

PROJECT FILE

CI-2341A Project:

**WIDEN, RESURFACE & BRIDGE SH-66:
FROM SH-102, EAST 4.0 MILES
TO US-177
LINCOLN COUNTY
J/P# 34318(04)**

Prepared For:



OKLAHOMA
Transportation

OKLAHOMA DEPARTMENT OF TRANSPORTATION

Environmental Programs Division
Oklahoma City, OK

Prepared By:



CC Environmental, LLC
PO Box 1292
Norman, OK 73070



Programmatic/Individual Categorical Exclusion

	PCE	X	ICE
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Date	12/5/2023	Project Number	J3-4318(004)
County	Lincoln	State Job Piece No:	34318(04)
NEPA Project Manager	Amanda Alexander	Phone Number	(405) 421-6820
ODOT Field District	3	Bridge NBI No. <i>(For County & State Projects)</i> & Location No. <i>(County Projects Only)</i>	26639, 12596
Project Description from JPINFO	Widen, Resurface & Bridge SH-66: From SH-102, east 4.0 miles to US-177		
This project is included in: <i>(Check all applicable ones)</i>	X	State 8 Year Construction Program	
		County 5 Year Construction Program	
	X	State Transportation Improvement Program	
This project has federal funds: <i>(Check applicable one.)</i>	X	Currently has Federal Funds	
		Potential for Future Federal Funds	
This project is in the Metropolitan Transportation Improvement Program (If applicable) <i>(Check applicable one)</i>		Yes	
	X	Not Applicable	

The Oklahoma Department of Transportation (ODOT) has completed the environmental analysis and review of the referenced project. ODOT has determined that this project does not individually or cumulatively have a significant impact of the environment as defined by the National Environmental Policy Act (NEPA) or involve unusual circumstances as defined in 23 CFR 771.117(b) and is therefore excluded from the requirements to prepare an Environmental Assessment or Environmental Impact Assessment.

<p>Existing Conditions:</p> <p>This SH-66 project consists of two discontinuous segments separated by a 1-mile-long Oklahoma Turnpike Authority (OTA) project, which is an exception. The first project segment begins roughly 0.35 mile east of the SH-102/SH-66 junction and extends east approximately 1.4 miles to just west of the I-44 Turner Turnpike southbound onramp. The second segment begins at the east side of the northbound onramp to I-44 (~1 mile east of the end of the first segment), and extends east 1.3 miles to the US-177/SH-66 junction. The existing SH-66 segment is an open section major collector facility with two 12-foot-wide paved driving lanes and 0 to 2-foot-wide shoulders. It has deteriorating pavement, narrow shoulders, poor traffic flow, and limited sight distances in places. Within the project areas there are two bridge sized structures—Bridge A (NBI #26639) over Captain Creek and Bridge B (NBI #12596) over Spring Creek. Bridge A is a 40-foot-wide span bridge with a sufficiency rating of 99.2. It is not structurally deficient, but needs some structural repairs. Bridge B is a 54-foot-long reinforced concrete box (RCB) with a 24-foot-wide roadway, and a sufficiency rating of 65.6. It is not structurally deficient, but is narrow and functionally obsolete.</p> <p>The 2022 Annual Average Daily Traffic (AADT) is 5,400 vehicles per day (vpd) with a future 30-year AADT of 7,000 vpd.</p>

Purpose & Need			
To correct a roadway with deteriorating pavement, narrow shoulders, poor traffic flow and limited sight distances, and to correct a functionally obsolete bridge and a bridge in need of repairs.			
Alternatives considered & Proposed Improvement			
The proposed improvements consist of symmetrically widening and reconstructing the roadway to create two 12-foot-wide driving lanes with 8-foot-wide paved shoulders. To improve traffic flow near the southbound turnpike entrance, a 12-foot-wide center turn lane and a dedicated right turn lane will be added. The 12-foot-wide center turn lane matches the three-lane facility associated with the OTA project and continues east to roughly 800 feet east of county road NS-3350, where the roadway transitions to a two-lane facility with 8-foot-wide paved shoulders. In both project segments, skewed driveways and entrances will be realigned to address limited sight distances. Bridge A will be repaired to correct pier and abutment deficiencies, and Bridge B will be extended to create a 99-foot-long structure with an 84-foot-wide clear roadway width.			
All improvements are anticipated to occur on existing alignment, but the acquisition of new right-of-way will be required. The roadway will remain open during construction by alternating traffic lanes and the use of a shoofly detour for Bridge B. A north alignment was selected for the lane shift in order to avoid impacts to wetlands on the south side of the bridge.			
Did the project have public involvement (<i>Check the applicable items and include public involvement <u>summary</u> and supporting documents in the appendix</i>)			
<input checked="" type="checkbox"/>	Property Owner Notification		Road Closure Letter
	Legal Notice/Website Posting	<input checked="" type="checkbox"/>	Small City Letter
			Public/Stakeholder Meeting
			None

All documentation, analyses, and agency coordination regarding this Categorical Exclusion are attached to this document and maintained in the project file at the Oklahoma Department of Transportation, Environmental Programs Division.

Criteria Identified in Section IV.A.1.b. of the 2019 FHWA/ODOT Programmatic Agreement for Processing Categorical Exclusions that would require Individual Review and Approval by FHWA:		
Check Yes or No below. If the answer to any of the questions below is Yes, an Individual CE will be required.		
Description/Question	Yes	No
i. Does the project result in capacity expansion of a roadway by addition of through lanes?		<input checked="" type="checkbox"/>
ii. Does the project involve any permanent changes limits of access control or to the operation of an Interstate highway, associated interchanges or ramps or requires an Access Justification Report (AJR)?		<input checked="" type="checkbox"/>
iii. Is the project not included in or is inconsistent with the statewide transportation improvement program, and in applicable urbanized areas, the transportation improvement program?		<input checked="" type="checkbox"/>
iv. Does the project involve acquisition of more than minor right-of-way not adjacent to the existing facility?		<input checked="" type="checkbox"/>
v. Does the project involve residential or commercial relocation?	<input checked="" type="checkbox"/>	
vi. Does the project include acquisition of land for hardship or protective purposes, or early acquisition pursuant to Federal acquisition project (23 U.S.C. § 108(d))		<input checked="" type="checkbox"/>
vii. Does the project have potential for disproportionately high and adverse impact on minority or low-income populations, based on known demographics in the project vicinity, extent of R/W, relocations, and other identified impacts?		<input checked="" type="checkbox"/>
viii. Does the project involve property in which another Federal Agency or Federally Recognized Tribe has ownership, oversight or any other encumbrance?		<input checked="" type="checkbox"/>
ix. Does the project involve a determination of adverse effect by Oklahoma State Preservation		<input checked="" type="checkbox"/>

Criteria Identified in Section IV.A.1.b. of the 2019 FHWA/ODOT Programmatic Agreement for Processing Categorical Exclusions that would require Individual Review and Approval by FHWA:		
Check Yes or No below. If the answer to any of the questions below is Yes, an Individual CE will be required.		
Description/Question	Yes	No
Office (SHPO) or a designated Tribal Historic Preservation Office (THPO) in accordance with Section 106?		
x. Does the project involve a Programmatic Section 4(f) or de minimis finding which has not been previously approved by FHWA?		X
xi. Requires the acquisition of lands under the protection of Section 6(f) of the Land and Water Conservation Act of 1965 (54 U.S.C. § 200305), the Federal Aid in Sport Fish Restoration Act (16 U.S.C. 777-777k, 64 Stat. 430), the Federal Aid in Wildlife Restoration Act (16 U.S.C. 669-669i; 50 Stat. 917), or other unique areas or special lands that were acquired in fee or easement with public-use money and have deed restrictions or covenants on the property		X
xii. Does the project involve any impact on Noise Abatement Criteria (NAC) Category A, B, C or D receptors?		X
xiii. Does the project involve a finding of “may effect, likely to adversely affect” determination under Section 7 of the Endangered Species Act or the Bald and Gold Eagle Protection Act and can be processed as under programmatic agreement?		X
a. Does the project involve a Section 7 Formal Consultation Process prior to start of construction?		X
xiv. Does the project require an Individual Section 404 Permit (This is generally for major River Crossings, waters or wetlands impact greater than 3.0 AC, Projects with Formal Consultation, structures on new alignment or others as determined by USACE.)?		X
xv. Does the project involve construction across or adjacent to a river designated as a component in the National System of Wild and Scenic Rivers?		X
xvi. Does the project require a Coast Guard Permit?		X
xvii. Does the project involve an adverse impact on prime farmland where Natural Resources Conservation Agency (NRCS) has required consideration of alternatives and measures to avoid and minimize impacts?		X
xviii. Does the project involve increase to the base 100 Year floodplain in a regulatory floodway (Zone A-E in a FEMA Map) that will require a flood map revision as determined by the appropriate state or local authority?		X
xix. Does the project not conform to the State Implementation Plan which is approved or promulgated by the U.S. Environmental Protection Agency in air quality non-attainment areas		X
xx. Does the project involve any known Superfund site?		X
xxi. If the project involves road or bridge closure or ramp closure, do any of the following conditions apply? (Check the boxes ONLY if the project involves road closure)		
a. No Access will be provided to local traffic or posted		
b. Through traffic dependent businesses will be affected		
c. The detour or closure will substantially alter the environmental consequences of the action, such as by creating unsafe conditions on the detour route or requiring additional work or expansion to detour routes to carry the additional traffic.		
d. There is a public controversy associated with the detour or closure		
e. The detour closure will interfere with special events or activities		
xxii. Does the project have substantial public or agency controversy on environmental grounds?		X

Explanation for Individual CE (If any of the answers above are YES):	
Item for which the answer is YES	v.
Explanation that CE Classification is appropriate	
<p>A Relocation Plan (Plan) completed on August 15, 2023 determined that one single-family residence relocation may be necessary due to the impact of the proposed right-of-way as depicted in the Proposed Right-of-Way plans dated June 6, 2023. Since this project involves symmetrical widening along the existing alignment, no communities will be separated, and the impacts and benefits to all population groups would be about the same. The proposed project is not anticipated to have a disproportionately high or adverse impact on minority or low-income populations within the study area.</p> <p>Acquisition and relocation assistance would be consistent with the U.S. Department of Transportation (USDOT) policy, as mandated by the Uniform Relocation Assistance and Real Properties Acquisitions Act (URARPA), as amended in 1987 (49 Code of Federal Regulations [CFR] 24.205 Relocation PI Services, and Coordination). Just compensation is based upon fair market value of the property.</p>	
Item for which the answer is YES	N/A
Explanation that CE Classification is appropriate	
N/A	
Pre-Construction Commitments:	
<p>The action may involve work in potentially jurisdictional waters and potentially jurisdictional wetlands. For State Projects, the 404 permit application form needs to be submitted by the Designer through Project Management Division to Environmental Programs Division at the time of Right-of-Way submittal for evaluation and determination of the appropriate Clean Water Act Section 404 permit application for the project.</p> <p>Monarch Commitment: ODOT, as a Certificate of Inclusion partner in the Nationwide Monarch Butterfly CCAA for Energy and Transportation lands, will adhere to the conservation measures, as well as minimize threats to the monarch butterfly as stipulated in this CCAA.</p>	
Right-of-Way and Utility Commitments	
<p>The following Construction Commitments requiring avoidance, restrictions or minimization of natural and human resources during Right-of-Way clearance and Utility relocation activities will be discussed with the Right-of-Way and Utility Owners at the start of Right-of-Way and Utility Process.</p>	
Construction Commitments	
<p>ODOT Commitment: All operators, employees, and contractors will be made aware of all environmental commitments, including the following Plan Notes.</p> <p>The following plan notes requiring avoidance, restrictions, or minimization of natural and human resources in the project and off-site project areas will be added to the final project plans under “Environmental Mitigation Notes” per policy Directive C-201-2.</p>	
Cultural Resources Avoidance:	
<p>Locations outside the project area in the following area must not be utilized for borrow, equipment staging, haul roads, spoil dumps or any off-site project-related activity.</p> <p>T14N R3E Section 20 NW ¼ NE ¼ NW ¼</p>	
<p>The Resident Engineer shall invite the ODOT Cultural Resources Specialist (phone number 405-325-7201) for the pre-work conference to discuss the placement of a commemorative sign detailing the “Wellston Gap.”</p>	

Species Plan Notes:

Non-Compliance: Failure to implement the commitments specified in the Plan Notes can result in non-compliance issues on the project. Work activities may be suspended on the project, for an undetermined duration, while working with regulators to bring the project back into compliance. The contractor will not be compensated for time lost.

Water Quality Conservation: Appropriate Best Management Practices to minimize impacts from storm water discharges and sedimentation in streams, as established by the Oklahoma Department of Environmental Quality, shall be conscientiously implemented throughout the proposed construction periods, in order to minimize any potential impacts to any listed species. The effectiveness of erosion controls shall be maintained for the duration of construction activities. Hazardous materials, chemicals, fuels, lubricating oils, and other such substances shall be stored at least 100 feet outside of the ordinary high water mark (OHWM). Refueling of construction equipment shall also be conducted at least 100 feet from the OHWMs. Sediment and erosion controls shall be installed around staging areas to prohibit discharge of materials from these sites. Construction waste materials and debris shall be stockpiled at least 25 feet outside of the OHWMs, and these materials shall be removed and disposed of properly following completion of the project. Preventative measure must be taken to prohibit the discharge of contaminants into any surface waters.

Bald Eagle Note: Suitable nesting, roosting or foraging habitat for the Bald Eagle occurs within the project's action area. The Bald Eagle nesting season in Oklahoma extends from September 16, through May 31. The Resident Engineer shall contact the ODOT Biologist to schedule a nest survey. Nest search surveys can only be conducted when leaves are not on the trees typically between December 1st and February 28th. No work may occur within suitable Bald Eagle habitat, located the full extent of the project area, during the nesting season (September 16, through May 31) until the completion of the survey by the ODOT Biologist. If nests are observed, a no-work buffer up to a distance of 660 feet shall be placed around the nest. The exact distance of the buffer zone shall be established by the ODOT Biologist in consultation with US Fish and Wildlife Services. If the buffer cannot be maintained, all clearing, external construction and landscaping activities, within the buffer, shall be conducted between June 1 and September 15 (outside the nesting season).

Migratory Bird Note: Migratory birds are protected by the federal Migratory Bird Treaty Act. Many birds commonly use bridges and culverts for nesting. The nesting season for most migratory bird species extends from March 1 to August 31. Migratory bird nesting use of the Captain Creek and Spring Creek bridges (NBI:26639 and 12596) and three culverts at STA 1650+22, STA 1658+85 and STA 1795+50 RT was observed. Painting, repair, retrofit, rehabilitation or demolition of the existing bridges and culverts shall be conducted between September 1, and February 28, when migratory bird nests are not occupied. If painting, repair, retrofit, rehabilitation, or demolition cannot be completed between September 1 and February 28, the bridges and culverts shall be protected from new nest establishment prior to March 1, by means that do not result in bird death or injury. Options include the exclusion of adult birds from suitable nest sites on or within a structure by the placement of weather-resistant polypropylene netting with 0.25-inch or smaller openings, prior to March 1. Methods other than netting must be pre-approved by the ODOT Biologist.

Although no nests were observed on all other structures, the birds may occupy the structures in the future. The Resident Engineer shall contact the ODOT Biologist if any bird use of these structures is observed. If birds are observed then painting, repair, retrofit, rehabilitation or demolition of the existing bridges and culverts shall be conducted between September 1, and February 28 (when migratory bird nests are not occupied).


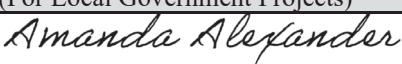
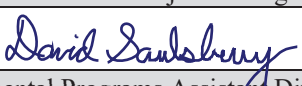

<u>Species</u>	<u>Seasonal Restriction Period</u>
Bald Eagle	September 16 – May 31
Migratory Birds: Swallows and Phoebes (NESTS PRESENT)	March 1 – August 31

The Environmental Programs Division shall provide **the final plan sheet with the mitigation notes** to the Designer for inclusion in Final Plans and keep a copy for the project records. The mitigation measures above should be discussed at all Pre-work conferences per Policy Directive C-201-2.

All documentation, analyses, and agency coordination regarding this Categorical Exclusion are contained in a Supporting Appendix maintained in the project file at the Oklahoma Department of Transportation, Environmental Programs Division.

Development of the project including coordination and assessment of potential social, economic and environmental impacts has been considered in accordance with DOT ORDER 5610.1C, and CEQ REGULATIONS 40 CFR 1500 - 1508 as amended, 23 CFR 771.117 and the 2019 FHWA/ODOT Programmatic Agreement for processing of categorical exclusions. Implementation of this action as a “Categorical Exclusion” will satisfy the requirements of the National Environmental Policy Act.

Preparer/Reviewer Signatures

 Geoffrey A. Canty	12/5/2023	
Environmental Consultant Project Manager (If Applicable)	Date	
CC Environmental, LLC	-	
Environmental Consultant Firm Name (If Applicable)	Date	
N/A	-	
County Commissioner or City Manager (For Local Government Projects)	Date	
 Amanda Alexander	12-6-2023	
ODOT NEPA Project Manager	Date	
 David Salsbery	12/6/2023	
ODOT Environmental Programs Assistant Division Manager	Date	
 Joe Burbata	12/6/2023	
ODOT Environmental Programs Division Manager	Date	
CONCLUSION:		
ODOT has reviewed the conditions identified in Section IV.A.1.b of Federal Highway Administration 2019 (FHWA)/ODOT Programmatic Agreement for Processing Categorical Exclusions (CE) and determined that an Individual CE must be submitted to FHWA for approval.	X	YES
		NO

For Individual CEs requiring FHWA Approval:

Concurrence that this project qualifies for a Categorical Exclusion:

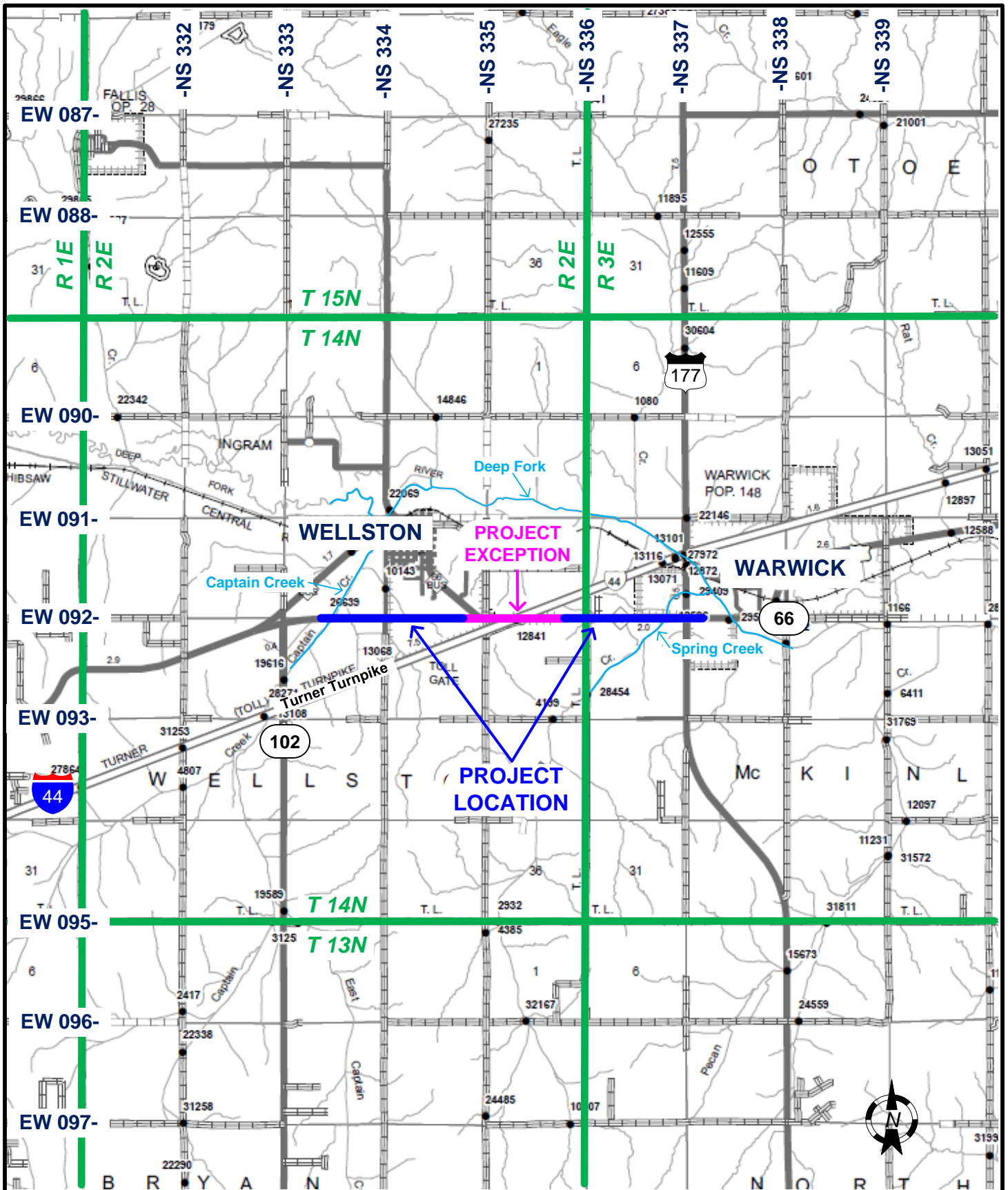
WILLIAM ALEXANDER ROWE <small>Digitally signed by WILLIAM ALEXANDER ROWE Date: 2023.12.14 13:05:54 -06'00'</small>	
Environmental Programs Manager, FHWA	Date

Attachments:

- | | |
|--------------------------------------|--|
| 1. Location Map | 5. Other Section – Initiation and Inspection Reports/NEPA Submittal Checklist, QA/QC Checklist |
| 2. Current Plans and Study Footprint | |
| 3. Early Coordination | |
| 4. Studies and Coordination | |

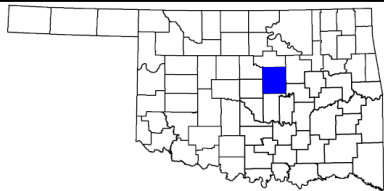

Distribution List (Check Applicable Ones)

<input checked="" type="checkbox"/>	Project Management Division (All State Projects)
<input checked="" type="checkbox"/>	Roadway Design Division (All State projects with the exception of projects from Traffic Division and Special Projects)
<input checked="" type="checkbox"/>	Bridge Division (All State Bridge Projects)
<input type="checkbox"/>	Traffic Division (For projects from Traffic Division)
<input type="checkbox"/>	Local Government Division (County, City, TAP or Special Projects)
<input checked="" type="checkbox"/>	District Engineer (All Projects)
<input checked="" type="checkbox"/>	Right-of-Way Division (All Projects)
<input type="checkbox"/>	Noise Specialist (For projects with noise studies)



LOCATION MAP

JP 34318(04); Project No. J3-4318(004)
 Widen, Resurface & Bridge SH-66:
 From SH-102, east to US-177
 Lincoln Co, OK

OKLAHOMA
 Transportation

Figure 1

**RIGHT-OF-WAY PLANS
AND
NEPA STUDY FOOTPRINT**

STATE OF OKLAHOMA DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED STATE HIGHWAY

FEDERAL AID PROJECT NO. XXXXXXXXXXXXXXXX WIDEN, RE-SURFACE, AND BRIDGE

SH-66

LINCOLN COUNTY

CONTROL SECTION NO. 66-41-06

STATE JOB NO. 34318(04)

BRIDGE "A" LOCATION NO. 4106 0372X

EXISTING NBI NO. 26639

BRIDGE "B" LOCATION NO. 4106 0702X

EXISTING NBI NO. 12596

FOR SURVEY CONTROL DATA, SEE SURVEY DATA SHEETS

RECEIVED OCT. 13, 2023

INDEX OF SHEETS

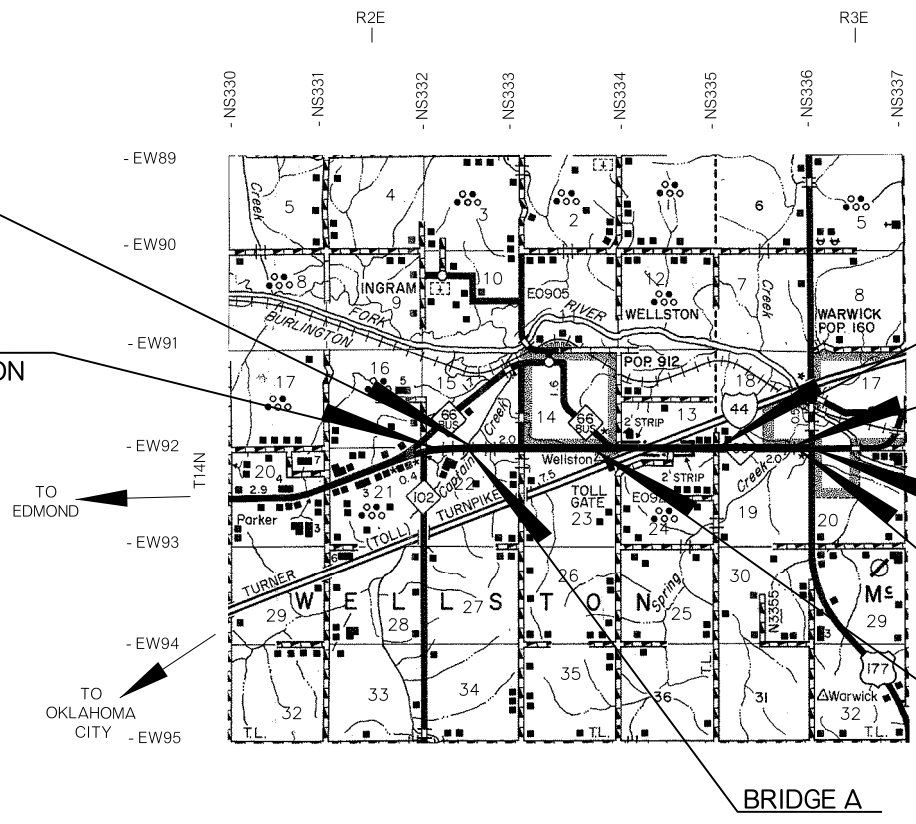
Table with 2 columns: SHEET NO. and DESCRIPTION. Lists sheet numbers and their corresponding descriptions like TITLE SHEET, TYPICAL SECTIONS, etc.

DESIGN DATA table listing AADT 2022, AADT 2042, DHV, K, D, T, T3, V, and 20YR FLEXALS with their respective values.

SCALES section with graphical scales for PLAN (1" = 100'), PROFILE HOR. (1" = 100'), VER. (1" = 10'), and LAYOUT MAP (1" = 5,280').

CONVENTIONAL SYMBOLS table listing symbols for PROPOSED ROAD, RAILROADS, RANGE & TOWNSHIP, SECTION LINES, QUARTER SECTION LINES, EXISTING FENCE, PROPOSED FENCE, GROUND LINE, EXISTING ROADS, BASE LINE, GRADE LINES, TELEPHONE & TELEGRAPH, POWER LINES, BUILDINGS, OILWELL, DRAINAGE STRUCTURES, RIGHT-OF-WAY LINES, CONTROLLED ACCESS, and RIGHT-OF-WAY FENCE.

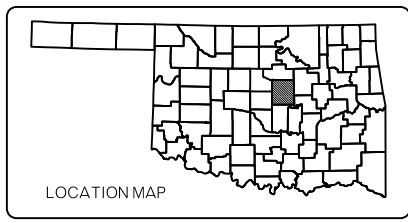
Stationing information: STA. 1622+10.00 END INCIDENTAL CONSTRUCTION BEGIN PROJECT, STA. 1606+51.45 BEGIN INCIDENTAL CONSTRUCTION, BEGIN STA. 1608+01.45 LENGTH = 335' BRIDGE "A" END STA. 1611+36.74, BEGIN STA. 1784+82.11 LENGTH = 43' BRIDGE "B" END STA. 1785+36.39



STA. 1731+25.90 END EXCEPTION, TO TULSA BRIDGE B, TO CHANDLER, STA. 1795+25.62 END INCIDENTAL CONSTRUCTION, STA. 1794+64.58 END PROJECT, STA. 1679+91.49 BEGIN EXCEPTION

THE FOLLOWING ODOT STANDARDS ARE REQUIRED FOR THIS PROJECT:

ROADWAY BRIDGE TRAFFIC



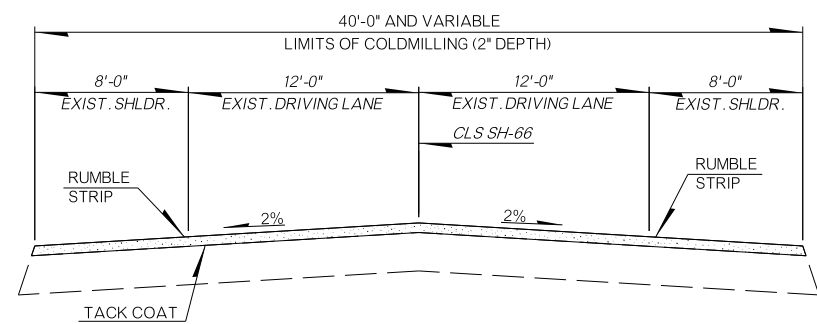
ROADWAY LENGTH 12,121.21 FT. 2.287 MI. BRIDGE LENGTH 43.00 FT. 0.008 MI. PROJECT LENGTH 2.295 MI.

EQUATIONS: CRL STA. 1669+00.00 BACK = CL SURVEY STA. = 1668+98.96 AHEAD EXCEPTIONS: STA. 1679+91.49 TO STA. 1731+25.90

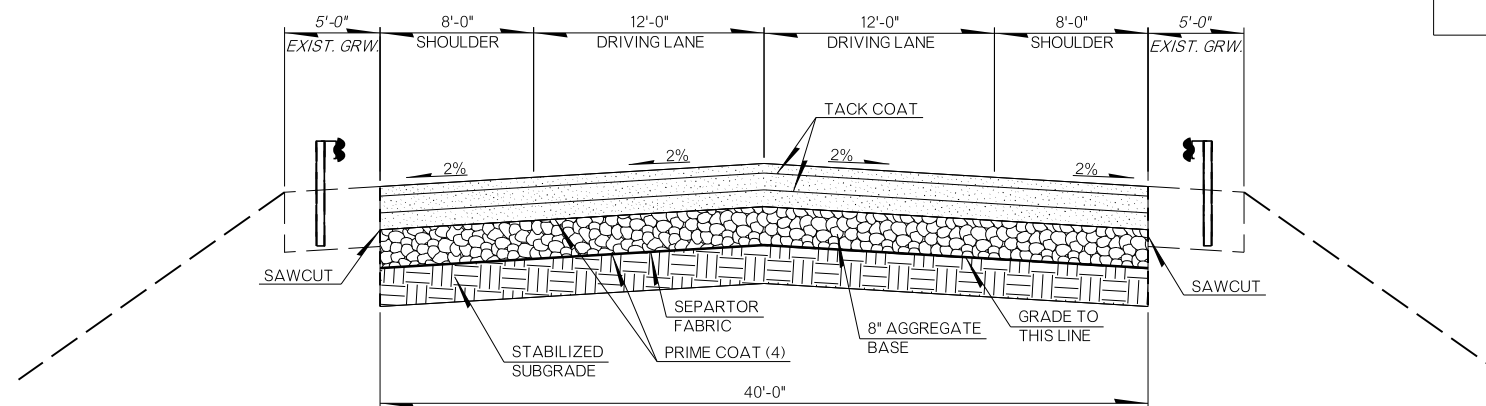
Prepared by: CEC CORPORATION, CA32 06/30/24, OKLAHOMA CITY, OKLAHOMA. Includes CEC logo and signature line for Eric R. Atkinson.

Approval section with fields for DATE APPROVED, BY, CHIEF ENGINEER, DIVISION ADMINISTRATOR, SWO, PROJECT NO., COUNTY, HIGHWAY, and SHEET NO.

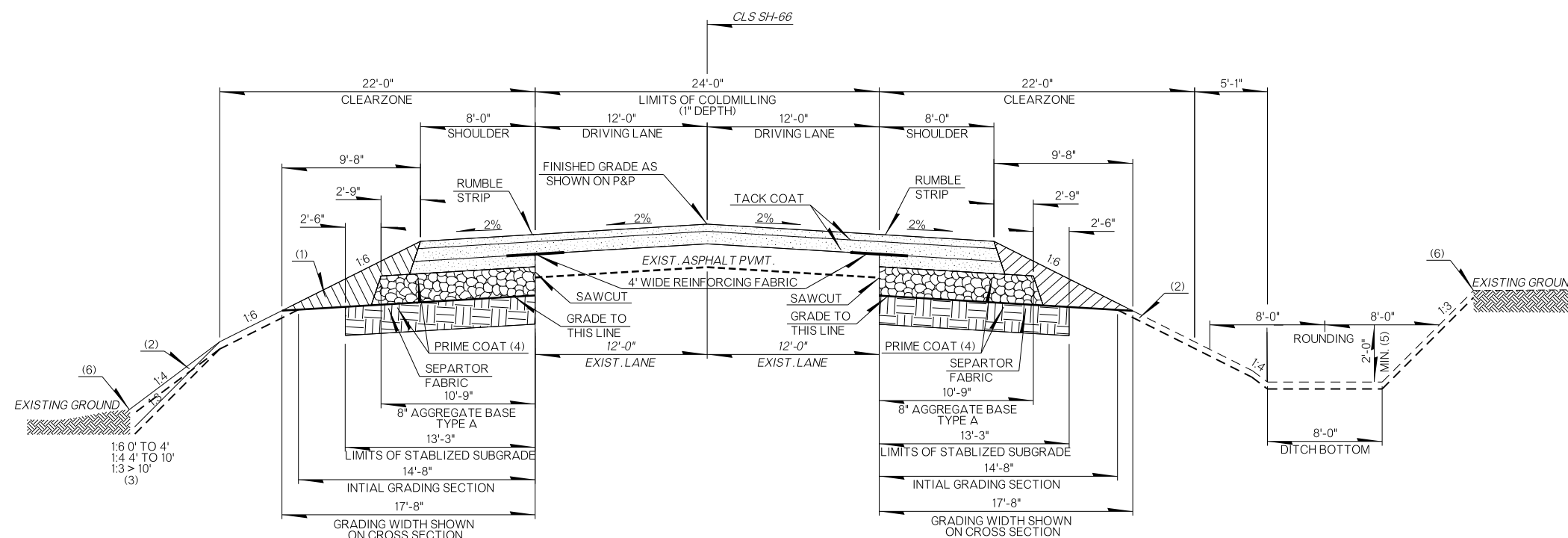
2019 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION GOVERN, APPROVED BY THE U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION, DECEMBER 18, 2019.



TYPICAL SECTION 1
STA. 1606+37.32 TO STA. 1607+51.45
STA. 1611+86.71 TO STA. 1622+10.00



TYPICAL SECTION 2 (CAPTAIN CREEK)
STA. 1607+51.45 TO STA. 1608+01.45
STA. 1611+36.71 TO STA. 1611+86.71



TYPICAL SECTION 3
STA. 1622+10.00 TO STA. 1639+00.00

(1) BACKFILL NOTE:
TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURED IN TBSC TYPE E.

(2) TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASS LINE BALANCE.

(3) DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

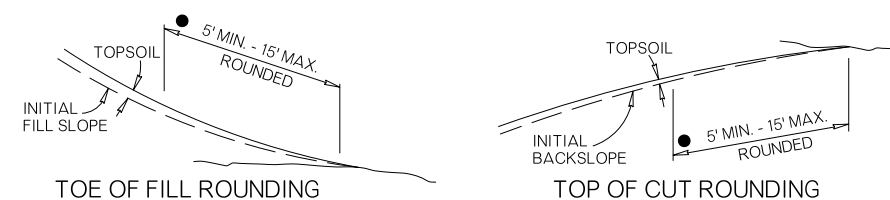
(4) PRIME COAT TO BE APPLIED TO THE TOP OF THE STABILIZED SUBGRADE AND THE TOP OF THE AGGREGATE BASE.

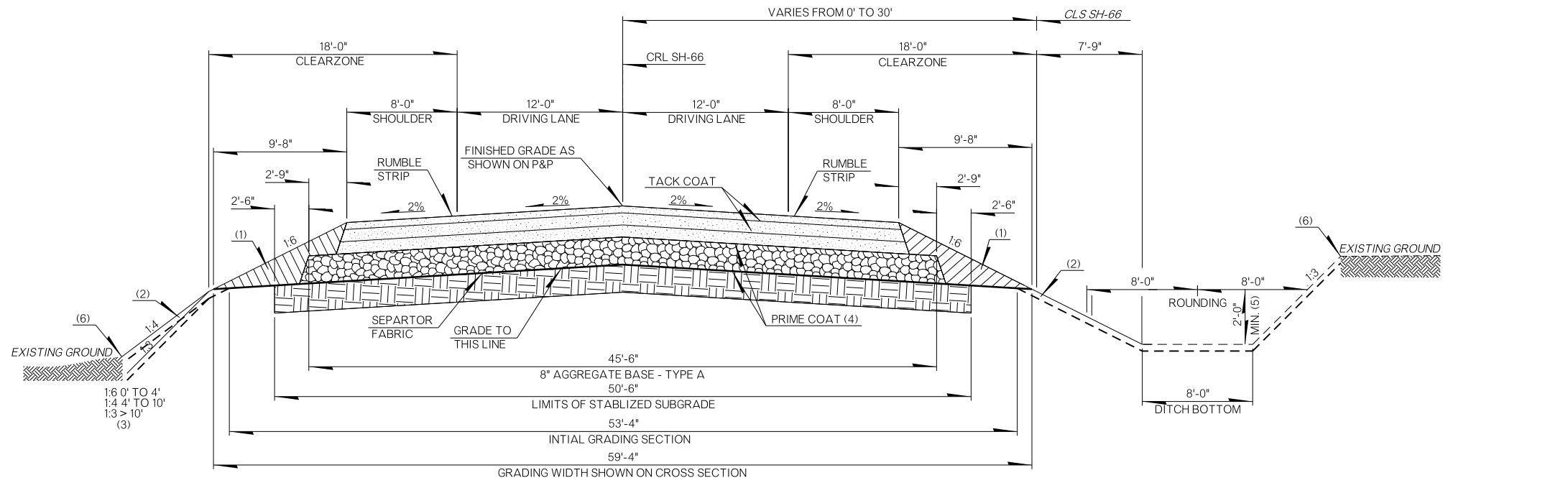
(5) UNLESS OTHERWISE NOTED ON CROSS SECTION.

(6) SEE ROUNDING DETAIL. THIS SHEET.

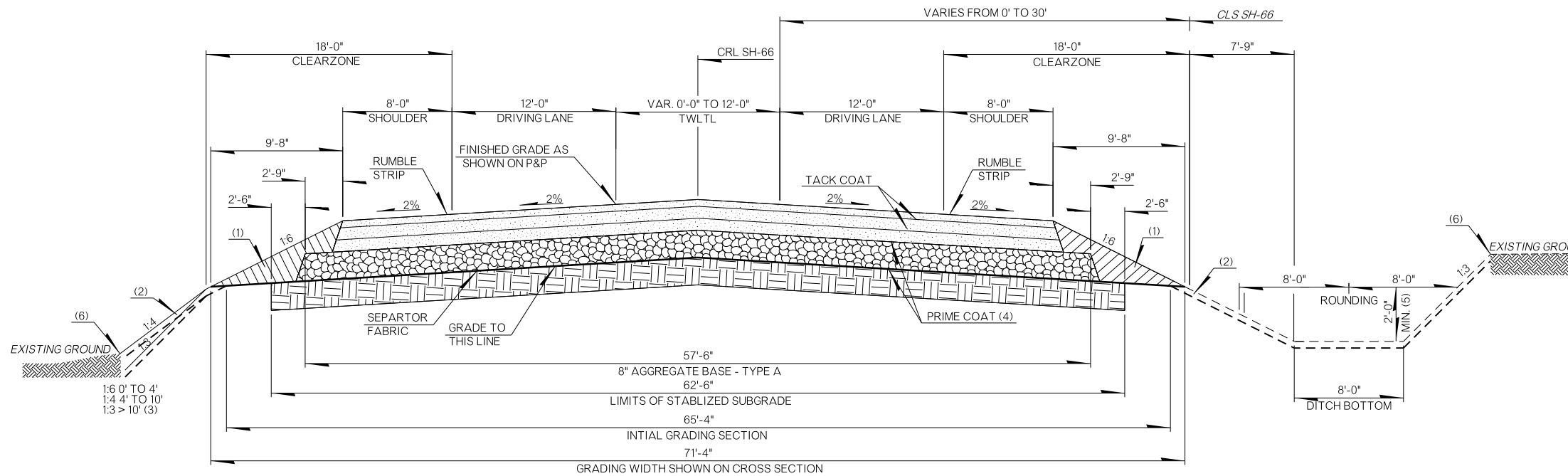
ROUNDING DETAIL

- INTERSECTION OF CUT AND/OR FILL SLOPES WITH GROUND LINE TO BE ROUNDED AS PART OF FINISHING OPERATIONS. ROUNDED SHALL BE 5' MINIMUM FOR SMALLER CUTS AND FILLS TO 15' MAXIMUM FOR LARGER CUTS AND FILLS OR AS DESIGNATED BY THE ENGINEER. COST OF ROUNDED TO BE INCLUDED IN PRICE BID FOR OTHER ITEMS OF WORK.





TYPICAL SECTION 4
STA. 1639+00.00 TO STA. 1660+60.70



TYPICAL SECTION 5
STA. 1660+60.70 TO STA. 1664+48.10

- (1) BACKFILL NOTE:
TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURED IN TBSC TYPE E.
- (2) TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

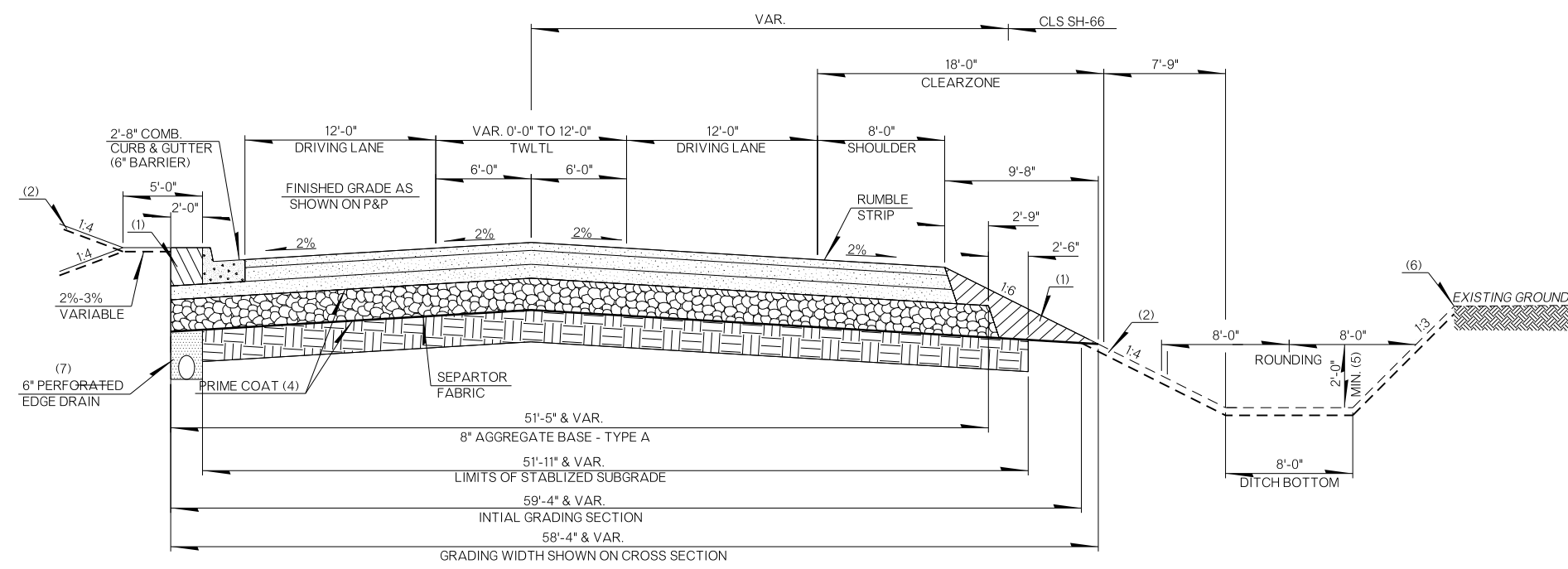
THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASS LINE BALANCE.
- (3) DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.
- (4) PRIME COAT TO BE APPLIED TO THE TOP OF THE STABILIZED SUBGRADE AND THE TOP OF THE AGGREGATE BASE.
- (5) UNLESS OTHERWISE NOTED ON CROSS SECTION.
- (6) SEE ROUNDING DETAIL. SHOOT 0002.

ROUNDING DETAIL

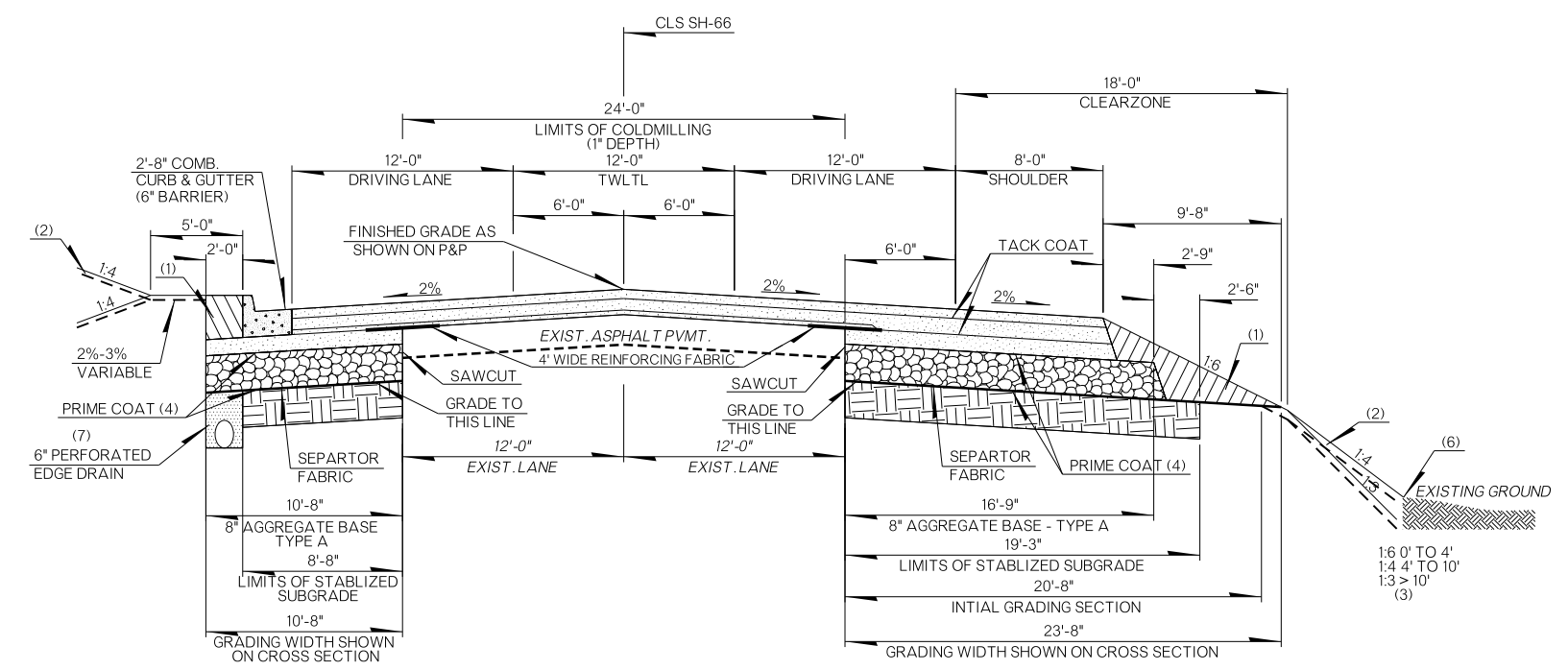
- INTERSECTION OF CUT AND/OR FILL SLOPES WITH GROUND LINE TO BE ROUNDED AS PART OF FINISHING OPERATIONS. ROUNDED SHALL BE 5' MINIMUM FOR SMALLER CUTS AND FILLS TO 15' MAXIMUM FOR LARGER CUTS AND FILLS OR AS DESIGNATED BY THE ENGINEER. COST OF ROUNDED TO BE INCLUDED IN PRICE BID FOR OTHER ITEMS OF WORK.



9/26/2023 N:\OKC\Transportation\Highway\Production\DOT1210532-CI 2341A-SH 66 Lincoln Co\Project Drawings\General\3431804-TYPICAL SECTIONS.dgn



TYPICAL SECTION 6
STA. 1664+48.10 TO STA. 1668+90.82



TYPICAL SECTION 7
STA. 1668+90.82 TO STA. 1676+00.19

(1) BACKFILL NOTE:
TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURED IN TBSC TYPE E.

AREA BEHIND CURB AND GUTTER IS MEASURED IN UNCLASSIFIED BORROW.

(2) TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASS LINE BALANCE.

(3) DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

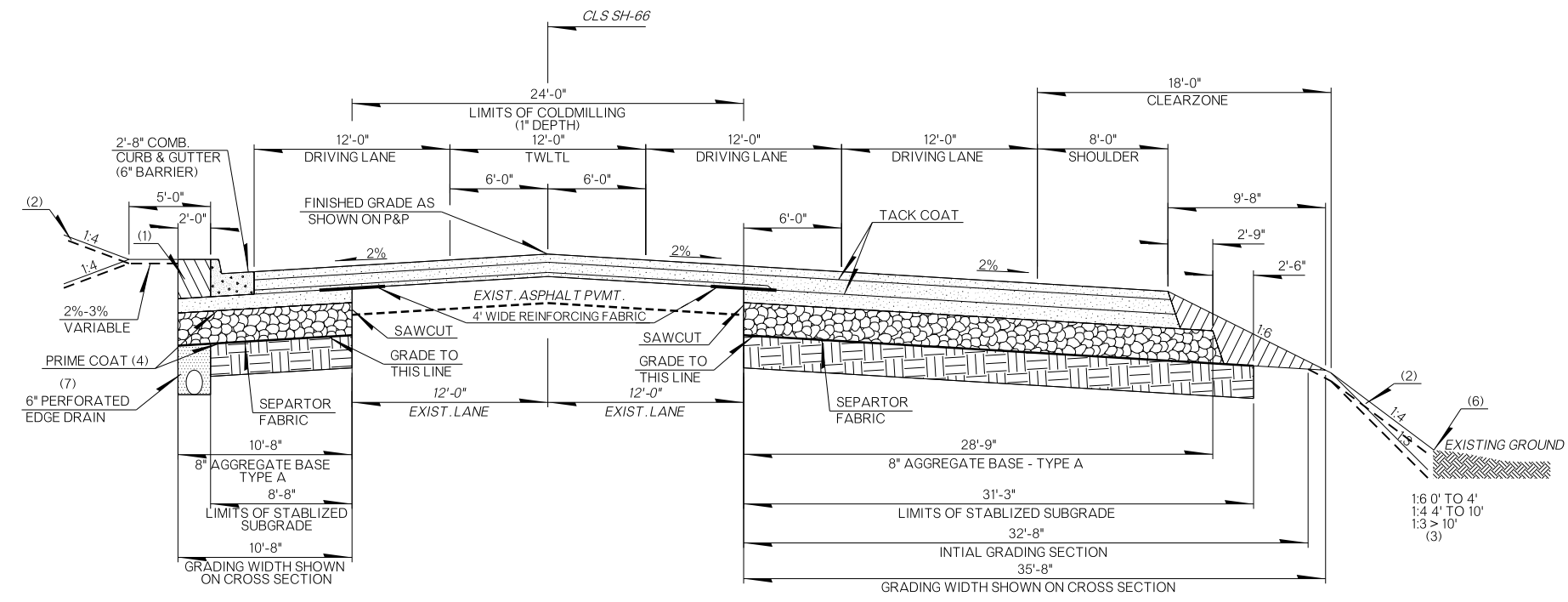
(4) PRIME COAT TO BE APPLIED TO THE TOP OF THE STABILIZED SUBGRADE AND THE TOP OF THE AGGREGATE BASE.

(5) UNLESS OTHERWISE NOTED ON CROSS SECTION.

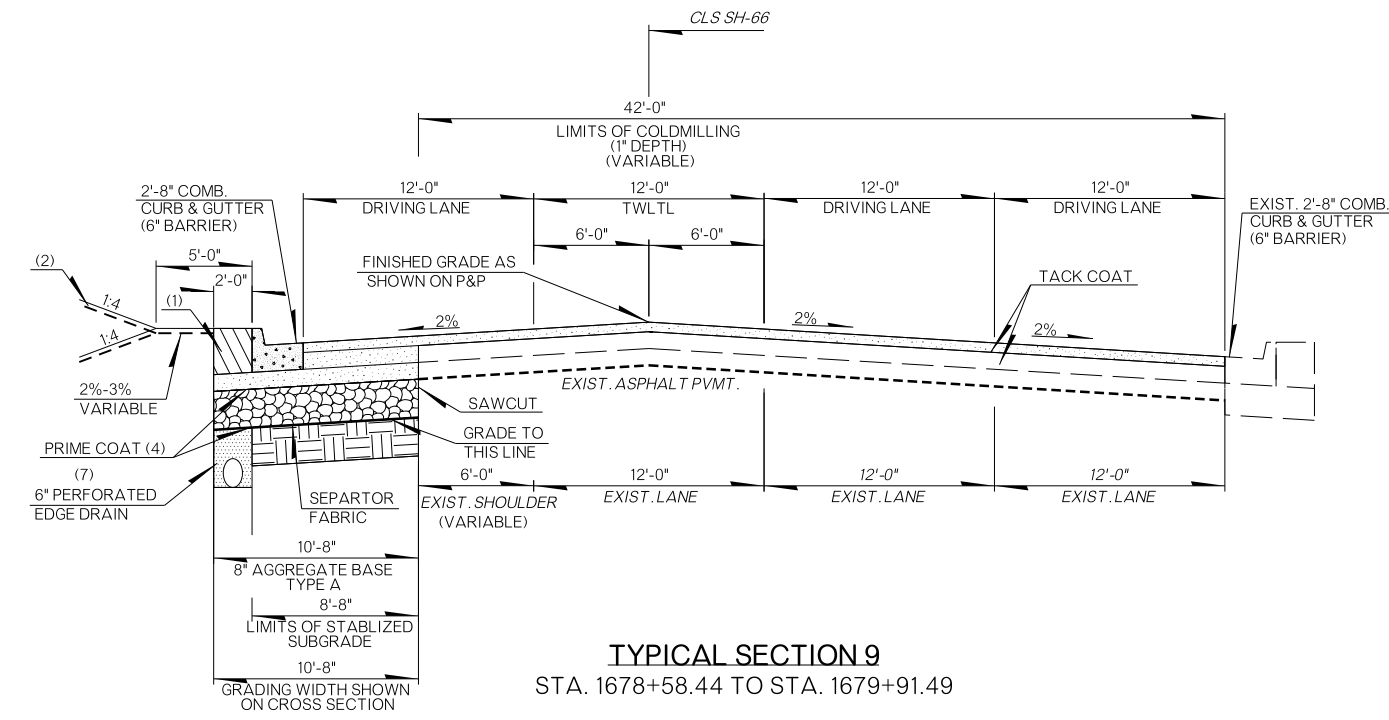
(6) SEE ROUNDING DETAIL SHEET 0002.

(7) EDGE DRAIN TO DRAIN TO DITCH OR PROPOSED STORM SEWER.

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9/26/2023



TYPICAL SECTION 8
STA. 1676+00.19 TO STA. 1678+58.44



TYPICAL SECTION 9
STA. 1678+58.44 TO STA. 1679+91.49

(1) BACKFILL NOTE:
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THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASS LINE BALANCE.

(3) DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

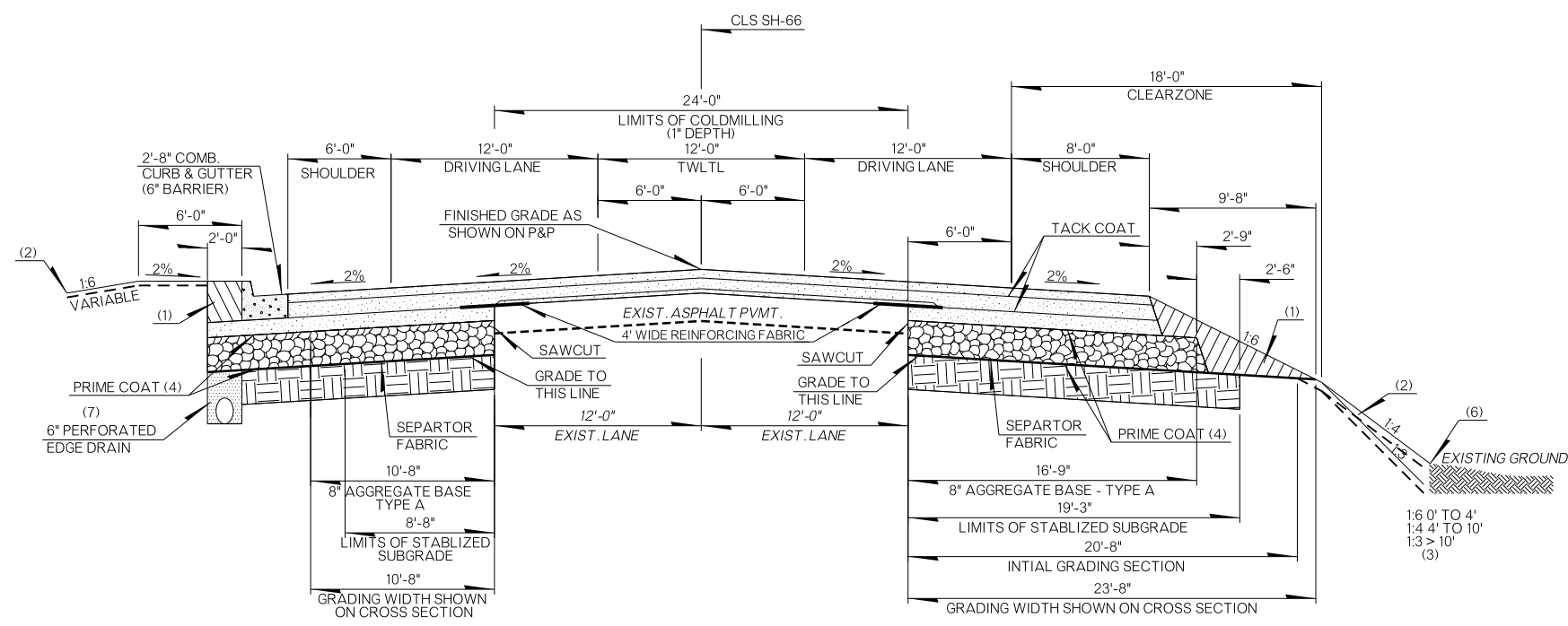
(4) PRIME COAT TO BE APPLIED TO THE TOP OF THE STABILIZED SUBGRADE AND THE TOP OF THE AGGREGATE BASE.

(5) UNLESS OTHERWISE NOTED ON CROSS SECTION.

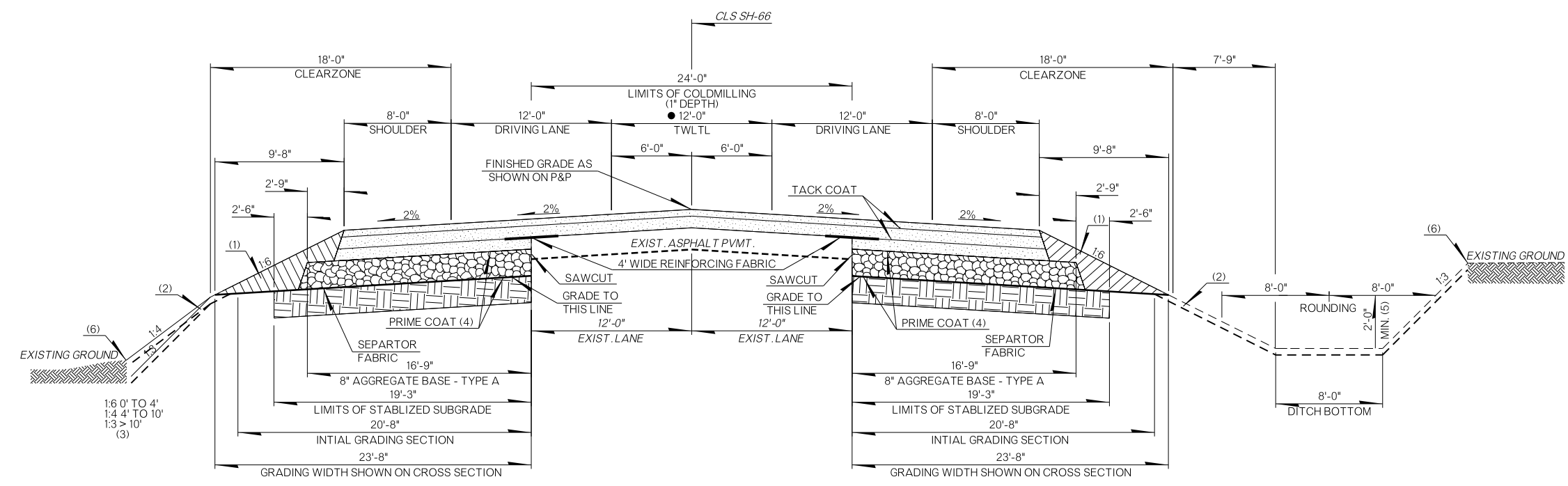
(6) SEE ROUNDING DETAIL. SHEET 0002.

9/26/2023 N:\OKC\Transportation\Highway\Production\DOT1210532-CI 2341A-SH 66 Lincoln Co\Project Drawings\General\3431804-TYPICAL SECTIONS.dgn

PAVEMENT DESIGN IS PRELIMINARY.
FOR INFORMATION PURPOSES ONLY.



TYPICAL SECTION 10
 STA. 1731+25.90 TO STA. 1733+95.90



TYPICAL SECTION 11
 STA. 1733+95.90 TO STA. 1758+14.71
 ● VARIES FROM 12' TO 0" FROM STA. 1754+24.71 TO STA. 1758+14.71

(1) BACKFILL NOTE:
 TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURED IN TBSC TYPE E.
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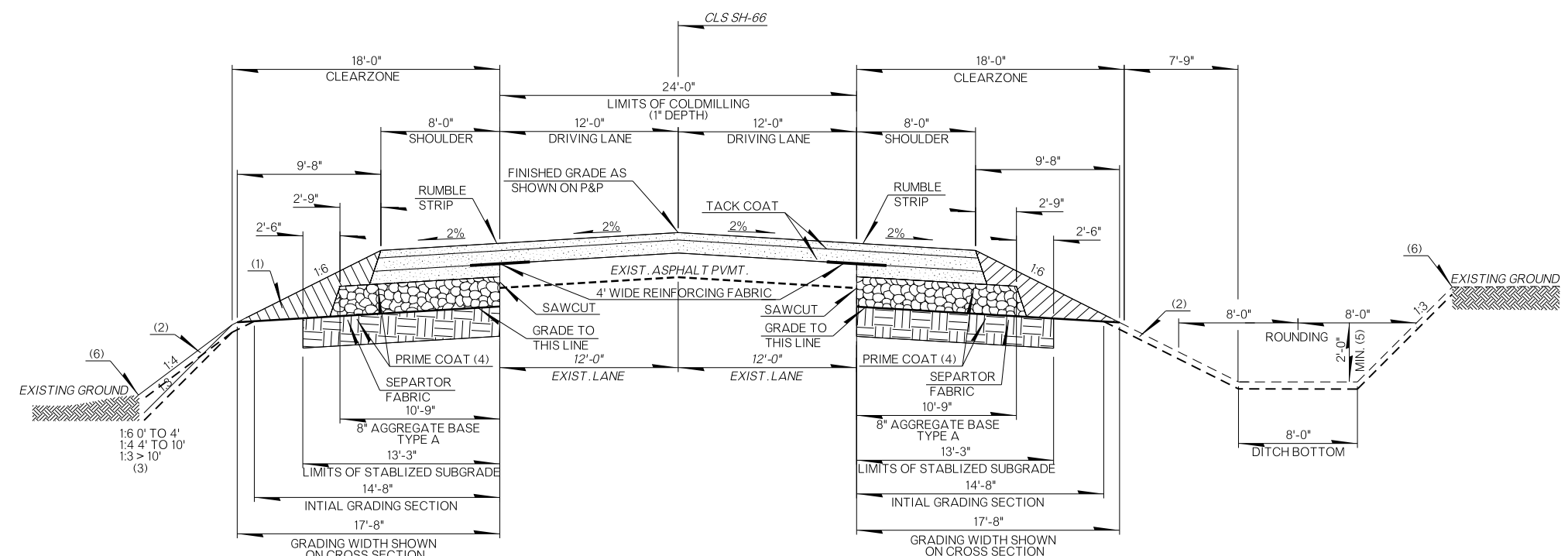
(3) DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

(4) PRIME COAT TO BE APPLIED TO THE TOP OF THE STABILIZED SUBGRADE AND THE TOP OF THE AGGREGATE BASE.

