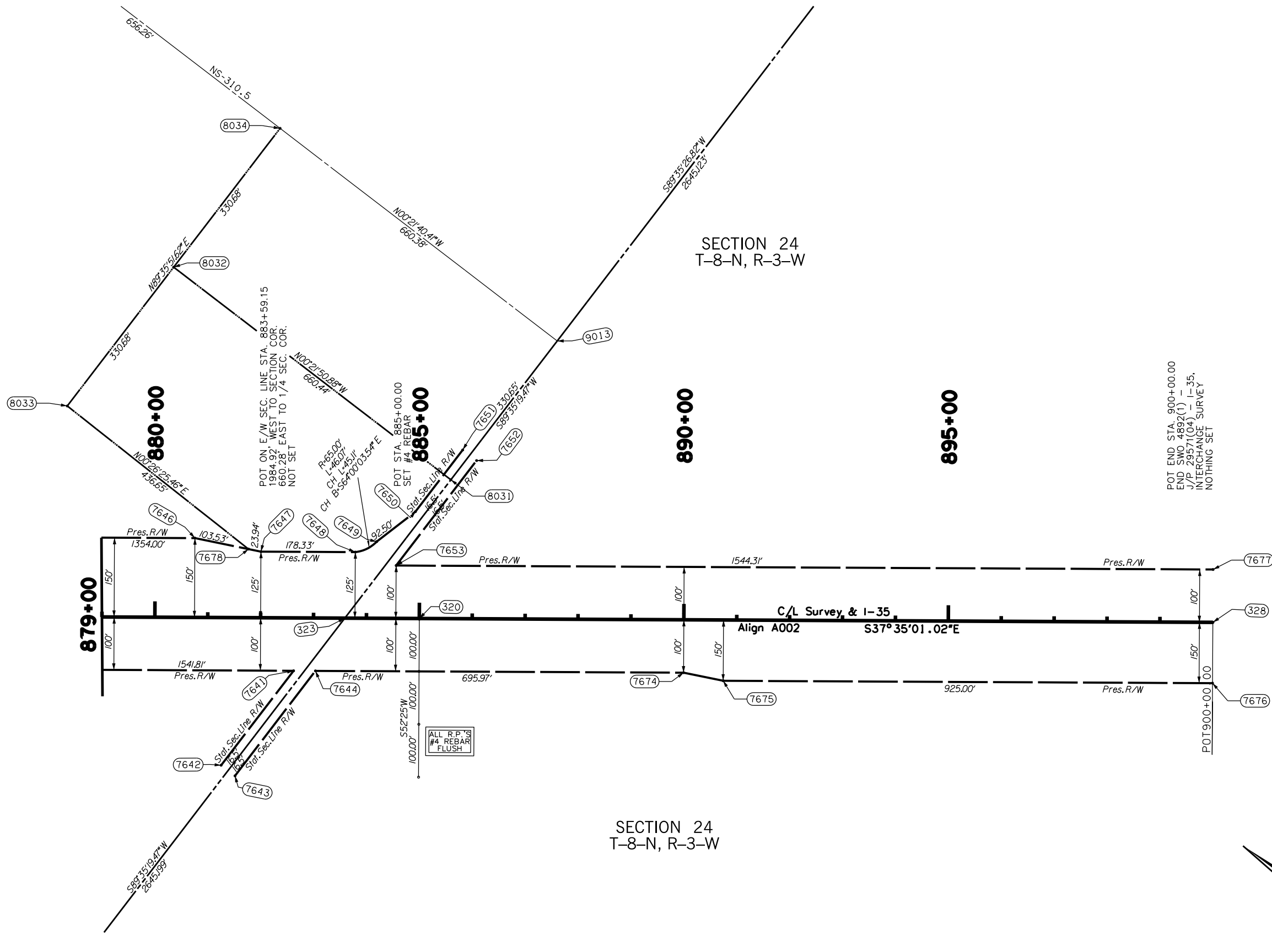


PLS	DEA		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	DEA		
CHECKED	DEA		
APPROVED	DEA		
CREW	Tecumseh		
COUNTY			McClain
HIGHWAY			1-354 SH74
STATE JOB NO.			295711041
SHEET NO.			S010

SURVEY DATA SHEET

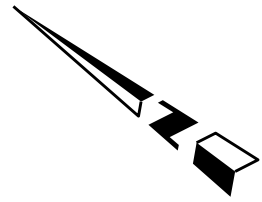
SWO 4892111





POT END STA. 900+00.00  
 END SWO 4892(1) I-35,  
 I/P 29571(04) I-35,  
 INTERCHANGE SURVEY  
 NOTHING SET

SECTION 24  
 T-8-N, R-3-W



SCALE 0 100 200 300 400 500 Feet

PLS	DEA		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	DEA		
CHECKED	DEA		
APPROVED	DEA		
CREW	Tecumseh		
COUNTY <b>McCain</b> HIGHWAY <b>I-35 SH74</b> STATE JOB NO. <b>29571(04)</b> SHEET NO. <b>S011</b>			<b>SURVEY DATA SHEET</b> SWO <b>4892(1)</b>

SECTION CORNER - O.D.O.T. M-44-550  
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON  
 CORNER RECORD FILED BY FRANCIS GUNNING. TWO  
 REFERENCE MONUMENTS WERE FOUND AND VERIFIED.  
 ALSO FOUND #3 REBAR 0.93' NORTH AND 4.97' WEST  
 OF CORNER.

1/4 SECTION CORNER - O.D.O.T. M-44-700  
 FOUND AND ACCEPTED MAG NAIL. THIS MONUMENT SET  
 WELL WITH EXISTING IMPROVEMENTS. ALSO FOUND #3  
 REBAR 4.95' NORTH AND 0.42' EAST OF CORNER. THERE  
 WAS NO CORNER RECORD ON FILE.

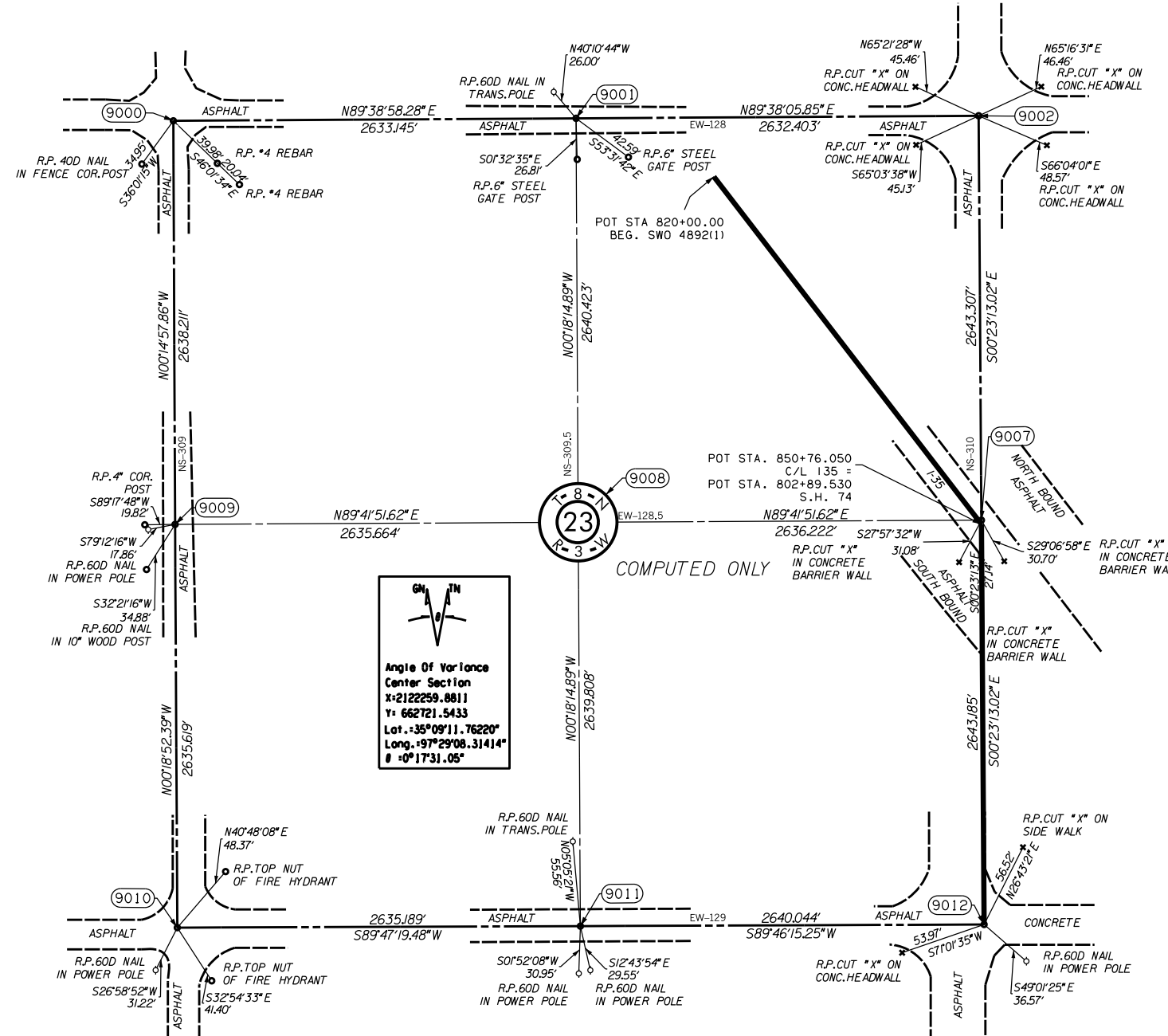
SECTION CORNER - O.D.O.T. M-44-699  
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON  
 CORNER RECORD FILED BY ROBERT OUTLAND. TWO  
 REFERENCE MONUMENTS WERE FOUND AND VERIFIED.

1/4 SECTION CORNER - O.D.O.T. M-44-551  
 FOUND AND ACCEPTED #3 REBAR STAMPED CA 1600  
 MORREN. FOUR REFERENCE MONUMENTS WERE FOUND  
 AND VERIFIED.

SECTION CORNER - O.D.O.T. M-44-216  
 FOUND AND ACCEPTED MAG NAIL STAMPED PLS 1660.  
 FOUR REFERENCE MONUMENTS WERE FOUND AND  
 VERIFIED.

1/4 SECTION CORNER - O.D.O.T. M-44-217  
 SET PK NAIL WITH SHINER STAMPED CA 2054. THIS  
 CORNER WAS RE-ESTABLISHED USING REFERENCES AND  
 INFORMATION FOUND ON CORNER RECORD FILE BY BOB  
 GARLAND.

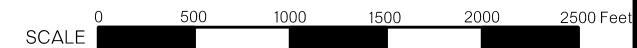
SECTION CORNER - O.D.O.T. M-44-218  
 FOUND AND ACCEPTED MAG NAIL. THE POSITION OF THIS  
 CORNER AGREES WITH SURVEY DATA OBTAINED IN FIELD  
 BOOK #1 SWO 2036(1) AND ONE REFERENCE MONUMENT  
 FOUND AND VERIFIED. ALSO FOUND #3 REBAR 0.39'  
 SOUTH AND 0.35' WEST OF CORNER



1/4 SECTION CORNER - O.D.O.T. M-44-698  
 FOUND AND ACCEPTED CST NAIL. THE POSITION OF THIS  
 CORNER CLOSELY AGREES WITH REFERENCES SHOWN ON  
 CORNER RECORD FILED BY T. WAYNE FISCH.

NOTE: LAND TIE INFORMATION SHOWN TAKEN FROM SWO4892(1) J/P NO.23283(04) SURVEY.

NOTE: REFERENCE'S SHOWN ARE NOT TO SCALE.



PLS	DEA		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	DEA		
CHECKED	DEA		
APPROVED	DEA		
CREW	Tecumseh		
COUNTY			McClain
HIGHWAY			1-358SH74
STATE JOB NO.			29571(04)
SHEET NO.			S012

SURVEY DATA SHEET

SWO 4892(1)

1/4 SECTION CORNER - O.D.O.T. M-44-223  
 SET #4 REBAR WITH CAP CA 2054. THIS CORNER WAS  
 RE-ESTABLISHED USING REFERENCES FOUND ON CORNER  
 RECORD FILE BY BOB GARLAND.

SECTION CORNER - O.D.O.T. M-44-216  
 FOUND AND ACCEPTED MAG NAIL STAMPED PLS 1660.  
 FOUR REFERENCE MONUMENTS WERE FOUND AND  
 VERIFIED.

SECTION CORNER - O.D.O.T. M-44-222  
 FOUND AND ACCEPTED ODOT BRASS CAP AS SHOWN ON  
 CORNER RECORD FILED BY BOB GARLAND.

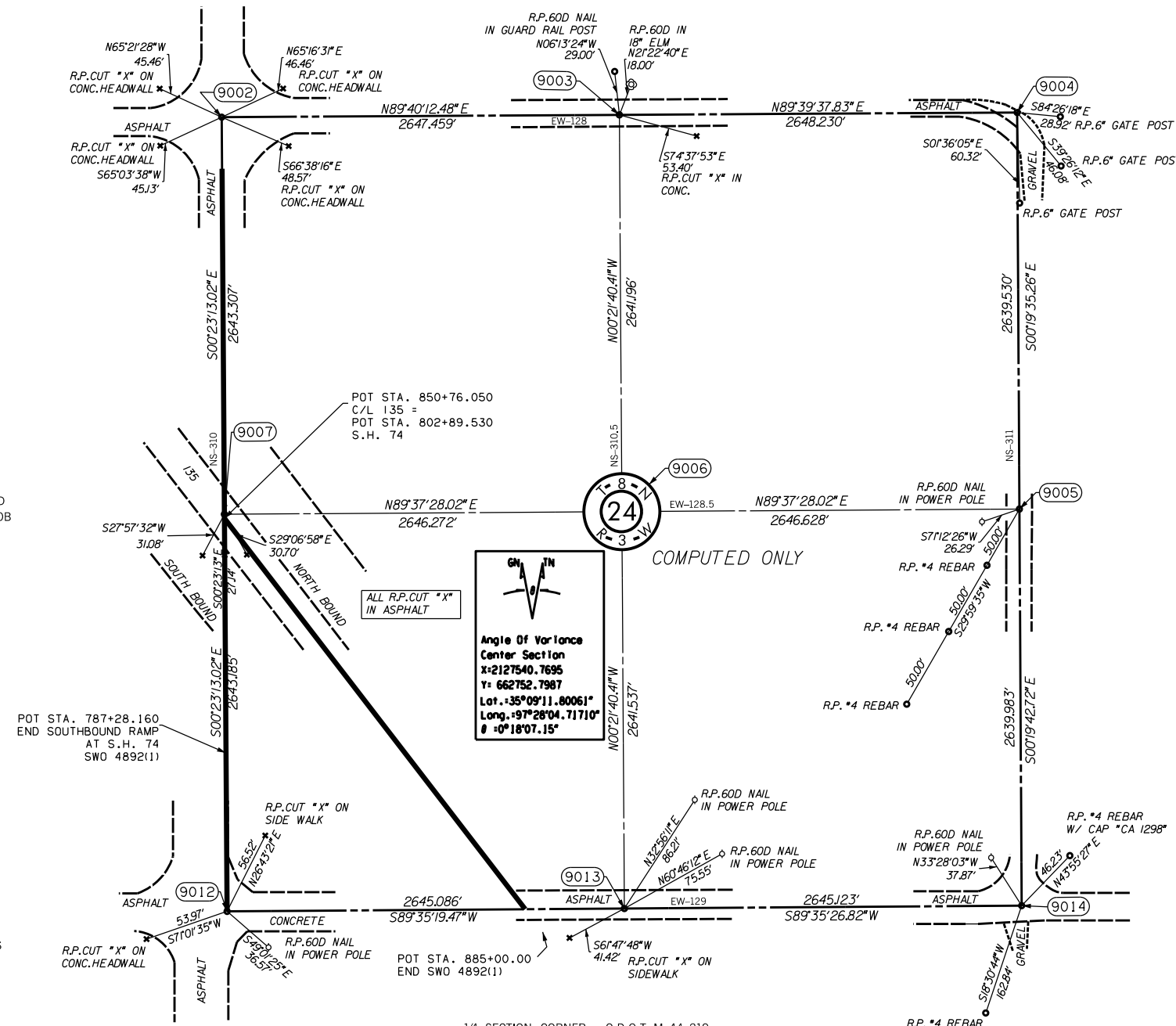
1/4 SECTION CORNER - O.D.O.T. M-44-217  
 SET PK NAIL WITH SHINER STAMPED CA 2054. THIS  
 CORNER WAS RE-ESTABLISHED USING REFERENCES AND  
 INFORMATION FOUND ON CORNER RECORD FILE BY BOB  
 GARLAND.

1/4 SECTION CORNER - O.D.O.T. M-44-221  
 FOUND AND ACCEPTED ODOT BRASS CAP AS SHOWN ON  
 CORNER RECORD FILED BY BOB GARLAND. ONE  
 REFERENCE MONUMENT WAS FOUND AND VERIFIED.

SECTION CORNER - O.D.O.T. M-44-218  
 FOUND AND ACCEPTED MAG NAIL. THE POSITION OF THIS  
 CORNER AGREES WITH SURVEY DATA OBTAINED IN FIELD  
 BOOK #1 SWO 2036(1) AND ONE REFERENCE MONUMENT  
 FOUND AND VERIFIED. ALSO FOUND #3 REBAR 0.39'  
 SOUTH AND 0.35' WEST OF CORNER

SECTION CORNER - O.D.O.T. M-44-220  
 FOUND AND ACCEPTED #4 REBAR AS STATED ON  
 CORNER RECORD FILED BY BOB GARLAND. CORNER  
 RECORD BY BOB GARLAND STATES THAT ODOT  
 MONUMENT WAS SET OVER 1/2" IRON PIN. ONE  
 REFERENCE MONUMENT WAS FOUND AND VERIFIED.

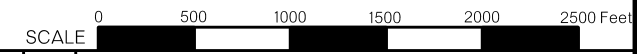
1/4 SECTION CORNER - O.D.O.T. M-44-219  
 FOUND AND ACCEPTED #3 REBAR. THE POSITION OF  
 THIS CORNER CLOSELY AGREES WITH REFERENCE SHOWN  
 ON CORNER RECORD FILED BOB GARLAND. ALSO FOUND  
 #3 REBAR 0.04' NORTH AND 6.77' EAST OF CORNER.



Angle Of Variance  
 Center Section  
 X: 2127540.7695  
 Y: 662752.7987  
 Lat.: 35°09'11.80061"  
 Long.: 97°28'04.71710"  
 θ = 0°18'07.15"

NOTE: LAND TIE INFORMATION SHOWN TAKEN FROM SWO4892(1) J/P NO. 23283(04) SURVEY.

NOTE: REFERENCE'S SHOWN ARE NOT TO SCALE.

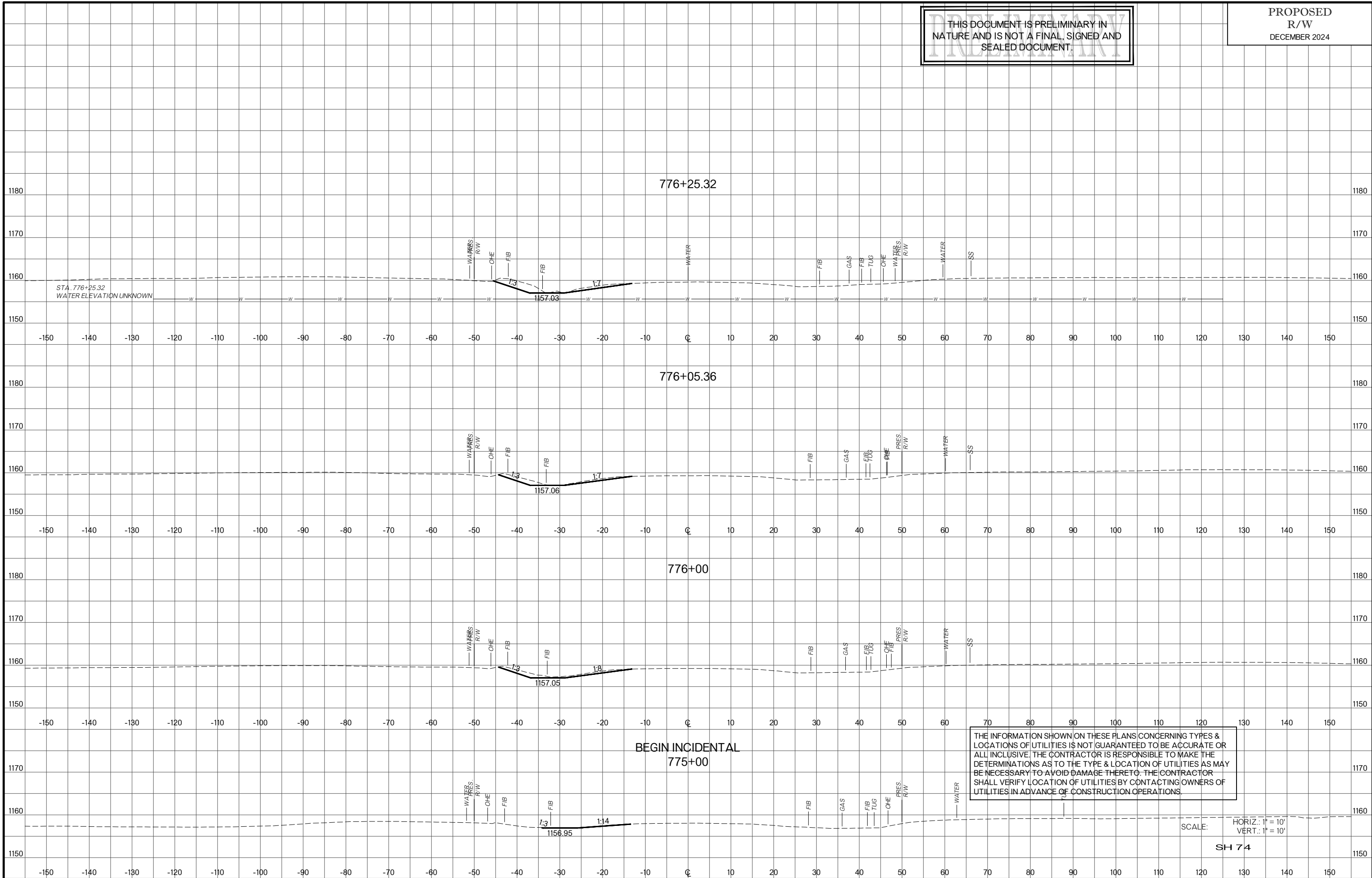


PLS	DEA		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	DEA		
CHECKED	DEA		
APPROVED	DEA		
CREW	Tecumseh		
COUNTY: <b>McCain</b> HIGHWAY: <b>1-358SH74</b> STATE JOB NO.: <b>295711041</b> SHEET NO.: <b>S013</b>			<b>SURVEY DATA SHEET</b> SWO <b>4892(1)</b>



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PROPOSED R/W  
DECEMBER 2024



STA. 776+25.32  
WATER ELEVATION UNKNOWN

776+25.32

776+05.36

776+00

BEGIN INCIDENTAL  
775+00

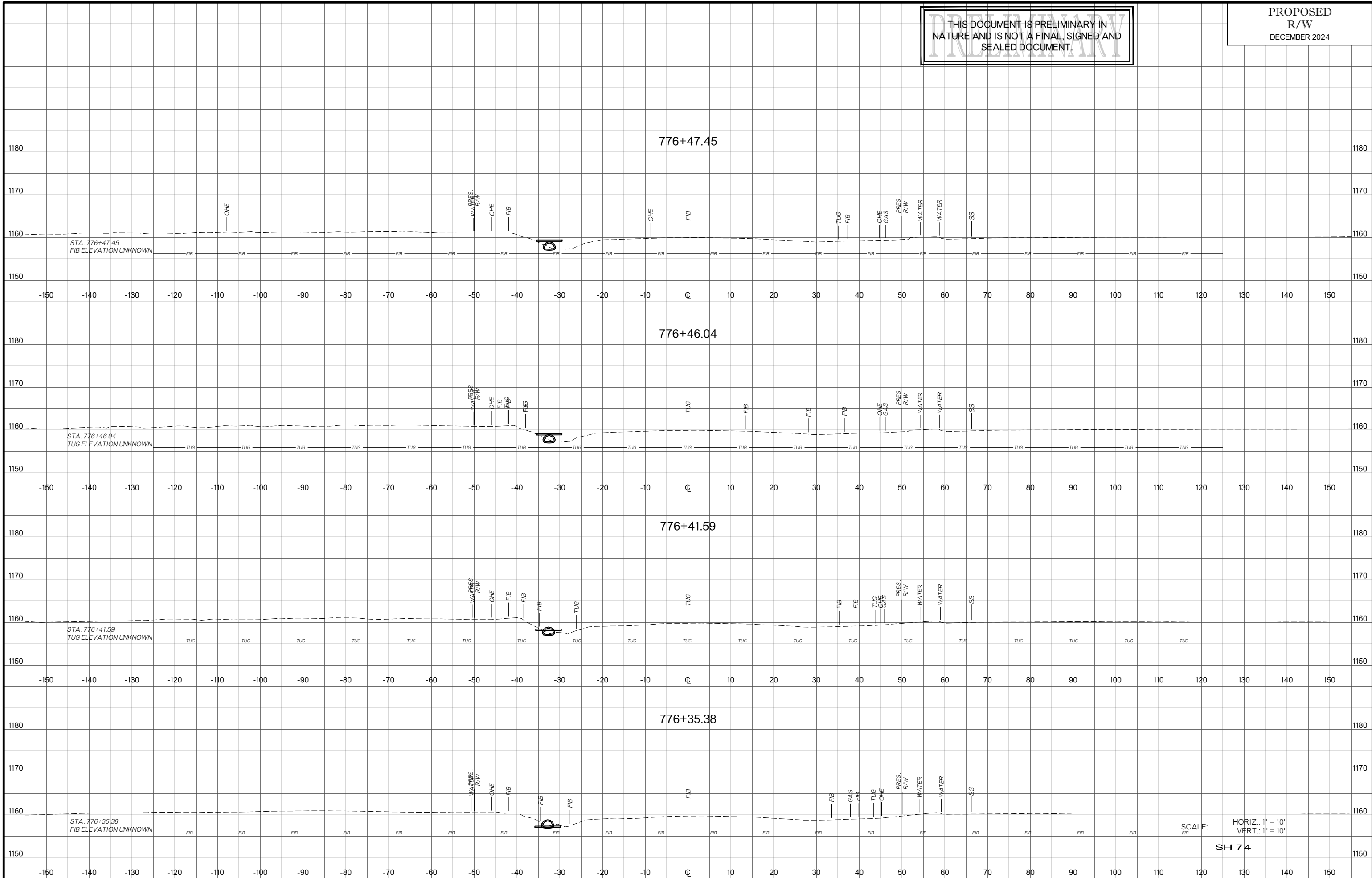
THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

SH 74

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROPOSED R/W  
DECEMBER 2024

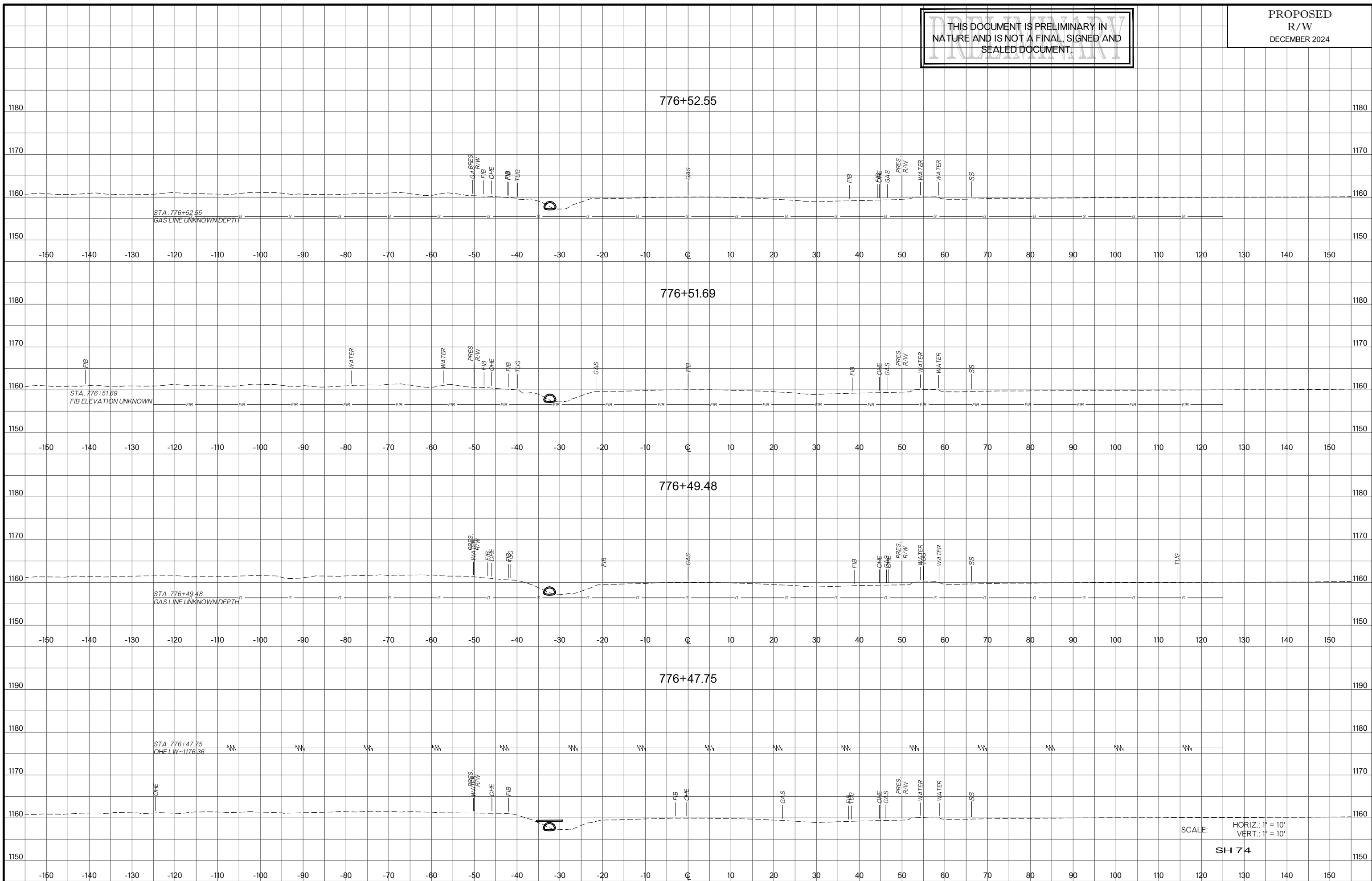


SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

SH 74

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PROPOSED R/W  
DECEMBER 2024

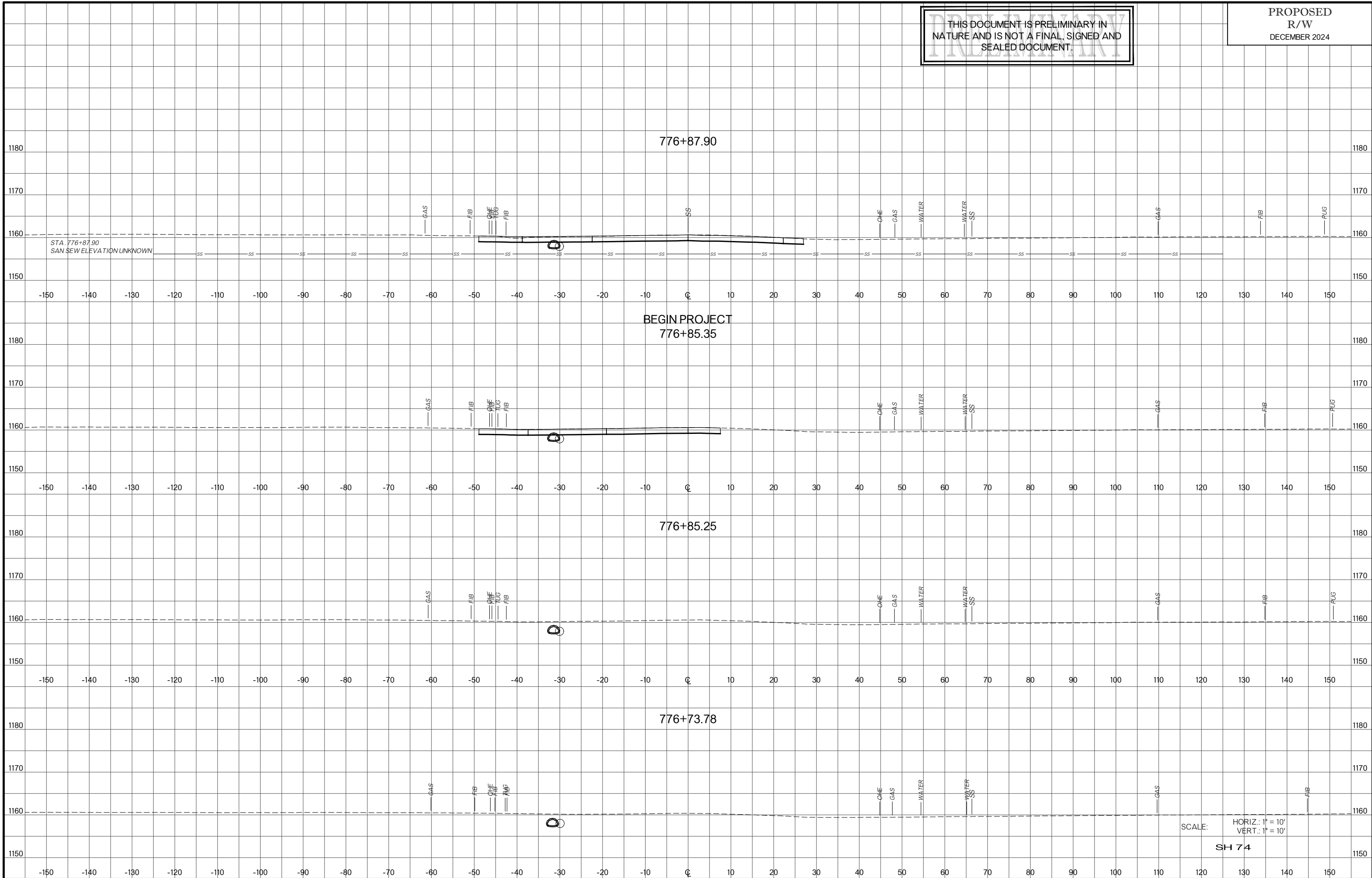


SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

SH 74

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PROPOSED R/W  
DECEMBER 2024

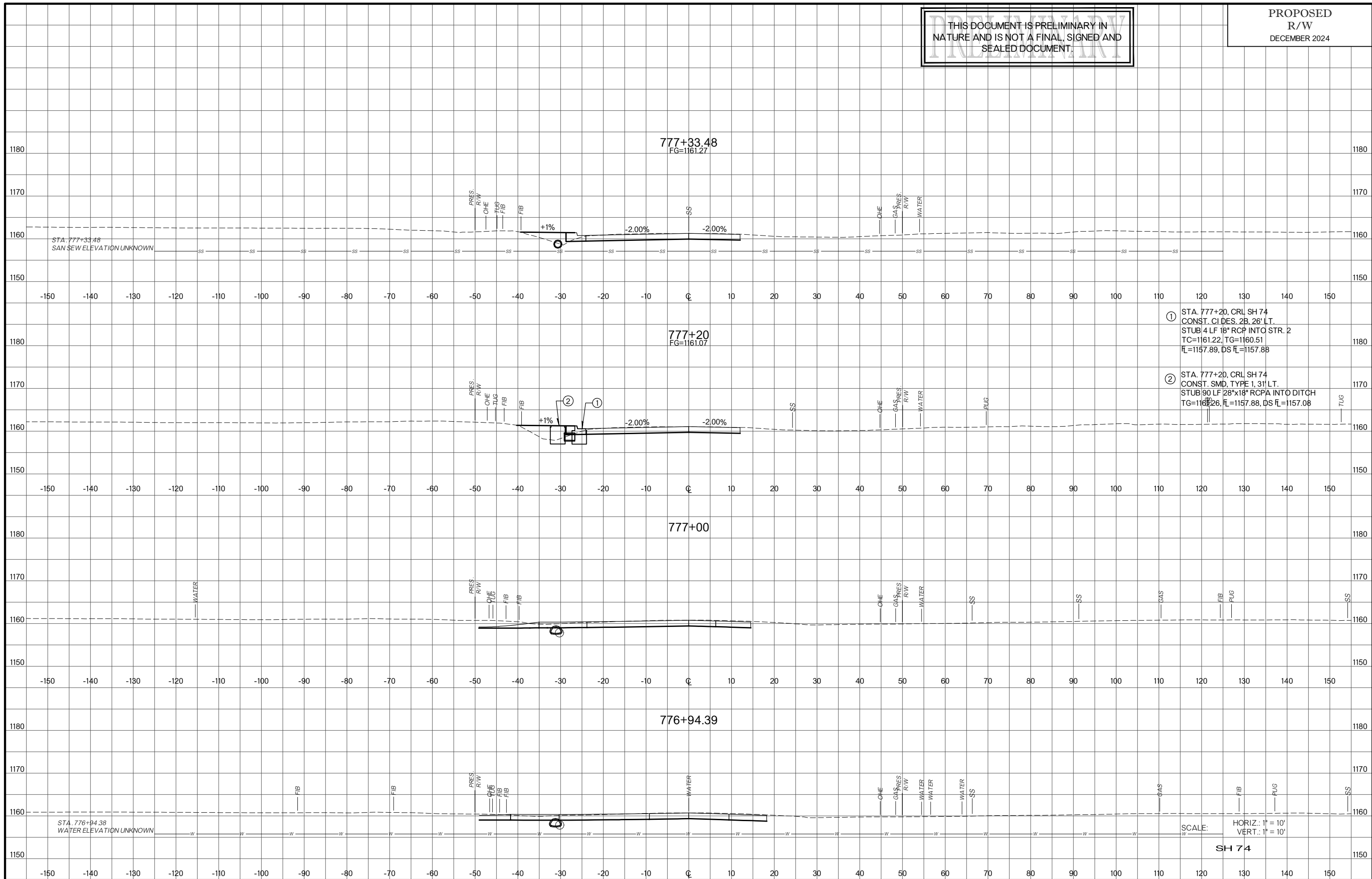


SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

SH 74

THIS DOCUMENT IS PRELIMINARY IN NATURE AND IS NOT A FINAL, SIGNED AND SEALED DOCUMENT.