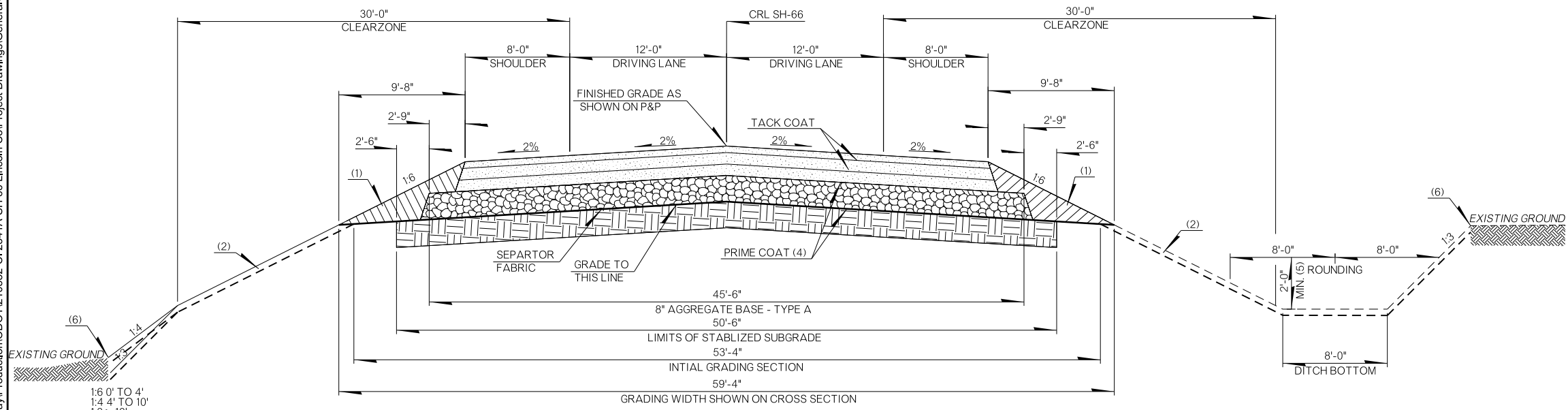
(5) UNLESS OTHERWISE NOTED ON CROSS SECTION.

(6) SEE ROUNDING DETAIL. SHEET 0002.

9/26/2023 N:\OKC\Transportation\Highway\Production\DOT1210532-CI 2341A-SH 66 Lincoln Co\Project Drawings\General\3431804-TYPICAL SECTIONS.dgn



TYPICAL SECTION 12
 STA. 1758+14.71 TO STA. 1783+25.00
 STA. 1787+15.00 TO STA. 1792+95.00

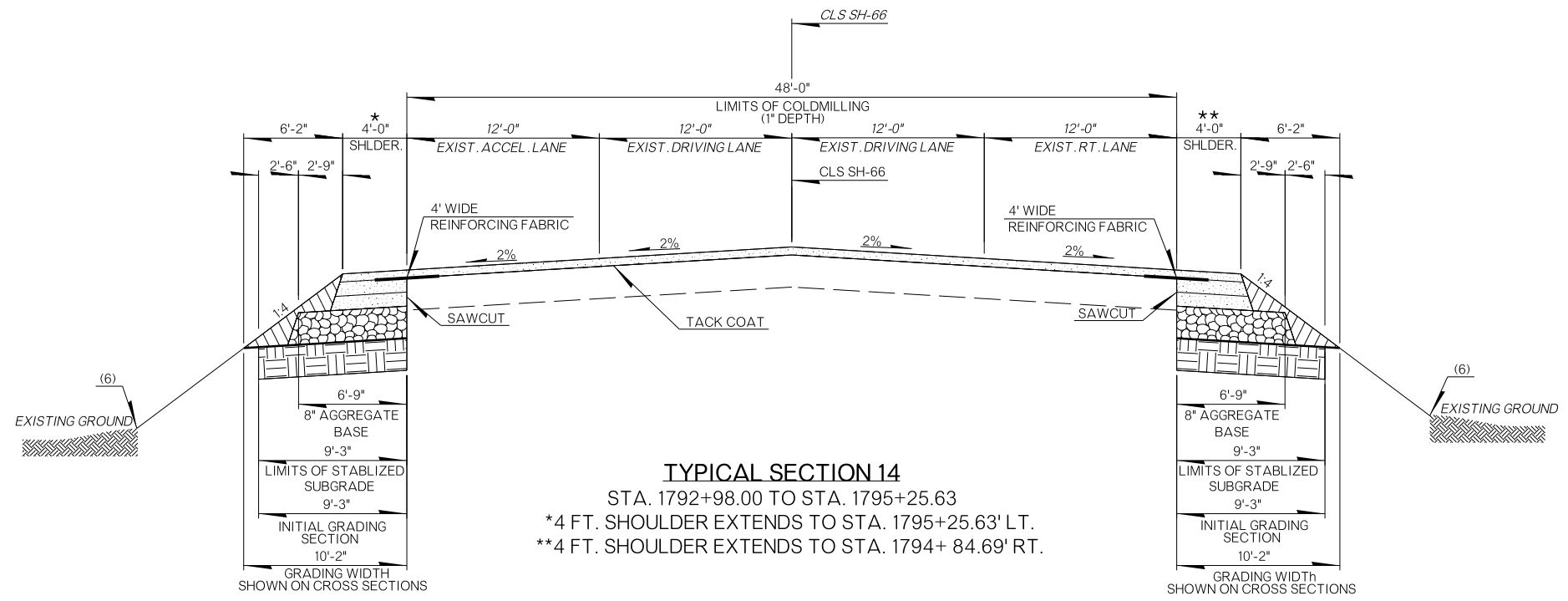


TYPICAL SECTION 13
 STA. 1783+25.00 TO STA. 1787+15.00

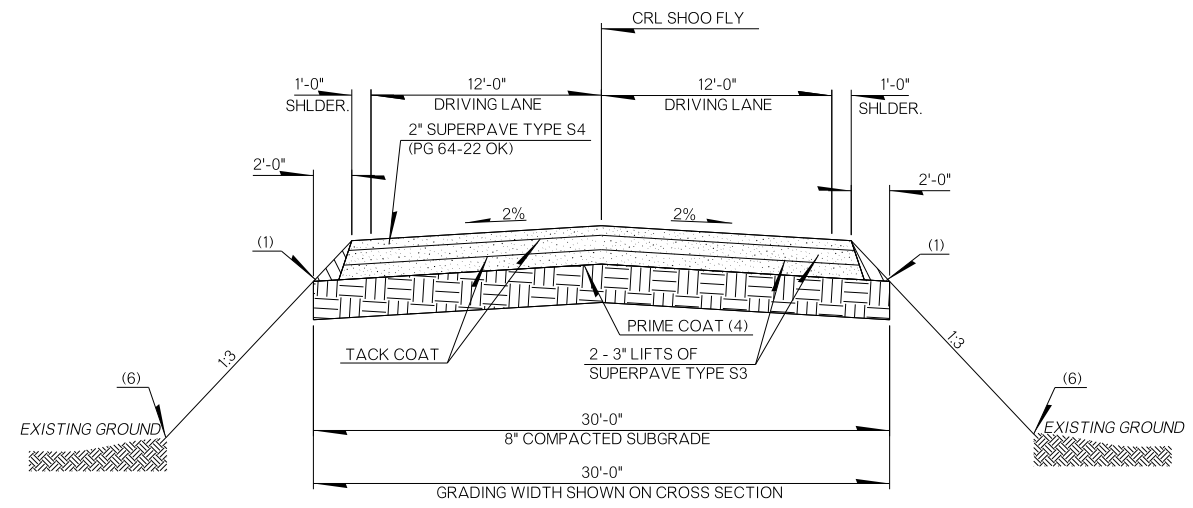
- (1) BACKFILL NOTE:
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- (5) UNLESS OTHERWISE NOTED ON CROSS SECTION.
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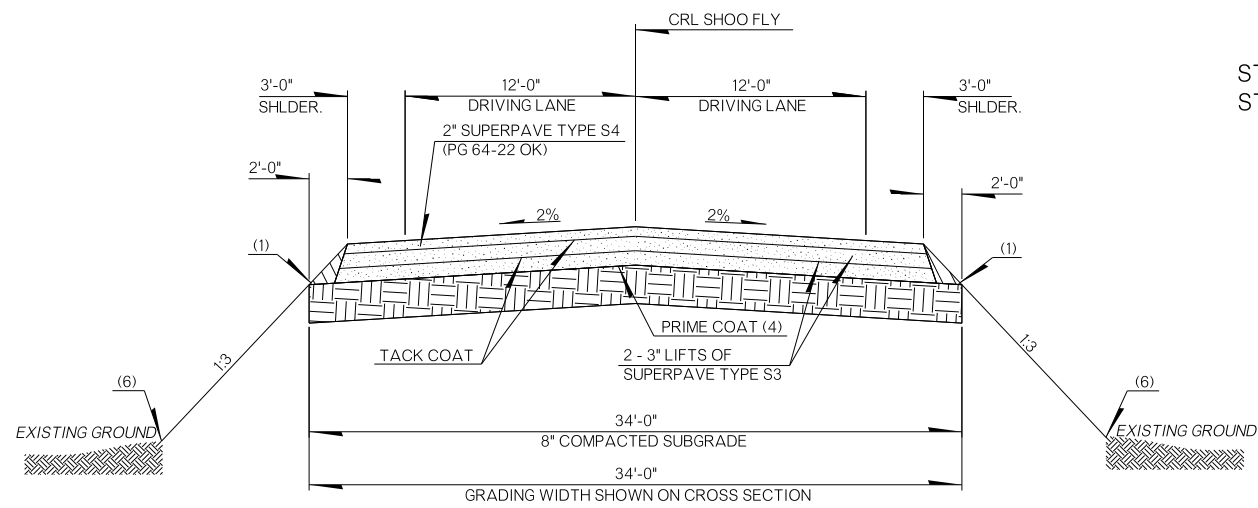
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TYPICAL SECTION 14
 STA. 1792+98.00 TO STA. 1795+25.63
 *4 FT. SHOULDER EXTENDS TO STA. 1795+25.63' LT.
 **4 FT. SHOULDER EXTENDS TO STA. 1794+ 84.69' RT.



SHOO-FLY TYPICAL SECTION 1
 STA. 1634+00.00 TO STA. 1649+92.89 (SHOO-FLY A)
 STA. 1775+50.00 TO STA. 1791+73.32 (SHOO-FLY C)

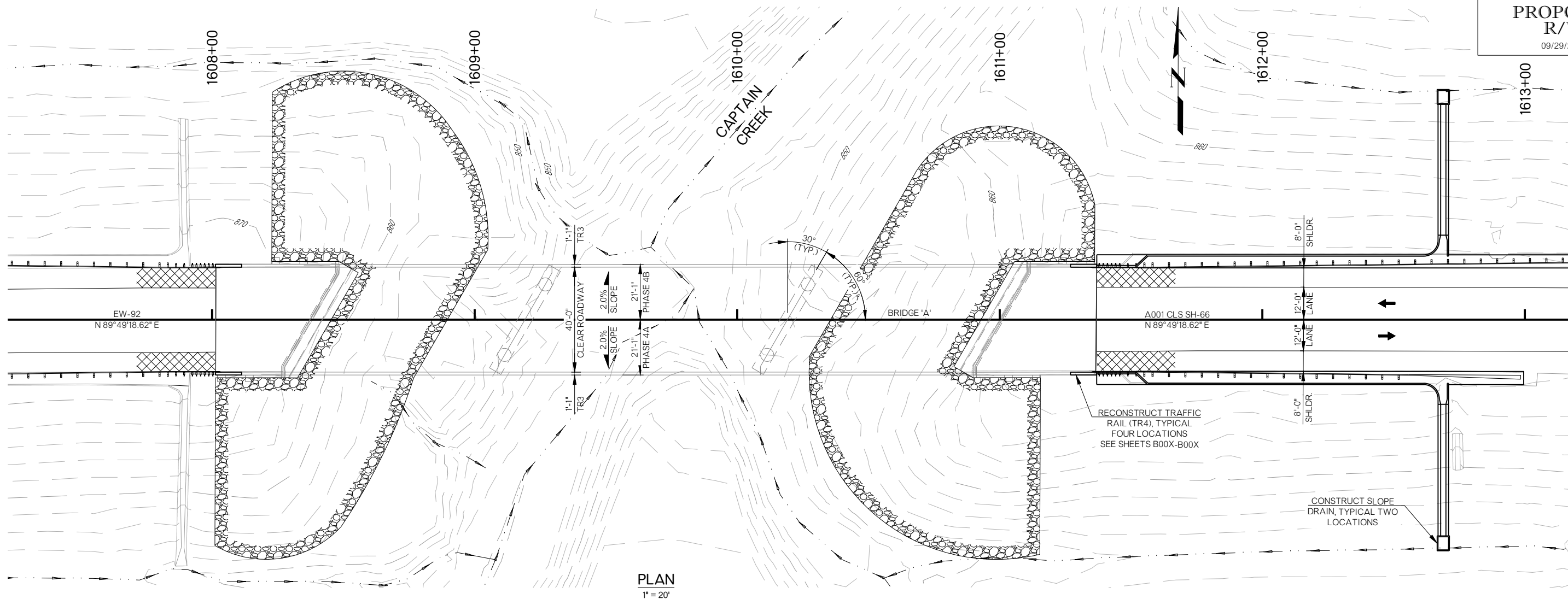


SHOO-FLY TYPICAL SECTION 2
 STA. 1658+90.00 TO STA. 1671+83.16 (SHOO-FLY B)

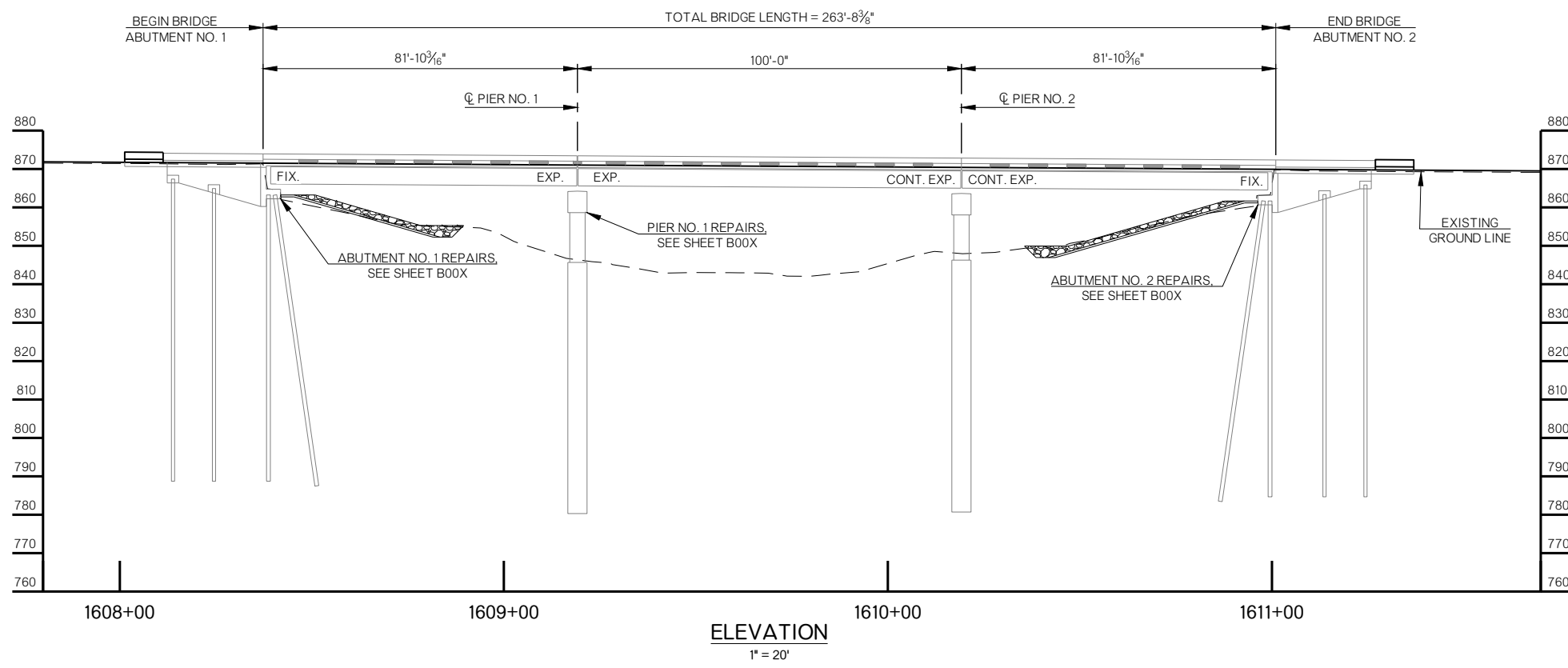
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- (5) UNLESS OTHERWISE NOTED ON CROSS SECTION.
- (6) SEE ROUNDING DETAIL, SHEET 0002.

9/26/2023 N:\OKC\Transportation\Highway\Production\DOT1210532-CI 2341A-SH 66 Lincoln Co\Project Drawings\General\3431804-TYPICAL SECTIONS.dgn



PLAN
1" = 20'



ELEVATION
1" = 20'

DIMENSIONS ARE TAKEN FROM THE "GENERAL PLAN & ELEVATION" SHEET FROM RECORD DRAWINGS. ACTUAL FIELD DIMENSIONS MAY VARY FROM WHAT IS SHOWN. CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS AS REQUIRED FOR CONSTRUCTION.

NOTES:
FOR DESIGN DATA, INDEX OF BRIDGE SHEETS, LIST OF STANDARDS AND SUMMARY OF BRIDGE QUANTITIES, SEE SHEET B002.

THIS DRAWING IS PRELIMINARY IN NATURE. IT IS NOT A FINAL SIGNED AND SEALED DRAWING

SH-66 OVER CAPTAIN CREEK BRIDGE 'A'	LINCOLN COUNTY	DESIGN	J.W.H.
GENERAL PLAN AND ELEVATION (1 OF 2)		DETAIL	J.F.R.
REHABILITATE EXISTING 80'-100'-80' TYPE IV P.C. BEAM SPANS, 40'-0" CLEAR ROADWAY, CONCRETE TRAFFIC RAIL (TR3), 30° LEFT FORWARD SKEW		CHECK	



N:\OKC\Bridges\Bridges\Production\1 ODOT\210532-CI\2341A-SH 66 Lincoln Co\7 Drawings\DWG Files\Bridges\Bridges for Captain Creek Bridge Rehab\B001-B002-34318(04)-GENERAL PLAN AND ELEVATION.dwg, 9/28/2023 1:23:36 PM, James Rinehart

**PROPOSED
R/W**

09/29/2023

SUMMARY OF BRIDGE QUANTITIES							
ITEM	UNIT	ABUTMENTS	PIERS	SUPERSTRUCTURE			TOTAL
				PHASE 1	PHASE 2	SUBTOTAL	
UNCLASSIFIED EXCAVATION	C.Y.	—	—	—	—	—	
CLSM BACKFILL	C.Y.						
ELASTOMERIC COATING	S.F.						
REINFORCING STEEL	L.B.						
CLASS C CONCRETE	C.Y.						
WATER REPELLENT (VISUALLY INSPECTED)	S.Y.						
PREPARATION OF CRACKS, ABOVE WATER	L.F.						
EPOXY RESIN, ABOVE WATER	GAL.						
TYPE I-A PLAIN RIPRAP	TON						
TYPE I-A FILTER BLANKET	TON						
(PL)REMOVAL OF DRIFT AND SILT	L.SUM						
REMOVAL OF BRIDGE ITEMS	EA.						

DESIGN DATA

LOAD AND RESISTANCE FACTOR DESIGN
(NEW CONSTRUCTION ONLY)

CLASS AA CONCRETE
REINFORCING STEEL (GRADE 60)

f'c = 4 K.S.I.
fy = 60 K.S.I.

LOADING:
HL-93 OR OKLAHOMA OVERLOAD TRUCK

DESIGN:
AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 9TH EDITION

INDEX OF BRIDGE SHEETS


B001-B002 GENERAL PLAN AND ELEVATION

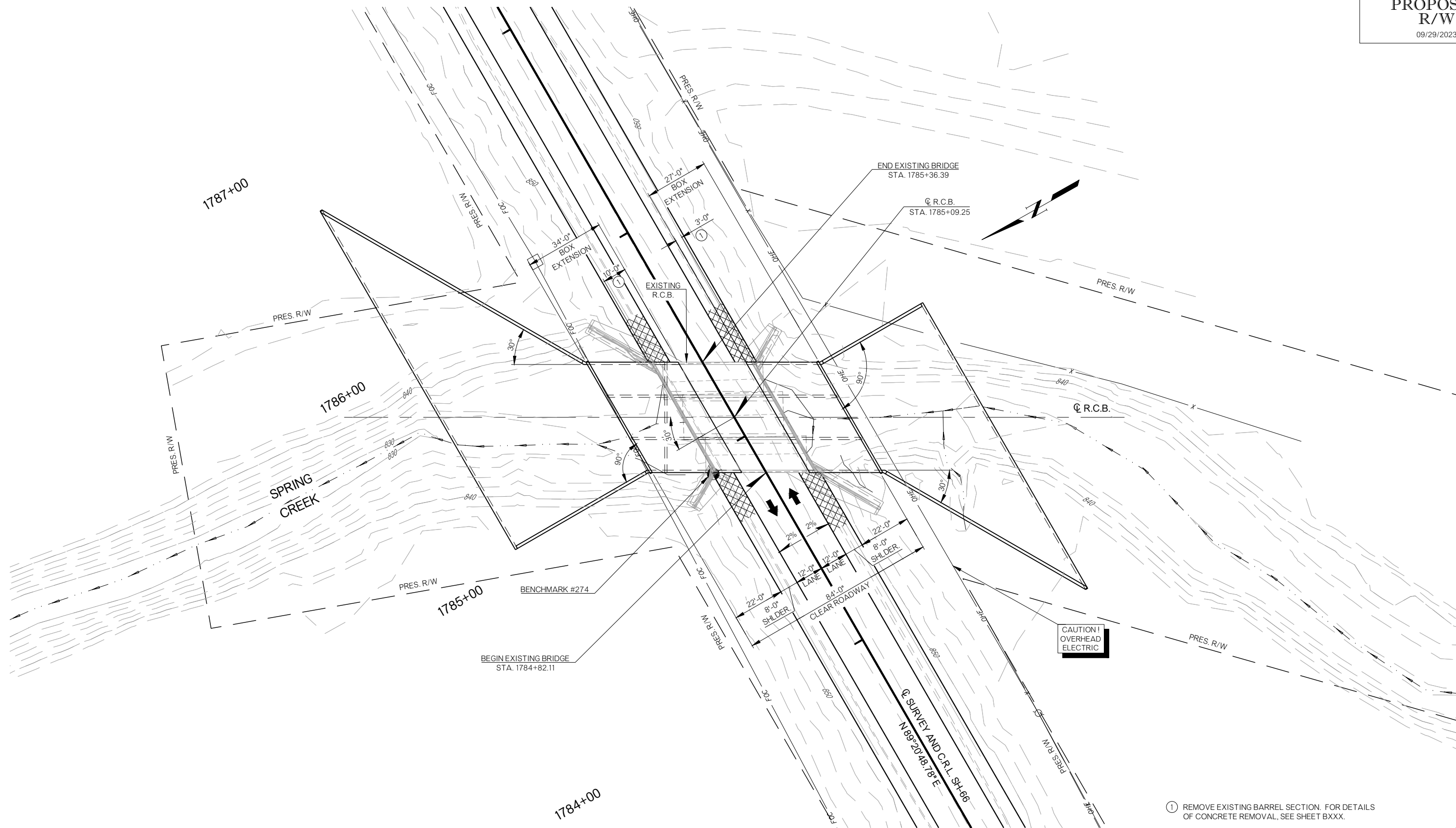
STANDARDS

UTILITIES

ONG	800-458-4251
OG&E	800-272-9741
AT&T DISTRIBUTION	918-456-8867
LINCOLN COUNTY RURAL WATER DIST. #3	405-258-2200
TOWN OF WELLSTON	405-356-2476
ONEOK GAS TRANSPORTATION	918-588-7000
CIRCLE 9 RESOURCES	405-702-7420
CHICKASAW TELEPHONE	918-663-3565
CAPONE/SCISSORTAIL ENERGY	918-588-5000
CENTURYLINK	866-642-0444
SPRINT NEXTEL	888-211-4727
SUPERIOR PIPELINE	918-382-7200

THIS DRAWING
IS PRELIMINARY IN
NATURE. IT IS NOT
A FINAL SIGNED
AND SEALED
DRAWING

SH-66 OVER CAPTAIN CREEK BRIDGE 'A'		LINCOLN COUNTY		DESIGN	J.W.H.	
GENERAL PLAN AND ELEVATION (2 OF 2) REHABILITATE EXISTING 80'-100'-80' TYPE IV P.C. BEAM SPANS, 40'-0" CLEAR ROADWAY, CONCRETE TRAFFIC RAIL (TR3), 30° LEFT FORWARD SKEW				DETAIL	J.F.R.	
				CHECK		
STATE OF OKLAHOMA		DEPARTMENT OF TRANSPORTATION				
JOB PIECE NO. 34318(04)		SHEET NO. B002				



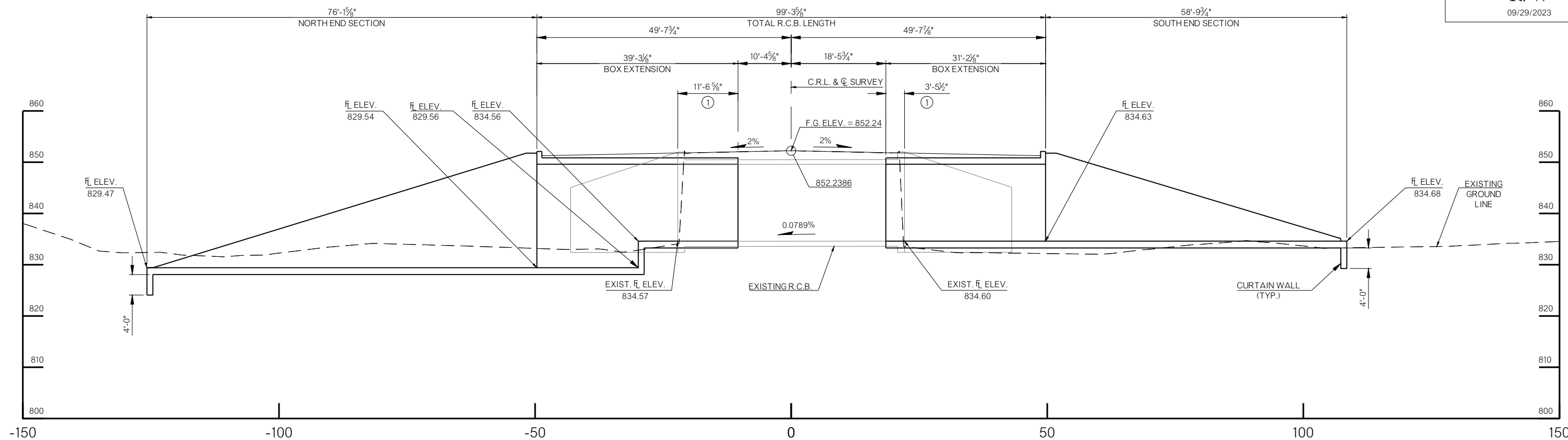
① REMOVE EXISTING BARREL SECTION. FOR DETAILS OF CONCRETE REMOVAL, SEE SHEET BXXX.

PLAN
1" = 20'

THIS DRAWING IS PRELIMINARY IN NATURE. IT IS NOT A FINAL SIGNED AND SEALED DRAWING

SH-66 OVER SPRING CREEK BRIDGE 'B'	LINCOLN COUNTY	DESIGN	J.W.H.
GENERAL PLAN AND ELEVATION (1 OF 2) EXTEND EXISTING (13' x 17' x 13') x 15' x 44' LONG R.C.B. TO 99' LONG R.C.B. CONST. (13' x 17' x 13') x 15' R.C.B. WITH 5' DROP TO (13' x 17' x 13') x 20'. 84' CLR. RDY. 30° L.F. SKEW, @ STA. 1785+09.25		DETAIL	J.F.R.
		CHECK	
STATE OF OKLAHOMA		DEPARTMENT OF TRANSPORTATION	

N:\OKC\Bridges\Projects\Production\1 ODOT\210532-CL\2341A-SH 66 Lincoln Co\7 Drawings\DWG Files\B008-B009-34318(04)-GENERAL PLAN AND ELEVATION.dwg, 9/28/2023 1:32:39 PM, James Rinehart



ELEVATION

1" = 10'
(LOOKING UPSTATION ALONG C. R.C.B.)

BENCHMARK NO. 274
"X" IN NW WINGWALL, 19.85' LT.
C. SURVEY STATION 1784+92.88, ELEV. = 852.175'

BENCHMARK NO. 275
3/8" IRON PIN, 54.44' LT.
C. SURVEY STATION 1790+84.49, ELEV. = 847.153'

① REMOVE EXISTING BARREL SECTION FOR DETAIL OF CONCRETE REMOVAL, SEE SHEET BXXX.

HYDRAULIC SUMMARY			
TOTAL DRAINAGE AREA	=	13.44 SQ. MILES	
CONTROLLED DRAINAGE AREA	=	1.35 SQ. MILES	
EFFECTIVE DRAINAGE AREA	=	12.09 SQ. MILES	
FREQUENCY (YEARS)	DISCHARGE (CFS)	WATER SURFACE ELEVATION (FT)	VELOCITY (FPS)
2	1,160	841.62	5.17
5	2,160	844.83	6.86
10	3,010	845.93	7.99
25	4,270	847.29	9.40
50	5,560	848.89	10.61
100	6,550	850.09	11.26
500	10,400	851.91	11.86
OT = 93	6,550	850.08	11.26

DESIGN DATA

LOAD AND RESISTANCE FACTOR DESIGN
(NEW CONSTRUCTION ONLY)

CLASS AA CONCRETE $f'_c = 4$ K.S.I.
REINFORCING STEEL (GRADE 60) $f_y = 60$ K.S.I.

LOADING:
HL-93 OR OKLAHOMA OVERLOAD TRUCK

DESIGN:
AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 9TH EDITION

INDEX OF BRIDGE SHEETS

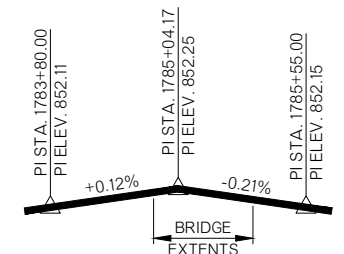
B003-B004 GENERAL PLAN AND ELEVATION

STANDARDS

SBI-5-2

UTILITIES

- ONG 800-458-4251
- OG&E 800-272-9741
- AT&T DISTRIBUTION 918-456-8867
- LINCOLN COUNTY RURAL WATER DIST. #3 405-258-2200
- TOWN OF WELLSTON 405-356-2476
- ONEOK GAS TRANSPORTATION 918-588-7000
- CIRCLE 9 RESOURCES 405-702-7420
- CHICKASAW TELEPHONE 918-663-3565
- CAPONE/SCISSORTAIL ENERGY 918-588-5000
- CENTURYLINK 866-642-0444
- SPRINT NEXTEL 888-211-4727
- SUPERIOR PIPELINE 918-382-7200



VERTICAL PROFILE DATA
C.R.L. SH-66

SUMMARY OF BRIDGE QUANTITIES		
ITEM	UNIT	TOTAL
UNCLASSIFIED EXCAVATION	C.Y.	
STRUCTURAL EXCAVATION UNCLASSIFIED	C.Y.	
CLSM BACKFILL	C.Y.	
CLASS AA CONCRETE	C.Y.	
REINFORCING STEEL	LB.	
REMOVAL OF BRIDGE ITEMS	L.SUM	

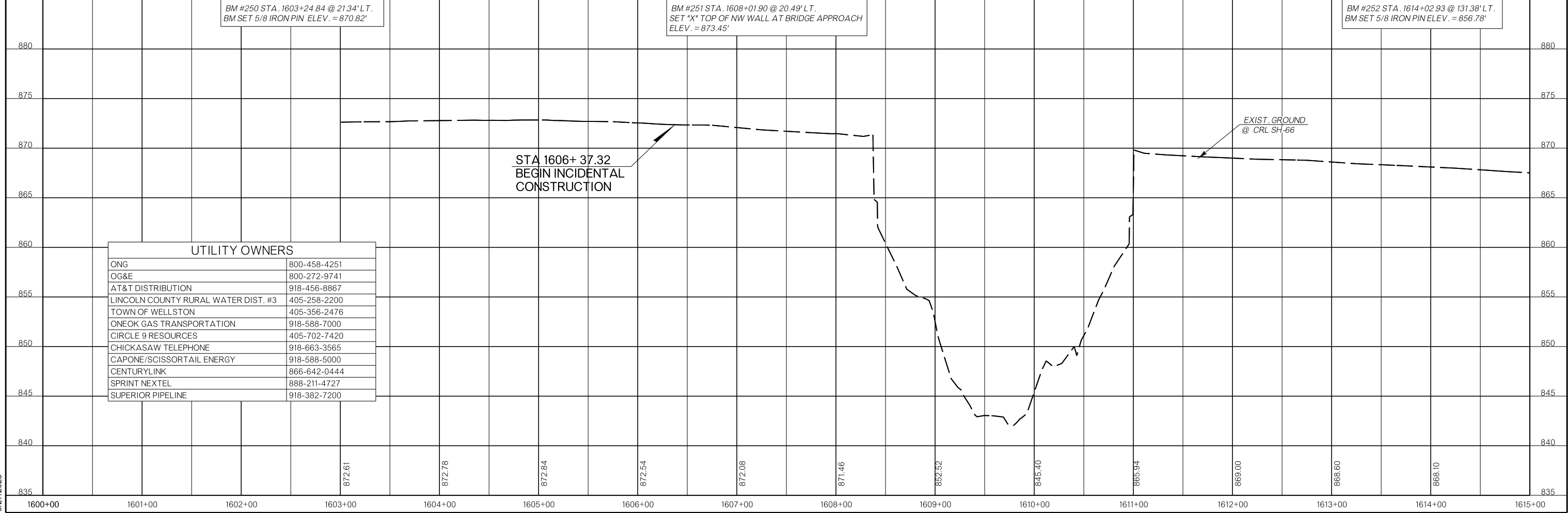
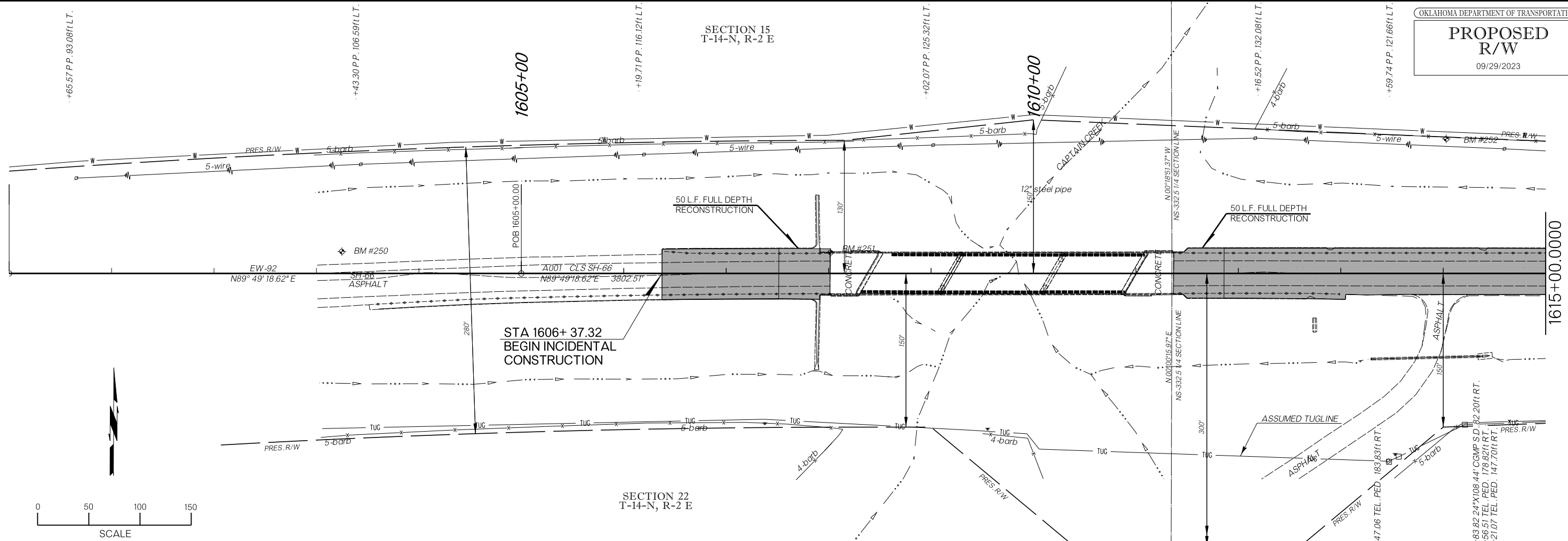
THIS DRAWING IS PRELIMINARY IN NATURE. IT IS NOT A FINAL SIGNED AND SEALED DRAWING

SH-66 OVER SPRING CREEK BRIDGE 'B'		LINCOLN COUNTY	DESIGN J.W.H.
GENERAL PLAN AND ELEVATION (2 OF 2)			DETAIL J.F.R.
EXTEND EXISTING (13' x 17' x 13') x 15' x 44' LONG R.C.B. TO 99' LONG R.C.B. CONST. (13' x 17' x 13') x 15' R.C.B. WITH 5' DROP TO (13' x 17' x 13') x 20'. 84' CLR. RDY. 30" L.F. SKEW, C. STA. 1785+09.25			CHECK
STATE OF OKLAHOMA		DEPARTMENT OF TRANSPORTATION	
JOB PIECE NO. 34318(04)		SHEET NO. B004	

N:\OKC\Bridges\Projects\Production\1 ODOT\210532-C\2341A-SH 66 Lincoln Co\7 Drawings\DWG Files\B008-B009-34318(04)-GENERAL PLAN AND ELEVATION.dwg, 9/28/2023 1:32:44 PM, James Rinehart

SECTION 15
 T-14-N, R-2 E

SECTION 22
 T-14-N, R-2 E



UTILITY OWNERS	
ONG	800-458-4251
OG&E	800-272-9741
AT&T DISTRIBUTION	918-456-8867
LINCOLN COUNTY RURAL WATER DIST. #3	405-258-2200
TOWN OF WELLSTON	405-356-2476
ONEOK GAS TRANSPORTATION	918-588-7000
CIRCLE 9 RESOURCES	405-702-7420
CHICKASAW TELEPHONE	918-663-3565
CAPONE/SCISSORTAIL ENERGY	918-588-5000
CENTURYLINK	866-642-0444
SPRINT NEXTEL	888-211-4727
SUPERIOR PIPELINE	918-382-7200

BM #250 STA. 1603+24.84 @ 21.34' LT.
 BM SET 5/8 IRON PIN ELEV. = 870.82'

BM #251 STA. 1608+01.90 @ 20.49' LT.
 SET 'X' TOP OF NW WALL AT BRIDGE APPROACH
 ELEV. = 873.45'

BM #252 STA. 1614+02.93 @ 131.38' LT.
 BM SET 5/8 IRON PIN ELEV. = 856.78'

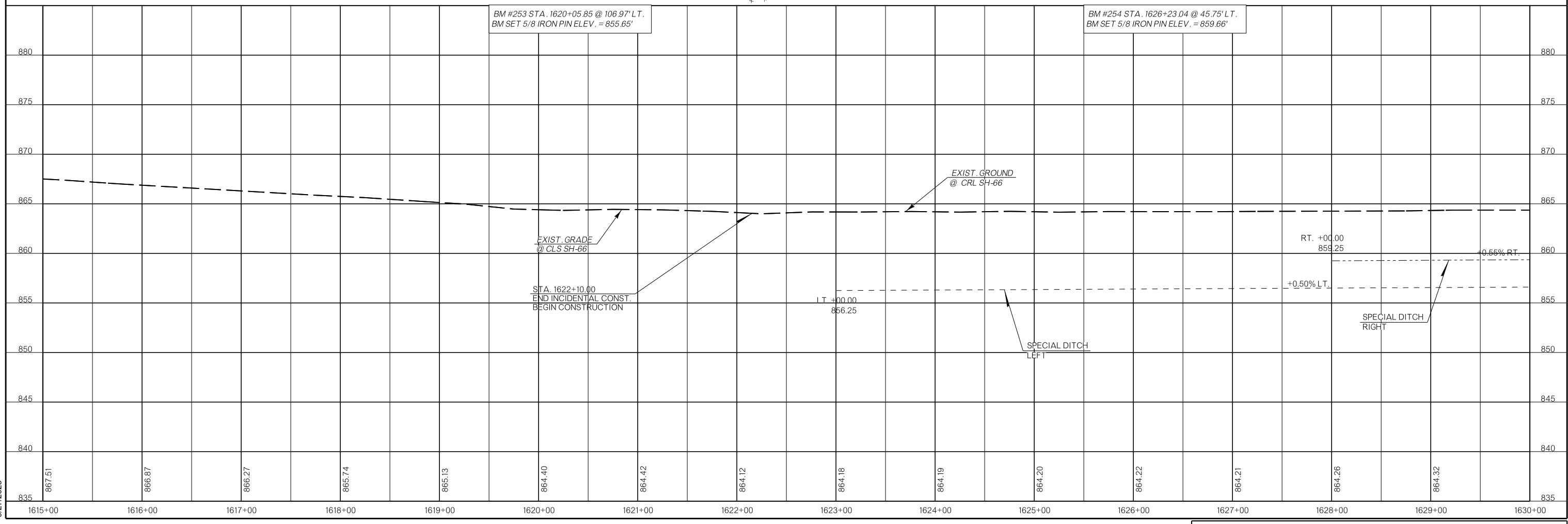
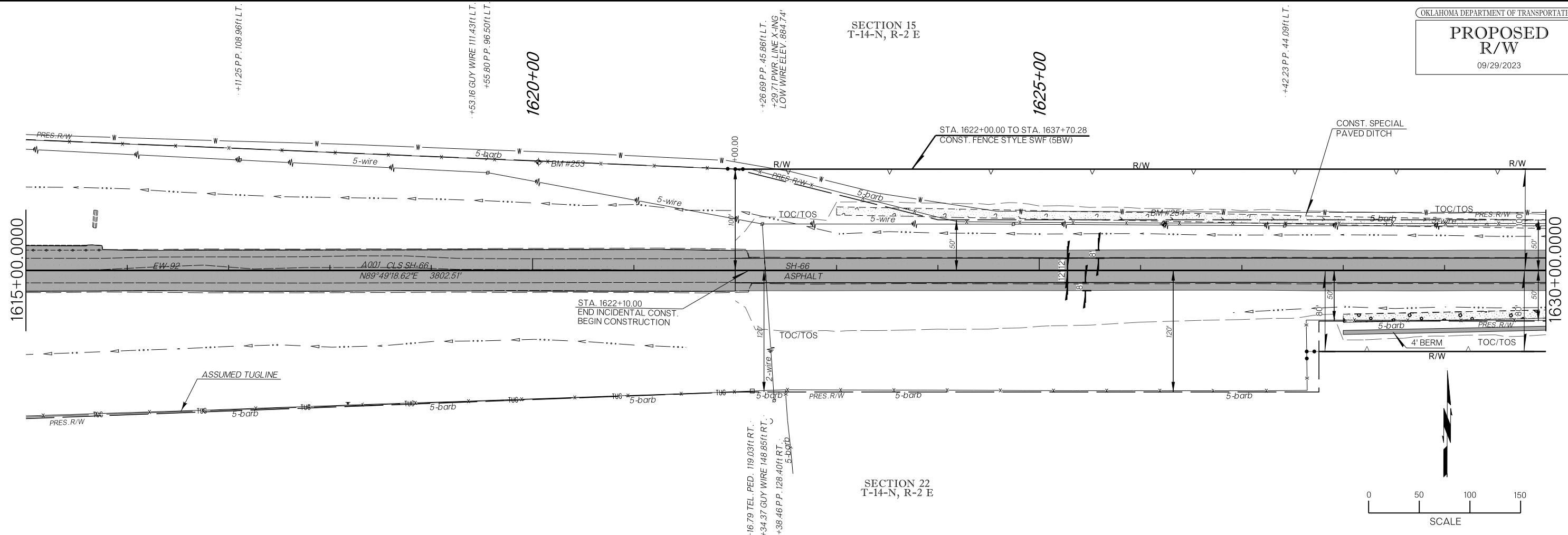
STA 1606+ 37.32
 BEGIN INCIDENTAL
 CONSTRUCTION

EXIST. GROUND
 @ CRL SH-66

9/27/2023

SECTION 15
 T-14-N, R-2 E

SECTION 22
 T-14-N, R-2 E



9/27/2023

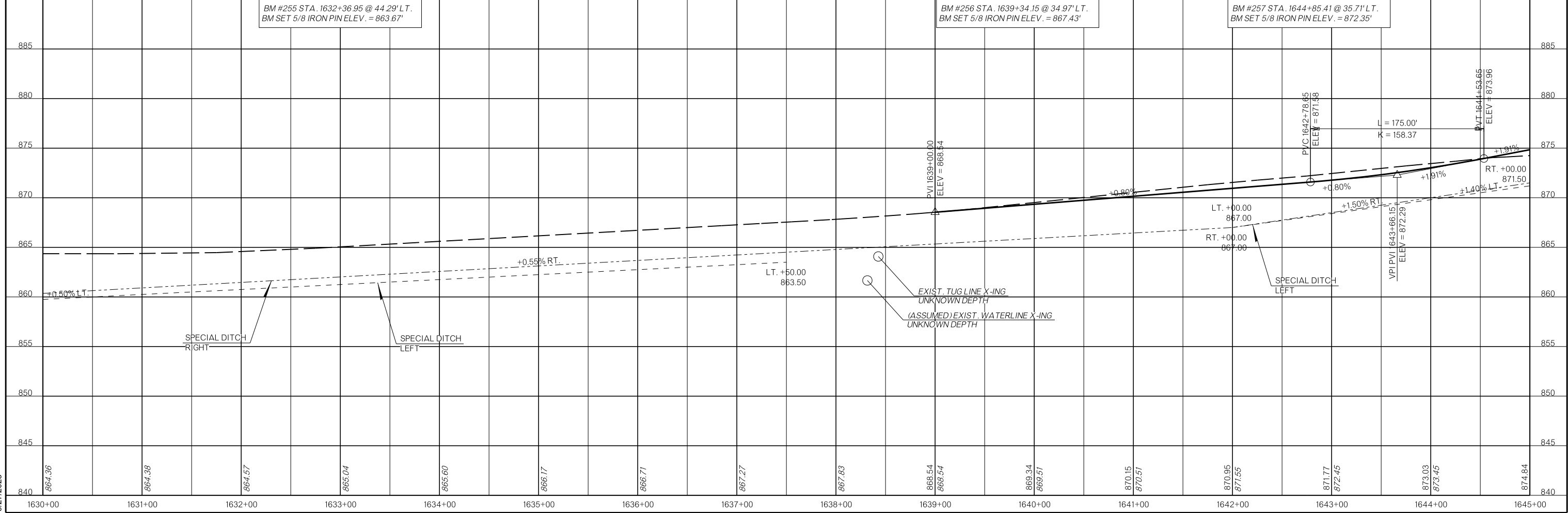
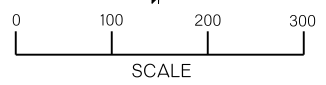
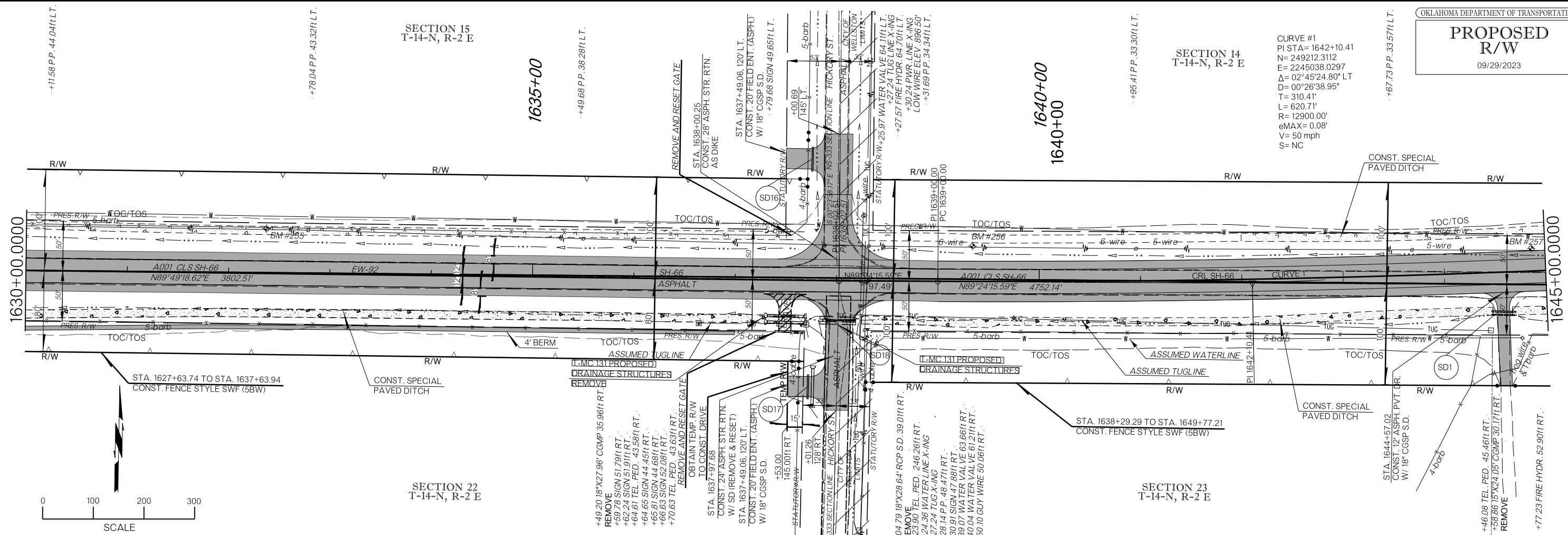
CURVE #1
PI STA= 1642+10.41
N= 249212.3112
E= 2245038.0297
Δ= 02°45'24.80" LT
D= 00°26'38.95"
T= 310.41'
L= 620.71'
R= 12900.00'
eMAX= 0.08'
V= 50 mph
S= NC

SECTION 15
T-14-N, R-2 E

SECTION 14
T-14-N, R-2 E

SECTION 22
T-14-N, R-2 E

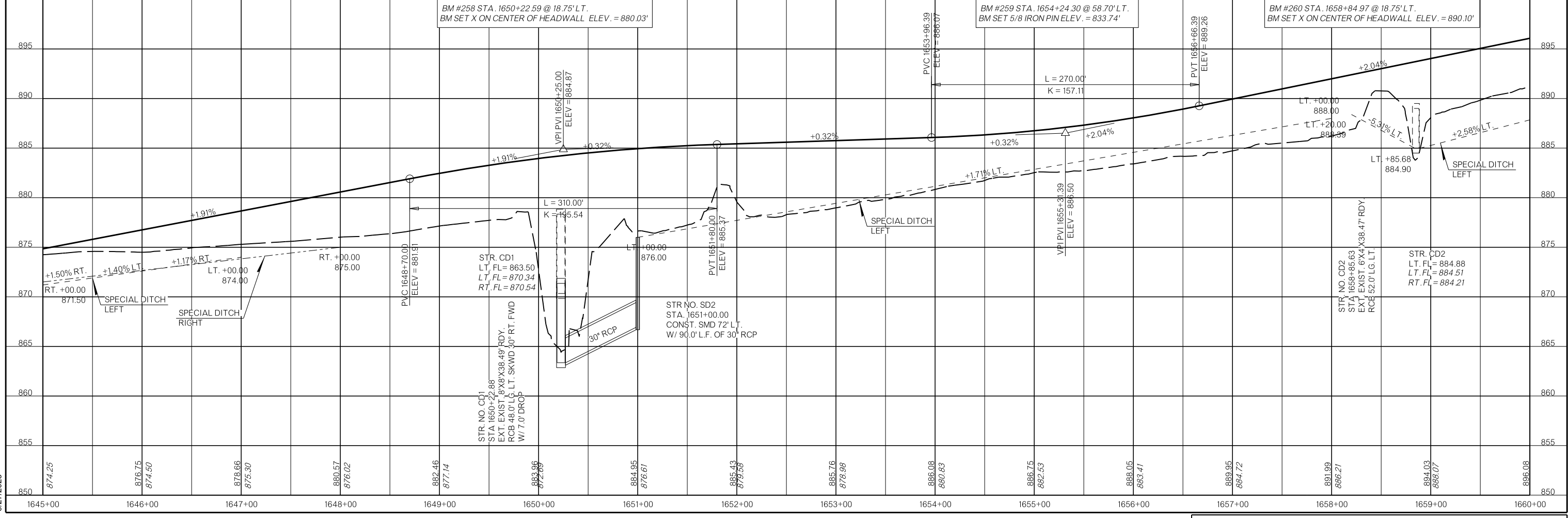
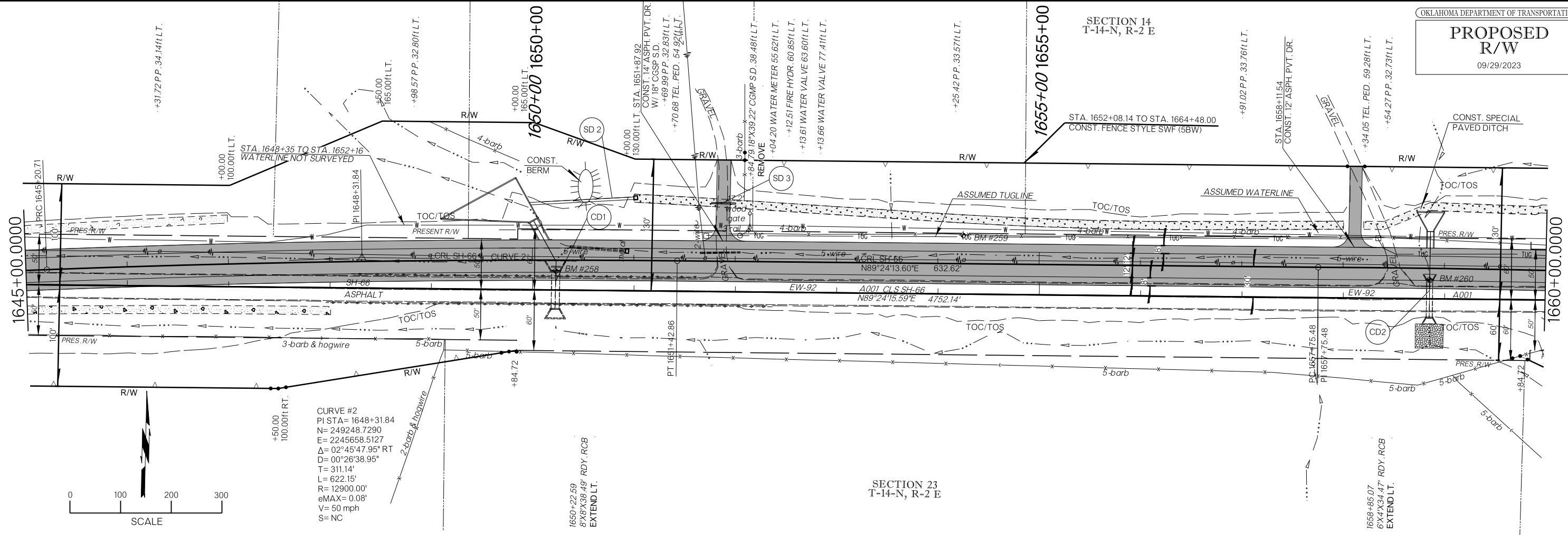
SECTION 23
T-14-N, R-2 E



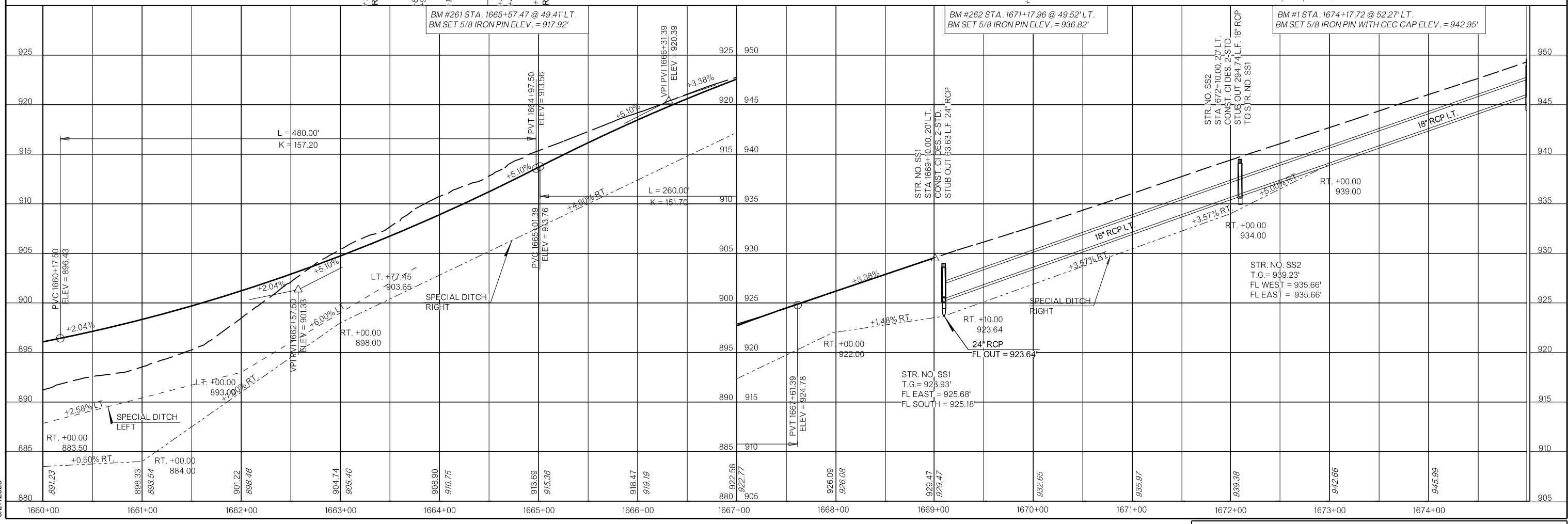
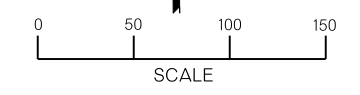
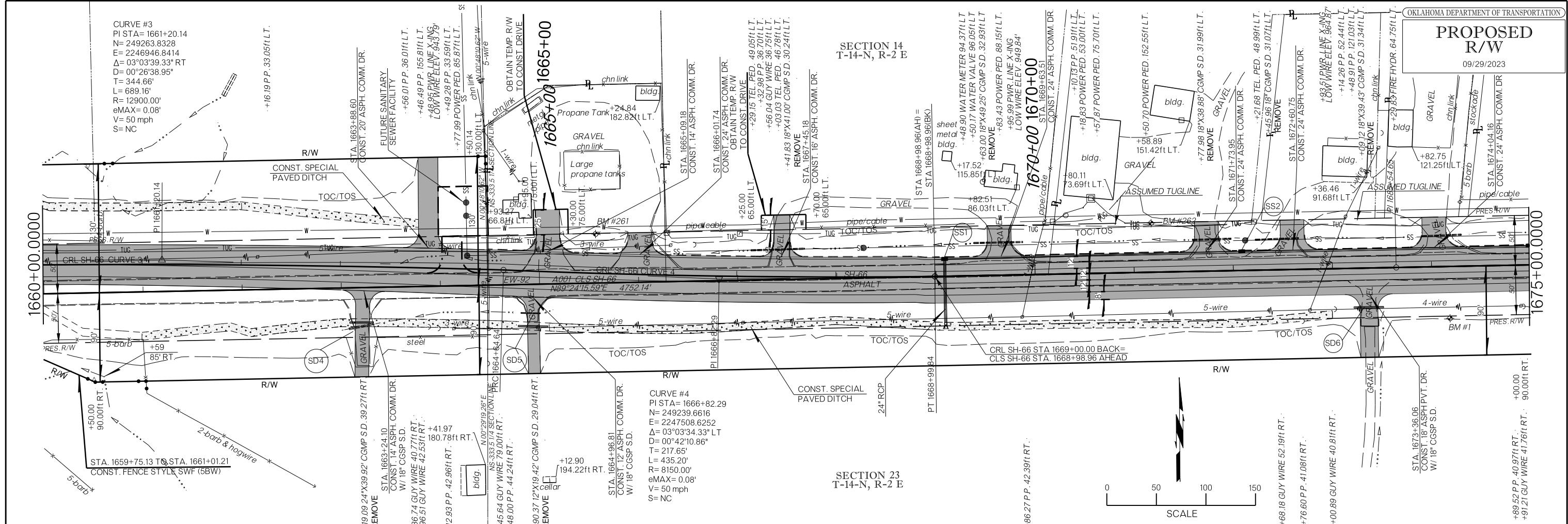
9/27/2023

SECTION 14
 T-14-N, R-2 E

SECTION 23
 T-14-N, R-2 E



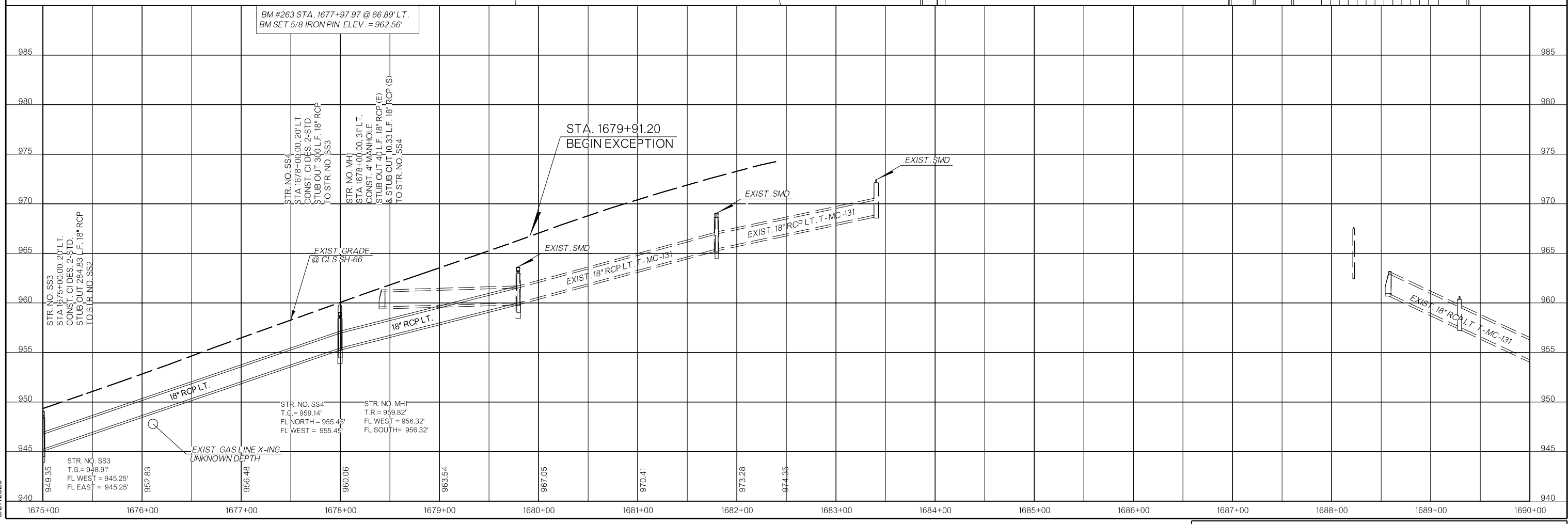
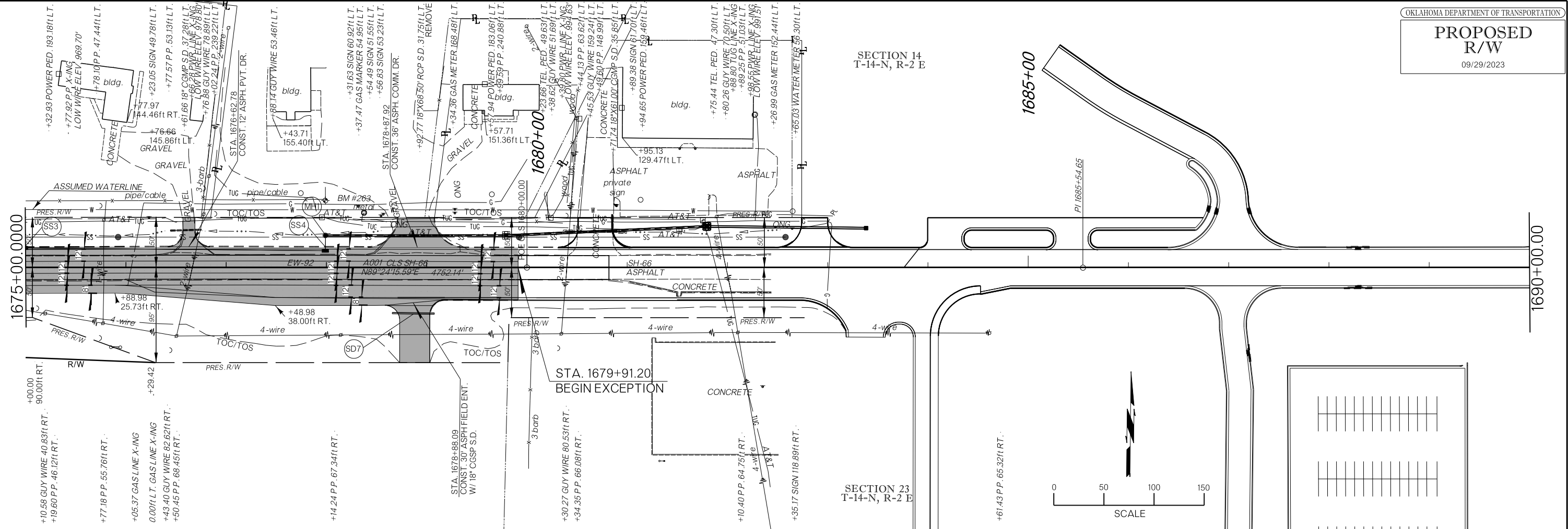
9/27/2023



9/27/2023

SECTION 14
 T-14-N, R-2 E

SECTION 23
 T-14-N, R-2 E



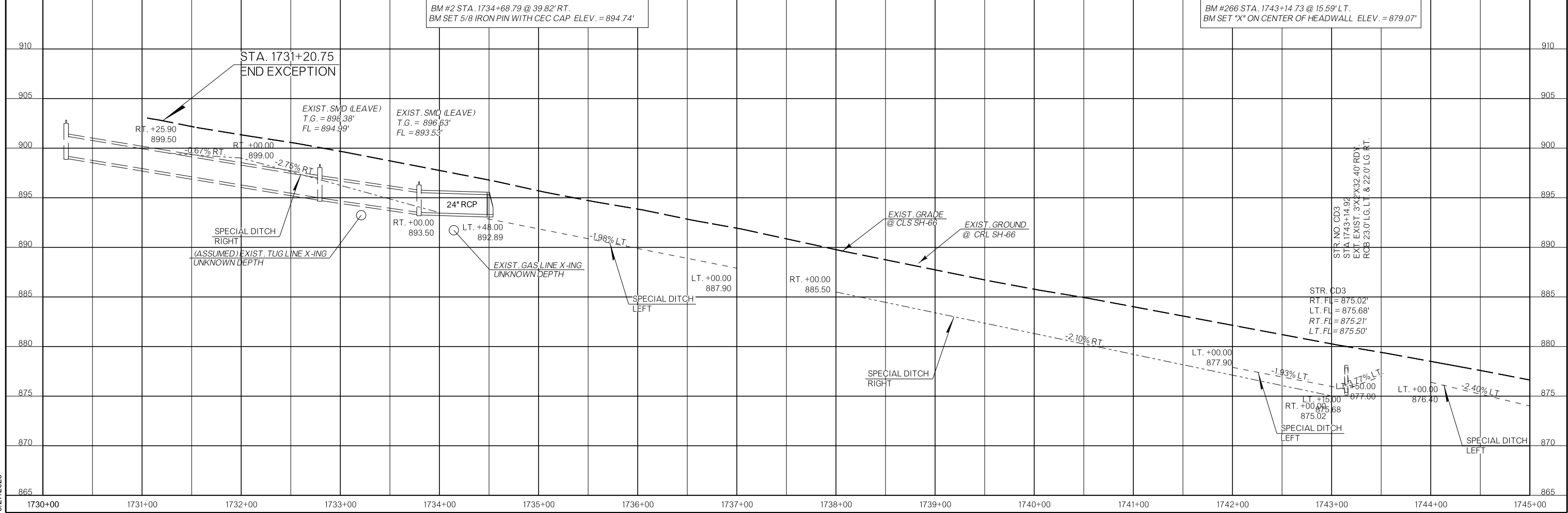
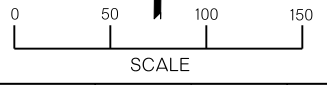
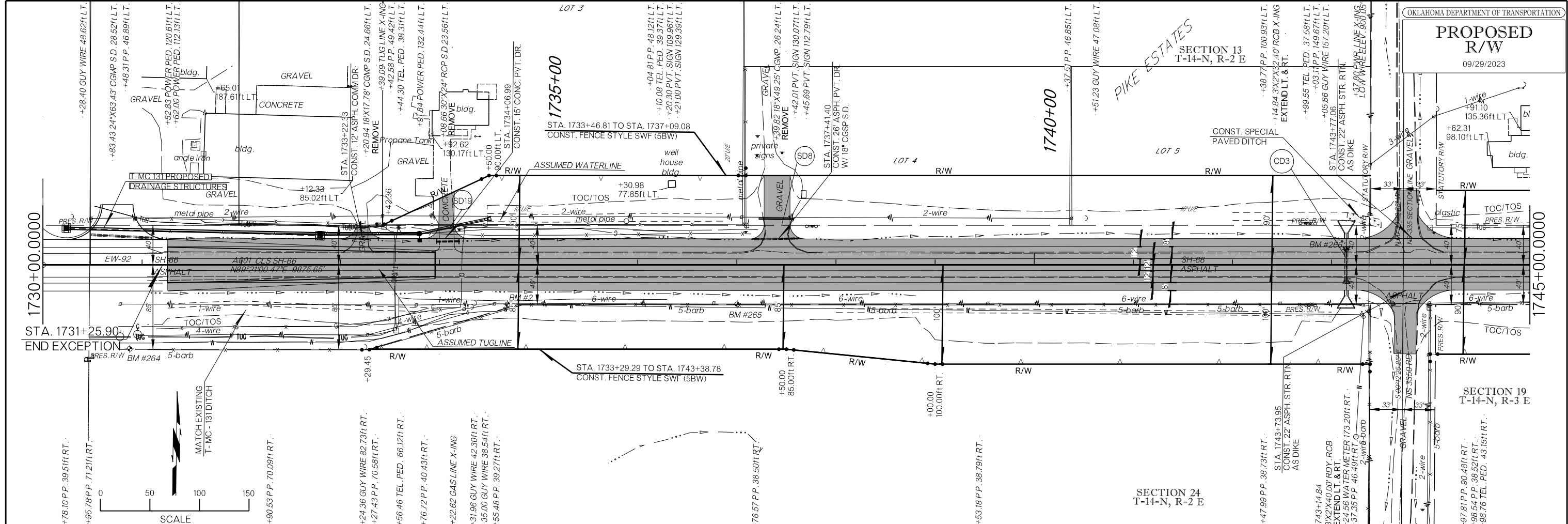
9/27/2023

PROPOSED R/W
09/29/2023

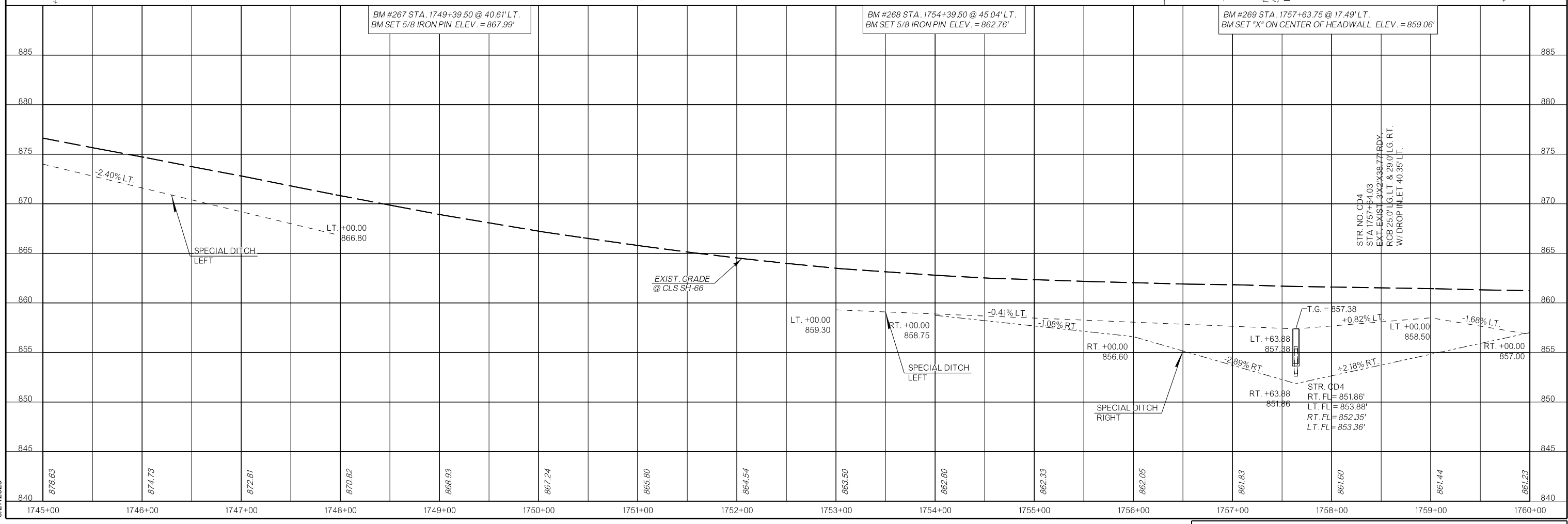
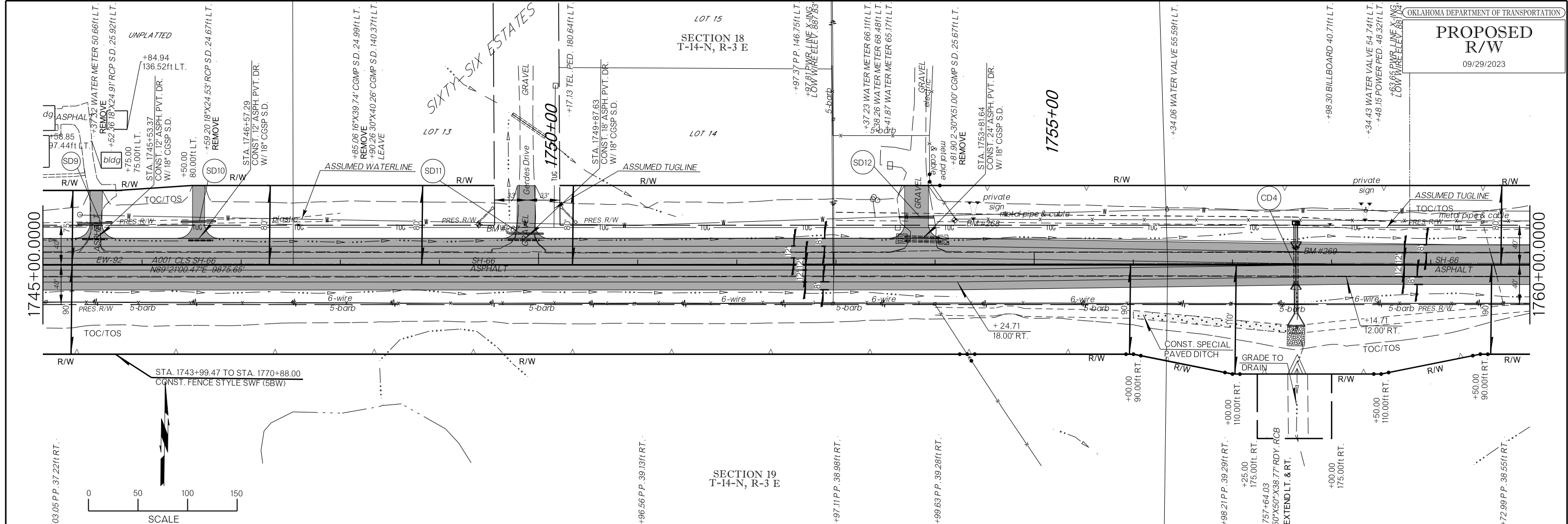
SECTION 13
T-14-N, R-2 E

SECTION 19
T-14-N, R-3 E

SECTION 24
T-14-N, R-2 E



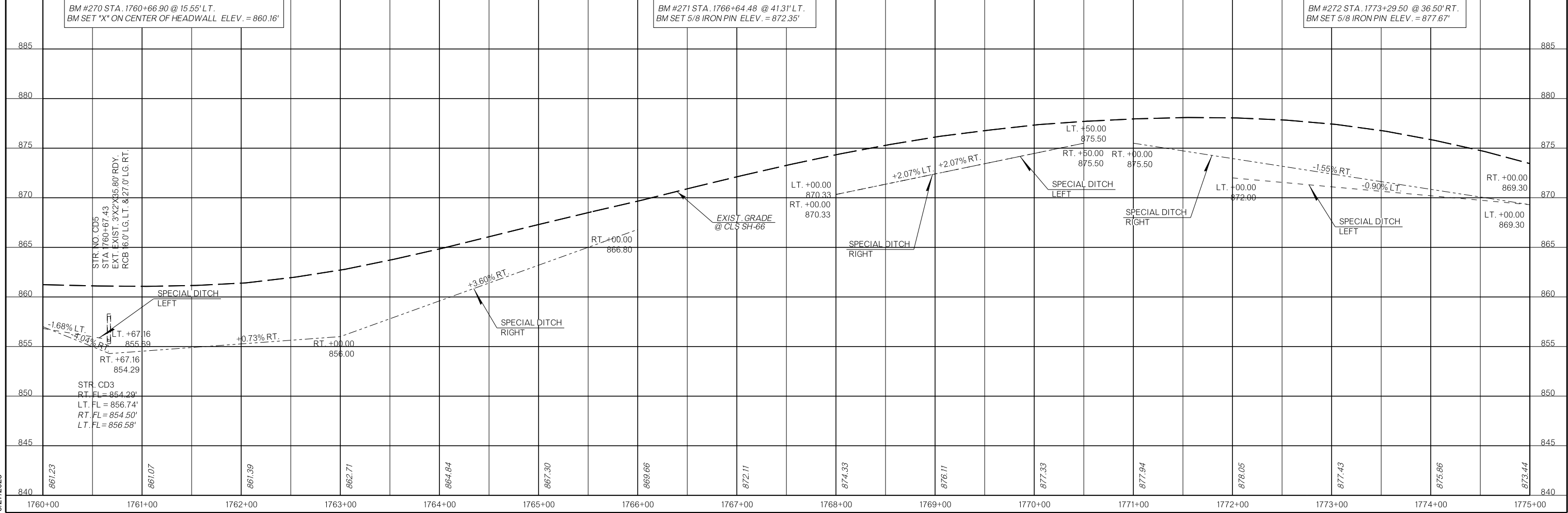
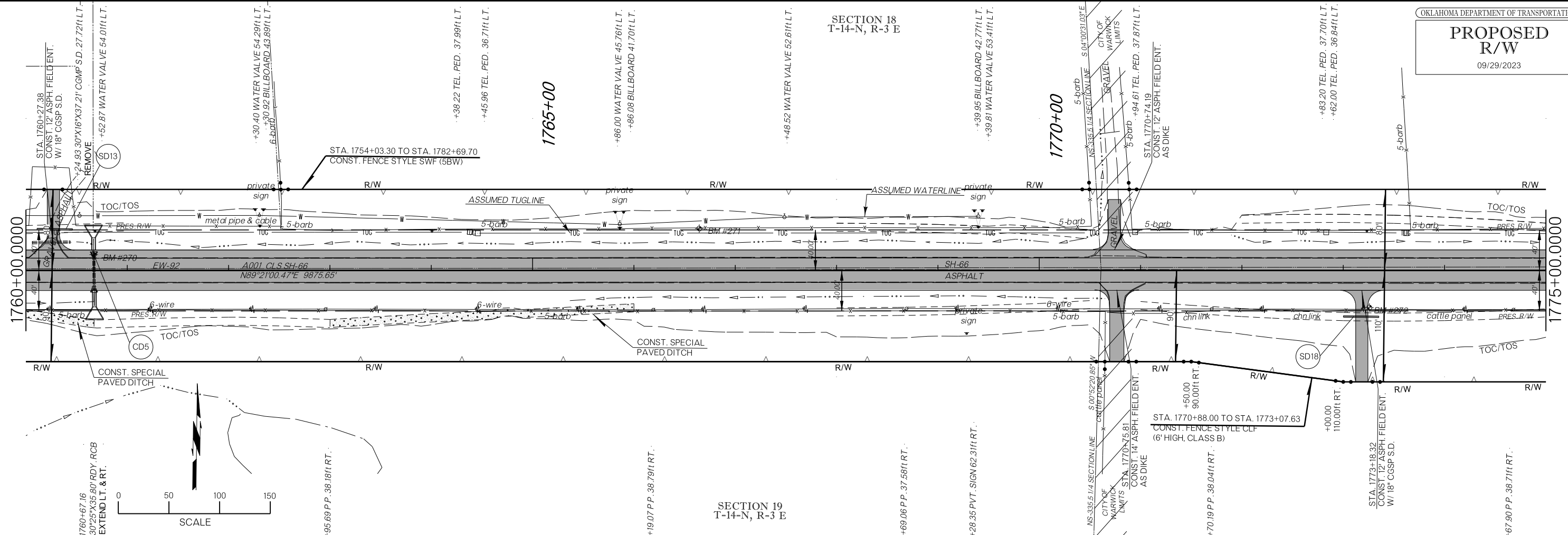
9/27/2023



9/27/2023

SECTION 18
 T-14-N, R-3 E

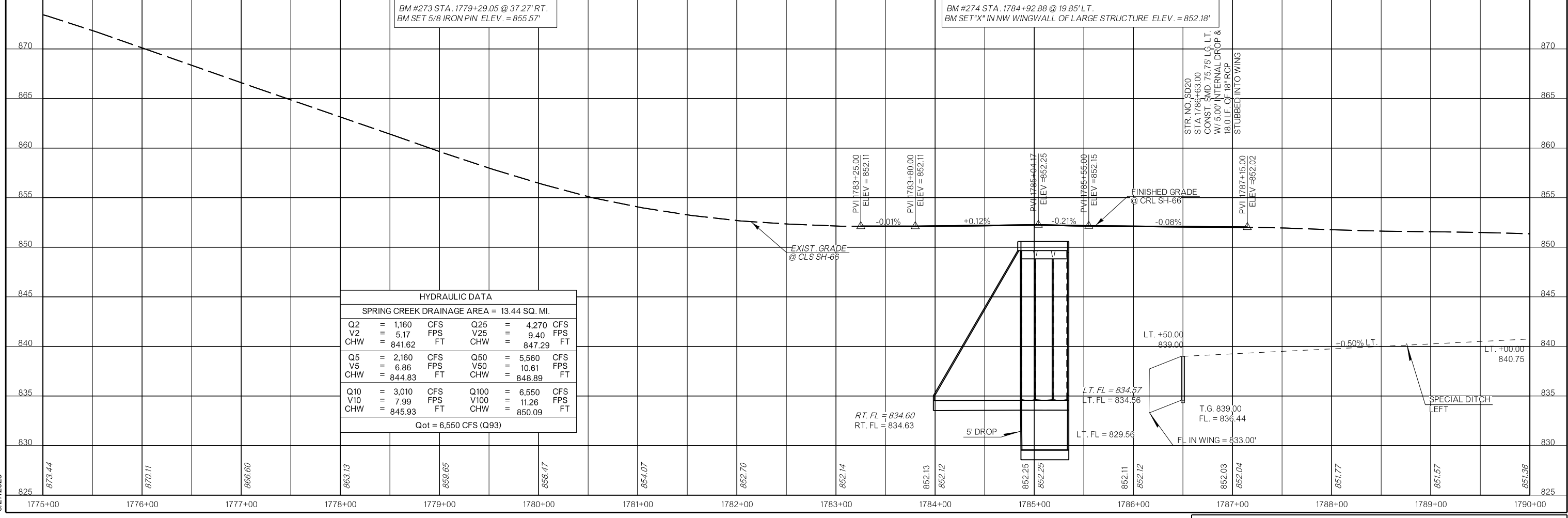
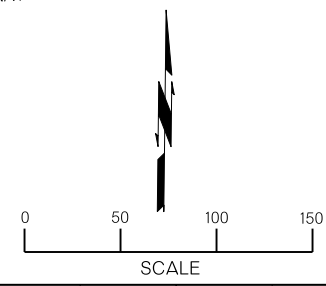
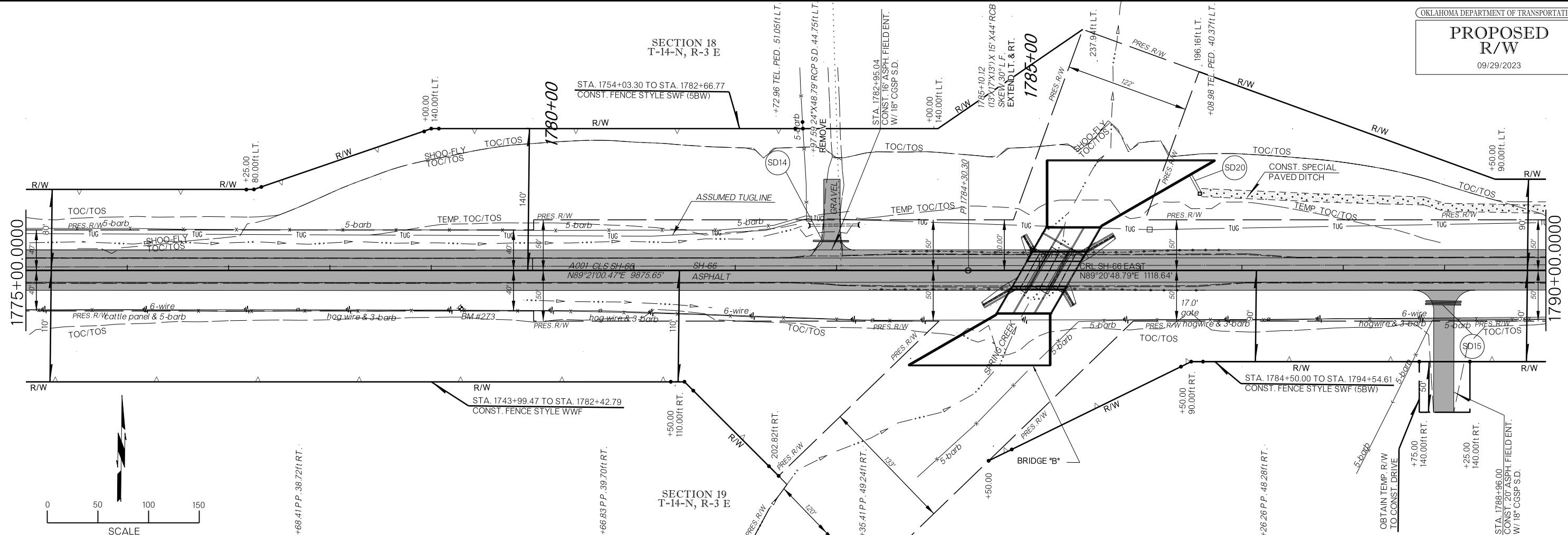
SECTION 19
 T-14-N, R-3 E



9/27/2023

SECTION 18
 T-14-N, R-3 E

SECTION 19
 T-14-N, R-3 E



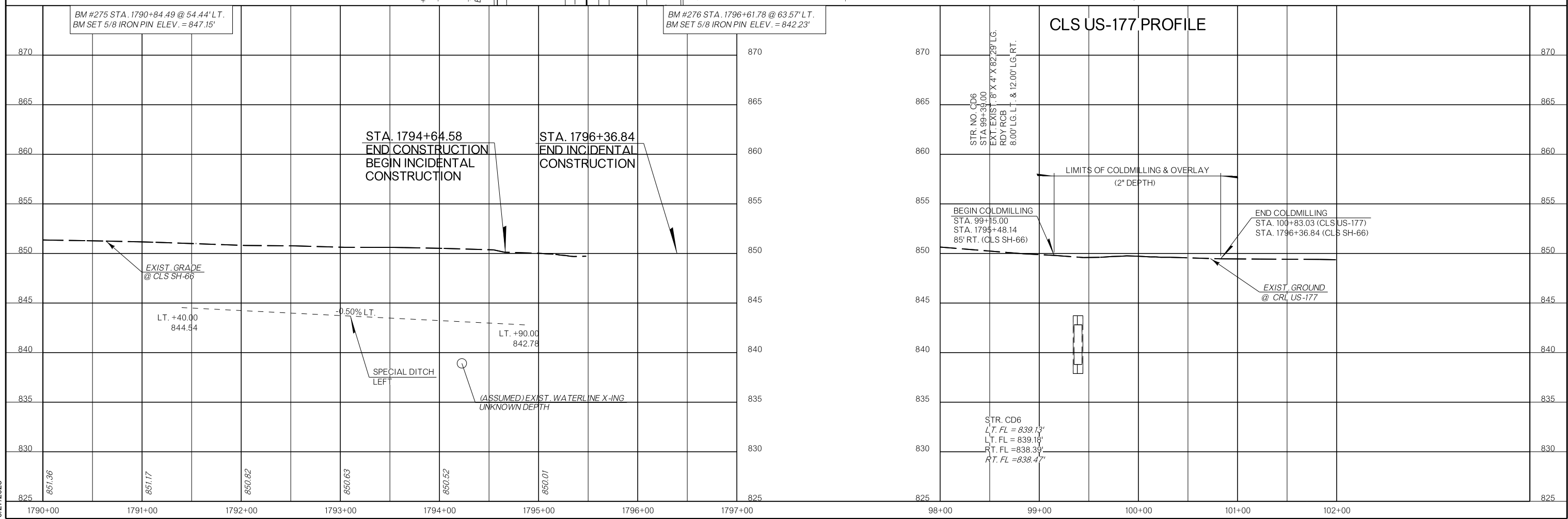
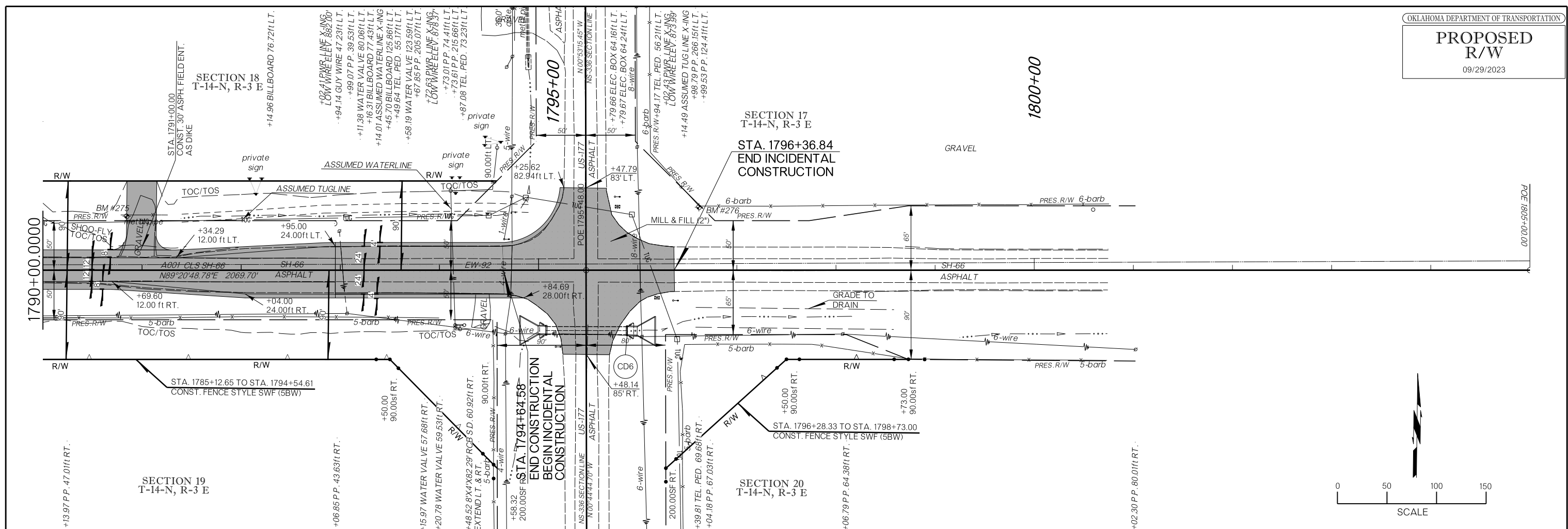
HYDRAULIC DATA

SPRING CREEK DRAINAGE AREA = 13.44 SQ. MI.

Q2 = 1,160 CFS	Q25 = 4,270 CFS
V2 = 5.17 FPS	V25 = 9.40 FPS
CHW = 841.62 FT	CHW = 847.29 FT
Q5 = 2,160 CFS	Q50 = 5,560 CFS
V5 = 6.86 FPS	V50 = 10.61 FPS
CHW = 844.83 FT	CHW = 848.89 FT
Q10 = 3,010 CFS	Q100 = 6,550 CFS
V10 = 7.99 FPS	V100 = 11.26 FPS
CHW = 845.93 FT	CHW = 850.09 FT

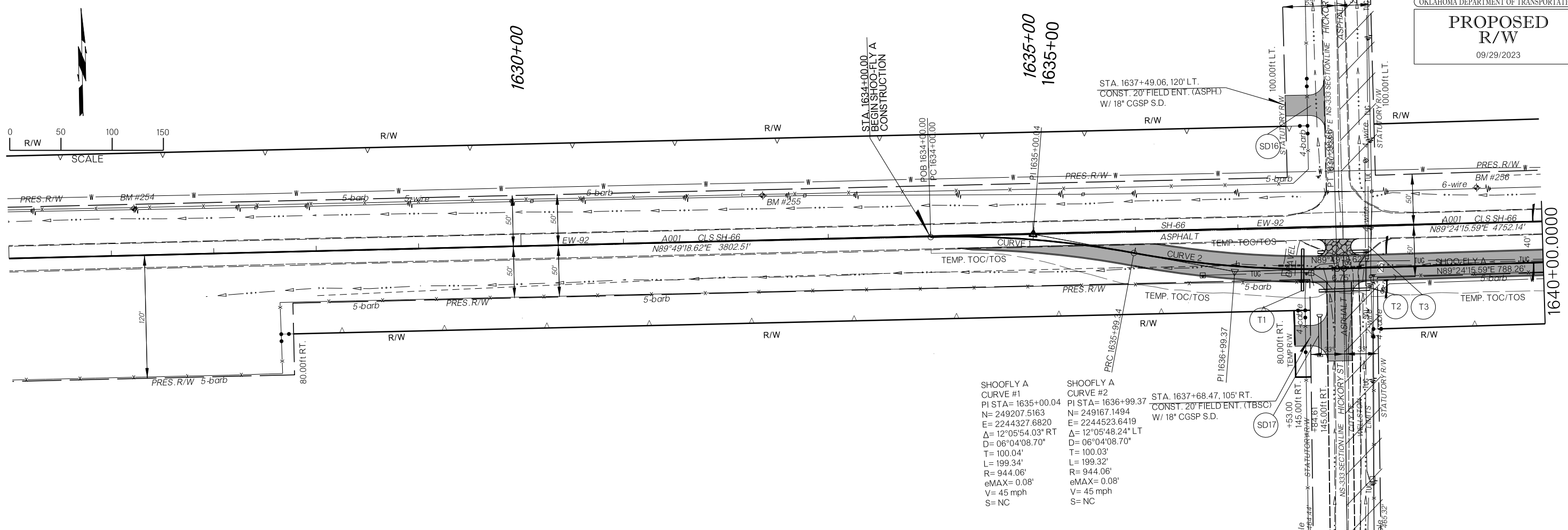
Qot = 6,550 CFS (Q93)

9/27/2023



9/27/2023

PROPOSED R/W
09/29/2023



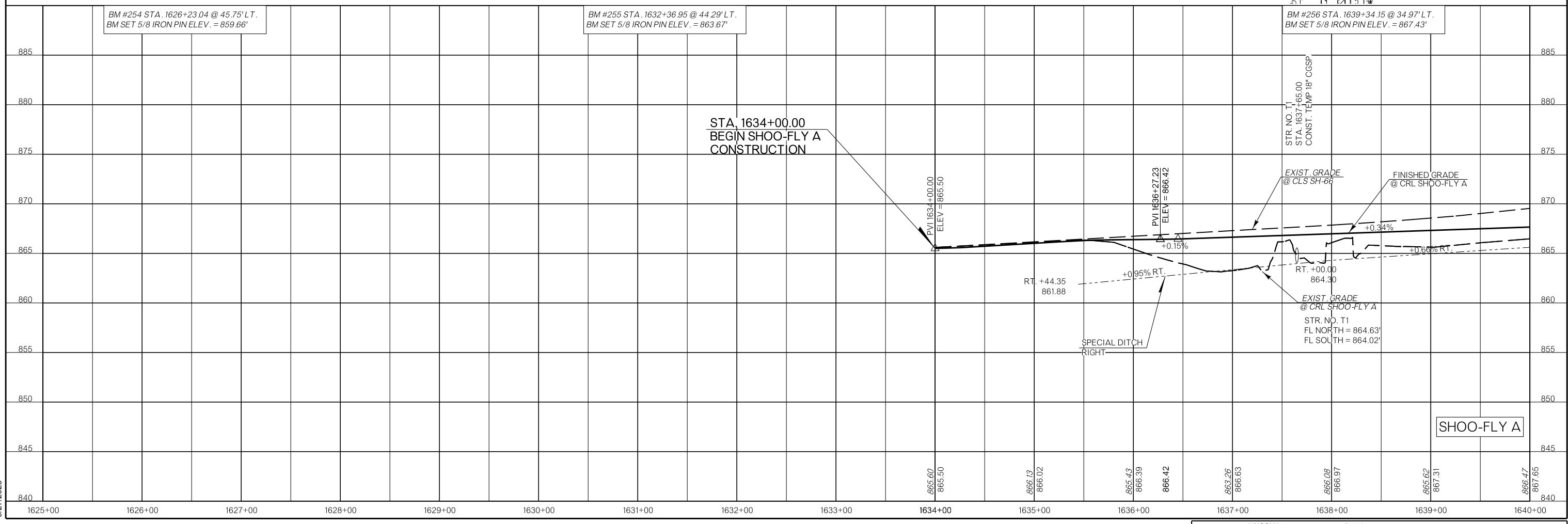
SHOOFLY A CURVE #1
 PI STA= 1635+00.04
 N= 249207.5163
 E= 2244327.6820
 $\Delta = 12^{\circ}05'54.03''$ RT
 D= 06^{\circ}04'08.70"
 T= 100.04'
 L= 199.34'
 R= 944.06'
 eMAX= 0.08'
 V= 45 mph
 S= NC

SHOOFLY A CURVE #2
 PI STA= 1636+99.37
 N= 249167.1494
 E= 2244523.6419
 $\Delta = 12^{\circ}05'48.24''$ LT
 D= 06^{\circ}04'08.70"
 T= 100.03'
 L= 199.32'
 R= 944.06'
 eMAX= 0.08'
 V= 45 mph
 S= NC

BM #254 STA. 1626+23.04 @ 45.75' LT.
 BM SET 5/8 IRON PIN ELEV. = 859.66'

BM #255 STA. 1632+36.95 @ 44.29' LT.
 BM SET 5/8 IRON PIN ELEV. = 863.67'

BM #256 STA. 1639+34.15 @ 34.97' LT.
 BM SET 5/8 IRON PIN ELEV. = 867.43'



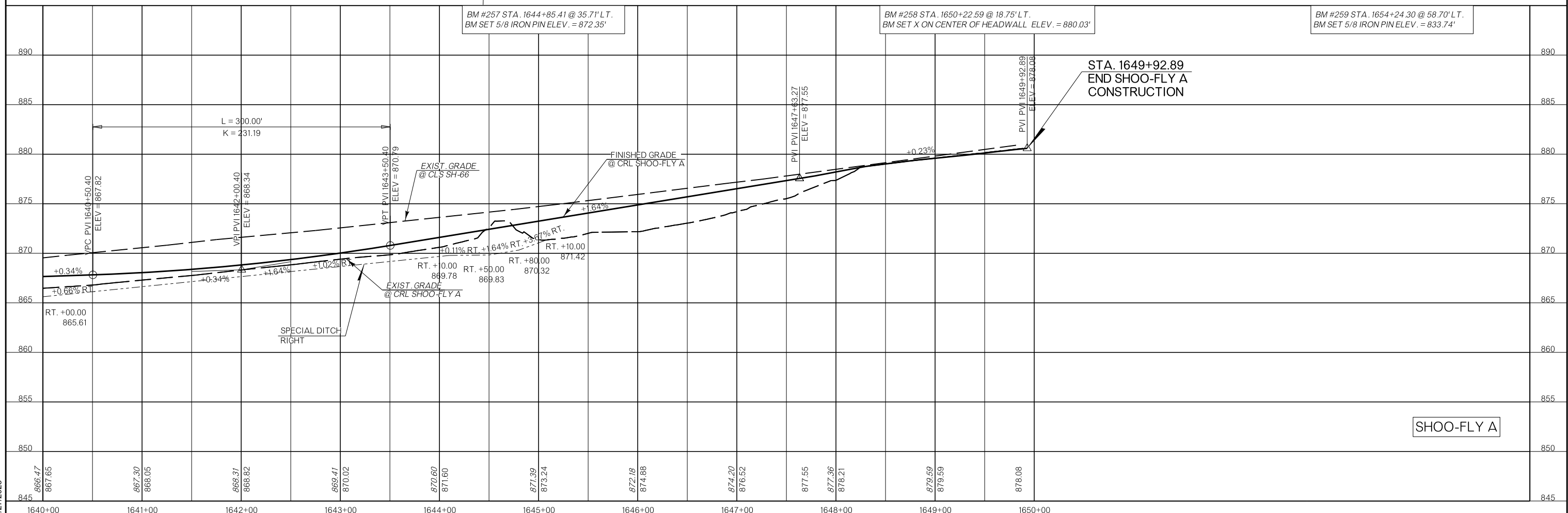
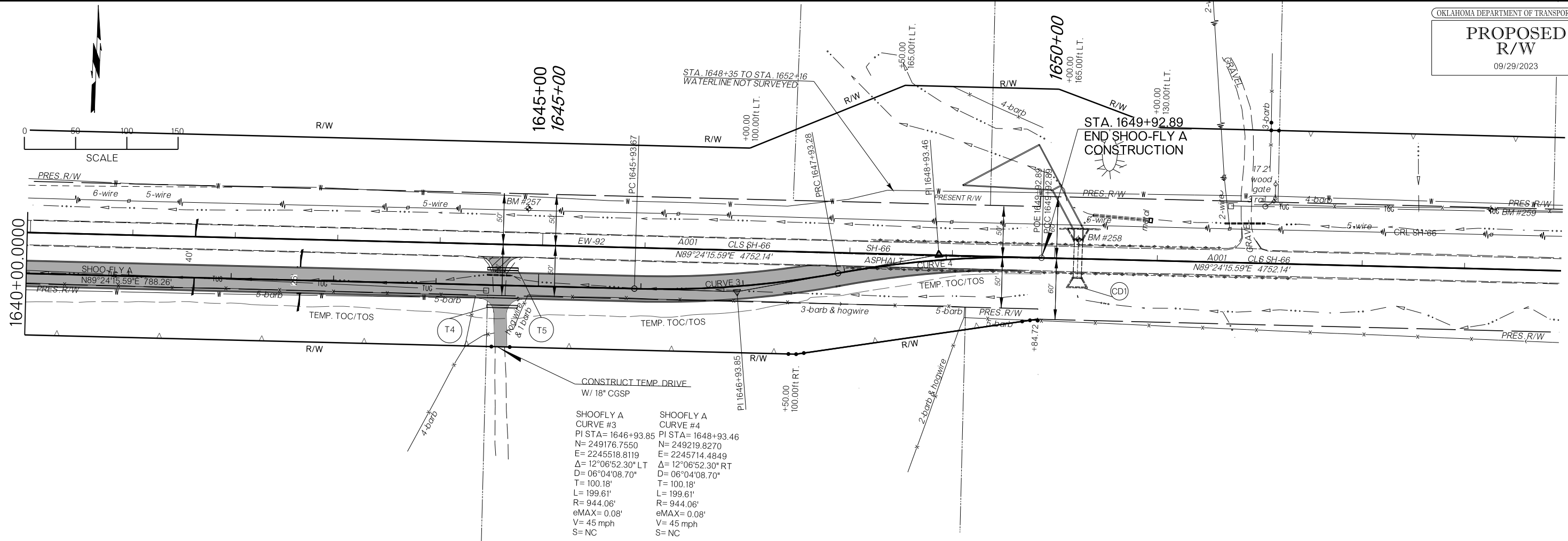
STA. 1634+00.00
 BEGIN SHOO-FLY A
 CONSTRUCTION

EXIST. GRADE @ CLS SH-66
 FINISHED GRADE @ CRL SHOO-FLY A

EXIST. GRADE @ CRL SHOO-FLY A
 STR. NO. T1
 FL NORTH = 864.63'
 FL SOUTH = 864.02'

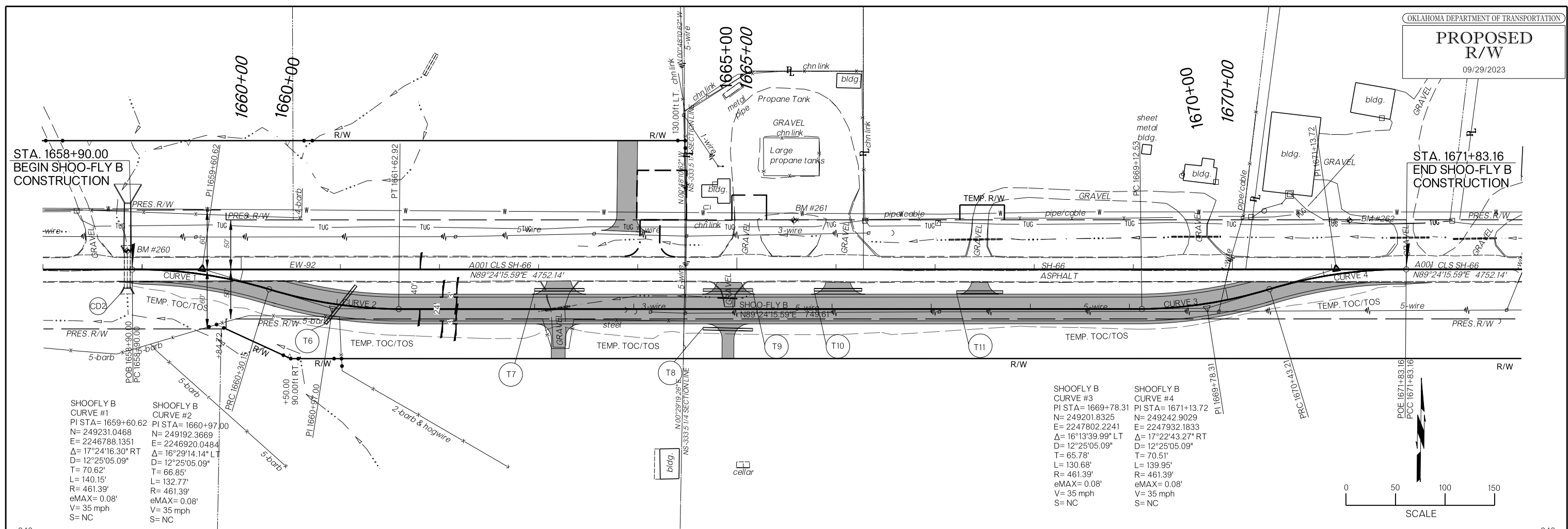
SHOO-FLY A

9/27/2023



SHOO-FLY A

9/27/2023

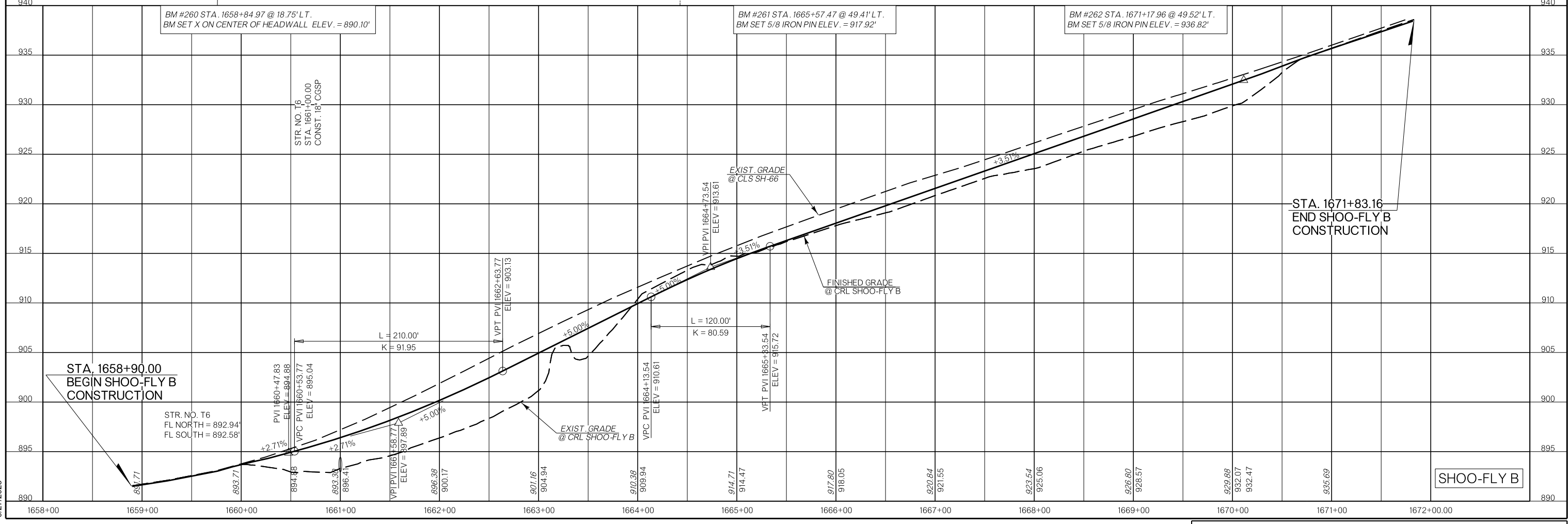
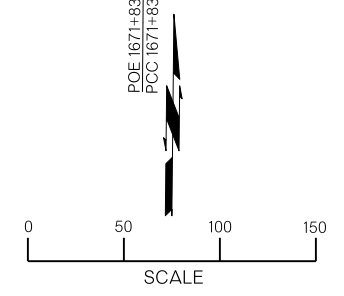


SHOOFLY B
CURVE #1
PI STA= 1659+60.62
N= 249231.0468
E= 2246788.1351
Δ= 17°24'16.30\" RT
D= 12°25'05.09\"
T= 70.62'
L= 140.15'
R= 461.39'
eMAX= 0.08'
V= 35 mph
S= NC

SHOOFLY B
CURVE #2
PI STA= 1660+97.00
N= 249192.3669
E= 2246920.0484
Δ= 16°29'14.14\" LT
D= 12°25'05.09\"
T= 66.85'
L= 132.77'
R= 461.39'
eMAX= 0.08'
V= 35 mph
S= NC

SHOOFLY B
CURVE #3
PI STA= 1669+78.31
N= 249201.8325
E= 2247802.2241
Δ= 16°13'39.99\" LT
D= 12°25'05.09\"
T= 65.78'
L= 130.68'
R= 461.39'
eMAX= 0.08'
V= 35 mph
S= NC

SHOOFLY B
CURVE #4
PI STA= 1671+13.72
N= 249242.9029
E= 2247932.1833
Δ= 17°22'43.27\" RT
D= 12°25'05.09\"
T= 70.51'
L= 139.95'
R= 461.39'
eMAX= 0.08'
V= 35 mph
S= NC



BM #260 STA. 1658+84.97 @ 18.75' LT.
BM SET X ON CENTER OF HEADWALL ELEV. = 890.10'

BM #261 STA. 1665+57.47 @ 49.41' LT.
BM SET 5/8 IRON PIN ELEV. = 917.92'

BM #262 STA. 1671+17.96 @ 49.52' LT.
BM SET 5/8 IRON PIN ELEV. = 936.82'

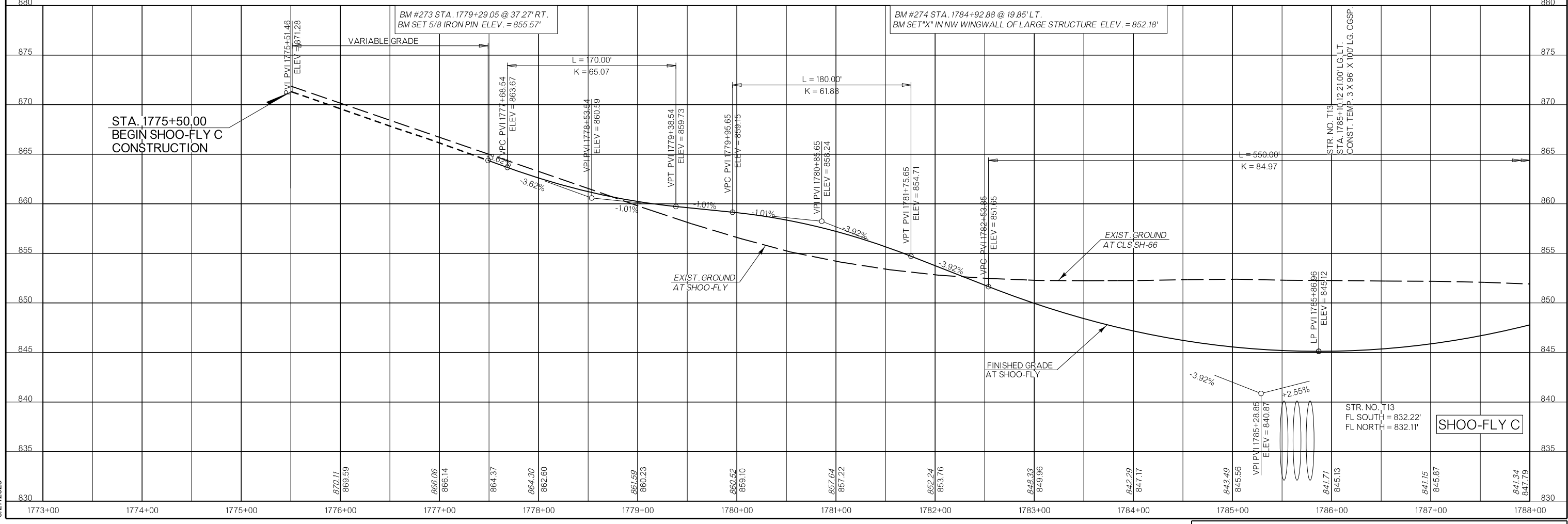
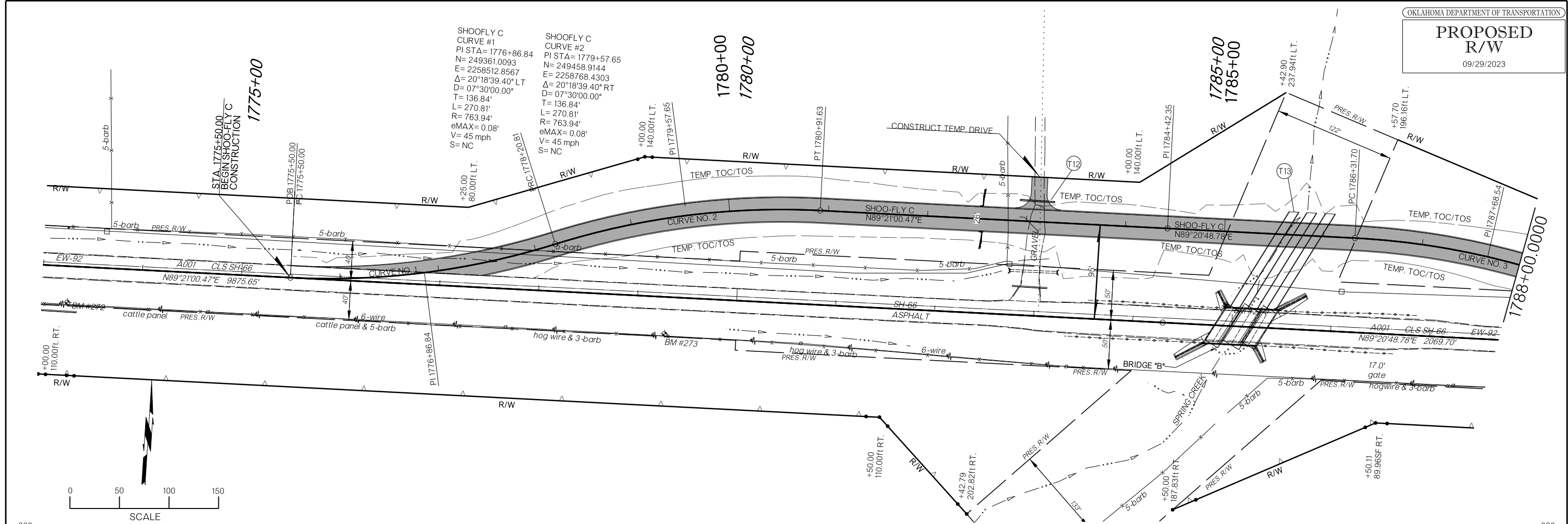
STA. 1671+83.16
END SHOOF-FLY B
CONSTRUCTION

STA. 1658+90.00
BEGIN SHOOF-FLY B
CONSTRUCTION

STR. NO. T6
FL NORTH = 892.94'
FL SOUTH = 892.58'

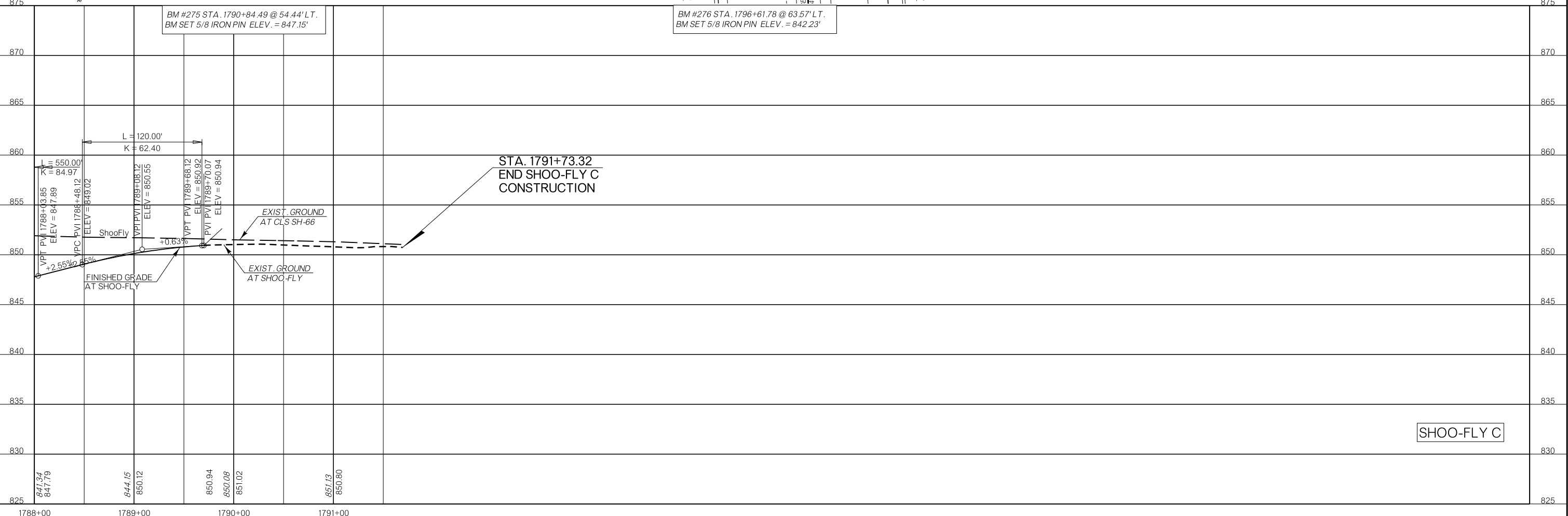
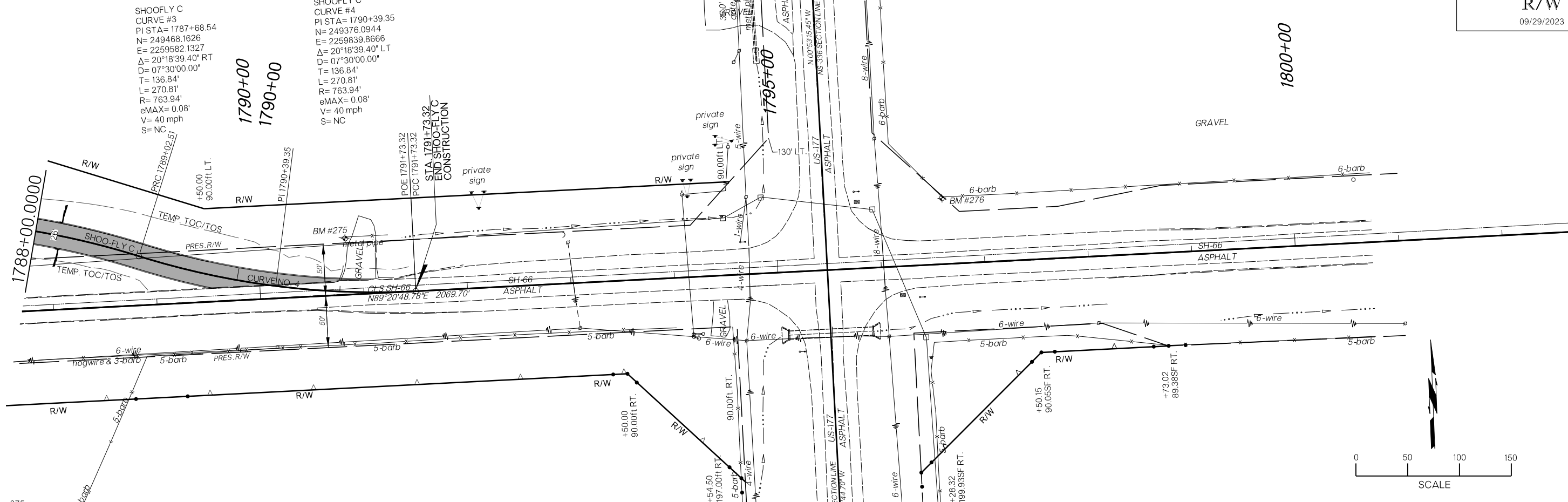
SHOOFLY B

9/27/2023

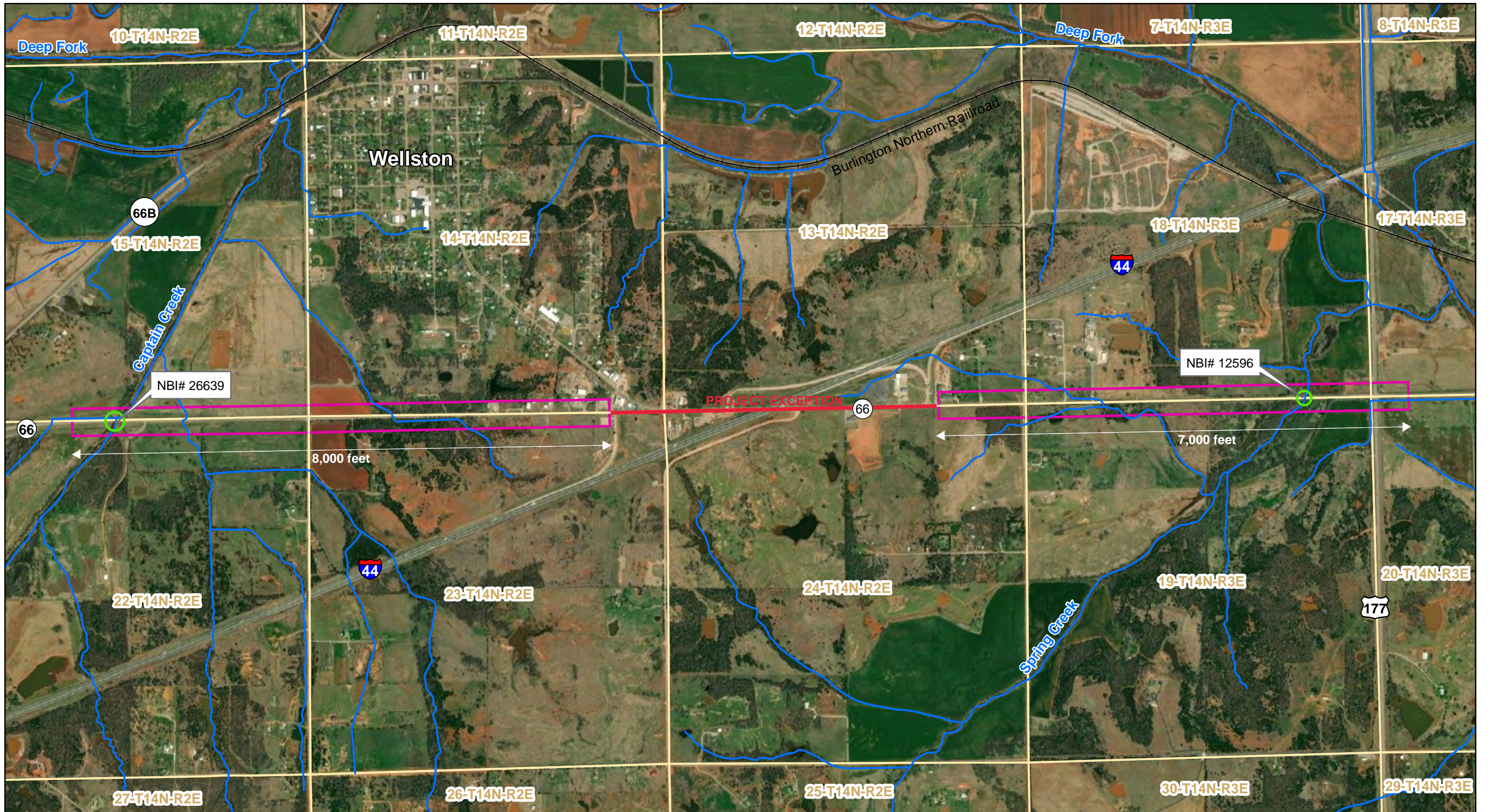


SHOO-FLY C

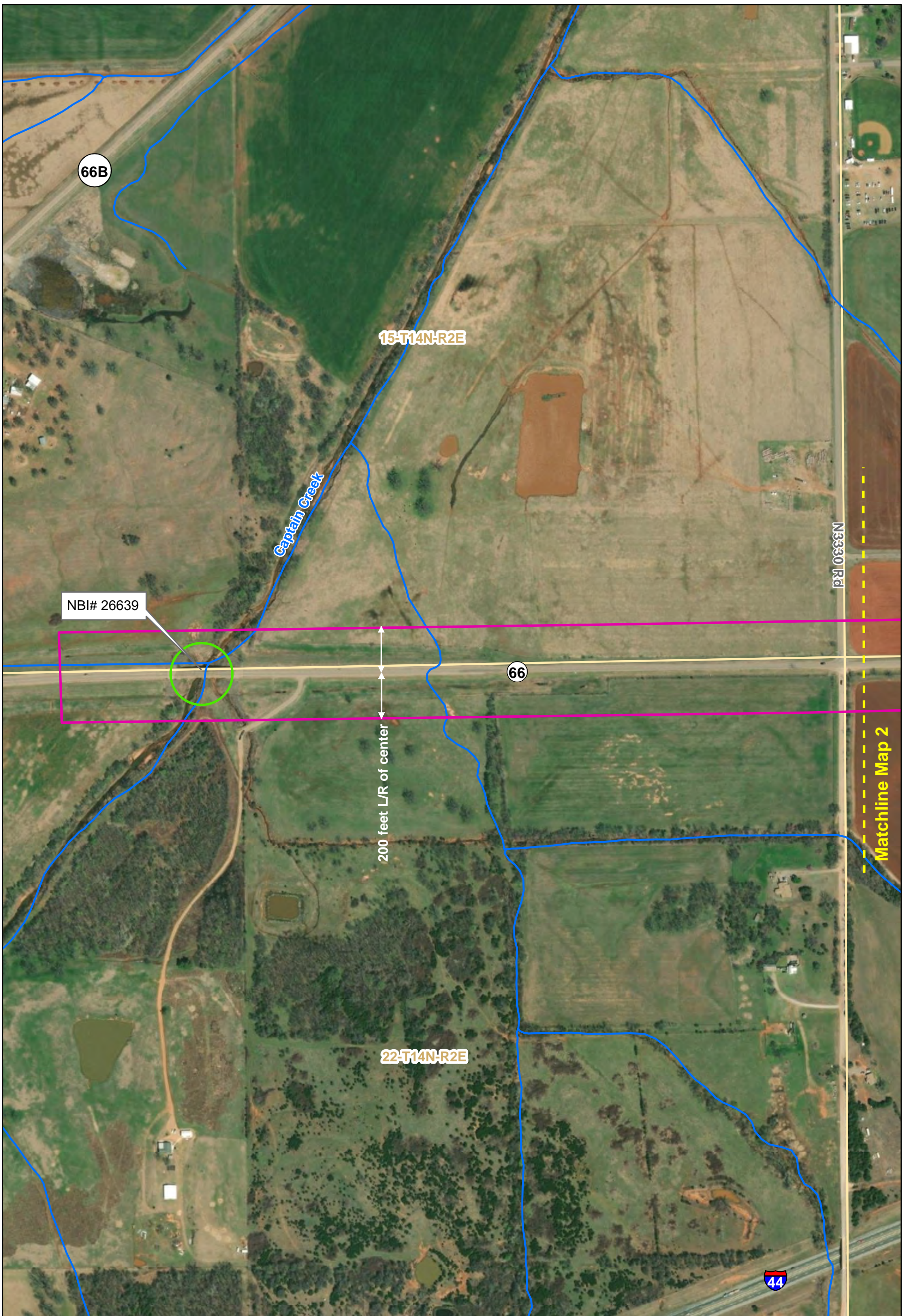
9/27/2023










9/27/2023



<p>COMMENTS/LEGEND</p> <p> Environmental Footprint</p> <p> NBI Bridges</p> <p> Project Exception</p> <p> Mapped Streams</p> <p> Sections</p>		<p>0 437.5 875 1,750 Feet</p> <p>THE DIAGRAM IS PARTIALLY DIAGRAMATIC; EXACT DIMENSIONS WILL VARY</p> <p>DRAWN BY: LMP CHECKED BY: GAC</p> <p>SOURCE: USGS, ESRI</p>	<p>NEPA Footprint Map</p> <p>JP 34318(04) / J3-4318(004)</p> <p>Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes 1.1 mile turnpike project exception)</p> <p>Lincoln County, OK</p>		<p> OKLAHOMA Transportation</p> <p>Date: 1/10/2022</p>	<p>FIGURE 1</p>
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COMMENTS/LEGEND	
	NEPA Environmental Footprint
	NBI Bridges
	Mapped Streams
	Project Exception
	Sections
	Matchlines
 0 100 200 400 600 800 Feet	