PROPOSED R/W  
DECEMBER 2024



- ① STA. 777+20, CRL SH 74  
CONST. CLDES. 2B, 26' LT.  
STUB 4 LF 18" RCP INTO STR. 2  
TC=1161.22, TG=1160.51  
IL=1157.89, DS IL=1157.88
- ② STA. 777+20, CRL SH 74  
CONST. SMD, TYPE 1, 31' LT.  
STUB 90 LF 28"x18" RCPA INTO DITCH  
TG=1161.26, IL=1157.88, DS IL=1157.08

SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

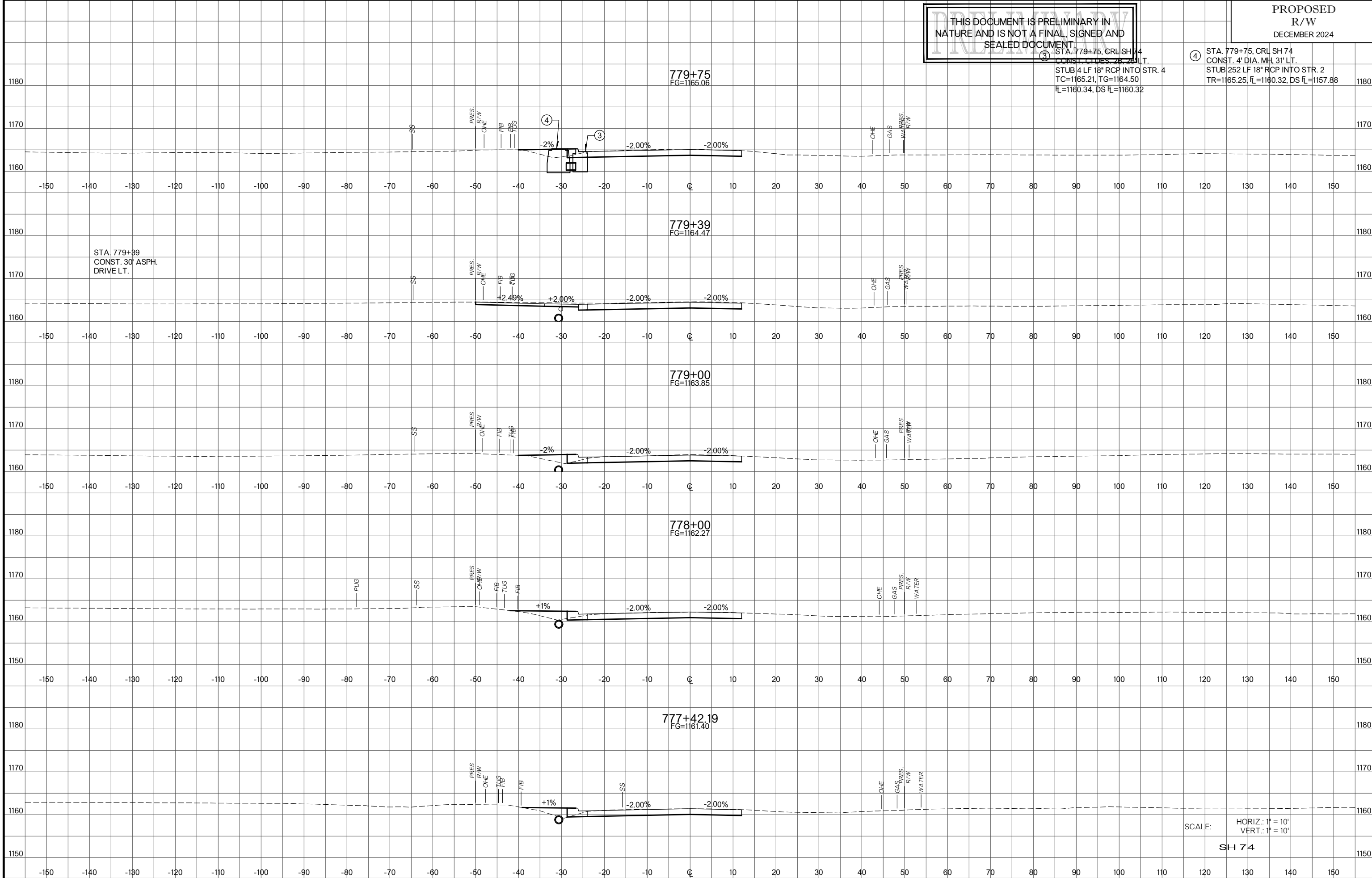
SH 74

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PROPOSED R/W  
DECEMBER 2024

③ STA. 779+75, CRL SH 74  
CONST. 4' DIA. MH, 31' LT.  
STUB 4 LF 18" RCP INTO STR. 4  
TC=1165.21, TG=1164.50  
FL=1160.34, DS FL=1160.32

④ STA. 779+75, CRL SH 74  
CONST. 4' DIA. MH, 31' LT.  
STUB 252 LF 18" RCP INTO STR. 2  
TR=1165.25, FL=1160.32, DS FL=1157.88



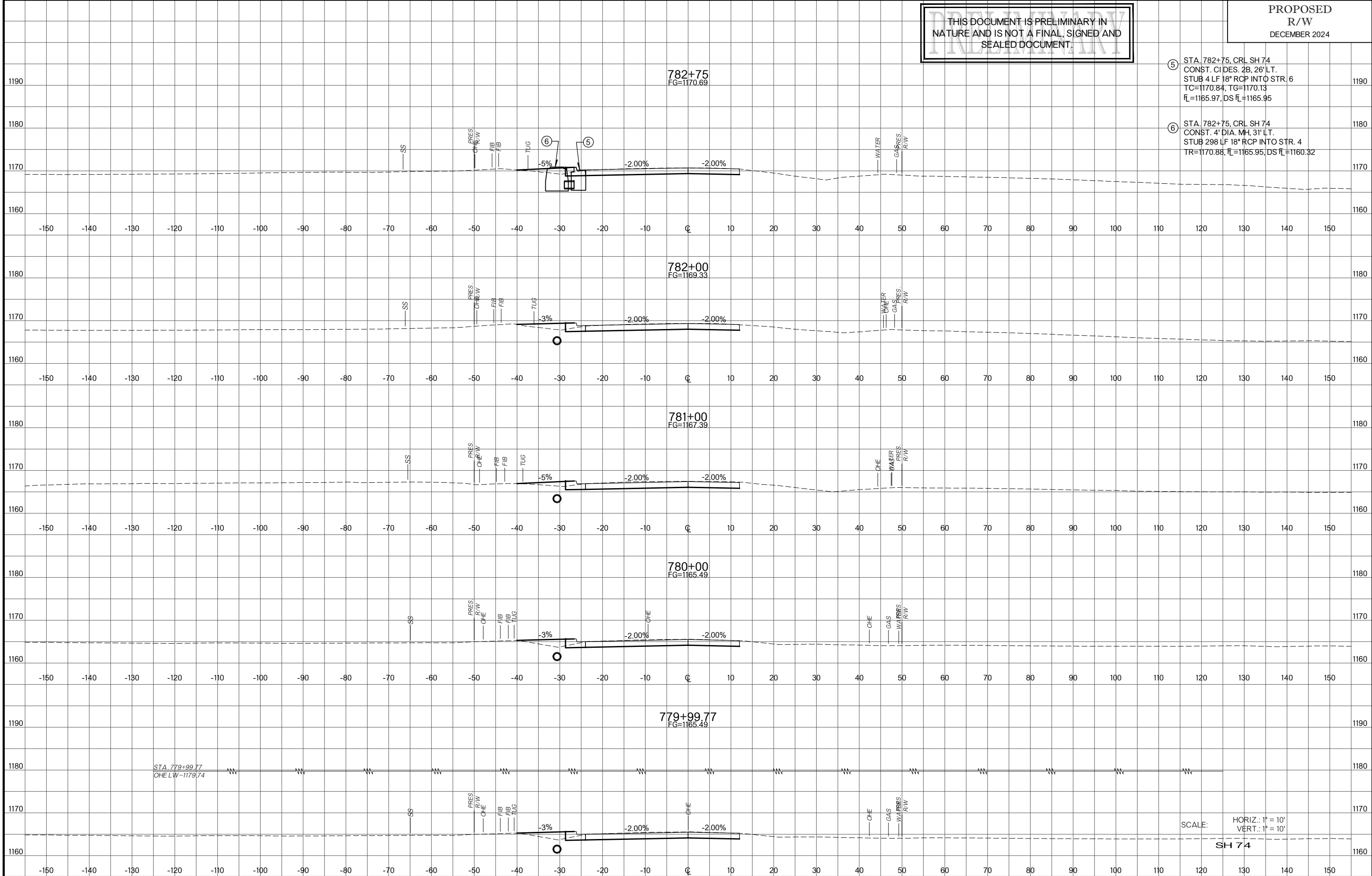
STA. 779+39  
CONST. 30' ASPH.  
DRIVE LT.

SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

SH 74

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PROPOSED R/W  
DECEMBER 2024



- ⑤ STA. 782+75, CRL SH 74  
CONST. CI DES. 2B, 26' LT.  
STUB 4 LF 18" RCP INTO STR. 6  
TC=1170.84, TG=1170.13  
FL=1165.97, DS FL=1165.95
- ⑥ STA. 782+75, CRL SH 74  
CONST. 4' DIA. MH, 31' LT.  
STUB 298 LF 18" RCP INTO STR. 4  
TR=1170.88, FL=1165.95, DS FL=1160.32

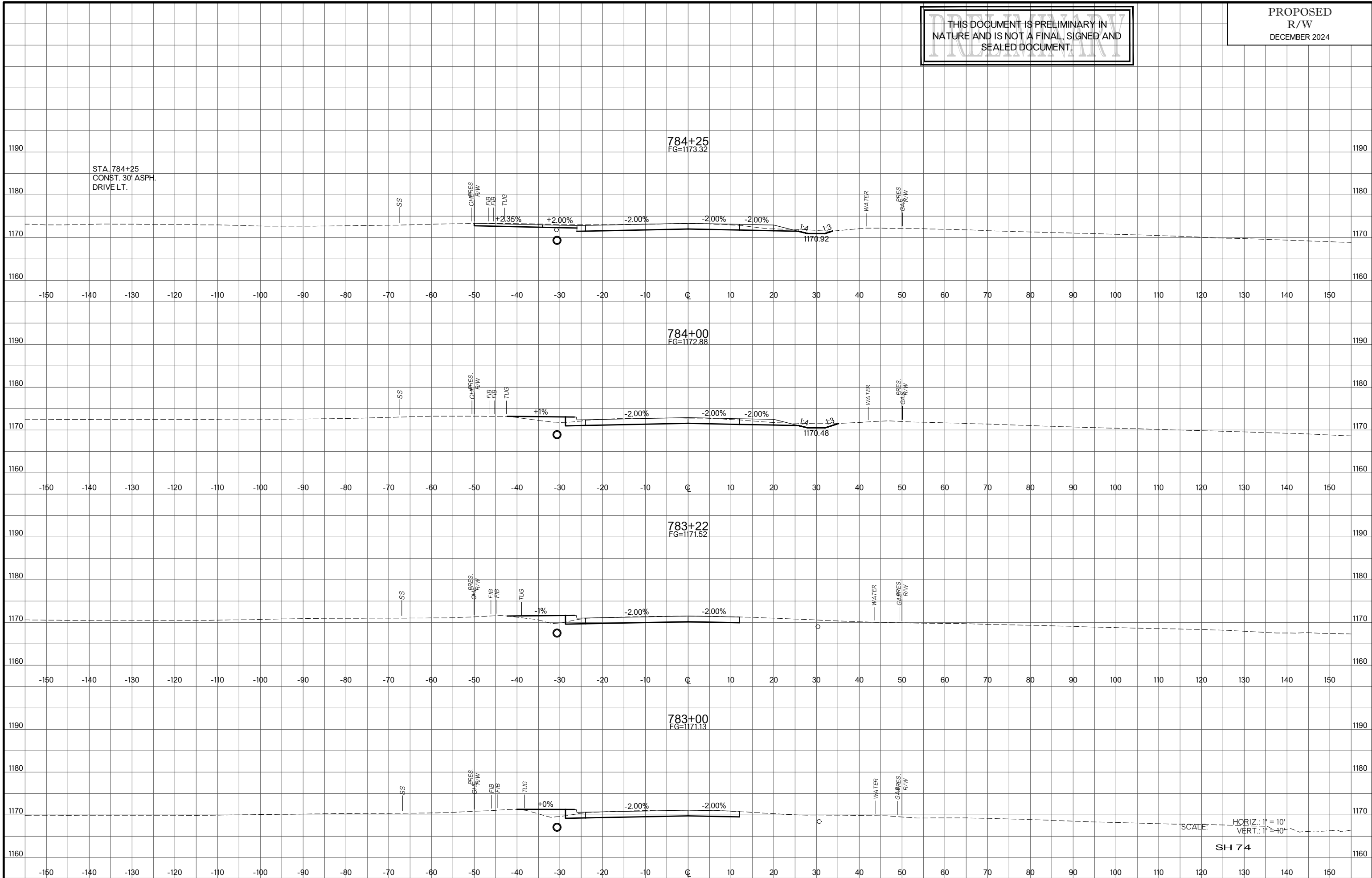
SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

SH 74

135/SH 74  
MCCLELLIN COUNTY

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PROPOSED R/W  
DECEMBER 2024



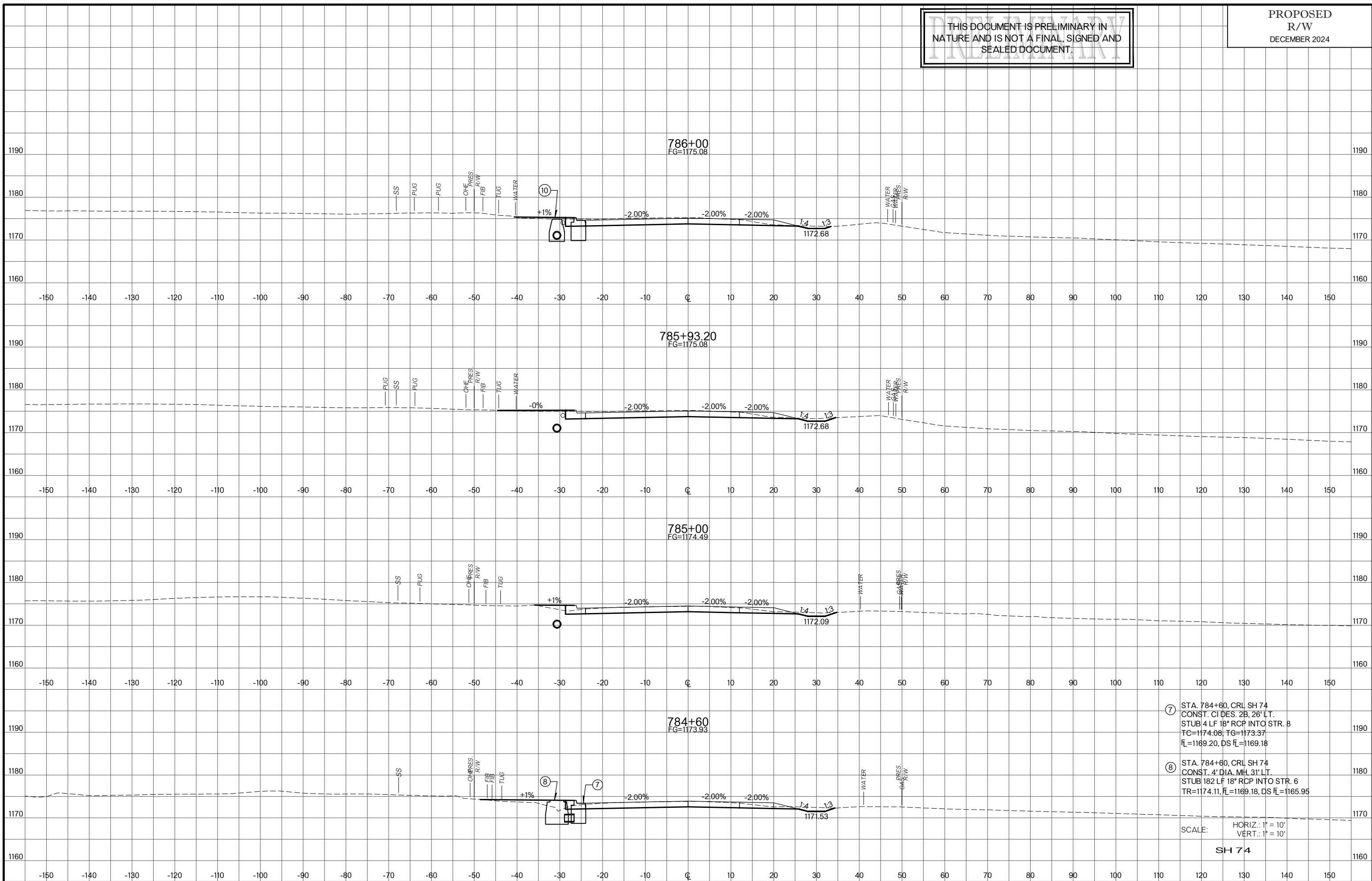
STA. 784+25  
CONST. 30' ASPH.  
DRIVE LT.

SCALE  
HORIZ.: 1" = 10'  
VERT.: 1" = 40'

SH 74

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PROPOSED R/W  
DECEMBER 2024



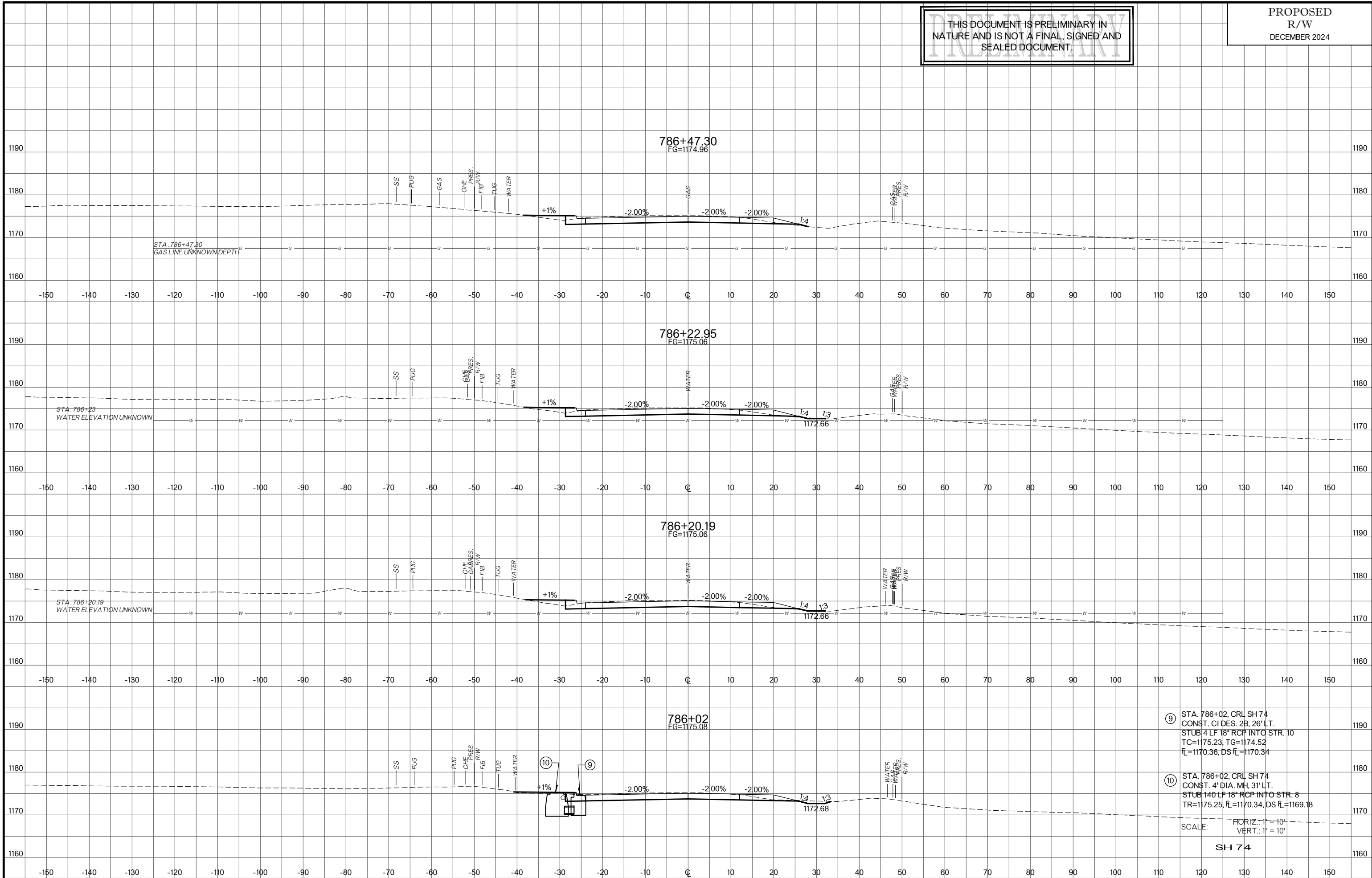
- ⑦ STA. 784+60, CRL SH 74  
CONST. CI DES. 2B, 26" LT.  
STUB 4 LF 18" RCP INTO STR. 8  
TC=1174.08, TG=1173.37  
FL=1169.20, DS FL=1169.18
- ⑧ STA. 784+60, CRL SH 74  
CONST. 4' DIA. MH, 31' LT.  
STUB 182 LF 18" RCP INTO STR. 6  
TR=1174.11, FL=1169.18, DS FL=1165.95

SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

SH 74

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PROPOSED R/W  
DECEMBER 2024



⑨ STA. 786+02, CRL SH 74  
CONST. CI DES. 2B, 26' LT.  
STUB 4 LF 18" RCP INTO STR. 10  
TC=1175.23, TG=1174.52  
FL=1170.36, DS FL=1170.34

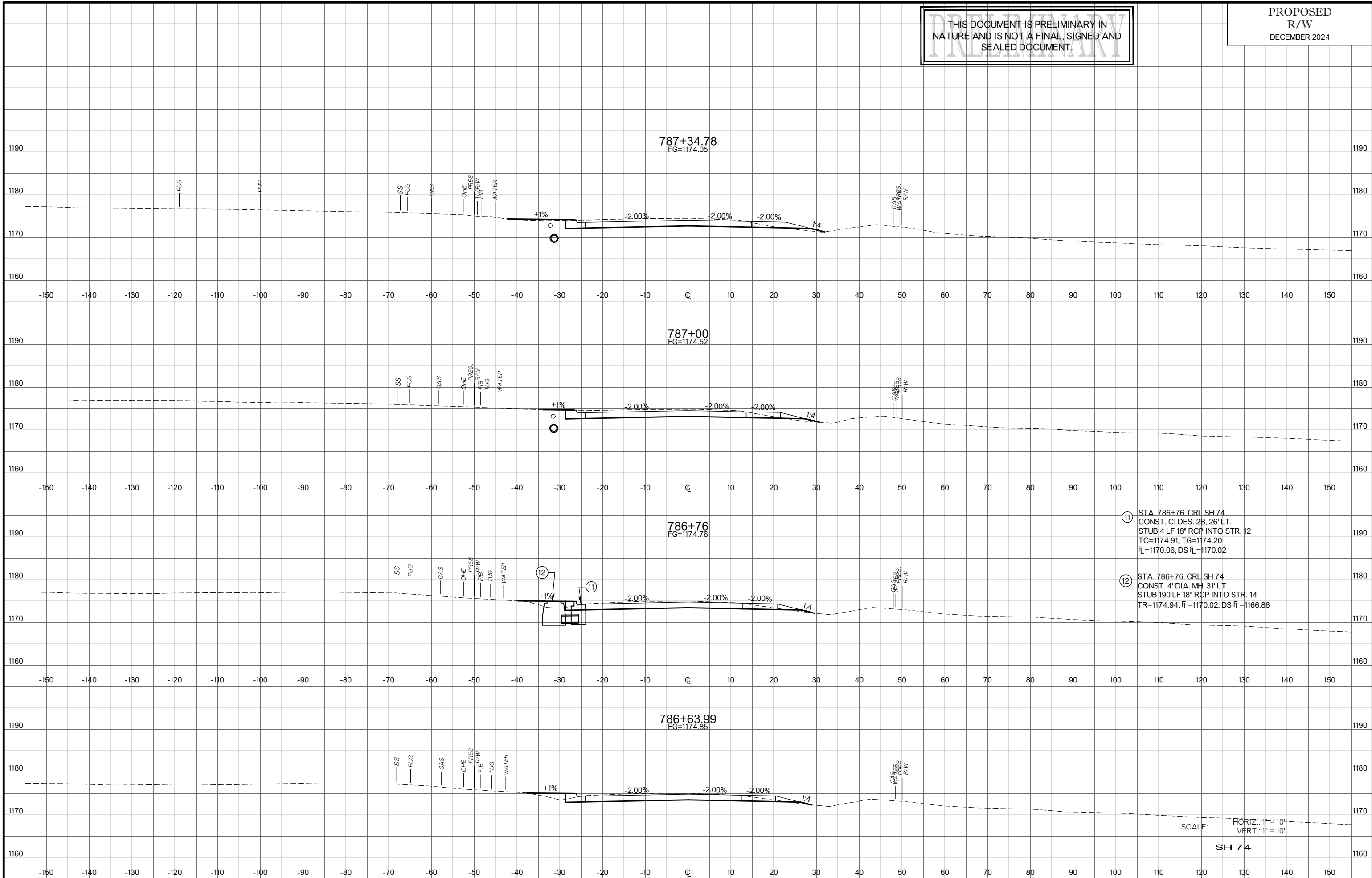
⑩ STA. 786+02, CRL SH 74  
CONST. 4' DIA. MH, 31' LT.  
STUB 140 LF 18" RCP INTO STR. 8  
TR=1175.25, FL=1170.34, DS FL=1169.18

SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

SH 74

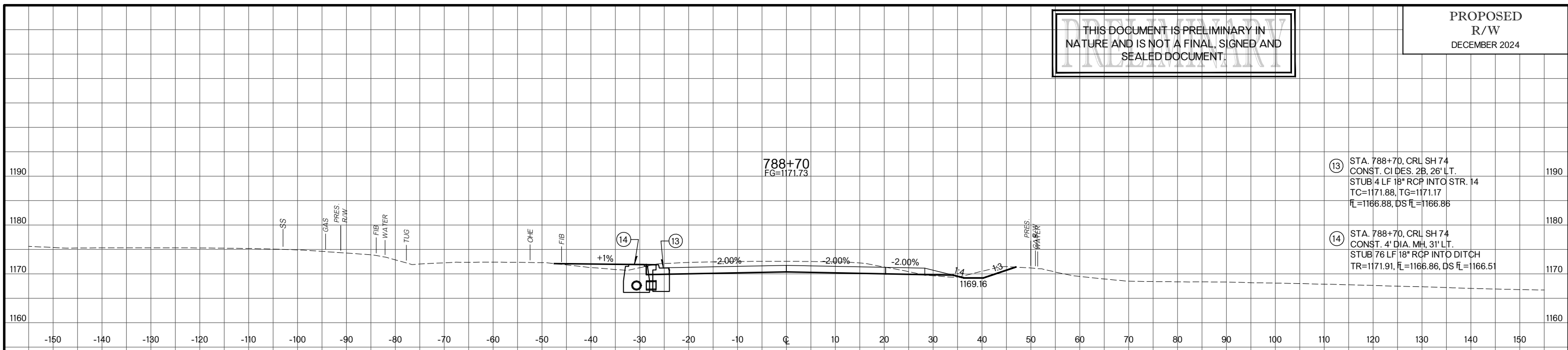
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PROPOSED R/W  
DECEMBER 2024

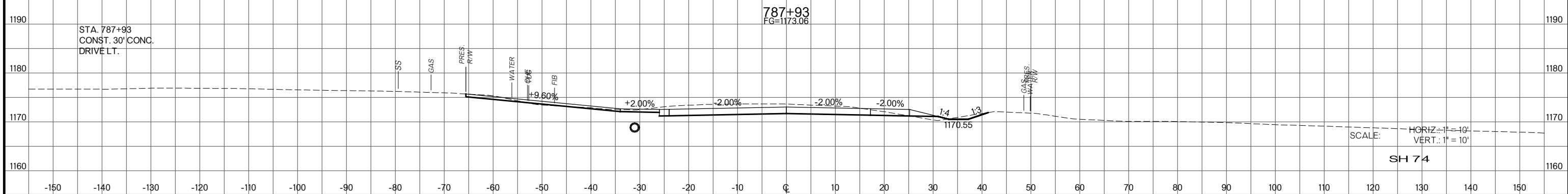
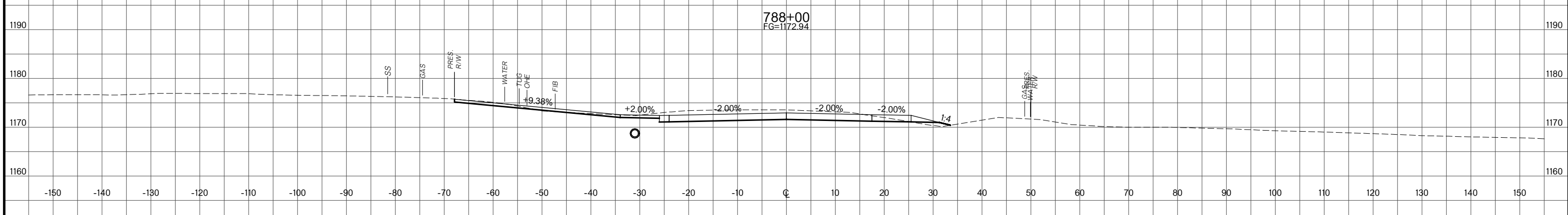
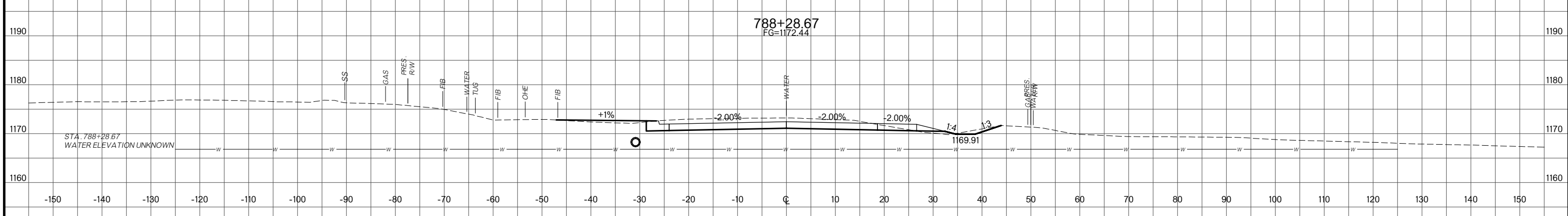


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PROPOSED R/W  
DECEMBER 2024



- 13 STA. 788+70, CRL SH 74  
CONST. CI DES. 2B, 26' LT.  
STUB 4 LF 18" RCP INTO STR. 14  
TC=1171.88, TG=1171.17  
FL=1166.88, DS FL=1166.86
- 14 STA. 788+70, CRL SH 74  
CONST. 4' DIA. MH, 31' LT.  
STUB 76 LF 18" RCP INTO DITCH  
TR=1171.91, FL=1166.86, DS FL=1166.51

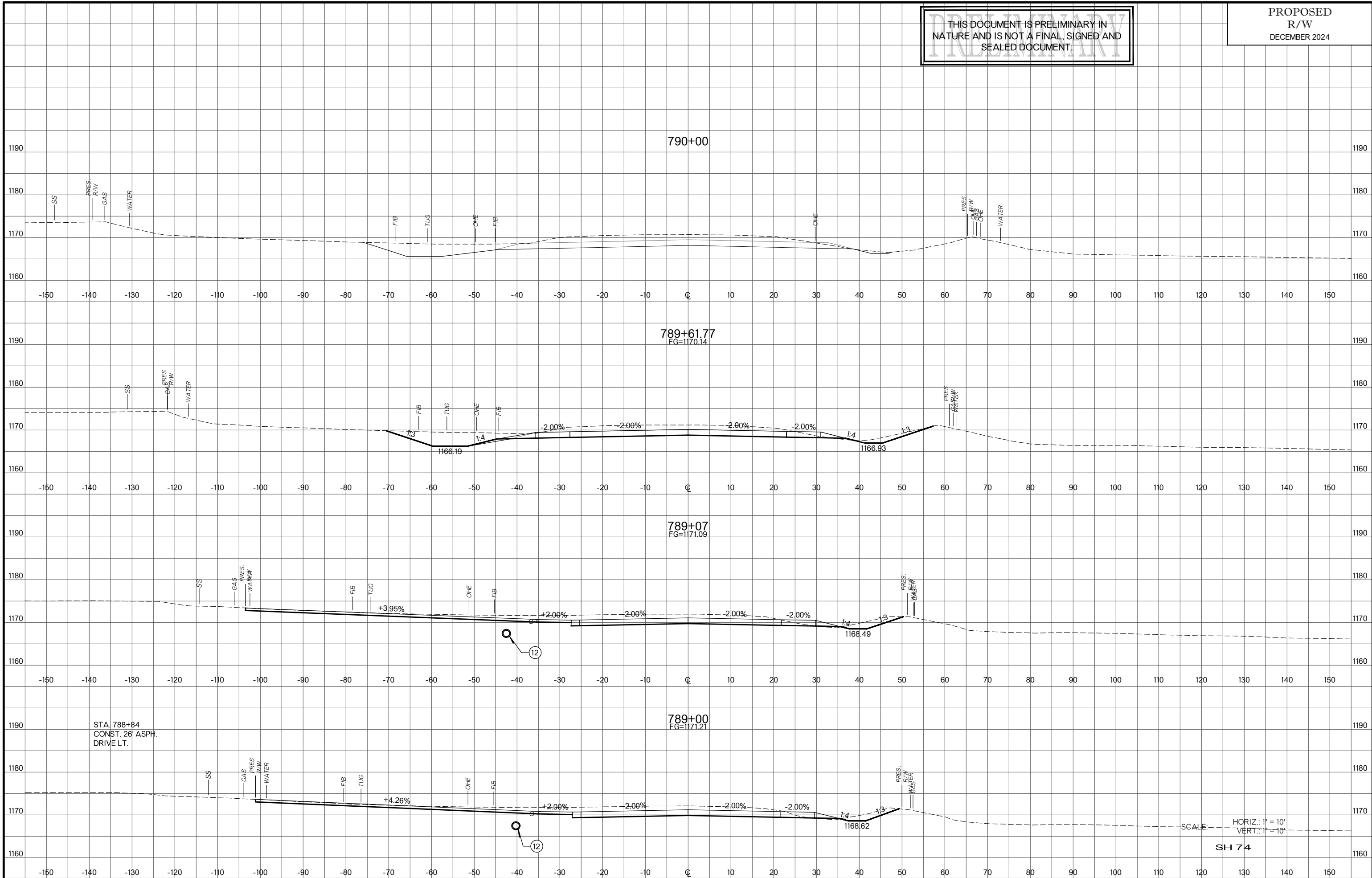


SCALE: HORIZ.: 1" = 40'  
VERT.: 1" = 10'

SH 74

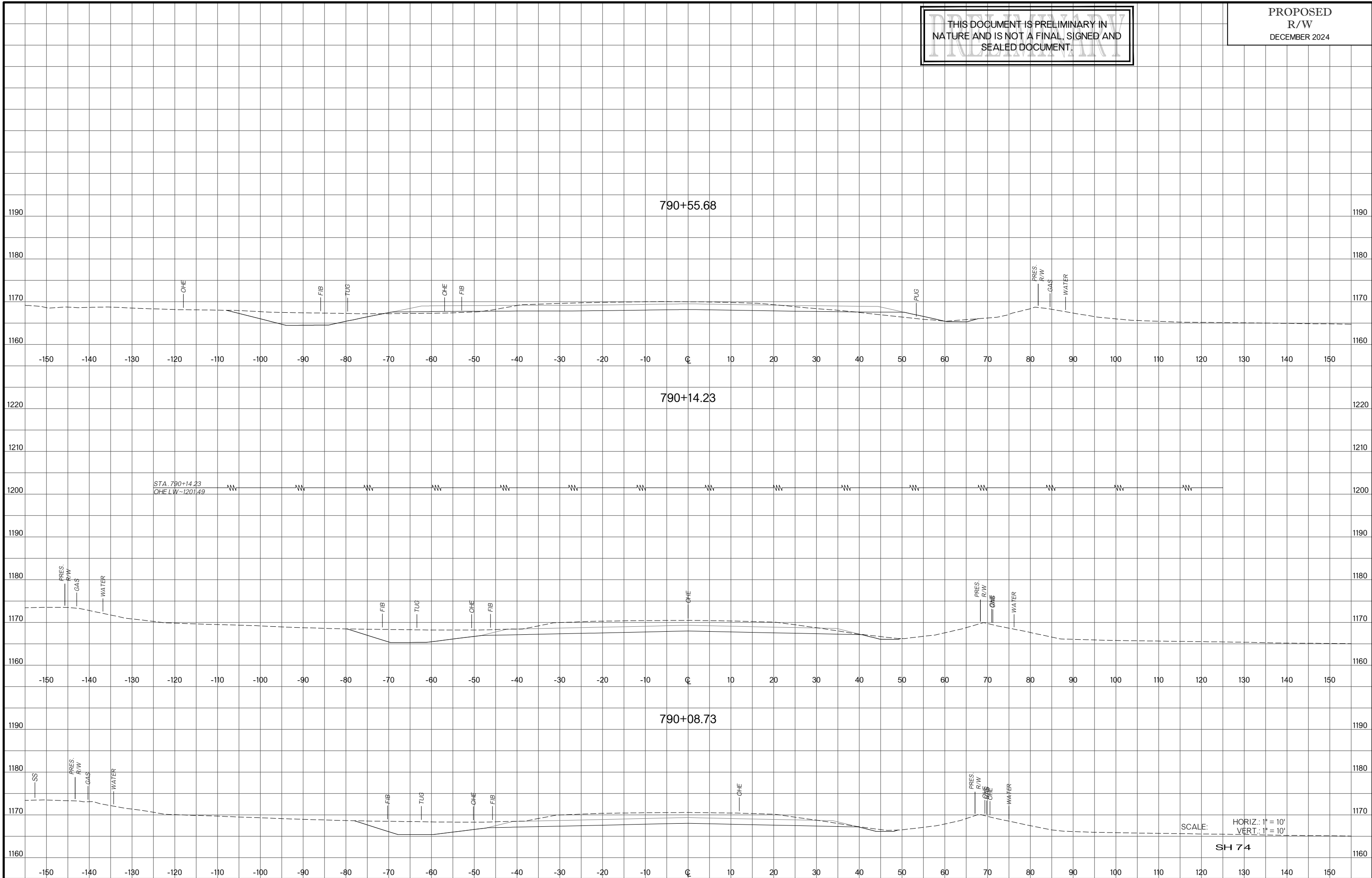
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PROPOSED R/W  
DECEMBER 2024