NEPA Footprint Map

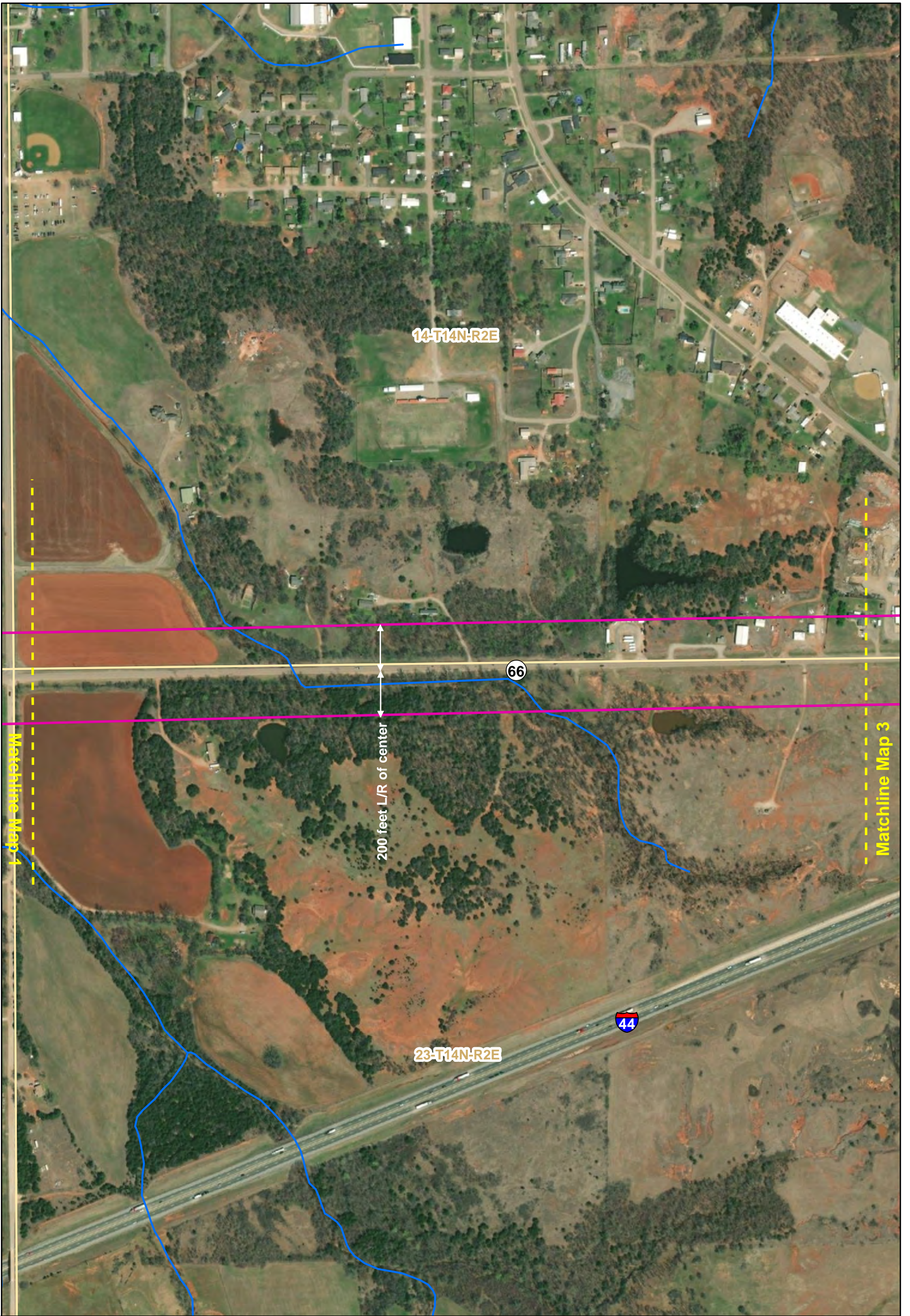
JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66 from
 SH-102, east to US-177 (includes 1.1 mile
 turnpike project exception)
 Lincoln County, OK



DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

**Figure
2a**

Date: 1/10/2022



COMMENTS/LEGEND	
	NEPA Environmental Footprint
	NBI Bridges
	Mapped Streams
	Project Exception
	Sections
	Matchlines
<p>0 100 200 400 600 800 Feet</p>	

NEPA Footprint Map

JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes 1.1 mile turnpike project exception)
 Lincoln County, OK








OKLAHOMA Transportation

DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000, USGS

Figure 2b


Date: 1/10/2022



COMMENTS/LEGEND	
	NEPA Environmental Footprint
	NBI Bridges
	Mapped Streams
	Project Exception
	Sections
	Matchlines
 0 100 200 400 600 800 Feet	

NEPA Footprint Map

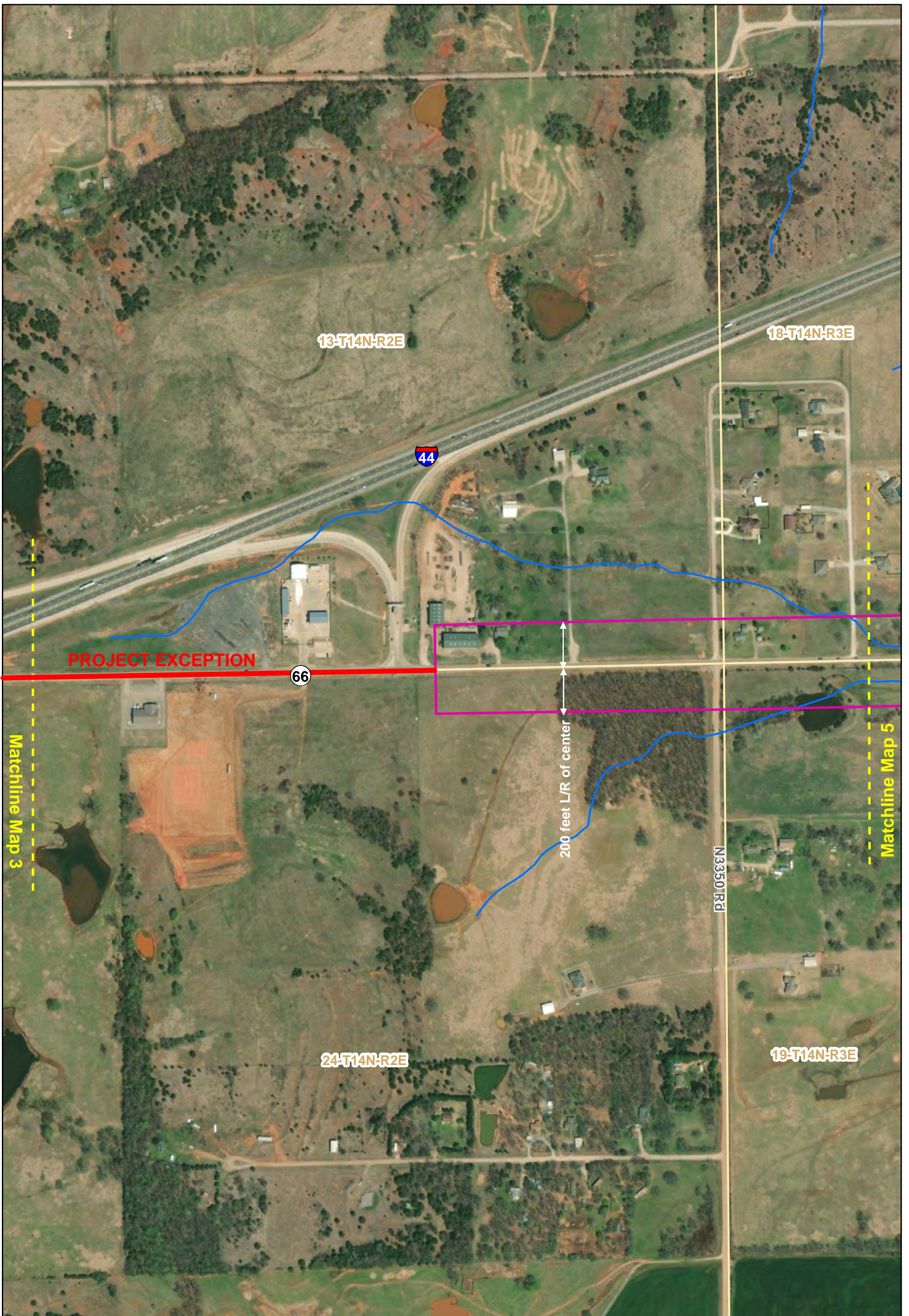
JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes 1.1 mile turnpike project exception)
 Lincoln County, OK



DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000, USGS

Figure 2c

Date: 1/10/2022



COMMENTS/LEGEND	
	NEPA Environmental Footprint
	NBI Bridges
	Mapped Streams
	Project Exception
	Matchlines
	Sections

NEPA Footprint Map

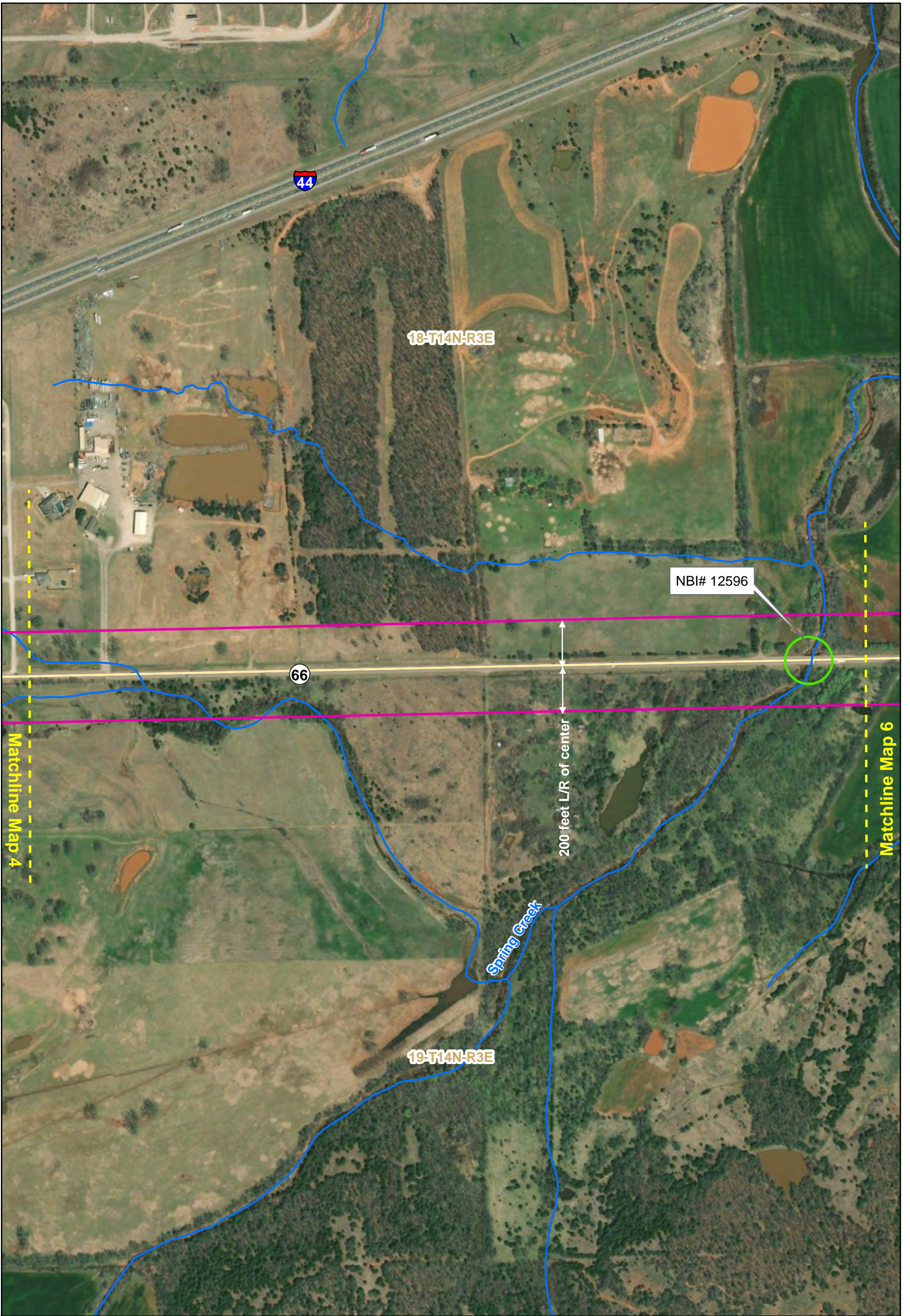
JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes 1.1 mile turnpike project exception)
 Lincoln County, OK

OKLAHOMA Transportation

DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000, USGS

Figure 2d

Date: 1/10/2022



COMMENTS/LEGEND	
	NEPA Environmental Footprint
	NBI Bridges
	Mapped Streams
	Project Exception
	Sections
	Matchlines

NEPA Footprint Map

JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes 1.1 mile turnpike project exception)
 Lincoln County, OK

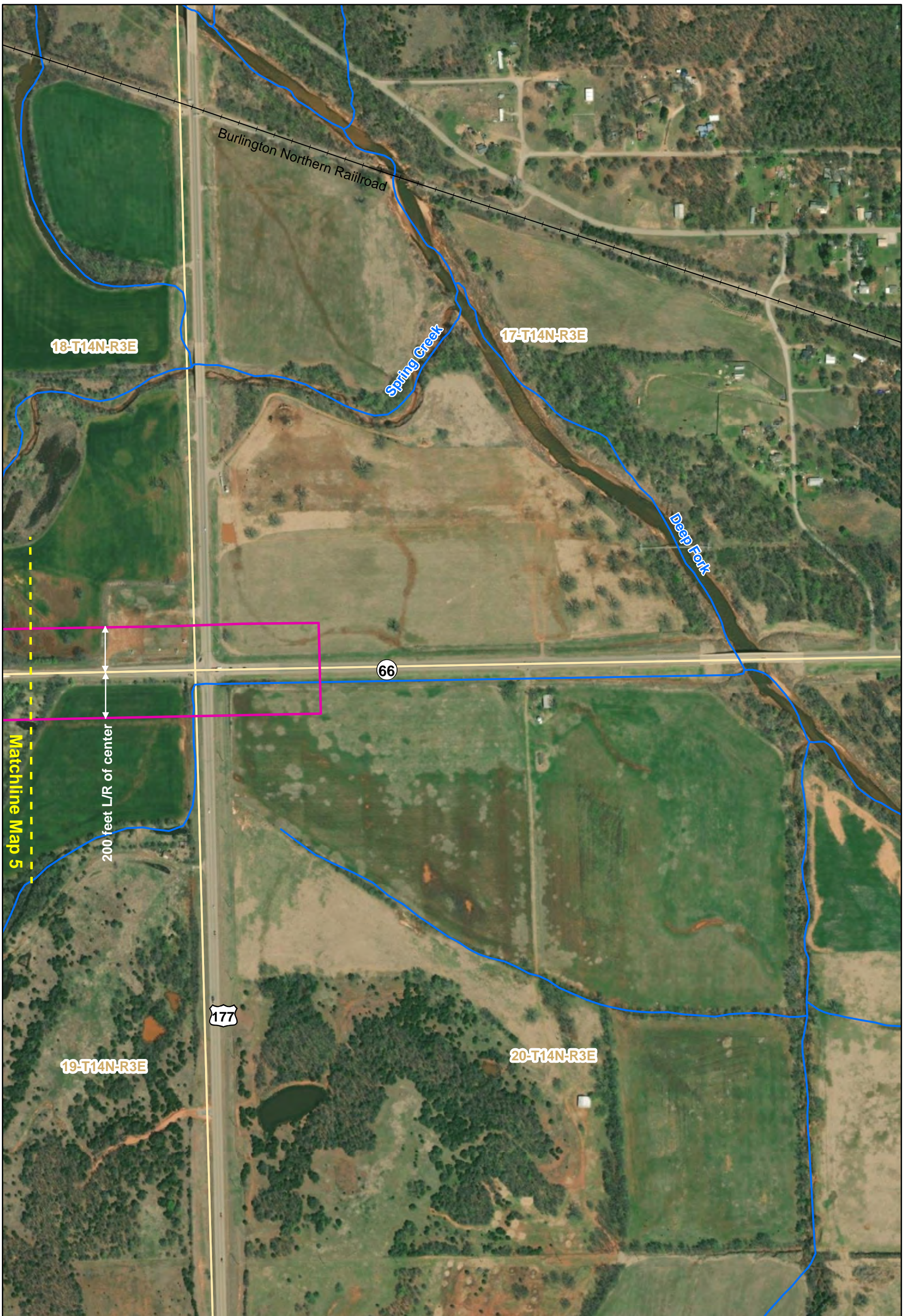
OKLAHOMA Transportation








DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000, USGS

Figure 2e

Date: 1/10/2022





COMMENTS/LEGEND	
	NEPA Environmental Footprint
	NBI Bridges
	Mapped Streams
	Project Exception
	Sections
	Matchlines
 0 100 200 400 600 800 Feet	

NEPA Footprint Map

JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66 from
 SH-102, east to US-177 (includes 1.1 mile
 turnpike project exception)
 Lincoln County, OK



DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

**Figure
2f**

Date: 1/10/2022

**EARLY COORDINATION LETTERS
AND RESPONSES**



January 13, 2022

**Subject: Widen, Resurface & Bridge SH-66: From SH-102, east to US-177
Lincoln County, JP #34318(04), Project #J3-4318(004)**

Dear Property Owner:

The Oklahoma Department of Transportation (ODOT), in cooperation with the Federal Highway Administration (FHWA), will be preparing an environmental document on a proposal to widen SH-66 and improve the bridges over Captain Creek and Spring Creek beginning at SH-102 and extending east to US-177. The project is scheduled for construction in 2029 in the current 8 Year Construction Program and ODOT is early in the project development process. The exact project scope and requirements will be clarified through the planning, environmental review, and design process. Issues that will be analyzed in the document include the project’s effects to noise, water quality, cultural and natural resources, and other effects to the environment. In accordance with the National Environmental Policy Act, the National Historic Preservation Act, and FHWA policy, ODOT requests any information or specific concerns you have regarding this project’s potential impact on the resources listed above. If you have reason to believe that there are resources such as underground oil or gas storage tanks, contaminated soil, archaeological or historic sites, human graves, places of religious or cultural importance to Native American tribes, or other sensitive resources, please respond to the contact provided below.

In accordance with Oklahoma Statute 69-702, employees or authorized agents of ODOT may enter your property for the purpose of surveying for the environmental considerations listed above. A copy of Oklahoma Statute 69-702 is provided with this letter. The results of the studies for cultural resources, biological resources, noise, and hazardous materials will be incorporated into the environmental document being prepared for this project. Minor hand digging on your property may be necessary as part of the survey. Any test holes will be filled in and cleaned up afterwards.

If you are currently leasing this property, please notify your lessee of our planned work.

Should you have any information or specific concerns, or if you have resources listed above that may be located on your property, please contact our authorized agent Geoff Canty with CC Environmental at (405) 761-1225 or geoff@ccenviro.net. If your concerns are related to places of traditional cultural or religious importance to Native American tribes or to burials or cemeteries affiliated with tribes, please contact Dr. Rhonda Fair, ODOT Director of Tribal Coordination, at (405) 517-5670 or rfair@odot.org. As always, your input is greatly appreciated.

Respectfully,

Joe Brutsché
Environmental Programs Interim Division Manager

JB/AA/CC Environmental

Enclosures: Location Map, Copy of Statute 69-702

Copy to:	Project Management	Field District Engineer	Survey Division
	Materials Division	Right-of-Way Division	Tribal Coordination
	Specialists		

“The mission of the Oklahoma Department of Transportation is to provide a safe, economical, and effective transportation network for the people, commerce and communities of Oklahoma.”

Lincoln County – JP #34318(04) Property Owners

FIRST NAME	LAST NAME	ADDRESS 1	CITY	STATE	ZIP
MARVIN BRUCE	MULLEN, ET AL	PO BOX 3596	PASCO	WA	99302
CHARLES DAVID BOUSKA, JR. &	WILMA L. BOUSKA	336562 E COMMERCIAL ST	CHANDLER	OK	74834
MICHAEL D.	SNYDER	PO BOX 15	WELLSTON	OK	74881
CLIFF RAY	KINCAID	331725 E HWY 66	WELLSTON	OK	74881
ERIC S. & ANGELA N.	BOZEMAN	332245 E HWY 66B	WELLSTON	OK	74881
WILLIAM GRANT & DANA LEA	GREEN	940413 S 3330 RD jt	WELLSTON	OK	74881
LAVERA F	MELROSE, TRUSTEE	920285 S 3330 RD	WELLSTON	OK	74881
KELLY	RALSTIN	10413 CRICKET CT	OKLAHOMA CITY	OK	73162
PAUL E. & STACIE L.	WHITNAH	PO BOX 464	WELLSTON	OK	74881
JOHN CURTIS & ALFRED JOSEPH	HANCOCK	PO BOX 517	WELLSTON	OK	74881
THOMAS W. & TONYA	COTTER	511 W HWY 66	WELLSTON	OK	74881
WELLSTON INDEPENDENT	SCHOOL DISTRICT	PO BOX 60	WELLSTON	OK	74881
OKLAHOMA TURNPIKE AUTHORITY	C/O ASSET MANAGER	PO BOX 11357	OKLAHOMA CITY	OK	73136
PAUL D. & SUSAN M.	GORDON	402 W HWY 66	WELLSTON	OK	74881
JERRY A. & MARY L.	HAGAR	PO BOX 506	WELLSTON	OK	74881
MARY ANGELA TUCKER	THOMAS	PO BOX 581	WELLSTON	OK	74881
WOMACK FAMILY	REVOCABLE TRUST	1775 S POTTAWATOMIE RD	WELLSTON	OK	74881
GREEN'S PROPANE-RE	HOLDINGS, LLC	1550 LARIMER ST, STE 118	DENVER	CO	80202
IVIE FAMILY TRUST	SHIRLEY IVIE, TRUSTEE	209 HAYES DR	WELLSTON	OK	74881
ALVIN & CAROL	IHRIG	950309 S 3350 RD	WELLSTON	OK	74881
OLIVER LEO & CAROLYN A.	BECKER	PO BOX 305	WELLSTON	OK	74881
JAMES R.	JACKSON, JR.	PO BOX 783	WELLSTON	OK	74881
JASON KENT & JAMIE BARNES	SAXON	PO BOX 585	WELLSTON	OK	74881
MARK & SARA	POSKEY	PO BOX 589	STOUD	OK	74079
JANA	DANKER	402 E HWY 66	WELLSTON	OK	74881
DAVID D. & LISA G.	BESS	PO BOX 119	WELLSTON	OK	74881
DAVID & LISA	BESS	332061 E HWY 66	WELLSTON	OK	74881
NICHOLAS HUNTER & SHANNON M.	BESS	PO BOX 134	WELLSTON	OK	74881
SY WELLSTON	INVESTMENTS, LLC	830 SHELDON RD	LANTANA	TX	76226
PERKINS UNLIMITED	PARTNERSHIP	916 W MAIN	STILLWATER	OK	74075
WELLSTON MISSIONARY	BAPTIST CHURCH	902 HAYES DR WELLSTON	WELLSTON	OK	74881

Lincoln County – JP #34318(04) Property Owners

J&D ✓	FABRICATORS, LLC	334760 E HWY 66	WELLSTON	OK	74881
CCA ✓	LLC	PO BOX 130	WELLSTON	OK	74881
LOREN DALE & MARY K.	McCORKLE ✓	920209 S 3350 RD	WELLSTON	OK	74881
LAWRENCE DON & VICKI I.	CHESSER ✓	334888 E HWY 66 JT	WELLSTON	OK	74881
BRYCE	PAYNE ✓	PO BOX 529	WELLSTON	OK	74881
CHARLES J.	GARRISON ✓	PO BOX 373	WELLSTON	OK	74881
ANGELA DEE	WALLACE ✓	910892 S 3350 RD	WELLSTON	OK	74881
ANDREW JERRY & JOYCE ELAINE	LACKEY ✓	910883 GERDES DR	WELLSTON	OK	74881
ANDREW THOMAS	TILLISON, JR. ✓	PO BOX 11	WELLSTON	OK	74881
STEPHEN SCOTT & HEATHER LEAH	WEBB ✓	910918 S GERDES DR	WELLSTON	OK	74881
TYSON A.	CHRISTY ✓	10390 WEBSTER RD.	OKMULGEE	OK	74447
JANICE	SUDHEIMER ✓	PO BOX 537	WELLSTON	OK	74881
TROY J. & TRACEY	SWAFFORD ✓	336108 E 970 RD	WELLSTON	OK	74881
STATE OF OKLAHOMA	COMMISSIONERS OF LAND OFFICE ✓	PO BOX 26910	OKLAHOMA CITY	OK	73126
MARCY SERVICE	CENTER, LLC (NOW DAVID & LISA BESS)	OK			
DONNA	HANCOCK (NOW JOHN CURTIS & ALFRED JOSEPH HANCOCK)	OK			
RONDLE L. & TAMARA S.	HUDSON (NOW PAUL & STACIE WHITNAH)	OK			
MELROSE, RC	PROPERTIES, LLC (NOW PAUL & STACIE WHITNAH)	OK			
JACKSON ELECTRICAL	CONSTR. LLC (NOW PAUL & STACIE WHITNAH)	OK			
CURTIS & STACY	WARTCHOW (NOW CHARLES J. GARRISON)	OK			
TJC,	LLC				
GEORGE L.	DENWALT, III				
MARVIN C. & DEANNA K. EVANS	FAMILY TRUST				
TODD & ANGELA	LEHMAN				
WALLAHACHIE	LLC				
SCOTT FAMILY TRUST	BILLIE M. SCOTT, TRUSTEE				
BARBARA EVANS	REVOCABLE TRUST				

DID NOT MAIL OK

=From Recon Report.

=From Recon Report, but outside NEPA Study Area.

1/13/2022
 [Signature]
 BA, BLUE & WELLSTON



United States Department of the Interior

BUREAU OF INDIAN AFFAIRS

Southern Plains Regional Office

P.O. Box 368

Anadarko, OK 73005

FEB 16 2022

Mr. Joe Brutsche
Environmental Programs Division Manager
Environmental Programs Division, ODOT
200 NE 21st Street
Oklahoma City, Oklahoma 73105-3204



Dear Mr. Brutsche:

Thank you for the opportunity to comment on the Oklahoma Department of Transportation (ODOT) Widen, Resurface & Bridge SH-66 project near the town of Warwick and its effects to Native American sites and/or landmarks. The project will improve two bridges over Captain Creek and Spring Creek and the overall safety of Sh-66 (the project is still in the planning process). The proposed project begins at SH-102 and extends east to US-177, and is located in Section 15, Township 14 North, Range 2 East in Lincoln County, Oklahoma.

A review of the Bureau of Indian Affairs (BIA) project location maps indicates that there are no Southern Plains Region tribal or Individual Indian trust lands within the project area. The BIA has no concerns that the proposed projects will impact Indian trust lands within the Southern Plains Region jurisdiction; however, we request that ODOT consult with federally recognized tribes on activities that might affect properties to which tribes attach religious and cultural significance.

If any additional information is required, please contact Kate Moore, Regional Archeologist, at 405-247-1565.

Sincerely,

Regional Director



January 13, 2022

Mr. James Schock
Regional Director
Southern Plains Regional Office
Bureau of Indian Affairs
P.O. Box 368
Anadarko, Oklahoma 73005

**Subject: Widen, Resurface & Bridge SH-66: From SH-102, east to US-177
Lincoln County, JP #34318(04), Project #J3-4318(004)**

Dear Mr. Schock:

The Oklahoma Department of Transportation (ODOT), in cooperation with the Federal Highway Administration (FHWA), will be preparing an environmental document on a proposal to widen SH-66 and improve the bridges over Captain Creek and Spring Creek beginning at SH-102 and extending east to US-177. The project is scheduled for 2029 in the current 8 Year Construction Program and ODOT is early in the project development process. The exact project scope and requirements will be clarified through the planning, environmental review, and design process; however, the improvements are considered necessary to improve the condition and safety of SH-66. Issues that will be analyzed in the document include the project's effects to noise, water quality, cultural and natural resources, and other effects to the environment. In accordance with the National Environmental Policy Act, the National Historic Preservation Act, and FHWA policy, ODOT requests any information or specific concerns you have regarding this project's potential impact on the resources listed above. We have enclosed a location map and the environmental study area.

This project is in the early developmental stages and any comments relative to the social, economic, or environmental effects of this proposal will be appreciated. To allow adequate time for evaluation of your comments, we would appreciate receiving a response within **fifteen days** from the date of this letter. Your written comments should be directed to the Environmental Program Division Engineer, Oklahoma Department of Transportation, 200 NE 21st Street, Oklahoma City, Oklahoma 73105 or emailed to environment@odot.org.

We sincerely appreciate your input in this matter. For further information or if you have any questions, please contact our authorized agent Geoff Canty with CC Environmental at (405) 761-1225 or geoff@ccenviro.net or Amanda Alexander, Environmental Project Manager at (405) 521-2312 or aalexander@odot.org.

Respectfully,

Joe Brutsché
Environmental Programs Interim Division Manager

JB/AA/CC Environmental

Enclosures: Location Map, Study Area Map

Copy to: Project Management Division
Field District Engineer
Tribal Liaison

Right-of-Way Division
ODOT Cultural Resources

January 13, 2022

Ms. Carolyn Russell
Assistant Field Manager – Resources
Oklahoma Field Office
Bureau of Land Management
201 Stephenson Parkway, Suite 1200
Norman, Oklahoma 73072-2037

**Subject: Widen, Resurface & Bridge SH-66: From SH-102, east to US-177
Lincoln County, JP #34318(04), Project #J3-4318(004)**

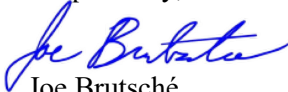
Dear Ms. Russell:

The Oklahoma Department of Transportation (ODOT), in cooperation with the Federal Highway Administration (FHWA), will be preparing an environmental document on a proposal to widen SH-66 and improve the bridges over Captain Creek and Spring Creek beginning at SH-102 and extending east to US-177. The project is scheduled for 2029 in the current 8 Year Construction Program and ODOT is early in the project development process. The exact project scope and requirements will be clarified through the planning, environmental review, and design process; however, the improvements are considered necessary to improve the condition and safety of SH-66. Issues that will be analyzed in the document include the project's effects to noise, water quality, cultural and natural resources, and other effects to the environment. In accordance with the National Environmental Policy Act, the National Historic Preservation Act, and FHWA policy, ODOT requests any information or specific concerns you have regarding this project's potential impact on the resources listed above. We have enclosed a location map and the environmental study area.

This project is in the early developmental stages and any comments relative to the social, economic, or environmental effects of this proposal will be appreciated. To allow adequate time for evaluation of your comments, we would appreciate receiving a response within **fifteen days** from the date of this letter. Your written comments should be directed to the Environmental Program Division Engineer, Oklahoma Department of Transportation, 200 NE 21st Street, Oklahoma City, Oklahoma 73105 or emailed to environment@odot.org.

We sincerely appreciate your input in this matter. For further information or if you have any questions, please contact our authorized agent Geoff Canty with CC Environmental at (405) 761-1225 or geoff@ccenviro.net or Amanda Alexander, Environmental Project Manager at (405) 521-2312 or aalexander@odot.org.

Respectfully,



Joe Brutsché
Environmental Programs Interim Division Manager

JB/AA/CC Environmental
Enclosures: Location Map, Study Area Map

Copy to: Project Management Division
Field District Engineer

Right-of-Way Division
ODOT Cultural Resources

January 13, 2022

The Honorable Paul E. Whitnah
Mayor of Wellston
Wellston City Hall
211 Cedar St.
Wellston, OK 74881

**Subject: Widen, Resurface & Bridge SH-66: From SH-102, east to US-177
Lincoln County, JP #34318(04), Project #J3-4318(004)**

Dear Mayor Whitnah:

The Oklahoma Department of Transportation (ODOT) is considering a proposal to widen SH-66 and improve the bridges over Captain Creek and Spring Creek beginning at SH-102 and extending east to US-177. The project is scheduled for 2029 in the current 8 Year Construction Program and ODOT is early in the project development process. The exact project scope and requirements will be clarified through the planning, environmental review, and design process.

At this time, we are interested in obtaining your input regarding your community's local priorities for ODOT to consider. These priorities may relate to construction timing, social, economic, and environmental impacts, or other concerns this project may have on your community. Your active participation in the project development process is essential to help ensure your concerns are considered while at the same time addressing broader state and national needs. In addition, we are also interested in finding out if this improvement might affect any historic sites or publicly owned parks or recreation areas. Please submit your input by mail or by email to environment@odot.org.

Your participation in this process will also allow you to fully understand any local financial obligations which may be associated with this project, potentially including utility relocation, removal of structures currently encroaching on highway right-of-way within your city limits, and possible future maintenance of the completed facility. As the exact project scope and requirements are clarified through the environmental review and design process, our Right of Way Division will be contacting you with further details. If you have any questions specific to right-of-way or utilities, please contact Mr. Robert Blackwell, Chief of Right-of-Way at (405) 521-2661 or rblackwell@odot.org.

Should you have any questions please contact our authorized agent Geoff Canty with CC Environmental at (405) 761-1225 or geoff@ccenviro.net. As always, your input is greatly appreciated.

Respectfully,



Joe Brutsché
Environmental Programs Interim Division Manager

JB/AA/CC Environmental
Enclosures: Location Map

Copy to: Project Management Division, Field District Engineer, Right-of-Way Division

"The mission of the Oklahoma Department of Transportation is to provide a safe, economical, and effective transportation network for the people, commerce and communities of Oklahoma."

**RELOCATIONS
AND
ENVIRONMENTAL JUSTICE**

Oklahoma Department of Transportation – Right-of-Way Division
Relocation Branch Room C7 Third Floor Office 521-2648 Fax 522-1858

August 15, 2023

To: **Environmental Programs Division**

Thru: Project Management Branch

From: Diana Barlow, Manager, Relocation Branch

Diana Barlow

Subject: 60% Relocation Plan – 34318(04), a.k.a. J3-4318(004), Lincoln Co.,
SH-66 From SH-102, East to US-177 (1.1 Mile Turnpike Exception Included).

Attached is the Relocation Plan for the above referenced project to be included in the Environmental Document.

60% RELOCATION PLAN

JP 34318(04) Lincoln County

This Pre-planning information is provided to the Environmental Programs Division to be included in the Environmental Document to satisfy Pre-planning requirements of the Federal Regulations 49 CFRi, § 24.205, relocation planning, advisory services and coordination.

The proposed study was done based on 60% Preliminary Right of Way Construction Plans dated 6/6/2023, for SH-66 From SH-102, East to US-177 (1.1 Mile Turnpike Exception Included).

A pre-planning drive out was conducted on August 3, 2023. One Residential Relocation was determined to be present and would be affected by the proposed grade, drain, and surface.

The “Relo Plan Inventory” table lists sites affected by the proposed Right of Way. Right of Way Construction Plans, dated 6/6/2023, have been “Marked-Up” labeling the sites inventoried.

Relo Inventory 30% Plans		
JP 34318(04) Lincoln Co.,		
SH-66 From SH-102, East to US-177		
(1.1 Mile Turnpike Exception Included)		
ALL MEASURES FROM CL SH-66		
8/3/2023		
Location	Station	DESCRIPTION
A	1745+00L	RESIDENTIAL RELO #1: 2000 SF Brick House
B	1794+00L	PORTABLE SHEDS "COST-TO-CURE"; NO RELO

Residential Relocations:

At the time of scoping, there is: 1-single family residences being affected. There were a small number of available properties for sale in the area. The local Realtorⁱⁱ solicited for help with this Relocation Plan in the Wellston area, is listed in “Relo Plan Resources Summary” (RPRS). Multiple Listing Service (MLS) is available. Web base realtor servicesⁱⁱⁱ supplied the most listings of available properties in the Wellston area (with some duplication of listings). The web-based realtor services utilized are listed in the RPRS. Replacement property searches were made for a minimum of: 2 and 3 bedroom; 1 and 2 bathroom houses; 2000 square feet. The search radius was 20 to 30 miles from Wellston. It is not known if the house is occupied by an Owner or Tenant. A search was also conducted for available rental property in the Wellston, Ok. Area.

60% RELOCATION PLAN

JP 34318(04) Lincoln County

Available Residential Replacement Properties:

1456 SF; 121 Key Dr., Wellston, Ok. 74881, 3 bed, 2 bath, 1819 SF; 960461 S. Hwy 102, Wellston, Ok. 74881, 4 bed, 3 bath, 2762 SF; 930804 S. Hwy 177, Wellston, Ok. 74881; 121 Key Dr., Wellston, Ok. 74881, 3 bed, 2 bath, 1819 SF; 100 E. 1st St., Wellston, Ok. 74881, 4 bed, 3 bath, 2617 SF.

Available Rental Properties:

There were limited available “rental” properties listed at the time of scoping: 330533 S. Hwy 66, Wellston, Ok.; 540 Dardanelle Pass, Edmond, Ok. 73025, 4 bed, 2.5 bath, 2576 SF.

At the time of this report, there were no businesses with employees being impacted.

The proposed alignment on the project does not have any adverse impacts on minority or low-income persons.

The CFR 49, Part 24, limits a payment not to exceed \$7,200.00 for rental assistance (RAP) or down payment assistance; and homeowner-occupant (RHP) payment may not exceed \$31,000.00. Last Resort Housing (LRH) allows for these amounts to be exceeded and will most likely be necessary to relocate persons affected by the proposed plans.

Solutions: None needed at this time.

Relocation Plan prepared by:
Nicholas Granko, Relocation Agent
ODOT Right of Way and Utilities Division
Relocation Branch
8/15/2023

Relocation Plan Resources Summary:

i Relocation Acronyms

SF = Square Feet
CFR = Code of Federal Regulations
FTE = Full Time Employees
PTE = Part Time Employees
DSS = Decent Safe and Sanitary
LRH = Last Resort Housing
MLS = Multiple Listing Service
RAP = Rental Assistance Payment
RHP = Relocation Housing Payment

ii Realtor solicited in the Wellston Ok. 74881 zip code area:

RE / Max Realty Plus, 906 E First St, Chandler, OK 74834

60% RELOCATION PLAN

JP 34318(04) Lincoln County

Amy Miller: salesbyamy@gmail.com (405) 240-6168 cell

iii **Web based Realtor sites:**

Realtor.com

Zillow.com

Trulia.com

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED
STATE HIGHWAY

FEDERAL AID PROJECT NO. XXXXXXXXXXXXXXXX
WIDEN, RE-SURFACE, AND BRIDGE

SH-66

LINCOLN COUNTY

CONTROL SECTION NO. 66-41-06

STATE JOB NO. 34318(04)

BRIDGE "A" LOCATION NO. 4106 0372X

EXISTING NBI NO. 26639

BRIDGE "B" LOCATION NO. 4106 0702X

EXISTING NBI NO. 12596

R/W Color Key

- Blue = Present R/W
- Red = Proposed New R/W
- Yellow = Utility Easement
- Green = Construction Temporary Easement
- Orange = Channel Easement

"Marked-Up" Relo Plan Plans
By ODOT Planning Agent: Nicholas Granko
8/15/2023

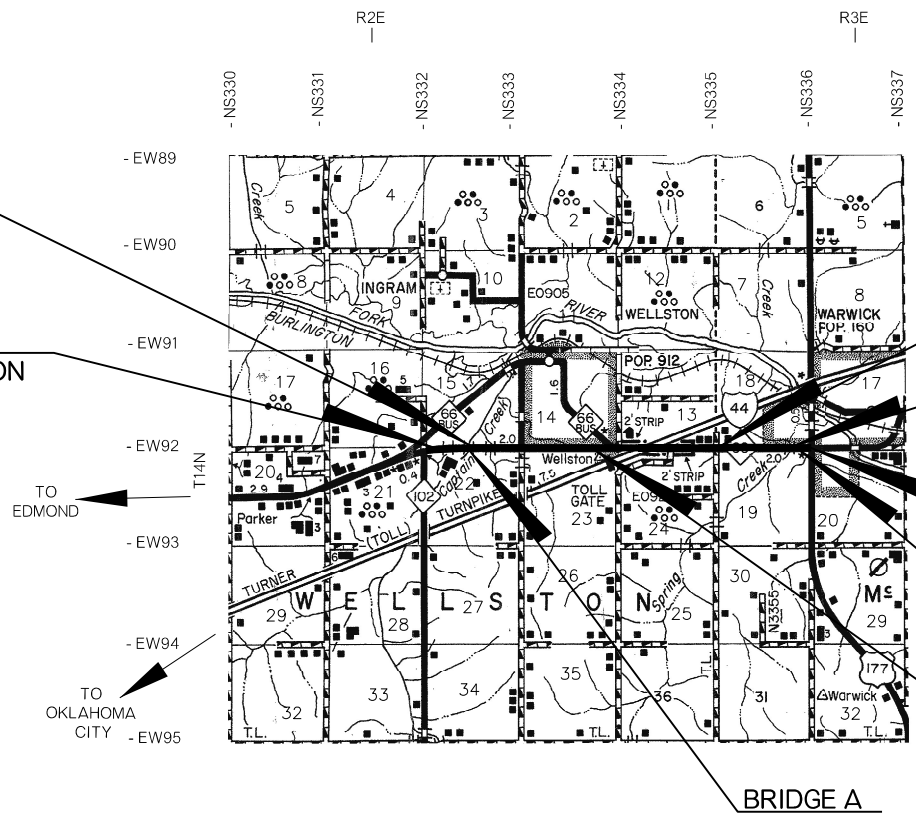
RELO INVENTORY ON SHEET R014

STA. 1622+10.00
END INCIDENTAL CONSTRUCTION
BEGIN PROJECT

STA. 1606+51.45
BEGIN INCIDENTAL CONSTRUCTION

BEGIN STA. 1608+01.45
LENGTH = 335'
END STA. 1611+36.74
BRIDGE "A"

BEGIN STA. 1784+82.11
LENGTH = 43'
END STA. 1785+36.39
BRIDGE "B"



STA. 1731+25.90
END EXCEPTION

TO TULSA
BRIDGE B

TO CHANDLER

STA. 1795+25.62
END INCIDENTAL CONSTRUCTION

STA. 1794+64.58
END PROJECT

STA. 1679+91.49
BEGIN EXCEPTION

THE FOLLOWING ODOT STANDARDS ARE
REQUIRED FOR THIS PROJECT:

ROADWAY BRIDGE TRAFFIC

60% Plans

ROADWAY LENGTH ----- 12,121.21 FT. 2.287 MI.
 BRIDGE LENGTH ----- 43.00 FT. 0.008 MI.
 PROJECT LENGTH ----- 2.295 MI.

EQUATIONS : CRL STA. 1669+00.00 BACK = CL SURVEY STA. = 1668+98.96 AHEAD
 EXCEPTIONS: STA. 1679+91.49 TO STA. 1731+25.90

THIS DRAWING IS PRELIMINARY IN NATURE. IT IS NOT A FINAL SIGNED AND SEALED DRAWING.

PREPARED BY:
CEC CORPORATION
CA32 06/30/24
OKLAHOMA CITY, OKLAHOMA

CEC

ERIC R. ATKINSON
OKLA. REG. NO. 21688

DATE

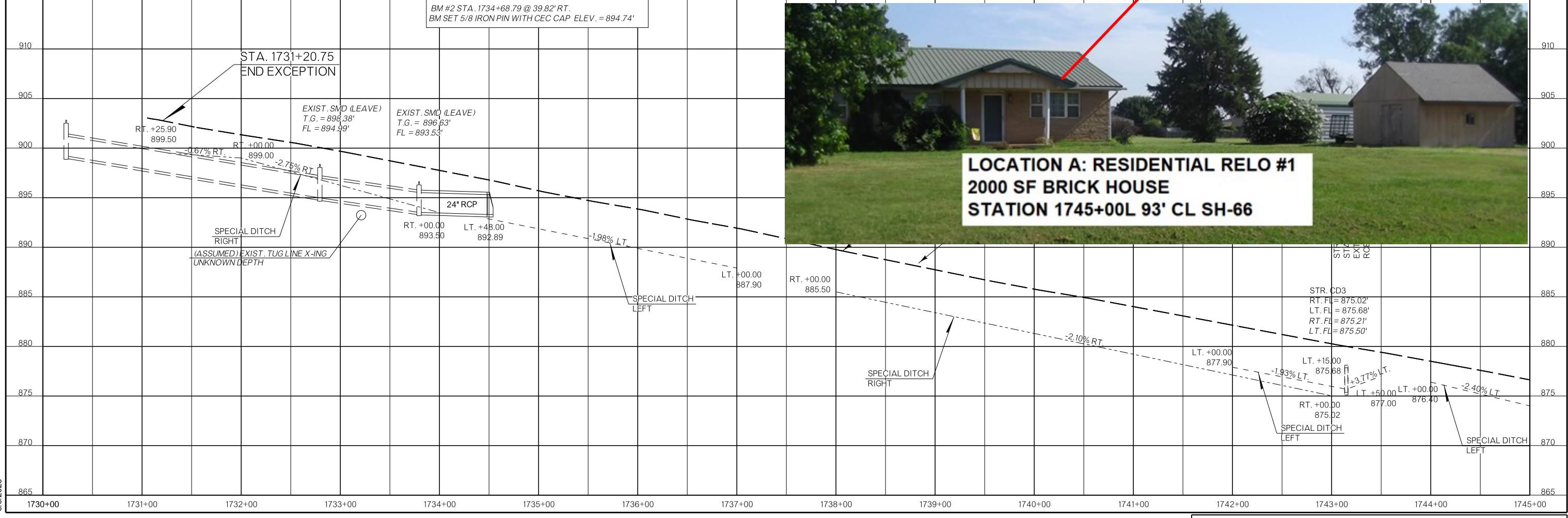
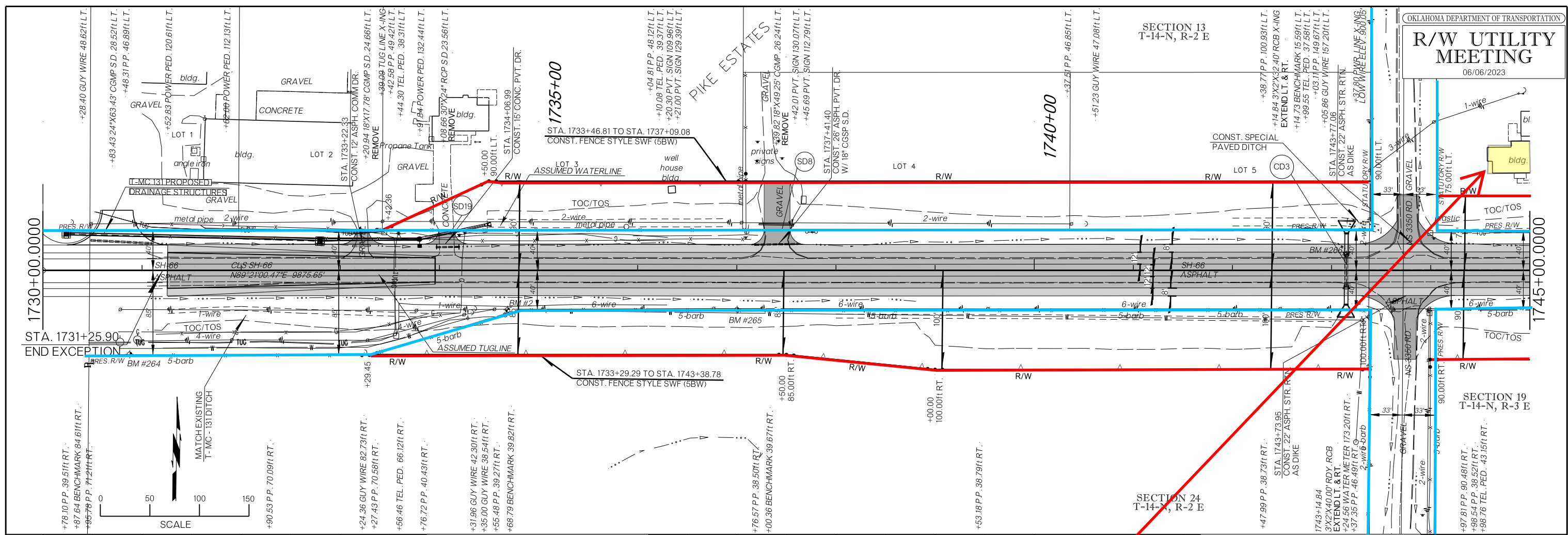
OKLAHOMA DEPARTMENT OF TRANSPORTATION

DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

DATE APPROVED _____
BY _____
CHIEF ENGINEER

DATE APPROVED _____
BY _____
DIVISION ADMINISTRATOR

SWO 5514(1) PROJECT NO. XX-XXXX(XXX)
COUNTY LINCOLN HIGHWAY SH-66 SHEET NO. 0001

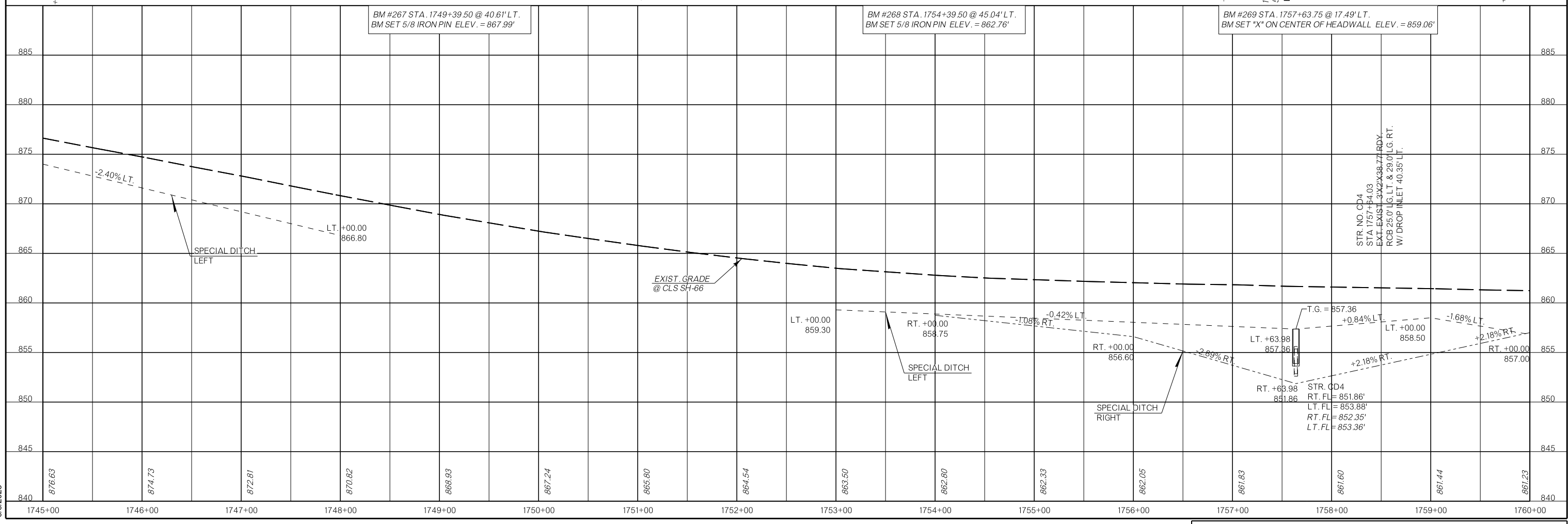
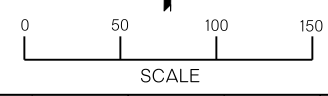
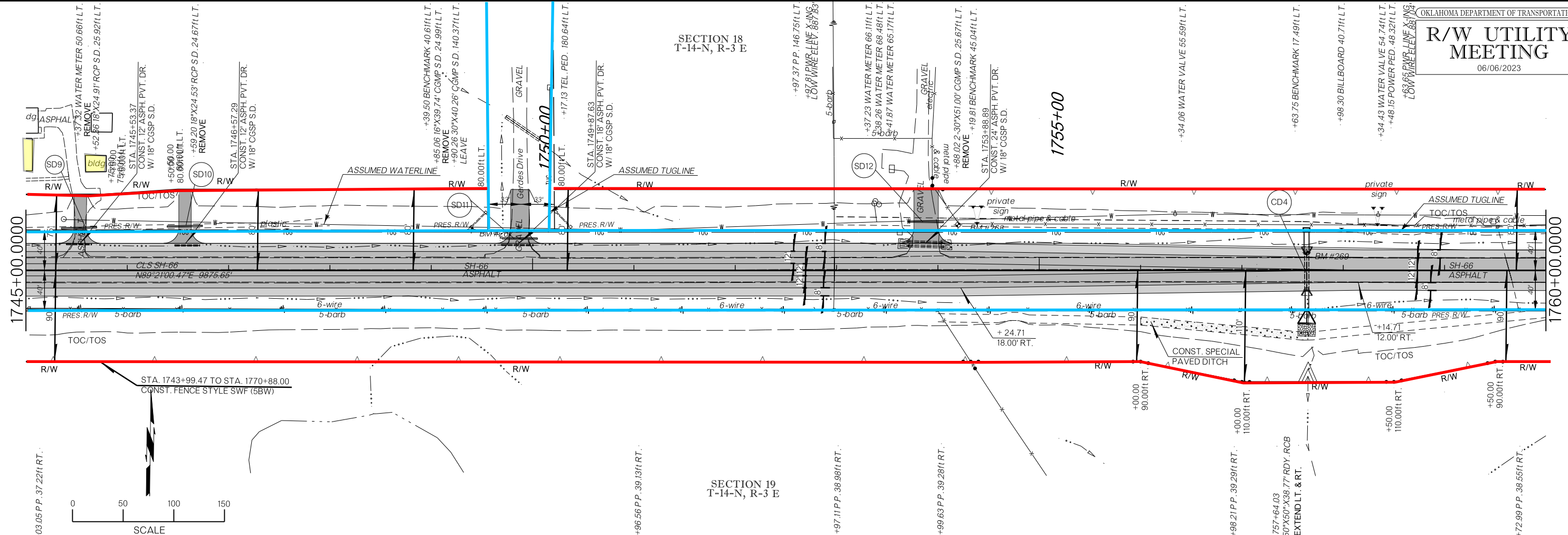


LOCATION A: RESIDENTIAL RELO #1
2000 SF BRICK HOUSE
STATION 1745+00L 93' CL SH-66

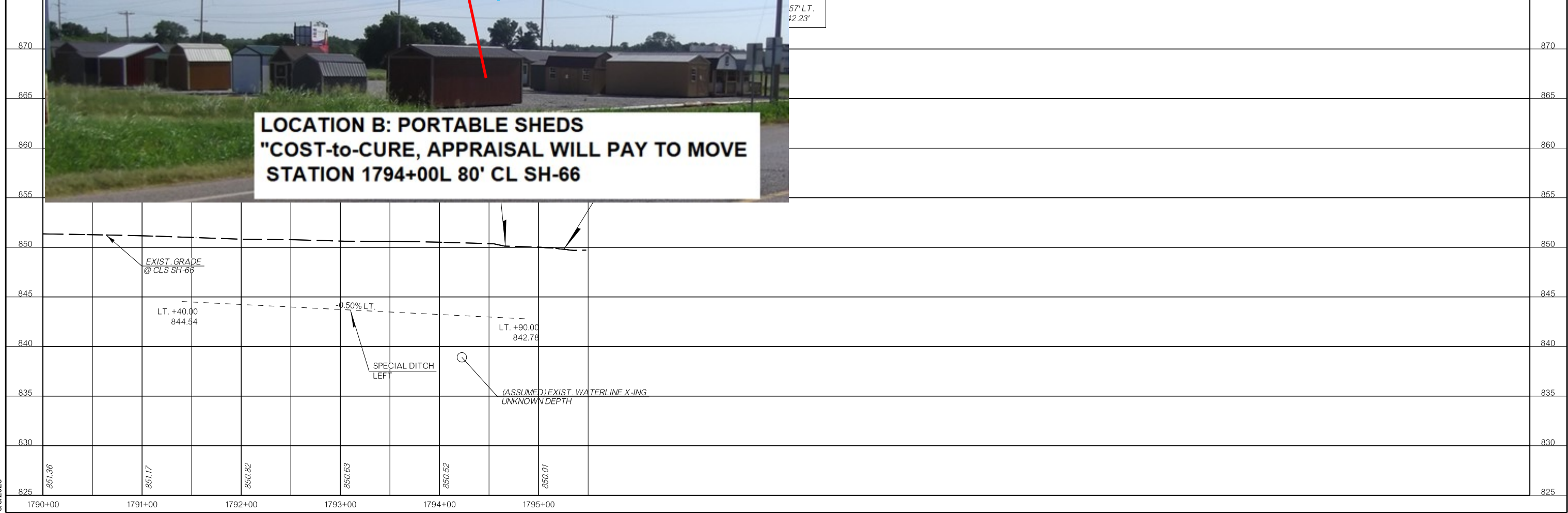
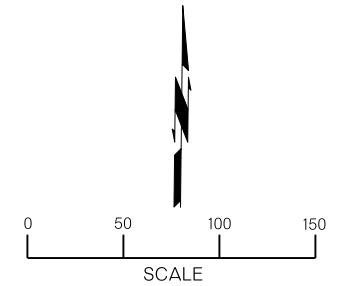
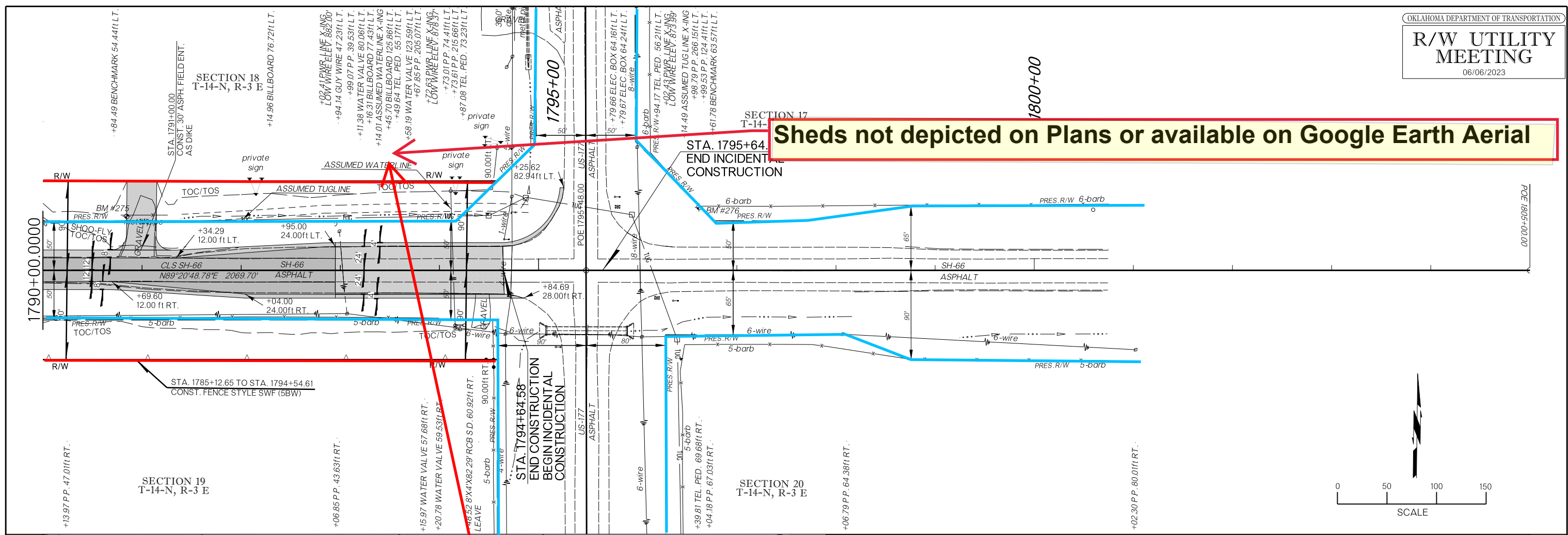
6/6/2023

SECTION 18 T-14-N, R-3 E

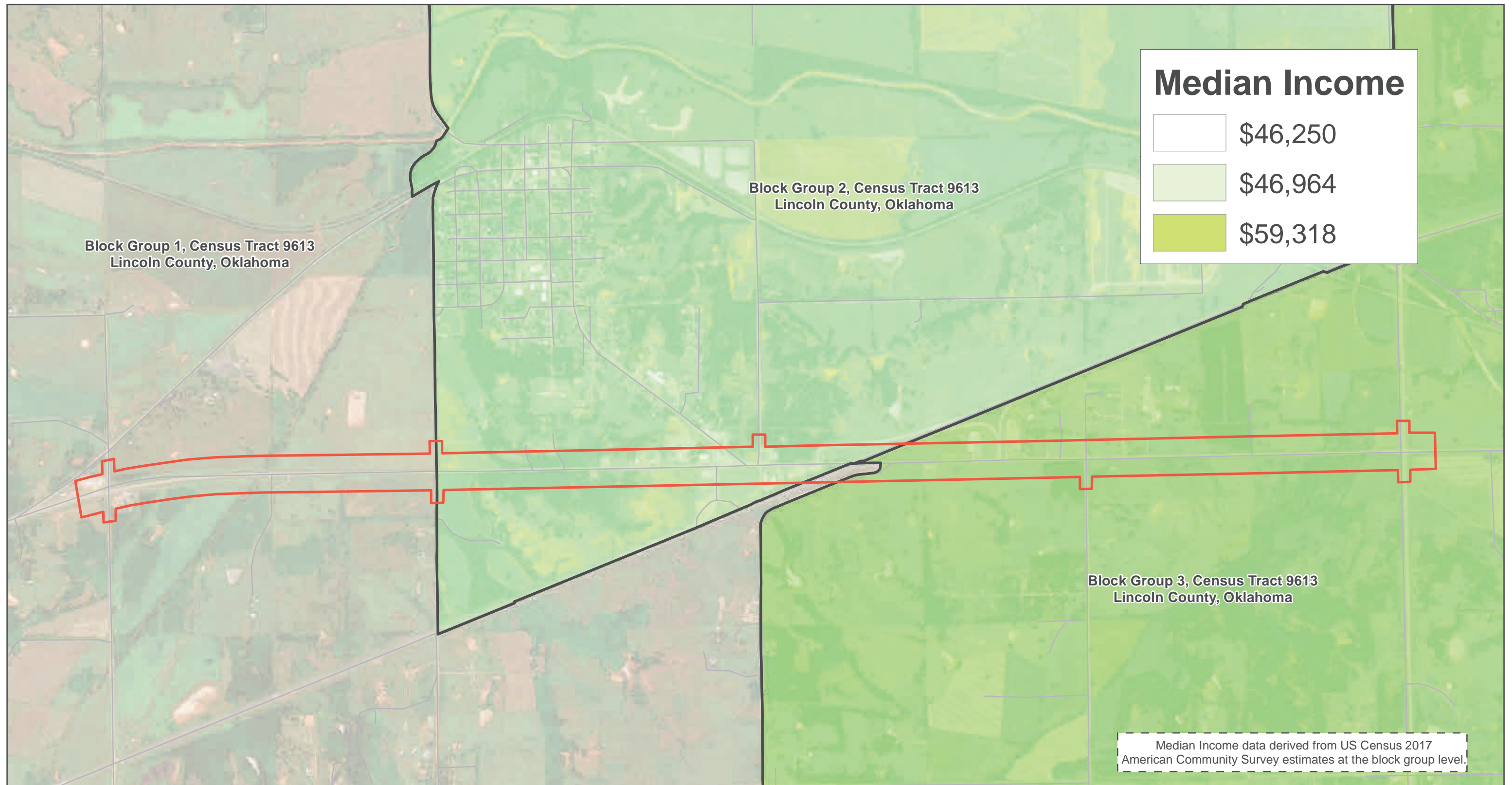
SECTION 19 T-14-N, R-3 E



Sheds not depicted on Plans or available on Google Earth Aerial



6/6/2023



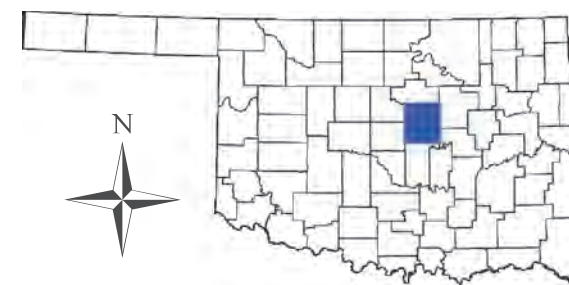
- Recon Footprint
- Block Group Boundaries
- Roads

THE DIAGRAM IS PARTIALLY DIAGRAMATIC;
EXACT DIMENSIONS WILL VARY



Median Income

Widen and Resurface
J/P 34318(04)
SH-66 from SH-102, east to US-177
Lincoln County, OK

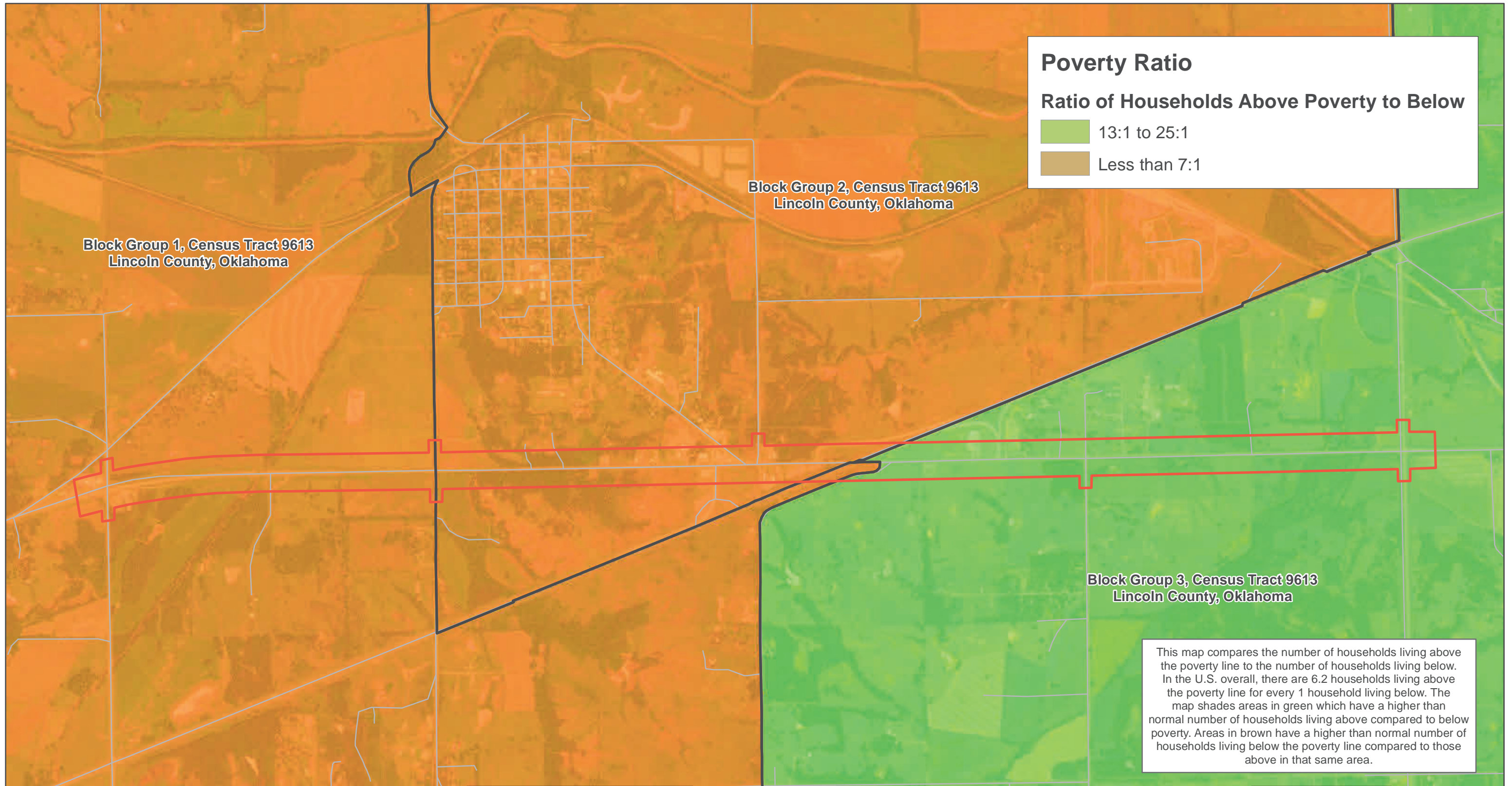


DRAWN BY: LMP
APPRV BY:

SOURCE: DEQ, Tiger 2000,
USGS

**Figure
11-1a**

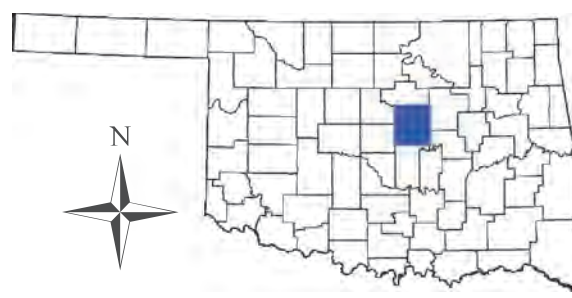
Date: 4/29/2020



- Recon Footprint
- Block Group Boundaries
- Roads

THE DIAGRAM IS PARTIALLY DIAGRAMATIC;
EXACT DIMENSIONS WILL VARY

Poverty Ratio
Widen and Resurface
J/P 34318(04)
SH-66 from SH-102, east to US-177
Lincoln County, OK



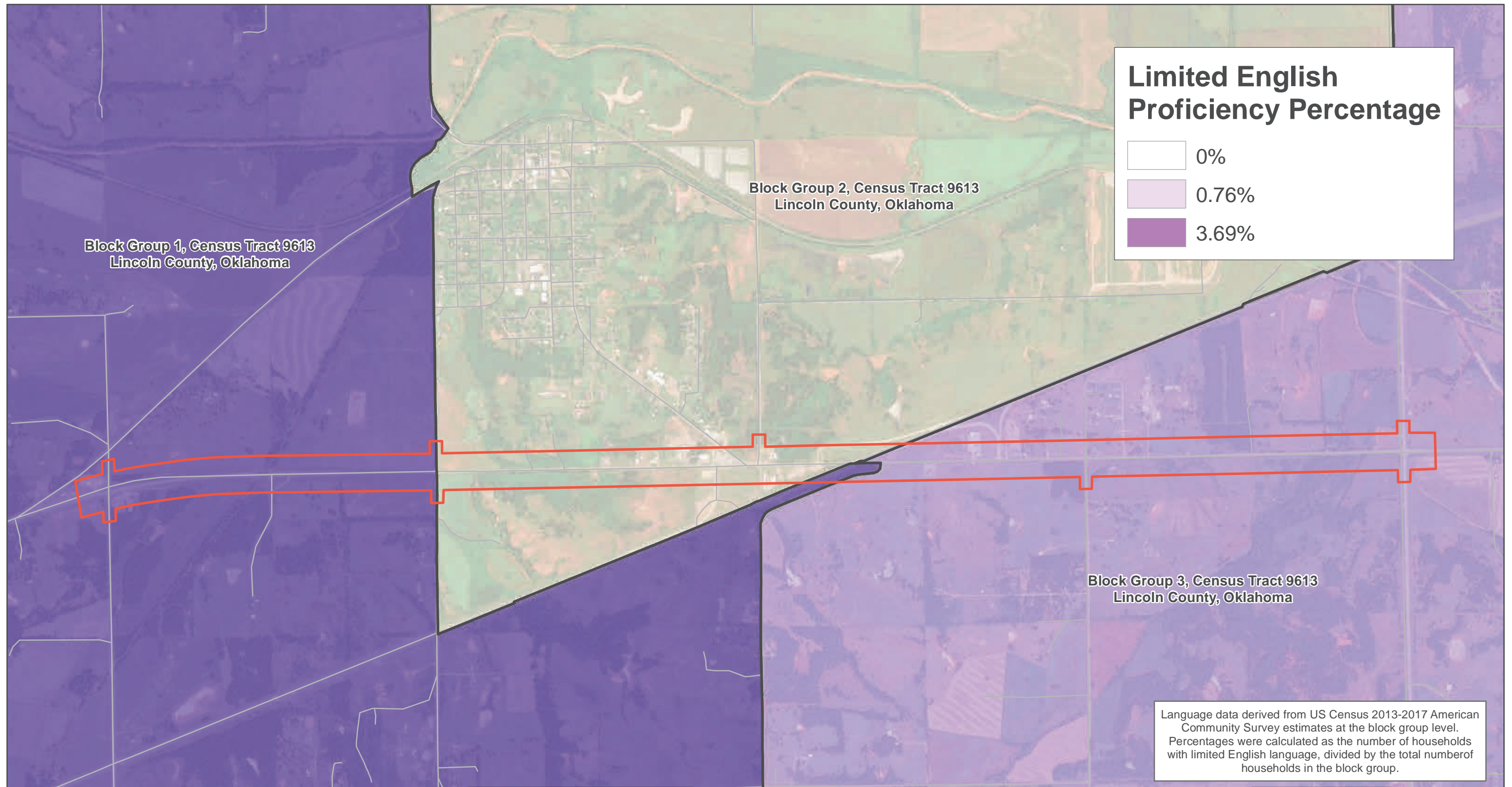
Environmental
SOLUTIONS

DRAWN BY: LMP
APPRV BY:

SOURCE: DEQ, Tiger 2000,
USGS

Figure 11-1b

Date: 4/29/2020



Limited English Proficiency Percentage

- 0%
- 0.76%
- 3.69%

Language data derived from US Census 2013-2017 American Community Survey estimates at the block group level. Percentages were calculated as the number of households with limited English language, divided by the total number of households in the block group.