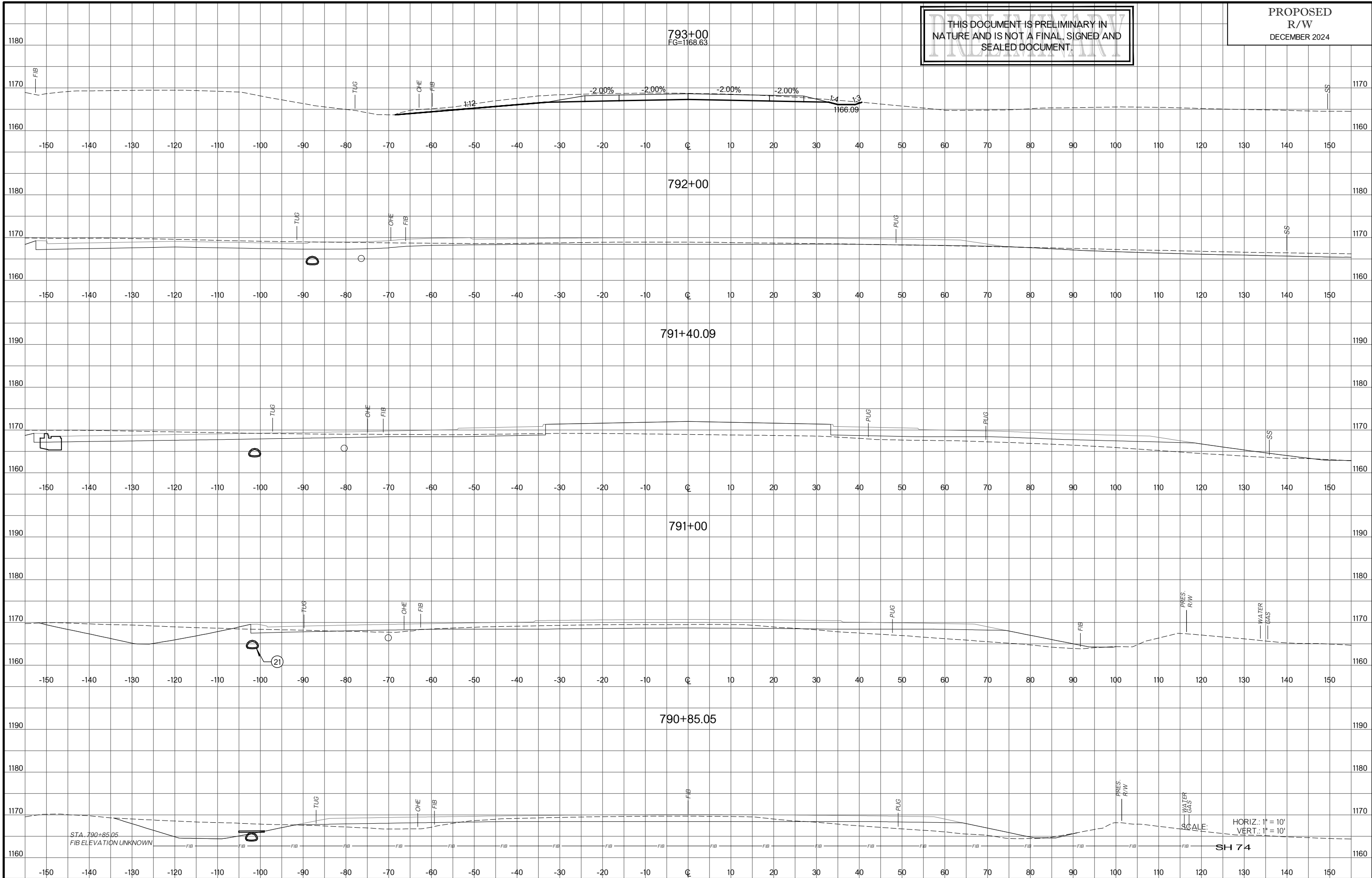
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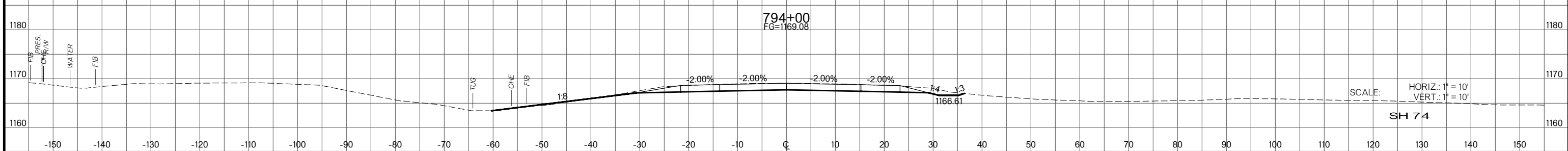
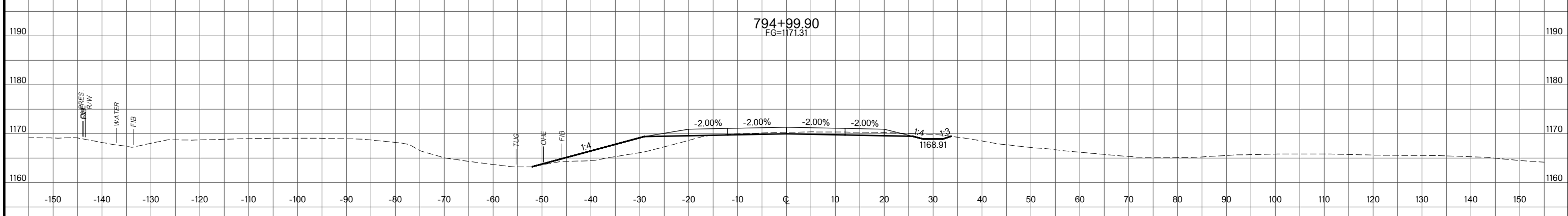
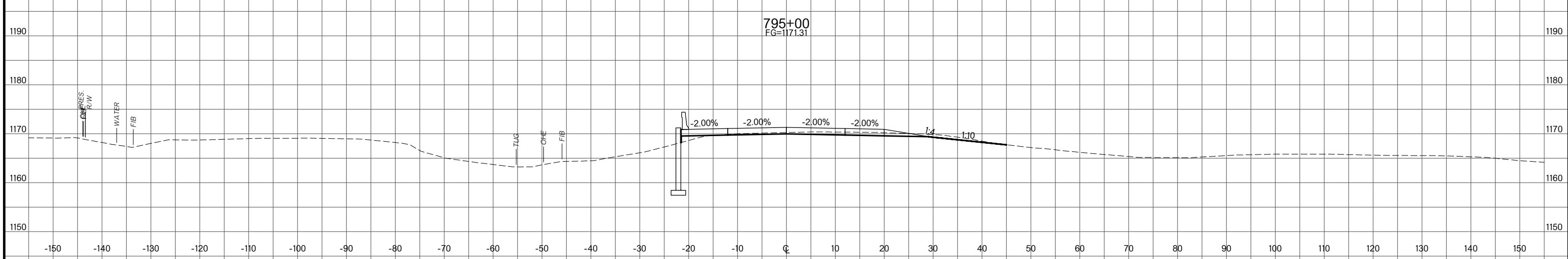
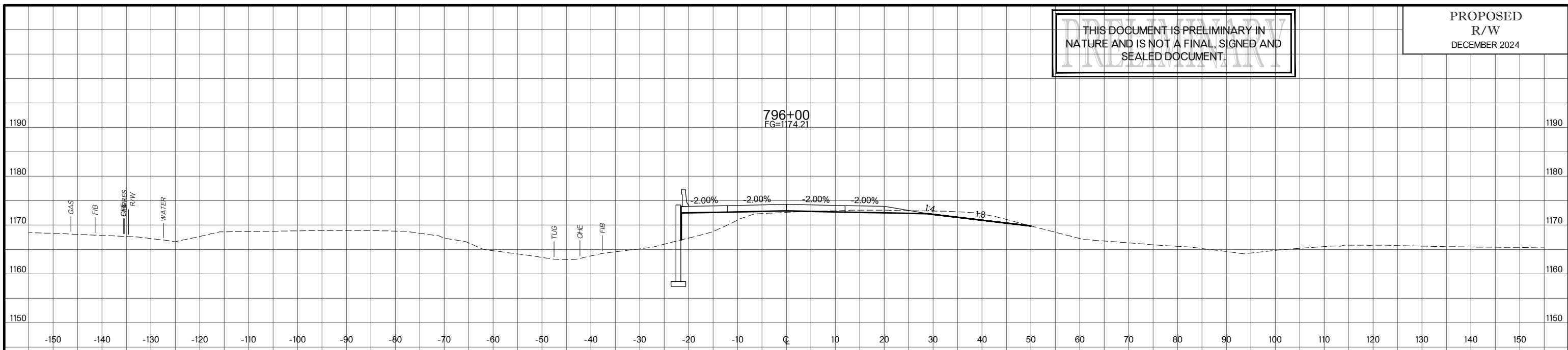
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DECEMBER 2024



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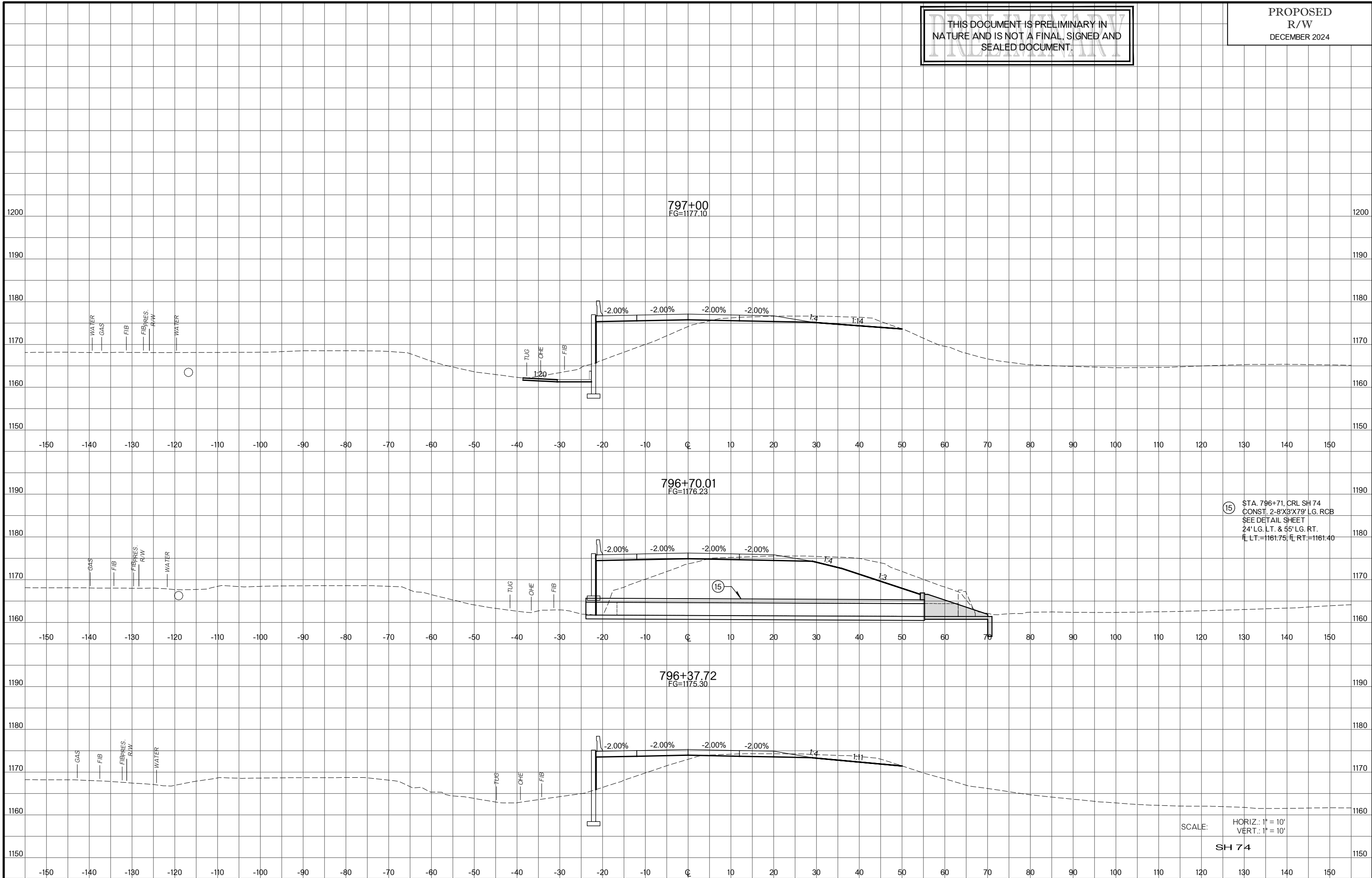


SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

SH 74

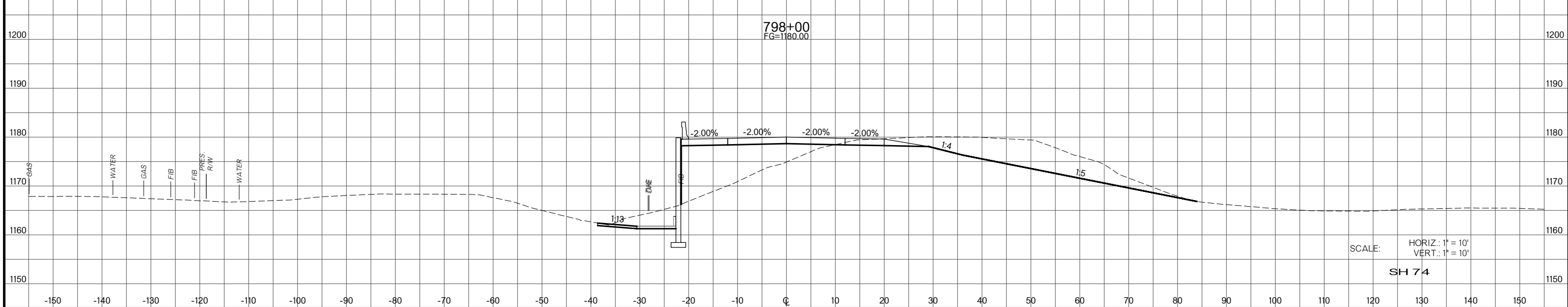
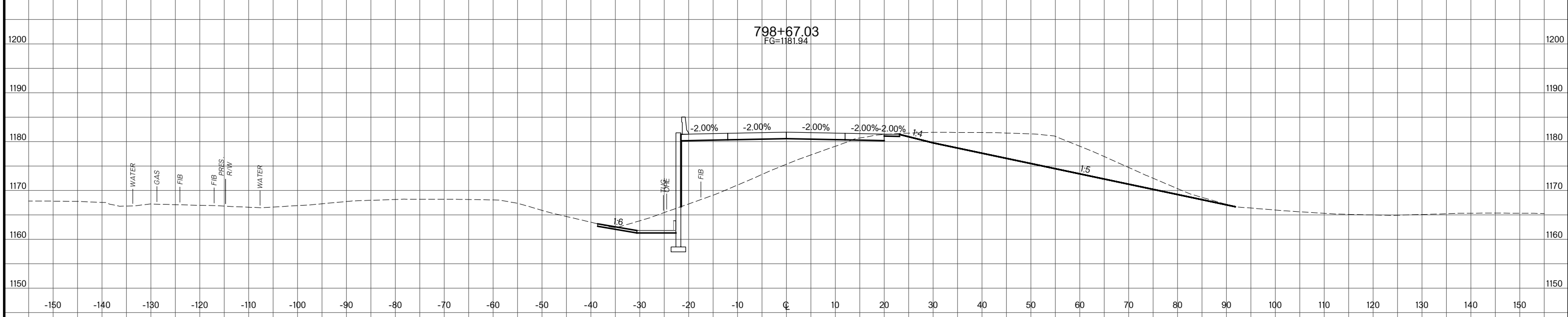
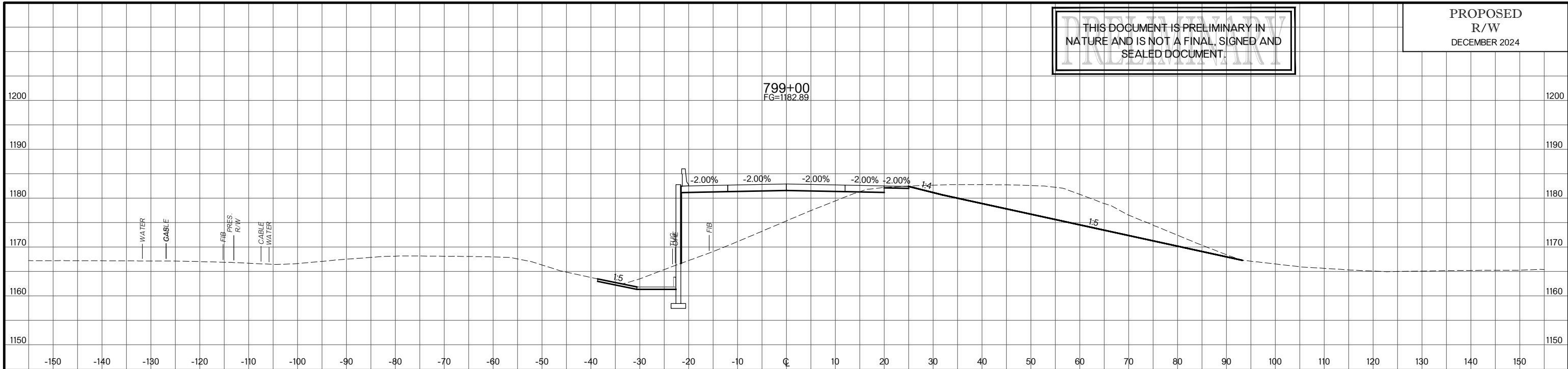
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DECEMBER 2024



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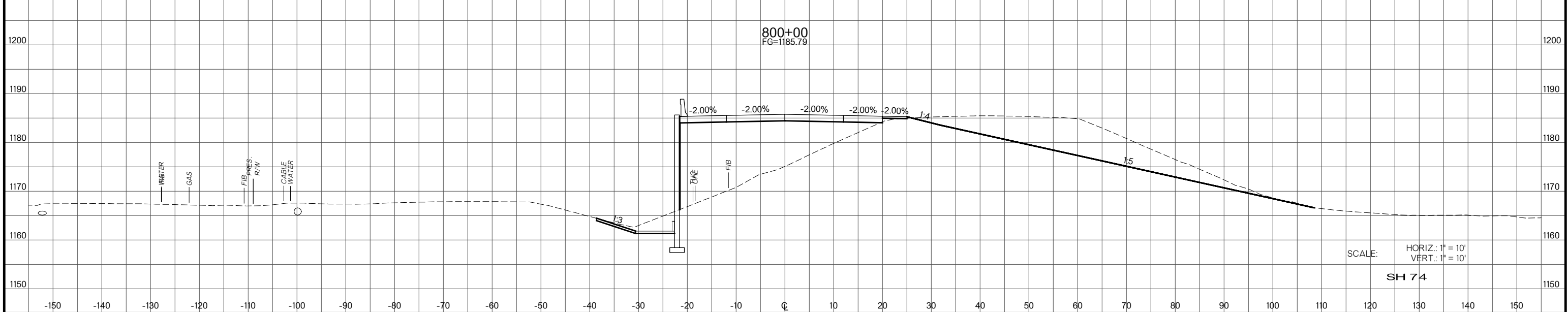
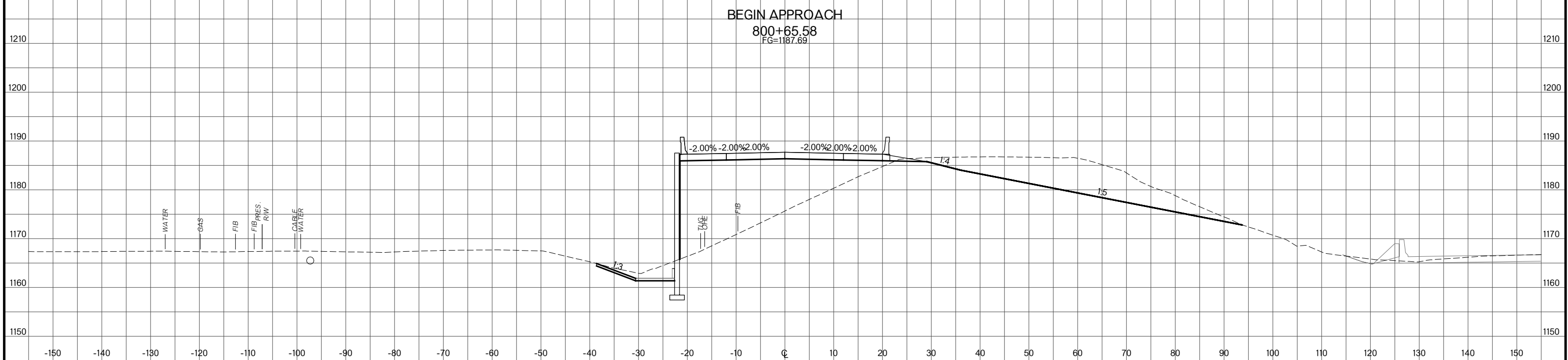
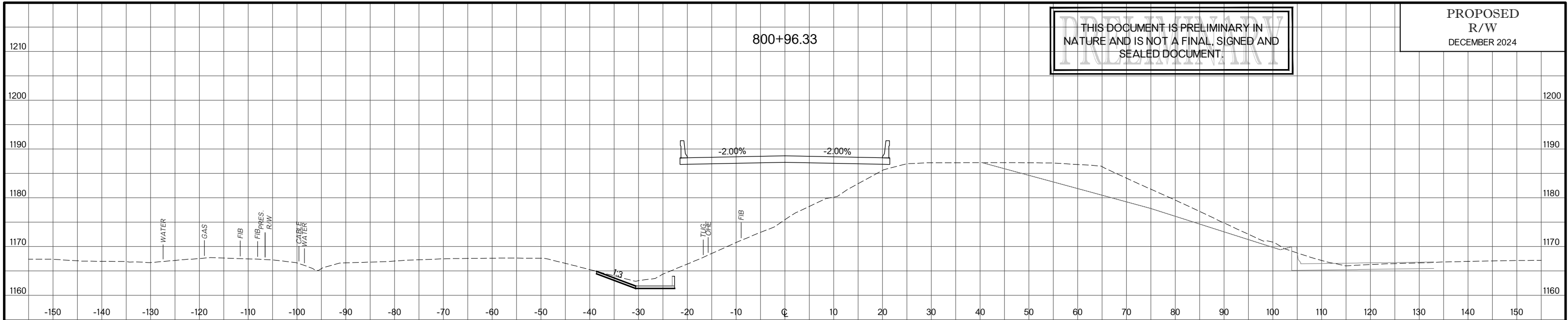
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DECEMBER 2024



SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

SH 74

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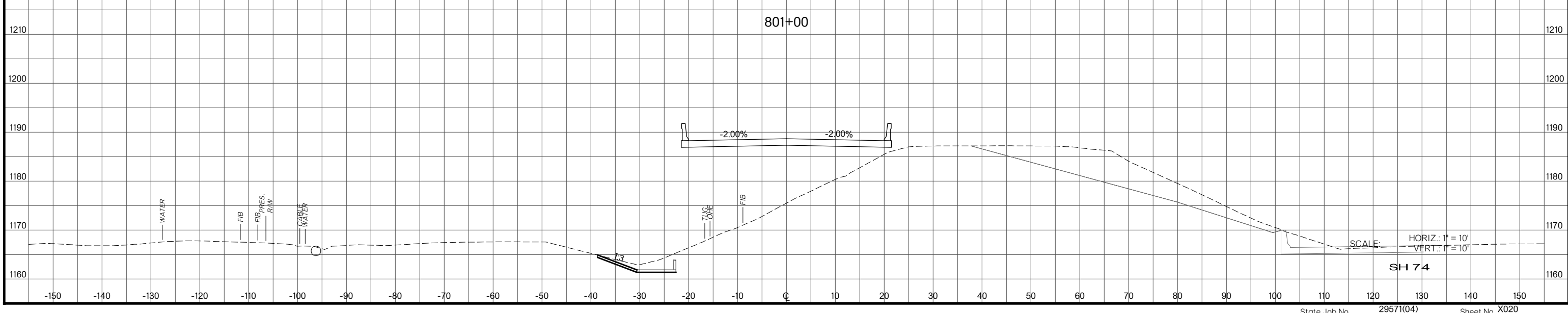
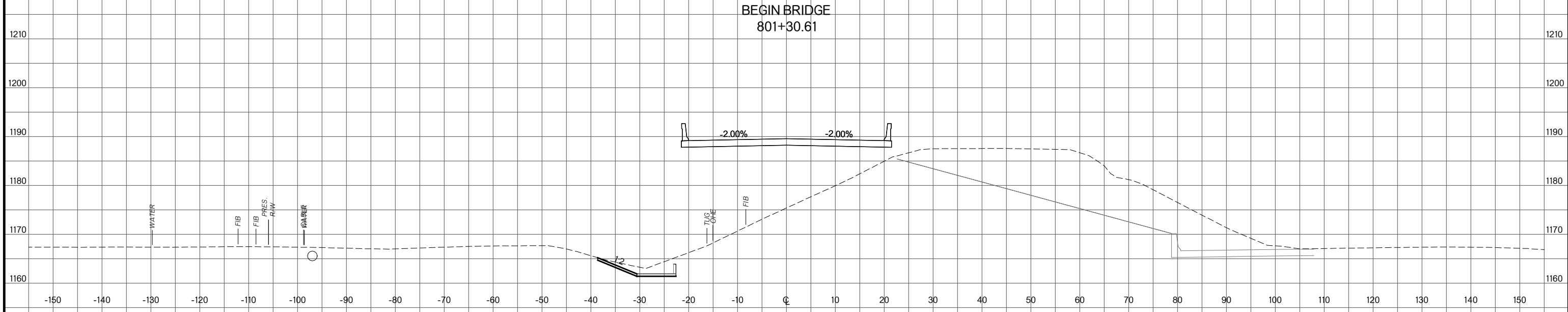
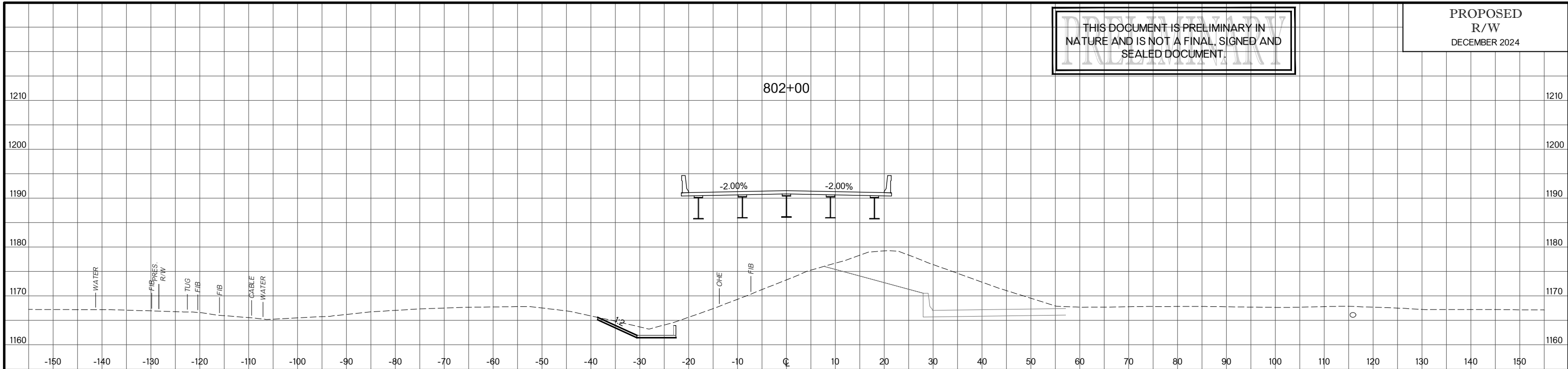


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SH 74

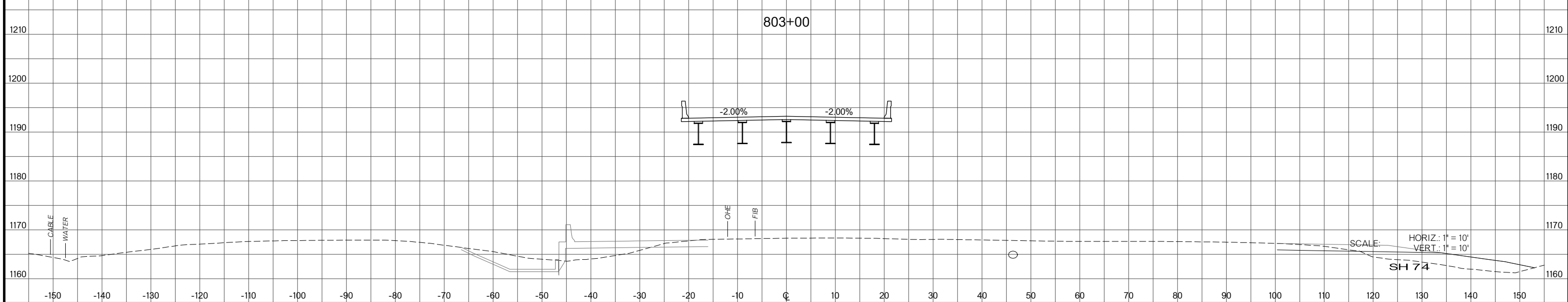
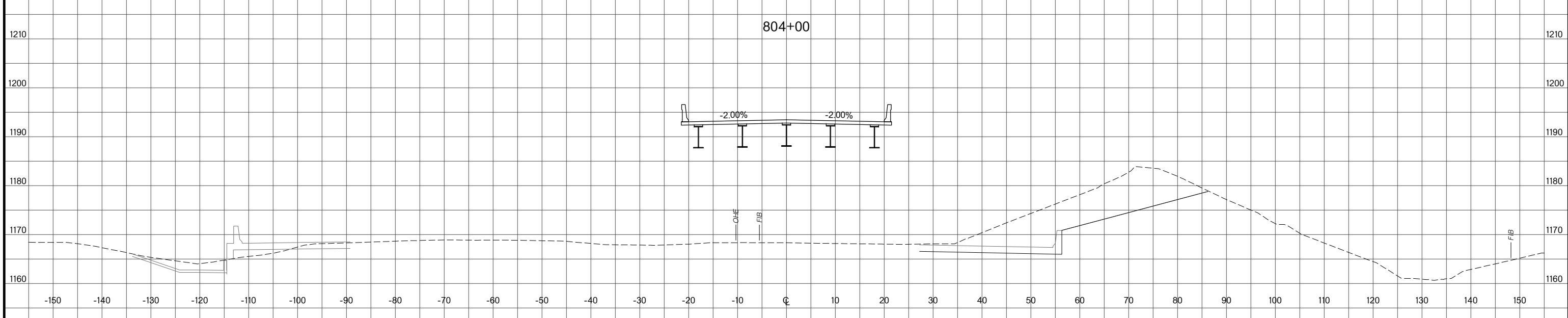
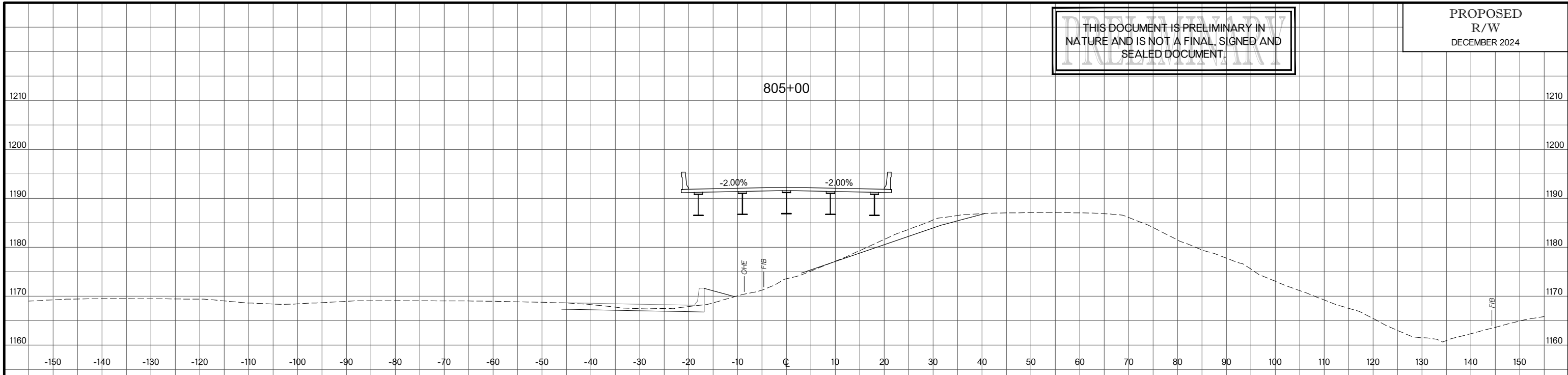
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PROPOSED R/W  
DECEMBER 2024



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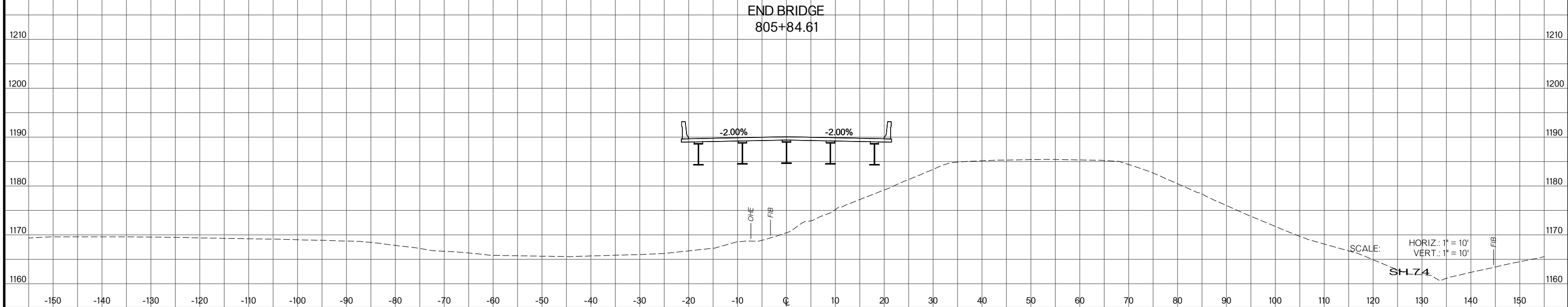
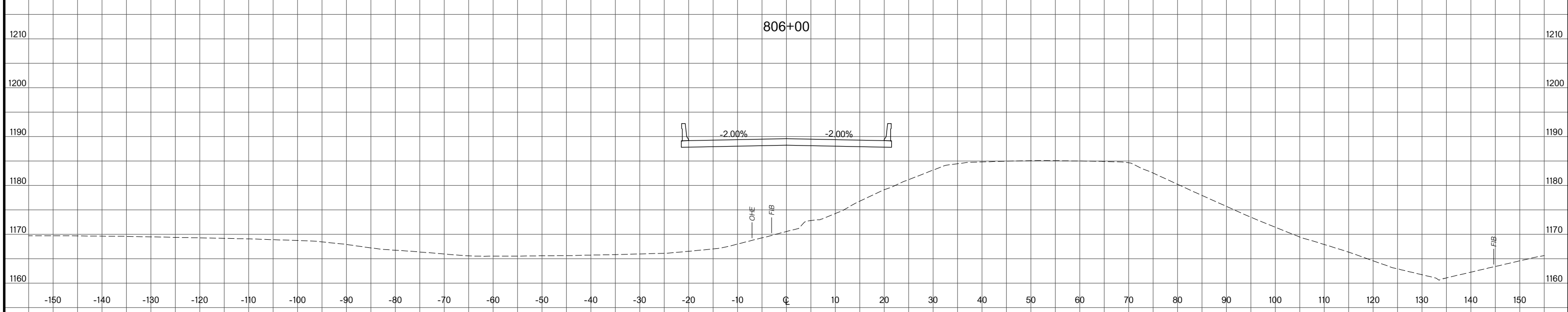
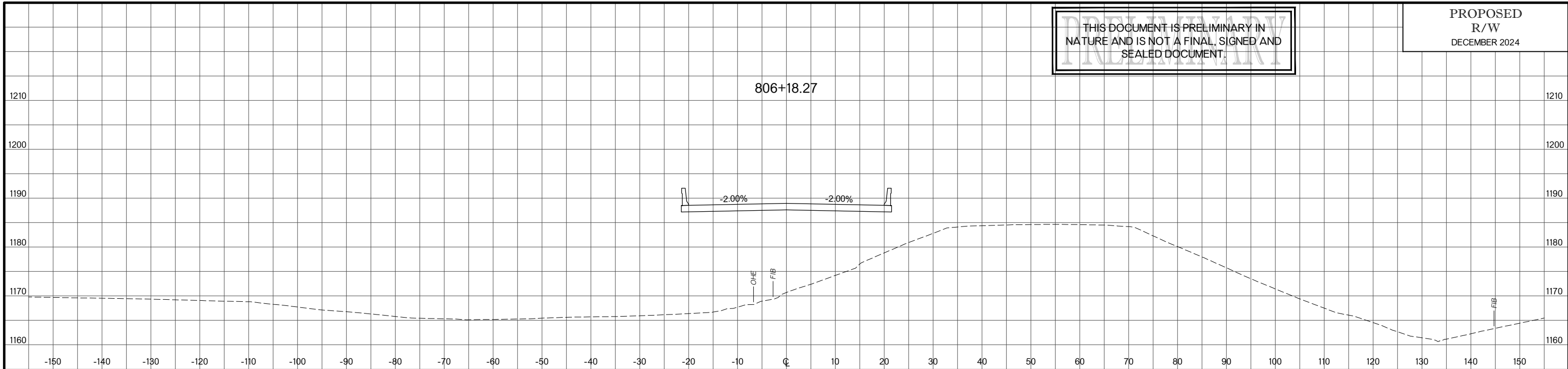
PROPOSED R/W  
DECEMBER 2024



SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

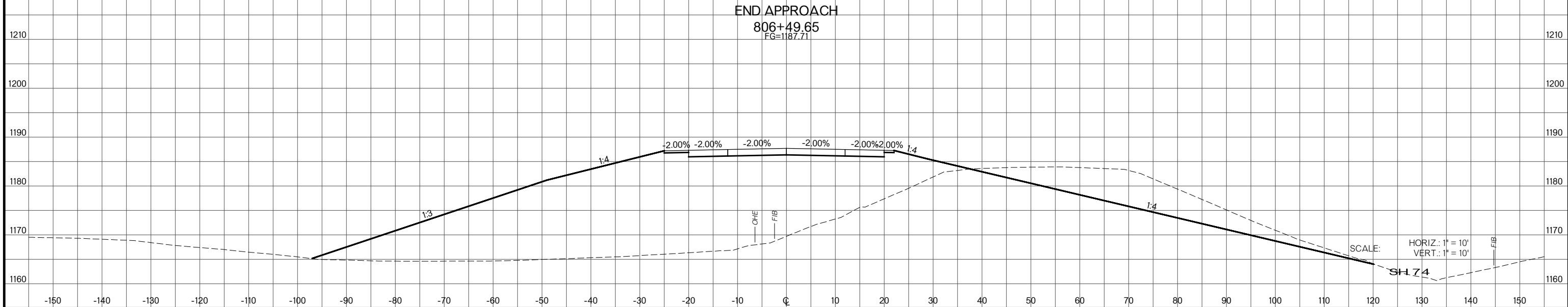
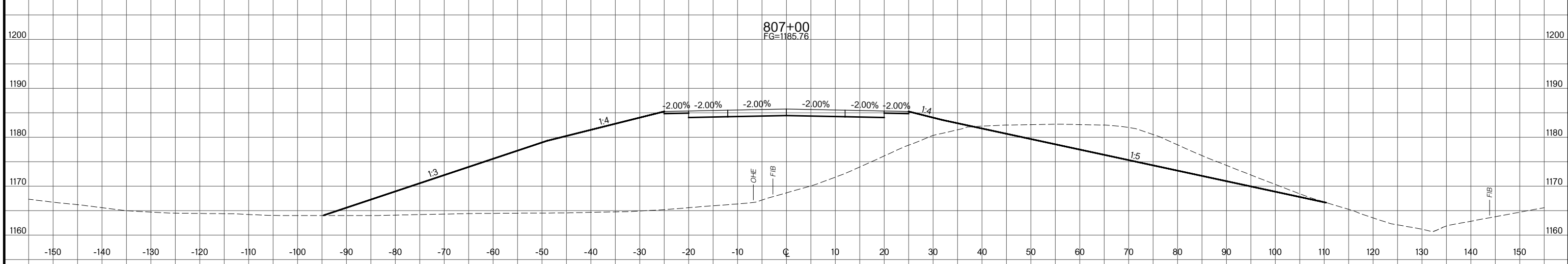
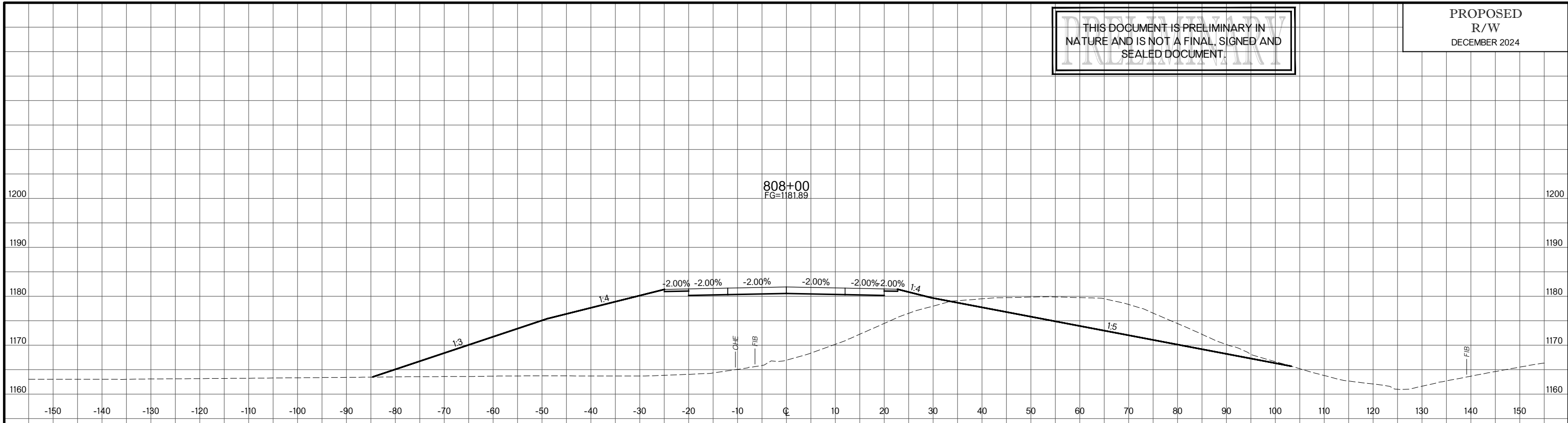
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PROPOSED R/W  
DECEMBER 2024



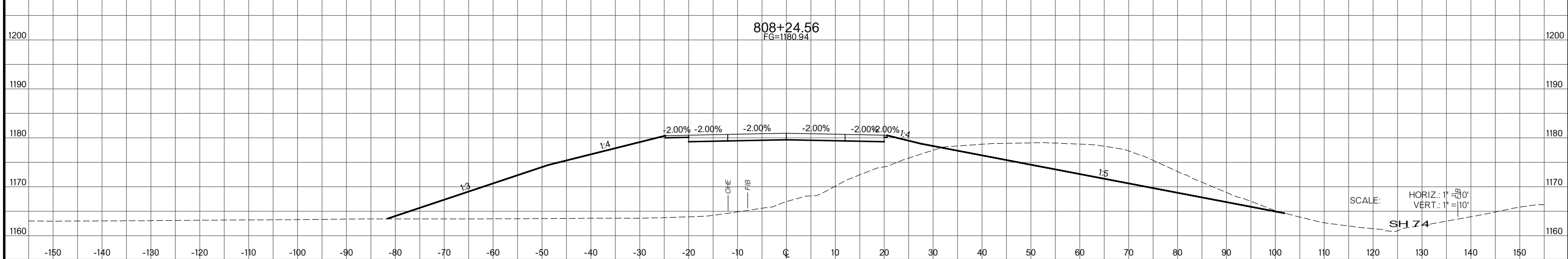
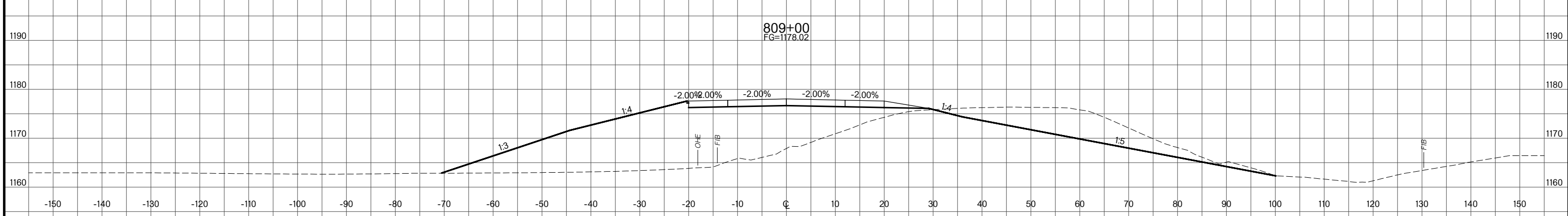
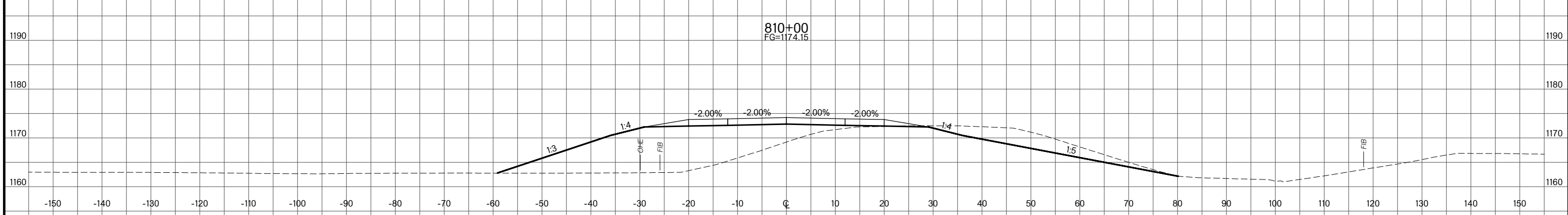
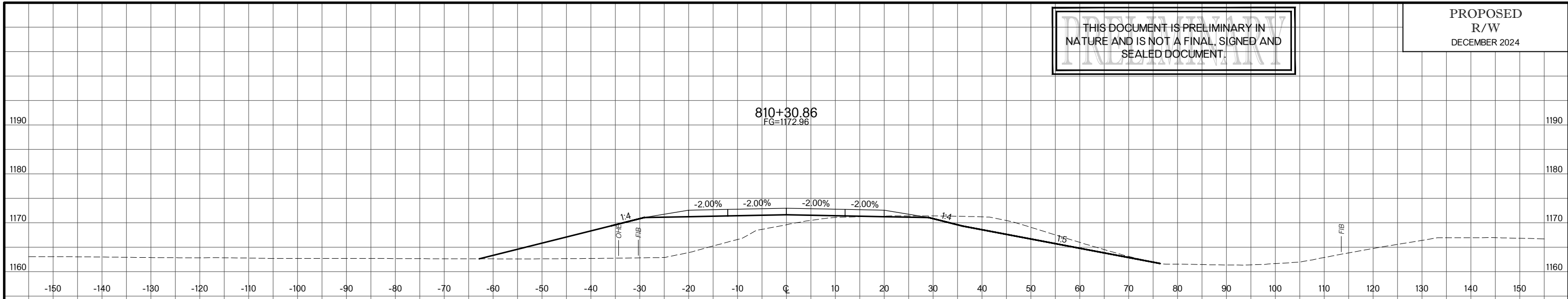
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PROPOSED R/W  
DECEMBER 2024



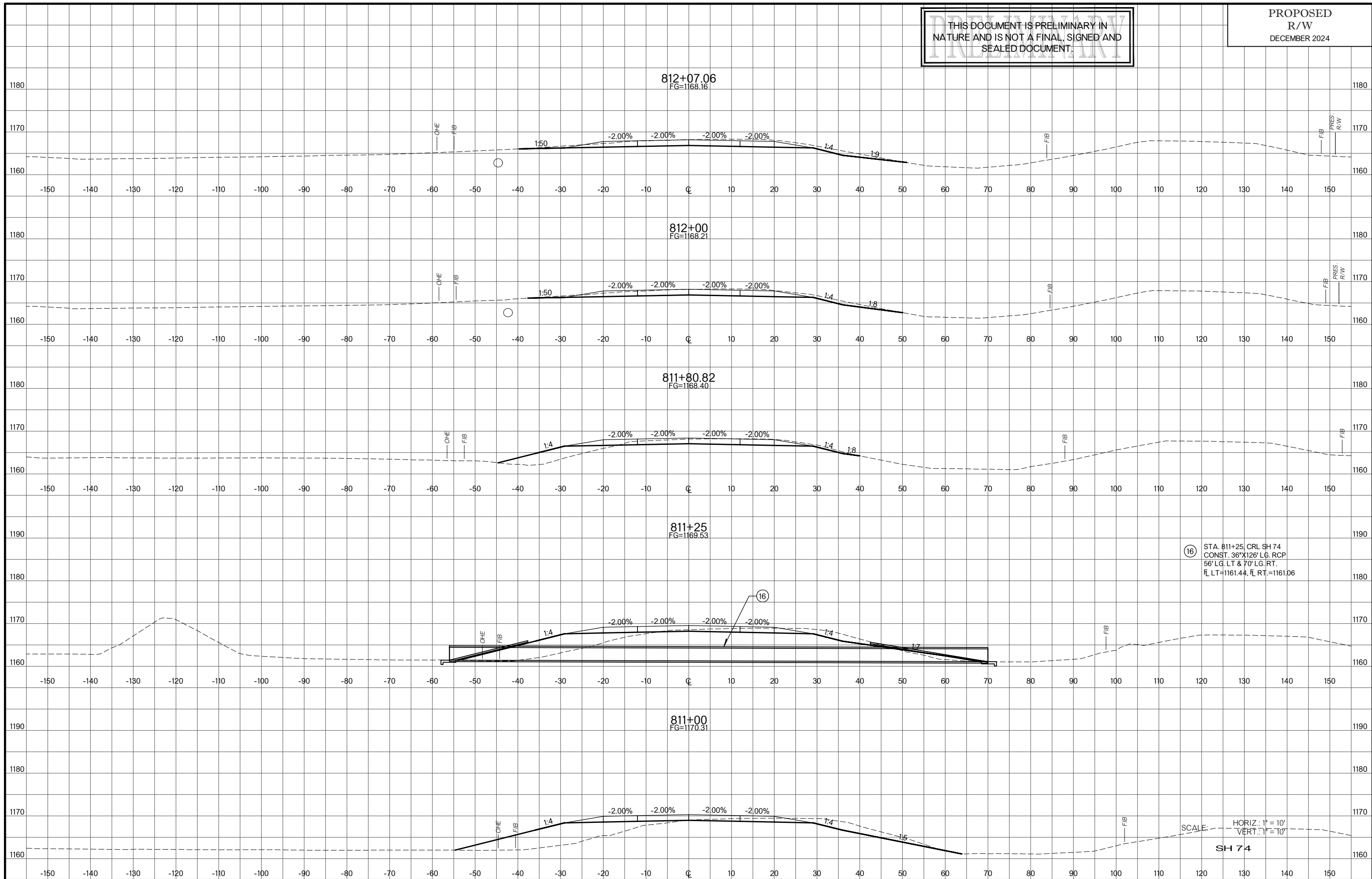
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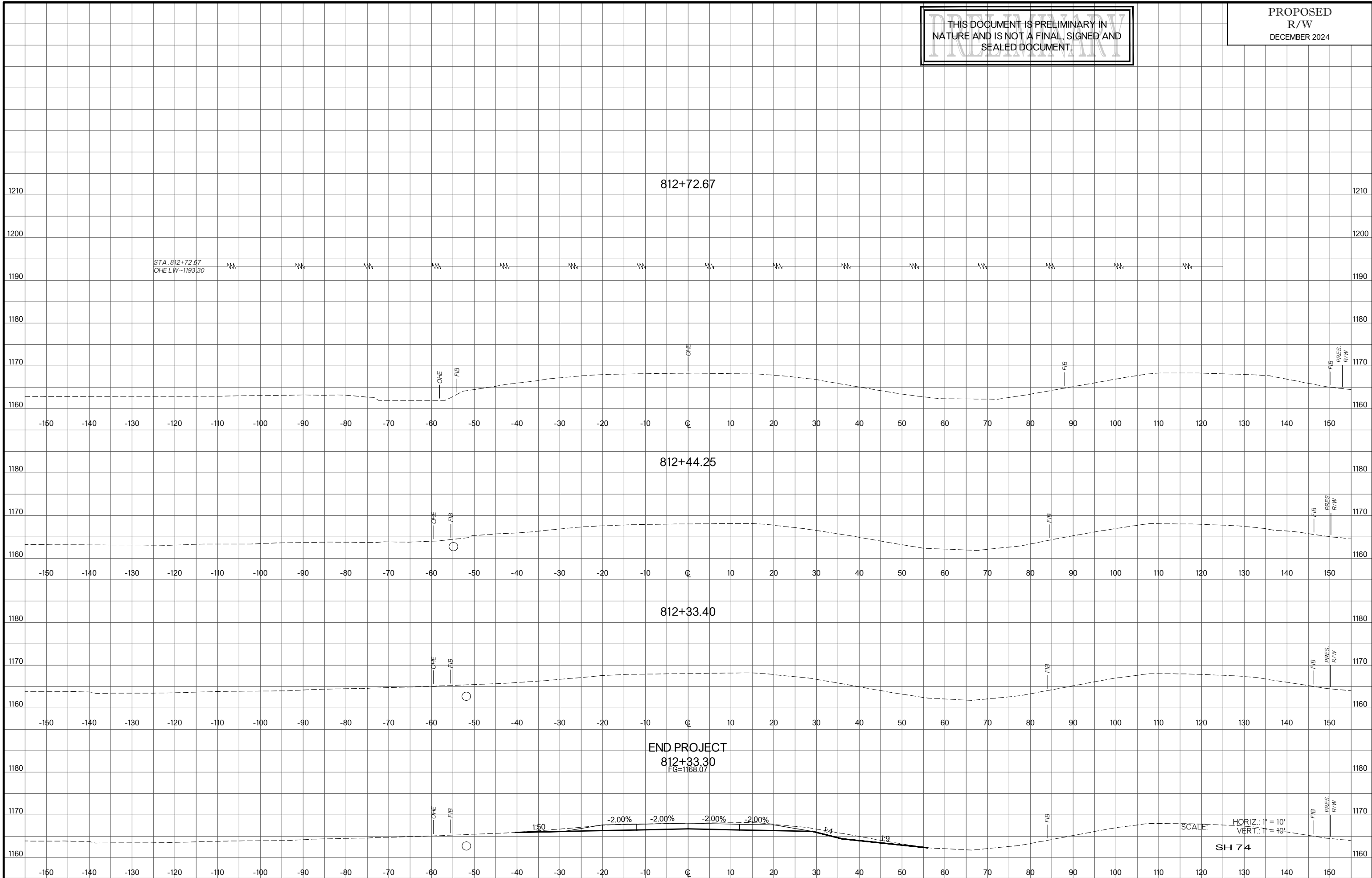
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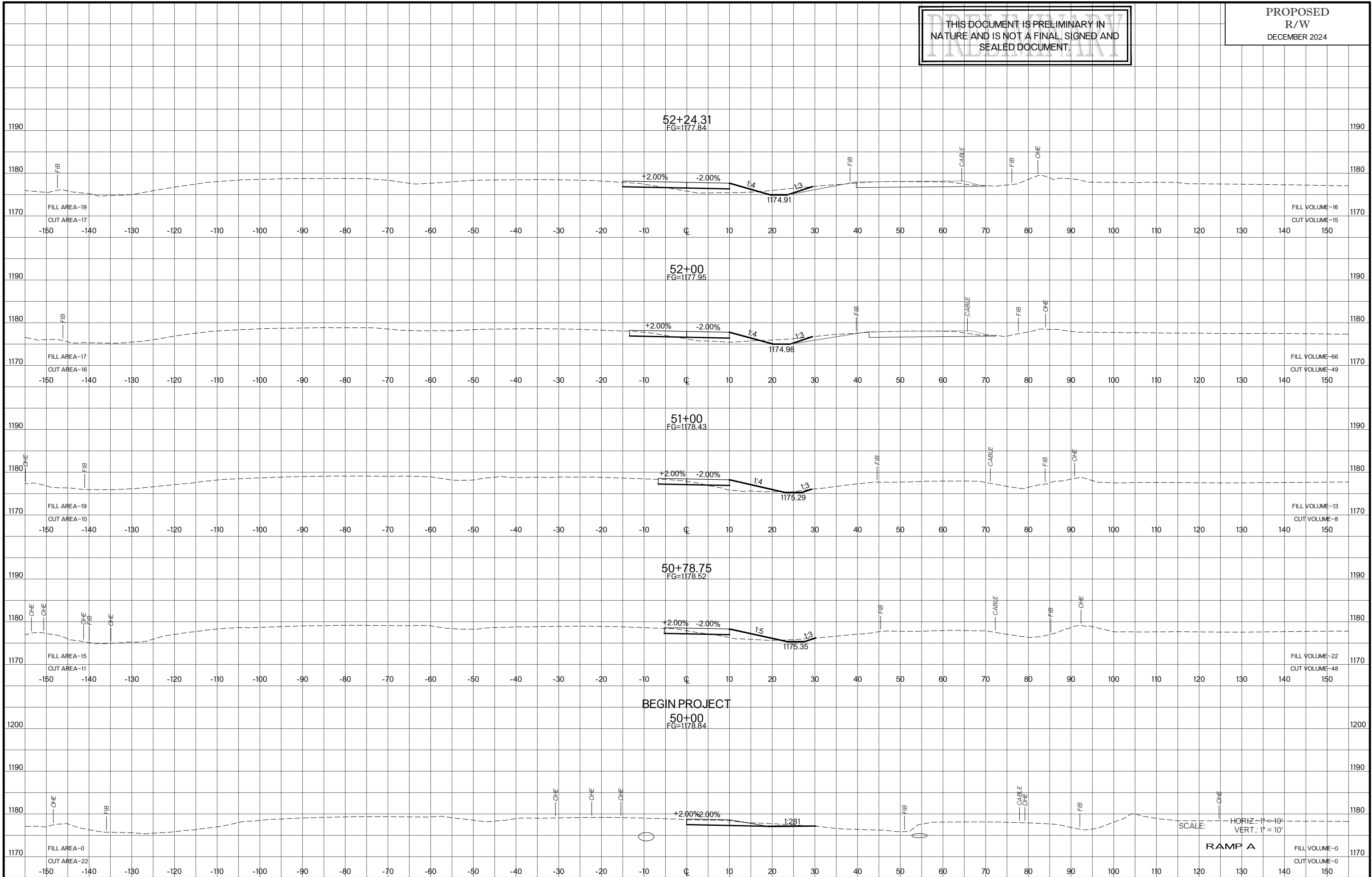
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DECEMBER 2024



BEGIN PROJECT  
50+00  
FG=1178.84

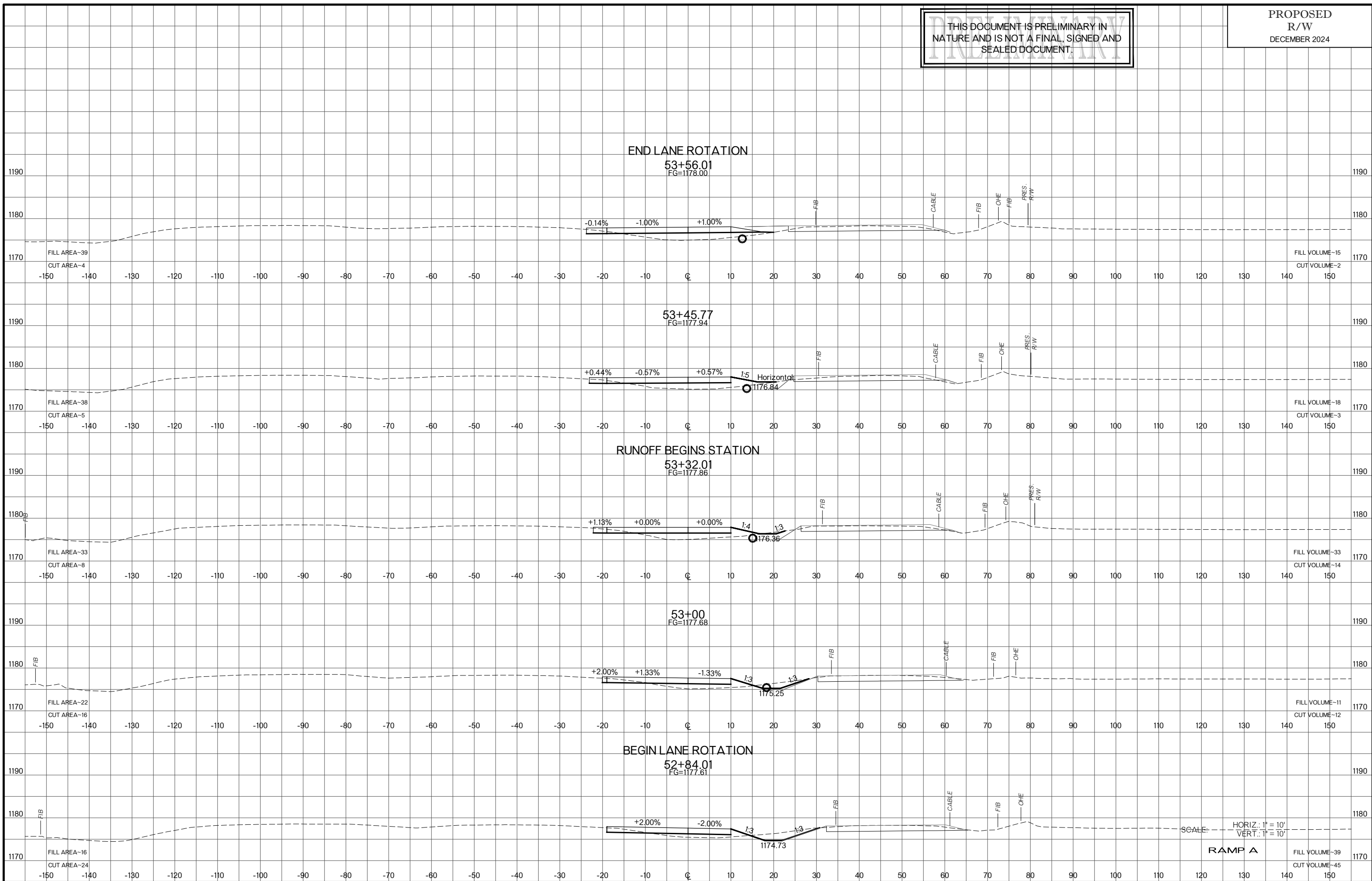
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VERT.: 1" = 10'

RAMP A

135/SH 74  
MCCLAIN COUNTY

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DECEMBER 2024



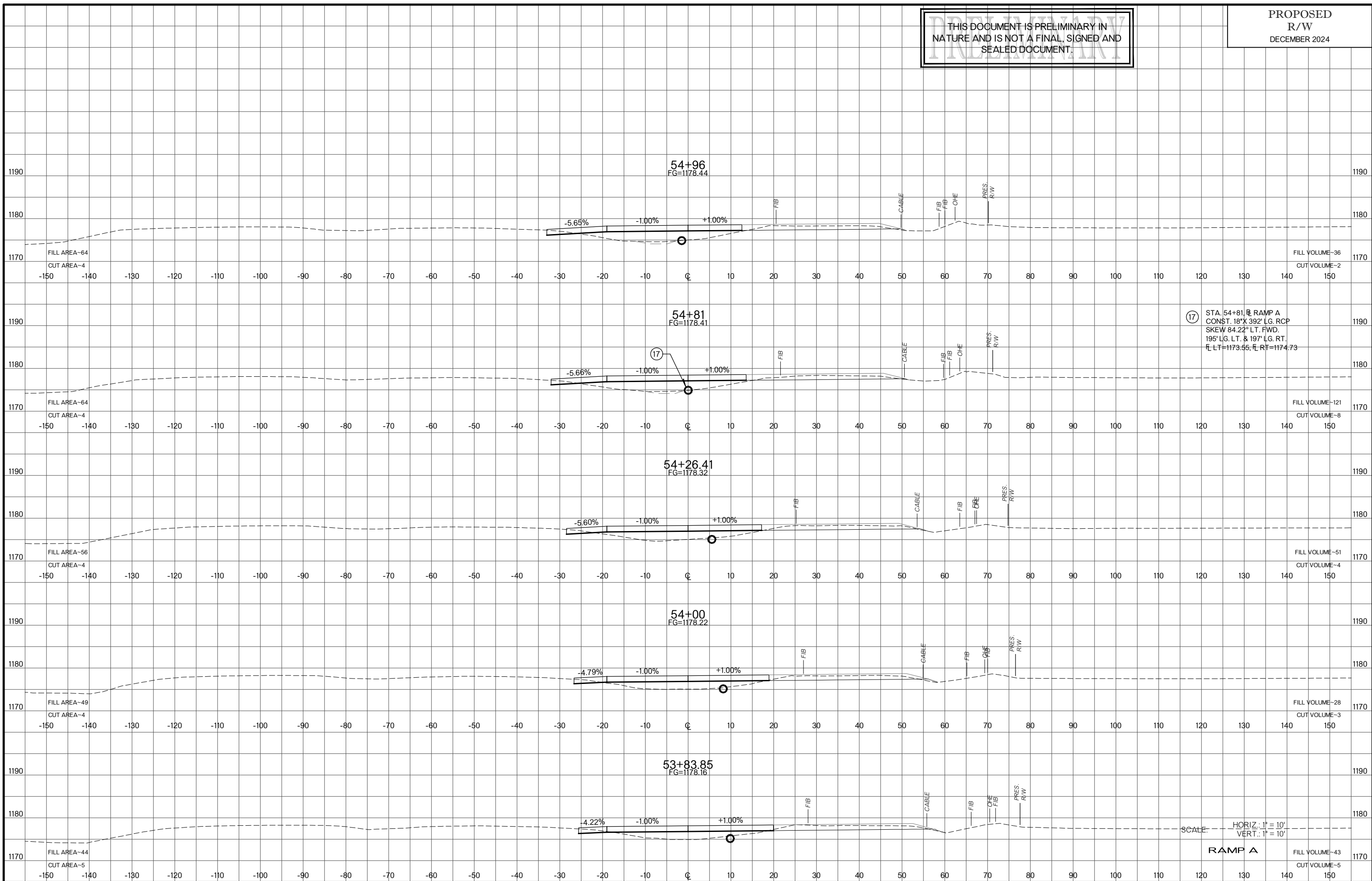
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VERT.: 1" = 10'

RAMP A

MCCLAIN COUNTY

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17 STA. 54+81, RAMP A  
CONST. 18" X 392' LG. RCP  
SKEW 84.22° LT. FWD.  
195' LG. LT. & 197' LG. RT.  
FL LT=1173.55, FL RT=1174.73

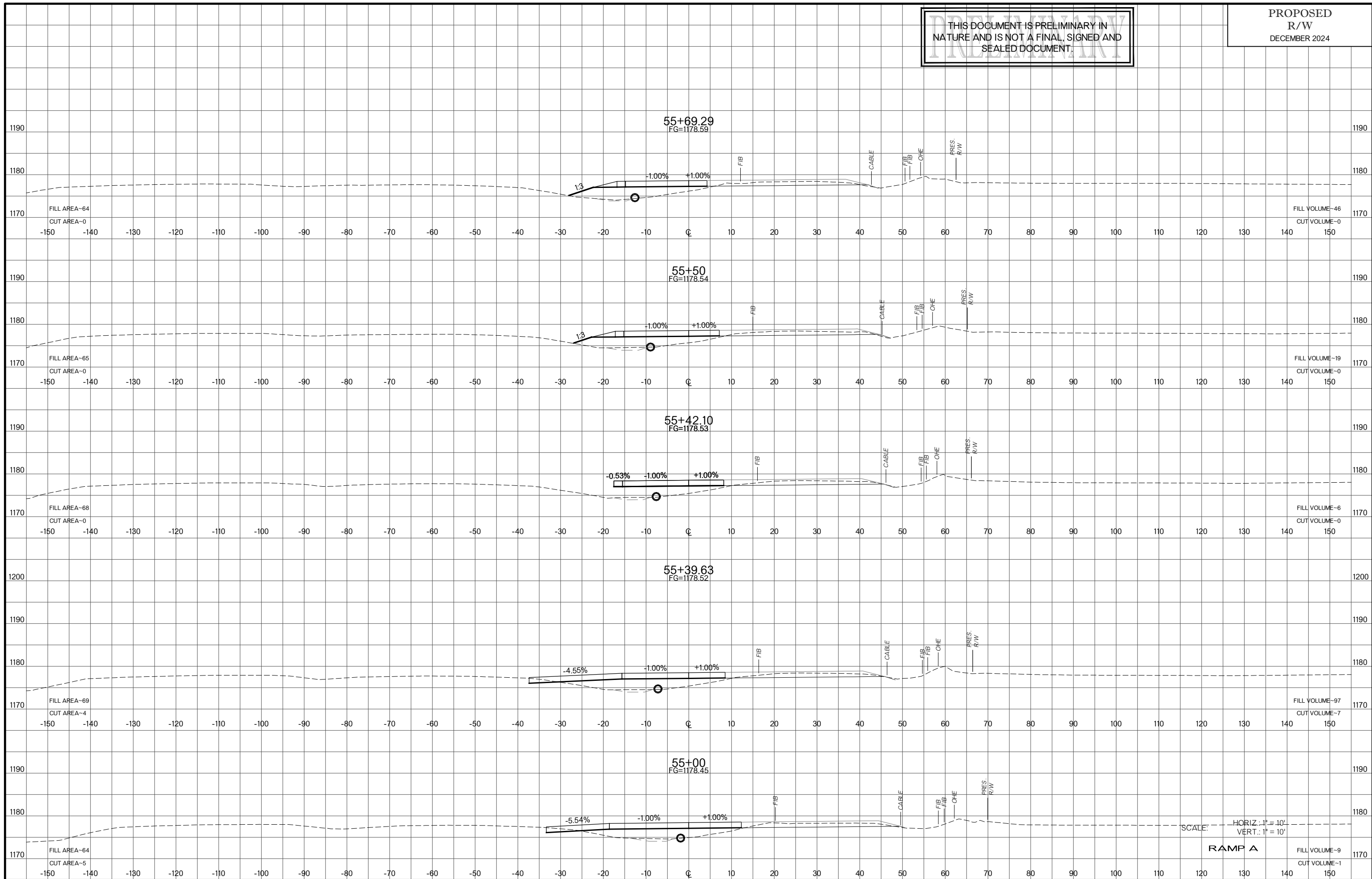
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HORIZ.: 1" = 10'  
VERT.: 1" = 10'

RAMP A

135/SH 74  
MCCLAIN COUNTY

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DECEMBER 2024



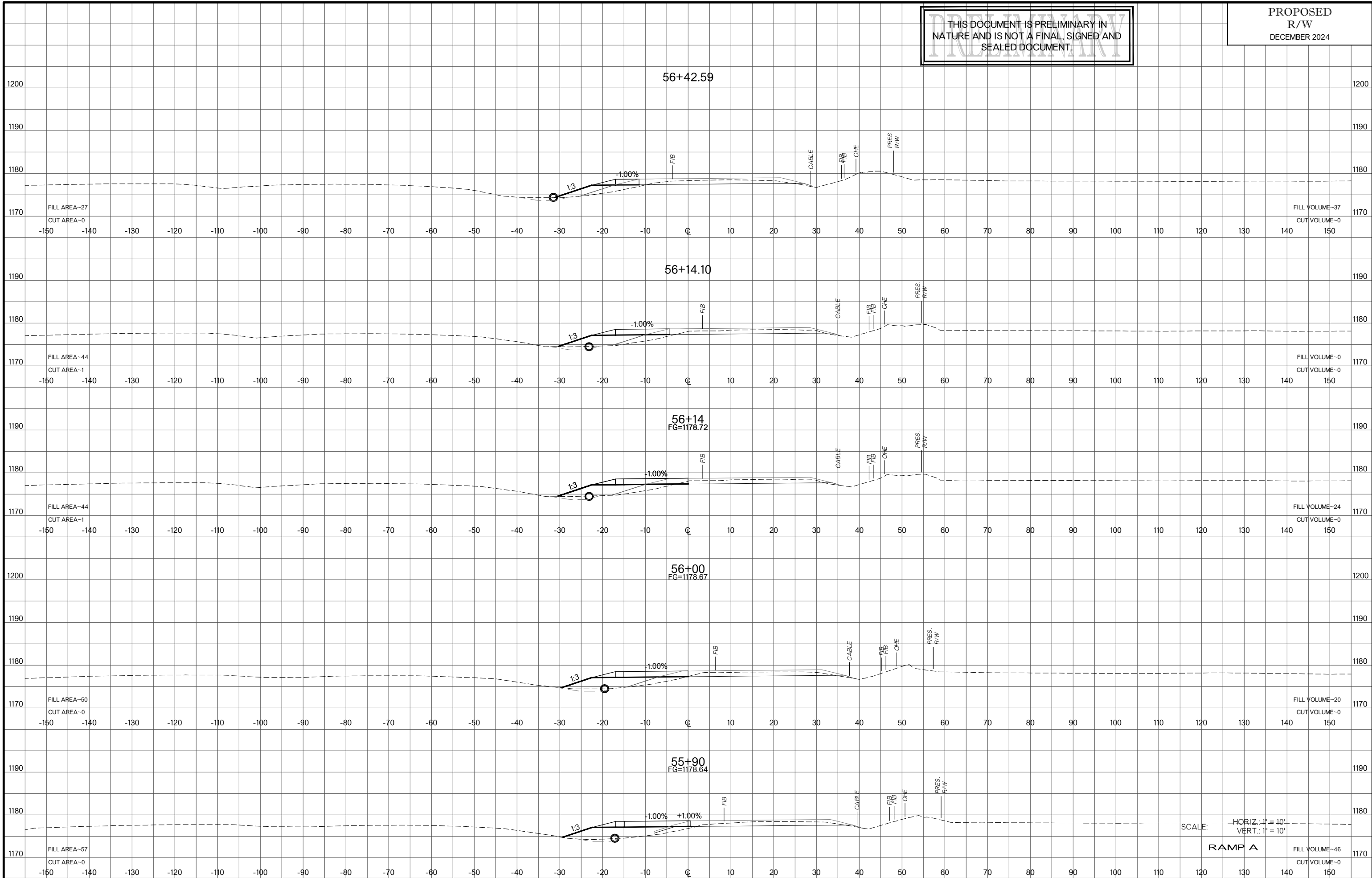
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RAMP A