- Recon Footprint
- Block Group Boundaries
- Roads

THE DIAGRAM IS PARTIALLY DIAGRAMATIC;
EXACT DIMENSIONS WILL VARY

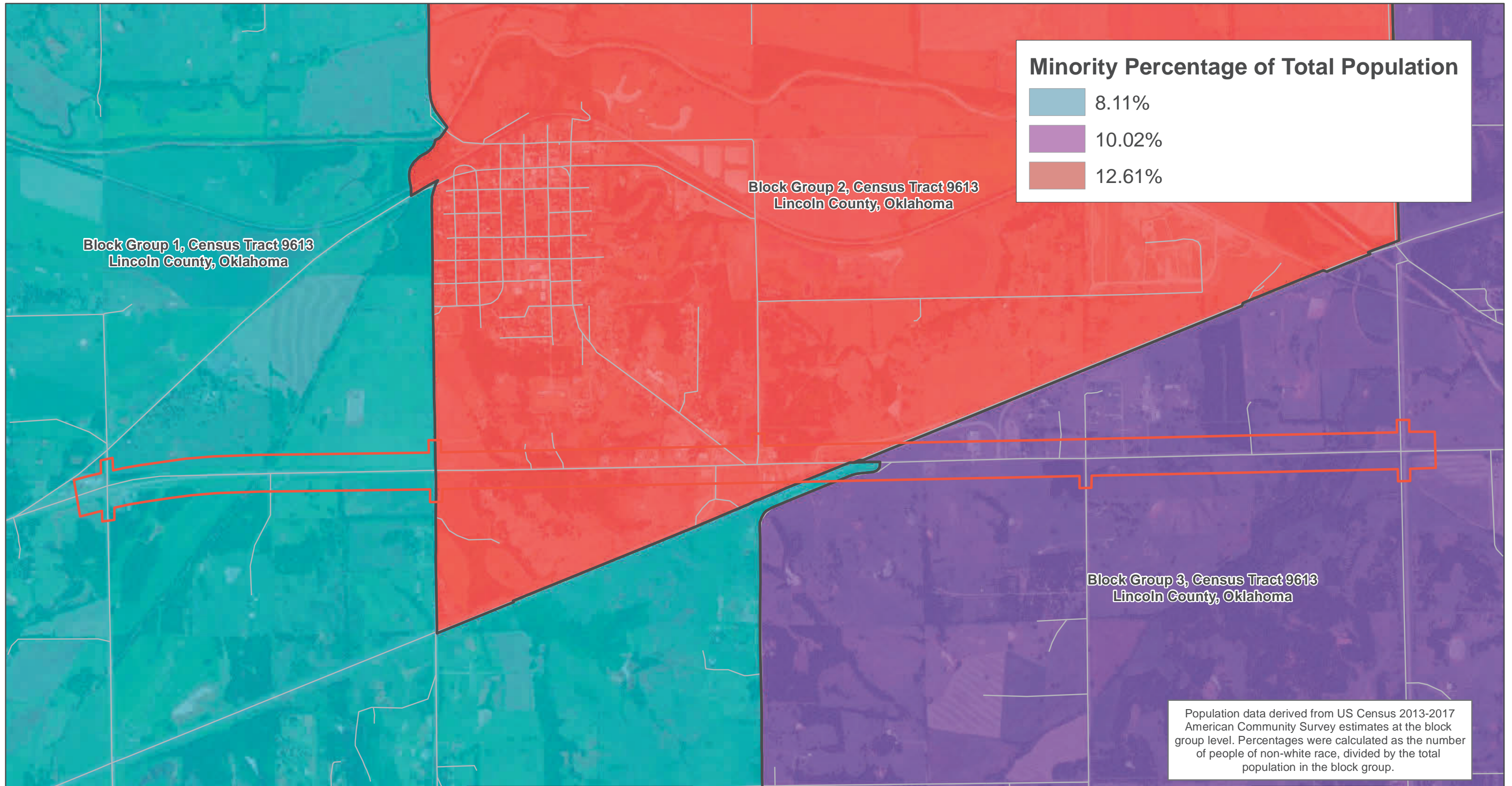
English Proficiency

Widen and Resurface
J/P 34318(04)
SH-66 from SH-102, east to US-177
Lincoln County, OK

DRAWN BY: LMP
APPRV BY:
SOURCE: DEQ, Tiger 2000, USGS

Figure 11-1c

Date: 4/29/2020



Recon Footprint
 Block Group Boundaries
 Roads

THE DIAGRAM IS PARTIALLY DIAGRAMATIC;
EXACT DIMENSIONS WILL VARY

0 365 730 1,460 2,190 2,920 Feet

Minority Population

Widen and Resurface
J/P 34318(04)
SH-66 from SH-102, east to US-177
Lincoln County, OK

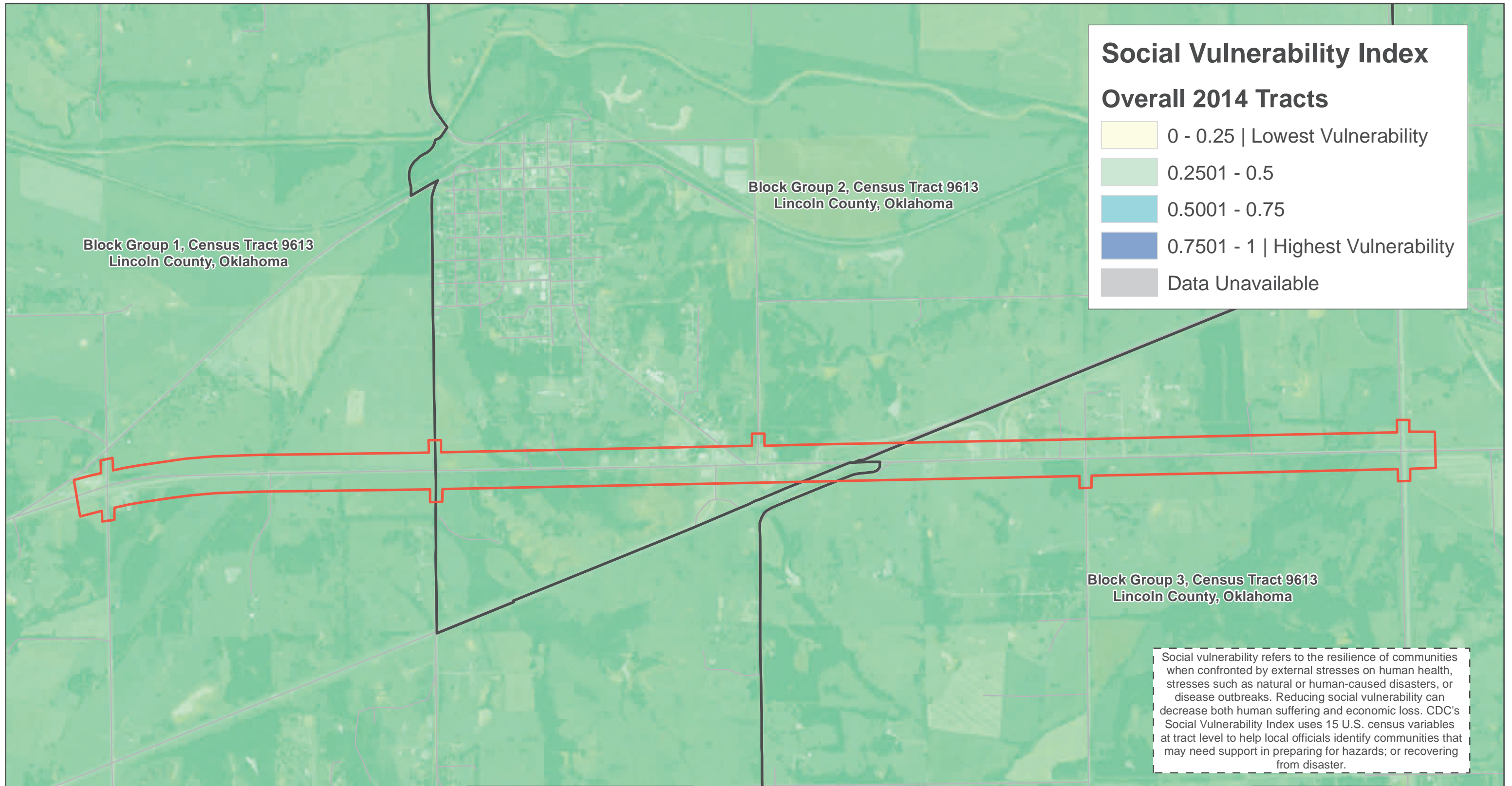
Environmental
PLANNING

DRAWN BY: LMP
APPRV BY:

SOURCE: DEQ, Tiger 2000,
USGS

Figure 11-1d

Date: 4/29/2020



- Recon Footprint
- Block Group Boundaries
- Roads

THE DIAGRAM IS PARTIALLY DIAGRAMATIC;
EXACT DIMENSIONS WILL VARY

Social Vulnerability Index

Widen and Resurface
J/P 34318(04)
SH-66 from SH-102, east to US-177
Lincoln County, OK

DRAWN BY: LMP
APPRV BY:

SOURCE: DEQ, Tiger 2000,
USGS

Figure 11-1e

Date: 4/29/2020

SECTION 106
CULTURAL RESOURCES STUDIES



OKLAHOMA
Transportation

Oklahoma Department of Transportation

Environmental Programs Division, Office 405.521.3050 / Fax 405.522.5193

DATE: August 14, 2022

TO: Geoff Canty, CC Environmental
Amanda Alexander, ODOT District 3 Environmental Project Manager

FROM: Scott Sundermeyer, ODOT Cultural Resources Program

SUBJECT: Lincoln County Project JP 34318(04): Proposed Improvements to SH-66: FROM 3.3 MILES EAST OF OKLAHOMA COUNTY LINE AT SH-102, EAST 4.0 MILES TO US-177.

The Oklahoma Department of Transportation (ODOT) completed Section 106 consultation on behalf of the Federal Highway Administration (FHWA) for proposed improvements to SH-66. The proposed improvements will address safety concerns with the roadway by constructing 8-foot shoulders and resurfacing the existing facility to provide for a 40-foot clear roadway. The project will also widen the existing bridge over Spring Creek (NBI 12596) to accommodate the 40-foot roadway. In all, 137 acres were surveyed.

As a result of the cultural resources study, one historic period archaeological site, five buildings or building complexes, two segments of Route 66, and one bridge was documented. The two segments of SH-66 (Route 66) and the bridge were found to be eligible for inclusion in the National Register of Historic Places (NRHP) under criterion A for their association with Route 66. Archaeological site 34LN238 and Building (complexes) 1 through 5 were found not eligible for inclusion in the NRHP.

ODOT provided a *no adverse effect* finding in our original November 8, 2022 submittal citing that safety improvements for certain transportation facilities were acceptable for such properties that are significant under criterion A. SHPO disagreed with our effect finding. Through a series of meetings and discussions, including a February 8, 2023 meeting with ODOT, SHPO, FHWA (Oklahoma Division and Headquarters), and ACHP, ODOT and FHWA reiterated this determination.

On May 18, 2023, ODOT provided accident data for the project to SHPO via email correspondence. SHPO responded and offered a condition to achieve a *no adverse effect* pending ODOT's agreement to include a commemorative sign, and requested ODOT provide this condition in official correspondence. ODOT responded and presented a condition of a commemorative sign detailing the "Wellston Gap". SHPO agreed to this commitment, provided a written response with the commitment included and asked ODOT to concur by signing the SHPO correspondence. ODOT did not sign the letter because the commitment was already memorialized in the Section 106 consultation.

Consultation with the State Historic Preservation Office (File #0313-23) and the State Archaeologist (OAS FY23-0316) resulted in concurrence a **no adverse effect** finding, pending the construction of the commemorative sign detailing the history of the Wellston Gap.

ODOT will need to ensure that the Designer, Project Management Division, Environmental Programs Division, and District 3 continue to incorporate the planning and design of the commemorative sign as part of the project development process so that the sign is included in the construction plans.

A plan note was prepared that includes an avoidance memo for off-site facilities and a memo to invite the Cultural Resources Program to the pre-work meeting to ensure that the commitment is met.

ODOT-CRP also consulted with the following tribes: Iowa Tribe of Oklahoma, Kickapoo Tribe of Oklahoma, Osage Nation, Sac and Fox Nation, and the Wichita & Affiliated Tribes. That summary memo is attached to the Section 106 consultation and this memo.

SAS



OKLAHOMA
Transportation

Oklahoma Department of Transportation

Environmental Programs Division, Office 405.521.3050 / Fax 405.522.5193

DATE: August 14, 2023

TO: Project Management Division

FROM: Environmental Programs Division

SUBJECT: Lincoln County Project JP 34318(04): Proposed Improvements to SH-66: FROM 3.3 MILES EAST OF OKLAHOMA COUNTY LINE AT SH-102, EAST 4.0 MILES TO US-177.

There are potentially significant archaeological sites within the general vicinity of the referenced project. Please have the following note added to a section of the project plans entitled "Environmental Mitigation Notes" per Policy Directive C-201-2D(2)

Locations outside the project area in the following area must not be utilized for borrow, equipment staging, haul roads, spoil dumps or any off-site project-related activity.

T14N R3E

Section 20 NW ¼, NE 1.4, NW ¼

The consultation with the State Historic Preservation Office is contingent upon completion of a construction-related commitment to construct a commemorative sign detailing the history of the "Wellston Gap". In order to meet this condition, the following must be done. Please have the following note added to a section of the project plans entitled "Environmental Mitigation Notes" per Policy Directive C-201-2D(2):

The Resident Engineer shall invite the ODOT Cultural Resources Specialist (phone number 405-325-7201) for the pre-work conference to discuss the placement of a commemorative sign detailing the "Wellston Gap".

SAS



Oklahoma Historical Society
State Historic Preservation Office

June 16, 2023

Mr. Scott Sundermeyer
ODOT Cultural Resources Program Manager
3200 Marshall Avenue, Room 110
Norman, OK 73072

RE: File #0313-23; SH-66 Proposed Improvements Project #JP-34318(04), Lincoln County

Dear Mr. Sundermeyer:

We have received and reviewed the additional materials submitted for the referenced undertaking in Lincoln County submitted with your letter dated May 24, 2023. We confirm that the condition, as proposed in your letter, is what was agreed upon through our recent correspondence. To reiterate and confirm concurrence, the project, as proposed will have **no adverse effect** on the affected Route 66 properties, provided the following condition is met:

CONDITION:

ODOT will construct a commemorative sign or marker along SH-66 at a location that is suitable for public viewing, but will not conflict with the purpose and need of the project, which is to address and correct safety concerns with the roadway. ODOT and SHPO agree that the marker should present some context related to the history of the properties involved. ODOT believes that this provides an opportunity to address "Wellston Gap", which bypassed Wellston, proper in 1932. The current undertaking is largely confined to the Wellston Gap corridor. The Marker will be placed at a location that is compatible with the Wellston Gap context of its construction, likely in the vicinity of the SH-66 and SH-66B intersection.

If this condition is acceptable to you, please return this document with the signature as indicated on Page 2 of this letter, confirming your concurrence. When we receive it, your agency will have completed the Section 106 process as outlined in the Advisory Council on Historic Preservation's regulations, 36 CFR Part 800. Specifically, 36 CFR Part 800.5(b) provides that modification of an undertaking in accordance with conditions imposed by the SHPO shall result in a "no adverse effect" determination.

Thank you for the opportunity to review this project. If you have any questions, please do not hesitate to call Ms. Sara Werneke, Historic Preservation Architect, at 405-522-4478.

Mr. Sundermeyer
June 16, 2023
Page 2

RE: File #0313-23; SH-66 Proposed Improvements Project #JP-34318(04), Lincoln County

Sincerely,



Lynda Ozan
Deputy State Historic
Preservation Officer

LO:pm

I hereby accept the condition stated in this letter.

Signature

Date



May 24, 2023

Ms. Lynda Ozan
Deputy State Historic Preservation Officer
State Historic Preservation Office
Oklahoma Historical Society
800 Nazih Zuhdi Drive
Oklahoma City, Oklahoma 73105-7917

Dear Ms. Ozan:

Re: SHPO File #0313-23; Lincoln County FHWA Project JP 34318(04): Proposed improvements to SH-66 from SH-102 east to US-177 (with one mile exception at Turnpike Interchange); submittal for comment under Section 106 of the National Historic Preservation Act.

Thank you for your comments of December 21, 2022 and for discussing this project through subsequent meetings and correspondence.

In our recent correspondence of May 18 and May 19, 2024, our offices consulted via email about the accident data on this segment of SH-66. I very much appreciate this email exchange and I believe we have agreed on a solution that will achieve a *no adverse effect*, with conditions. With this submittal, I am attaching a hard copy of the accident data for your files and formalizing our commitments as the following condition to achieve the effect finding.

Condition:

ODOT will construct a commemorative sign or marker along SH-66 at a location that is suitable for public viewing, but will not conflict with the purpose and need of the project, which is to address and correct safety concerns with the roadway. ODOT and SHPO agree that the marker should present some context related to the history of the properties involved. ODOT believes that this provides an opportunity to address “Wellston Gap”, which bypassed Wellston, proper in 1932. The current undertaking is largely confined to the Wellston Gap corridor. The marker will be placed at a location that is compatible with the Wellston Gap context of its construction, likely in the vicinity of the SH-66 and SH-66B intersection.

With these conditions imposed, ODOT is proposing a finding of **no adverse effect**, in accordance with 36 CFR 800.5(b). We respectfully request your comment to our finding and proposed conditions, and we look forward to continuing to work with your office on our ongoing Oklahoma Route 66 Inventory and Evaluation guidance to address the NRHP eligibility of Route 66 and associated resources throughout the state.

If you have any questions regarding this project, please contact me at 325-7201.

Sincerely

A handwritten signature in blue ink, appearing to read 'SS' followed by a long horizontal flourish.

Scott A. Sundermeyer
ODOT Cultural Resources Program Manager

From: [Lynda Ozan](#)
To: [Scott Sundermeyer](#)
Subject: RE: SHPO File 0313-23; Lincoln 34318(04) SH-66 improvements near Wellston
Date: Tuesday, May 23, 2023 3:20:13 PM
Attachments: [image001.png](#)
[image002.png](#)

Sounds great.

Lynda Ozan

Deputy State Historic Preservation Officer

Oklahoma Historical Society
800 Nazih Zuhdi Drive
Oklahoma City, OK 73105

405-522-4484
okhistory.org



Oklahoma
Historical
Society

From: Scott Sundermeyer <[SSundermeyer@odot.org](mailto:ssundermeyer@odot.org)>
Sent: Tuesday, May 23, 2023 6:28 AM
To: Lynda Ozan <Lynda.Ozan@history.ok.gov>
Subject: RE: SHPO File 0313-23; Lincoln 34318(04) SH-66 improvements near Wellston

Morning Lynda – apologies for not responding yesterday. I discussed with the District Engineer for ODOT's District 3 to ensure that this was something that we could accomplish. He assures me that we can commit to a sign. I'd be happy to drop a letter in the mail today confirming our commitment. We have an interesting opportunity to incorporate the "Wellston Gap" component into the marker. <https://www.okhistory.org/publications/enc/entry?entry=WE011> and include it in our story map as well.

Thanks again.

Scott A. Sundermeyer, RPA
Cultural Resources Program Manager
Desk: 405-325-7201
Cell: 405-388-2201
ssundermeyer@odot.org

From: Lynda Ozan <Lynda.Ozan@history.ok.gov>
Sent: Friday, May 19, 2023 4:44 PM

To: Scott Sundermeyer <SSundermeyer@odot.org>

Subject: RE: SHPO File 0313-23; Lincoln 34318(04) SH-66 improvements near Wellston

Scott:

Thank you for the information. Accident information is pertinent to how you review projects but not how SHPO reviews them. We have to look at the historical resources located within the project area which has nothing to do with accident data.

We discussed the project again and really come down to something along the lines of a conditional no-adverse if you put up a historic marker to indicate what used to be there as Route 66.

Lynda Ozan

Deputy State Historic Preservation Officer

Oklahoma Historical Society
800 Nazih Zuhdi Drive
Oklahoma City, OK 73105

405-522-4484
okhistory.org



From: Scott Sundermeyer <SSundermeyer@odot.org>

Sent: Thursday, May 18, 2023 10:59 AM

To: Lynda Ozan <Lynda.Ozan@history.ok.gov>

Subject: SHPO File 0313-23; Lincoln 34318(04) SH-66 improvements near Wellston

Hi Lynda –

As part of the consultation for the referenced project, I wanted to provide you the accident data for the section of roadbed between SH-102 and US-177. You'll note several injury and fatality accidents throughout the project area from 20087 to 2018 that are necessitating the project – specifically the addition of shoulders. The graphic is compelling, but the data is also compiled into a series of tables that illustrate more details of the occurrences.

I wonder if you could review this information and allow me to prepare a response to your December 21, 2022 letter that includes the accident history as the driver for the project in support of a NAE finding.

We have discussed this approach before for ADA compliance (traffic calming devices).

Please feel free to give me a call if you would like to discuss.

Thank you!



Oklahoma Historical Society

Founded May 27, 1893

State Historic Preservation Office

Oklahoma History Center • 800 Nazih Zuhdi Drive • Oklahoma City, OK 73105-7917
(405) 521-6249 • Fax (405) 522-0816 • www.okhistory.org/shpo/shpom.htm

December 21, 2022

Mr. Scott Sundermeyer
ODOT Cultural Resources Program Manager
3200 Marshall Avenue, Room 110
Norman, OK 73072

RE: File #0313-23; SH-66 Proposed Improvements Project #JP-34318(04), Lincoln County

Dear Mr. Sundermeyer:

Thank you for submitting additional information with your letter dated November 30, 2022, for the referenced project. We have completed our review. We concur with your opinion that archaeological site 34LN238, the buildings/building complexes #1A through #5D (numbers #1-#12 on the attached list) documented during survey efforts, and the Bridge over Captain Creek (#NBI-26639, #16 on the attached list) are **not eligible** for the National Register of Historic Places (NRHP).

Regarding Structures/Route 66 Roadbed Segments #6 and #7 (numbers #13 and #14 on the attached list) and the Bridge over Spring Creek (#NBI-12596, #15 on the attached list), we are of the opinion that these properties are **eligible** for the NRHP at the local level of significance, under Criterion A for Transportation in the context of the NRHP Multiple Property Documentation Form "Route 66 and Associated Historic Resources in Oklahoma." As the statewide inventory of Route 66 in Oklahoma is still under way by Mead & Hunt and ODOT, we find that we are unable to comment on the potential eligibility of these resources as part of a statewide linear historic district. However, we do not concur with your opinion that the project, as proposed, will have no adverse effect on the historic resources. We are of the opinion that the project, as proposed, will have an **adverse effect** on the historic resources.

Our opinion is based on the following facts:

Structure #6

- Increasing the roadway width from 28 feet to 40 feet

Structure #7

- Increasing the roadway width from 28 feet to 40 feet
- Increasing the roadway width from 28 feet to 52 feet at the location of the proposed center turn lane

Bridge over Spring Creek (#NBI-12596)

- Increasing the bridge width to accommodate a 40-foot roadway

Thus, the project, as proposed, would negatively impact the integrity of design for Structures #6, #7, and #NBI-12596.

Although previous consultation (#JP-28890(04)/SHPO File #0806-21) was referenced as justification for the implementation of road surface expansion and improvements resulting in a no adverse effect, the referenced project was limited to minor expansion of the driving lanes and shoulders and kept the surface contained within the original 38-foot width for both the pavement and shoulders as prescribed by the original Route 66 design specifications.

Mr. Sundermeyer
December 21, 2022
Page 2

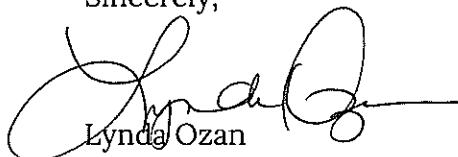
RE: File #0313-23; SH-66 Proposed Improvements Project #JP-34318(04), Lincoln County

We welcome the opportunity to continue consultation with you to mitigate, minimize, or eliminate the adverse effect of the proposed project. However, if we are unable to eliminate the adverse effect of the project, the Federal Highways Administration will need to contact and invite the participation of the Advisory Council on Historic Preservation (ACHP) in order to complete the Section 106 process as outlined in 36 CFR Part 800. Should the ACHP choose not to participate in the consultation, Federal Highways Administration and the SHPO may execute a Memorandum of Agreement (MOA). Upon the execution of an MOA, a copy must be filed with the ACHP to complete the Section 106 process.

Thank you for the opportunity to review this project. Should further correspondence pertaining to this project be necessary, please reference the above underlined file number.

If you have any questions, please do not hesitate to call Kristina Wyckoff, Historical Archaeologist, at 405-521-6381; Matthew Pearce, Ph.D., National Register Program Coordinator, at 405-522-4479; or Sara Werneke, Historic Preservation Architect, at 405-522-4478.

Sincerely,



Lynda Ozan
Deputy State Historic
Preservation Officer

LO:pm

Attachment

FILE # LIST OF PROPERTIES

0313-23 SH-66 PROPOSED IMPROVEMENTS
PROJECT #JP-34318(04),
LINCOLN COUNTY

- I. SH-66 FROM SH-102 EAST TO US-77,
SECS 13,14,15,22,23,24 T14N R2E, &
SECS 17,18,19,20 T14N R3E

STRUCTURES:

- 1-2. #1A-B, COMMERCIAL BUILDING
& OUTBUILDINGS, 3352 SH-66,
WELLSTON
3. #2, HOUSE, 504 SH-66, WELLSTON
4. #3A, HOUSE, NORTH SIDE OF SH-66,
SEC13 T14N R2E, WELLSTON VICINITY
5. #3B, SHED, NORTH SIDE OF SH-66,
SEC13 T14N R2E, WELLSTON VICINITY
6. #3C, SHED, NORTH SIDE OF SH-66,
SEC13 T14N R2E, WELLSTON VICINITY
7. #4A, HOUSE, 335030 SH-66, WELLSTON
VICINITY
8. #4B, SHED, 335030 SH-66, WELLSTON
VICINITY
9. #5A, SET OF 3 TANKS, SOUTH SIDE OF
SH-66, SEC19 T14N R3E, WELLSTON
VICINITY
10. #5B, OUTBUILDING, SOUTH SIDE OF SH-66,
SEC19 T14N R3E, WELLSTON VICINITY
11. #5C, OUTBUILDING, SOUTH SIDE OF SH-66,
SEC19 T14N R3E, WELLSTON VICINITY
12. #5D, OUTBUILDING, SOUTH SIDE OF SH-66,
SEC19 T14N R3E, WELLSTON VICINITY
13. #6, SH-66 - ROUTE 66 ROADBED SEGMENT,
FROM APPROX. 730' WEST OF HAYES DRIVE
TO APPROX. 630' WEST OF CAPTAIN CREEK,
SECTIONLINE BETWEEN SEC4 & SEC23,
SEC15 & SEC22 T14N R2E, WELLSTON

14. #7, SH-66 - ROUTE 66 TWO-LANE ROADBED
SEGMENT FROM EASTBOUND TURNER TURNPIKE
ENTRANCE TO APPROX. 460' EAST OF US-177,
SECTIONLINE BETWEEN SEC13 & SEC24,
SEC18 & SEC19, & SEC17 & SEC20 T14N
R2E & R3E, WELLSTON VICINITY
15. BRIDGE #41060702X [NBI-12596] ON SH-66
OVER SPRING CREEK, WELLSTON VICINITY
16. BRIDGE #41060372X [NBI-26639] ON SH-66
OVER CAPTAIN CREEK (2001)



November 30, 2022

Dr. Matthew Pearce, Ph.D.
National Register Program Coordinator
State Historic Preservation Office
Oklahoma Historical Society
800 Nazih Zuhdi Drive
Oklahoma City, Oklahoma 73105-7917

Dear Dr. Pearce:

Re: SHPO File #0313-23; Lincoln County FHWA Project JP 34318(04): Proposed improvements to SH-66 from SH-102 east to US-177 (with one mile exception at Turnpike Interchange); additional information requested November 22, 2022

Thank you very much for your correspondence from November 22, 2022 in which you asked ODOT to provide supporting information to confirm our National Register evaluation of two segments of SH-66 near Wellston. I also appreciate our telephone correspondence on November 29, 2022.

As discussed in our phone conversation, please find a copy of an email transmittal between Dr. Matt Corpolongo and Mr. Chad Moffett (Mead & Hunt) regarding these particular segments. At the time of the email transmittal, Mead & Hunt had organized two meetings with ODOT and SHPO to discuss evaluation methodology to be employed for the statewide inventory of Route 66 in Oklahoma (February 5 and July 1, 2021). You may find correspondence of these meetings and our prior submittals under MOA #468/File 0076-21. As you are aware, there are multiple property document forms (MPDF) for Route 66 and Mead & Hunt is using these documents to provide guidance in evaluation Route 66 in Oklahoma.

During our discussion, you presented an inconsistency in some of the of language in the CR report submittal. I would like to provide some clarification in my working of the last paragraph of Page 2 of the transmittal letter:

*While the results of the Mead & Hunt study may provide a revised eligibility assessment for this segment once completed, currently, the 27947(04) APE has a consensus **not eligible** finding from our agencies. ODOT must rely on the consultation for that undertaking at this time.*

The intent of this paragraph was to inform SHPO that the 27947(04) segment has been previously subjected to Section 106 review and has a consensus determination of *not eligible* for inclusion in the National Register of Historic Places (NRHP). This statement's purpose was to provide a western boundary for the proposed NRHP-eligible segment of SH-66 that includes Structures 6 and 7 and a 1.1 mile-long project exception between the two Turner Turnpike interchange access points. The eastern boundary of the historic property is roughly 460 feet east of the US-177. SH-66 has not yet been evaluated east of this location.

Our evaluation for the current undertaking is not only informed by the MPDF and the attached email correspondence, but also from precedence guided through our consultation on another SH-66 project between Bristow and Depew, Oklahoma (JP 28890(04); SHPO File 0806-21), in which SHPO provided our office evidence for which the roadbed was eligible for listing on the NRHP and ODOT reversed our eligibility finding for this segment. The undertaking resulted in a *no adverse effect* determination.

The attached materials represent the most current information and uses an established or current methodology in which to evaluate SH-66 for this undertaking. ODOT has involved SHPO in two meetings in which to discuss the approach and have provided a scope of work in an April 14, 2022 correspondence. We invite your review of those materials. Unfortunately, with many of our broad-based National Register evaluations, we are often challenged to complete such evaluations for individual undertakings prior to the completion of the inventory and evaluation for the broader class of resource.

Pursuant to 36 CFR 800.5(b), it is our determination that the project, as proposed, will have *no adverse effect* to historic properties. We respectfully request your comment to our determination, and we look forward to continuing to work with your office on our ongoing Oklahoma Route 66 Inventory and Evaluation guidance to address the NRHP eligibility of Route 66 and associated resources throughout the state.

If you have any questions regarding this project, please contact me at 325-7201.

Sincerely



Scott A. Sundermeyer
ODOT Cultural Resources Program Manager



Oklahoma Historical Society

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State Historic Preservation Office

Oklahoma History Center • 800 Nazih Zuhdi Drive • Oklahoma City, OK 73105-7917
(405) 521-6249 • Fax (405) 522-0816 • www.okhistory.org/shpo/shpom.htm

November 22, 2022

Mr. Scott Sundermeyer
ODOT Cultural Resources Program Manager
3200 Marshall Avenue, Room 110
Norman, OK 73072

RE: File #0313-23; SH-66 Proposed Improvements Project #JP-34318(04), Lincoln County

Dear Mr. Sundermeyer:

We have reviewed the documentation submitted on the referenced project. We find that we are unable to complete the review of your project without the following additional information:

We find that we are unable to concur with your recommendation that Structures #6 and #7 (segments of SH-66) are eligible for the National Register of Historic Places at this time. Please provide additional information regarding the evaluation methodology being utilized for ODOT's statewide inventory and evaluation of Route 66 as a statewide linear historic district, even if such information is only available in draft form at this time.

If you should have any questions concerning this request, please contact me at 405/522-4479.

Thank you for the opportunity to review this project. Please reference the above underlined file number when responding.

Sincerely,

Matthew Pearce, Ph.D.
National Register Program Coordinator

MP:pm



November 8, 2022

Ms. Lynda Ozan
Deputy State Historic Preservation Officer
State Historic Preservation Office
Oklahoma Historical Society
800 Nazih Zuhdi Drive
Oklahoma City, Oklahoma 73105-7917

Dear Ms. Ozan:

Re: SHPO File #0990-20; Lincoln County FHWA Project JP 34318(04): Proposed improvements to SH-66 from SH-102 east to US-177 (with one mile exception at Turnpike Interchange); submittal for comment under Section 106 of the National Historic Preservation Act.

Please find the attached cultural resources report, archaeological site form, Historic Preservation Resource Identification (HPRI) forms for five buildings or building complexes, HPRI forms for two segments of SH-66, and a Bridge Inventory form for NBI 12596. These properties were documented in advance of proposed improvements to two segments of SH-66 (Route 66) south of Wellston, Oklahoma. The existing facility consists of two 12-foot paved driving lanes with 2-foot sod shoulders.

The proposed roadway improvements include mill and overlay of 12-foot-wide driving lanes, construction of new 8-foot-wide shoulders, and extension of a single triple-cell reinforced concrete box (RCB) identified as NBI 12596. An additional safety improvement includes the construction of a 12-foot-wide center turn lane between two access points to the Turner Turnpike (from approximately 2100 feet east of County Road NS 335, extending west to the interchange of SH-66 and the Turner Turnpike access point). Bridge improvements to NBI 12596 will include extending the existing bridge structure to accommodate the roadway improvements described above. The structure will retain the current NBI number. There is an approximately 1.1-mile-long exception in the center of the project (on either side of the I-44 Turnpike interchange) where no work will be completed. The preliminary area of potential effect (APE) includes two sections of roadbed and bridge improvements, and is described as the “study area” in the attached report and figures. At the time of this cultural resources evaluation and reporting, project plans have not been prepared.

During the cultural resources investigations, one new archaeological site was documented. Site 34LN238 represents the remains of a historic-period farmstead associated with the Goggins family from some time prior to 1926 until 1984. The site is a light scatter of marginally diagnostic artifacts that place the site within the early-to-mid 20th century. Based on the presence of features, deed research was conducted to identify significance with important persons or events. The consultant recommended that the site was not eligible for inclusion in the National Register of Historic Places (NRHP) due to lack of important associations or significant deposits that would address research questions. The site does not convey significance under criterion D and we find that the site is *not eligible* for inclusion in the National Register of Historic Places (NRHP).

Five buildings or building complexes were documented during the survey efforts. These properties are all confined to construction dates of circa 1960 through 1975 and represent domestic or commercial dwellings and outbuildings. These properties do not embody distinctive characteristics or represent the works of a master, and no associations were identified that would convey individual significance or significance as a portion of a linear district under National Register criteria. ODOT finds that these properties are *not eligible* for inclusion in the NRHP.

Two segments of SH-66 were also documented. Structure 6 is a 1.5-mile-long portion of a segment of SH-66 that was constructed as part of the Wellston Gap in 1933. The documented segment begins roughly 750 feet west of the SH-66/SH-66B intersection and extends west 1.5 miles. As documented in prior consultation and on the NRHP nomination form for the Captain Creek truss bridge (NRID 4000134), the Wellston Gap was a controversial bypass of Wellston, which removed Wellston, and the former 1926 alignment from its formal designation as Route 66. As discussed in the report and on HPRI forms, this segment has been altered in materials since its construction in 1933.

Structure 7 is a 1.3 mile-long portion of a segment of the 1926 alignment of Route 66. The documented section extends from the Turner Turnpike interchange entrance, east 1.3 miles to approximately 460 feet east of the US-177/SH-66 intersection. As discussed in the report and on HPRI forms, this segment has been altered in materials since its construction in 1926.

A single bridge was documented within the eastern (1926) roadbed segment. Structure NBI 12596 was constructed in 1951 as part of a series of roadway improvements to this segment. The structure is a triple-cell bridge class concrete culvert.

As ODOT is currently completing a statewide inventory and evaluation of Route 66 in Oklahoma, we collected information on these two segments using evaluation criteria currently being used in that study, and specifically from Multi-Property Documentation Form(s) (MPDF) being used to assist in the evaluation of Route 66. As you are aware, Mead & Hunt is completing that evaluation for ODOT.

Based on the interpretation of the MPDF and on the evaluation methodology being employed for the statewide Route 66 inventory and evaluation, Mead & Hunt has identified, and would recommend, both segments of the roadbed as eligible for contributing to a statewide linear Route 66 historic district under Criterion A for significance in the area of Transportation. The eastern segment would include the 1951 bridge (NBI 12596) as a contributing resource as well. Using this information, Stantec has recommended these two segments and the subject bridge as **eligible** for inclusion in the NRHP as contributing elements to the linear Route 66 historic district.

Previous alterations to the roadbed, as discussed in the attached CR report, would not diminish the integrity of either segment. Both roadbeds appear to retain integrity of association and contribute to a larger district under Criterion A. Information does not suggest the segments would possess significance under any other areas of significance under Criterion A or under Criterion B, C, or D.

ODOT NBI 12596 is also a contributing resource of this linear district. The attached CR report indicates that “the concrete parapet and the metal guardrails are later additions”, however this structure retains its essential physical features – which for a RCB is its superstructure – and contributes. This bridge would not be individually eligible for its association with Route 66 or under any other criteria.

Based on the information attached, and our effort to employ information currently being used by Mead & Hunt to evaluate Route 66 in Oklahoma, we find that the SH-66 (Route 66) segment from 460 feet east of the SH-66/US-177 intersection, west to approximately 1000 feet east of the SH-66/SH-102 intersection is **eligible** for listing on the NRHP at the local level as contributing elements to a statewide linear Route 66 historic district under Criterion A in Transportation in the context of the NRHP MPDF) “Route 66 and Associated Historic Resources in Oklahoma” (amended submission 2003). The boundary of the historic property includes two segments of Route 66 with a 1926 and a 1933 construction date, a triple-cell RCB constructed in 1951 (NBI 12956) and the 1.1 mile-long project exception between the two Turner Turnpike interchange access points. Please note that this segment has been altered to include addition of Turnpike on/off ramp interchanges and flyovers. While this area is outside the APE for the current undertaking, ODOT has employed the methodology used by Mead & Hunt and can reasonably extend the historic property boundaries to include this segment.

The western boundary of the historic property coincides with the eastern terminus of an undertaking that was previously submitted for review and comment under Lincoln County JP 27947(04) (SHPO File 1253-15) in which our consultation resulted in a finding that SH-66 and associated roadbed features were **not eligible** for inclusion in the NRHP. While the results of the Mead & Hunt study may provide a revised eligibility assessment for this

segment once completed, currently, the 27947(04) APE has a consensus *not eligible* finding from our agencies. ODOT must rely on the consultation for that undertaking at this time.

The eastern boundary of the historic property must be terminated at the eastern project terminus (some 460 feet east of US-177) at this time. The area east of SH-177 towards Chandler, Oklahoma has not been subject to any formal inventory and evaluation as part of a Section 106 undertaking, nor have the formal results of the Mead & Hunt study been submitted for SHPO and consulting party review and comment. ODOT looks forward to providing your office with an evaluation of the area east of US-177 as part of the statewide inventory, once complete.

As ODOT has consulted with your office on another recent undertaking for improvements to SH-66 between Bristow and Depew, Oklahoma (Creek County JP 28890(04); SHPO [File 0806-21](#)), we have reviewed the results of that consultation, including the finding of *no adverse effect* for that undertaking, to inform our effect finding for the current undertaking.

National Register Eligibility under Criterion A, as demonstrated in this segment of roadbed, does not rely entirely on integrity of materials and workmanship. Roads are inherently meant to evolve and adapt to accommodate the traffic that they carry. As such, they can retain the essential qualities (such as location, alignment, and lane configuration) that convey their significance under Criterion A in spite of later material alterations or safety improvements. These are all consistent with our consultation for the Creek County undertaking..

The property is significant through its association with Route 66 and is conveyed through its inherent location and alignment. Such features provide area residents, visitors, and motorists with a similar feeling of association with Route 66. The roadbed, as a structure defined by National Park Service (NPS) Bulletin 15, will function, intact, as such, continue to allow travelers to experience the facility and its surroundings in a similar fashion as those who used it historically. The safety alterations proposed for this project will not compromise those character-defining features (location, setting, association, and feeling).

As the proposed undertaking includes the addition of shoulders and resurfacing of the existing facility, which are natural and expected evolutionary modifications of the roadbed that provide a needed safety improvement, we find that the undertaking will not adversely affect the character-defining features of the roadbed segment or RCB, nor will it compromise the resource's significance as associated with Route 66 and the economic development generated by its completion. As the roadbed segment has experienced periodic alterations since 1926 and 1933 to its design, materials, and workmanship, as identified in the attached report, the addition of pavement rehabilitation, shoulders, and even the addition of the center turn lane to address Turnpike access safety improvements would not compromise the setting, location, association, or feeling of the resource. Improvements proposed by the undertaking, therefore, represent no dramatic alteration to the aspects of integrity that convey the property's significance.

Pursuant to 36 CFR 800.5(b), it is our determination that the project, as proposed, will have *no adverse effect* to historic properties. We respectfully request your comment to our determination, and we look forward to continuing to work with your office on our ongoing Oklahoma Route 66 Inventory and Evaluation guidance to address the NRHP eligibility of Route 66 and associated resources throughout the state.

If you have any questions regarding this project, please contact me at 325-7201.

Sincerely



Scott A. Sundermeyer
ODOT Cultural Resources Program Manager

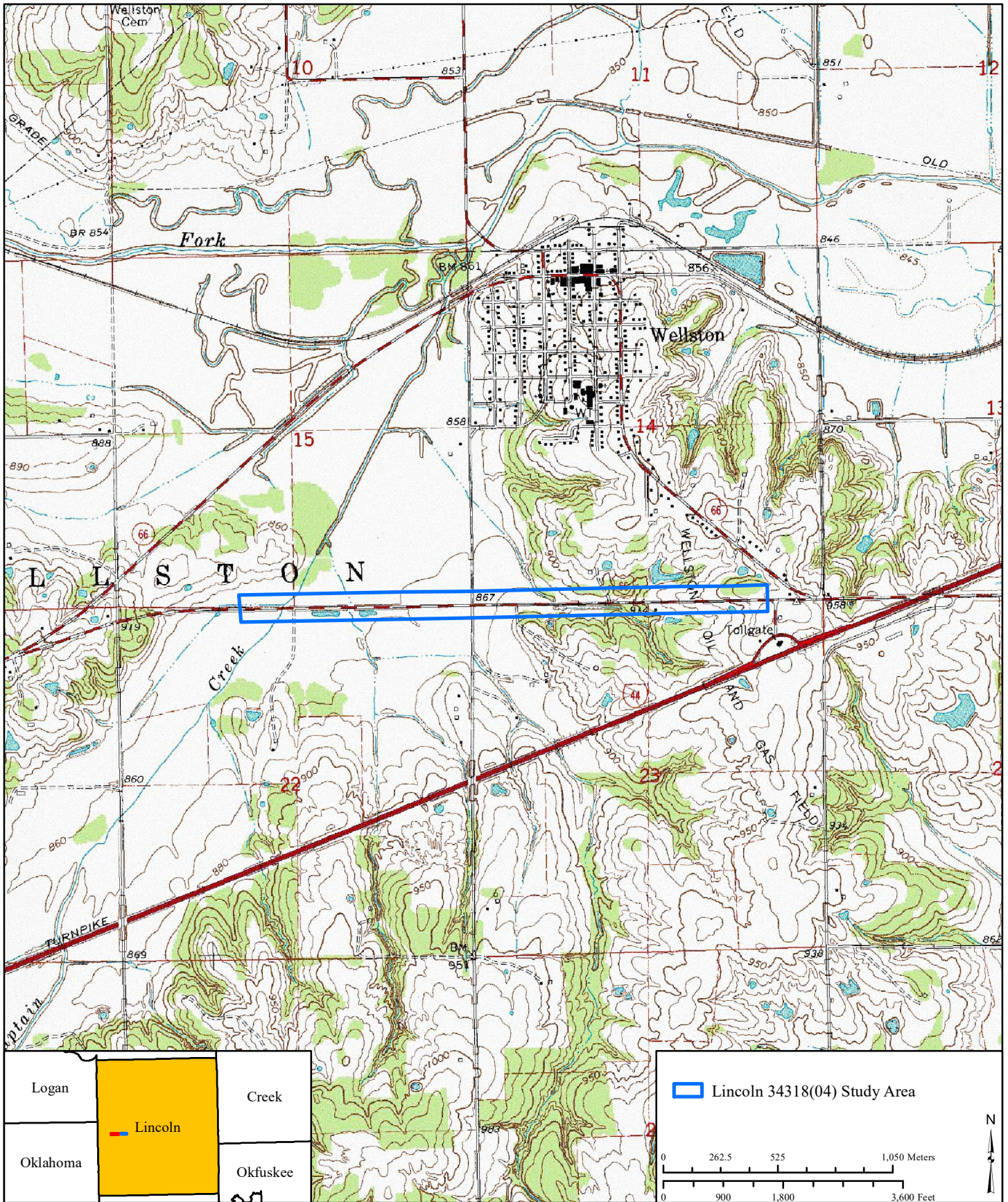


Figure 1a. Lincoln County JP 34318(04): SH-66 improvements near Wellston.

Basemap: USGS Wellston 7.5' Quadrangle [1966 (1981)]
 Legal Location: T14N R3E Sections 17-20; T14N R2E Sections 13-15, 22-24



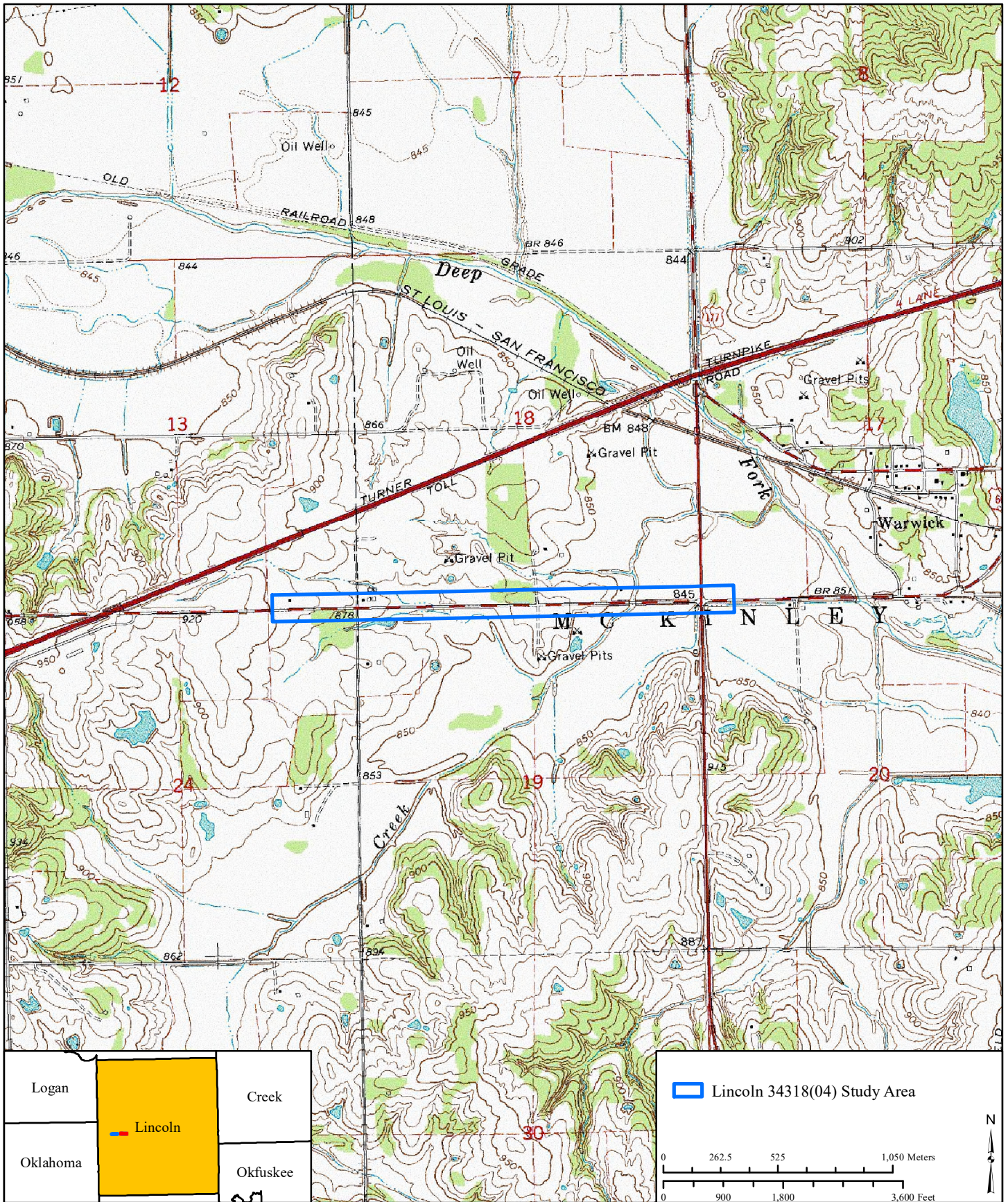


Figure 1b. Lincoln County JP 34318(04): SH-66 improvements near Wellston.

Basemap: USGS Wellston 7.5' Quadrangle [1966 (1981)]

Legal Location: T14N R3E Sections 17-20; T14N R2E Sections 13-15, 22-24





Oklahoma Archeological Survey

THE UNIVERSITY OF OKLAHOMA

December 9, 2022

Scott Sundermeyer, Director
ODOT Cultural Resources Program
3200 Marshall Ave, Room 110
Norman, OK 73019

Re: OAS FY23-0316 ODOT *Proposed Improvements & Bridge Replacement on State Hwy 66*.
Report by Kimberly Wright & Kory Van Hemert (Stantec).
ODOT J/P: 34318(04)
Legal Description: Sections 13-15, 22-24, T14N, R2E; Sections 17-20, T14N, R3E, Lincoln
County, Oklahoma.

Dear Mr. Sundermeyer,

This agency received the submitted ODOT cultural resources survey report of investigations regarding the above-referenced undertaking for review and comment. From the information provided, we understand that Stantec staff surveyed the 137-acre study area, which encompasses the Area of Potential Effect (APE) on March 21-25, 2022. One new historic site, 34LN238, was identified in the proposed project area. ODOT recommends this site as not eligible for listing on the National Register of Historic Places (NRHP). ODOT recommends the project as proposed will have *No Adverse Effect on Historic Properties*.

I concur with the findings and recommendations as they pertain to prehistoric archaeological resources and defer opinion on 34LN238 and overall project effects to Historic Archaeologist with the State Historic Preservation Office.

This review has been conducted in cooperation with the State Historic Preservation Office, Oklahoma Historical Society. You must also have a letter from that office to document your consultation pursuant to Section 106 of the National Historic Preservation Act.

Sincerely,

Kary L. Stackelbeck, Ph.D.
State Archaeologist

cc: SHPO



OKLAHOMA DEPARTMENT OF TRANSPORTATION

CULTURAL RESOURCES SURVEY REPORT

Lincoln JP 34318(04): Proposed Road Improvements and Bridge Replacement
on State Highway 66

Prepared by:

Kim Wright and Kory Van Hemert, MSc of Cox|McLain
Environmental Consulting, Inc., now Stantec (Stantec)

Principal Investigator:

Haley Rush, MA, RPA Stantec

August 26, 2022

Lead Federal Agency: Federal Highway Administration



OKLAHOMA

County:	Lincoln
J/P#:	34318(04)
Surveyed by:	Kim Wright, Jacob Cumberland, Amanda Temple, Brandon Curry, Jeremiah Camp, Jeremiah Perkins, and Kory Van Hemert
Survey Date:	March 21–25, 2022
Prime Consultant:	CC Environmental, LLC

MANAGEMENT SUMMARY:

The Oklahoma Department of Transportation (ODOT) proposes roadway improvements and bridge replacement to State Highway (SH-) 66 in Lincoln County. The study area begins 500 feet (152 meters) east of the intersection of SH-66 and United States Highway (US-) 177 and extends approximately 3.75 miles (6 kilometers) west. There is a 0.9 mile-long (1.4 kilometer-long) exception beginning 1.2 miles (1.9 kilometers) east of the of SH-66/US-177 intersection. The two discontinuous segments of the study area total 2.8 miles (4.5 kilometers) in length. The existing facility consists of a two-lane roadway with 12-foot-wide driving lanes and 2-foot-wide shoulders. Proposed improvements include widening and resurfacing the roadway, adding shoulders, repairing the bridge at Captain Creek, and improving or replacing the existing culvert over Spring Creek. As of 2020, the Average Daily Traffic (ADT) is 5,100 vehicles per day.

There are two bridges located within the study area.

The bridge over Spring Creek (ODOT Structure # 4106 0702 X [NBI 12596]) is a concrete culvert constructed in 1951. This bridge is of the type discussed in the Program Comment for post-1945 concrete and steel bridges; however, because this bridge is on the Oklahoma Historic Route 66 Scenic Byway, it is an exception to the Program Comment and was documented. Documentation was submitted to the ODOT Cultural Resources Program (ODOT-CRP).

The bridge over Captain Creek (ODOT Structure # 4106 0372 X [NBI 26639]) is a steel stringer/multibeam/girder constructed in 2001. While the bridge is on the Oklahoma Historic Route 66 Scenic Byway, the structure is not yet 45 years of age and was not documented.

The study area consists of two discontinuous segments and has a total length of approximately 2.8 miles (4.5 kilometers) It extends approximately 200 feet (61 meters) on either side of the SH-66 centerline. In total, the study area encompasses approximately 137 acres.

Cultural resources fieldwork was completed in March 2022. Cultural resources fieldwork methodology was developed in consultation with ODOT-CRP and conformed to the methods outlined in the ODOT-CRP manual (October 2017). Archeological fieldwork consisted of pedestrian survey augmented with shovel test and bucket auger excavations. Transects were set at a distance not exceeding 98.4 feet (30 meters) apart, with two transects along each side of the SH-66 roadway. One historic-age archeological site (34LN238) was recorded during field investigations. This site is recommended **not eligible** for inclusion in the National Register of Historic Places (NRHP).

A built environment survey resulted in the recordation of four building complexes (Building Complex 1, Building Complex 3–5), one building (Building 2), and two segments of historic Route 66 (Structures 6 and 7) with a total of thirteen historic-age resources (built in 1977 or earlier). These resources were recorded and documented on Historic Preservation Resource Identification (HPRI) forms. One bridge over Spring Creek (ODOT Structure # 4106 0702 X [NBI 12596]) was documented on an Oklahoma Historic Bridge Inventory form. Building Complex 1, Building 2, and Building Complexes 3, 4, and 5 are recommended **not eligible** for inclusion in the NRHP. According to newly established methodology for assessing Route 66 resources as proposed by Mead & Hunt and preliminarily accepted by the Oklahoma Historical Society / State Historic Preservation Office (OK / SHPO), the two segments of historic Route 66 in the study area (Structures 6 and 7), and the bridge over Spring Creek (ODOT Structure # 4106 0702 X [NBI12596]) are recommended **eligible** for inclusion in the NRHP under Criterion A for significance in the area of Transportation, as contributing resources to a statewide linear Route 66 historic district with a Period of Significance (POS) of 1926–1970.

1. PROJECT DESCRIPTION:

The Oklahoma Department of Transportation (ODOT) proposes roadway improvements and bridge replacements to State Highway (SH-) 66 in Lincoln County. The existing facility consists of a two-lane roadway with 12-foot-wide driving lanes and 2-foot-wide shoulders. Proposed improvements include widening and resurfacing the roadway, adding shoulders, repairing the bridge at Captain Creek, and improving or replacing the existing culvert over Spring Creek. As of 2020, the Average Daily Traffic (ADT) is 5,100 vehicles per day.

The study area begins 500 feet (152 meters) east of the intersection of SH-66 and United States Highway (US-) 177 and extends approximately 3.75 miles (6 kilometers) west. It extends approximately 200 feet (61 meters) on either side of the SH-66 centerline. There is a 0.9 mile-long (1.4 kilometer-long) exception beginning 1.2 miles (1.9 kilometers) east of the of SH-66/US-177 intersection. The two discontinuous segments of the study area total 2.8 miles (4.5 kilometers) in length. In total, the study area encompasses approximately 137 acres.

The bridge over Spring Creek (ODOT Structure # 4106 0702 X [NBI 12596]) is a concrete culvert constructed in 1951. This bridge is of the type discussed in the ACHP Program Comment for post-1945 bridges. Because the bridge is located on the Route 66 Scenic Byway, it is exempted from the Program Comment.

The bridge over Captain Creek (ODOT Structure # 4106 0372 X [NBI 26639]) is a steel stringer/multibeam/girder constructed in 2001. This bridge is not 45 years of age.

Legal Location: Sections 13, 14, 15, 22, 23, and 24, Township 14N, Range 2E
Sections 17, 18, 19 and 20, Township 14N, Range 3E.

U.S.G.S. Quadrangle: Wellston, OK (1991, 2018)

2. ENVIRONMENTAL SETTING:

Geomorphic/Physiographic Region:

The study area is in the Northern Cross Timbers subregion of the Cross Timbers ecoregion. The Cross Timbers ecoregion is characterized by a belt of low, rolling hills covered by a mosaic of woodland, pastureland, and rangeland. It separated drier, prairie-dominated western ecoregions from moister, more forested eastern areas (Woods et al. 2005). The study area is located within the Central Red-Bed Plains geomorphic province, which is underlain by Permian red shales and sandstone that have formed gently rolling hills and broad, flat plains within the area (Curtis et al. 2008).

Geology and Soils:

According to United States Geological Survey (USGS) data, the study area is primarily underlain by Holocene-aged alluvium and Late Pennsylvanian Oscar Group. Holocene-age alluvium consists of sand, silt, clay, and lenticular beds of gravel. Oscar Group consists of red-brown to gray shale and orange-brown fine-grained, cross-bedded sandstone that grades southward into arkosic sandstone and conglomerate (USGS 2022a).

Natural Resources Conservation Service (NRCS) data lists multiple soil complexes throughout the study area, many of which occur on floodplains and have moderate to high potential for deep profiles and/or buried A horizons. Ashport soils consist of very deep, well drained soils that formed in loamy alluvium of Holocene age and occur in approximately 28 percent of the study area. Ustibuck soils consist of very deep, poorly drained, very slowly permeable soils that formed in clayey and loamy alluvium derived from the red beds of Permian Age. Pulaski soils consist of very deep, well drained, moderately rapidly permeable flood plain soils that formed in loamy alluvial sediments of Holocene age. Miller soils consist of very deep, moderately well drained soils that formed in material weathered from predominantly clayey alluvium of Pleistocene ages. Easpor soils consist of very deep, well drained soils that formed in loamy alluvium of Holocene age. Mapped soils are detailed in **Table 1** (Soil Survey Staff 2022).

Table 1. Mapped Soils

Soil	Landform	Typical Profile	A horizon depth	Buried A?	% of Study Area
Ashport clay loam, 0 to 1 percent slopes, occasionally flooded	Flood plains	Ap/A/Bw/Ab/Bwb	133 cmbs*	Yes	28.2
Konawa loamy fine sand, 3 to 8 percent slopes, severely eroded	Hillslopes	A/Bt/BC/C	15 cmbs	No	12.2
Teller loam, 3 to 5 percent slopes, eroded	Paleoterraces	A/Bt/C	20 cmbs	No	10.5
Ustibuck clay, 0 to 1 percent slopes, occasionally flooded	Flood plains	A/AC/C	64 cmbs	No	8.3
Grainola-Ironmound complex, 5 to 15 percent slopes	Hillslopes	A/BA/Bt/Cr	18 cmbs	No	7.8
Darnell-Stephenville complex, 3 to 12 percent slopes	Hillslopes	A/E/Bw/Cr	20 cmbs	No	7.5
Pulaski fine sandy loam, 0 to 1 percent slopes, occasionally flooded	Flood plains	Ap/C1/C2	48 cmbs	No	6.5
Miller clay, 0 to 1 percent slopes, occasionally flooded	Flood plains	A/AC/C	60 cmbs	No	3.7
Easpur loam, 0 to 1 percent slopes, occasionally flooded	Flood plains	A/AC/C	92 cmbs	No	3.3
Coyle and Seminole soils, 3 to 5 percent slopes, severely eroded	Hillslopes	AP/BA/BT/BC/Cr	15 cmbs	No	3.0
Dougherty loamy fine sand, 3 to 8 percent slopes	Dunes of hillslopes	A/E/Bt/BC/C	15 cmbs	No	1.8
Teller loam, 1 to 3 percent slopes	Paleoterraces	Ap/BA/Bt/C	38 cmbs	No	1.7
Stephenville fine sandy loam, 3 to 5 percent slopes	Hillslopes	A/E/Bt/BC/Cr	18 cmbs	No	1.5
Grainola clay loam, 3 to 5 percent slopes	Hillslopes	Ap/BA/Btk/C r	15 cmbs	No	1.1
Vanoss loam, 1 to 3 percent slopes	Paleoterraces	Ap/BA/Bt/C	28 cmbs	No	0.7
Renthin-Grainola complex, 3 to 5 percent slopes, severely eroded	Hillslopes	A/Bt1/Bt2/BC/Cr	20 cmbs	No	0.6

Source: Soil Survey Staff (2022); *centimeters below surface

Vegetation:

The study area is situated within the Post Oak-Blackjack Forest vegetational region, which is known locally as the Cross Timbers. This region presents a mosaic of forest, woodland, and grassland vegetation. Predominant trees consist of post oak and blackjack oak. Blackhaw, black oak, black hickory, buckbrush, gum bumelia, Mexican plum, redbud, roughleaf dogwood, and smooth and winged sumac are common woody plants. Other common plants include beebalm, big bluestem, poverty grass, pussytoes, trailing lespedeza, and purpletop (Hoagland 2008).

According to the Multi-Resolution Land Characteristics (MRLC) Consortium Viewer (MRLC 2022), the study area is composed mostly of pasturelands and grasslands with portions of cultivated cropland, deciduous forest, and developed spaces.

An overview of the Google Earth Imagery of the study area generally corroborates with the information provided above.

Surface Visibility:

<u>XXX</u>	0-25%	Developed areas, thicket, pastures, forests
<u>XXX</u>	25-50%	Riparian vegetation around the creek
	50-75%	
<u>XXX</u>	75-100%	Barren agricultural fields; eroded and washed-out areas on slopes and around drainages

3. CULTURAL BACKGROUND:

Background Research:

- XXX Oklahoma Archeological Survey (OAS) site files.
- XXX SHPO NRHP/DOE files.
- XXX Bridge Evaluations, including Spans of Time, WPA Study, Program Comment, etc.
- XXX Historic maps, aerial photographs, geology maps, etc.

An OAS file search was conducted by ODOT-CRP on behalf of Stantec in September 2021. No cemeteries are recorded within the study area or within one mile of the study area. There are no previously recorded sites within the study area. One previously recorded site is located within one mile of the study area.

Site 34LN180 is located approximately 1,000 feet (310 meters) east of the eastern section’s eastern terminus. Site 34LN180 is a historic farmstead recorded by McKay in 2009. The site was assessed as not eligible for listing in the NRHP.

One previous survey is recorded within the study area: ODOT BRO-014C(241) surveyed by Bartlett 7/12/00.

There are nine previous surveys recorded within one mile of the study area:

- ODOT JP28034(04) surveyed by ODOT 3/30/2017
- BRF-141C(207) Surveyed by Areary 6/16/09
- ODOT Bridge Repl. Surveyed by Mckay 6/25/09? BRFY-141B (286)
- ODOT Bridge Repl. SAB-141C(162) Surveyed by Oshea 1/4/07
- ODOT BRO-141D(144)CO Surveyed by O’Shea 12/23/05
- ODOT BRFY-041C (251) Surveyed by O’Shea & Neustocall 5/2/03
- ODOT BRF-041C (239) Surveyed by Bartlett 9/14/2000
- ODOT ERS JP-412(310) Surveyed by Bartlett 5/7/90
- ODOT BRY-0912(237) (date and surveyor unknown)

Other materials reviewed include the 1872 and 1893 General Land Office (GLO) Survey Plats; Luther 1:62,500 (1909), Oklahoma City 1:250,000 (1954, 1957, 1963), Wellston 1:24,000 (1966), and Oklahoma City North 1:100,000 (1990) topographic maps; aerial photographs (1964, 1981, 1985, 1995, 2003, 2005, 2006, 2010, 2011, 2012, 2014, and 2017); National Register of Historic Places (NRHP) and Determination of Eligibility (DOE) listings; Oklahoma Landmark Inventory (OLI); Oklahoma historic bridge inventories; Google Earth imagery; National Resource Conservation Service (NRCS) soil data viewed through the SoilWeb KMZ for Google Earth;

and United States Geological Service (USGS) geologic data viewed through the Oklahoma Geologic Database KMZ for Google Earth.

The current SH-66 roadway corridor is not depicted on the 1898 GLO Maps, nor are any structures shown (BLM 2022). The 1909 topographic map shows SH-66 and the town of Wellston and one building within the study area in Section 13, Township 14N, Range 2E. The subsequent 1954, 1957, and 1963 topographic maps do not depict individual buildings due to the large scale, but it does show the roadway within its current alignment. The 1966 topographic map shows seven buildings within the study area; of these, one is in the same position as the 1901 structure shown on the 1909 map (USGS 2022b and 2022c). The earliest aerial imagery dates to 1964. This aerial image largely matches the 1966 topographic map. Although some of the complexes appear to be destroyed and no longer remain (NETR 2022).

Stantec historians performed a desktop review for designated and historic-age (45 years of age, or older) resources within the study area. A review of the Oklahoma SHPO database for NRHP listed properties, DOE, and OLI was conducted. No such properties were identified within the study area (OHS 2022a; OHS 2022b, OHS 2022c). A former alignment of Route 66 is within the study area. A specific eligibility determination was not given for the two segments of the former alignment of Route 66 within the study area in the 2002 *Oklahoma Route 66 Roadbed Documentation Project (1926-1970)*. New methodology for assessing Route 66 resources in Oklahoma has been developed by Mead & Hunt as part of a statewide inventory and assessment of Route 66 in Oklahoma.

There are two bridges located within the study area. The bridge over Spring Creek (ODOT Structure # 4106 0702 X [NBI 12596]) is a concrete culvert constructed in 1951. This bridge is of the type discussed in the Program Comment for post-1945 concrete and steel bridges; however, because this bridge is on the Oklahoma Historic Route 66 Scenic Byway, it is an exception to the Program Comment and was documented. The bridge over Captain Creek (ODOT Structure # 4106 0372 X [NBI 26639]) is a steel stringer/multibeam/girder constructed in 2001. As this bridge is not yet 45 years of age, it was not documented.

Given the nature of the soils, underlying geology, topographic setting, OAS file search results, and setting, Stantec anticipated a low to moderate likelihood for intact prehistoric-age cultural resources within the study area. If present, prehistoric sites would be expected to be expressed as artifact scatters that could contain chipped lithic artifacts (e.g., flakes, cores, projectile points), groundstone, ceramics, faunal remains, and other materials associated with short- and long-term occupations.

4. METHODOLOGY:

Field Investigation Methodology:

Cultural resources fieldwork was completed in March 2022. The cultural resources fieldwork was developed in consultation with ODOT-CRP and conformed to the methods outlined in the ODOT-CRP manual (October 2017). Archeological survey transects were set at a distance not exceeding 30 meters (98.4 feet) apart, with two transects on each side of SH-66. Shovel tests were excavated at 30-meter (98.4-foot) intervals along each transect and were excavated in arbitrary 10-centimeter (4-inch) levels until reaching culturally sterile subsoil, inundated soils, gravel/root impasse, bedrock, or disturbances such that intact cultural deposits were unlikely. Because multiple soil series with moderate to high potential for buried A Horizons and/or deep profiles are mapped throughout large portions of the study area, bucket augers were used to extend every fourth shovel test (every 120 meters) in those areas.

Excavated matrix was screened through 0.25-inch (0.64-centimeter) hardware screen and deposits were described using conventional texture classifications with reference to NRCS soil series and Munsell color designations. All observations were recorded on handheld tablets using the ArcGIS Survey 123 (version 3.12.277) and the ArcGIS Collector (version 20.2.2) applications. All shovel test units were backfilled after recordation. Photographs were taken of the project area. Each photograph was recorded in digital format, and a photographic log was kept for all photographs. These logs include sequential identification listing the name of the photographer, date, subject, camera

orientation, photographic details, and other notes. All field photographs were captured with the subject in focus and with a camera capable of photographic resolution of a minimum of 10 megapixels.

A reconnaissance survey of the built environment was conducted of historic-age (built in or before 1977) buildings, structures, objects, sites, districts, and historic landscapes within the study area. Stantec historians photographed historic-age and associated non-historic resources and conducted desktop research to identify and contextualize the resources. Historians then documented identified historic-age resources on OK/SHPO HPRI and Oklahoma Historic Bridge Inventory forms according to the standards laid out in *Cultural Resource Studies: A Manual for Cultural Resources Staff and Department Consultants* (ODOT 2017).

5. RESULTS OF INVESTIGATION:

_____ No archeological sites or buildings recorded in study area.

XXX Resources recorded in study area assessed as **not eligible** for the NRHP. Forms being submitted for agency review.

XXX Oklahoma Archeological Site Survey Form(s) for State Archeologist files.

XXX Historic Preservation Resource Identification Form(s) for SHPO files.

_____ Oklahoma Bridge Survey and Inventory Form.

XXX **NRHP-eligible properties** recorded in study area.

Forms being submitted for agency review.

_____ Oklahoma Archeological Site Survey Form(s) for State Archeologist files.

XXX Historic Preservation Resource Identification Form(s) for SHPO files.

XXX Oklahoma Bridge Survey and Inventory Form.

_____ Archeological sites requiring further assessment (i.e. evaluative testing)

COMMENTS AND DESCRIPTION OF FINDINGS:

Cultural Resources Survey Results

Archeological fieldwork identified one new archeological site (34LN238), an early to mid-twentieth-century farmstead.

The built environment historic resources survey identified four building complexes (Building Complex 1, Building Complexes 3–5), one building (Building 2), and two segments of a former alignment of Route 66 (Structures 6 and 7), with a total of thirteen historic-age (built in 1977-6 or earlier) resources documented on HPRI forms. **Table 3** summarizes these results. One bridge (ODOT Structure # 4106 0702 X [NBI 12596]) was documented on the Oklahoma Historic Bridge Inventory form.

All identified resources are recommended **not eligible** for inclusion in the NRHP.

Archeology

State Highway 66 is a paved roadway flanked by agricultural fields, pastures, residential and commercial properties, wooded areas, and riparian vegetation around creeks (**Figure 1**). The study area extends through the SH-66 right-of-way and into surrounding livestock pastures, agricultural fields, forested areas, creek terraces, and developed properties. The easternmost portion of the western segment is within the town of Wellston. This area is the most

developed portion of the study area and consisted of residential and commercial properties, yards, driveways, and parking lots. Surface visibility ranged in these areas from zero to 25 percent (**Figure 3, Photo 1**). The remainder of the study area alternated between pastures and agricultural fields with patches of forest, thicket and briar patches, and a few buildings. Surface visibility was high in plowed agricultural fields ranging from 75 to 100 percent (**Figure 3, Photo 2**); and low in pastures, forested areas, and areas with scrub and thicket ranging from zero to 25 percent (**Figure 3, Photos 3, 4, and 5**).

Disturbances to the study area consisted of patches of developed areas, construction and maintenance disturbances, buried and overhead utilities, standing water, and drainage erosion (**Figure 3, Photos 6, 7 and 8**). Long term use of the land adjacent to the study area for agricultural activities has likely contributed to disturbances and erosion. The agricultural fields showed evidence of extensive plowing (**Figure 3, Photo 2**). Shovel tests were placed along two transects on either side of the roadway for a total of four transects (**Figure 2**). Wherever possible, shovel tests were placed outside of existing right-of-way where soils were less likely to have been impacted by previous roadway construction. A livestock pasture with large bulls was deemed unsafe and inaccessible for shovel testing. This area was subjected to visual inspection from outside of the fences (Figure 2e). Shovel tests were terminated at depths ranging from 7 to 70 cmbs at culturally sterile subsoil, inundated soils, gravel/root impasse, bedrock, or disturbances such that intact cultural deposits were unlikely. The average depth of shovel test termination was 39 cmbs.

Many soil profiles were observed in shovel testing, the most common profile was a dark red (2.5YR 3/6) silty or fine sandy clay loam extending on average to 25 cmbs. This was underlain by red (2.5YR 4/8) silt loam, silty clay loam or fine sandy loam. This profile does not fit the description of the most mapped soils within the study area, however, due to observed erosion, the upper horizons were likely not preserved and lower horizons were exposed.

Because multiple soil complexes mapped within the study area have moderate to high potential for buried A horizons and/or deep profiles, bucket augers were used to extend every fourth shovel test throughout those areas. Additional bucket auger units were placed around the creeks (**Figure 2**). In total, 63 bucket auger tests were excavated throughout the study area. Bucket auger units ranged in depth from 50 to 140 cmbs and were terminated at extremely compacted soils/root impasse, culturally sterile subsoil, inundated soils, and/or depth of auger. Soil profiles in bucket auger units were similar to those in the shovel tests. The most common soil profile consisted of dark red (2.5YR 3/6) or red (2.5YR 4/6) sandy clay loam underlain by dark reddish brown (2.5YR 3/6) loamy sand, sand, or sandy clay loam. Often there were no major changes in soil color or texture below 75 cmbs; however, eighteen shovel tests presented soils indicative of a buried A horizon. These auger tests reached a third horizon consisting of dark reddish brown (2.5YR 3/3, 3/4, or 3/6) clay, sandy clay, sandy clay loam or silty clay. No cultural resources were encountered in any of the bucket auger tests.

One historic-age archeological site was recorded during field investigations. No additional archeological resources were encountered either in subsurface testing or on the ground surface.

Newly Recorded Archeological Site - 34LN238

One historic-age archeological site (34LN238) was recorded during field investigations. Site 34LN238 is an early to mid-twentieth-century farmstead consisting of four surface features and a surficial and subsurface artifact scatter. This site is located along the south side of SH-66 approximately 0.35 miles (0.56 kilometers) west of SH-177, approximately 70 feet (21 meters) south of the SH-66 centerline (**Figure 2e**). The site measures approximately 790 feet (241 meters) north–south and 535 feet (163 meters) east–west and encompasses 8.1 acres (32,737 square meters). The site boundary was determined by the extent of positive shovel tests, surface features, the surface scatter of discarded farm equipment and building supplies, and the highway right-of-way. The site is situated on privately-owned property. Vegetation consists of sections of open forest, sumac thicket, tall grasses, and briar. Surface visibility was poor at 0 to 20 percent.

The site was initially identified in shovel test MC061; an additional 19 shovel tests were excavated to evaluate the extent of artifacts and potential for subsurface features (**Table 1**). Four surficial features were present: a metal separator structure (Feature 1; also recorded as Building 5A in the built environment survey), and three outbuildings (Features 2, 3, and 4; also recorded as Buildings 5B, 5C, and 5D in the built environment survey). Approximately 18 subsurface historic-age artifacts were observed during site recordation. Additionally, many objects were present

on the ground surface but were not inventoried as artifacts as the date of these items was not known and they could be modern. These included discarded farm equipment and equipment parts, scrap metal, and building materials.

Stantec excavated 20 shovel tests to delineate this site and 7 shovel tests were positive for cultural materials. All artifacts were found at depths between 5 and 30 cmbs. Additionally, farmstead building materials, machinery, and fence materials were present. Shovel tests ranged in depth from 26 cmbs to 50 cmbs. Shovel tests were terminated at culturally sterile subsoil or gravel/root impasse. Soils resembled the mapped Konawa soils series. Typical soil profiles consisted of a dark reddish brown (5YR 3/3 or 2.5YR 3/4), reddish brown (5YR 3/3 or 2.5YR 4/4) or dark brown (7.5YR 3/3) silty clay loam or fine sandy clay loam. This was underlain by red (2.5YR 4/8), yellowish red (5YR 4/6), or light red (2.5YR 6/6) silty clay loam or sandy clay loam. Some variation in color and texture was observed and likely due to disturbances.

Historic-age artifacts observed within shovel tests included window and bottle glass sherds including aquamarine and solarized glass, a milk glass lid, a ceramic sherd, metal fragments, a machine-made nail, and brick and concrete fragments.

Although the historic artifacts lacked specific temporal characteristics, they are generally indicative of an early to mid-twentieth century time period. Solarized or 'Amethyst' glass is the result of the exposure of colorless glass with manganese dioxide to sunlight, which can produce a purple color in a variety of intensities. Solarized glass generally dates to or prior to World War I (1915 to 1920), making it a good diagnostic material. Milkglass (opaline glass) was rarely used prior to 1870 but generally lacks further temporal diagnostic utility (SHA 2022). Aqua/Aquamarine glass was a common color from the early nineteenth century to the 1920s except for fruit jars, which continued into the 1930s, and soda glass, which continues today (particularly Coca Cola) (SHA 2022).

Review of historic aerials reveals the silos' (Feature 1) construction to be pre-1964. The earliest available aerial image (1964) shows a house immediately southwest of the silos and within the site boundary, which by the 2010 aerial is no longer extant (NETR 2022). The partially collapsed barn (Feature 2) is first present on the 1973 aerial image.

Due to the presence of features, deed research was conducted at the Lincoln County Clerk's office (**Table 2**). The site is shown to have had one landowner and a few tenants between the period of occupation suggested by the limited artifacts present. A review of ancestry and genealogy websites, oral interview transcripts, historic newspapers, and newspaper archives revealed no historically significant information about the landowners listed below.

This site is a common light scatter of historic-age artifacts with four associated historic-age surficial features. No associations could be established with historically important events or persons (Criteria A and B). It lacks features or structures with distinctive characteristics that could contribute to the type, period, or method of construction (Criterion C). While a subsurface artifact assemblage exists, all fieldwork observations indicate this is a light scatter and has low potential for deriving significant information contributing to our understanding of the regional history (Criteria D). Based on this information and pursuant to 36 CFR 60.4, site 34LN238 does not rise to the level necessary to convey any historic significance for NRHP eligibility under any criteria. Site 34LN238 is recommended as **not eligible** for inclusion in the NRHP.

Table 2. Deed Records

Grantor	Grantee	Type	Book/Page	Date
Commissioners of Land Office	John Goggin	Certificate	11/113	Prior to 1926*
John T. Goggin	L.P. King	Mortgage	46/332	Prior to 1926
John T. Goggin	Em Overman	Mortgage	50/325	Prior to 1926
Em Overman	John T. Goggin	Release	13/334	Prior to 1926
J.T. Goggin	Wayne Darlington	Lease	36/385	Prior to 1926
J.T. Goggin	The American Insurance Company	Mortgage	59/428	Prior to 1926
The American Insurance Company	J.T. Goggin	Release of Mortgage	21/436	2/15/1926
John T. and Margaret Goggin	Chas Schwartz	Lease	67/264	2/23/1926
John T. and Margaret Goggin	Roy Ward	Mortgage	74/218	2/2/1932
Roy Ward	John T. Goggin	Release of Mortgage	15/559	7/26/1937
Margaret Goggin	E.S. Haraway	Lease	136/389	9/25/1944
H.W. Goggin	Comm Land Office	Mortgage	393/314	10/26/1959
Comm Land Office	H.W. Goggin	Release Mortgage	941/832	3/2/1984
*Courthouse deed records listed no dates prior to 1926.				

Built Environment

The built environment historic resources survey identified four building complexes (Building Complex 1, Building Complexes 3–5), one building (Building 2), and two segments of a former alignment of Route 66 (Structures 6 and 7), with a total of thirteen historic-age (built in 1977 or earlier) resources documented on HPRI forms. **Table 3** summarizes these results. One bridge (ODOT Structure # 4106 0702 X [NBI 12596]) was documented on the Oklahoma Historic Bridge Inventory form.

Building Complex 1 includes a ca. 1970 commercial building (Building 1A), two non-historic-age ca. 2000 storage tanks (Structures 1B, 1C), and one ca. 2000 non-historic-age shed (Building 1D). Building 1A is a single-story, side-gabled commercial building with a metal-clad roof. It is clad in metal and has a rear, hipped addition, a single door, and tall, narrow, fixed, metal windows. Alterations include replacement cladding, roofing, the rear addition, and a replacement door. The parcel is currently operating as the business “Green’s Propane.”

Building 1A does not embody distinctive characteristics of a type, period, or method of construction, represent the work of a master, or possess high artistic value. Because Building 1A has undergone alterations, as described above, its integrity of materials, workmanship, and design have been diminished. No associations were identified linking Building 1A to persons or events of historic importance. Because Building 1A offers neither a local interpretation of a standard design nor an undocumented construction technique, it has limited potential to yield important information. As a result, Building 1A is recommended **not eligible** for the NRHP under Criterion A, B, C, or D.

Building 2 is a ca. 1970 single story, single-family Ranch style dwelling with a low-pitched, cross-gabled, metal-clad roof. It is clad in brick with wood siding in the gable-ends. It has single-hung and fixed vinyl windows, an attached garage, and a partial-width porch with a spindlework porch support. Alterations include replacement roof cladding, replacement windows, and replacement doors and garage doors. The dwelling is now surrounded by commercial development.

Building 2 does not embody distinctive characteristics of a type, period, or method of construction, represent the work of a master, or possess high artistic value. Because Building 2 has undergone alterations, as described above, its integrity of materials, workmanship, and setting have been diminished. No associations were identified linking Building 2 to persons or events of historic importance. Because Building 2 offers neither a local interpretation of a standard design nor an undocumented construction technique, it has limited potential to yield important information. As a result, Building 2 is recommended **not eligible** for the NRHP under Criterion A, B, C, or D.

Building Complex 3 includes a ca. 1975 dwelling turned office (Building 3A), a ca. 1975 shed (Building 3B), and a ca. 1975 shed or pump house (Building 3C). Building 3A is a single-story Ranch style dwelling that has been turned into an office for the nearby J&D Fabricators. It has a side-gabled, asphalt-clad roof and is clad in brick with vinyl siding in the gable-ends and the enclosed garage. It has single-hung vinyl windows and single door entrances, as well as a partial-width porch with spindlework supports. Alterations include the enclosed garage, some cladding replacement, window replacement, door replacement, and its change in use to a commercial office. Building 3B is a single-story, front-gabled shed that is clad and roofed in metal. It has a sliding door on its primary, south facade and a single-hung metal window. It appears to have entrances on its side facades, and a shed-roofed addition to the rear. The shed-roofed rear addition appears to be the only alteration. Building 3C is a single-story shed with a front-gabled, asphalt-clad roof that has a small, decorative, gabled vent-like roof feature. The shed is made of concrete blocks and has wood cladding in the gable-ends, and a wood door on its west facade. The building likely operates as a pump shed.

Buildings 3A, 3B, and 3C do not embody distinctive characteristics of a type, period, or method of construction, represent the work of a master, or possess high artistic value. Because Building 3A has undergone alterations, as outlined above, its integrity of materials, workmanship, design, feeling, and association have been diminished. No associations were identified linking Buildings 3A, 3B, and 3C to persons or events of historic importance. Because the resources offer neither a local interpretation of a standard design nor an undocumented construction technique, they have limited potential to yield important information. As a result, they are recommended **not eligible** for the NRHP under Criterion A, B, C, or D.

Building Complex 4 includes a ca. 1965 dwelling (Building 4A), a ca. 1960 shed (Building 4B), and a non-historic-age ca. 2000 carport (Building 4C). Building 4A is a single-story, single-family Ranch style dwelling with a side-gabled, metal-clad roof. It is clad in brick with wood siding in the gable-ends. The gable-ends each have a projecting hood. The dwelling has single-hung and fixed vinyl windows, a projecting partial-width porch, and a garage attached to the rear via an open hyphen. Alterations include replacement windows, replacement garage door, and replacement porch supports. Building 4B is a single-story shed with a front-gabled, asphalt-clad roof and double, wood doors. It is clad in wood siding and has vinyl siding in the gable-ends and sliding metal windows on the southern and eastern facades. Alterations include replacement vinyl siding, the removal and loss of other gable-end cladding, likely due to storm damage, and replacement windows and doors. Aerial photographs indicate Building 4B appears to be from a previous set of buildings on this parcel. The previous buildings appear to have been the approximate width of Building 4B and arranged in multiple rows. Building 4B was at the southern end of the westernmost row. The former use of the shed, or the now demolished buildings was not identified.

Buildings 4A and 4B do not embody distinctive characteristics of a type, period, or method of construction, represent the work of a master, or possess high artistic value. Because the resources have undergone alterations, as outlined above, their integrity of materials, workmanship, design, and setting have been diminished. No associations were identified linking Buildings 4A and 4B to persons or events of historic importance. Because the resources offer neither a local interpretation of a standard design nor an undocumented construction technique, they have limited potential to yield important information. As a result, they are recommended **not eligible** for the NRHP under Criterion A, B, C, or D.

Building Complex 5 includes a ca. 1960 separator (Structure 5A), a ca. 1970 outbuilding (Building 5B), a ca. 1975 outbuilding (Building 5C), and a ca. 1970 outbuilding (Building 5D). Building Complex 5's parcel has been occupied since at least 1909 and appears to have supported various forms of mineral extraction including oil and gravel pits (NETR 2022). Structure 5A is a ca. 1960 structure composed of three connected vertical metal cylinders that are made of bolted metal sheets. They likely operated as an oil separator, and resemble versions seen in *A Field Guide to Industrial Properties in Texas* (Dase 2003). The cylinders are no longer in use and are situated amongst a stand of young trees and encroaching vegetation. Building 5B (ca. 1970) and Building 5C (ca. 1975) are single-story outbuildings that are in a state of disrepair. They are wood frame construction and have metal roofing and cladding when present. Most cladding and roofing on both buildings is missing due to wind or storm damage. Aerial photographs indicate the eastern half of Building 5B has largely been removed or collapsed. The western portion of Building 5C also has collapsed. Building 5D is a ca. 1970 single-story outbuilding with a flat, metal roof. It appears to be clad in various types of metal sheets, and some areas exhibit the loss of cladding. There are window openings, but no windows remain. The north facade has a few open bays and an attached fence and gate that indicate a former livestock use for the outbuilding. There is a single, paneled wood door on the west facade.

Structure 5A, Buildings 5B, 5C, and 5D do not embody distinctive characteristics of a type, period, or method of construction, represent the work of a master, or possess high artistic value. Structure 5A has undergone alterations that have diminished its integrity of feeling, setting, and association due to vacancy and the removal of nearby buildings and the mineral extraction operation from the parcel. Buildings 5B, 5C, and 5D have undergone alterations that have diminished their integrity of materials, workmanship, design, setting, feeling, and association due to vacancy, vegetative encroachment, cladding replacement or removal, and general decay. Because the resources offer neither a local interpretation of a standard design nor an undocumented construction technique, they have limited potential to yield important information. As a result, they are recommended **not eligible** for the NRHP under Criterion A, B, C, or D.

Structure 6 is a segment of Route 66 that extends approximately 1.5 miles from approximately 730 ft west of Hayes Drive to approximately 630 ft west of Captain Creek. This segment of Route 66 was designed and built as a graded, but unpaved road at least as early as 1930 as a straight, direct route that bypassed the nearby town of Wellston, through which an alternate alignment of Route 66 passed (*Daily Oklahoman* 1930). Structure 6 is a portion of the full "Wellston Gap," segment, that bypassed Wellston. The Wellston Gap was not paved until 1933 (*Daily Ardmoreite* 1933) It is possibly an original (if unpaved) 1926 segment of Route 66. The alternate route through Wellston was also paved in 1933. Structure 6 is notable for the controversy of its construction and the removal of the U.S. 66 designation from the alignment through the town of Wellston. The Wellston alignment became known as S.H. 66 as the "Wellston Gap" became the alignment of U.S. 66 in 1933. Structure 6 is a two-lane roadway with asphalt overlays. This segment was paved with Portland concrete in 1933, likely at 20 ft wide. According to ODOT data, the segment was widened to 24 ft and overlaid with a 9-inch-thick asphalt surface in 1951 and asphalt overlays were reapplied on most of the segment in 1971, 1993, and 2003. There are no shoulders on the segment, except near the 2001 bridge over Captain creek and various driveways, entrances and exits.

Structure 6 has been resurfaced, widened, and undergone the addition of shoulders in some locations. The bridge over Captain Creek (NBI 26639) was constructed in 2001 and replaced the original camelback pony truss constructed in 1932 (NBI 03378) with the current steel beam structure. The initial widening of the roadway occurred during the newly established period of significance, 1926-1970. Most other alterations and the ca. 2001 bridge replacement occurred outside the period of significance. Additionally, this segment's early operation as an unpaved continuous bypass is considered particularly important to the Route 66 service, according to the new methodology. According to "Oklahoma Route 66 Roadbed Documentation Project (1926-1970): A Survey of Roadbed and Integral Structures", significant segments of Route 66 will contain elements of first-generation paving

or Portland Concrete (Oklahoma Route 66 Association 2002). No portion of the original paving is apparent in Structure 6. It is currently unknown what bearing the lack of the original paving material will have on the eligibility of this segment according to the new methodology. However, despite the replacement of the bridge over Captain Creek and the repaving, Structure 6 appears to retain sufficient integrity to convey its significance under Criterion A in the area of Transportation, as a contributing resource to a statewide linear historic district for its association with Route 66 and its service as a carrier of the route. No information was found to indicate Structure 6 would be eligible under Criterion B, C, or D. Structure 6 is recommended **eligible** for NRHP listing under Criterion A in the area of Transportation, as a contributing resource to a statewide linear historic district.

Structure 7 is a segment of Route 66 that extends approximately from the eastbound Turner Turnpike entrance to approximately 460 ft east of U.S. 177. This roadbed consists of two lanes of asphalt-paved roadway. This segment was first paved with asphalt in 1928, likely no wider than 20 ft. According to ODOT data, it was repaved in 9-inch thick asphalt and widened to 24 ft in 1951 with further asphalt overlays to much of the segment in 1971, 1993, and 2003. A portion of the segment near the eastern terminus added shoulders in 2015. The segment was part of a larger portion of Route 66 from Chandler to Wellston that was asphalt paved in 1928.

Structure 7 has been widened, resurfaced, and slightly redesigned, with the addition of shoulders and turn lanes at the roadway's junction with U.S. 177. The initial widening of the roadway and the insertion of the 1951 bridge occurred during the newly established period of significance, 1926-1970. Most other alterations occurred outside the period of significance. According to "Oklahoma Route 66 Roadbed Documentation Project (1926-1970): A Survey of Roadbed and Integral Structures", significant segments will contain elements of first-generation paving, which was generally Portland Concrete, but asphalt on this segment. The segment has a documented continued use of asphalt on the roadbed; however, it is unknown what bearing this will have on the eligibility of the segment according to the new methodology. Structure 7 appears to retain sufficient integrity to convey its significance under Criterion A in the area of Transportation as a contributing resource to a statewide linear historic district for its association with Route 66 and its service as a carrier of the route. The 1951 bridge on Structure 7 was inserted during the POS and is itself considered eligible for the NRHP and does not negatively impact Structure 7's eligibility. Therefore, Structure 7 is recommended **eligible** for NRHP listing. No information was found to indicate Structure 7 is eligible under Criterion B, C, or D.

The bridge over Spring Creek (ODOT Structure # 4106 0702 X [NBI 12596]) is a concrete culvert constructed in 1951. This bridge is of the type discussed in the Program Comment for post-1945 concrete and steel bridges; however, because this bridge is on the Oklahoma Historic Route 66 Scenic Byway, the structure is an exception to the Program Comment. The structure was documented and evaluated, and documentation was submitted to the ODOT Cultural Resources Program (ODOT-CRP).

NBI 12596 is a reinforced concrete box culvert (RCB) constructed of concrete with concrete wingwalls and concrete solid piers. The RCB has an attached concrete parapet guardrail that runs the full length of the RCB's three cells. A metal guardrail is attached to the concrete parapet and stretches beyond the bridge along the approach roadway on both sides. The three cells of the RCB are of equal dimensions but are skewed southwest-northeast against the east-west SH 66 roadway above it. The concrete parapet and the metal guardrails are later additions to the RCB.

According to Mead & Hunt's *Oklahoma Historic Bridge Update for Metal Truss, Masonry Arch, and Concrete Arch Bridges Constructed through 1980*, standard designs for culverts were in use by the Oklahoma Highway Department as early as 1914 and their use was commonplace (Mead & Hunt 2021). The bridge-class culvert is undistinguished from an engineering or architectural perspective, lacks individual distinction or significance, and is commonplace. It is not an important example of a type, period, or method of construction, does not represent the work of a master, and does not possess high artistic value. The bridge-class culvert offers neither a local interpretation of a standard design nor an undocumented construction technique. It has limited potential to yield important information. The bridge, as built in 1951, falls within the newly established POS, 1926–1970 for the Route 66 in Oklahoma. The alterations to the bridge, which may fall outside the POS do not detract from its character-defining features, which is the reinforced concrete box superstructure. Therefore, the bridge would be eligible under Criterion A in the area of Transportation as a contributing resource to the Route 66 statewide linear historic district for its association with Route 66 and its service as a carrier of the route during the POS, 1926–1970. Therefore NBI 12596 is recommended **eligible** for NRHP listing under Criterion A as a contributing resource as

outlined above. The bridge is not individually eligible for its association with Route 66 or under any other criteria.

Table 3: Historic-Age Resources of the Built Environment

Resource Name/ID	Address/Parcel	Date of Construction	Stylistic Influence	NRHP Recommendation
Building Complex 1	3352 SH 66, Wellston, Lincoln County, OK			
Commercial Building/Building 1A		ca. 1970	No style	Not eligible
Building 2	504 SH 66 Wellston, Lincoln County, OK	ca. 1970	Ranch style	Not eligible
Building Complex 3	N Side SH 66, approx. 960 ft W of N 3350 Rd, Wellston, Lincoln County, OK			
Office/ Building 3A		ca. 1975	Ranch style	Not eligible
Shed/ Building 3B		ca. 1975	No style	Not eligible
Water shed/ Building 3C		ca. 1975	No style	Not eligible
Building Complex 4	335030 SH 66, Wellston, Lincoln County, OK			
Dwelling / Building 4A		ca. 1965	Ranch style	Not eligible
Shed/ Building 4B		ca. 1960	No style	Not eligible
Building Complex 5	South Side of SH 66, approx. 2235 ft W of US 177, Wellston vicinity, Lincoln County, OK			
Separator/ Structure 5A		ca. 1960	No style	Not eligible
Outbuilding/ Building 5B		ca. 1970	No style	Not eligible
Outbuilding/ Building 5C		ca. 1975	No style	Not eligible
Outbuilding/ Building 5D		ca. 1970	No style	Not eligible
Structure 6	SH 66, approx. 730 ft W of Hayes Dr to approx. 630 ft W of Captain Creek, Wellston, Lincoln County, OK	1933	No style	Eligible under Criterion A– contributing resource
Structure 7	SH 66, Turnpike entrance to approx. 460 ft E of US 177, Wellston vicinity, Lincoln County, OK	1928	No style	Eligible under Criterion A – contributing resource
ODOT Structure # 4106 0702 X [NBI 12596]	7 miles E of Oklahoma County Line, Lincoln County, OK	1951	RCB	Eligible under Criterion A – contributing resource

6. RECOMMENDATIONS:

XXX **Plan Notes** requiring avoidance of cultural resources in off-project areas

_____ **Approval Recommended** with the proposed project as planned with no additional research. If subsurface archaeological materials are exposed during construction, the Contractor and Resident Engineer shall notify the Department Archaeologist in accordance with Section 202.04(a), Standard Specifications for Highway Construction.

XXX **Approval NOT Recommended**, until one or more of the following measures are completed.

_____ XXX **Additional consultation with SHPO** regarding NRHP-eligible Properties

_____ **Revise design** to avoid/protect resources

_____ **NRHP Eligibility Archaeological Test Excavations**

_____ **Implementation of MOA** with SHPO regarding Mitigation of Adverse Effects to Historic Properties

SUMMARY AND COMMENTS REGARDING RECOMMENDATIONS:

The study area was subjected to an archeological survey that included the excavation of shovel test units, 63 bucket auger units, and pedestrian survey along both sides of the SH-66 roadway. One historic-age archeological site was recorded. This site is recommended **not eligible** for inclusion in the NRHP.

The study area was subjected to a built environment historical resources survey. A built environment survey resulted in the recordation of four building complexes (Building Complex 1, Building Complex 3–5), one building (Building 2), and two segments of historic Route 66 (Structures 6 and 7) for a total of thirteen historic-age resources (built in 1977 or earlier). Building Complex 1, Building 2, and Building Complexes 3–5 do not embody distinctive characteristics of a type, period, or method of construction, represent the work of a master, or possess high artistic value. No associations were identified linking them to persons or events of historic importance. Because the resources offer neither a local interpretation of a standard design nor an undocumented construction technique, they have limited potential to yield important information. As a result, Building Complex 1, Building 2, Building Complexes 3, 4, and 5 are recommended **not eligible** for the NRHP under Criterion A, B, C, or D. Structures 6 and 7 have an association with Route 66 and, according to newly established methodology from Mead & Hunt, retain sufficient integrity to convey their significance. Structure 6 and 7 are recommended **eligible** under Criterion A in the area of Transportation as a contributing resource to a statewide linear historic district for their association with Route 66 and their service as a carrier of the route during the POS, 1926–1970.

To avoid non-NRHP-assessed cultural resources during off-project activities such as fill borrowing, it is recommended that the following areas be avoided:

T14N R3E
Section 20 NW/NE/NW

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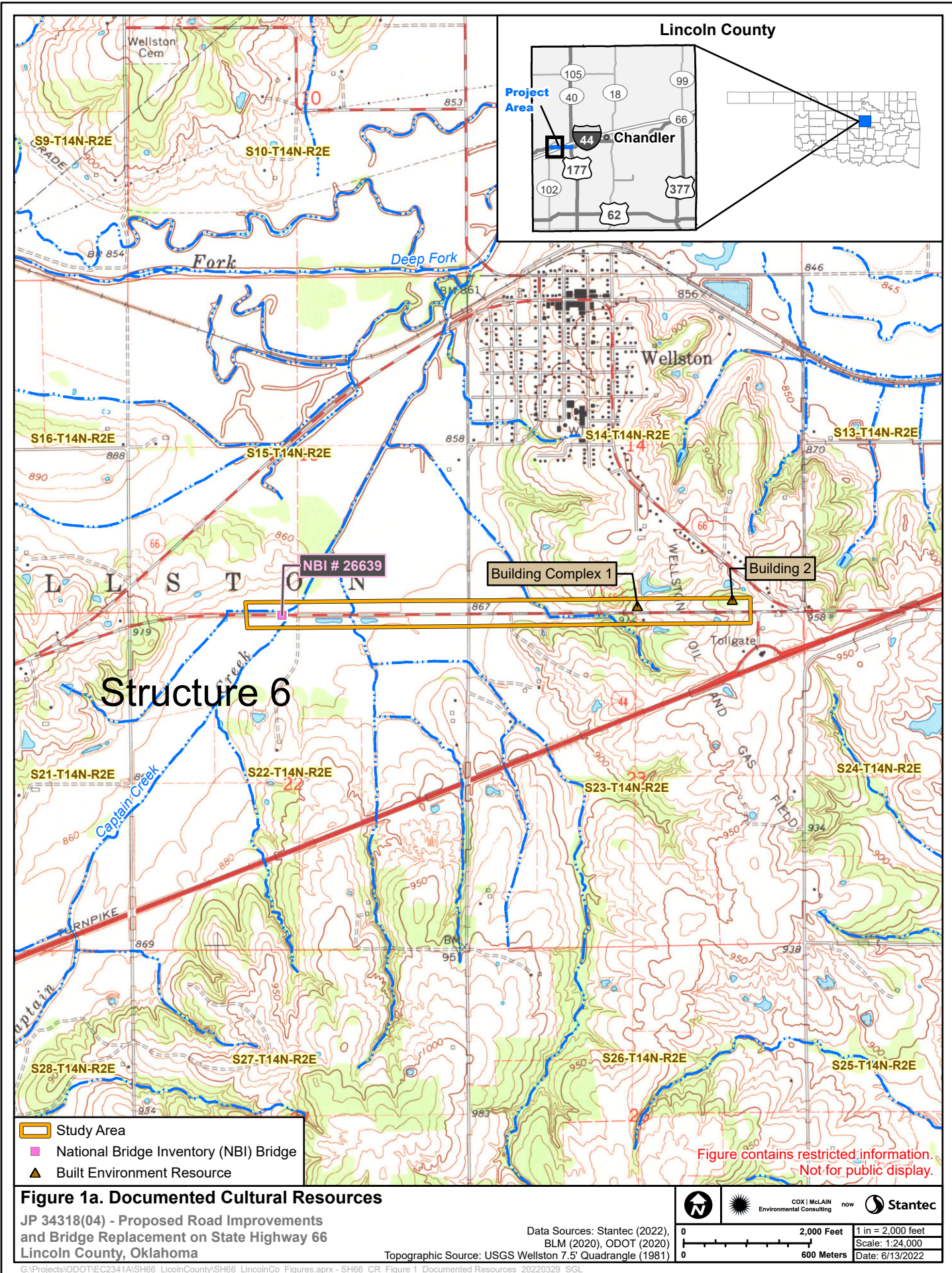
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Structure 6



Figure contains restricted information.
Not for public display.

Figure 1a. Documented Cultural Resources

JP 34318(04) - Proposed Road Improvements and Bridge Replacement on State Highway 66
Lincoln County, Oklahoma

Data Sources: Stantec (2022),
BLM (2020), ODOT (2020)

Topographic Source: USGS Wellston 7.5' Quadrangle (1981)

0 2,000 Feet 1 in = 2,000 feet
 0 600 Meters Scale: 1:24,000
 Date: 6/13/2022

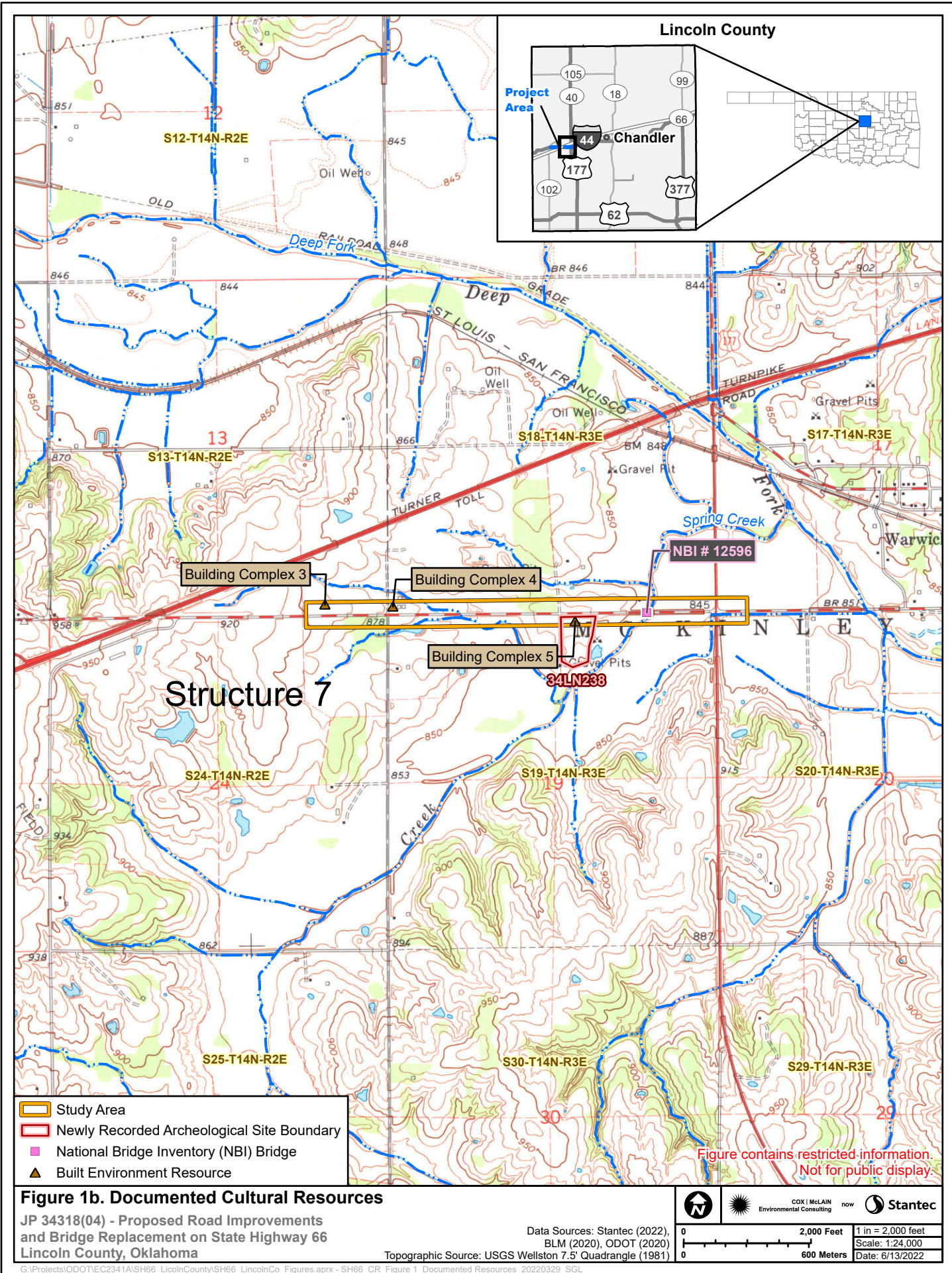




Figure contains restricted information.
Not for public display.

Figure 1b. Documented Cultural Resources

JP 34318(04) - Proposed Road Improvements and Bridge Replacement on State Highway 66
Lincoln County, Oklahoma

Topographic Source: USGS Wellston 7.5' Quadrangle (1981)



 COX | McLAIN
Environmental Consulting

Data Sources: Stantec (2022),
BLM (2020), ODOT (2020)
 Scale: 1:24,000
 Date: 6/13/2022

0 2,000 Feet 1 in = 2,000 feet
 0 600 Meters

December 19, 2022

To: Environmental Programs Division

From: Rhonda S. Fair, Director – Tribal Coordination

Re: Summary of tribal consultation for Lincoln County JP# 34318(04) - Improvements (widen and resurface) and bridge replacement on State Highway 66 over Captain Creek, from State Highway 102 east to U.S. 177

A file search conducted on 5/18/2022 found no known potentially sensitive areas in the project area or its vicinity.

ODOT consulted the following Tribal Nations on 1/18/2022 and 11/9/2022:

- Iowa Tribe of Oklahoma
- Kickapoo Tribe of Oklahoma
- Osage Nation
- Sac and Fox Nation
- Wichita and Affiliated Tribes

ODOT received the following comments:

- Iowa Tribe of Oklahoma: No comment or objections on the proposed undertaking at this time (11/28/2022).

Statement of possible tribal impacts:

- This project lies within the Oklahoma Statistical Tribal Area of the Kickapoo Tribe of Oklahoma.
- Based on the information provided in the Request for Specialists Studies, the area of potential effect DOES NOT involve tribal trust land, individual Indian trust land, restricted land, or tribally owned fee land.
- No known tribal facilities, such as housing authority properties, tribal offices, or tribal businesses, are located within or adjacent to the project area.

Oklahoma Transportation
200 N.E 21st street
Oklahoma city, Ok 73105
Attn: Rhonda Fair
rfair@odot.org



Date: November 28, 2022

Regarding: **Job Piece 34318(04)**

Request MOU or MOA

Copy of SHPO Report.

I concur with (OSA) Office State Archaeologist

Site Visit/Have Concerns May wish to monitor ground disturbance

No interest in the area geographically

No Comment or Objections on the proposed undertaking at this time.

No objections to the project as proposed. Concur with SHPO

If human skeletal remains and/or any objects falling under NAGPRA are uncovered during construction
Please stop immediately and notify this office.

Sincerely,

Candace Pershall,
THPO
335588 E. 750 rd
Perkins Ok 74059
Iowa Tribe of Oklahoma 405-547-2402 Ext: 3502 cpershall@iowanation.org

November 9, 2022

Iowa Tribe of Oklahoma
Attn: Chairman Perri Ahhaitty
335588 East 750 Road
Perkins, OK 74059

Dear Chairman Ahhaitty:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is conducting Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking.

County	Lincoln	Job Piece #	34318(04)	Anticipated Let Date	2028
Project description	Improvements (widen and resurface) and bridge replacement on State Highway 66 over Captain Creek, from State Highway 102 east to U.S. 177				

In accordance with 36 CFR Part 800.4, the area of potential effect (APE) was surveyed for cultural resources in order to identify historic properties that may be affected by the undertaking. A copy of this report is enclosed.

This investigation documented one new archaeological site and five buildings or building complexes, all of which we find not eligible for the National Register of Historic Places. Additionally, this investigation documented two segments of State Highway 66 (including one bridge) which are considered NRHP-eligible under Criterion A, but which will not be adversely affected by the proposed undertaking. Ongoing tribal consultation has not identified any areas of concern. Pursuant to 36 CFR 800.5(b), our determination is that the project, as proposed, will have no adverse effect to historic properties.

If this undertaking may affect properties of religious and cultural significance to your tribe or tribal trust land, please notify me as soon as possible. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517.5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Historic Preservation Office

January 18, 2022

Iowa Tribe of Oklahoma
Attn: Chairman Edgar Kent
335588 East 750 Road
Perkins, OK 74059

Dear Chairman Kent:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is initiating Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking:

County	Lincoln	Job Piece #	34318(04)	Anticipated Let Date	2029
Project description	Improvements (widen and resurface) and bridge replacement on State Highway 66 over Captain Creek, from State Highway 102 east to U.S. 177				
Location	Sec 17, 18, 19, & 20 T14N R3E and Sec 13, 14, 15, 22, 23, & 24 T14N R2E. See enclosed map.				
Additional information	This project is on a new alignment: <input type="checkbox"/> yes <input checked="" type="checkbox"/> no This project will require new or temporary right of way: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no This project involves ground disturbance: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no				

If this undertaking may affect burials, cemeteries, or properties of religious and cultural significance to your tribe, please notify me as soon as possible. Likewise, if this undertaking occurs on land held in trust for the tribe and the tribe has 101(d)(2) status from the National Park Service, please make this office aware of the location of the trust property. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

The area of potential effect (APE) will be subject to a cultural resources survey. The goal of this survey is to make a reasonable and good faith effort to identify historic properties within the APE, in accordance with 36 CFR Part 800.4. The survey will be performed in consultation with the Oklahoma State Historic Preservation Office and other consulting parties as appropriate. You will be provided a copy of the cultural resources report upon its completion.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517.5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Historic Preservation Office

November 9, 2022

Kickapoo Tribe of Oklahoma
Attn: Chairman Darwin Kaskaske
P.O. Box 70
McCloud, OK 74851

Dear Chairman Kaskaske:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is conducting Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking.

County	Lincoln	Job Piece #	34318(04)	Anticipated Let Date	2028
Project description	Improvements (widen and resurface) and bridge replacement on State Highway 66 over Captain Creek, from State Highway 102 east to U.S. 177				

In accordance with 36 CFR Part 800.4, the area of potential effect (APE) was surveyed for cultural resources in order to identify historic properties that may be affected by the undertaking. A copy of this report is enclosed.

This investigation documented one new archaeological site and five buildings or building complexes, all of which we find not eligible for the National Register of Historic Places. Additionally, this investigation documented two segments of State Highway 66 (including one bridge) which are considered NRHP-eligible under Criterion A, but which will not be adversely affected by the proposed undertaking. Ongoing tribal consultation has not identified any areas of concern. Pursuant to 36 CFR 800.5(b), our determination is that the project, as proposed, will have no adverse effect to historic properties.

If this undertaking may affect properties of religious and cultural significance to your tribe or tribal trust land, please notify me as soon as possible. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517.5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Pamela Wesley

January 18, 2022

Kickapoo Tribe of Oklahoma
Attn: Chairman David Pacheco, Jr.
P.O. Box 70
McCloud, OK 74851

Dear Chairman Pacheco:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is initiating Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking:

County	Lincoln	Job Piece #	34318(04)	Anticipated Let Date	2029
Project description	Improvements (widen and resurface) and bridge replacement on State Highway 66 over Captain Creek, from State Highway 102 east to U.S. 177				
Location	Sec 17, 18, 19, & 20 T14N R3E and Sec 13, 14, 15, 22, 23, & 24 T14N R2E. See enclosed map.				
Additional information	This project is on a new alignment: <input type="checkbox"/> yes <input checked="" type="checkbox"/> no This project will require new or temporary right of way: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no This project involves ground disturbance: <input checked="" type="checkbox"/> yes <input type="checkbox"/> no				

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If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517.5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Historic Preservation Office

November 9, 2022

Osage Nation
Attn: Principal Chief Geoffrey Standing Bear
627 Grandview
Pawhuska, OK 74056

Dear Principal Chief Standing Bear:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is conducting Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking.

County	Lincoln	Job Piece #	34318(04)	Anticipated Let Date	2028
Project description	Improvements (widen and resurface) and bridge replacement on State Highway 66 over Captain Creek, from State Highway 102 east to U.S. 177				

In accordance with 36 CFR Part 800.4, the area of potential effect (APE) was surveyed for cultural resources in order to identify historic properties that may be affected by the undertaking. A copy of this report is enclosed.

This investigation documented one new archaeological site and five buildings or building complexes, all of which we find not eligible for the National Register of Historic Places. Additionally, this investigation documented two segments of State Highway 66 (including one bridge) which are considered NRHP-eligible under Criterion A, but which will not be adversely affected by the proposed undertaking. Ongoing tribal consultation has not identified any areas of concern. Pursuant to 36 CFR 800.5(b), our determination is that the project, as proposed, will have no adverse effect to historic properties.

If this undertaking may affect properties of religious and cultural significance to your tribe or tribal trust land, please notify me as soon as possible. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517.5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Andrea Hunter, THPO

January 18, 2022

Osage Nation
Attn: Principal Chief Geoffrey Standing Bear
627 Grandview
Pawhuska, OK 74056

Dear Principal Chief Standing Bear:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is initiating Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking:

County	Lincoln	Job Piece #	34318(04)	Anticipated Let Date	2029
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If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517.5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Andrea Hunter, THPO

November 9, 2022

Sac and Fox Nation
Attn: Chief Don Abney
920963 State Highway 99
Stroud, OK 74079

Dear Chief Abney:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is conducting Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking.

County	Lincoln	Job Piece #	34318(04)	Anticipated Let Date	2028
Project description	Improvements (widen and resurface) and bridge replacement on State Highway 66 over Captain Creek, from State Highway 102 east to U.S. 177				

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This investigation documented one new archaeological site and five buildings or building complexes, all of which we find not eligible for the National Register of Historic Places. Additionally, this investigation documented two segments of State Highway 66 (including one bridge) which are considered NRHP-eligible under Criterion A, but which will not be adversely affected by the proposed undertaking. Ongoing tribal consultation has not identified any areas of concern. Pursuant to 36 CFR 800.5(b), our determination is that the project, as proposed, will have no adverse effect to historic properties.

If this undertaking may affect properties of religious and cultural significance to your tribe or tribal trust land, please notify me as soon as possible. In order to provide the most thorough consideration of these properties in the planning process, we appreciate receiving your response to this request within 30 days. Please rest assured that we will respect your wishes regarding the confidentiality of any information that you provide.

If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517.5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Chris Boyd

January 18, 2022

Sac & Fox Nation
Attn: Chief Justin Wood
920963 State Highway 99
Stroud, OK 74079

Dear Chief Wood:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is initiating Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking:

County	Lincoln	Job Piece #	34318(04)	Anticipated Let Date	2029
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If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517.5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Chris Boyd

November 9, 2022

Wichita and Affiliated Tribes
Attn: President Terri Parton
P.O. Box 729
Anadarko, OK 73005

Dear President Parton:

Pursuant to 36 CFR Part 800.2(c)(2), the Oklahoma Department of Transportation is conducting Section 106 consultation on behalf of the Federal Highway Administration regarding historic properties that may be affected by the following Federal-Aid undertaking.

County	Lincoln	Job Piece #	34318(04)	Anticipated Let Date	2028
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If you have any questions or would like to meet regarding this project, please contact me by telephone at 405.517.5670 or email at rfair@odot.org.

Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc: Gary McAdams, THPO

January 18, 2022

Wichita & Affiliated Tribes
Attn: President Terri Parton
P.O. Box 729
Anadarko, OK 73005

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Sincerely,



Rhonda S. Fair, Ph.D.
Director - Tribal Coordination

cc:

BIOLOGICAL STUDIES

BIOLOGICAL STUDIES TRACKING FORM

NEPA Project Manager	Geoff Canty / Amanda Alexander
State or Local Government Project	State
USFWS Project Code #	2022-0029790
Original IPaC List	4/8/2022
Email used to request IpaC official species list	Leah@ccenviro.net
Last Updated Species List Date	Click here to enter a date.
ROW	2025
Let Date	2029
90 Day Prior to Let IpaC List	Click here to enter a date.
Duration expected	Click here to enter text.
Original Biological Assessment and Waters and Wetlands Report Prepared By:	CC Environmental
Most Recent Field Date:	3/28/2022
Original Report Date:	4/9/2022
USFWS Consultation Submittal:	Monarch only
USFWS Concurrence:	Not required for Candidate species
Original Tracking Form Prepared by:	Elizabeth Nichols
Original Tracking Form date:	4/21/2022
Update Reason	Click here to enter text.
Amended USFWS Consultation Submittal:	Click here to enter a date.
Amended USFWS Concurrence:	Click here to enter a date.
Tracking Form Updated By Whom:	Click here to enter text.
Tracking Form Updated Date:	Click here to enter a date.
<i>ADD MORE LINES AS NEEDED FOR EACH TIME PROJECT IS UPDATED</i>	

Form Date: February 2022

Project Name from Oracle

Widen, Resurface & Bridge SH-66: from SH-102, east to US-177

Project Description

Widen and Resurface Existing Lanes

Check if any of the following is expected as part of the proposed action

- Work within the OHWM is expected
- Project is OFF-SET alignment
- Project is NEW alignment
- Project involves **NO OFF EXISTING PAVEMENT** work
- Project requires new ROW (permanent &/or temporary)

2. FEDERALLY LISTED SPECIES AND DESIGNATED CRITICAL HABITAT

Species	Listing Status	IPaC	Effect Determination for IPaC listed species
		Check if Yes	
Red-cockaded Woodpecker	Endangered	<input type="checkbox"/>	Choose an item.
Whooping Crane	Endangered	<input checked="" type="checkbox"/>	No Effect
Gray Bat	Endangered	<input type="checkbox"/>	Choose an item.
Indiana Bat	Endangered	<input type="checkbox"/>	Choose an item.
Ozark Big-eared Bat	Endangered	<input type="checkbox"/>	Choose an item.
Peppered Chub	Endangered	<input type="checkbox"/>	Choose an item.
Neosho Mucket	Endangered	<input type="checkbox"/>	Choose an item.
Ouachita Rock Pocketbook	Endangered	<input type="checkbox"/>	Choose an item.
Scaleshell Mussel	Endangered	<input type="checkbox"/>	Choose an item.
Winged Mapleleaf	Endangered	<input type="checkbox"/>	Choose an item.
Harperella	Endangered	<input type="checkbox"/>	Choose an item.
American Burying Beetle	Threatened	<input type="checkbox"/>	Choose an item.
Eastern Black Rail	Threatened	<input type="checkbox"/>	Choose an item.
Piping Plover	Threatened	<input checked="" type="checkbox"/>	No Effect
Red Knot	Threatened	<input checked="" type="checkbox"/>	No Effect
Northern Long-eared Bat	Threatened	<input type="checkbox"/>	Choose an item
Arkansas River Shiner	Threatened	<input type="checkbox"/>	Choose an item.
Leopard Darter	Threatened	<input type="checkbox"/>	Choose an item.
Neosho Madtom	Threatened	<input type="checkbox"/>	Choose an item.
Ozark Cavefish	Threatened	<input type="checkbox"/>	Choose an item.
American Alligator	Threatened	<input type="checkbox"/>	Choose an item.
Rabbitsfoot Mussel	Threatened	<input type="checkbox"/>	Choose an item.
Alligator Snapping Turtle	Proposed	<input type="checkbox"/>	Choose an item.
Monarch Butterfly	Candidate	<input checked="" type="checkbox"/>	Not likely to jeopardize the continued existence
Rattlesnake-master Borer Moth	Candidate	<input type="checkbox"/>	Choose an item.
Whooping Crane Critical Habitat	Designated	<input type="checkbox"/>	Choose an item.
Peppered Chub Critical Habitat	Designated	<input type="checkbox"/>	Choose an item.
Arkansas River Shiner Critical Habitat	Designated	<input type="checkbox"/>	Choose an item.
Leopard Darter Critical Habitat	Designated	<input type="checkbox"/>	Choose an item.
Neosho Mucket Critical Habitat	Designated	<input type="checkbox"/>	Choose an item.
Rabbitsfoot Critical Habitat	Designated	<input type="checkbox"/>	Choose an item.

	NEPA Footprint	Construction Footprint
Number of acres within the NEPA Study Footprint & Construction Footprint (if known)	137.3	Click here to enter text.

Presence of milkweed and nectar plants	YES
Bald Eagle Assessment	May impact
Migratory Bird Assessment of Transportation Structures	Migratory birds found nesting on transportation structures
Migratory Bird Impacts	nesting habitat for migratory birds will be impacted
Birds of Conservation Concern	No BCC listed
Interior Least Tern (MBTA)	No habitat

<u>Species (choose those that apply)</u>	<u>Seasonal Restriction Period</u>
Bald Eagle	September 16 – May 31
Migratory Birds: Swallows and Phoebe (NESTS PRESENT)	March 1 – August 31

Conservation Commitments

ODOT Commitment: All operators, employees, and contractors will be made aware of all environmental commitments, including the following Plan Notes.

Monarch Commitment: ODOT, as a Certificate of Inclusion partner in the Nationwide Monarch Butterfly CCAA for Energy and Transportation lands, will adhere to the conservation measures, as well as minimize threats to the monarch butterfly as stipulated in this CCAA.

Species Plan Notes

Non-Compliance: Failure to implement the commitments specified in the Plan Notes can result in non-compliance issues on the project. Work activities may be suspended on the project, for an undetermined duration, while working with regulators to bring the project back into compliance. The contractor will not be compensated for time lost.

Water Quality Conservation: Appropriate Best Management Practices to minimize impacts from storm water discharges and sedimentation in streams, as established by the Oklahoma Department of Environmental Quality, shall be conscientiously implemented throughout the proposed construction periods, in order to minimize any potential impacts to any listed species. The effectiveness of erosion controls shall be maintained for the duration of construction activities. Hazardous materials, chemicals, fuels, lubricating oils, and other such substances shall be stored at least 100 feet outside of the ordinary high water mark (OHWM). Refueling of construction equipment shall also be conducted at least 100 feet from the OHWMs. Sediment and erosion controls shall be installed around staging areas to prohibit discharge of materials from these sites. Construction waste materials and debris shall be stockpiled at least 25 feet outside of the OHWMs, and these materials shall be removed and disposed of properly following completion of the project. Preventative measure must be taken to prohibit the discharge of contaminants into any surface waters.

Bald Eagle Note: Suitable nesting, roosting or foraging habitat for the Bald Eagle occurs within the project’s action area. The Bald Eagle nesting season in Oklahoma extends from September 16, through May 31. The Resident Engineer shall contact the ODOT Biologist to schedule a nest survey. Nest search surveys can only be conducted when leaves are not on the trees typically between December 1st and February 28th. No work may occur within suitable Bald Eagle habitat, located the full extent of the project area, during the nesting season (September 16, through May 31) until the completion of the survey by the ODOT Biologist. If nests are observed, a no-work buffer up to a distance of 660 feet shall be placed around the nest. The exact distance of the buffer zone shall be established by the ODOT Biologist in consultation with US Fish and Wildlife Services. If the buffer cannot be maintained, all clearing, external construction

and landscaping activities, within the buffer, shall be conducted between June 1 and September 15 (outside the nesting season).