135/SH 74  
MCCLAIN COUNTY

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DECEMBER 2024

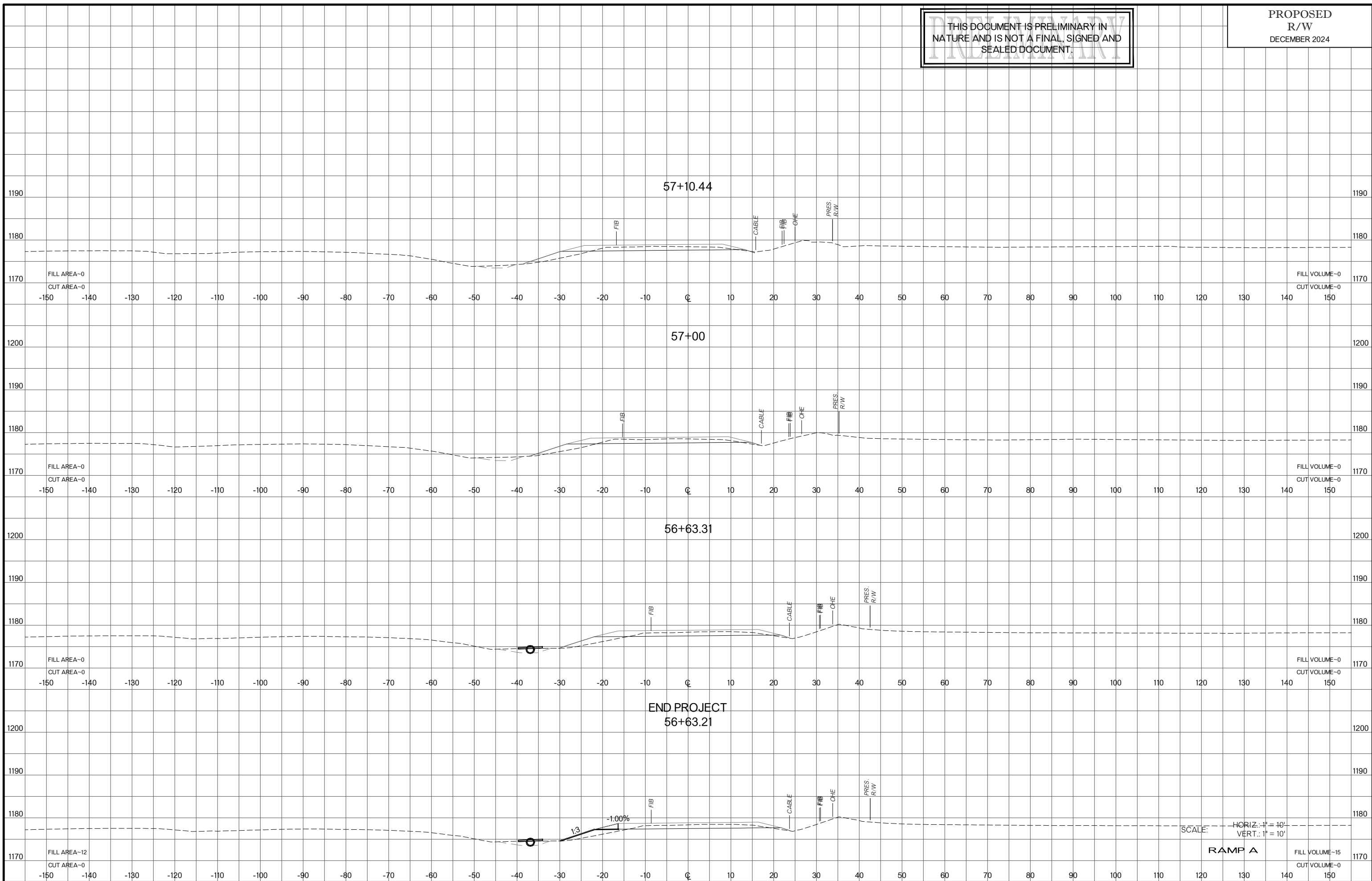


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RAMP A

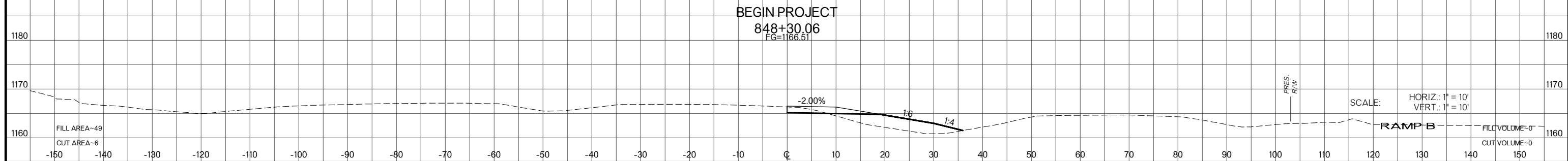
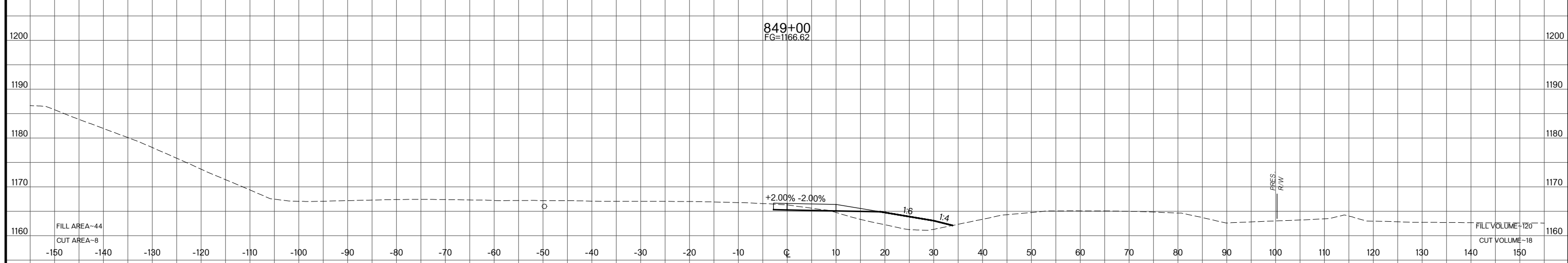
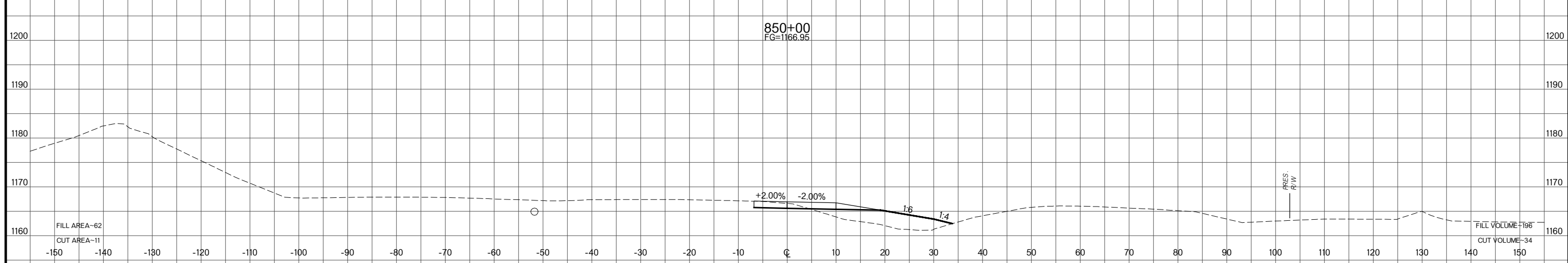
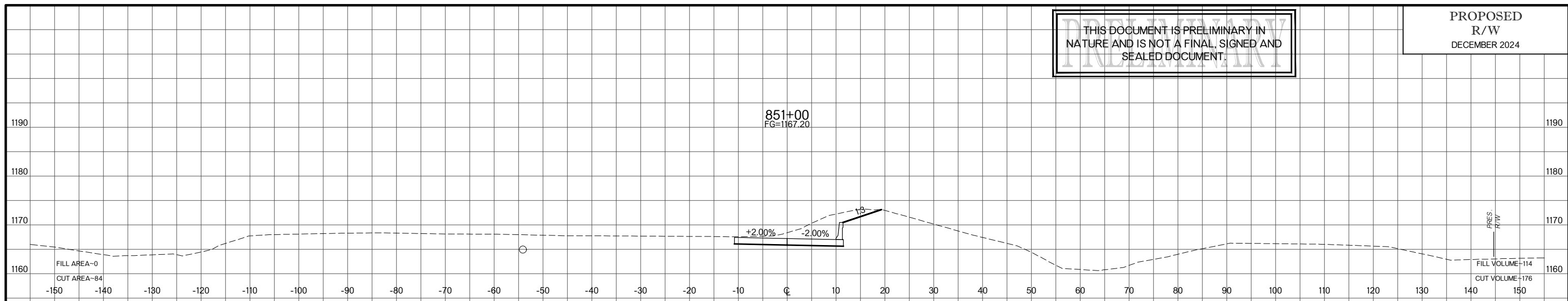
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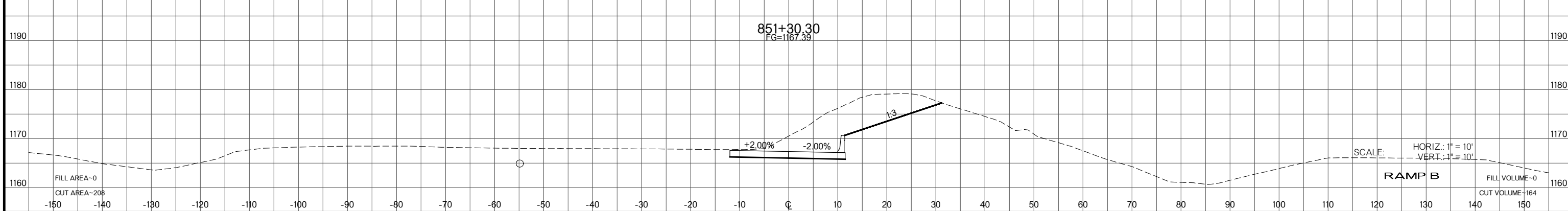
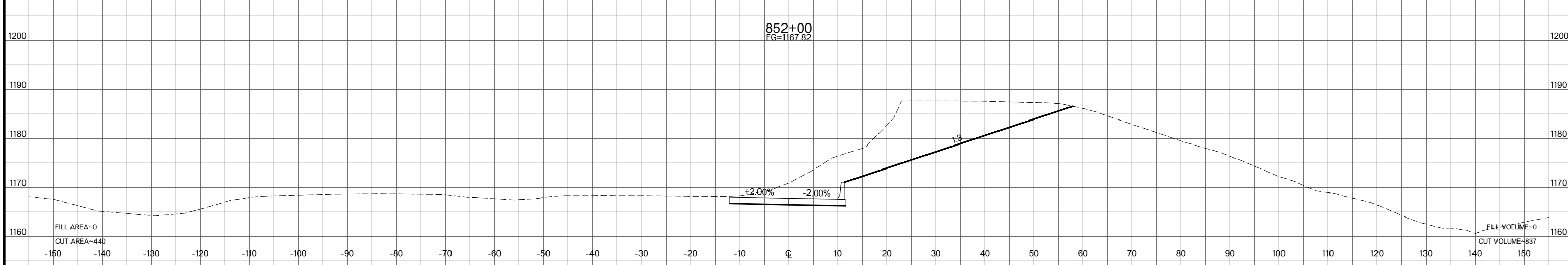
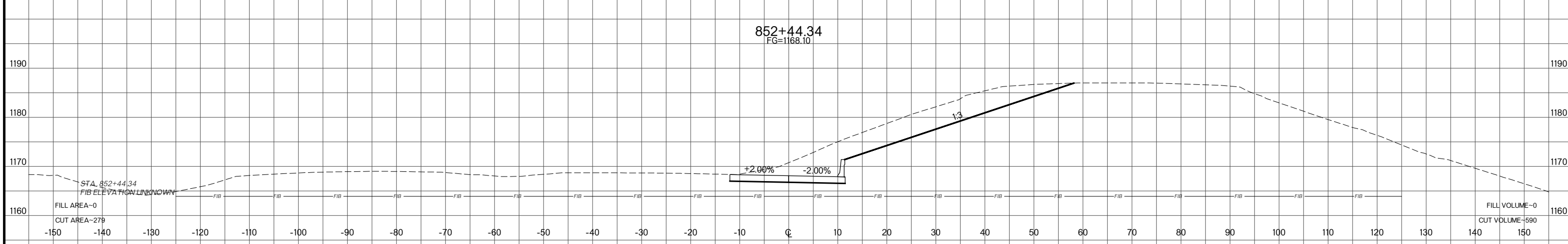
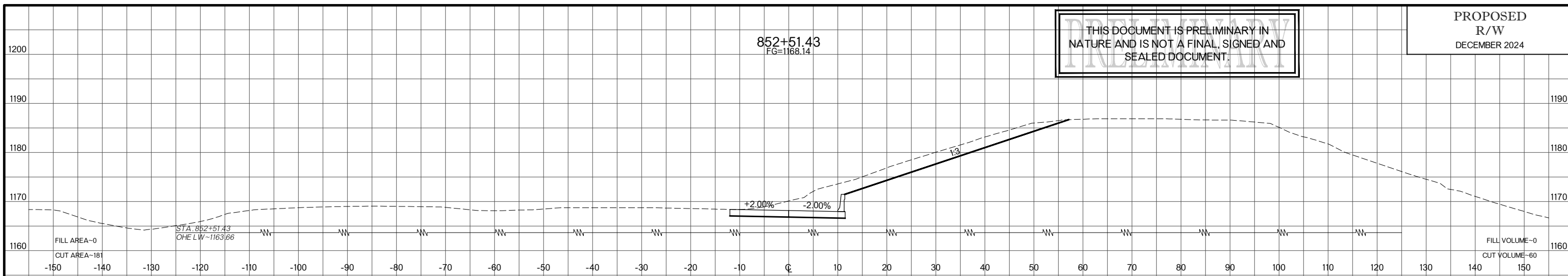


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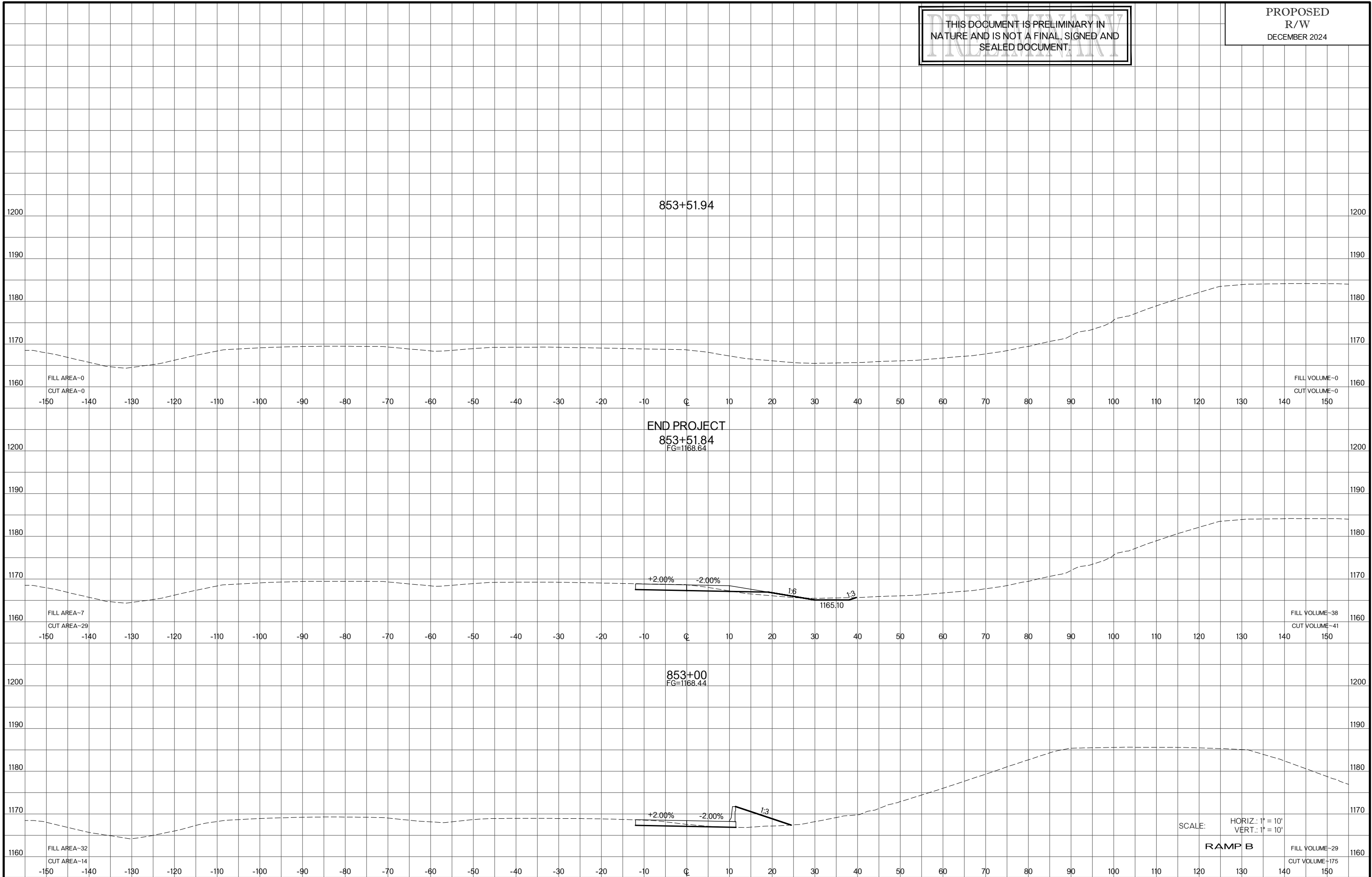


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PROPOSED R/W  
DECEMBER 2024



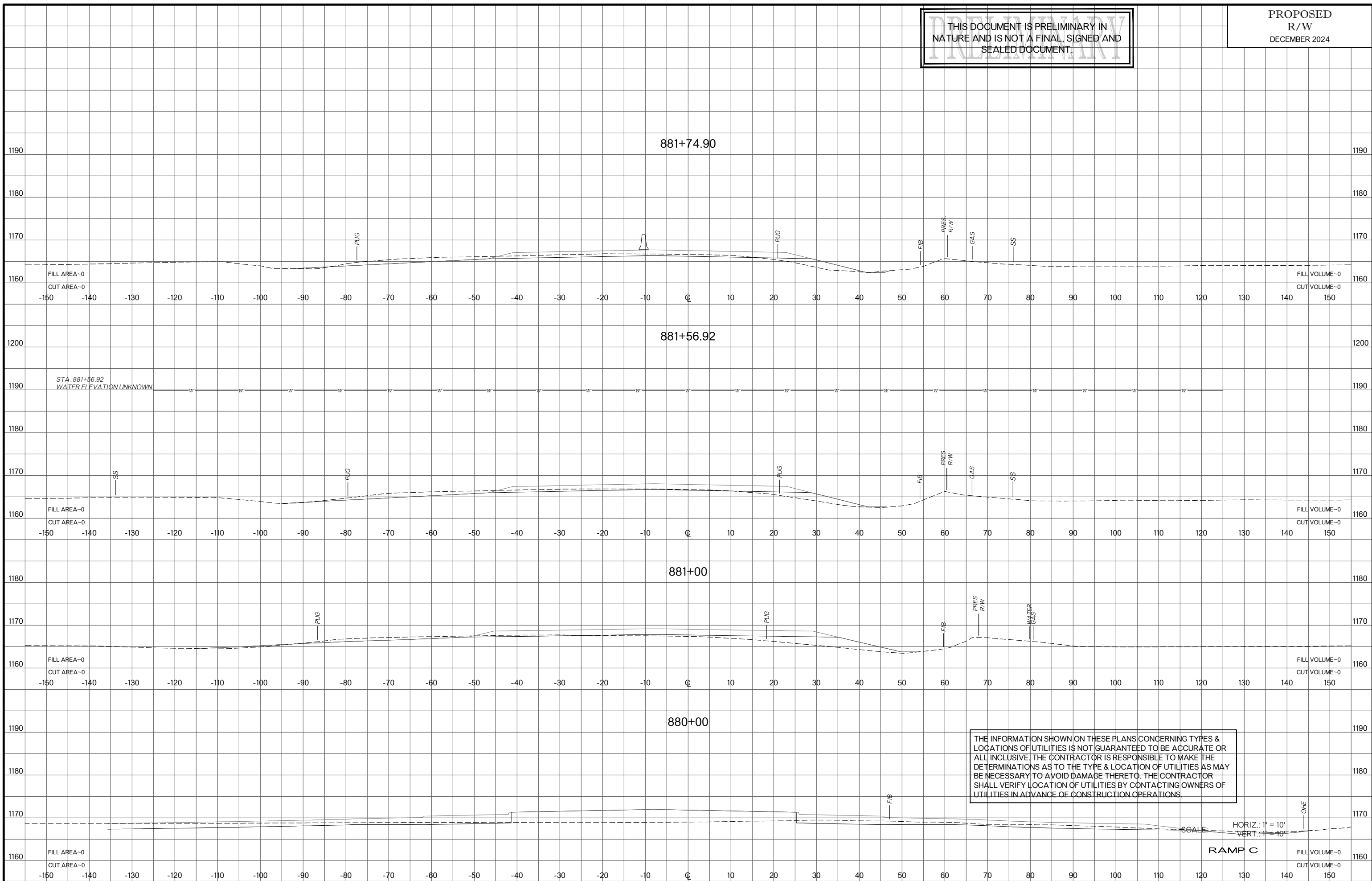
SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

RAMP B

MCCLAIN COUNTY

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PROPOSED R/W  
DECEMBER 2024



1190 881+74.90 1190

1180 1180

1170 PUG PUG FIB PRES. R/W GAS SS 1170

1160 FILL AREA-0 CUT AREA-0 FILL VOLUME-0 CUT VOLUME-0 1160

1200 881+56.92 1200

1190 STA. 881+56.92 WATER ELEVATION UNKNOWN 1190

1180 1180

1170 SS PUG PUG FIB PRES. R/W GAS SS 1170

1160 FILL AREA-0 CUT AREA-0 FILL VOLUME-0 CUT VOLUME-0 1160

1180 881+00 1180

1170 PUG PUG FIB PRES. R/W WATER GAS 1170

1160 FILL AREA-0 CUT AREA-0 FILL VOLUME-0 CUT VOLUME-0 1160

1190 880+00 1190

1180 1180

1170 FIB OHE 1170

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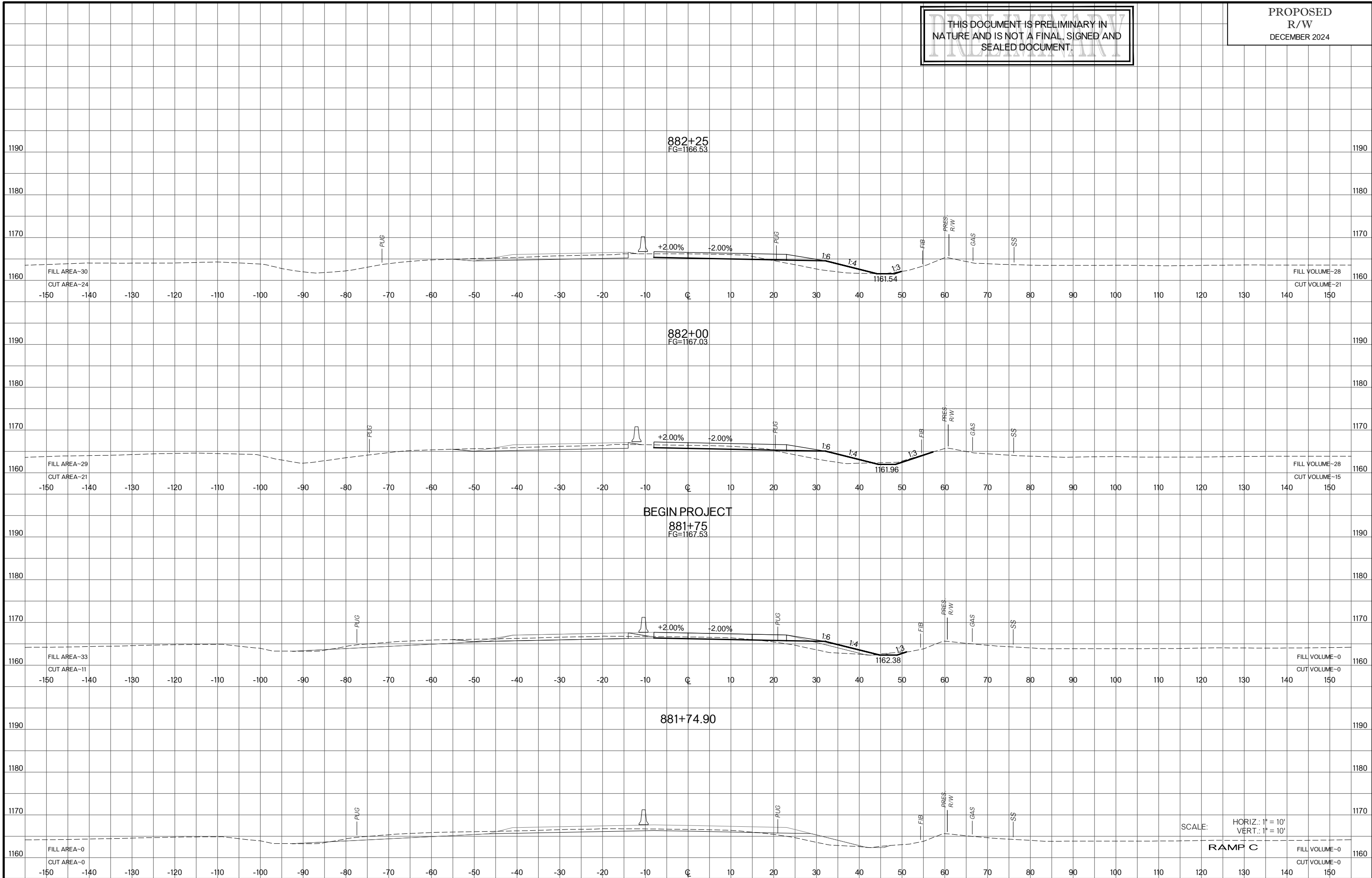
THE INFORMATION SHOWN ON THESE PLANS CONCERNING TYPES & LOCATIONS OF UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE TO MAKE THE DETERMINATIONS AS TO THE TYPE & LOCATION OF UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. THE CONTRACTOR SHALL VERIFY LOCATION OF UTILITIES BY CONTACTING OWNERS OF UTILITIES IN ADVANCE OF CONSTRUCTION OPERATIONS.

HORIZ.: 1" = 10'  
VERT.: 1" = 4'

RAMP C

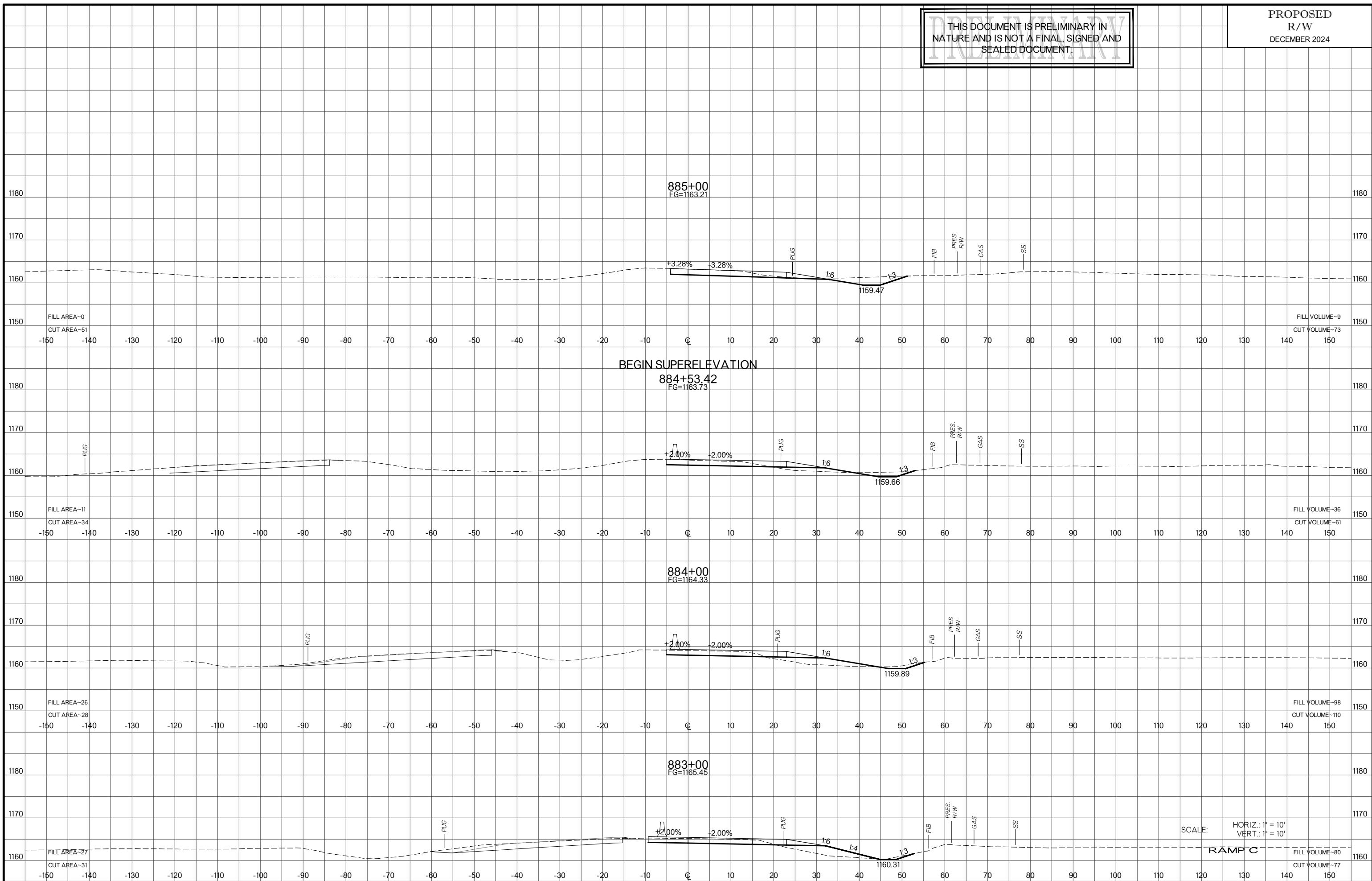
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PROPOSED R/W  
DECEMBER 2024



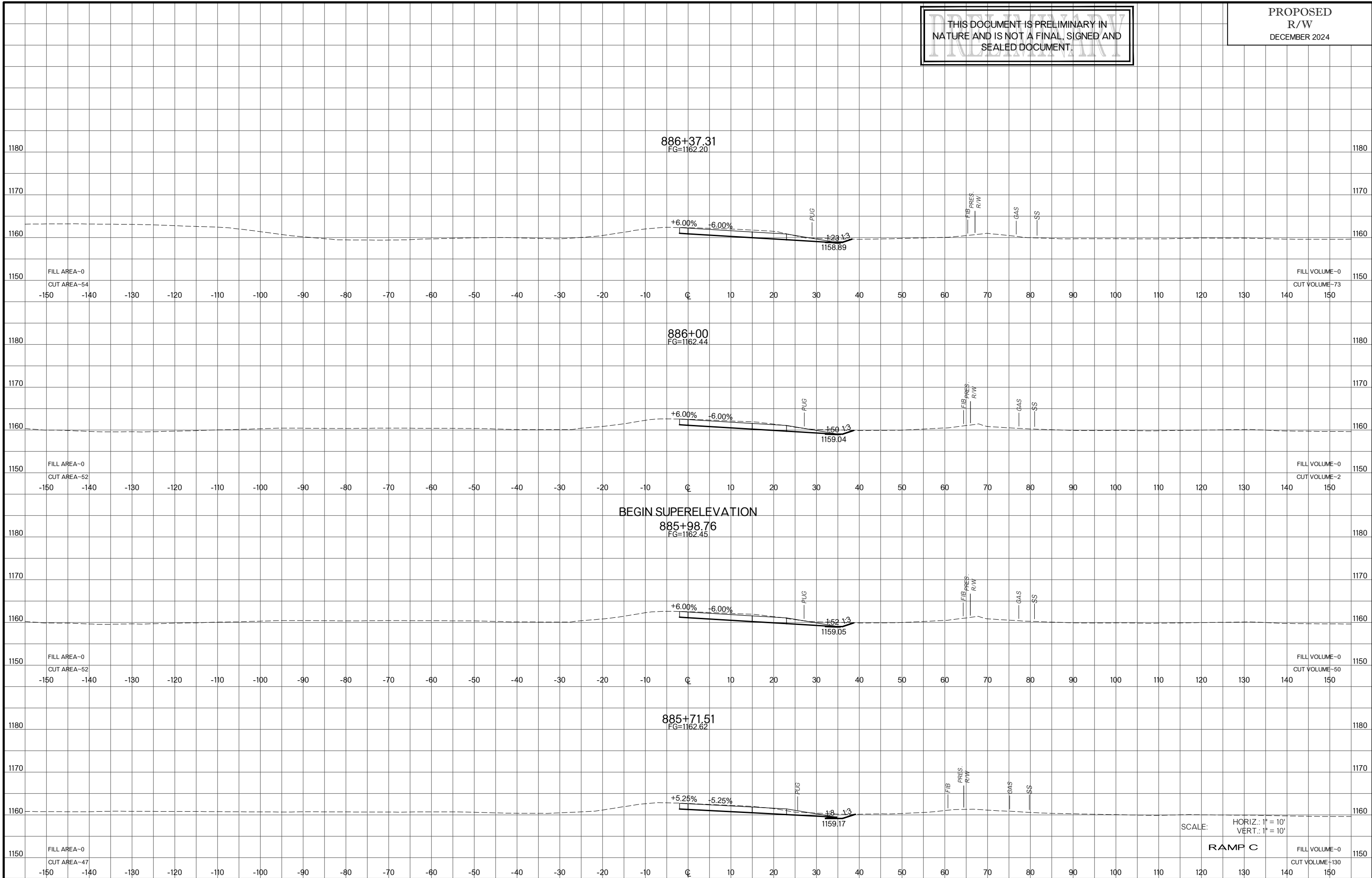
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DECEMBER 2024



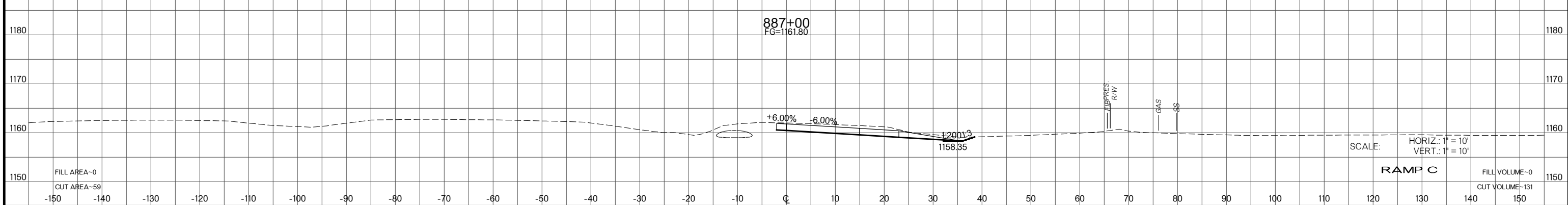
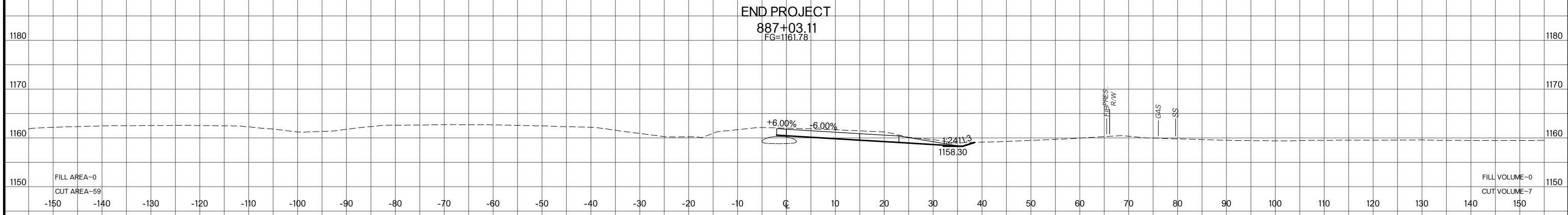
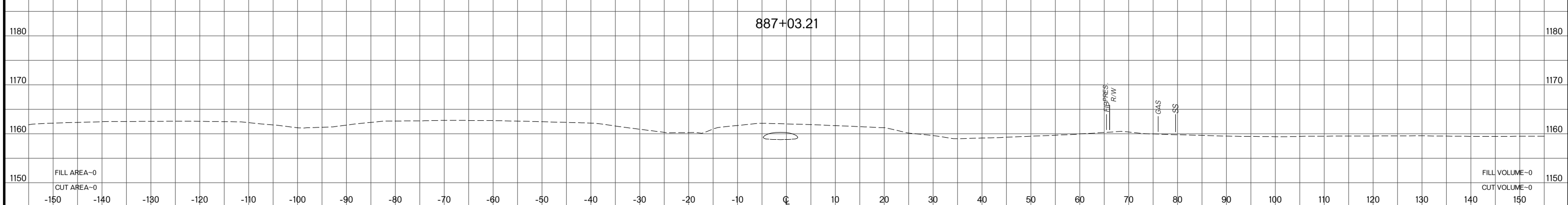
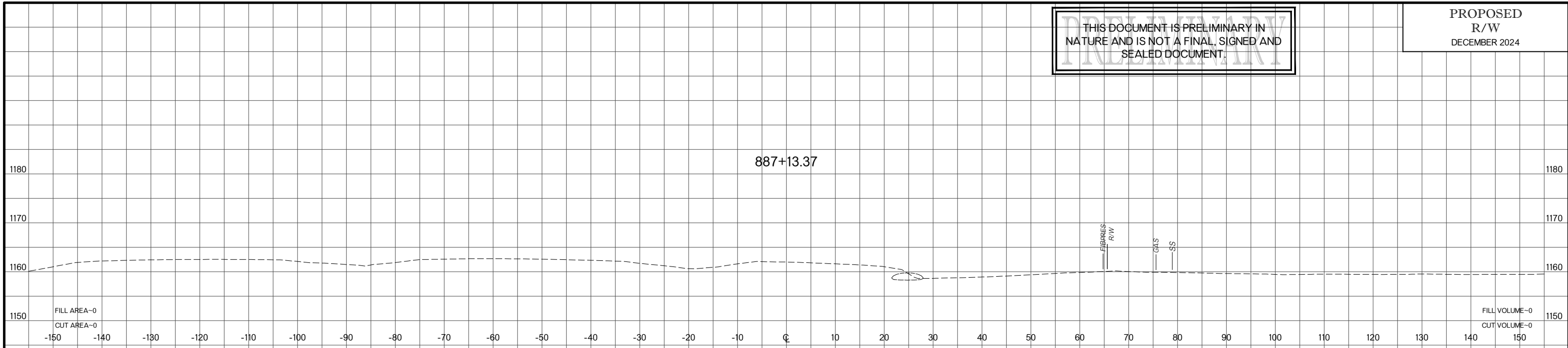
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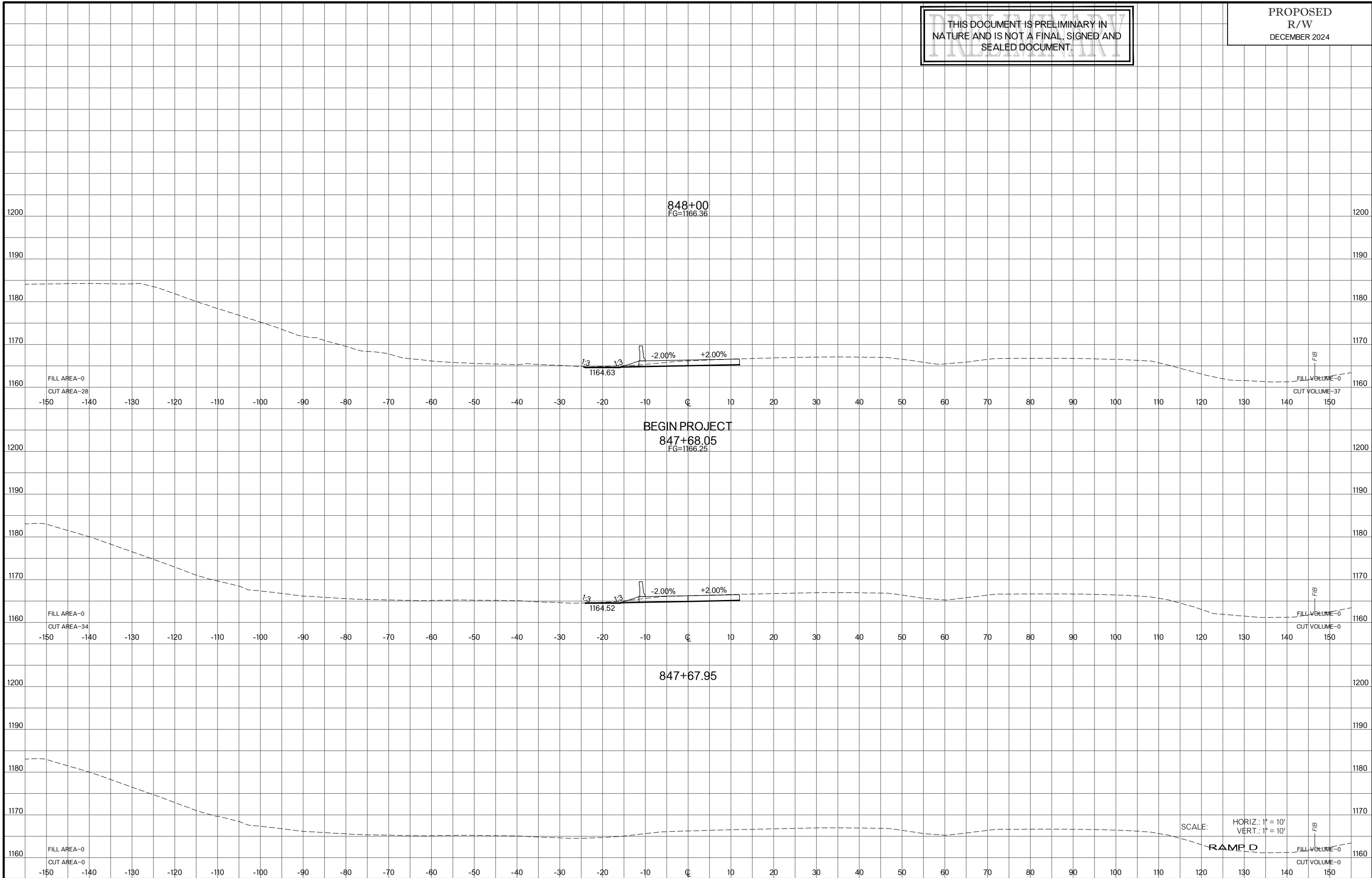