Migratory Bird Note: Migratory birds are protected by the federal Migratory Bird Treaty Act. Many birds commonly use bridges and culverts for nesting. The nesting season for most migratory bird species extends from March 1 to August 31. Migratory bird nesting use of the Captain Creek and Spring Creek bridges (NBI:26639 and 12596) and three culverts at (35.680977°, -97.066135°), (35.680983°, -97.063251°) and (35.680865°, -97.017213°) was observed. Painting, repair, retrofit, rehabilitation or demolition of the existing bridges and culverts shall be conducted between September 1, and February 28, when migratory bird nests are not occupied. If painting, repair, retrofit, rehabilitation or demolition cannot be completed between September 1 and February 28, the bridges and culverts shall be protected from new nest establishment prior to March 1, by means that do not result in bird death or injury. Options include the exclusion of adult birds from suitable nest sites on or within a structure by the placement of weather-resistant polypropylene netting with 0.25-inch or smaller openings, prior to March 1. Methods other than netting must be pre-approved by the ODOT Biologist.

Although no nests were observed on all other structures, the birds may occupy the structures in the future. The Resident Engineer shall contact the ODOT Biologist if any bird use of these structures is observed. If birds are observed then painting, repair, retrofit, rehabilitation or demolition of the existing bridges and culverts shall be conducted between September 1, and February 28 (when migratory bird nests are not occupied).

Waters and Wetlands Delineation Status

Original delineation

Wetlands and Ponds

Total Number of Sites	Water Body Type	Potential Jurisdiction Status	Acres within the NEPA Footprint
1	Herbaceous Wetland	Unlikely Jurisdictional	0.016
1	Forested Wetland	Likely Jurisdictional	0.450
Total Wetlands			0.466
2	Pond	Likely Jurisdictional	0.370

Streams and Drainages

Total Number of sites	Water body name	USGS Designation	Potential Jurisdictional Status	Acres within the NEPA Footprint	Liner Feet within the NEPA Footprint
1	Captain Creek	mapped perennial	Likely Jurisdictional	0.210	487
1	Spring Creek	mapped intermittent	Likely Jurisdictional	0.250	550
5	Tributaries to Captain Creek, Spring Creek, Deep Fork and other streams	mapped intermittent	Likely Jurisdictional	0.941	4,252
1	drainage	unmapped ephemeral drainages	Likely Jurisdictional	0.009	202
Total Likely Jurisdictional				1.41	5,491

Lincoln JP 34318(04)

Total Number of sites	Water body name	USGS Designation	Potential Jurisdictional Status	Acres within the NEPA Footprint	Liner Feet within the NEPA Footprint
1	Tributary to Captain Creek	mapped intermittent	Unlikely Jurisdictional	0.048	661

**ENDANGERED, THREATENED AND CANDIDATE SPECIES, DESIGNATED
CRITICAL HABITAT, BALD EAGLE AND MIGRATORY BIRD ASSESSMENTS**

For

USFWS TAILS #	2022-0029790				
Email used to request IPaC official species list	Leah@ccenviro.net				
County	Lincoln	JP Number	34318(04)	Project Number	J3-4318(004)
Road Number	SH-66	Water Body Name	Captain Creek and Spring Creek and unnamed tributaries		
ROW Date	2025	Let Date	2029	Project Length	~2.8 miles
Project General Location	SH-66 south of Wellston, OK between SH-102 and US-177 (includes an approximately 1-mile turnpike project exception)				
Project Description & Statement From Oracle	Widen, Resurface & Bridge SH-66: from SH-102, east to US-177				

Prepared for:
Oklahoma Department of Transportation
Environmental Programs Division
200 NE 21st Street
Oklahoma City, OK 73105

Prepared by:

Biologist Name	Leah M Peterson and Dale W Daniel
Company/Agency Name	CC Environmental, LLC
Address	P.O. Box 1292
City, State Zip	Norman, OK 73070

Report Date:	April 9, 2022
Field Survey Date	March 28, 2022
Field Survey Biologist(s)	Dale Daniel

1. PROJECT OVERVIEW

1.1 Federal Nexus

This biological assessment, prepared by the above named Company/Agency for the Oklahoma Department of Transportation (ODOT), addresses the above named project in compliance with Section 7(c) of the Endangered Species Act (ESA) of 1973, as amended. Section 7 of the ESA requires that, through consultation with the U.S. Fish and Wildlife Service (Service), federal actions do not jeopardize the continued existence of any threatened, endangered, or proposed species or result in the destruction or adverse modification of critical habitat. This assessment evaluates the potential effects of the proposed transportation project on species that are federally listed under the ESA. Specific project design elements are identified that avoid or minimize adverse effects of the proposed project on listed species and designated critical habitat.

1.2. Project Description

Grade, Drain, Surface and Bridge

Description of the existing bridge/roadway facility and reason for proposed project

This project consists of two discontinuous segments of SH-66 located in a rural area south of Wellston, OK, and traverses rolling hills and intersects I-44 (Turner Turnpike). The first segment begins roughly 0.35 mile east of the SH-102/SH-66 junction and extends east approximately 1.5 miles to just west of the I-44 Turner Turnpike southbound onramp. The second segment begins just east of the northbound onramp to I-44 (~1 mile east of the end of the first segment) and extends east 1.3 miles to 0.1 mile east of the US-177/SH-66 junction. The existing project portion of SH-66 is an open section major collector facility with two 12-foot-wide paved driving lanes and 0 to 2-foot-wide shoulders. It has deteriorating pavement, narrow shoulders, poor traffic flow, and limited sight distances in places. The current traffic volume is estimated at 5,300 vehicles per day (VPD).

Within the project areas there are two bridge sized structures—Bridge #1 (NBI #26639) over Captain Creek and Bridge #2 (NBI #12596) over Spring Creek. NBI# 26639 over Captain Creek is a 3-span bridge situated at a 30-degree skew with the stream channel. The three spans measure 80-100-80-feet in length. The total structure length is 262 feet and the horizontal clearance is 40 feet wide. This structure was constructed in 2001, has a sufficiency rating of 99.2, is considered not deficient, but is in need of structural repair. NBI# 12596 over Spring Creek is a large reinforced concrete box (RCB) structure with three cells that measure 13-17-13-feet-wide by 15 feet tall and 36 feet long with concrete wing walls. This structure is situated at a 30-degree skew to SH-66 to accommodate Spring Creek stream channel. The structure has guard rails and a horizontal clearance of 24 feet. This RCB was constructed in 1951, has a sufficiency rating of 65.60 and is considered functionally obsolete.

The purpose of the proposed project is to improve safety, and the need for the project is to correct a roadway with deteriorating pavement, narrow shoulders, poor traffic flow and limited sight distances, and to correct a functionally obsolete bridge and a bridge at risk of becoming structurally deficient.

Description of proposed improvements

The proposed improvements consist of widening and resurfacing the existing lanes on SH-66 to create two 12-foot-wide driving lanes with 8-foot-wide paved shoulders. Improvements will be made symmetrically on existing alignment beginning approximately 1,075 feet east of Captain Creek, extending east to Hickory Ave (CR-3330). Beginning at Hickory Ave and extending east approximately 1,200 feet, improvements will transition to an offset alignment to the north to avoid impacts to the parallel stream channel south of SH-66. Immediately east of the offset improvements and extending east to approximately 950 feet west of CR-3550, a 12-foot-wide left turn lane will be added to match the 52-foot-wide road width of the turnpike’s typical section (within the project design exception area). Starting approximately 1,875 feet east of CR-3350, improvements will transition back down to two-lane improvements on the existing alignment.

NBI# 26639 over Captain Creek is not considered deficient; however, structural repairs will be made to the east pier and backwalls as part of the proposed construction activities.

NBI# 12596 over Spring Creek will be evaluated to assess whether the structure will be rehabilitated and extended or replaced with a 3-span bridge structure on the existing alignment.

The improvements on existing and offset alignments to the north will require the acquisition of new right-of-way, as well as the relocation of some utilities. Phased construction will be utilized as well as temporary widening to the north to maintain 2 driving lanes at all times throughout construction.

Check if any of the following is expected s part of the proposed action

- Work within OHWM is expected
- Project is OFF-SET alignment or NEW alignment
- Project involves **NO OFF EXISTING PAVEMENT** work
- Project requires new ROW (permanent &/or temporary)

1.3. Project Area and Setting

Project Location		Environmental Study Footprint		Ecoregion & Game Type	
<u>Section Range & Township</u>	<u>Lat/Long NAD 83</u>	<u>Dimensions</u>	<u>Acreage</u>	<u>Level IV Ecoregion (Woods et al. 2005)</u>	<u>Game Type (Duck and Fletcher 1943)</u>
S13, 14, 15, 22, 23, 24-T14N-R2E; S17, 18, 19, 20-T14N-R3E	West Boundary: (35.680993°, -97.081982°) East Boundary: (35.681000°, -97.015518°)	Two separate study areas: West study area 8,000 feet long and 200 feet L/R of center line. East study area 7,000 feet long and 200 feet L/R of center line.	137.3 acres	29a – Northern Cross Timbers	Postoak-Blackjack Oak Forest

Action Area:

The action area is equivalent to the NEPA Environmental Study Footprint.

2. FEDERALLY LISTED SPECIES AND DESIGNATED CRITICAL HABITAT

Species Range and Occurrence Evaluation (Check all that apply)

Species	IPaC ¹	Watershed ²	Water Body ³	Records ⁴
	Check if Yes	Check if YES	Check if Yes	Check if Yes
Red-cockaded Woodpecker	<input type="checkbox"/>			<input type="checkbox"/>
Whooping Crane	<input checked="" type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>
Gray Bat	<input type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>
Indiana Bat	<input type="checkbox"/>			<input type="checkbox"/>
Ozark Big-eared Bat	<input type="checkbox"/>			<input type="checkbox"/>
Neosho Mucket	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Ouachita Rock Pocketbook	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Scaleshell Mussel	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Winged Mapleleaf	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Harperella	<input type="checkbox"/>			<input type="checkbox"/>
American Burying Beetle	<input type="checkbox"/>			<input type="checkbox"/>
Eastern Black Rail	<input type="checkbox"/>			<input type="checkbox"/>
Piping Plover	<input checked="" type="checkbox"/>			<input type="checkbox"/>
Red Knot	<input checked="" type="checkbox"/>			<input type="checkbox"/>
Northern Long-eared Bat	<input type="checkbox"/>			<input type="checkbox"/>
Arkansas River Shiner	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Leopard Darter	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Neosho Madtom	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Ozark Cavefish	<input type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>
American Alligator	<input type="checkbox"/>			<input type="checkbox"/>
Rabbitsfoot Mussel	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Monarch Butterfly	<input checked="" type="checkbox"/>			<input type="checkbox"/>
Rattlesnake-master Borer Moth	<input type="checkbox"/>			<input type="checkbox"/>
Peppered Chub	<input type="checkbox"/>	<input type="checkbox"/>		<input type="checkbox"/>

¹Species is on the Proposed Project's IPaC List

²Action Area is within a watershed associated with occupied water bodies

³Action Area includes an occupied water body

⁴Project site within 5 miles of known records

Designated or Proposed Critical Habitat	Action Area includes Designated Critical Habitat (Check <input checked="" type="checkbox"/> if Yes)
Whooping Crane	<input type="checkbox"/>
Arkansas River Shiner	<input type="checkbox"/>
Leopard Darter	<input type="checkbox"/>
Neosho Mucket	<input type="checkbox"/>
Rabbitsfoot	<input type="checkbox"/>
Peppered Chub	<input type="checkbox"/>

Action area is adjacent to McAlester Army Ammunition Plant or Camp Gruber/Cherokee WMA

All of part of the action area is within the 10 mile **gray bat** priority area (ODOT will check)

All of part of the action area is within the 2 mile **gray bat** priority area (ODOT will check)

Action area is within what percentage **Whooping Crane** migratory corridor **100%**

Action area is within 15 miles of Salt Plains NWR, Hackberry Flat, or Foss Reservoir.

Action area is within the historic range of the **Red-cockaded Woodpecker**

Action area is within 10 miles of the McCurtain County Wilderness Area

Action area is within 10 miles of the Pushmataha Wildlife Management Area

3. ENVIRONMENTAL BASELINE

3.1. Ecological Processes and Conditions

Soils (Use Soil Map of Oklahoma by Carter and Gregory 2008)

Soil Class	Northern Cross Timbers	
Soil Name	Stephenville-Darnell-Niotaze	
Soil Type	Alfisols; Inceptisols	
Soil Characteristics	Shallow, sandy and loamy, moderately acid, and humus-poor soils on steep slopes (up to 18%)	

Climate (Use Woods et al. 2005)

Precipitation	Mean annual inches	36-46 inches, increasing eastward
Growing Season	Number of days	195-235 days
Mean Temperatures	Summer min/max	270/94 degrees F
	Winter min/max	26/49 degrees F

River System

Review of the U.S. Geological Survey (USGS) 7.5-Minute Topographic Map and National Wetlands Inventory (NWI) Maps identified eight mapped streams including Captain Creek and three unnamed tributaries, Spring Creek and two unnamed tributaries, and one unnamed tributary to Deep Fork. In addition, a few wetlands were noted within the study area during field review.
--

Land Use and Land Ownership

From Woods et al. 2005	Woodland, grassland, rangeland, pastureland, and limited cropland.
From Field investigation	The study area was comprised mostly of rural residential or cattle-grazed pastureland with some commercial properties near the Turner Turnpike interchange. Some small hay fields were observed as well.

Terrestrial and Aquatic Community Descriptions (based on field site visit)

The study area is located in the Northern Cross Timbers ecoregion of Oklahoma. This ecoregion is typified by rolling hills with a mosaic of tall grass prairie (e.g., big bluestem, little bluestem, switchgrass, and Indiangrass), intermixed with oak-hickory woodlands. Land uses are dominated by rangeland, pastureland, and limited cropland with small grains, sorghum, hay, and soybeans. Woodlands have become more common with fire suppression and passive land use.

The study area occurs in the East Captain Creek and Spring Creek-Deep Fork Canadian River Subwatersheds. Review of the U.S. Geological Survey (USGS) 7.5-Minute Topographic Map and National Wetlands Inventory (NWI) Maps identified eight mapped streams including Captain Creek and three unnamed tributaries, Spring Creek and two unnamed tributaries, and one unnamed tributary to Deep Fork. In addition, multiple wetlands were noted within the study area during field review.

The following community types were identified within the NEPA study footprint:

Maintained Right-of-Way (ROW) Vegetation- Dominant vegetation in this community type included: cheatgrass (*Bromus tectorum*), henbit (*Lamium amplexicaule*), bermudagrass (*Cynodon dactylon*), switchgrass (*Panicum virgatum*), little bluestem (*Schizachyrium scoparium*), and johnsongrass (*Sorghum halepense*).

Upland Tall-Grass Prairie- Dominant vegetation in this community type included: bermudagrass (*Cynodon dactylon*), switchgrass (*Panicum virgatum*), little bluestem (*Schizachyrium scoparium*), big bluestem (*Andropogon gerardi*), Indiangrass (*Sorghastrum nutans*), and johnsongrass (*Sorghum halepense*).

Upland Woodland- Dominant vegetation in this community type included: cedar (*Juniperus virginiana*), elm (*Ulmus spp.*), post oak (*Quercus stellata*), and blackjack oak (*Quercus marilandica*), bradford pear (*Pyrus calleryana*), and redbud (*Cercis canadensis*).

Wooded Riparian Corridor- Dominant vegetation in this community type included: various oaks (*Quercus spp.*), sycamore (*Platanus occidentalis*), cottonwood (*Populus deltoides*), eastern red cedar (*Juniperus virginiana*), redbud (*Cercis canadensis*), and elm (*Ulmus spp.*). Understory herbaceous vegetation included creek oats (*Chasmanthium latifolium*), horsetail (*Equisetum hymale*), greenbrier (*Smilax bona-nox*), and coralberry (*Symphoricarpos orbiculatus*).

3.2 Species Habitat Analysis

- Pedestrian survey of entire NEPA study footprint (including 300-foot work zone buffer in karst areas)
- Bridge/Structure inspected for bat use (Complete the Bridge Inspection Form)

SPECIES	HABITAT	
Whooping Crane	Shallowly-submerged sandbars in large river channels occur within the 0.25 miles of the NEPA Environmental Study Footprint.	<input type="checkbox"/>
	If within the 75% migration corridor, provide the number of acres of emergent wetlands that occur within the NEPA Environmental Study Footprint.	enter acres.
	Croplands suitable for foraging occur within the 0.25 miles of the NEPA Environmental Study Footprint and is within the 95% migration corridor.	<input type="checkbox"/>
Piping Plover	Sparsely vegetated sandy or gravelly shorelines and islands associated with the major river systems occur within the 0.25 miles of the NEPA Environmental Study Footprint.	<input type="checkbox"/>
	Salt flats or mudflats associated with reservoirs occur within the 0.25 miles of the NEPA Environmental Study Footprint.	<input type="checkbox"/>
Red Knot	Mudflats associated with reservoirs occur within the 0.25 miles of the NEPA Environmental Study Footprint.	<input type="checkbox"/>
Monarch Butterfly	Presence of milkweed (<i>Asclepias sp.</i>) species within the NEPA Environmental Study Footprint.	<input type="checkbox"/>
	Presence of flowering or potentially flowering nectar plants (<i>defined as forbs that can provide nectar for monarchs at some point in the growing season</i>) within the NEPA Environmental Study Footprint.	<input checked="" type="checkbox"/>
	Presence of additional native habitat within the NEPA Environmental Study Footprint.	<input checked="" type="checkbox"/>

4. ANALYSIS OF EFFECTS

4.1 Direct Effects

Species/ Resource	Habitat impacts expected from project activities	<u>Describe specific ACTIONS of the project and the results of those actions on species habitats, including indirect impacts to prey or drinking water, as well as improvements to habitat as a result of specific actions. If habitat within the action area identified above will not be impacted, describe why.</u>
Monarch butterfly	☒	The proposed project includes widening and resurfacing on existing and offset alignment, as well as repairs/replacement of two bridges. Proposed activities will include clearing and grubbing of vegetation within the existing and new ROW. Some potential habitat was identified, specifically in some overgrown grassland and woody riparian areas. Proposed construction may include clearing of potential foraging and reproductive habitat areas, the conversion of vegetated areas to pavement (i.e. wider roadway widths), and the storage of construction equipment on suitable habitat – all of which could result in direct habitat losses.

4.2 Indirect Effects

Long-term habitat alterations

Species/ Resource	<u>Identify long-term, permanent changes in habitat</u>
Monarch butterfly	Most habitat disturbance will likely be temporary. Any temporary storage of equipment and/or pavement materials could be considered temporary disturbance. Any conversion of grassland to maintained ROW or pavement will result in permanent cover change.

Indirect land use impacts

New permanent ROW will be required for the purpose of road widening and resurfacing. ROW acquisition will be minimal, and is not expected to impact or alter the land use.
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4.3 Interrelated and Interdependent Actions and Activities

New permanent ROW will be required for the proposed road widening and resurfacing. The project may require relocation of utilities. Public safety is expected to improve with the upgraded road surface, drainage, and traffic flow.
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USFWS TAILS Number:	2022-0029790
ODOT Project JP Number:	34318(04)

SPECIES / DESIGNATED CRITICAL HABIT	CONCLUSION		ESA SECTION 7			NOTES AND DOCUMENTATION Check <input checked="" type="checkbox"/> all that apply			
	Species Habitat present within the action area	Project Activities expected to impact habitat	No Effect	May affect, not likely to adversely affect	May affect, Likely to adversely affect	Field Studies	ONHI database / ABB	USFWS occupied waterbodies & watersheds	Whooping Crane Migration Corridor
Whooping crane	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
Piping plover	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Red knot	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Monarch butterfly	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

CONCLUSIONS

No Effect	Whooping crane, red knot, piping plover
May affect, not likely to adversely affect	
May affect, likely to adversely affect	
Not likely to jeopardize the continued existence of the species – Candidate species only	Monarch butterfly
Appropriate Effect Determination for ABB has been made under the BO for the final 4(d) rule	<input type="checkbox"/>
Appropriate Effect Determination has been made under the FHWA NLEB/Ibat Programmatic BA & BO	<input type="checkbox"/>
Appropriate Effect Determination for NLEB has been made under the BO for the final 4(d) rule	<input type="checkbox"/>

RECOMMENDED AVOIDANCE AND MINIMIZATION MEASURES

ODOT, as a Certificate of Inclusion partner in the Nationwide **Monarch Butterfly** CCAA for Energy and Transportation lands, will adhere to the conservation measures, as well as minimize threats to the monarch butterfly as stipulated in this CCAA.

5. BALD AND GOLDEN EAGLE PROTECTION ACT ASSESSMENT

5.1. Bald Eagle Assessment

The Bald Eagle (*Haliaeetus leucocephalus*) is a large predatory bird protected by the Bald and Golden Eagle Protection Act and the Migratory Bird Treaty Act. Activities that would disturb eagles are prohibited under the Bald and Golden Eagle Protection Act. “Disturb” means to agitate an eagle to the degree that causes or is likely to (1) cause injury, (2) interfere with breeding, feeding or sheltering behavior, or (3) nest abandonment.

Potential Bald Eagle Habitat Present	w/in NEPA Footprint	w/in 660 ft Buffer of NEPA Footprint	DO NOT LEAVE BLANK
Presence of Cottonwood, Sycamore, Pecan or Pine	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	Wooded riparian corridors were dominated by cottonwood and sycamore interspersed among oak and elm.
Open foraging areas with large trees	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	The study area and buffer were dominated by open grazed pastures, rural residential yards and some cultivated hayfields dissected by wooded riparian corridors.

Potential Bald Eagle Habitat Present	w/in NEPA Footprint	w/in 660 ft Buffer of NEPA Footprint	DO NOT LEAVE BLANK
Distance to closest perennial water body	River or Lake	0.35 mi	Deep Fork River flows north to south approximately 0.35 mile east of the easternmost project footprint. Captain and Spring Creeks are substantial streams that intersect the study area.
	Stream or Pond	0 feet	
Potential Bald Eagle Nests Observed	<input type="checkbox"/>	<input type="checkbox"/>	None observed
Bald Eagles Observed in the general vicinity	<input type="checkbox"/>	<input type="checkbox"/>	No bald eagles were observed during field site visit, but there is one recorded occurrence within the vicinity of the project
General Description of Bald Eagle Nesting Habitat and Impact Determination, within the NEPA Footprint and within 660-ft of the NEPA Footprint	The NEPA Environmental Study footprint and 660-foot buffer included some potential nesting, foraging, and perching habitat, primarily along the banks of Captain Creek and Spring Creek. Although potential habitat was observed, the banks of Deep Fork River are likely to be preferential to areas within the study footprint and buffer.		
Station #s for Buffered Bald Eagle Habitat	The 660-foot buffer of potential habitat areas intersect the western footprint section between (35.681005°, -97.081403°) and (35.681014°, -97.077110°) and again from (35.680970°, -97.062360°) to (35.680977°, -97.057517°). Then the buffer intersects the eastern footprint portion from (35.681028°, -97.023899°) to (35.681021°, -97.018454°). This functionally is the full extent of the project area.		
In order to avoid impacts to Bald Eagles, if Bald Eagles or their habitat are observed during the biological assessment, a survey for eagles and their nests will be conducted within 660 feet of the work zone, during the winter prior to, and within one year of, the start of construction. If a nest is found, appropriate conservation measures based on the National Bald Eagle Management Guidelines will be implemented.			

6. MIGRATORY BIRD TREATY ACT (MBTA) ASSESSMENT

6.1 Structure Assessment

Cliff Swallows (*Petrochelidon pyrrhonota*) and Barn Swallows (*Hirundo rustica*) are small colonial and semi-colonial nesting birds protected by the federal Migratory Bird Treaty Act. Barn Swallows use man-made structures for nesting and live in close association with humans. Both species commonly use bridges and culverts in Oklahoma for nesting. Other migratory birds can also nest on transportation structures.

Identify <u>ALL</u> structures including pipe culverts and whether positive or negative for migratory birds (identify named streams where possible rather than just FS#). Provide shapefiles and map of structures identifying pos/neg swallow structures.	Approx. Number of Cliff Swallow Nests	Approx. Number of Barn Swallow Nests	Approx. Number of Eastern Phoebe Nests
Structure #1 [NBI: 26639] (35.681032°, -97.079794°)	100+	2	
Structure #2 (35.680977°, -97.066135°)		2	
Structure #3 (35.680983°, -97.063251°)		1	
Structure #4 (35.681013°, -97.034843°)	None		
Structure #5 (35.681400°, -97.032573°)	None		
Structure #6 (35.681017°, -97.029982°)	None		
Structure #7 (35.681018°, -97.028944°)	None		
Structure #8 [NBI: 12596] (35.681029°, -97.020703°)	100+		
Structure #9 (35.680865°, -97.017213°)		1	
Other MB and Nests Observed	N/A		
Based on existing plans, no work on suitable drainage structures will occur		<input type="checkbox"/>	
In order to avoid impacts to migratory birds, if structures are being used by these birds, any activities that may destroy active nests, eggs or birds shall be completed between September 1, and February 28, when nests are not occupied. If seasonal avoidance cannot be accomplished, structures shall be protected from new nest establishment prior to March 1, by means that do not result in death or injury to these birds.			

6.2 Birds of Conservation Concern

<u>Species Identified on IPaC list</u>	<u>Breeding Season</u>
No species of Birds of Conservation Concern were listed for the proposed project; therefore, no impact is expected.	

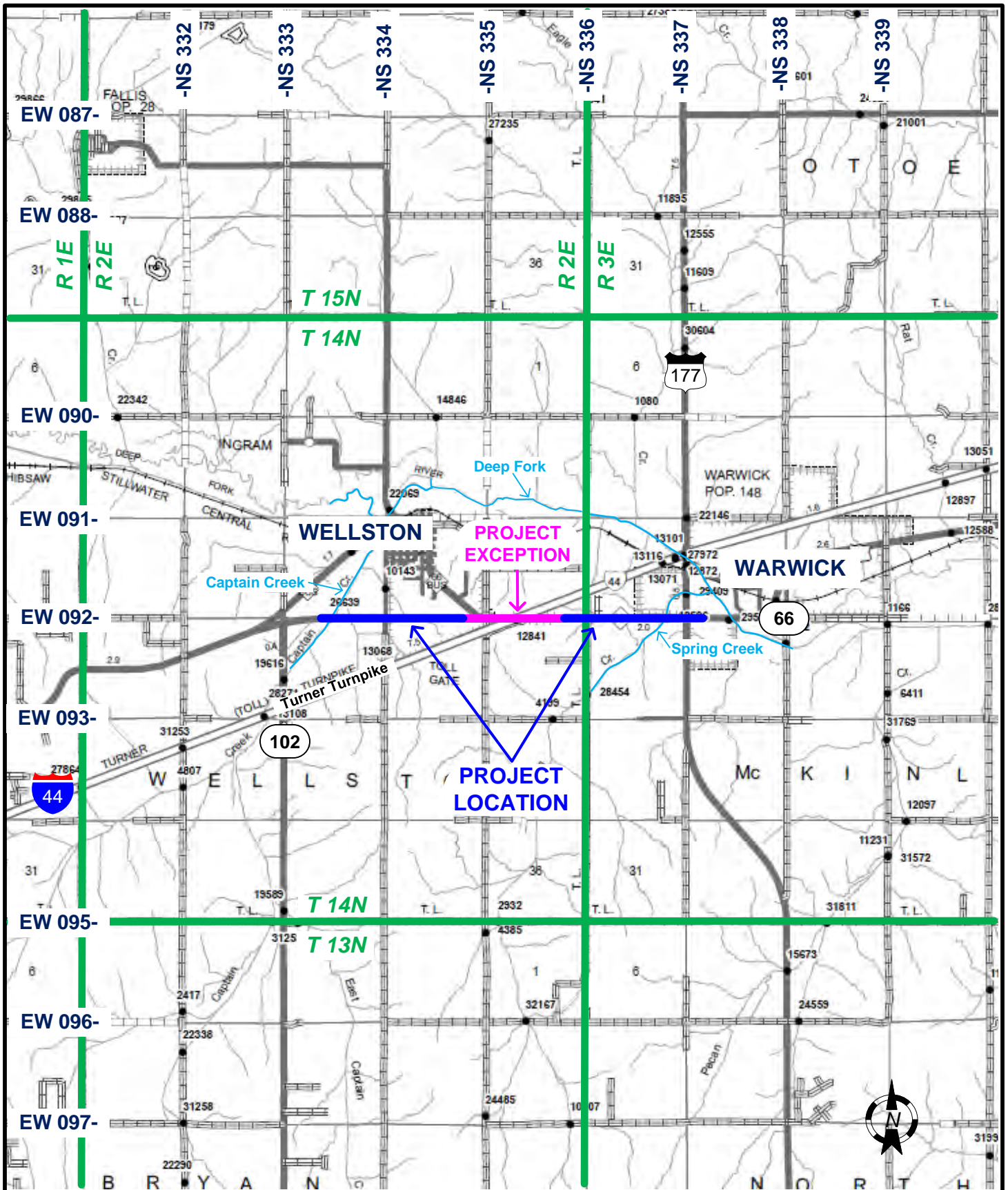
6.3 Interior Least Tern

Sparsely vegetated islands or sandbars along large rivers, with nearby areas of shallow water, occur within the 0.25 miles of the NEPA Environmental Study Footprint.	<input type="checkbox"/>
No potential habitat was identified; therefore, no impact is expected.	

7. REFERENCES:

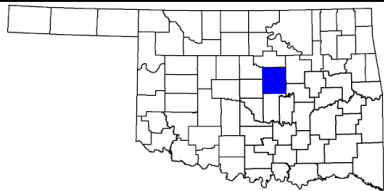
- Carter, B.J. and M.S. Gregory (2008), *Soil Map of Oklahoma; Earth Sciences and Mineral Resources of Oklahoma*. Educational Publication 9, Oklahoma Geological Survey, University of Oklahoma, Norman OK, p. 16, map scale 1:2,000,000.
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- Duck, L.G., and J.B. Fletcher (1945), *A Survey of the Game and Furbearing Animals of Oklahoma*; Chapter 2, *The Game Types of Oklahoma*. Oklahoma Game and Fish Commission, Division of Wildlife Restoration and Research, Oklahoma City, OK.
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- USGS (1966), *USGS Topographic Map*, US Geological Survey on-line viewer, accessed April 2022, 7.5-minute quadrangles, *Wellston, OK*, maps scale 1: 24,000.
- Woods, A.J., Omernik, J.M., Butler, D.R., Ford, J.G., Henley, J.E., Hoagland, B.W., Arndt, D.S., and Moran, B.C. (2005), *Ecoregions of Oklahoma*; U.S. Geological Survey, Reston, VA, map scale 1:1,250,000

8. FIGURES



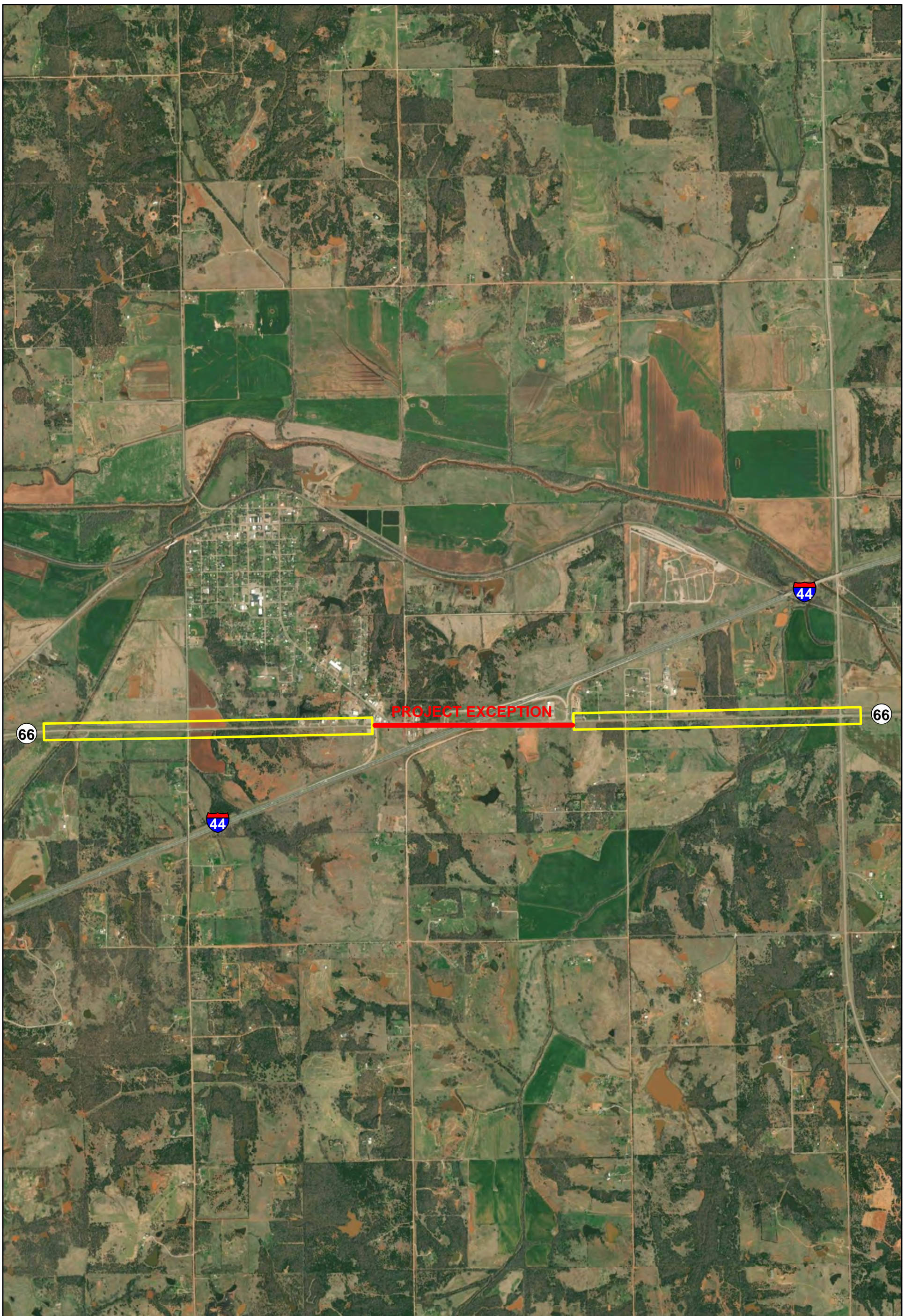
LOCATION MAP




JP 34318(04); Project No. J3-4318(004)
 Widen, Resurface & Bridge SH-66:
 From SH-102, east to US-177
 Lincoln Co, OK



OKLAHOMA
 Transportation

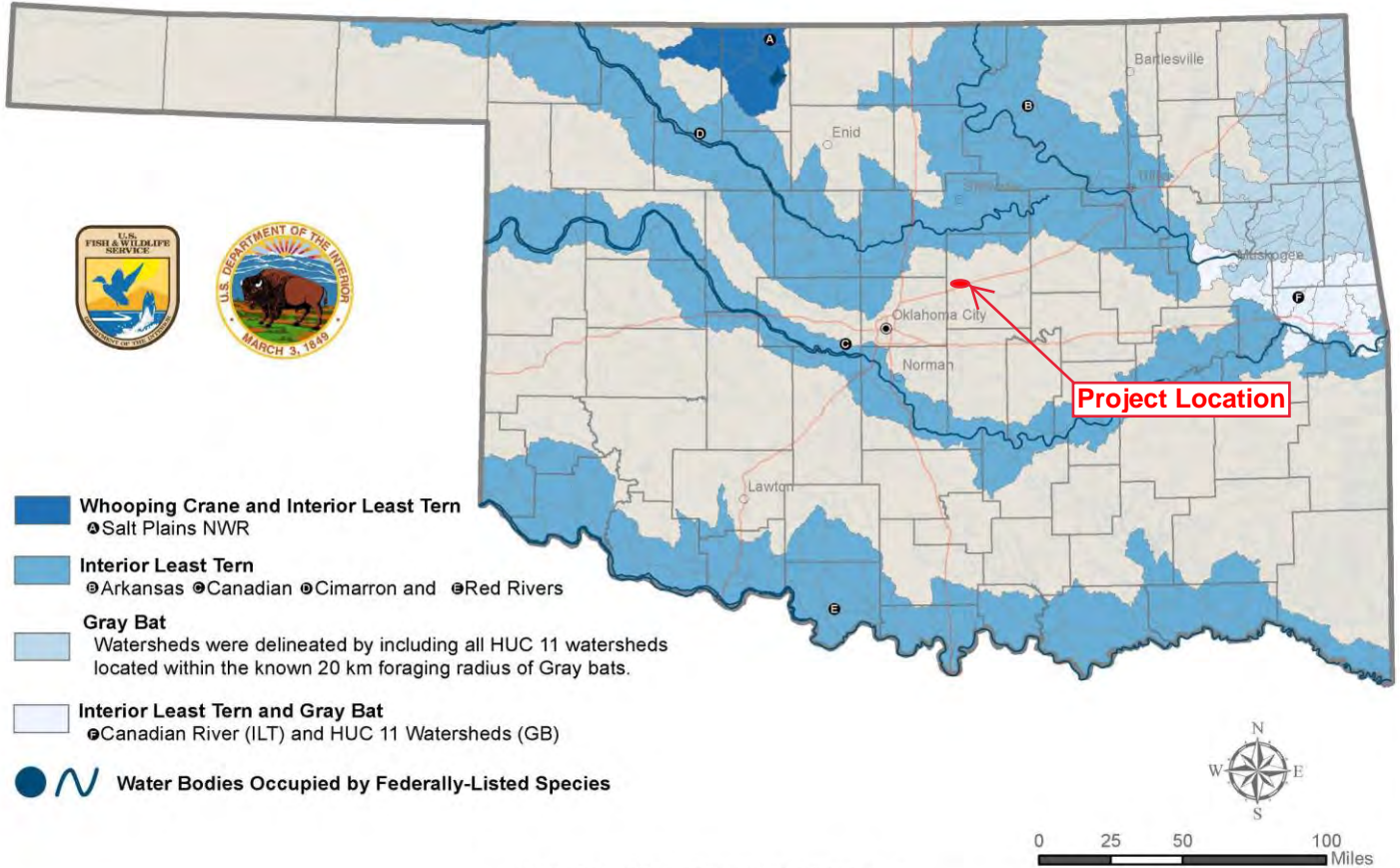
Figure 1



<p>COMMENTS/LEGEND</p> <p> NEPA Environmental Footprint / Action Area</p> <p>0 550 1,100 2,200 3,300 4,400 Feet</p> 	<p>Action Area</p> <p>JP 34318(04) / J3-4318(004) Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK</p>	 <p>DRAWN BY: LMP APPRV BY: SOURCE: DEQ, Tiger 2000, USGS</p>	<p>Figure 2</p> <p>Date: 4/7/2022</p>
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Federally-Listed Aquatic Dependent Species Watersheds of Oklahoma

These watersheds were delineated using 11 digit Hydrologic Unit Code (HUC) watersheds. All watersheds adjacent to water bodies occupied by federally-listed species are included in the delineation, as well as those 11 digit HUC watersheds within 10 miles of the occupied water body. Please note that not all 11 digit HUC watersheds that feed into sensitive occupied water bodies are included in this delineation and effects to those watersheds outside of this delineation could impact sensitive water bodies.



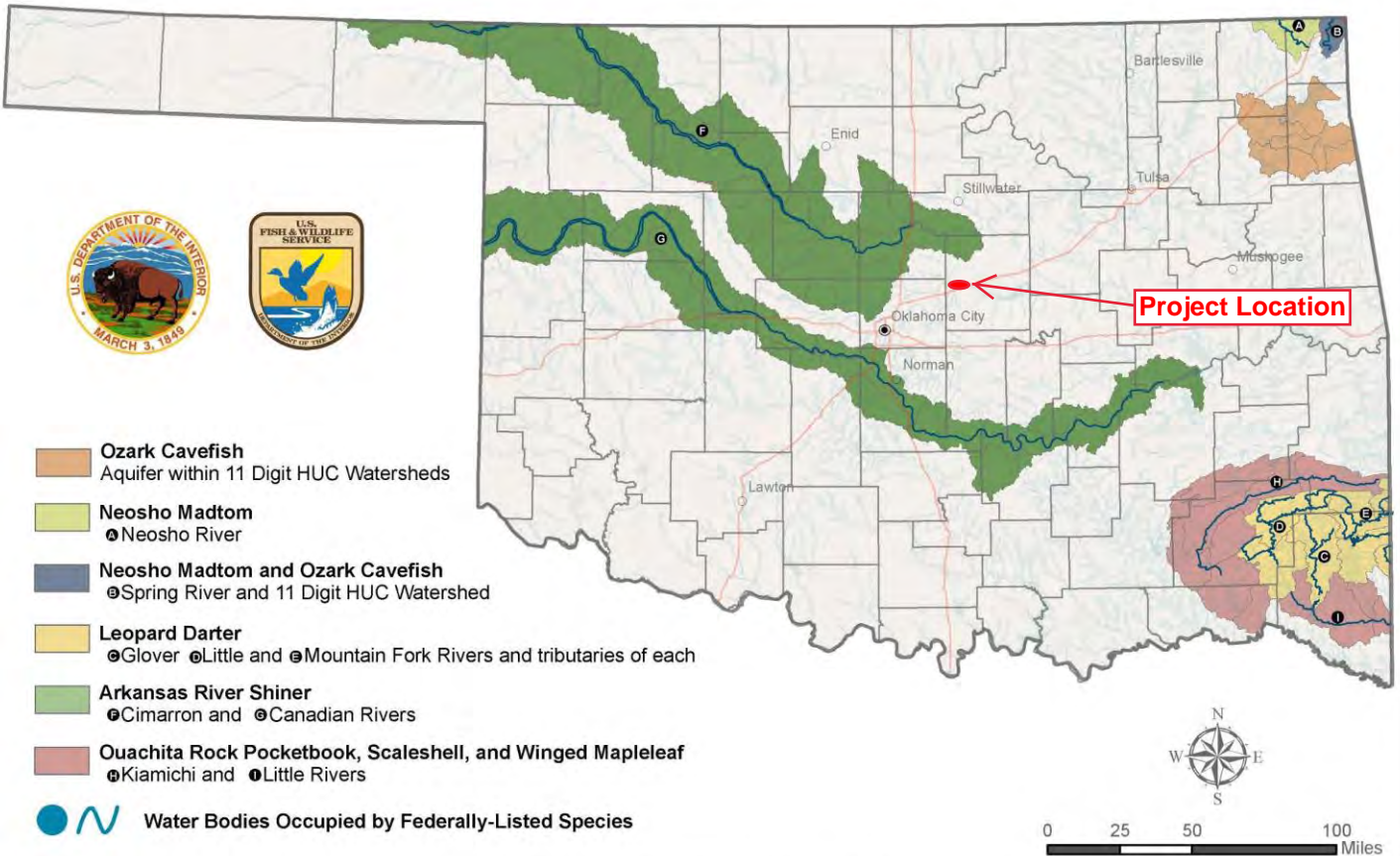
USFWS - Oklahoma Ecological Services Field Office - April 2011



COMMENTS/LEGEND	AQUATIC DEPENDENT SPECIES WATERSHEDS		Figure 3a
	JP 34318(04) / J3-4318(004) Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 Lincoln County, OK		
	 PO BOX 1292 NORMAN OK 73070	DRWN BY: LMP APPRVD BY: GAC CHKD BY: GAC SOURCE: USFWS	4/4/2022

Federally-Listed Aquatic Species Watersheds of Oklahoma

These watersheds were delineated using 11 digit Hydrologic Unit Code (HUC) watersheds. All watersheds adjacent to water bodies occupied by federally-listed species are included in the delineation, as well as those 11 digit HUC watersheds within 10 miles of the occupied water body. Please note that not all 11 digit HUC watersheds that feed into sensitive occupied water bodies are included in this delineation and effects to those watersheds outside of this delineation could impact sensitive water bodies.



USFWS - Oklahoma Ecological Services Field Office - April 2010



COMMENTS/LEGEND

AQUATIC SPECIES WATERSHEDS

JP 34318(04) / J3-4318(004)
Widen, Resurface & Bridge on SH-66
from SH-102, east to US-177
Lincoln County, OK



Figure
3b

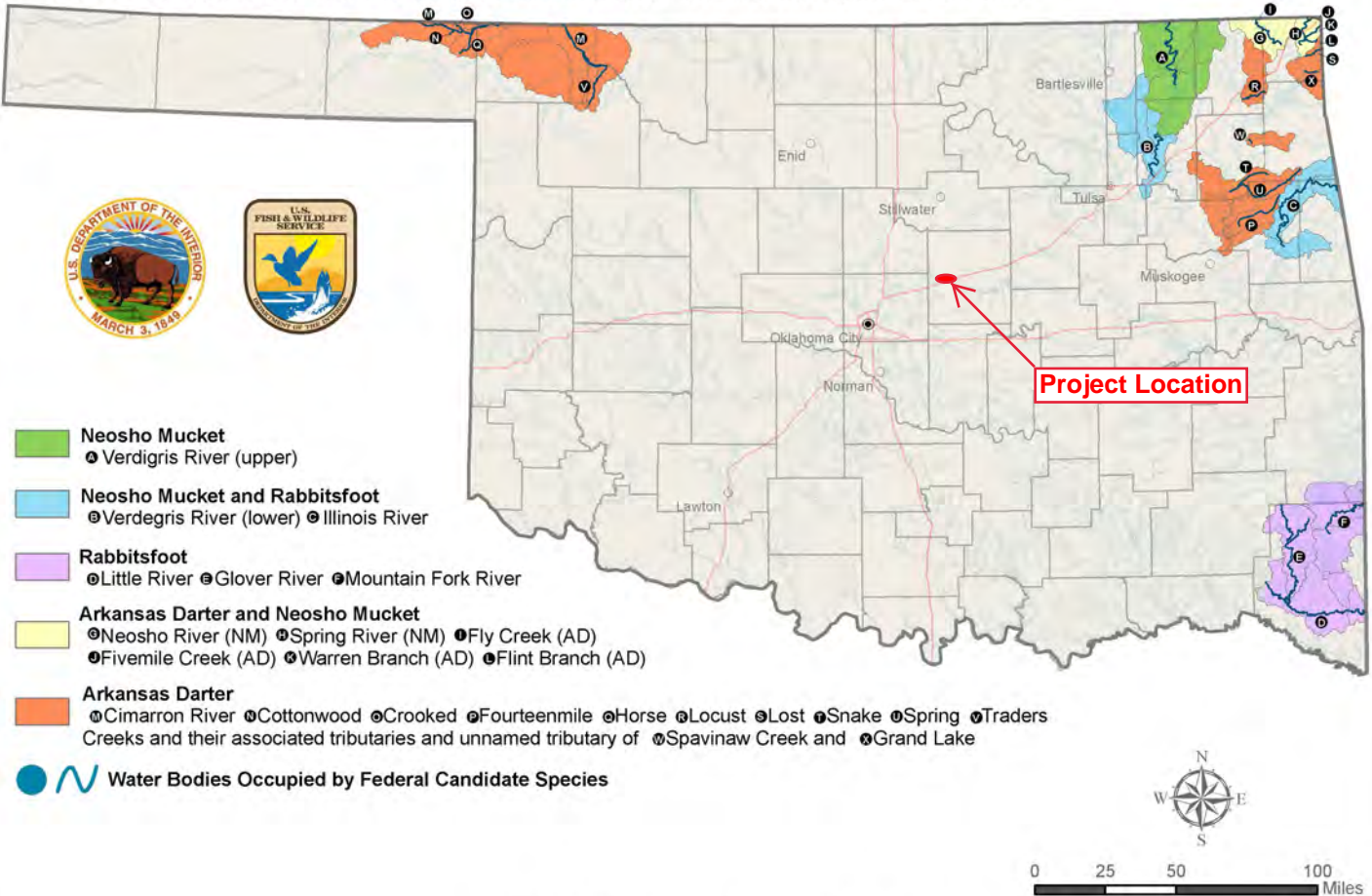


DRWN BY: LMP APPRVD BY: GAC
CHKD BY: GAC SOURCE: USFWS

4/4/2022

Federal Candidate Aquatic Species Watersheds of Oklahoma

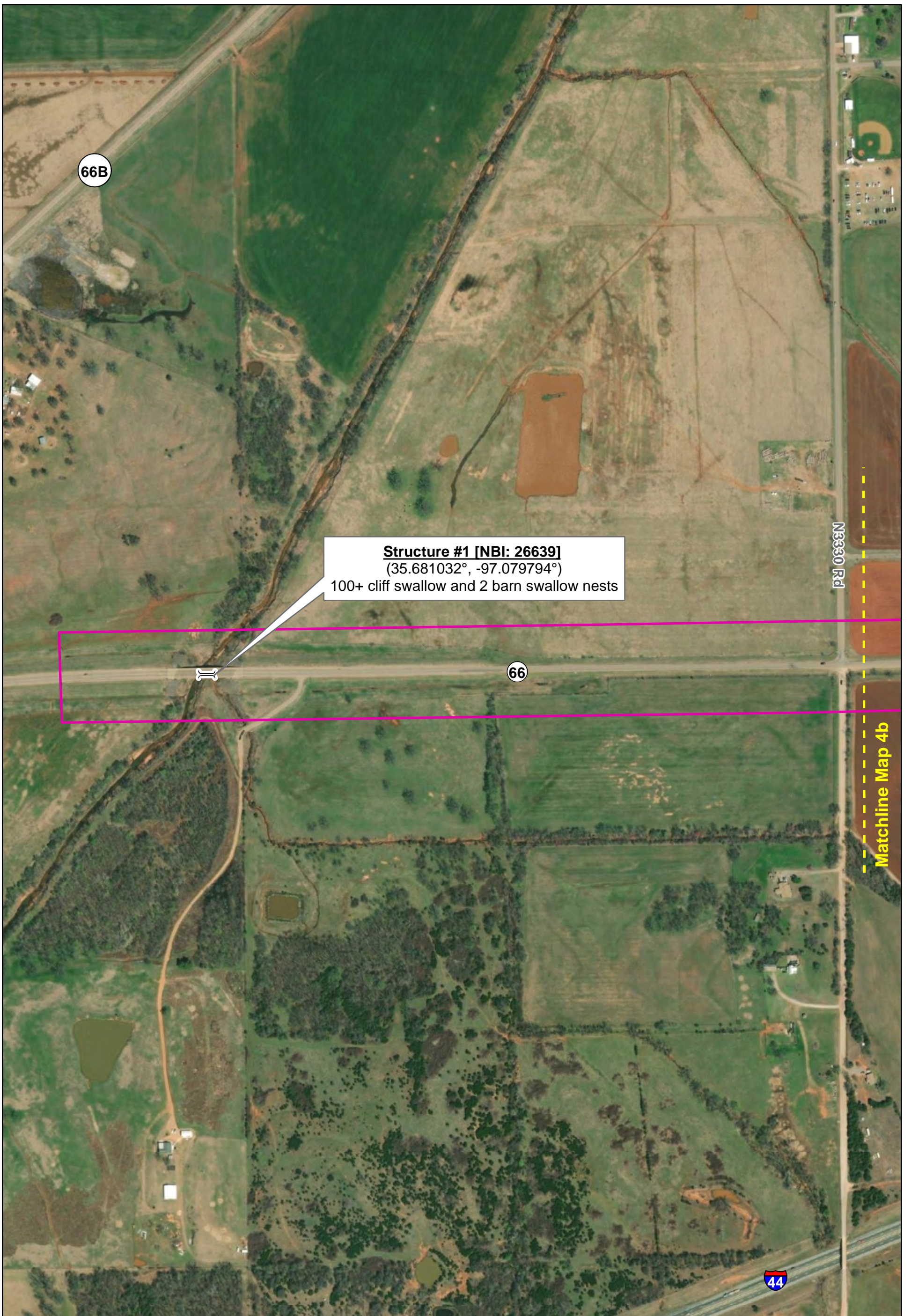
These watersheds were delineated using 11 digit Hydrologic Unit Code (HUC) watersheds. All watersheds adjacent to water bodies occupied by federal candidate species are included in the delineation, as well as those 11 digit HUC watersheds within 10 miles of the occupied water body. Please note that not all 11 digit HUC watersheds that feed into sensitive occupied water bodies are included in this delineation and effects to those watersheds outside of this delineation could impact sensitive water bodies.



USFWS - Oklahoma Ecological Services Field Office - April 2010







COMMENTS/LEGEND	CANDIDATE SPECIES WATERSHEDS JP 34318(04) / J3-4318(004) Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 Lincoln County, OK	 OKLAHOMA Transportation	Figure 3c
	 Environmental PO BOX 1292 NORMAN OK 73070	DRWN BY: LMP APPRVD BY: GAC CHKD BY: GAC SOURCE: USFWS	4/4/2022



Structure #1 [NBI: 26639]
 (35.681032°, -97.079794°)
 100+ cliff swallow and 2 barn swallow nests



COMMENTS/LEGEND	
	NEPA Environmental Footprint
	Matchlines
	Culvert
	NBI Bridge



Structures
 JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66
 from SH-102, east to US-177 (includes an
 approx. 1 mile turnpike project exception)
 Lincoln County, OK


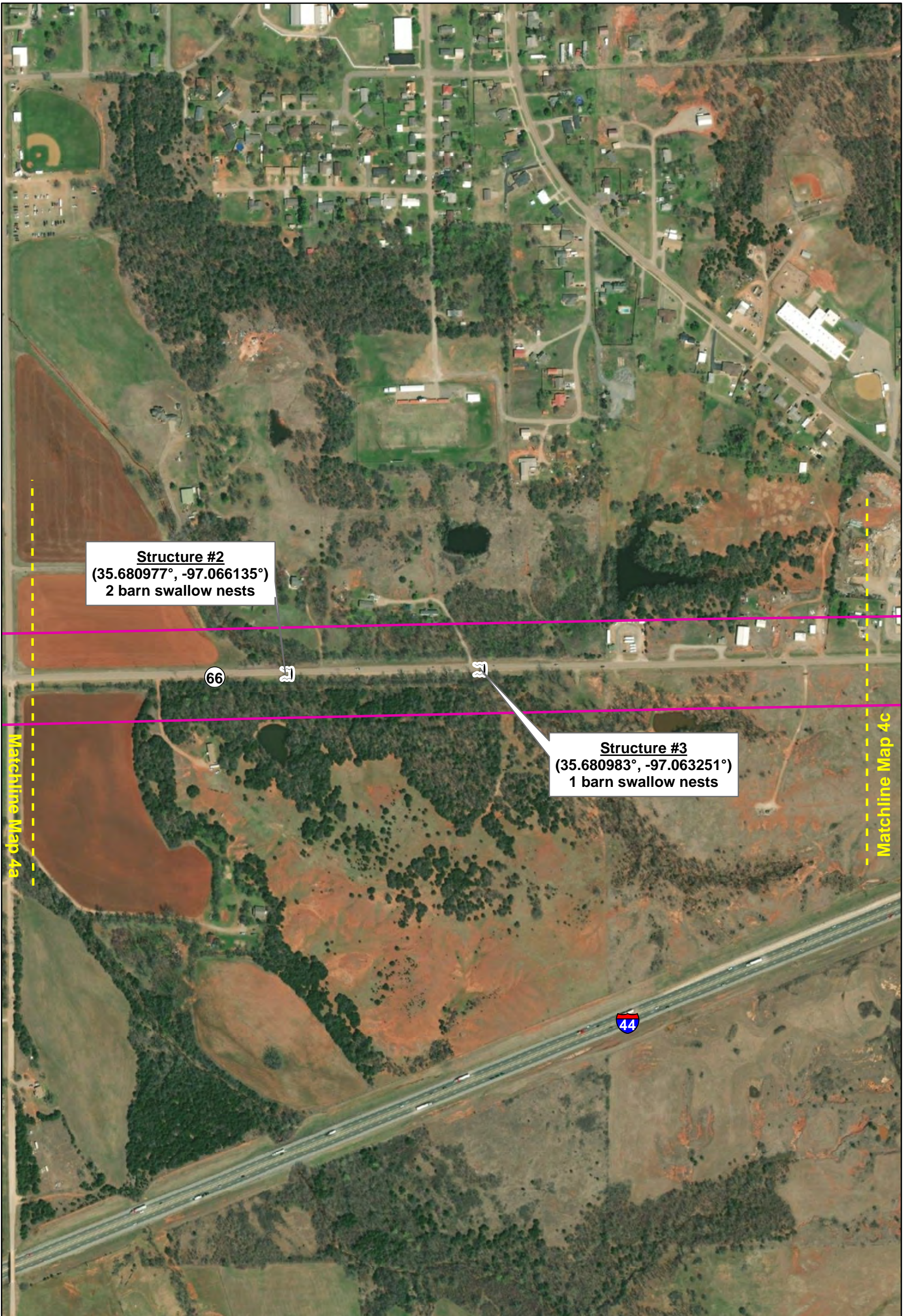

 DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

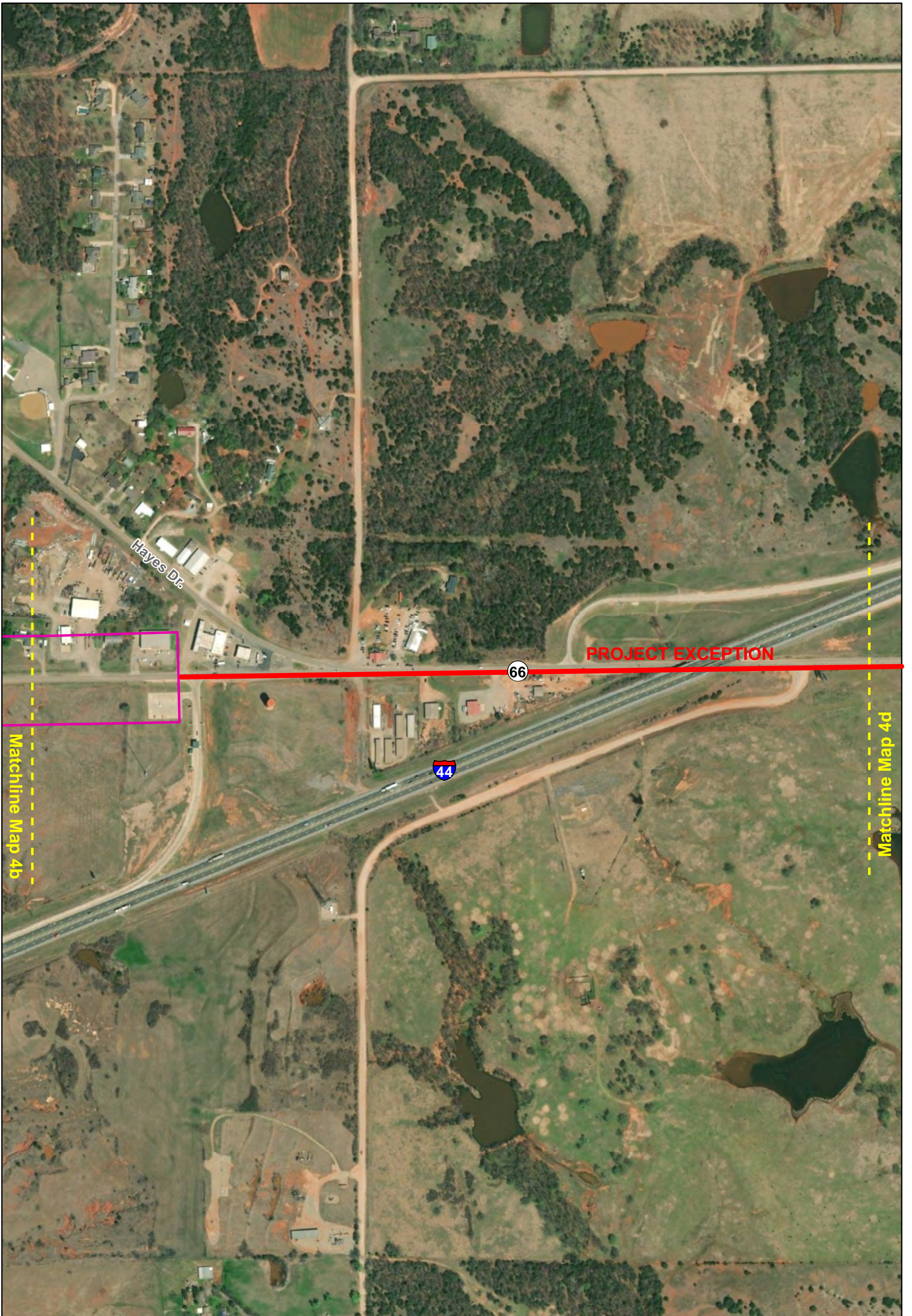
Figure 4a
 Date: 4/7/2022



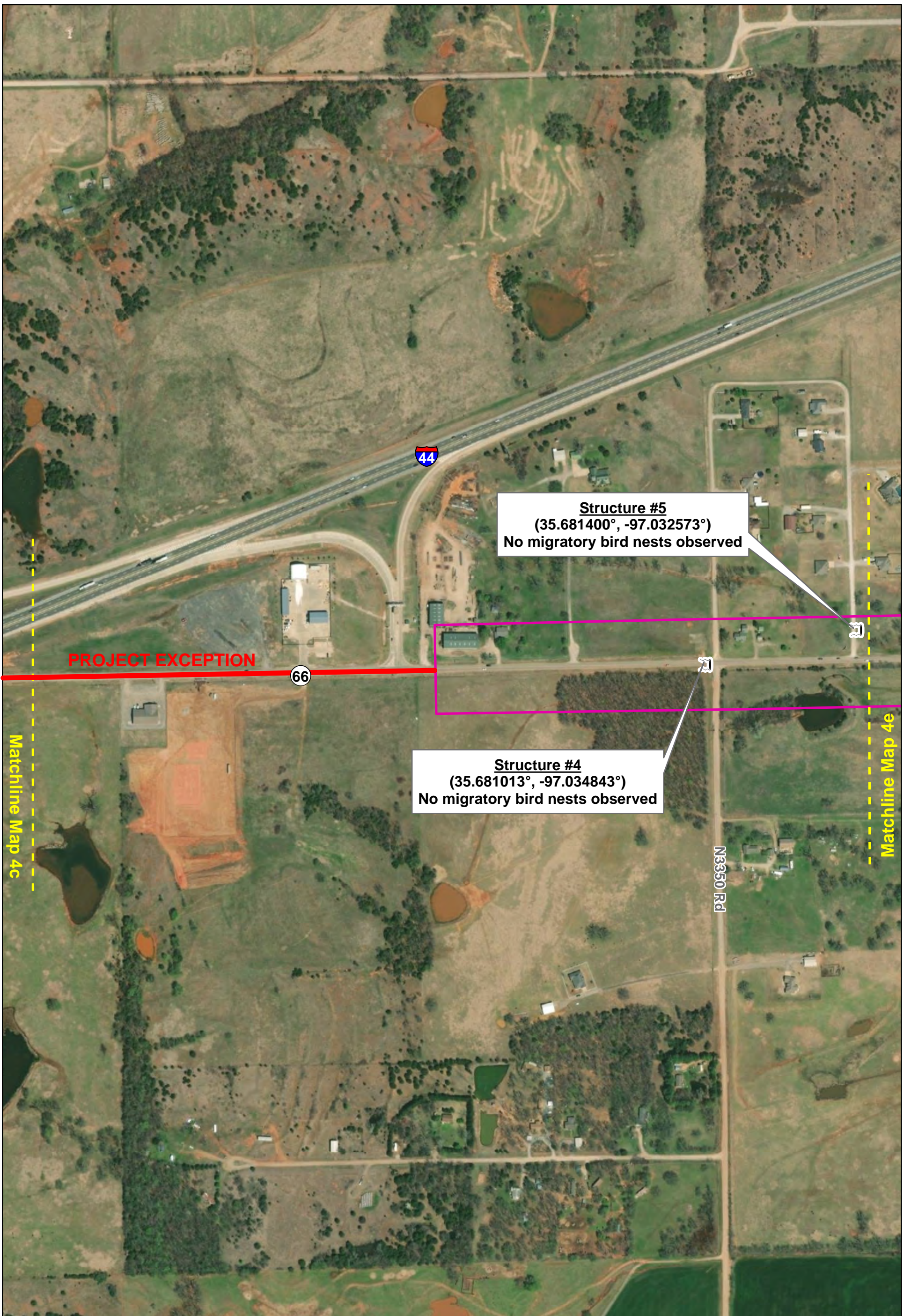
Structure #2
 (35.680977°, -97.066135°)
 2 barn swallow nests

Structure #3
 (35.680983°, -97.063251°)
 1 barn swallow nests

COMMENTS/LEGEND		Structures JP 34318(04) / J3-4318(004) Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK		Figure 4b
NEPA Environmental Footprint Matchlines Culvert NBI Bridge	 			DRAWN BY: LMP APPRV BY: SOURCE: DEQ, Tiger 2000, USGS



COMMENTS/LEGEND			<h2 style="margin: 0;">Structures</h2> <p style="margin: 0;">JP 34318(04) / J3-4318(004) Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK</p>		Figure 4c
NEPA Environmental Footprint Culvert NBI Bridge Matchlines					DRAWN BY: LMP APPRV BY: SOURCE: DEQ, Tiger 2000, USGS