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RAMP C

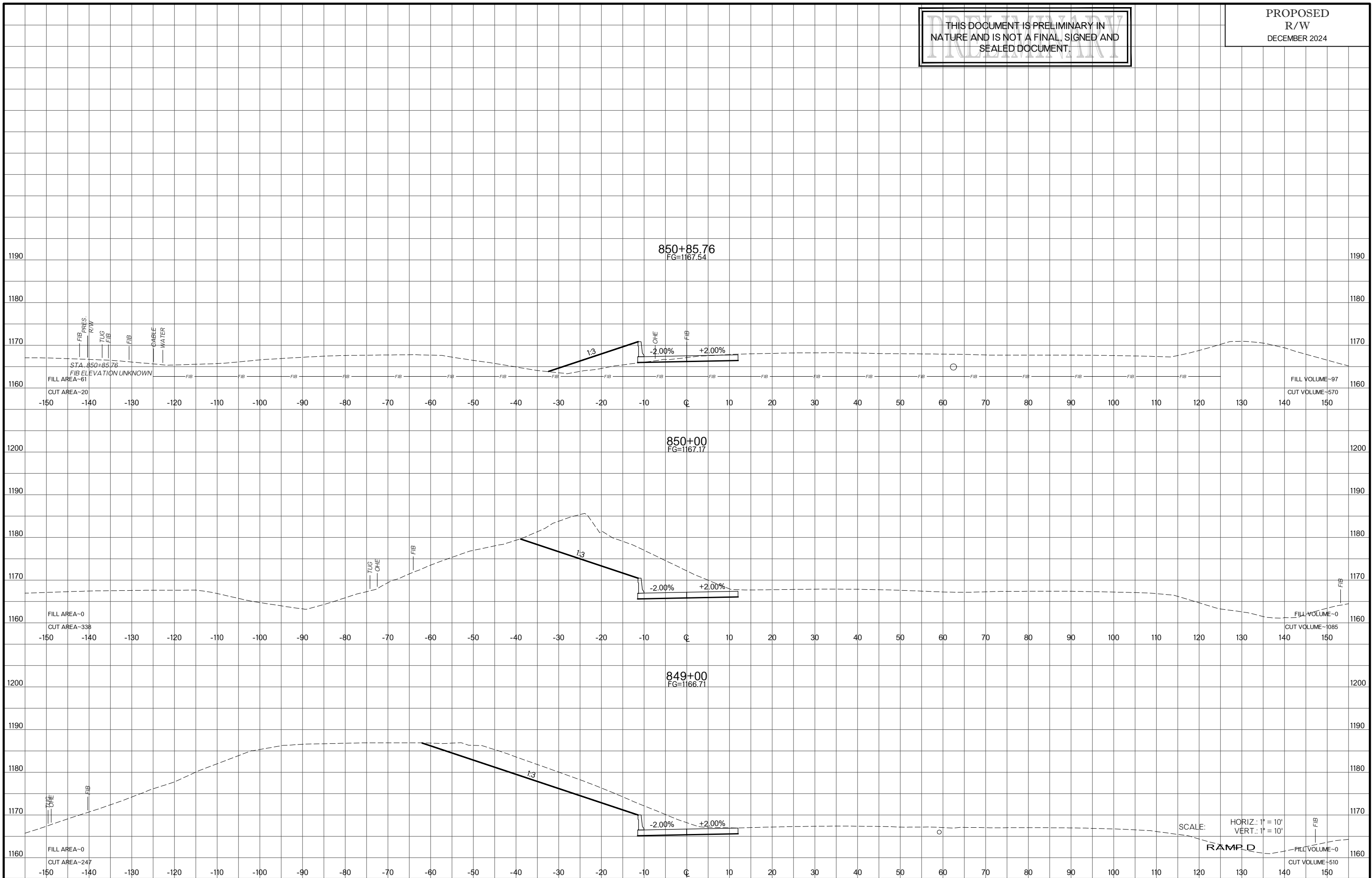
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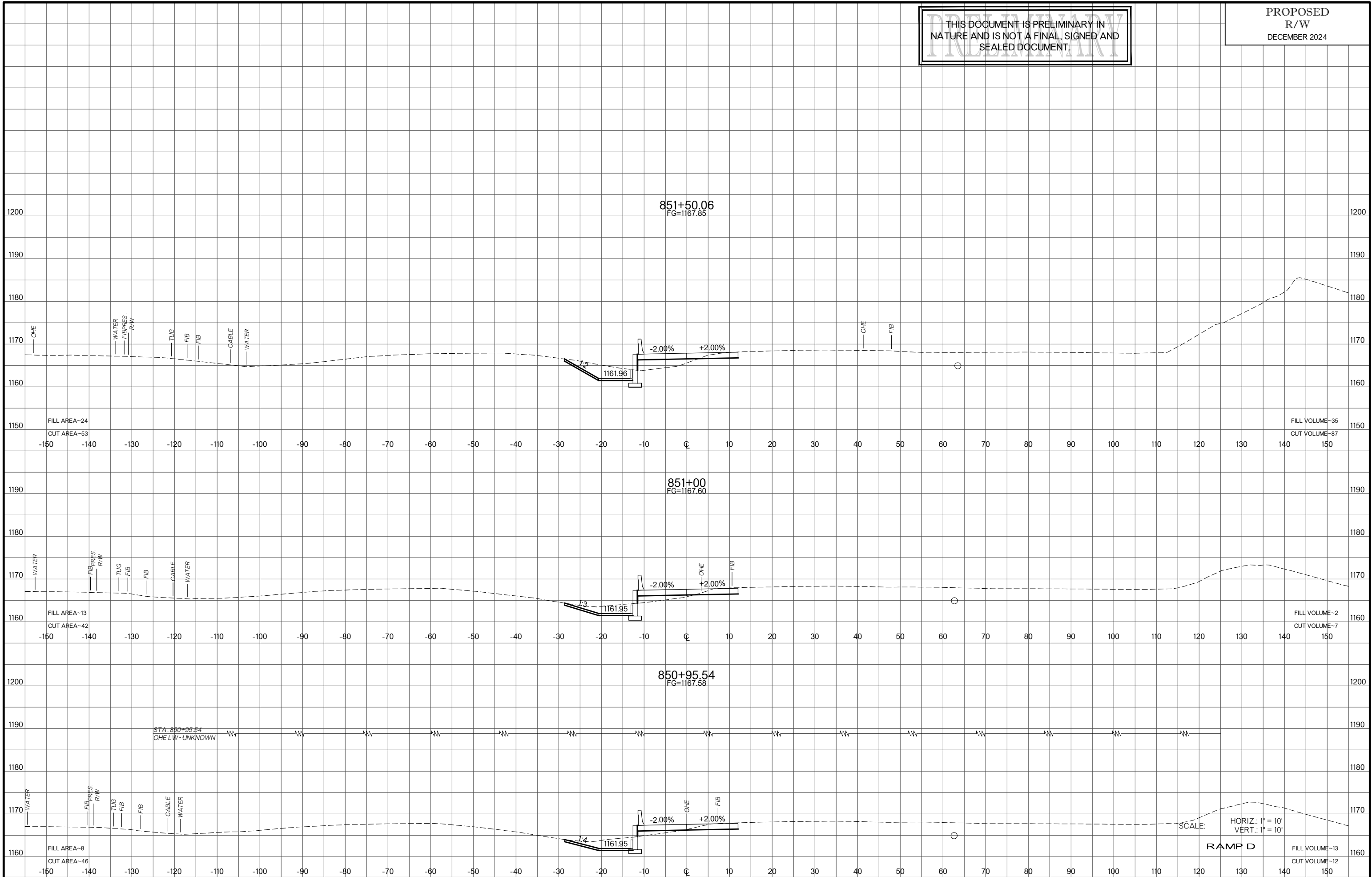
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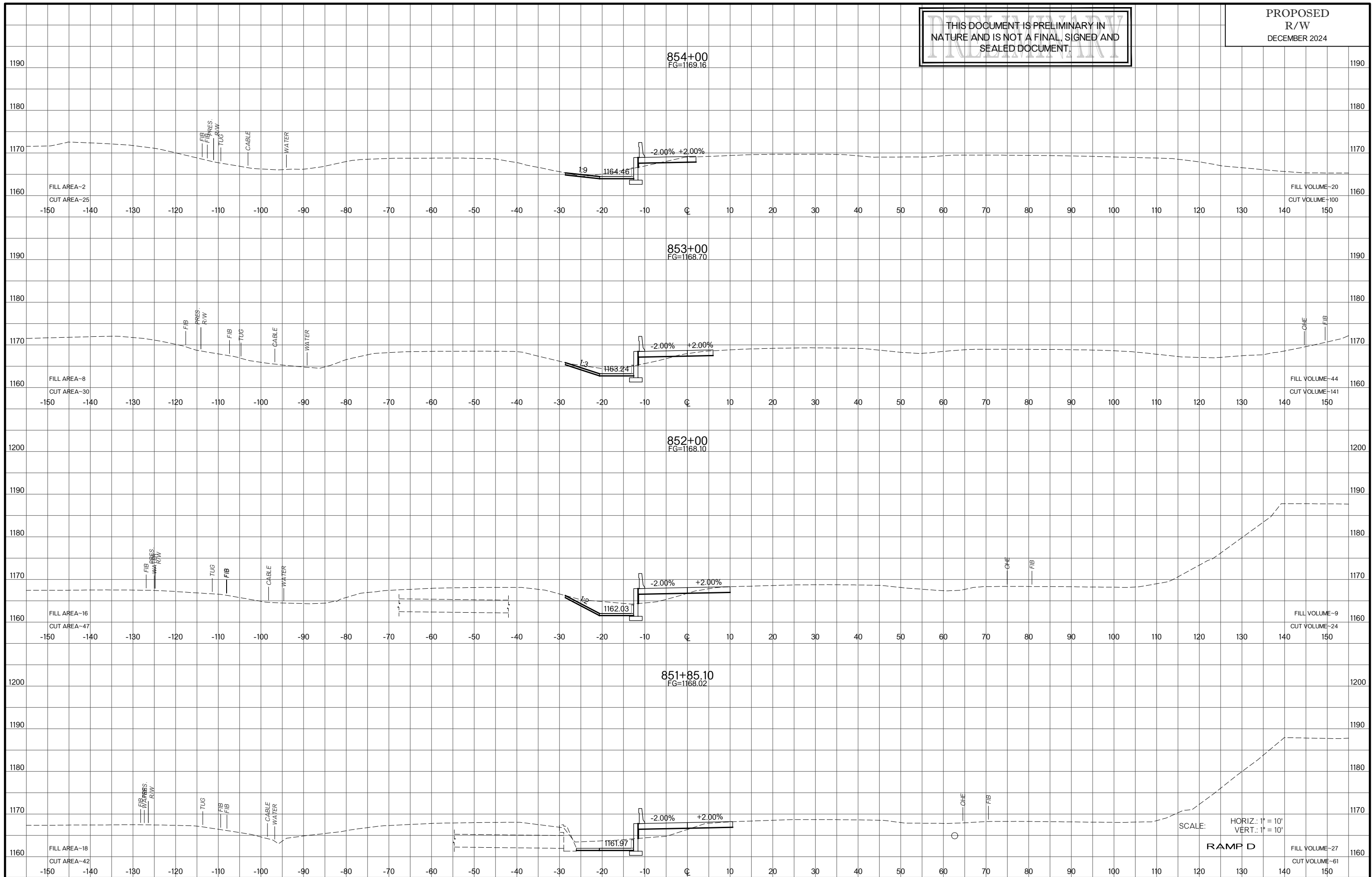
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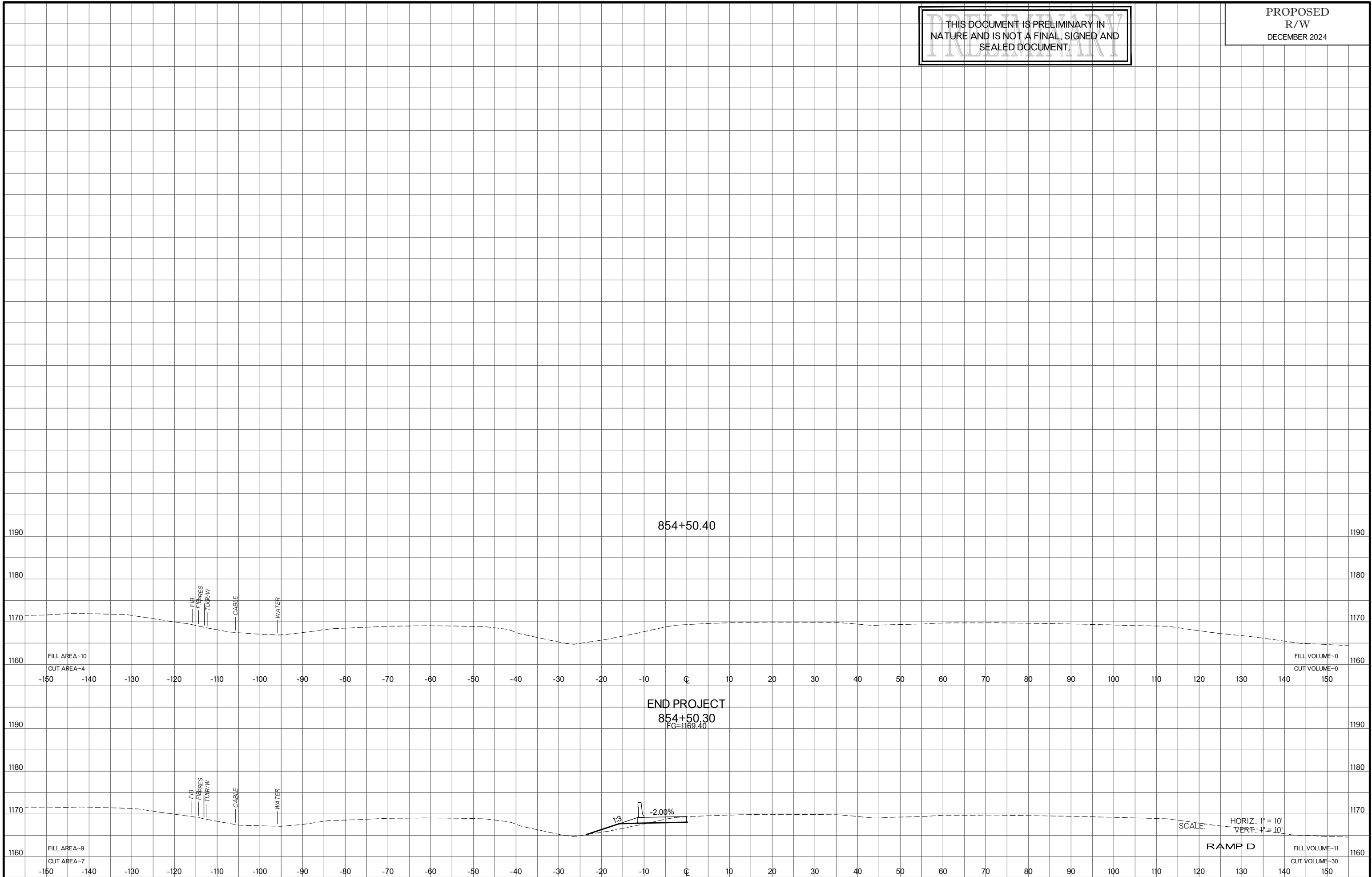
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RAMP D

135/SH 74  
MCCLAIN COUNTY

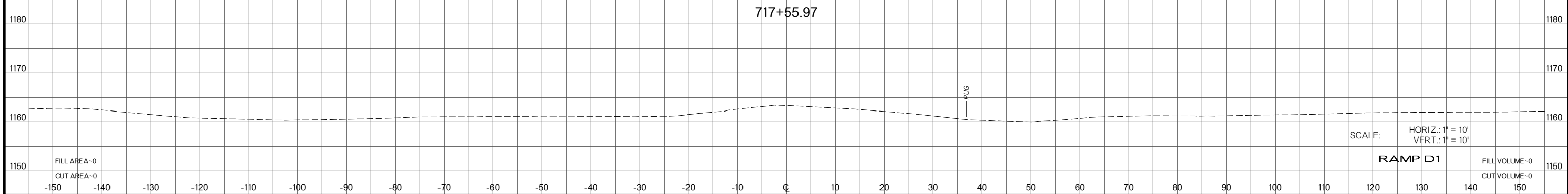
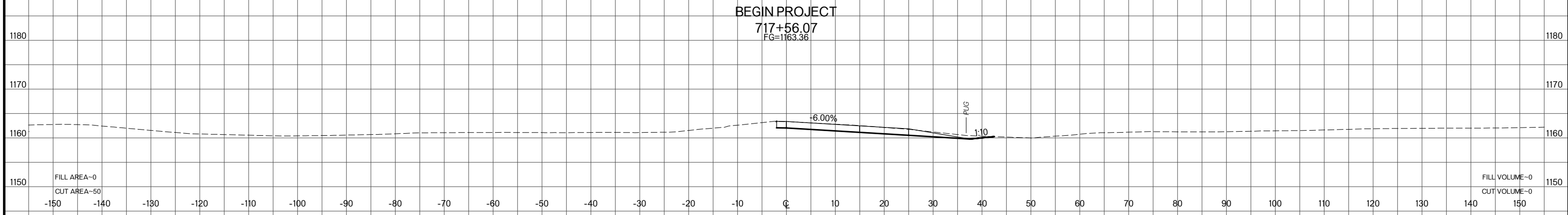
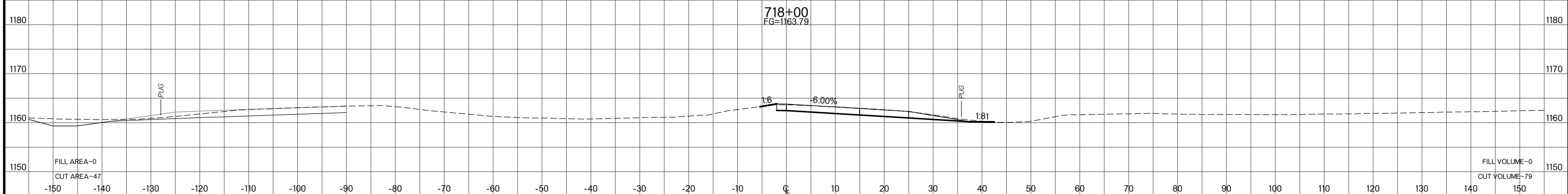
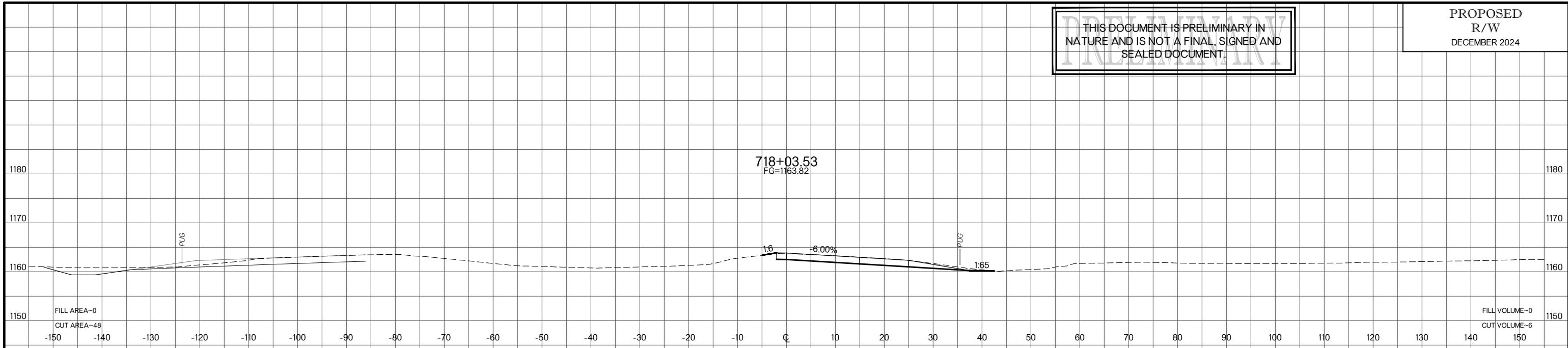
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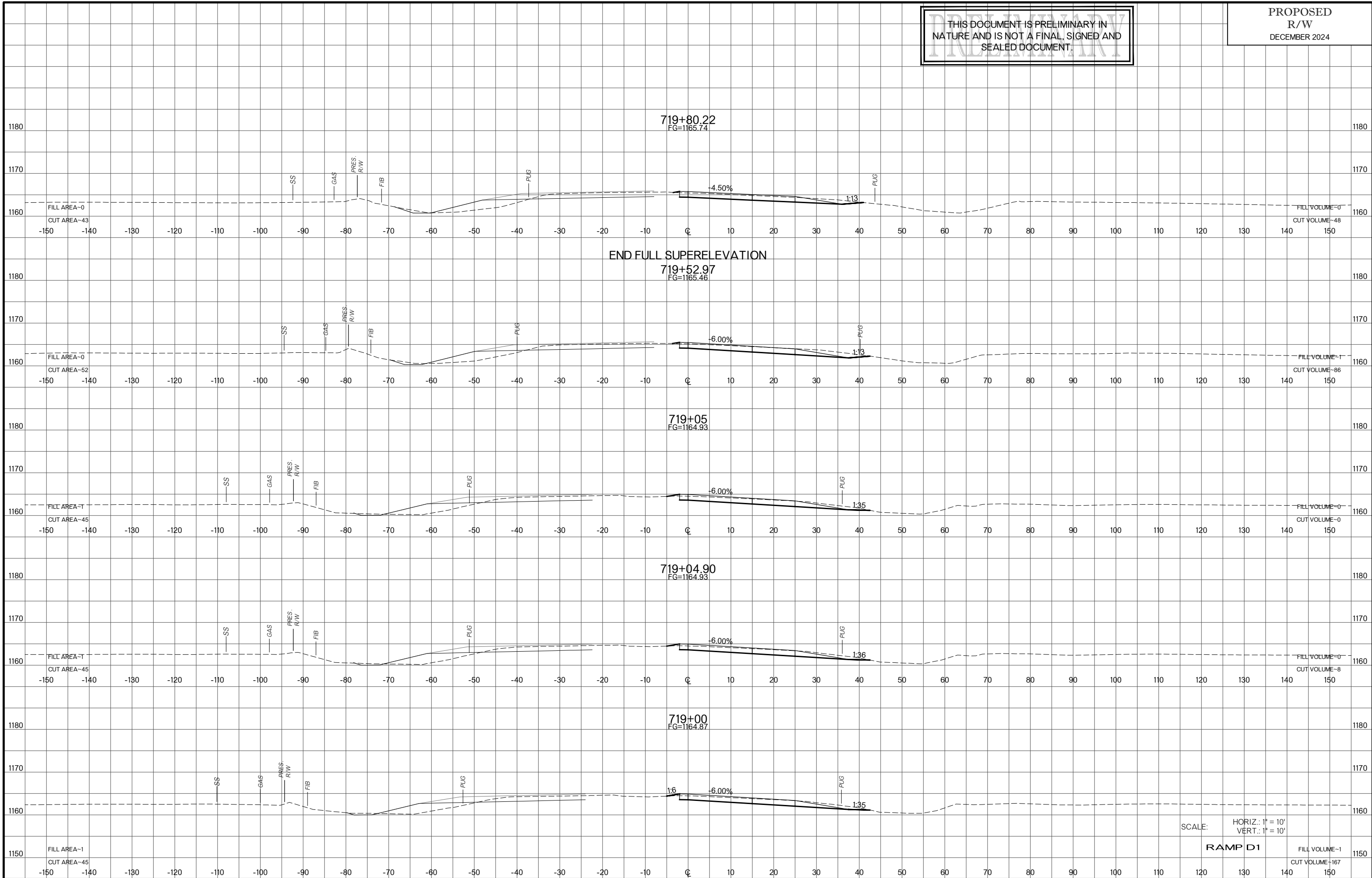


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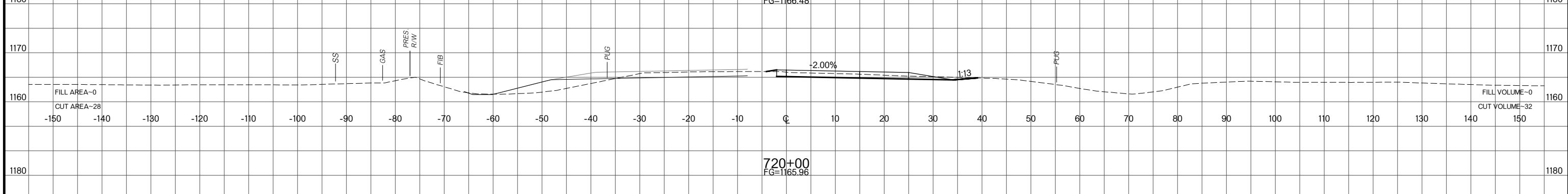
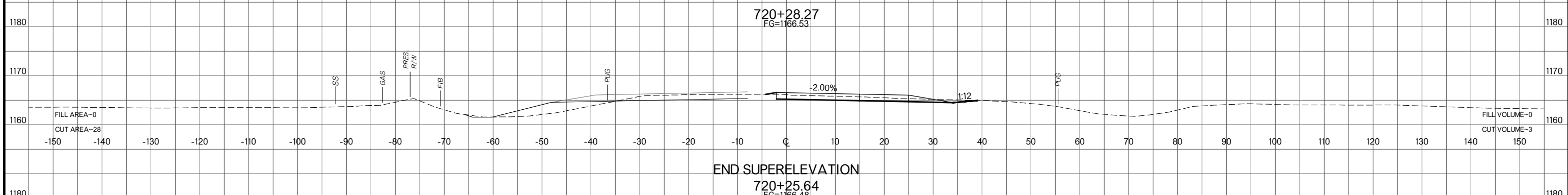
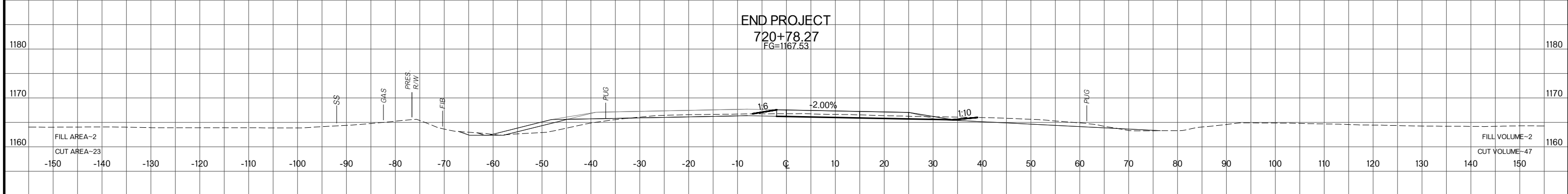
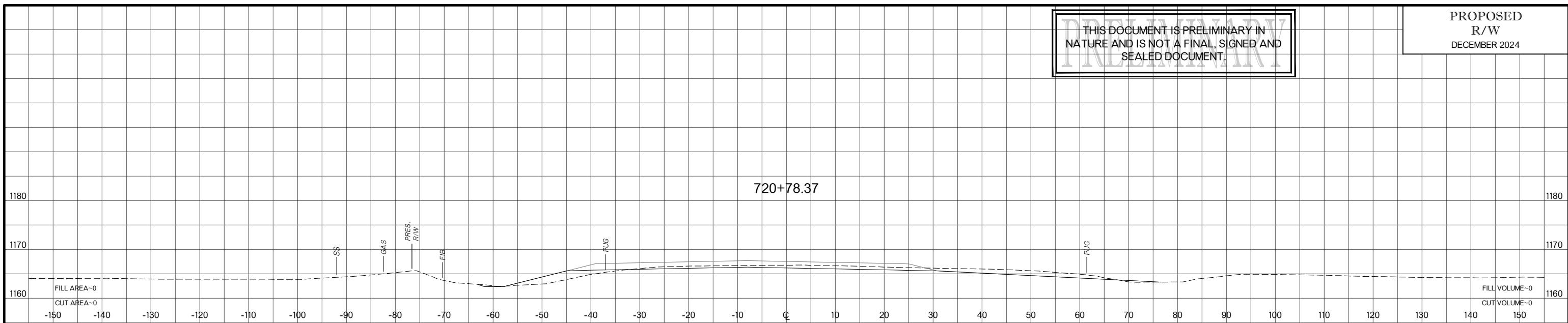
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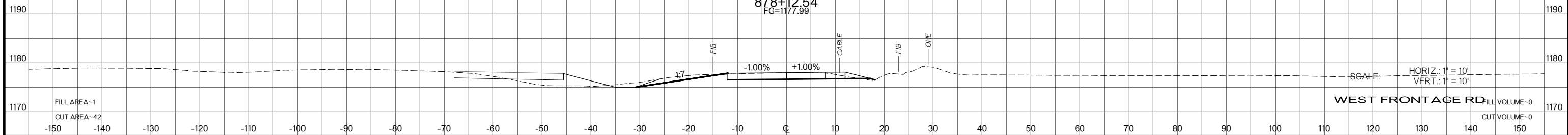
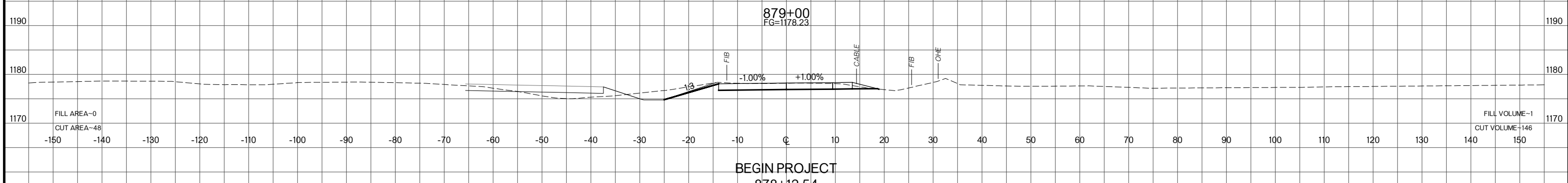
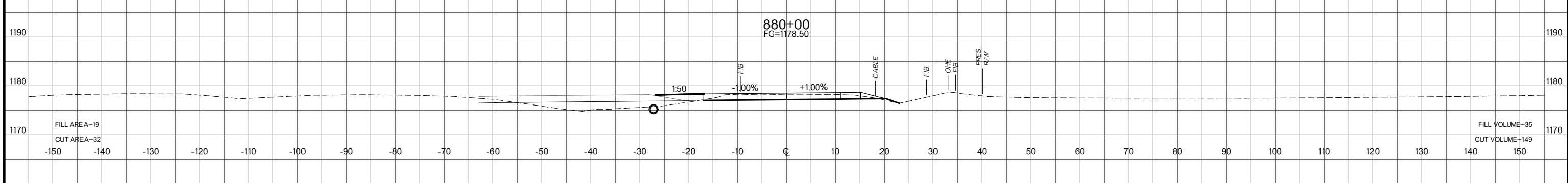
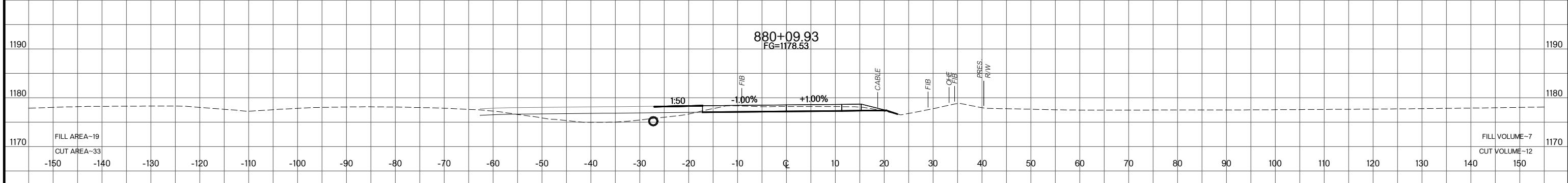
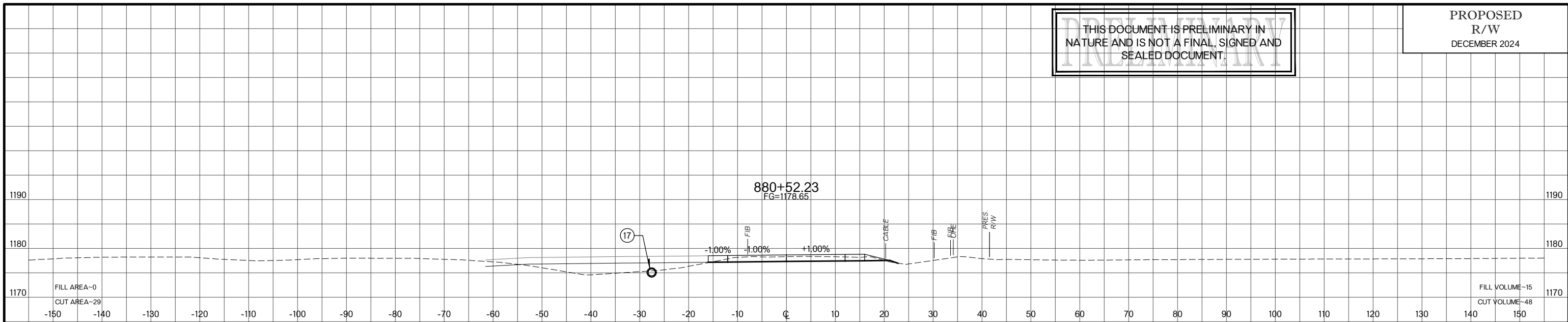
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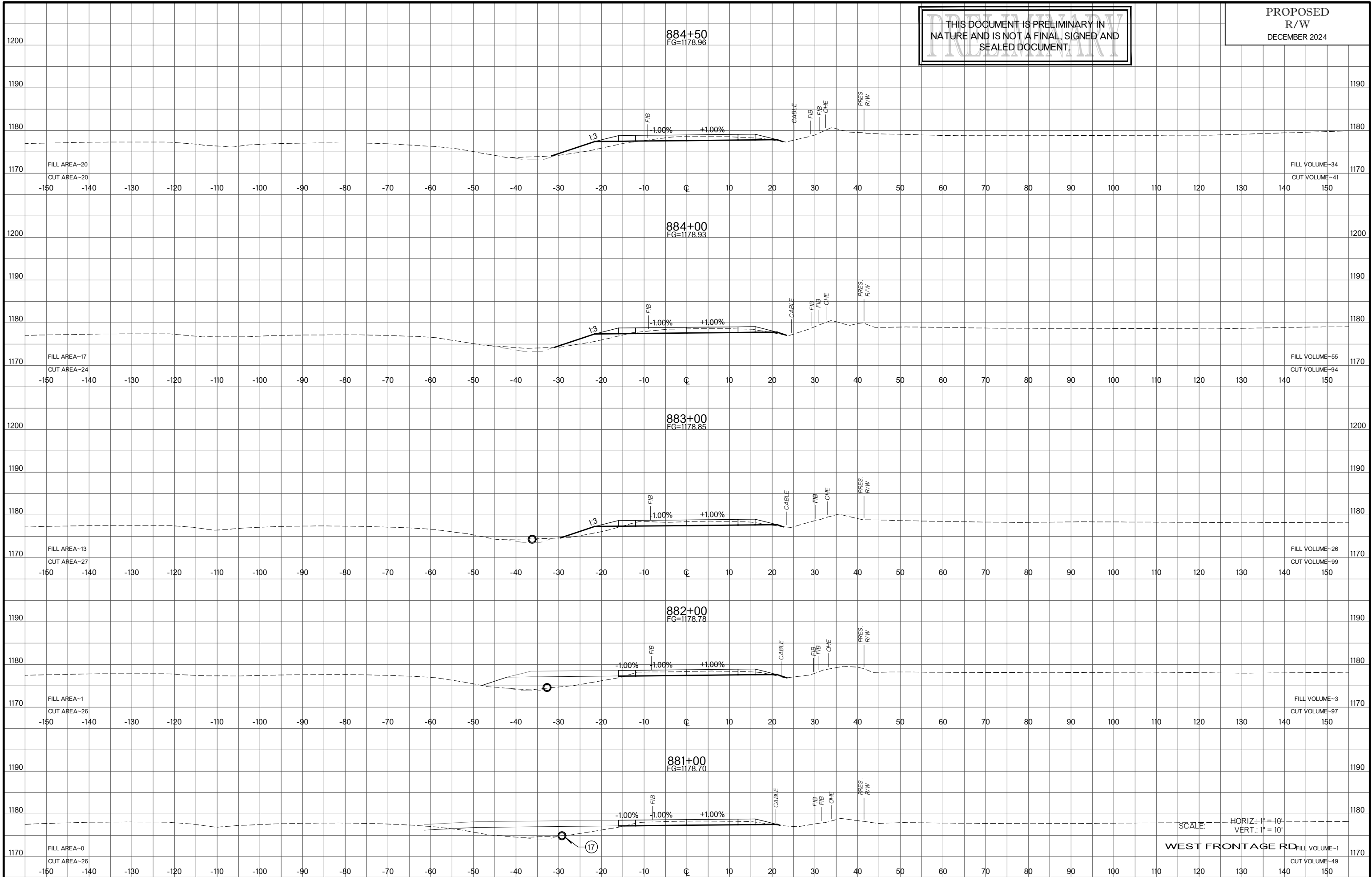
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WEST FRONTAGE RD

MCCLAIN COUNTY 135/SH 74

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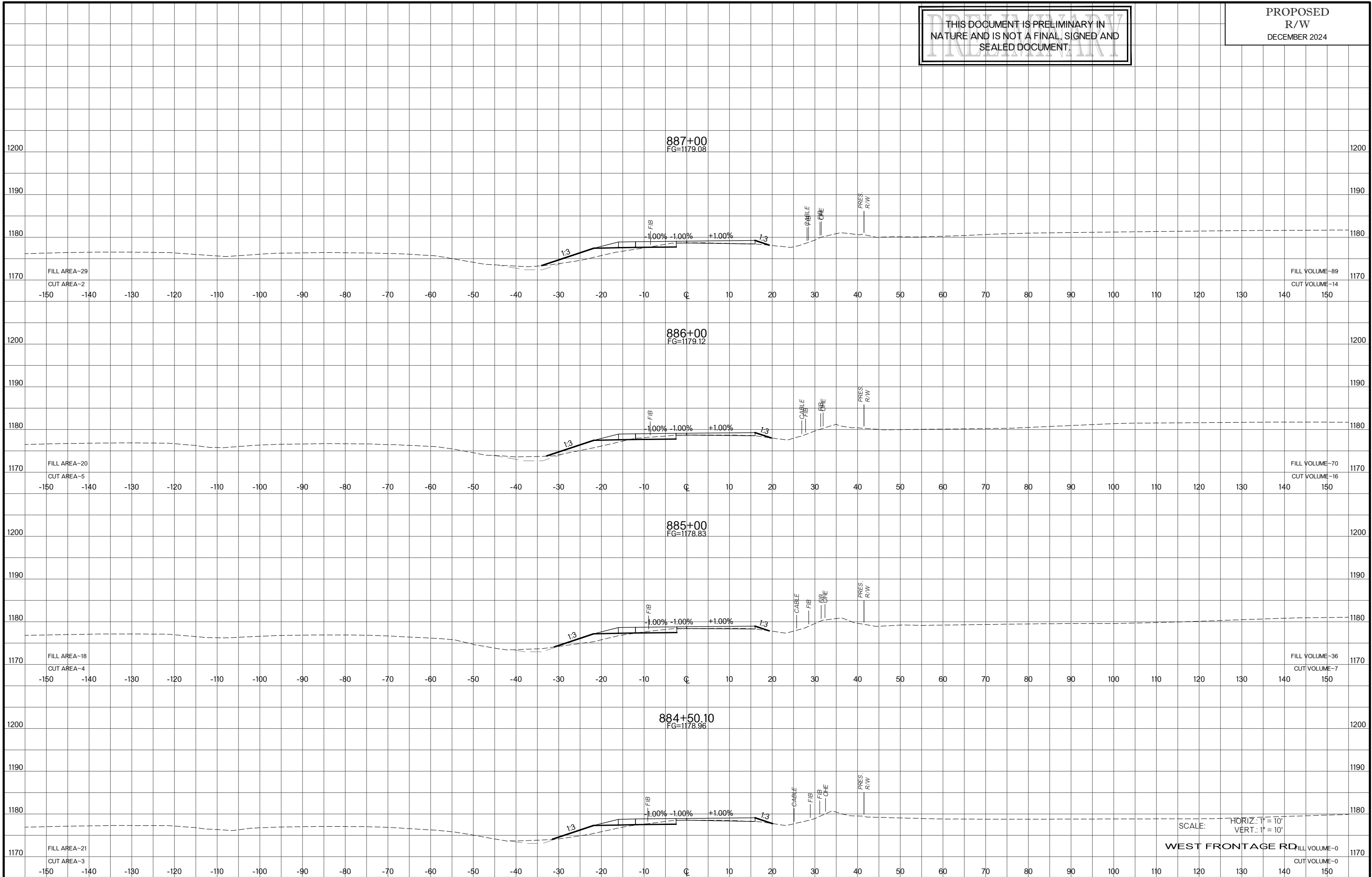
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135/SH 74  
MCLAIN COUNTY

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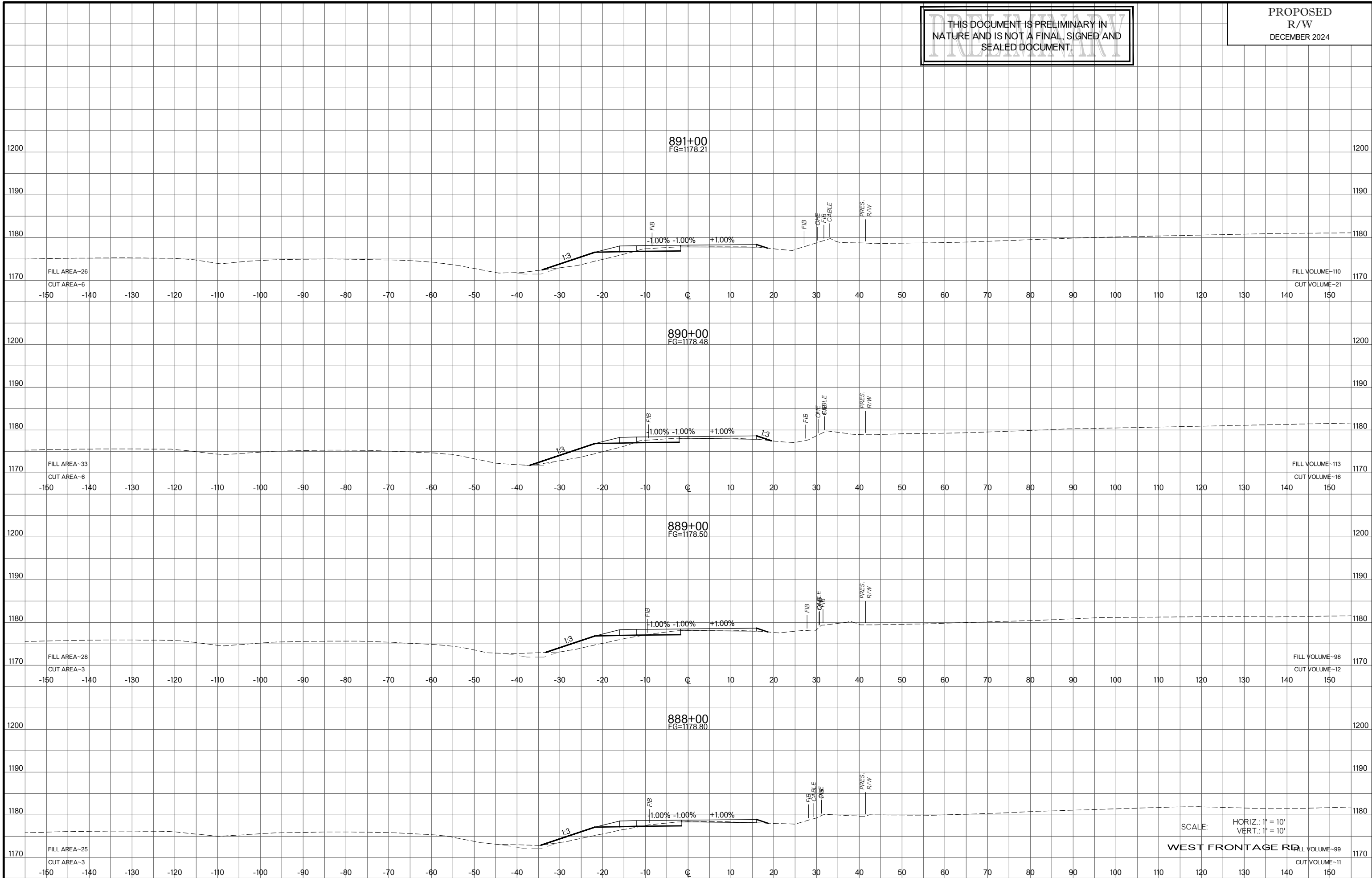
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WEST FRONTAGE RD

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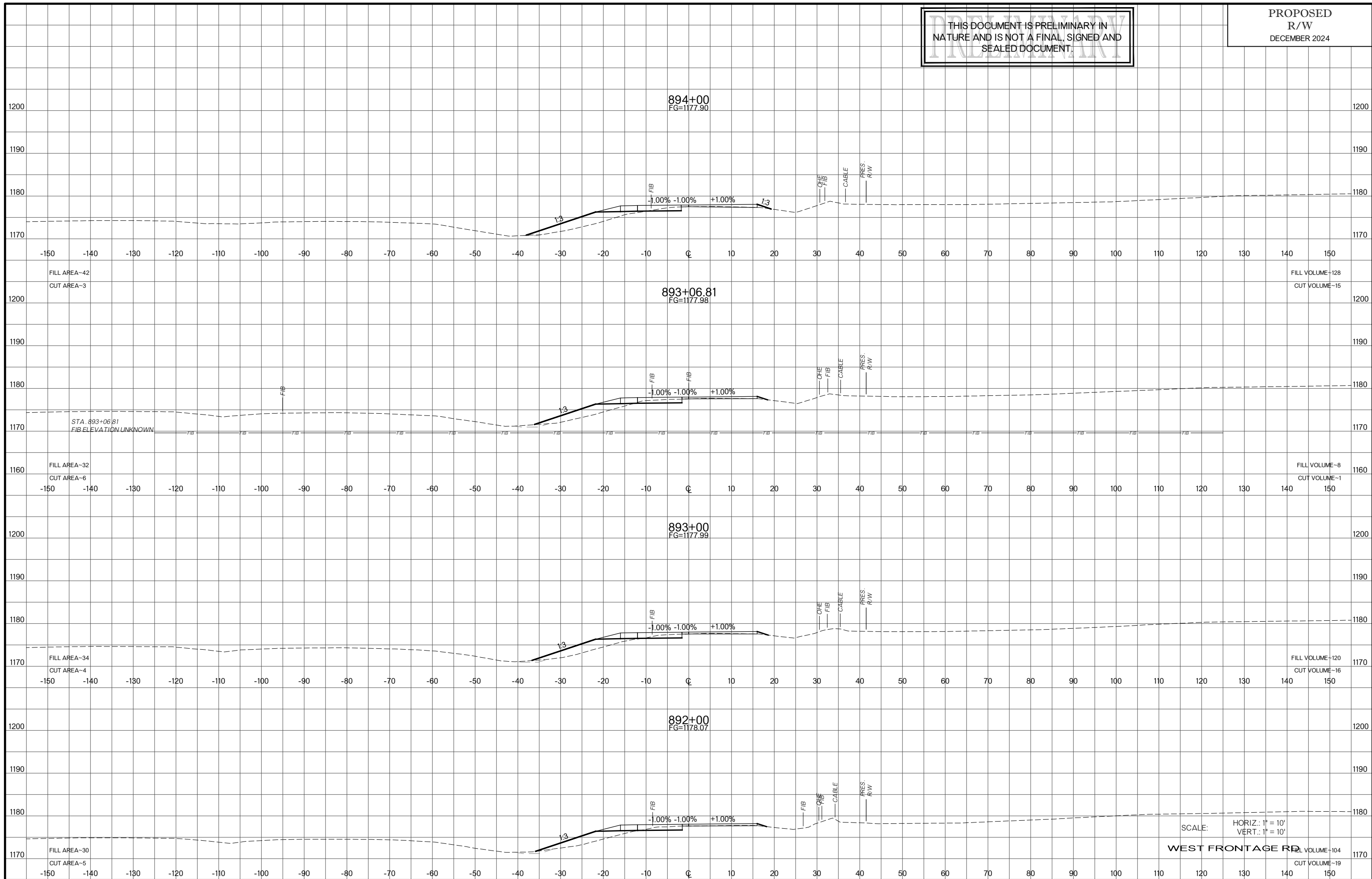


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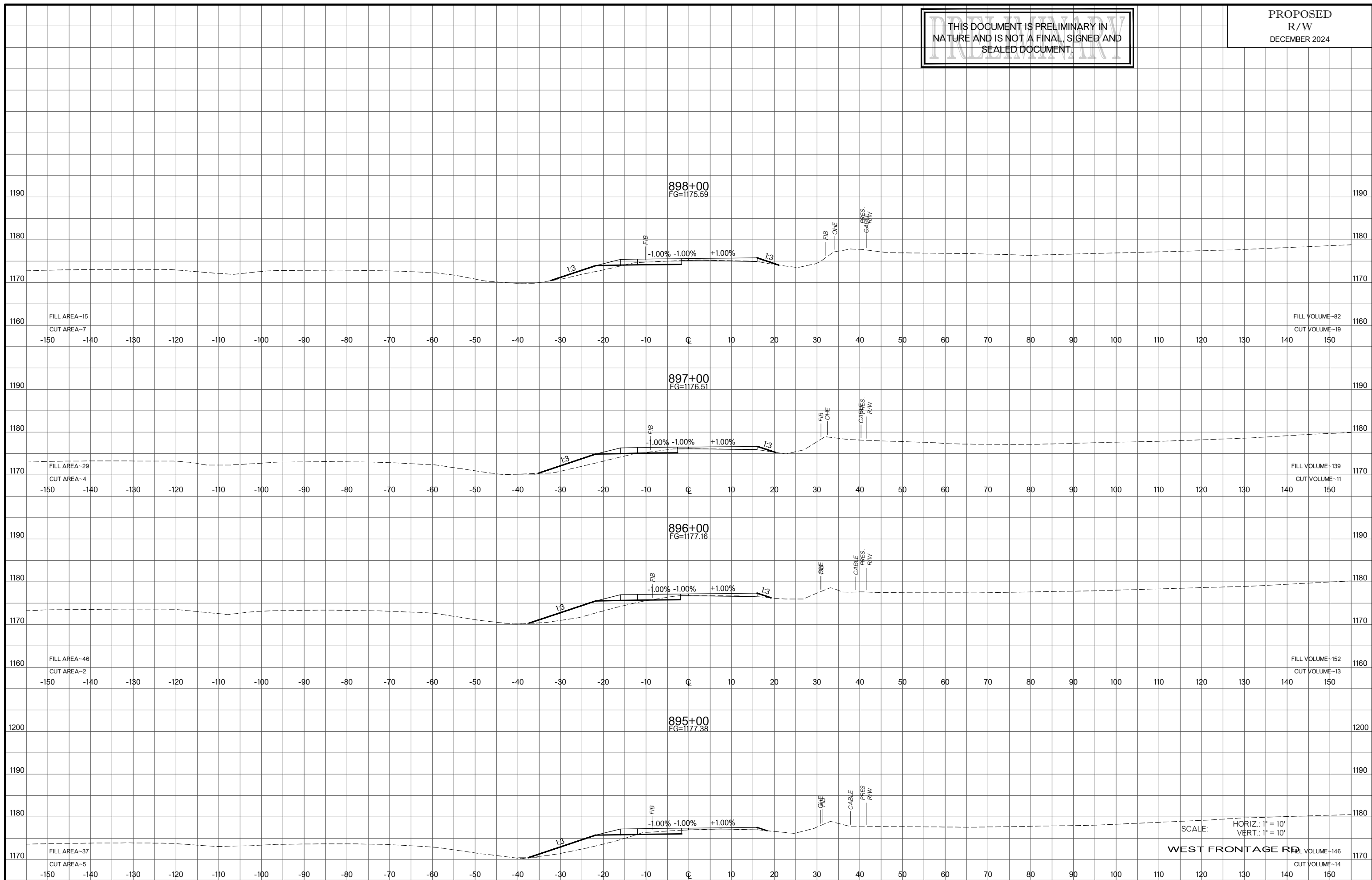


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WEST FRONTAGE RD

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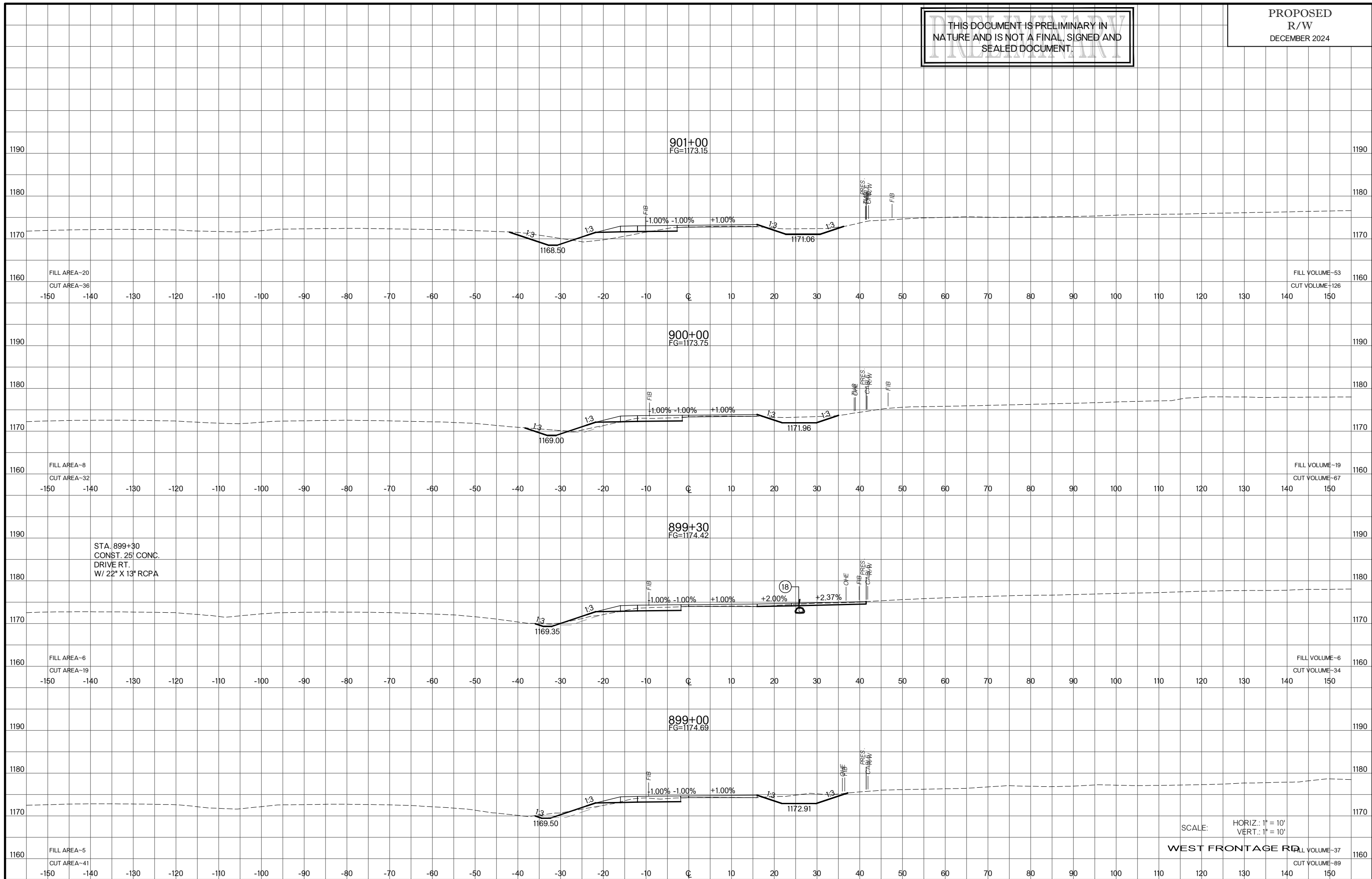


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WEST FRONTAGE RD

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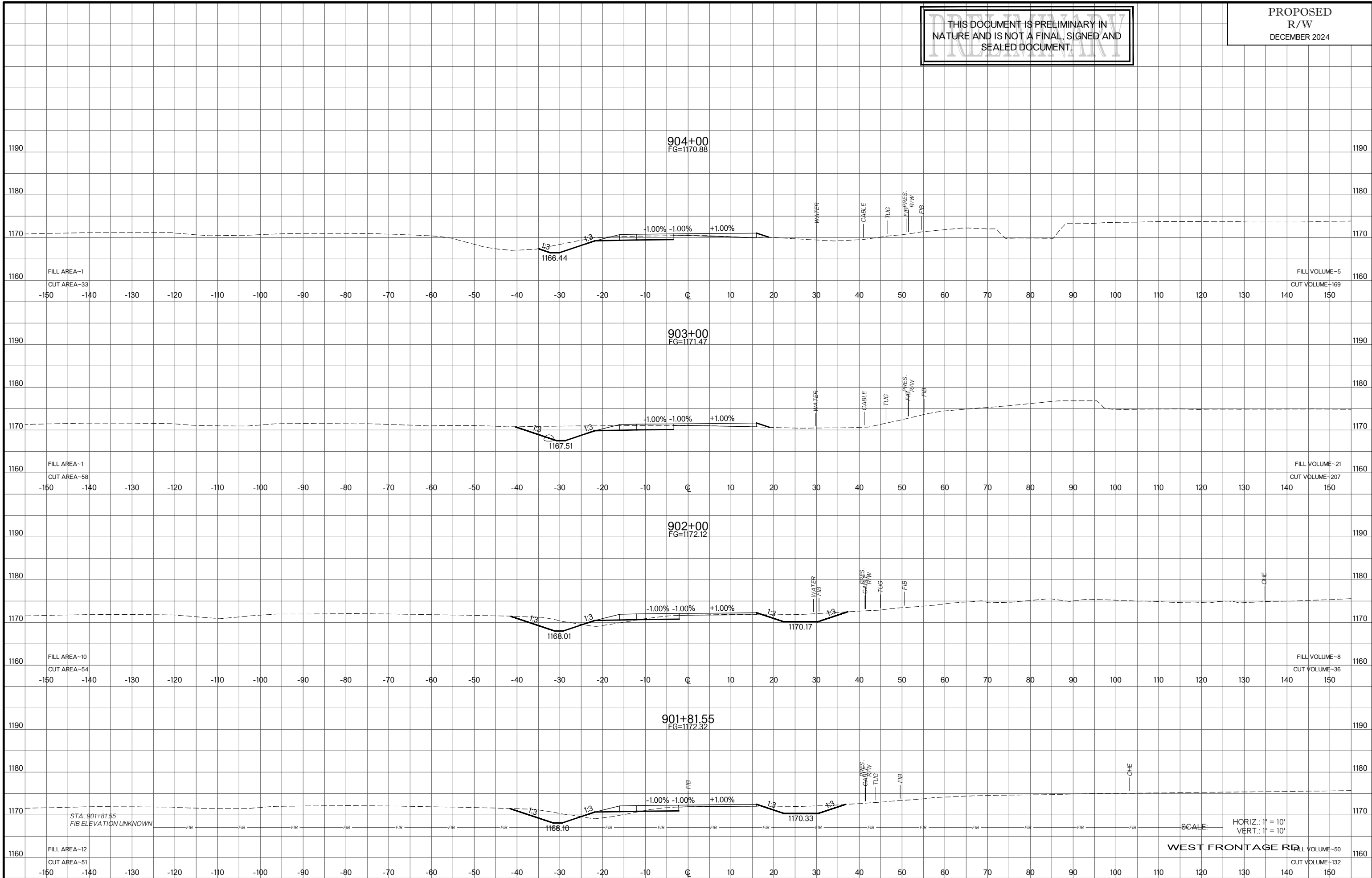
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WEST FRONTAGE RD

135/SH 74  
MCCLAIN COUNTY

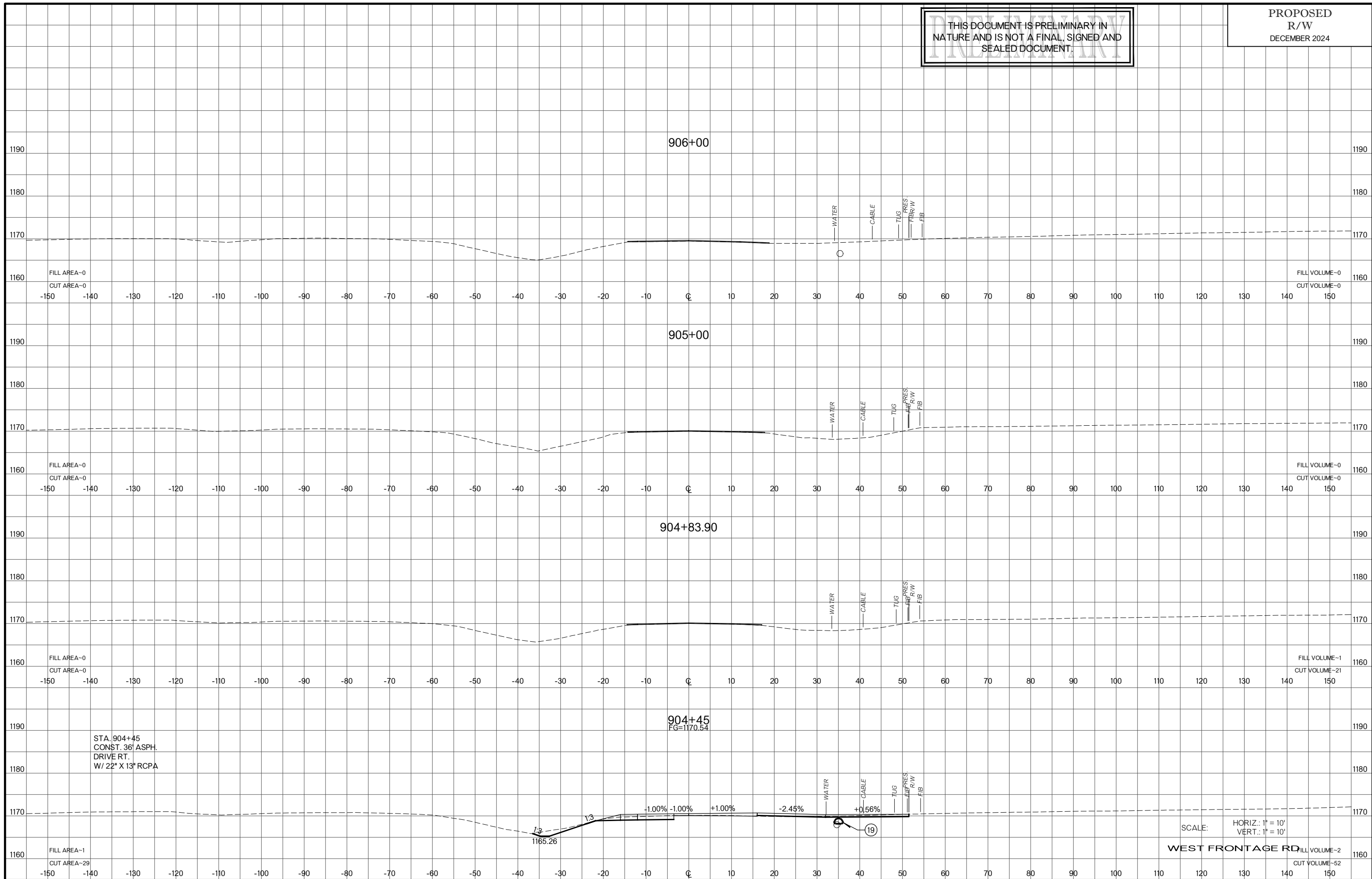
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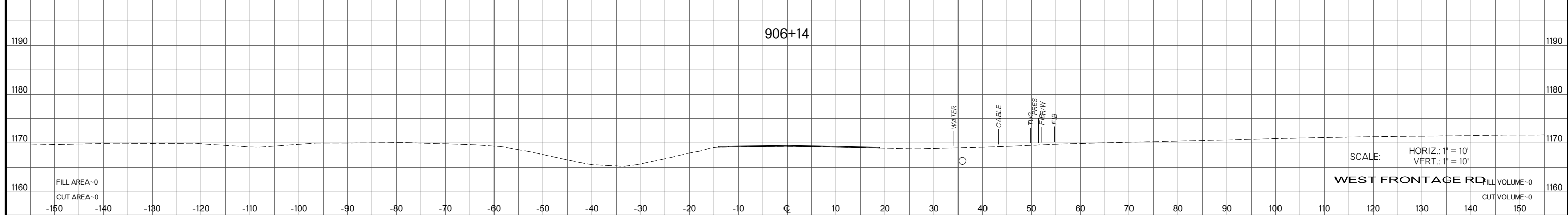
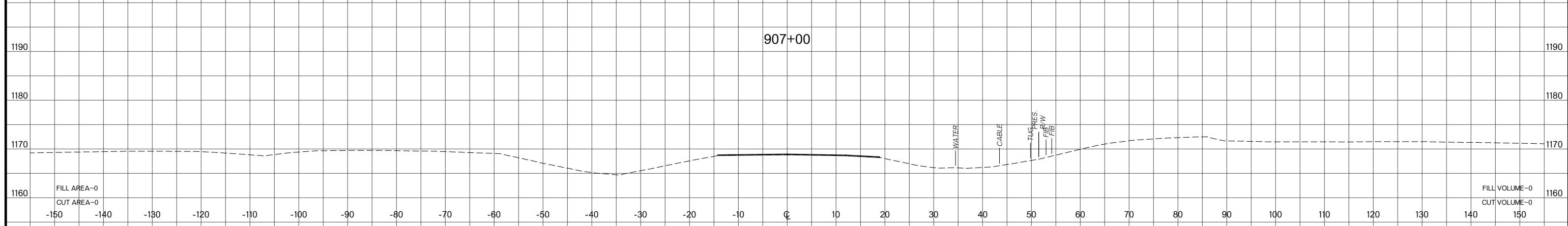
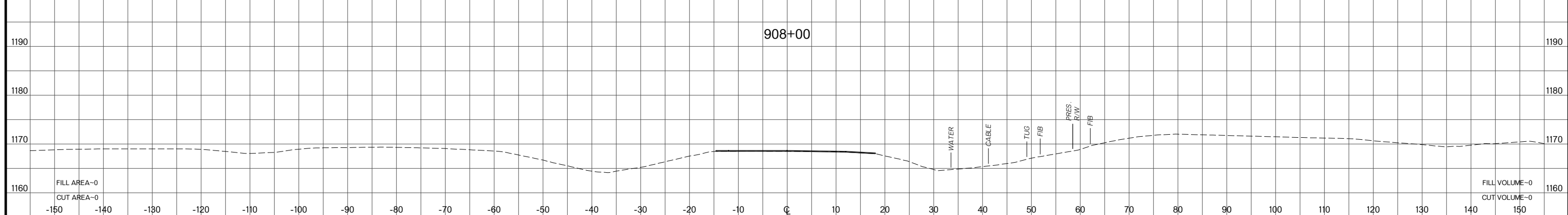
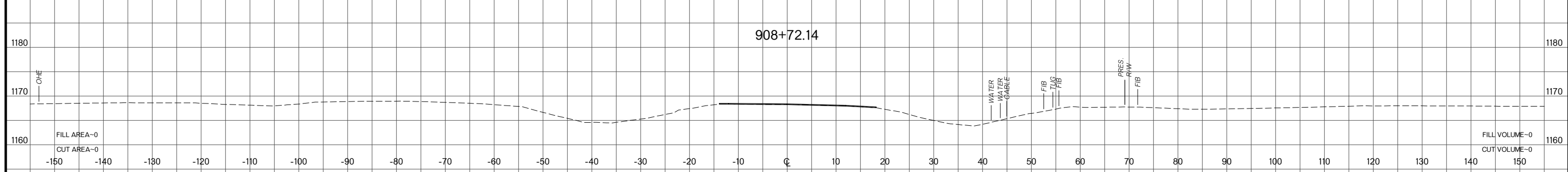
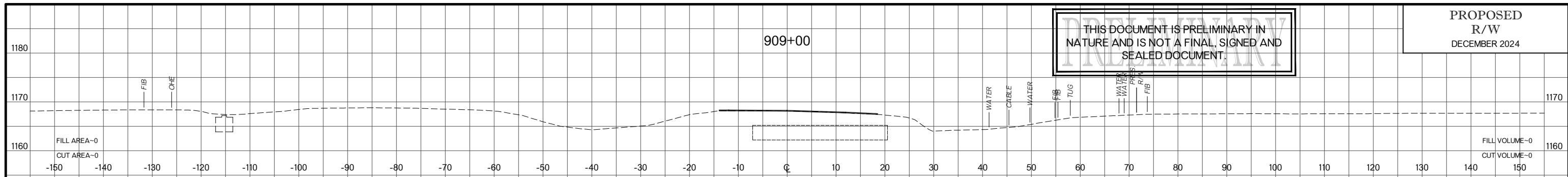
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VERT.: 1" = 10'

WEST FRONTAGE RD

MCCLAIN COUNTY 135/SH 74

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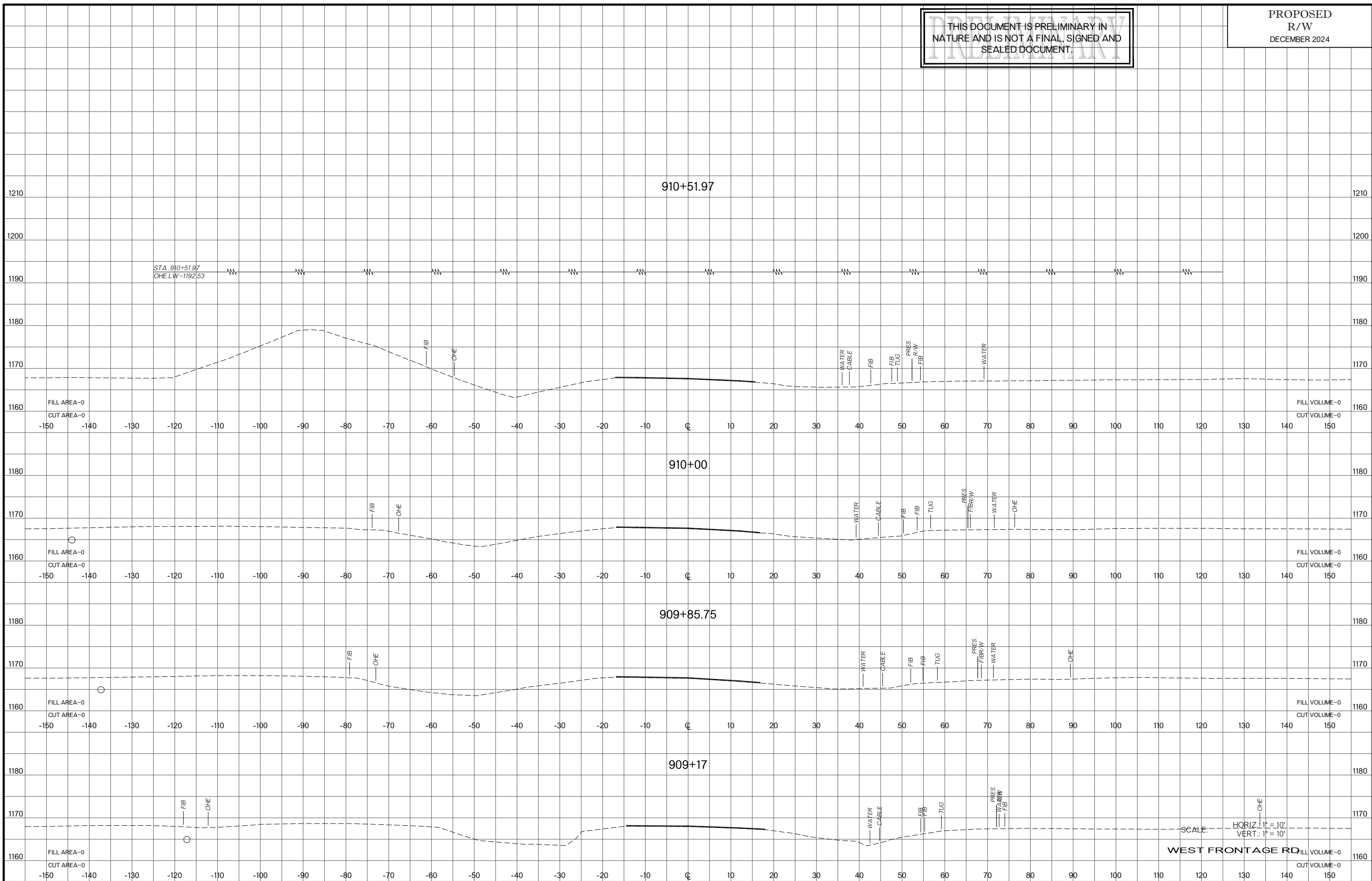


SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

WEST FRONTAGE RD

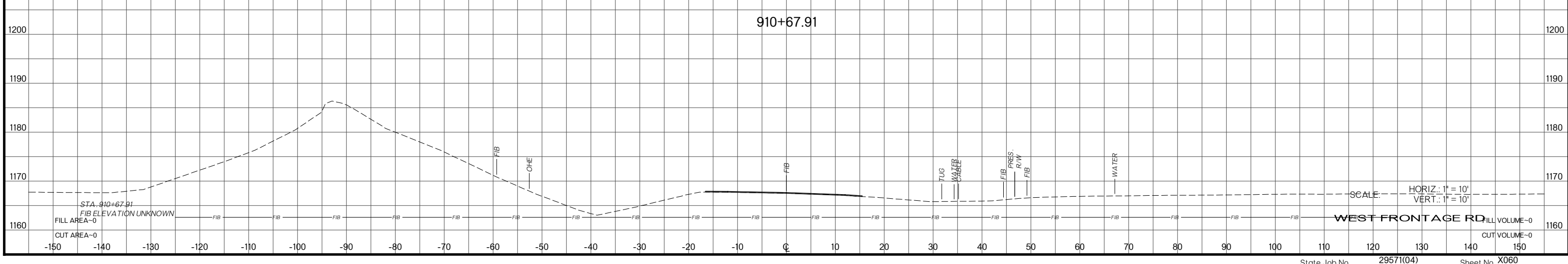
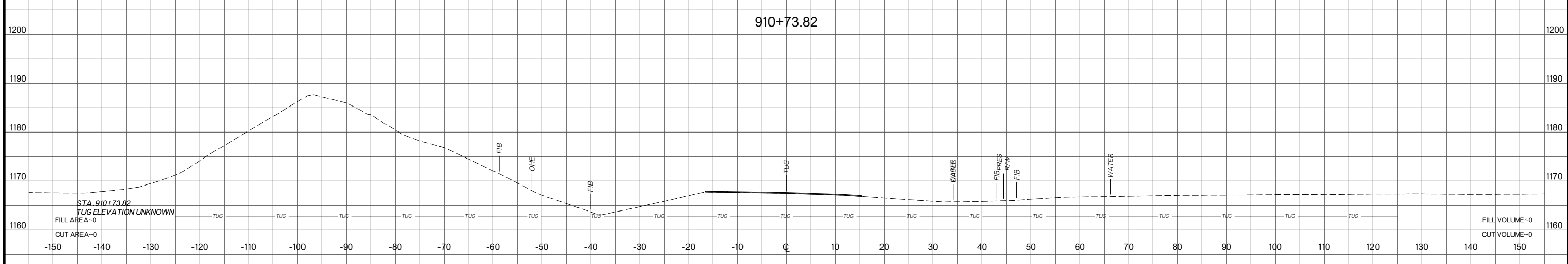
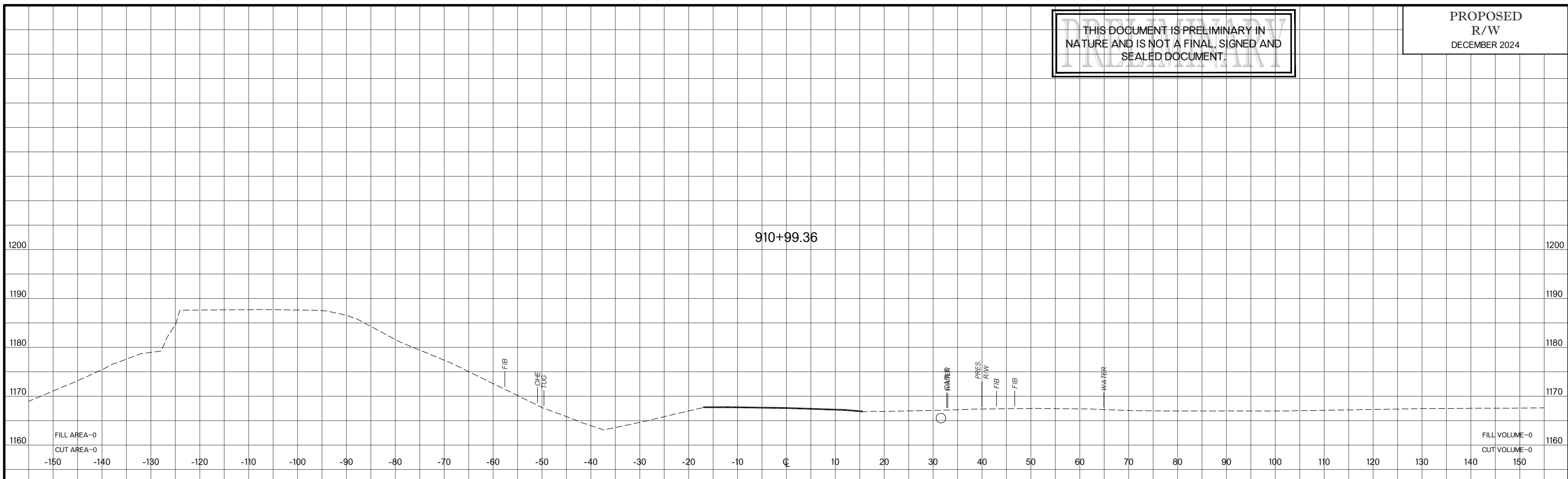
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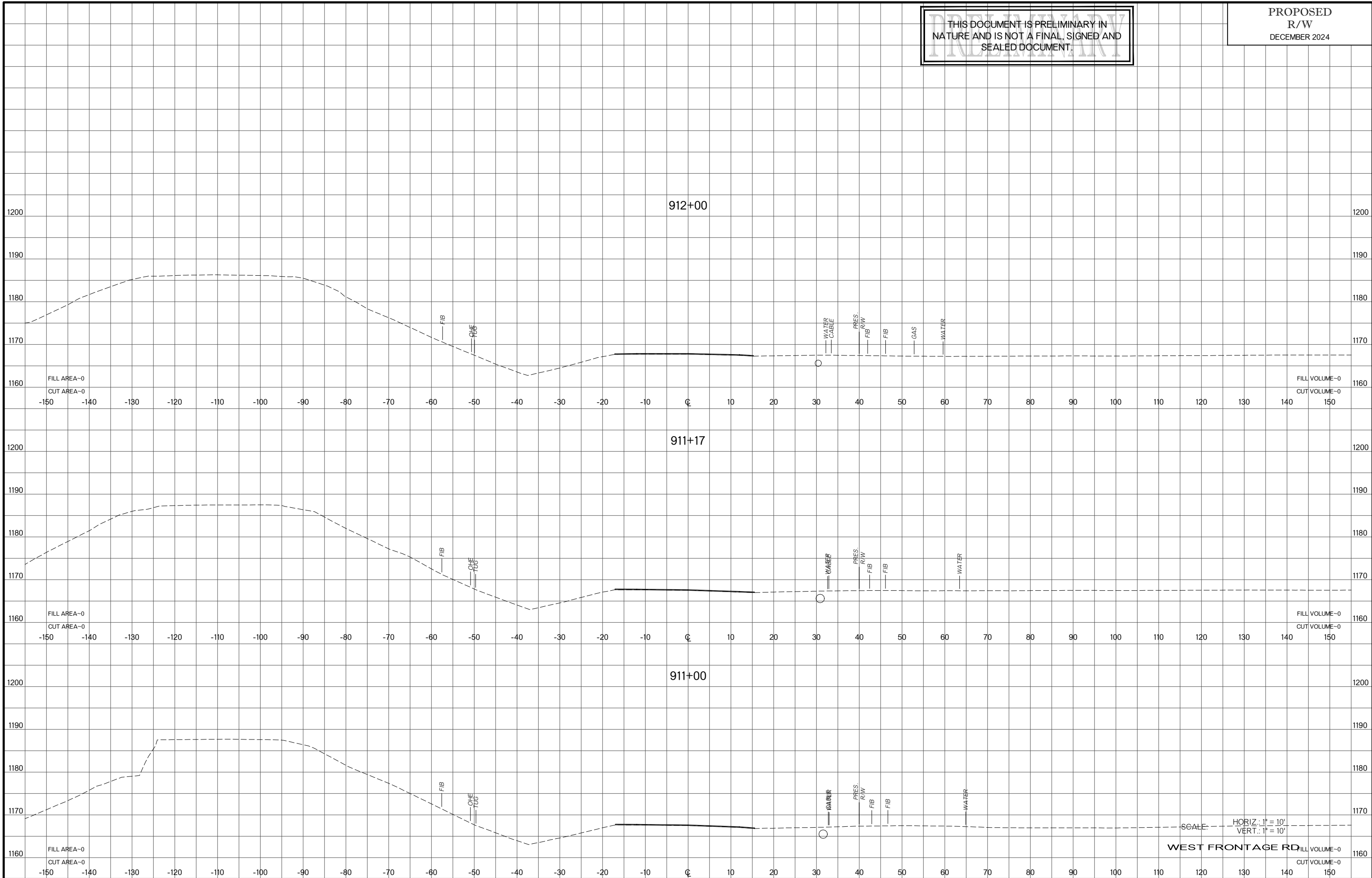
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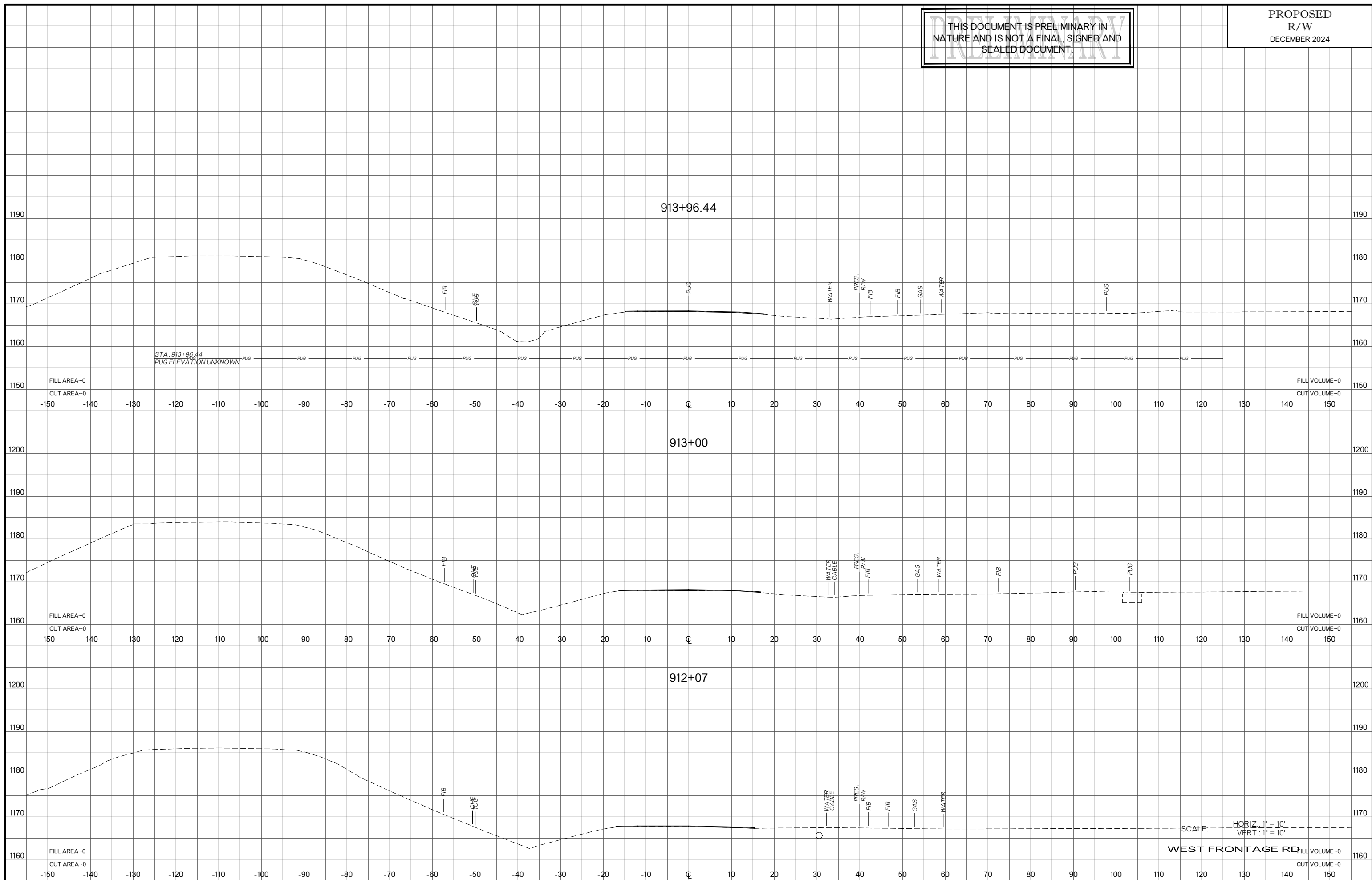
SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

WEST FRONTAGE RD

135/SH 74  
MCCLELLIN COUNTY

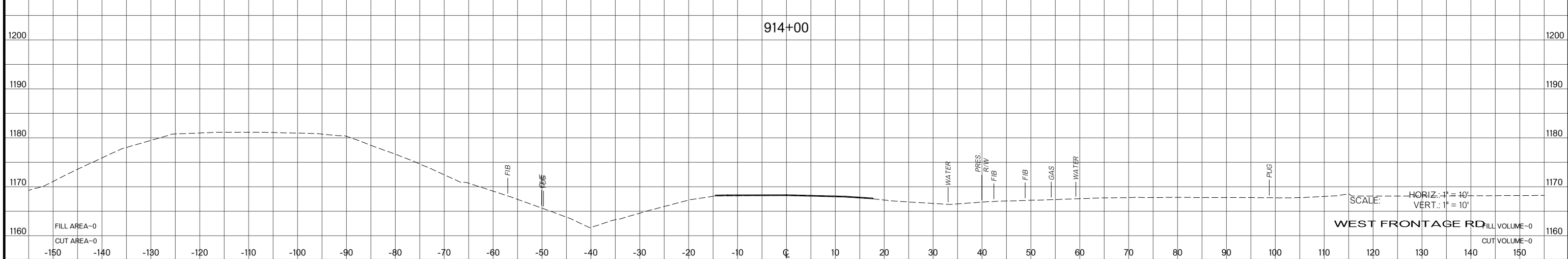
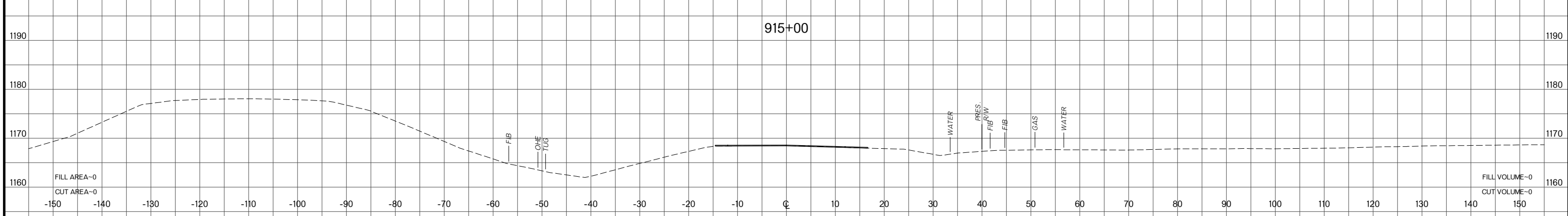
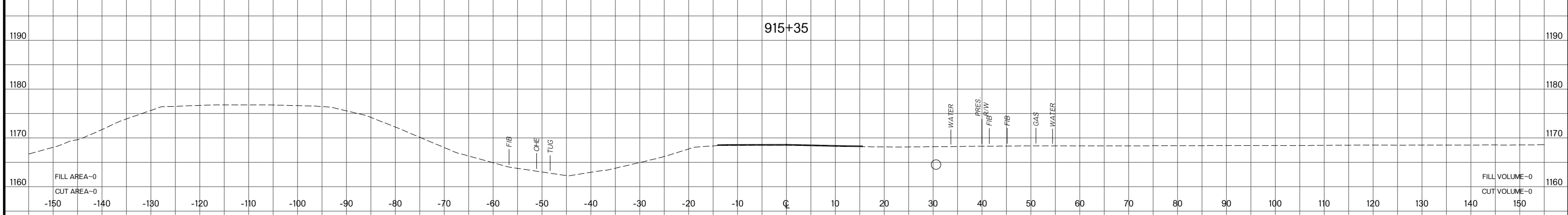
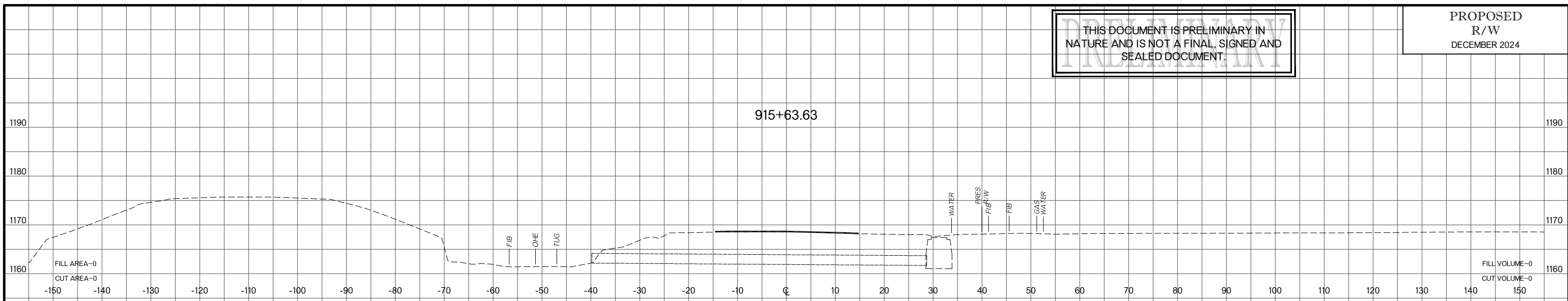
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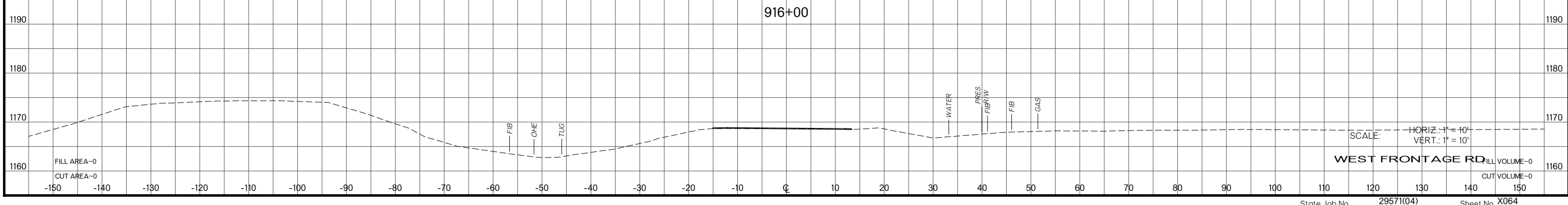
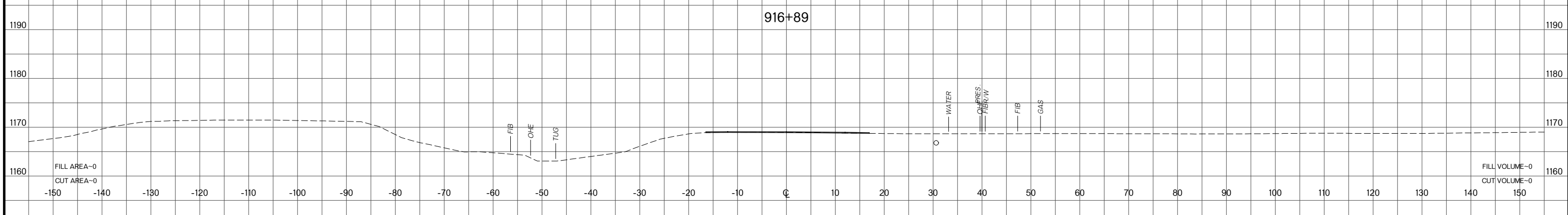
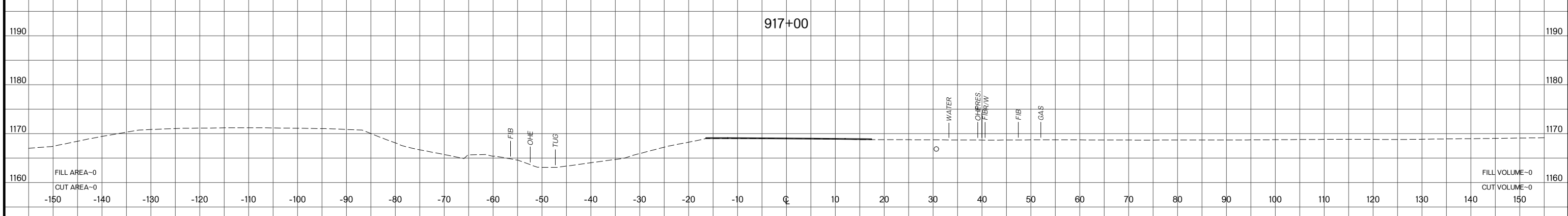
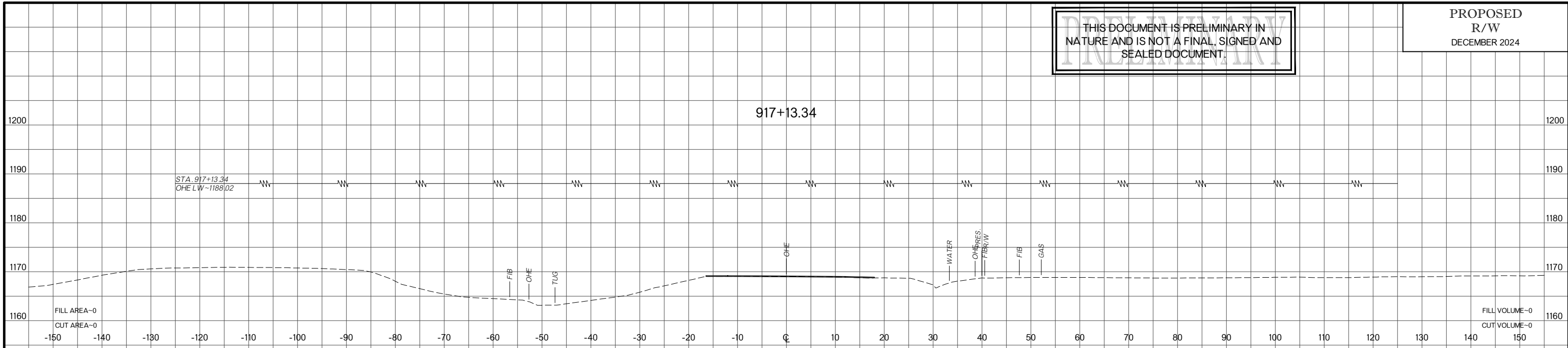
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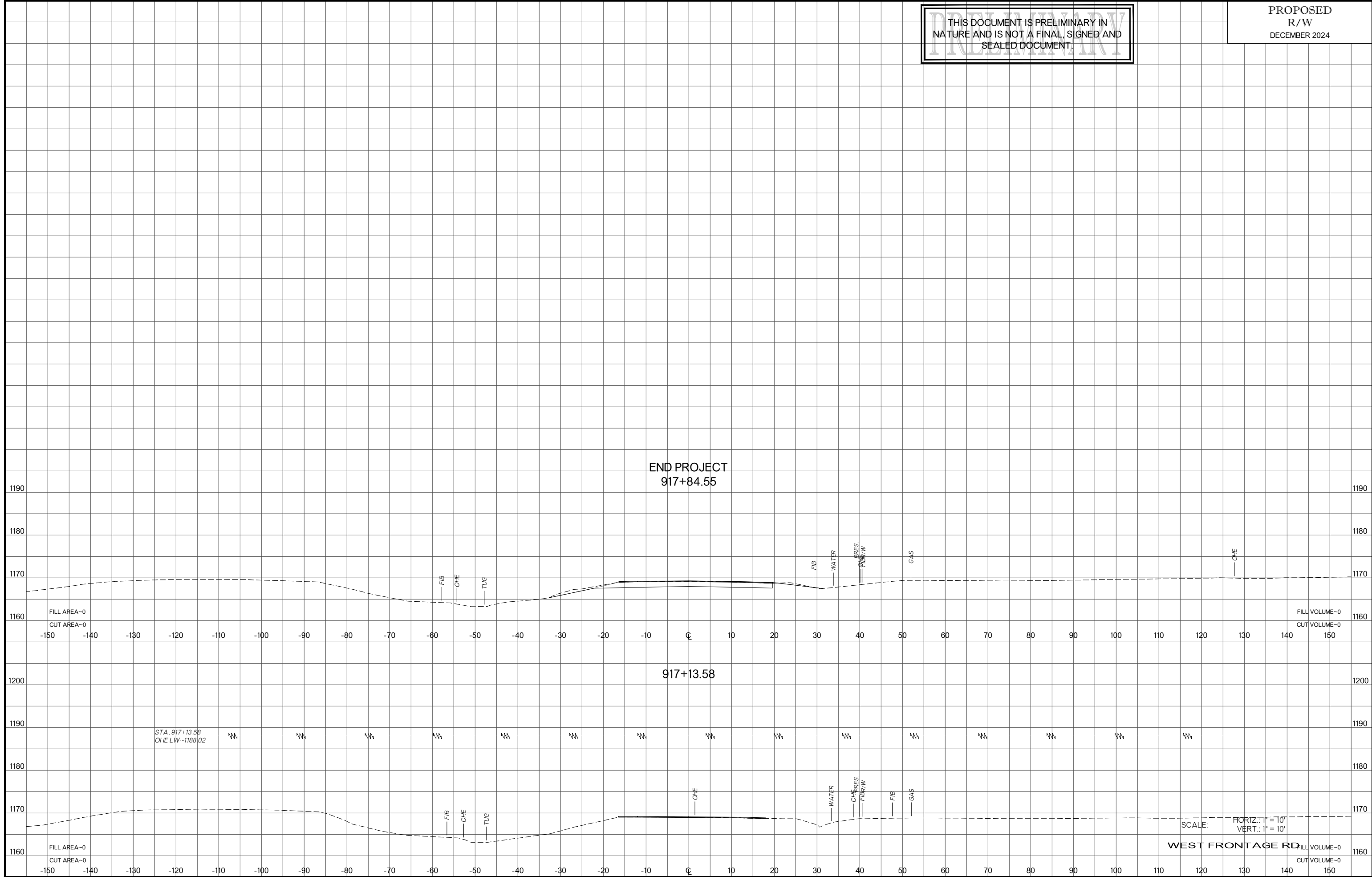
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VERT.: 1" = 10'

WEST FRONTAGE RD

135/SH 74  
MCCLELLIN COUNTY

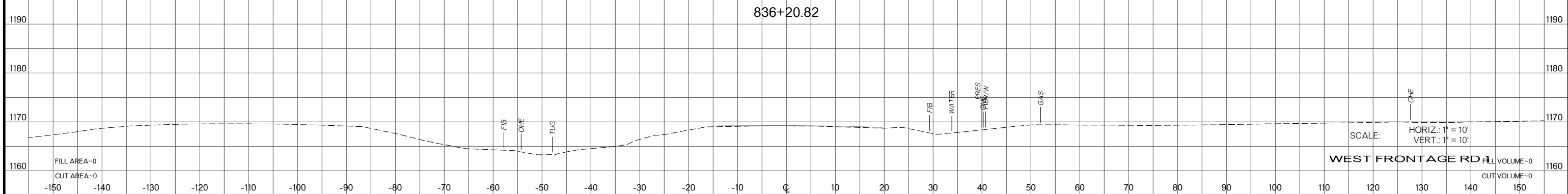
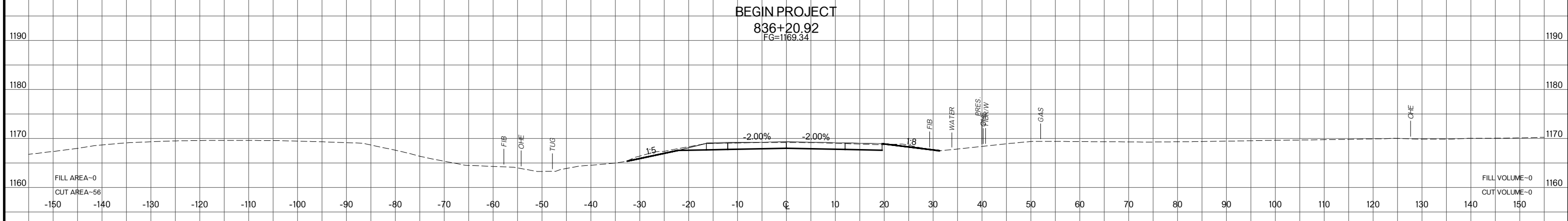
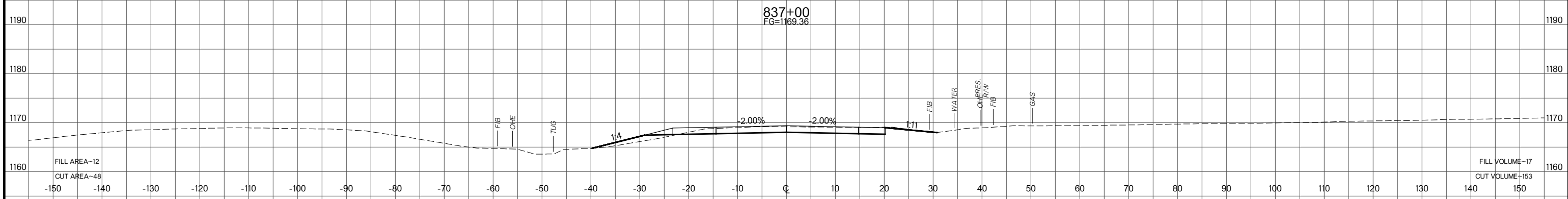
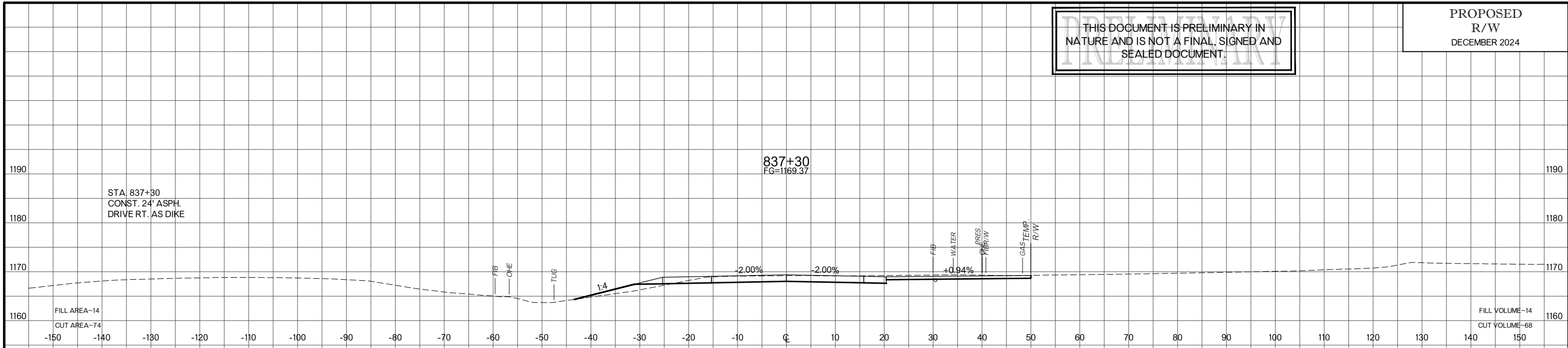
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SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

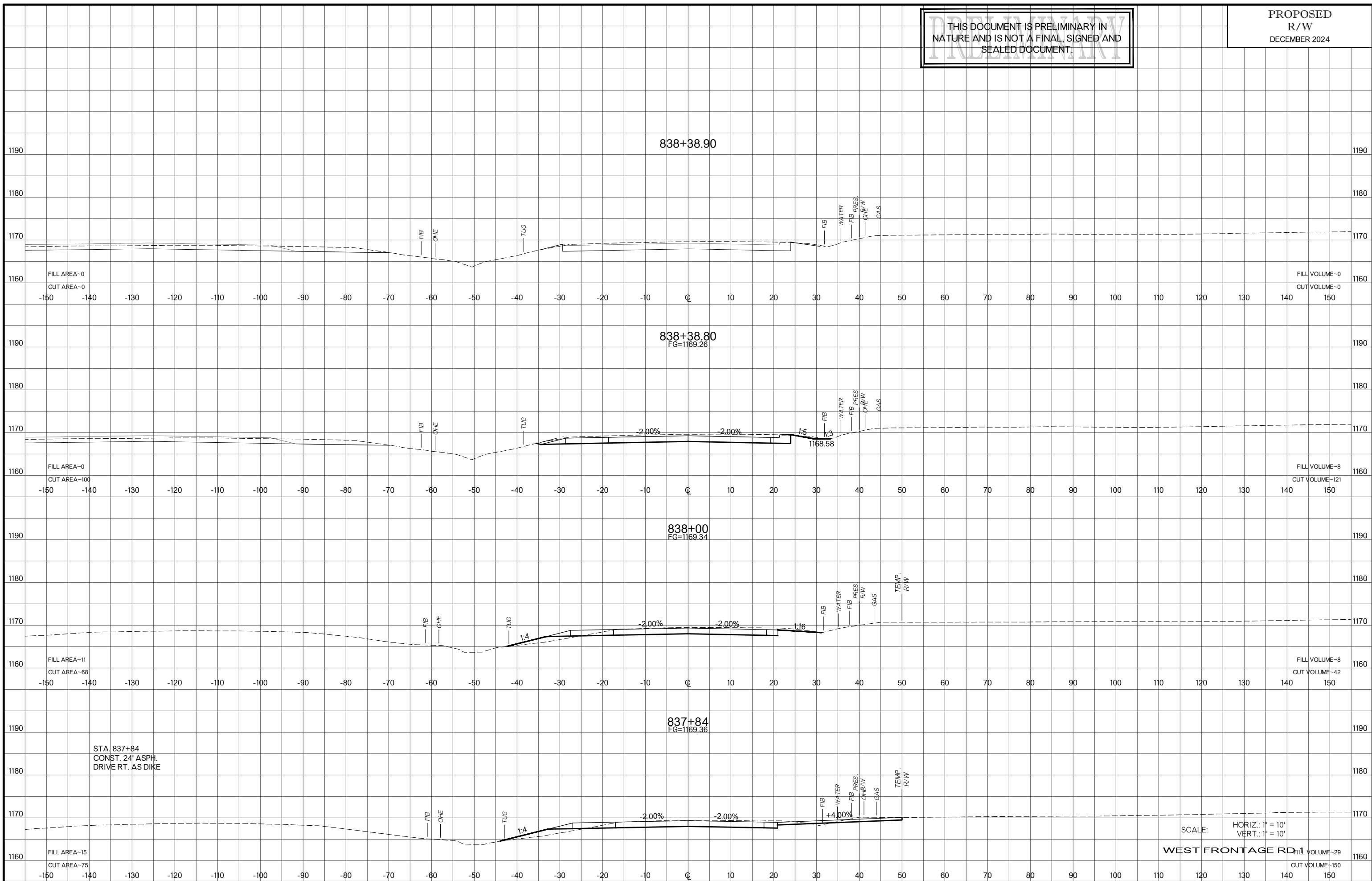
WEST FRONTAGE RD

State Job No. 29571(04) Sheet No. X066

135/SH 74  
MCCLELLIN COUNTY

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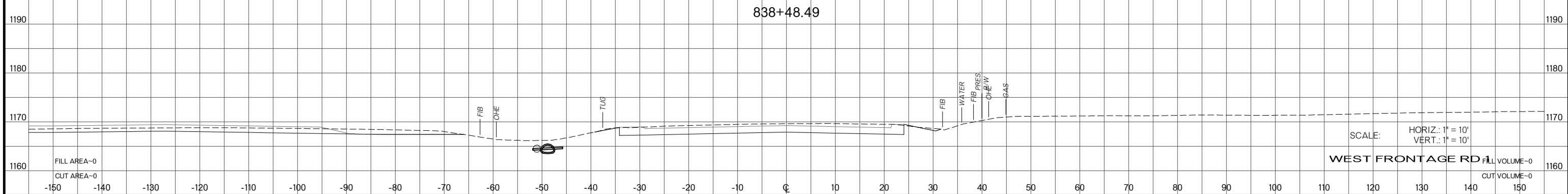
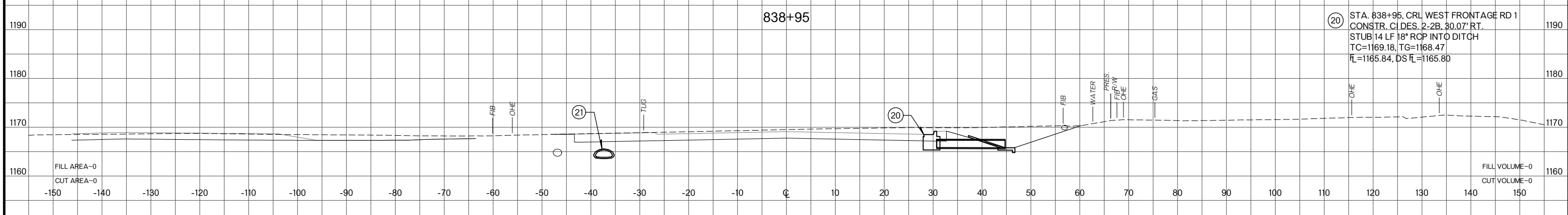
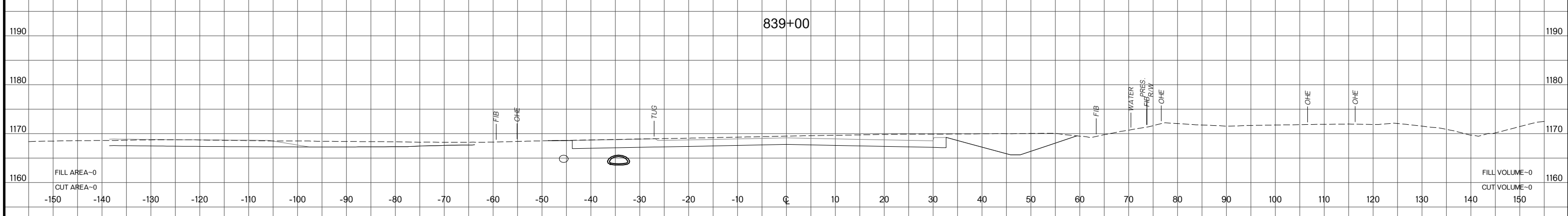
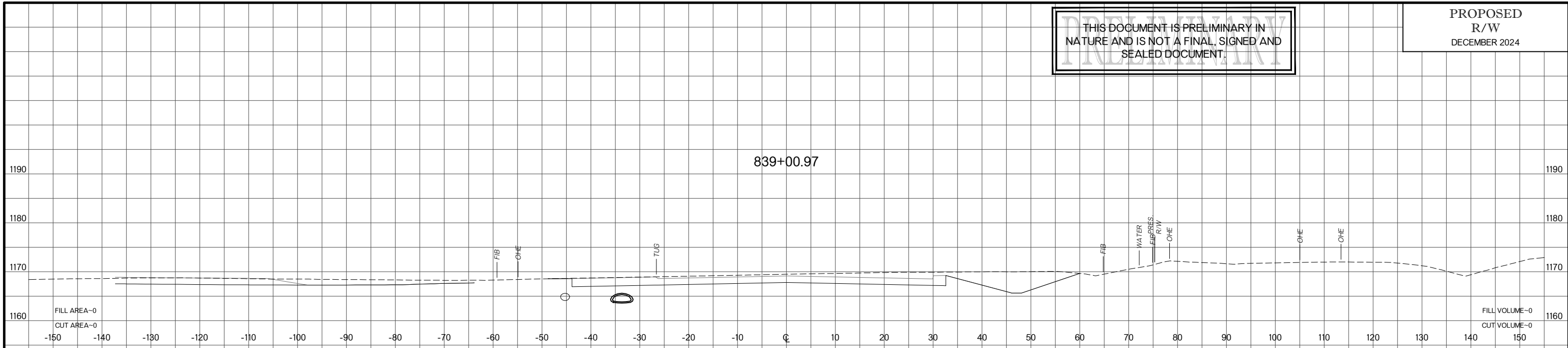


SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

WEST FRONTAGE RD. VOLUME-29  
CUT VOLUME-150

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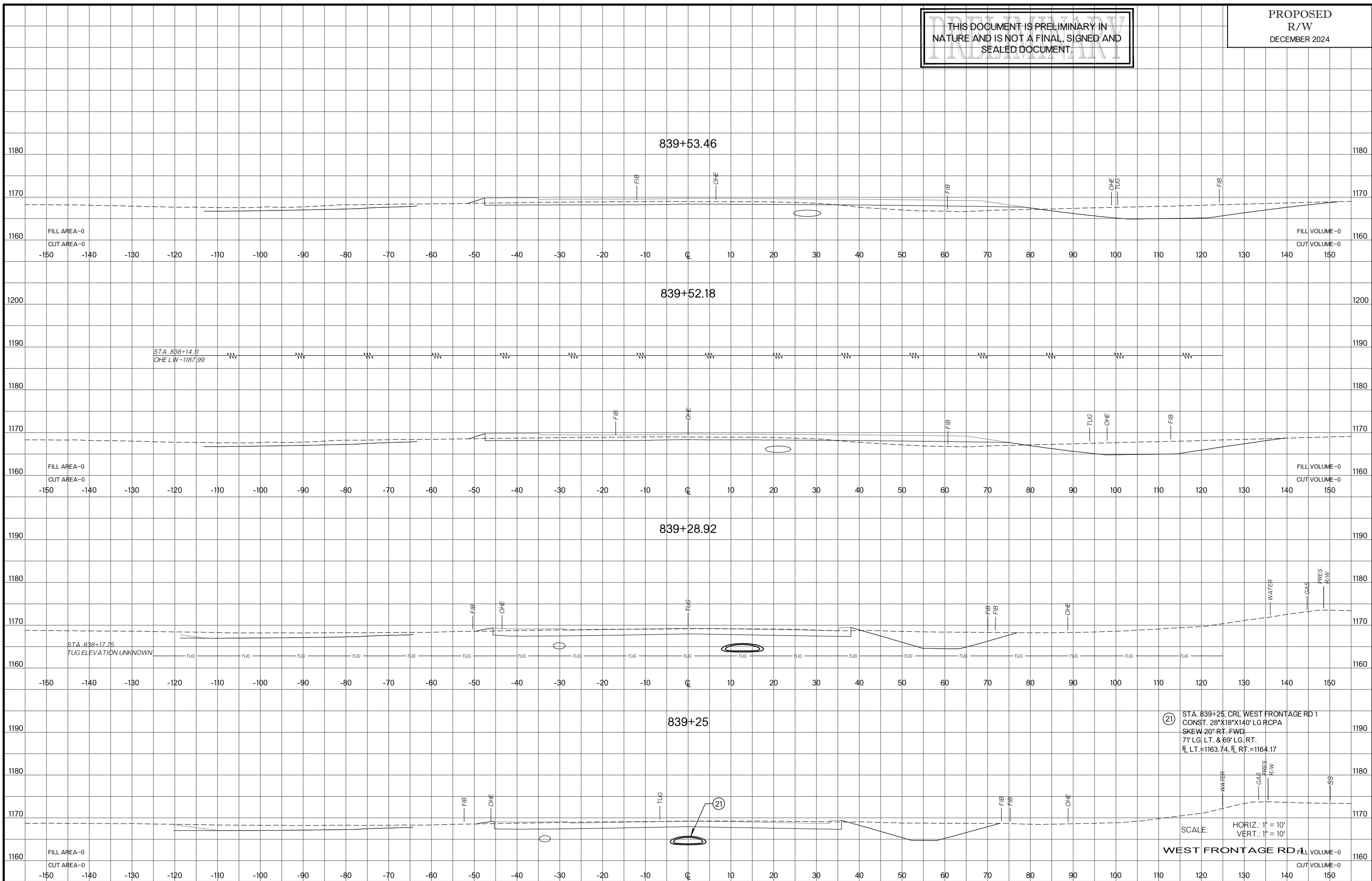


SCALE: HORIZ.: 1" = 10'  
VERT.: 1" = 10'

WEST FRONTAGE RD

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