PROJECT EXCEPTION

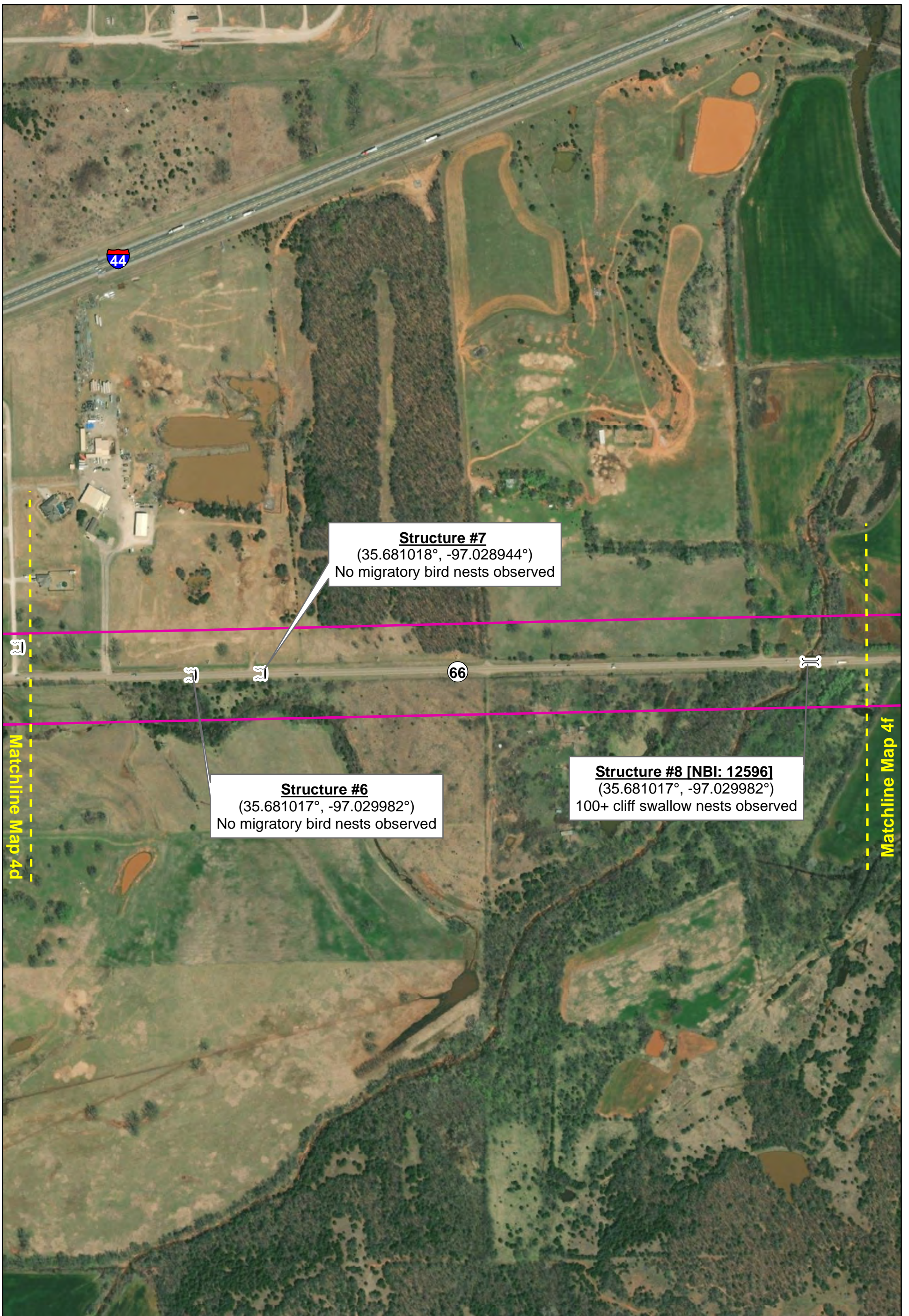
Structure #5
 (35.681400°, -97.032573°)
 No migratory bird nests observed

Structure #4
 (35.681013°, -97.034843°)
 No migratory bird nests observed

Matchline Map 4c

Matchline Map 4e

COMMENTS/LEGEND		Structures		Figure 4d
<p> NEPA Environmental Footprint</p> <p> Culvert</p> <p> NBI Bridge</p> <p> Matchlines</p>	<p>0 100 200 400 600 800 Feet</p>			



Structure #7
 (35.681018°, -97.028944°)
 No migratory bird nests observed

Structure #6
 (35.681017°, -97.029982°)
 No migratory bird nests observed

Structure #8 [NBI: 12596]
 (35.681017°, -97.029982°)
 100+ cliff swallow nests observed

COMMENTS/LEGEND		Structures JP 34318(04) / J3-4318(004) Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK		Figure 4e
NEPA Environmental Footprint Matchlines Culvert NBI Bridge	 			DRAWN BY: LMP APPRV BY: SOURCE: DEQ, Tiger 2000, USGS



Structure #9
 (35.680865°, -97.017213°)
 No migratory bird nests observed

Matchline Map 4e

COMMENTS/LEGEND			Structures JP 34318(04) / J3-4318(004) Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK		Figure 4f
NEPA Environmental Footprint Culvert NBI Bridge Matchlines					DRAWN BY: LMP APPRV BY: SOURCE: DEQ, Tiger 2000, USGS








COMMENTS/LEGEND	
	NEPA Environmental Footprint
	Photo Locations
	Matchlines
 0 100 200 400 600 800 Feet	

Photo Locations

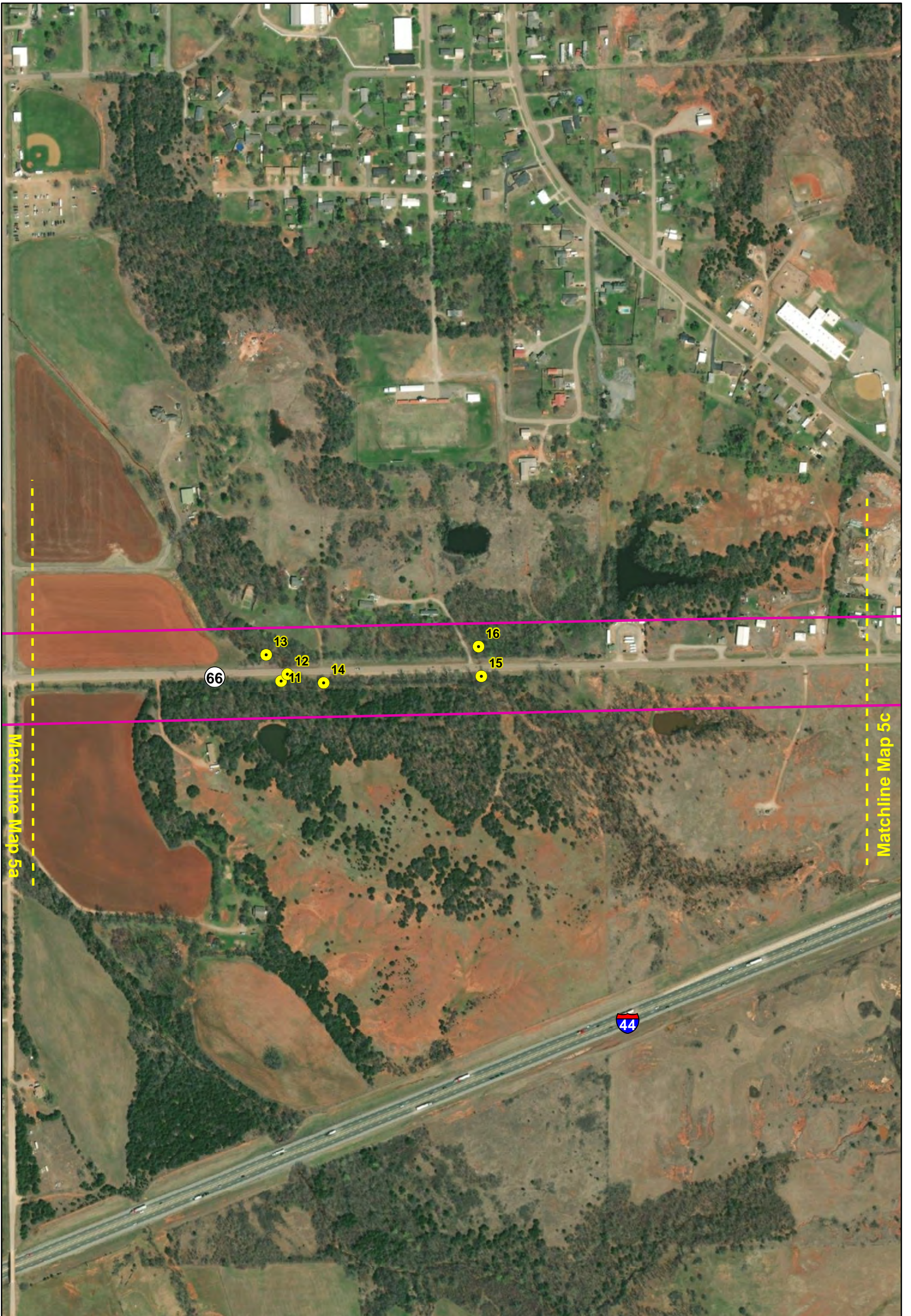
JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66
 from SH-102, east to US-177 (includes an
 approx. 1 mile turnpike project exception)
 Lincoln County, OK






DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

**Figure
5a**

Date: 4/7/2022



COMMENTS/LEGEND	
	NEPA Environmental Footprint
	Photo Locations
	Matchlines

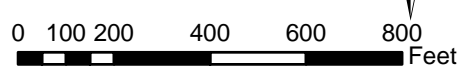


Photo Locations
 JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66
 from SH-102, east to US-177 (includes an
 approx. 1 mile turnpike project exception)
 Lincoln County, OK


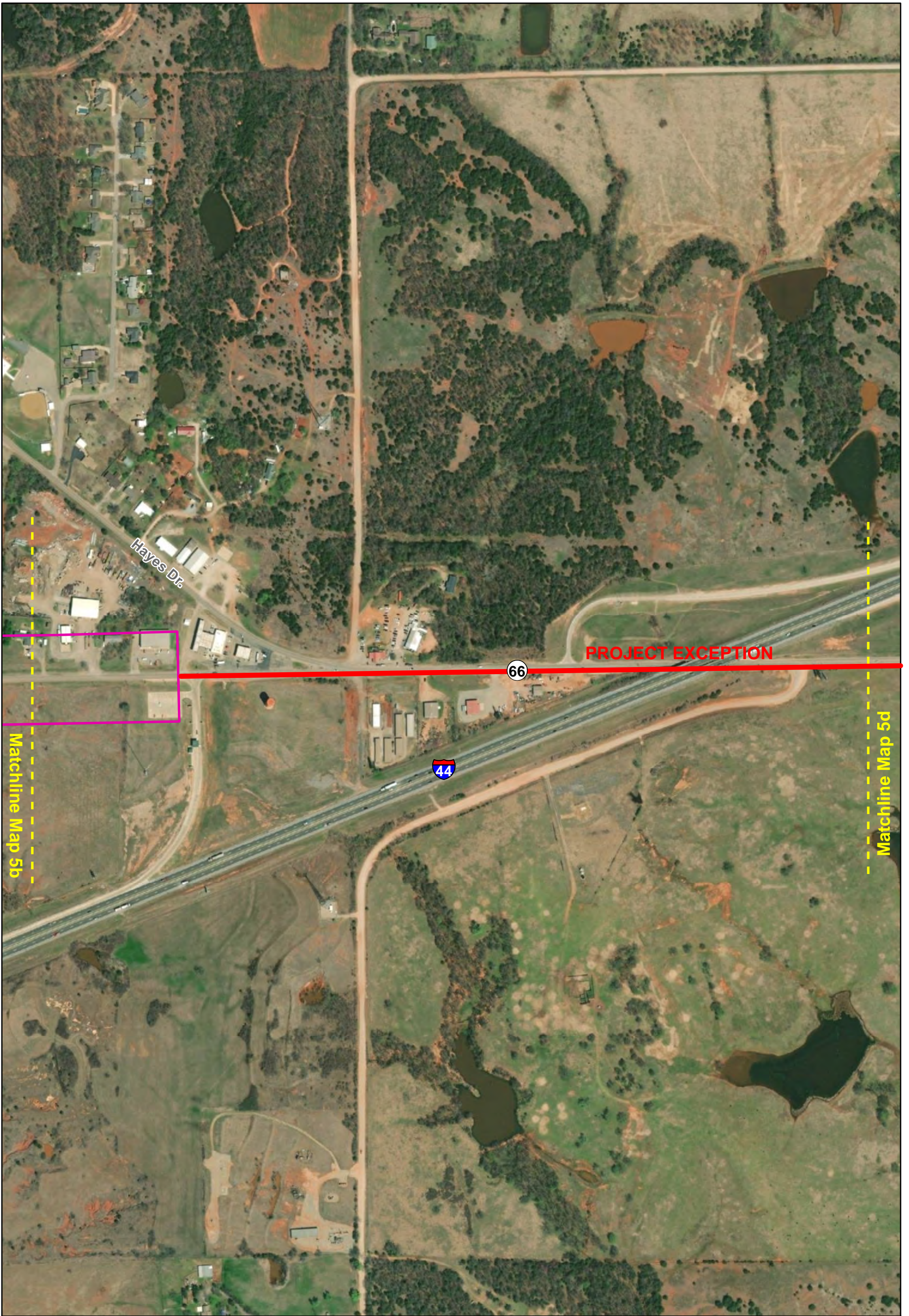
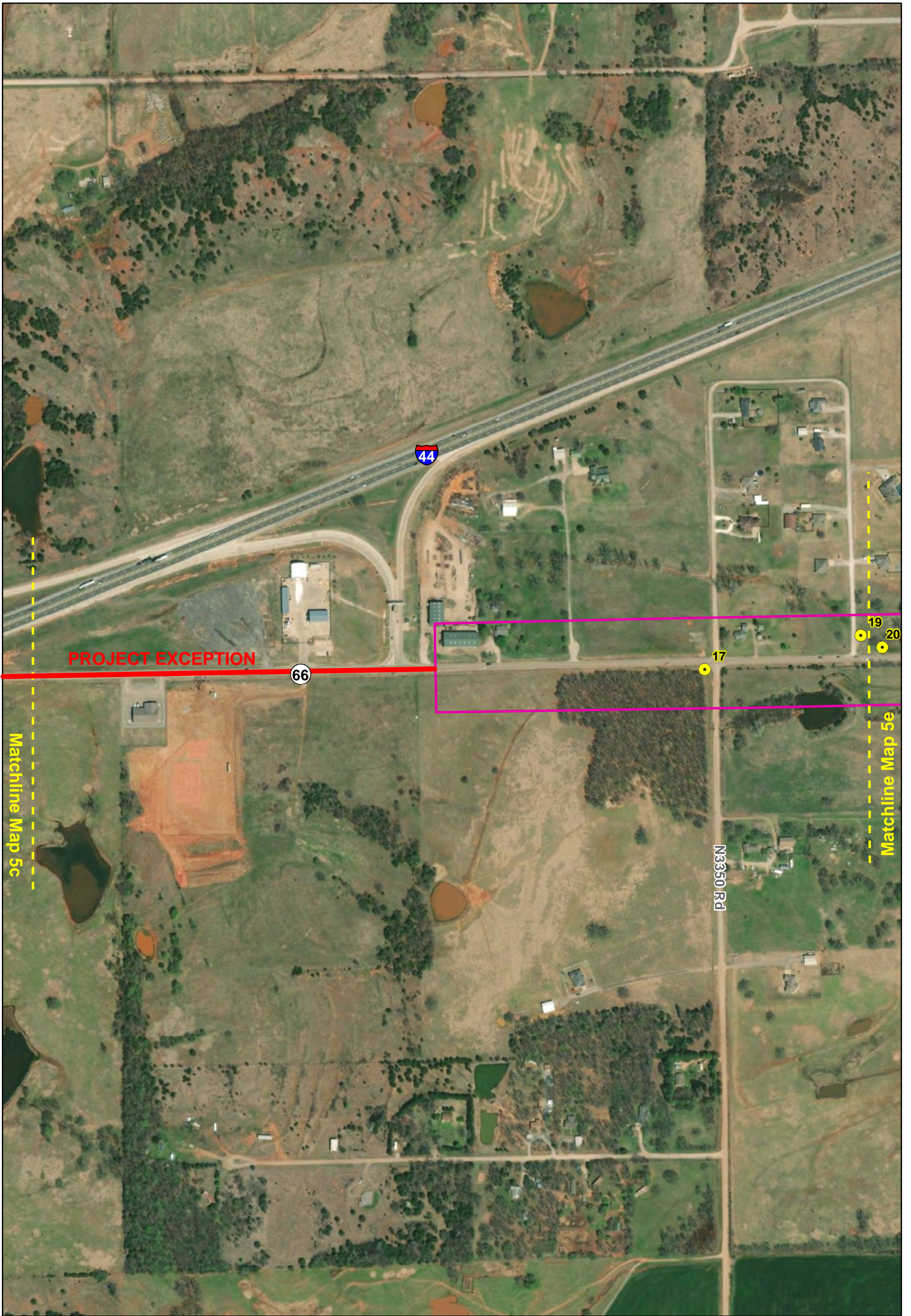

 DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

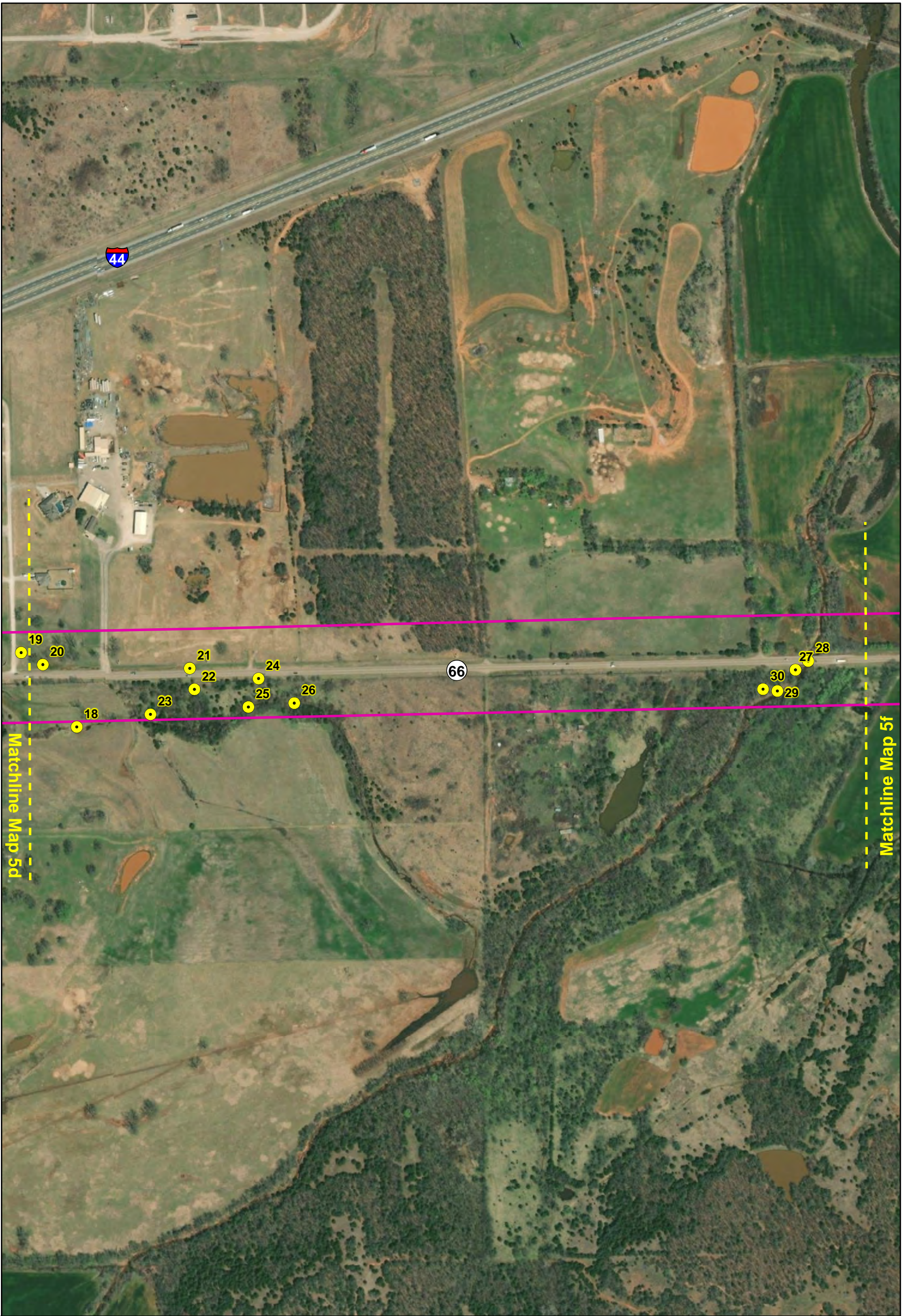
Figure 5b
 Date: 4/7/2022



COMMENTS/LEGEND			Photo Locations JP 34318(04) / J3-4318(004) Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK		Figure 5c
NEPA Environmental Footprint Photo Locations Matchlines	0 100 200 400 600 800 Feet			DRAWN BY: LMP APPRV BY: SOURCE: DEQ, Tiger 2000, USGS	Date: 4/7/2022



<p>COMMENTS/LEGEND</p> <p> NEPA Environmental Footprint</p> <p> Photo Locations</p> <p> Matchlines</p> <p>0 100 200 400 600 800 Feet</p>		<p>Photo Locations</p> <p>JP 34318(04) / J3-4318(004)</p> <p>Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK</p>	<p> OKLAHOMA Transportation</p> <p>DRAWN BY: LMP APPRV BY:</p> <p>SOURCE: DEQ, Tiger 2000, USGS</p>	<p>Figure 5d</p>
<p></p>				<p>Date: 4/7/2022</p>



COMMENTS/LEGEND				NEPA Environmental Footprint	<h2 style="margin: 0;">Photo Locations</h2> <p style="margin: 0;">JP 34318(04) / J3-4318(004) Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK</p>		<h2 style="margin: 0;">Figure 5e</h2>
Photo Locations Matchlines				<small>DRAWN BY: LMP APPRV BY: SOURCE: DEQ, Tiger 2000, USGS</small>		<small>Date: 4/7/2022</small>	



COMMENTS/LEGEND			Photo Locations JP 34318(04) / J3-4318(004) Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK		Figure 5f
NEPA Environmental Footprint Photo Locations Matchlines	0 100 200 400 600 800 Feet				DRAWN BY: LMP APPRV BY: SOURCE: DEQ, Tiger 2000, USGS



Photograph 1-A: Western edge of study area, facing west out of the footprint along SH-66.



Photograph 1-B: Western edge of study area, facing east into footprint along SH-66.



Photograph 2: NBI: 26639 over R2UBHx-01 (Captain Creek), migratory bird nests present.



Photograph 3: Beneath NBI: 26639.



Photograph 4: R2UBHx-01 (Captain Creek) facing south.



Photograph 5: R4SBCAx-01 (Unnamed Tributary to Captain Creek).



Photograph 6: Steeply sloped bank of R2UBHx-01 (Captain Creek).



Photograph 7: R4SBC-01 (Unnamed Tributary to Captain Creek) north of roadway.



Photograph 8: Location of PUSCx (no wetland feature present).



Photograph 9: Vegetated swale and location of a PEM1A north of roadway (no wetland feature present).



Photograph 10: Location of PEM1A-01, facing north.



Photograph 11: Structure 2 at R4SBC-02 (Unnamed trib to Captain Creek), migratory bird nests present.



Photograph 12: Beneath Structure 2, barn swallow nest.



Photograph 13: R4SBC-02 (Unnamed trib to Captain Creek) north of roadway.



Photograph 14: R4SBC-02 (Unnamed trib to Captain Creek) along south roadway ditch.



Photograph 15: Structure 3 at NDF-1, migratory bird nests present.



Photograph 16: NDF-1 north of roadway.



Photograph 17: Structure 4, no migratory bird nests or water feature present.



Photograph 18: Drainway (R4SBC-03) from PUBFh-01, facing west.



Photograph 19: Structure 5 at R4SBC-04, no migratory bird nests present.



Photograph 20: R4SBC-04 (Unnamed trib to Spring Creek) north of roadway.



Photograph 21: Structure 6 at R4SBC-04 (Unnamed trib to Spring Creek), no migratory bird nests present.



Photograph 22: R4SBC-04 south of roadway.



Photograph 23: R4SBC-03 (Unnamed Tributary) south of roadway.



Photograph 24: Structure 7, no migratory bird nests or water feature present.



Photograph 25: R4SBC-03 (Unnamed Tributary) south of roadway.



Photograph 26: PUBFh-02 south of roadway.



Photograph 27: Structure 8 (NBI: 12596) over R4SBA-01 (Spring Creek), migratory bird nests present.



Photograph 28: Cliff swallow nests beneath Structure 8 (NBI: 12596).



Photograph 29: R4SBA-01 (Spring Creek) south of roadway.



Photograph 30: Staining on trees in PW-01 south of roadway.



Photograph 31: Structure 9 at R4SBC-05, migratory bird nest present.



Photograph 32: R4SBC-05 (Unnamed Tributary to Deep Fork) south of roadway.



Photograph 33-A: East boundary study area, facing west toward study area.



Photograph 33-B: East boundary study area, facing east outside of study area.



United States Department of the Interior



FISH AND WILDLIFE SERVICE
Oklahoma Ecological Services Field Office
9014 East 21st Street
Tulsa, OK 74129-1428
Phone: (918) 581-7458 Fax: (918) 581-7467
<http://www.fws.gov/southwest/es/Oklahoma/>

In Reply Refer To:

April 08, 2022

Project Code: 2022-0029790

Project Name: Lincoln JP 34318(04) Widen, Resurface & Bridge on SH-66

Subject: List of threatened and endangered species that may occur in your proposed project location or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the ECOS-IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the ECOS-IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

A Biological Assessment is required for construction projects (or other undertakings having similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2))

(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<http://www.fws.gov/endangered/esa-library/pdf/TOC-GLOS.PDF>

Migratory Birds: In addition to responsibilities to protect threatened and endangered species under the Endangered Species Act (ESA), there are additional responsibilities under the Migratory Bird Treaty Act (MBTA) and the Bald and Golden Eagle Protection Act (BGEPA) to protect native birds from project-related impacts. Any activity, intentional or unintentional, resulting in take of migratory birds, including eagles, is prohibited unless otherwise permitted by the U.S. Fish and Wildlife Service (50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)). For more information regarding these Acts see <https://www.fws.gov/birds/policies-and-regulations.php>.

The MBTA has no provision for allowing take of migratory birds that may be unintentionally killed or injured by otherwise lawful activities. It is the responsibility of the project proponent to comply with these Acts by identifying potential impacts to migratory birds and eagles within applicable NEPA documents (when there is a federal nexus) or a Bird/Eagle Conservation Plan (when there is no federal nexus). Proponents should implement conservation measures to avoid or minimize the production of project-related stressors or minimize the exposure of birds and their resources to the project-related stressors. For more information on avian stressors and recommended conservation measures see <https://www.fws.gov/birds/bird-enthusiasts/threats-to-birds.php>.

In addition to MBTA and BGEPA, Executive Order 13186: *Responsibilities of Federal Agencies to Protect Migratory Birds*, obligates all Federal agencies that engage in or authorize activities that might affect migratory birds, to minimize those effects and encourage conservation measures that will improve bird populations. Executive Order 13186 provides for the protection of both migratory birds and migratory bird habitat. For information regarding the implementation of Executive Order 13186, please visit <https://www.fws.gov/birds/policies-and-regulations/executive-orders/e0-13186.php>.

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. Please include the Consultation Code in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment(s):

- Official Species List
 - USFWS National Wildlife Refuges and Fish Hatcheries
 - Migratory Birds
 - Wetlands
-

Official Species List

This list is provided pursuant to Section 7 of the Endangered Species Act, and fulfills the requirement for Federal agencies to "request of the Secretary of the Interior information whether any species which is listed or proposed to be listed may be present in the area of a proposed action".

This species list is provided by:

Oklahoma Ecological Services Field Office

9014 East 21st Street

Tulsa, OK 74129-1428

(918) 581-7458

Project Summary

Project Code: 2022-0029790
Event Code: None
Project Name: Lincoln JP 34318(04) Widen, Resurface & Bridge on SH-66
Project Type: Road/Hwy - Maintenance/Modification
Project Description: The proposed improvements consist of widening and resurfacing the existing lanes on SH-66 to create two 12-foot-wide driving lanes with 8-foot-wide paved shoulders. Improvements will be made symmetrically on existing alignment beginning approximately 1,075 feet east of Captain Creek, extending east to Hickory Ave (CR-3330). Beginning at Hickory Ave and extending east approximately 1,200 feet, improvements will transition to an offset alignment to the north to avoid impacts to the parallel stream channel south of SH-66. Immediately east of the offset improvements and extending east to approximately 950 feet west of CR-3550, a 12-foot-wide left turn lane will be added to match the 52-foot-wide road width of the OTA typical section (within the project design exception area). Starting approximately 1,875 feet east of CR-3350, improvements will transition back down to two-lane improvements on the existing alignment.

NBI# 26639 over Captain Creek is not considered deficient; however, erosion repairs will be made to the east pier and backwalls of the structure as part of the proposed construction activities.

NBI# 12596 over Spring Creek will be evaluated to assess whether the structure will be rehabilitated and extended or replaced with a 3-span bridge structure on the existing alignment.

The improvements on existing and offset alignments to the north will require the acquisition of new right-of-way, as well as the relocation of some utilities. Phased construction will be utilized as well as temporary widening to the north to maintain 2 driving lanes at all times throughout construction.

Project Location:

Approximate location of the project can be viewed in Google Maps: <https://www.google.com/maps/@35.6809988,-97.06862232292224,14z>



Counties: Lincoln County, Oklahoma

Endangered Species Act Species

There is a total of 4 threatened, endangered, or candidate species on this species list.

Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species.

IPaC does not display listed species or critical habitats under the sole jurisdiction of NOAA Fisheries¹, as USFWS does not have the authority to speak on behalf of NOAA and the Department of Commerce.

See the "Critical habitats" section below for those critical habitats that lie wholly or partially within your project area under this office's jurisdiction. Please contact the designated FWS office if you have questions.

1. [NOAA Fisheries](#), also known as the National Marine Fisheries Service (NMFS), is an office of the National Oceanic and Atmospheric Administration within the Department of Commerce.

Birds

NAME	STATUS
Piping Plover <i>Charadrius melodus</i> Population: [Atlantic Coast and Northern Great Plains populations] - Wherever found, except those areas where listed as endangered. There is final critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/6039	Threatened
Red Knot <i>Calidris canutus rufa</i> There is proposed critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/1864	Threatened
Whooping Crane <i>Grus americana</i> Population: Wherever found, except where listed as an experimental population There is final critical habitat for this species. The location of the critical habitat is not available. Species profile: https://ecos.fws.gov/ecp/species/758	Endangered

Insects

NAME	STATUS
Monarch Butterfly <i>Danaus plexippus</i> No critical habitat has been designated for this species. Species profile: https://ecos.fws.gov/ecp/species/9743	Candidate

Critical habitats

THERE ARE NO CRITICAL HABITATS WITHIN YOUR PROJECT AREA UNDER THIS OFFICE'S JURISDICTION.

USFWS National Wildlife Refuge Lands And Fish Hatcheries

Any activity proposed on lands managed by the [National Wildlife Refuge](#) system must undergo a 'Compatibility Determination' conducted by the Refuge. Please contact the individual Refuges to discuss any questions or concerns.

THERE ARE NO REFUGE LANDS OR FISH HATCHERIES WITHIN YOUR PROJECT AREA.

Migratory Birds

Certain birds are protected under the Migratory Bird Treaty Act¹ and the Bald and Golden Eagle Protection Act².

Any person or organization who plans or conducts activities that may result in impacts to migratory birds, eagles, and their habitats should follow appropriate regulations and consider implementing appropriate conservation measures, as described [below](#).

1. The [Migratory Birds Treaty Act](#) of 1918.
2. The [Bald and Golden Eagle Protection Act](#) of 1940.
3. 50 C.F.R. Sec. 10.12 and 16 U.S.C. Sec. 668(a)

THERE ARE NO FWS MIGRATORY BIRDS OF CONCERN WITHIN THE VICINITY OF YOUR PROJECT AREA.

Migratory Birds FAQ

Tell me more about conservation measures I can implement to avoid or minimize impacts to migratory birds.

[Nationwide Conservation Measures](#) describes measures that can help avoid and minimize impacts to all birds at any location year round. Implementation of these measures is particularly important when birds are most likely to occur in the project area. When birds may be breeding in the area, identifying the locations of any active nests and avoiding their destruction is a very helpful impact minimization measure. To see when birds are most likely to occur and be breeding in your project area, view the Probability of Presence Summary. [Additional measures](#) or [permits](#) may be advisable depending on the type of activity you are conducting and the type of infrastructure or bird species present on your project site.

What does IPaC use to generate the migratory birds potentially occurring in my specified location?

The Migratory Bird Resource List is comprised of USFWS [Birds of Conservation Concern \(BCC\)](#) and other species that may warrant special attention in your project location.

The migratory bird list generated for your project is derived from data provided by the [Avian Knowledge Network \(AKN\)](#). The AKN data is based on a growing collection of [survey, banding, and citizen science datasets](#) and is queried and filtered to return a list of those birds reported as occurring in the 10km grid cell(s) which your project intersects, and that have been identified as warranting special attention because they are a BCC species in that area, an eagle ([Eagle Act](#) requirements may apply), or a species that has a particular vulnerability to offshore activities or development.

Again, the Migratory Bird Resource list includes only a subset of birds that may occur in your project area. It is not representative of all birds that may occur in your project area. To get a list of all birds potentially present in your project area, please visit the [AKN Phenology Tool](#).

What does IPaC use to generate the probability of presence graphs for the migratory birds potentially occurring in my specified location?

The probability of presence graphs associated with your migratory bird list are based on data provided by the [Avian Knowledge Network \(AKN\)](#). This data is derived from a growing collection of [survey, banding, and citizen science datasets](#).

Probability of presence data is continuously being updated as new and better information becomes available. To learn more about how the probability of presence graphs are produced and how to interpret them, go to the Probability of Presence Summary and then click on the "Tell me about these graphs" link.

How do I know if a bird is breeding, wintering, migrating or present year-round in my project area?

To see what part of a particular bird's range your project area falls within (i.e. breeding, wintering, migrating or year-round), you may refer to the following resources: [The Cornell Lab of Ornithology All About Birds Bird Guide](#), or (if you are unsuccessful in locating the bird of interest there), the [Cornell Lab of Ornithology Neotropical Birds guide](#). If a bird on your migratory bird species list has a breeding season associated with it, if that bird does occur in your project area, there may be nests present at some point within the timeframe specified. If "Breeds elsewhere" is indicated, then the bird likely does not breed in your project area.

What are the levels of concern for migratory birds?

Migratory birds delivered through IPaC fall into the following distinct categories of concern:

1. "BCC Rangewide" birds are [Birds of Conservation Concern](#) (BCC) that are of concern throughout their range anywhere within the USA (including Hawaii, the Pacific Islands, Puerto Rico, and the Virgin Islands);
2. "BCC - BCR" birds are BCCs that are of concern only in particular Bird Conservation Regions (BCRs) in the continental USA; and
3. "Non-BCC - Vulnerable" birds are not BCC species in your project area, but appear on your list either because of the [Eagle Act](#) requirements (for eagles) or (for non-eagles) potential susceptibilities in offshore areas from certain types of development or activities (e.g. offshore energy development or longline fishing).

Although it is important to try to avoid and minimize impacts to all birds, efforts should be made, in particular, to avoid and minimize impacts to the birds on this list, especially eagles and BCC species of rangewide concern. For more information on conservation measures you can implement to help avoid and minimize migratory bird impacts and requirements for eagles, please see the FAQs for these topics.

Details about birds that are potentially affected by offshore projects

For additional details about the relative occurrence and abundance of both individual bird species and groups of bird species within your project area off the Atlantic Coast, please visit the [Northeast Ocean Data Portal](#). The Portal also offers data and information about other taxa besides birds that may be helpful to you in your project review. Alternately, you may download the bird model results files underlying the portal maps through the [NOAA NCCOS Integrative Statistical](#)

[Modeling and Predictive Mapping of Marine Bird Distributions and Abundance on the Atlantic Outer Continental Shelf](#) project webpage.

Bird tracking data can also provide additional details about occurrence and habitat use throughout the year, including migration. Models relying on survey data may not include this information. For additional information on marine bird tracking data, see the [Diving Bird Study](#) and the [nanotag studies](#) or contact [Caleb Spiegel](#) or [Pam Loring](#).

What if I have eagles on my list?

If your project has the potential to disturb or kill eagles, you may need to [obtain a permit](#) to avoid violating the Eagle Act should such impacts occur.

Proper Interpretation and Use of Your Migratory Bird Report

The migratory bird list generated is not a list of all birds in your project area, only a subset of birds of priority concern. To learn more about how your list is generated, and see options for identifying what other birds may be in your project area, please see the FAQ "What does IPaC use to generate the migratory birds potentially occurring in my specified location". Please be aware this report provides the "probability of presence" of birds within the 10 km grid cell(s) that overlap your project; not your exact project footprint. On the graphs provided, please also look carefully at the survey effort (indicated by the black vertical bar) and for the existence of the "no data" indicator (a red horizontal bar). A high survey effort is the key component. If the survey effort is high, then the probability of presence score can be viewed as more dependable. In contrast, a low survey effort bar or no data bar means a lack of data and, therefore, a lack of certainty about presence of the species. This list is not perfect; it is simply a starting point for identifying what birds of concern have the potential to be in your project area, when they might be there, and if they might be breeding (which means nests might be present). The list helps you know what to look for to confirm presence, and helps guide you in knowing when to implement conservation measures to avoid or minimize potential impacts from your project activities, should presence be confirmed. To learn more about conservation measures, visit the FAQ "Tell me about conservation measures I can implement to avoid or minimize impacts to migratory birds" at the bottom of your migratory bird trust resources page.

Wetlands

Impacts to [NWI wetlands](#) and other aquatic habitats may be subject to regulation under Section 404 of the Clean Water Act, or other State/Federal statutes.

For more information please contact the Regulatory Program of the local [U.S. Army Corps of Engineers District](#).

Please note that the NWI data being shown may be out of date. We are currently working to update our NWI data set. We recommend you verify these results with a site visit to determine the actual extent of wetlands on site.

RIVERINE

- [R2UBHx](#)
- [R5UBF](#)
- [R4SBAx](#)
- [R4SBA](#)
- [R4SBC](#)

FRESHWATER FORESTED/SHRUB WETLAND

- [PFO1A](#)

FRESHWATER POND

- [PUBFx](#)
- [PUBFh](#)
- [PUSC_x](#)

FRESHWATER EMERGENT WETLAND

- [PEM1A](#)
-

IPaC User Contact Information

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State: OK
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Phone: 4053218181

Lead Agency Contact Information

Lead Agency: Federal Highway Administration

WATERS AND WETLANDS EVALUATION REPORT

For

County	Lincoln	JP Number	34318(04)	Project Number	J3-4318(004)
Road Number	SH-66	Water Body Name		Captain Creek and Spring Creek and unnamed tributaries	
ROW Date	2025	Let Date	2029	Project Length	~2.8 miles
Project General Location		SH-66 south of Wellston, OK between SH-102 and US-177 (includes an approximately 1-mile turnpike project exception)			
Project Description & Statement From Oracle		Widen, Resurface & Bridge SH-66: from SH-102, east to US-177			

Prepared for:
 Oklahoma Department of Transportation
 Environmental Programs Division
 200 NE 21st Street
 Oklahoma City, OK 73105

Prepared by:

Biologist Name	Leah M Peterson and Dale W. Daniel
Company/Agency Name	CC Environmental, LLC
Address	P.O. Box 1292
City, State Zip	Norman, OK 73070

Report Date:	April 9, 2022
Field Date:	March 28, 2022

PROJECT OVERVIEW

Project Type (Choose one)	Check ✓
Bridge and Approaches or bridge widening/structure extension	
Grade, Drain, Surface and Bridge	✓
Grade, Drain and Surface	
Asphalt Overlay Resurfacing	
Widen and Resurface existing lanes	
Pavement Reconstruction or rehabilitation	
Bridge Rehabilitation	
Safety Improvements (Cable Barrier, Guardrail, signage)	
Intersection Modifications	
Safe Routes to School (Describe)	
Enhancements (Describe)	
Other (Describe)	

Description of the **existing** bridge/roadway

This project consists of two discontinuous segments of SH-66 located in a rural area south of Wellston, OK, and traverses rolling hills and intersects I-44 (Turner Turnpike). The first segment begins roughly 0.35 mile east of the SH-102/SH-66 junction and extends east approximately 1.5 miles to just west of the I-44 Turner Turnpike southbound onramp. The second segment begins just east of the northbound onramp to I-44 (~1 mile east of the end of the first segment) and extends east 1.3 miles to 0.1 mile east of the US-177/SH-66 junction. The existing project portion of SH-66 is an open section major collector facility with two 12-foot-wide paved driving lanes and 0 to 2-foot-wide shoulders. It has deteriorating pavement, narrow shoulders, poor traffic flow, and limited sight distances in places. The current traffic volume is estimated at 5,300 vehicles per day (VPD).

Within the project areas there are two bridge sized structures—Bridge #1 (NBI #26639) over Captain Creek and Bridge #2 (NBI #12596) over Spring Creek. NBI# 26639 over Captain Creek is a 3-span bridge situated at a 30-degree skew with the stream channel. The three spans measure 80-100-80-feet in length. The total structure length is 262 feet and the horizontal clearance is 40 feet wide. This structure was constructed in 2001, has a sufficiency rating of 99.2, is considered not deficient, but is in need of structural repair. NBI# 12596 over Spring Creek is a large reinforced concrete box (RCB) structure with three cells that measure 13-17-13-feet-wide by 15 feet tall and 36 feet long with concrete wing walls. This structure is situated at a 30-degree skew to SH-66 to accommodate Spring Creek stream channel. The structure has guard rails and a

horizontal clearance of 24 feet. This RCB was constructed in 1951, has a sufficiency rating of 65.60 and is considered functionally obsolete.

The purpose of the proposed project is to improve safety, and the need for the project is to correct a roadway with deteriorating pavement, narrow shoulders, poor traffic flow and limited sight distances, and to correct a functionally obsolete bridge and a bridge at risk of becoming structurally deficient.

Description of proposed improvements SPECIFIC TO THIS PROJECT

The proposed improvements consist of widening and resurfacing the existing lanes on SH-66 to create two 12-foot-wide driving lanes with 8-foot-wide paved shoulders. Improvements will be made symmetrically on existing alignment beginning approximately 1,075 feet east of Captain Creek, extending east to Hickory Ave (CR-3330). Beginning at Hickory Ave and extending east approximately 1,200 feet, improvements will transition to an offset alignment to the north to avoid impacts to the parallel stream channel south of SH-66. Immediately east of the offset improvements and extending east to approximately 950 feet west of CR-3550, a 12-foot-wide left turn lane will be added to match the 52-foot-wide road width of the turnpike’s typical section (within the project design exception area). Starting approximately 1,875 feet east of CR-3350, improvements will transition back down to two-lane improvements on the existing alignment.

NBI# 26639 over Captain Creek is not considered deficient; however, structural repairs will be made to the east pier and backwalls as part of the proposed construction activities.

NBI# 12596 over Spring Creek will be evaluated to assess whether the structure will be rehabilitated and extended or replaced with a 3-span bridge structure on the existing alignment.

The improvements on existing and offset alignments to the north will require the acquisition of new right-of-way, as well as the relocation of some utilities. Phased construction will be utilized as well as temporary widening to the north to maintain 2 driving lanes at all times throughout construction.

Project Environmental Study Footprint

Project Location		Environmental Study Footprint	
<u>Section Range & Township</u>	<u>Lat/Long (NAD 83)</u>	<u>Dimensions</u>	<u>Acreage</u>
S13, 14, 15, 22, 23, 24-T14N-R2E;	West Boundary: (35.680993°, -97.081982°) East Boundary: (35.681000°, -97.015518°)	Two separate study areas: West study area 8,000 feet long and 200 feet L/R of center line. East study area 7,000 feet long and 200 feet L/R of center line.	137.3 acres
S17, 18, 19, 20-T14N-R3E			

Environmental Study Footprint Soils (NRCS Soil Survey Map)

Map Unit Name	Percent Slope	Drainage Class	Hydric Rating		Description
			YES	NO	
CbC3 – Coyle and Seminole soils	3 to 5	Well drained		X	Loam, severely eroded
DaSE –Darnell-Stephenville Complex	3 to 12	Excessively drained		X	Fine sandy loam
DuD – Dougherty	3 to 5	Well drained		X	Loamy fine sand
KoD3 – Konawa	3 to 8	Well drained		X	Loamy fine sand, severely eroded
Mc – Miller	0 to 1	Moderately well drained		X	Clay, occasionally flooded
Pc – Ashport	0 to 1	Well drained		X	Clay loam, occasionally flooded
Po – Easpor	0 to 1	Well drained		X	Loam, occasionally flooded
Ps – Pulaski	0 to 1	Well drained		X	Fine sandy loam, occasionally flooded
RvC3 – Renthin-Grainola complex	3 to 5	Well drained		X	Silty clay loam, severely eroded
Rx – Ustibuck	0 to 1	Poorly drained	X		Clay, occasionally flooded
StC – Stephenville	3 to 5	Well drained		X	Fine sandy loam
TeB – Teller	1 to 3	Well drained		X	Loam
TeC2 – Teller	3 to 5	Well drained		X	Loam, eroded
VaB – Vanoss	1 to 3	Well drained		X	Loam
VcC – Grainola	3 to 5	Well drained		X	Clay loam
VIE – Grainola-Ironmound complex	5 to 15	Well drained		X	Clay loam

Environmental Study Footprint General Description and Vegetation Present

The study area is located in the Northern Cross Timbers ecoregion of Oklahoma. This ecoregion is typified by rolling hills with a mosaic of tall grass prairie (e.g., big bluestem, little bluestem, switchgrass, and Indiangrass), intermixed with oak-hickory woodlands. Land uses are dominated by rangeland, pastureland, and limited cropland with small grains, sorghum, hay, and soybeans. Woodlands have become more common with fire suppression and passive land use.

The study area occurs in the East Captain Creek and Spring Creek-Deep Fork Canadian River Subwatersheds. Review of the U.S. Geological Survey (USGS) 7.5-Minute Topographic Map and National Wetlands Inventory (NWI) Maps identified eight mapped streams including Captain Creek and three unnamed tributaries, Spring Creek and two unnamed tributaries, and one unnamed tributary to Deep Fork. In addition, multiple wetlands were noted within the study area during field review.

The following community types were identified within the NEPA study footprint:

Maintained Right-of-Way (ROW) Vegetation- Dominant vegetation in this community type included: cheatgrass (*Bromus tectorum*), henbit (*Lamium amplexicaule*), bermudagrass (*Cynodon dactylon*), switchgrass (*Panicum virgatum*), little bluestem (*Schizachyrium scoparium*), and johnsongrass (*Sorghum halepense*).

Upland Tall-Grass Prairie- Dominant vegetation in this community type included: bermudagrass (*Cynodon dactylon*), switchgrass (*Panicum virgatum*), little bluestem (*Schizachyrium scoparium*), big bluestem (*Andropogon gerardi*), Indiangrass (*Sorghastrum nutans*), and johnsongrass (*Sorghum halepense*).

Upland Woodland- Dominant vegetation in this community type included: cedar (*Juniperus virginiana*), elm (*Ulmus spp.*), post oak (*Quercus stellata*), and blackjack oak (*Quercus marilandica*), bradford pear (*Pyrus calleryana*), and redbud (*Cercis canadensis*).

Wooded Riparian Corridor- Dominant vegetation in this community type included: various oaks (*Quercus spp.*), sycamore (*Platanus occidentalis*), cottonwood (*Populus deltoides*), eastern red cedar (*Juniperus virginiana*), redbud (*Cercis canadensis*), and elm (*Ulmus spp.*). Understory herbaceous vegetation included creek oats (*Chasmanthium latifolium*), horsetail (*Equisetum hymale*), greenbrier (*Smilax bona-nox*), and coralberry (*Symphoricarpos orbiculatus*).

WATERS AND WETLANDS EVALUATION

Data Sources Reviewed (list)

USGS 7.5 minute Quad	NWI Map	USACE Wetland Regional Supplement	Additional Resources Reviewed
Wellston (1966)	Wellston (1989)	Great Plains	

Wetlands and Ponds Summary Table

Field Sites	Type of Wetland or Pond	Cowardin Classification	Potential Jurisdictional Status	Acres within Environmental Study Footprint
PEM1A-01	Depressional Freshwater Emergent	PEM1A	Not Likely	0.016 acre
PUBFh-01	Stock Pond	PUBFh	Likely	0.260 acre
PUBFh-02	Stock Pond	PUBFh	Likely	0.110 acre
PW-01	Riverine Wetland	N/A (not on NWI)	Likely	0.450 acre

Streams and Drainages Summary Table

Field Sites	Stream Name	USGS Mapped Status	Potential Jurisdictional Status	Acres within Environmental Study Footprint	Linear Feet within Environmental Study Footprint
R2UBHx-01	Captain Creek	Mapped Perennial	Likely	0.210 acre	487 feet
R4SBAx-01	Unnamed Tributary to Captain Creek	Mapped Intermittent	Likely	0.058 acre	226 feet
R4SBC-01	Unnamed Tributary to Captain Creek	Mapped Intermittent	Not Likely	0.048 acre	661 feet
R4SBC-02	Unnamed Tributary to Captain Creek	Mapped Intermittent	Likely	0.430 acre	1,570 feet
NDF-1	Non-Mapped Drainage Feature	Not Mapped	Likely	0.009 acre	202 feet
R4SBC-03	Unnamed Tributary	Mapped Intermittent	Likely	0.153 acre	606 feet
R4SBC-04	Unnamed Tributary to Spring Creek	Mapped Intermittent	Likely	0.179 acre	1,130 feet
R4SBA-01	Spring Creek	Mapped Intermittent	Likely	0.250 acre	550 feet

Field Sites	Stream Name	USGS Mapped Status	Potential Jurisdictional Status	Acres within Environmental Study Footprint	Linear Feet within Environmental Study Footprint
R4SBC-05	Unnamed Tributary to Deep Fork	Mapped Intermittent	Likely	0.121 acre	720 feet

Streams and other linear aquatic features

R2UBHx-01 (Captain Creek) was classified as a permanently flooded, lower perennial, riverine streambed that has been excavated. This feature is a fourth-order stream that flows from south to north within the study area before ultimately emptying into Deep Fork north of the study area. Approximately 487 linear feet (0.210 acre) of R2UBHx-01 occurs within the study area. The ordinary high-water mark was approximately 20 feet in width. At the time of reconnaissance, the stream banks were very steeply incised, and R2UBHx-01 had approximately 1 to 3 feet of slowly moving water over a silty loam substrate overlaid with scattered riprap. Riparian vegetation was a mix of right-of-way vegetation including johnsongrass and bermudagrass as well as riparian wooded corridor including sycamore, eastern red cedar, redbud, and elm. Because of its mapped status and field-verified bed and bank characteristics, R2UBHx-01 is considered **likely** jurisdictional.

R4SBAx-01 (Unnamed Tributary to Captain Creek) was classified as a temporarily flooded, intermittent, riverine streambed that has been excavated. This feature is a third-order stream that flows from south to north within the study area before ultimately emptying into Captain Creek at Structure 1 (NBI: 26639). The feature appears to have been redirected south of the roadway and no longer flows north of SH-66 as mapped. Approximately 226 linear feet (0.058 acre) of R4SBAx-01 occurs within the study area. The ordinary high-water mark was approximately 3 feet in width. At the time of reconnaissance, the stream banks were very steeply incised and mostly flowed through riprap. Riparian vegetation was primarily johnsongrass. Because of its mapped status and field-verified bed and bank characteristics, R4SBAx-01 is considered **likely** jurisdictional.

R4SBC-01 (Unnamed Tributary to Captain Creek) was classified as a seasonally flooded, intermittent, riverine streambed. This feature is a first-order stream that flows from west to east along the north side roadway ditch of SH-66 before emptying into Captain Creek. Approximately 661 linear feet (0.048 acre) of R4SBC-01 occurs within the study area. At the time of reconnaissance, the mapped drainage way of the feature appeared to be vegetated and regularly maintained as part of the highway maintenance. Furthermore, it did not appear that the watershed for this feature included surface water south of the roadway as indicated by the USGS and NWI maps. R4SBC-01 is characteristic of a roadside drainage ditch and does not exhibit clear bed and bank or stream hydrology; consequently, R4SBC-01 is considered **not likely** jurisdictional.

R4SBC-02 (Unnamed Tributary to Captain Creek) was classified as a seasonally flooded, intermittent, riverine streambed. This feature is a first-order stream that flows east to west along the roadway ditch on the south side of SH-66 before crossing the highway at Structure 2 and continuing northwest to Captain Creek. Approximately 1,570 linear feet (0.430 acre) of R4SBC-02 occurs within the study area. The ordinary high-water mark was approximately 6 feet in width. At the time of reconnaissance, the stream banks were very steeply incised and R4SBC-02 had approximately 6 inches of slowly moving water over a silty loam substrate. Riparian vegetation was consistent with the riparian wooded corridor ecotype described above and included sycamore, eastern red cedar, redbud, and elm with an understory of greenbrier and coralberry. Because of its mapped status and field-verified bed and bank characteristics, R4SBC-02 is considered **likely** jurisdictional.

NDF-1 (Non-Mapped Drainage Feature) was a non-mapped feature identified north of the roadway. This feature appears to flow from north to south before emptying into R4SBC-02 at Structure 3. Approximately 202 linear feet (0.009 acre) of NDF-1 occurs within the study area. The ordinary high-water mark was approximately 2 feet in width. At the time of reconnaissance, the stream banks were incised and the feature was dry. Riparian vegetation was consistent with the riparian wooded corridor ecotype described above and contained species including redbud, elm, bradford pear, and eastern red cedar with greenbrier in the understory. Because NDF-1 exhibited bed/bank characteristics along with the hydrogeomorphology of a stream feature, it is considered **likely** jurisdictional.

R4SBC-03 (Unnamed Tributary) was classified as a seasonally flooded, intermittent, riverine streambed. This feature is a first-order stream that flows west to east along the south boundary of the study area. Approximately 606 linear feet (0.153 acre) of R4SBC-03 occurs within the study footprint. The ordinary high-water mark ranged from 3 to 6 feet in width. At the time of reconnaissance, the stream banks were narrow and shallowly incised. In some portions of the feature, the streambed was heavily vegetated and poorly defined. Other portions of the stream contained approximately 6 inches of slowly-moving water over a silty loam substrate. Riparian vegetation was consistent with the upland tall-grass prairie and riparian wooded corridor ecotypes described above, and included a mix of little bluestem, Indiangrass, bermudagrass, johnsongrass, blackberry (*Rubus oklahomus*) and switchgrass as well as sycamore, willow (*Salix nigra*), cottonwood, eastern red cedar, redbud, and elm. Because of its mapped status and field-verified bed and bank characteristics, R4SBC-03 is considered **likely** jurisdictional.

R4SBC-04 (Unnamed Tributary to Spring Creek) was classified as a seasonally flooded, intermittent, riverine streambed. This feature is a first-order stream that flows west to east along the roadway ditch on the north side of SH-66 before crossing the roadway at Structure 6 and merging with R4SBC-03 near the south boundary of the study area. Approximately 1,130 linear feet (0.179 acre) of R4SBC-04 occurs within the study area. The ordinary high-water mark was approximately 3 feet in width. At the time of reconnaissance, the stream banks were shallowly incised and R4SBC-04 had approximately 6 inches of slowly moving water over a silty loam substrate with some rock and riprap along the roadside ditch. Riparian vegetation was consistent with maintained right-of-way vegetation ecotype described above and included primarily bermudagrass. South of the roadway, R4SBC-04 flowed through some wooded riparian corridor

consisting of eastern red cedar, redbud, and elm. Because of its mapped status and field-verified bed and bank characteristics, R4SBC-04 is considered likely jurisdictional.

R4SBA-01 (Spring Creek) was classified as a temporarily flooded, intermittent, riverine streambed. This feature is a third-order stream that flows south to north crossing the roadway at Structure 8 (NBI: 12596) before emptying into Deep Fork north of the study area. Approximately 550 linear feet (0.250 acre) of R4SBA-01 occurs within the study footprint. The ordinary high-water mark was approximately 15 feet in width. At the time of reconnaissance, the stream banks were very steeply incised and R4SBA-01 had approximately 12 inches of slowly moving water over a silty loam substrate. Riparian vegetation was consistent with the riparian wooded corridor ecotype described above and included sycamore, eastern red cedar, redbud, and elm with an understory of creek oats and horsetail. Because of its mapped status and field-verified bed and bank characteristics, R4SBA-01 is considered likely jurisdictional.

R4SBC-05 (Unnamed Tributary to Deep Fork) was classified as a seasonally flooded, intermittent, riverine streambed. This feature is a first-order stream that flows south to north along the roadway ditch on the west side of US-177 before crossing the roadway at Structure 9 and continuing to the east to Deep Fork. Approximately 720 linear feet (0.121 acre) of R4SBC-05 occurs within the study area. The ordinary high-water mark was approximately 6 feet in width. The stream flows through a primarily vegetated and regularly maintained SH-66 ditch. Riparian vegetation was consistent with the maintained right-of-way vegetation ecotype described above and included primarily bermudagrass. Because of its mapped status and field-verified bed and bank characteristics, R4SBC-05 is considered likely jurisdictional.

Wetlands and ponds

PEMIA-01 (Depressional freshwater emergent) was classified as a temporarily flooded, persistent emergent, palustrine system. This wetland appeared to be an isolated depressional feature embedded in a grazed pasture north of the roadway. Emergent vegetation noted within the wetland footprint included spikerush (*Eleocharis palustris*) and bulrush (*Schoenoplectus fluviatilis*). Wetland soils contained a high clay content and exhibited redoximorphic features. This feature exhibited characteristics consistent with a wetland feature but did not have a significant surface water nexus to jurisdictional waterbodies and thus, was considered not likely jurisdictional.

PUBFh-01 and -02 (Stock Ponds) were classified as semi-permanently flooded, palustrine impoundments with unconsolidated bottoms. Both of these features appear to be constructed impoundments excavated within a potentially jurisdictional stream (R4SBC-03). These were open water features that share a significant surface water nexus to a potentially jurisdictional waterbody and thus, were both considered likely jurisdictional.

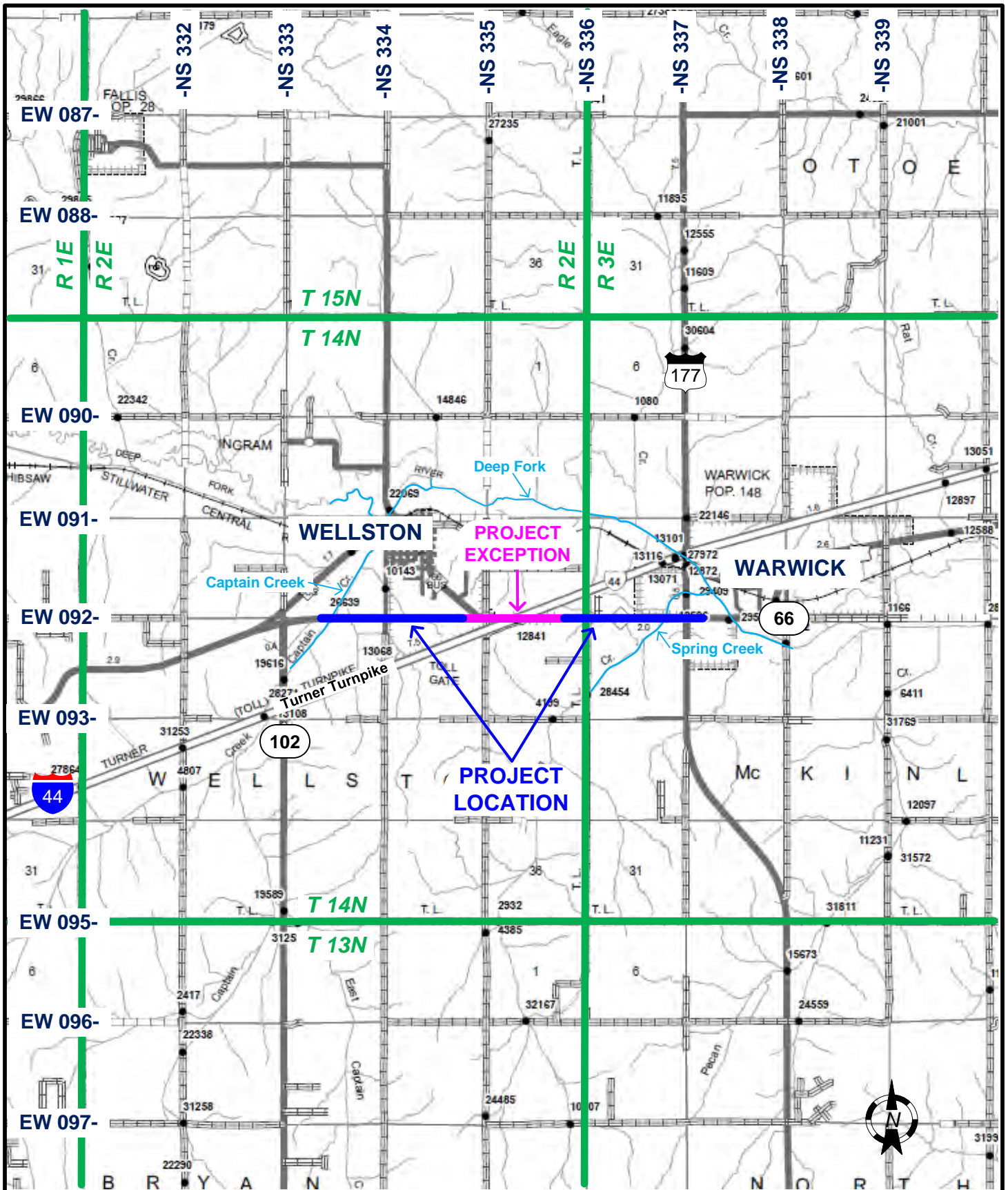
PW-01 (Riverine Wetland) was not identified on the NWI maps; however, during field reconnaissance, a depressional wetland feature was noted in the wooded riparian corridor on the west side of Spring Creek, south of the roadway. The feature was dry at the time of reconnaissance, but significant water staining was noted on tree trunks, and soils exhibited redoximorphic features.

Vegetation noted within the wetland included elm, sycamore, horsetail, and creek oats. It appears the major hydrologic input into PW-01 is overbank flow from Spring Creek, thus PW-01 is considered likely jurisdictional.

REFERENCES

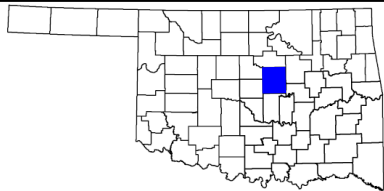
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FIGURES



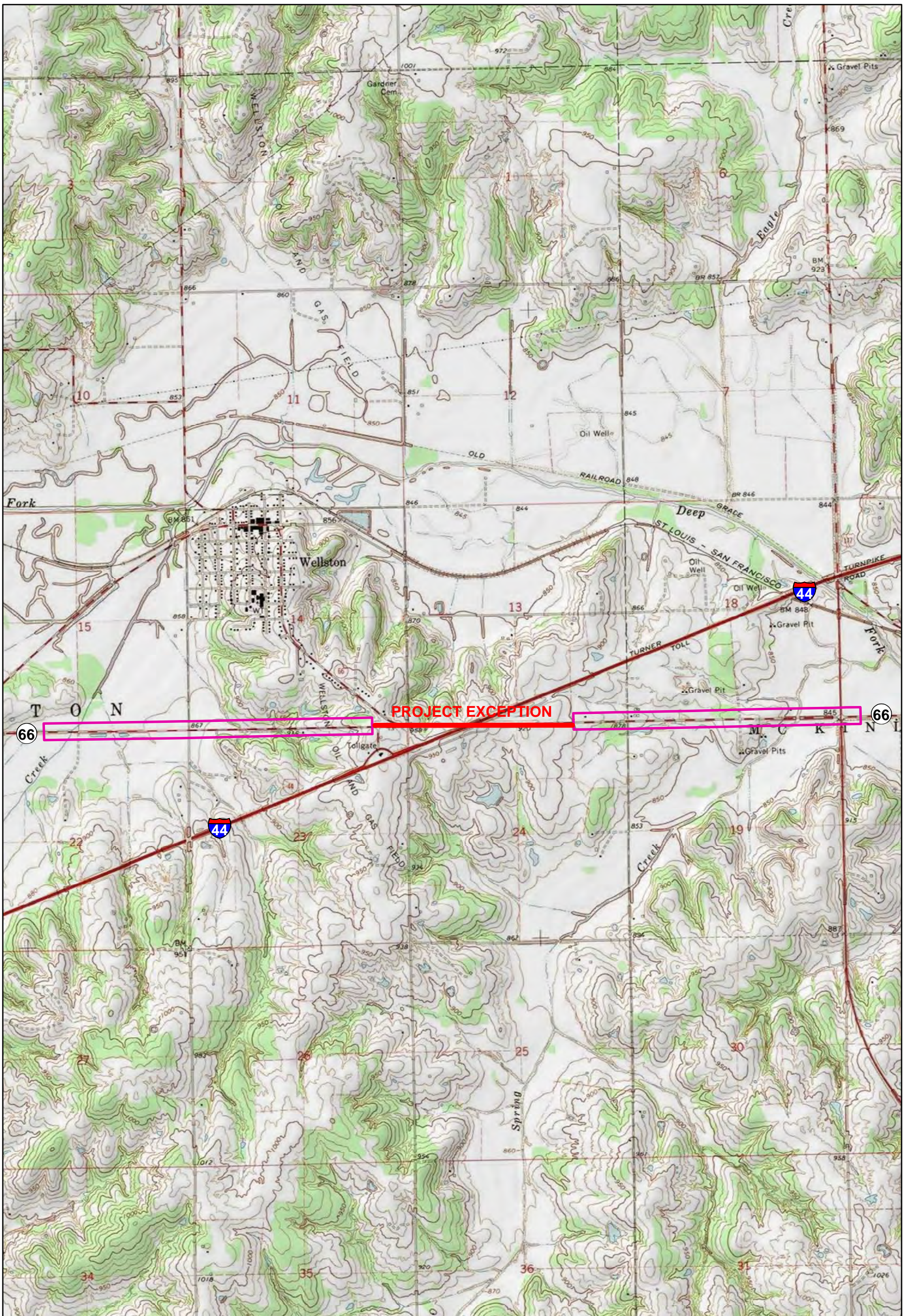
LOCATION MAP

JP 34318(04); Project No. J3-4318(004)
 Widen, Resurface & Bridge SH-66:
 From SH-102, east to US-177
 Lincoln Co, OK




OKLAHOMA
 Transportation


Figure 1



COMMENTS/LEGEND

 NEPA Environmental Footprint

0 550 1,100 2,200 3,300 4,400 Feet



Topographic Map

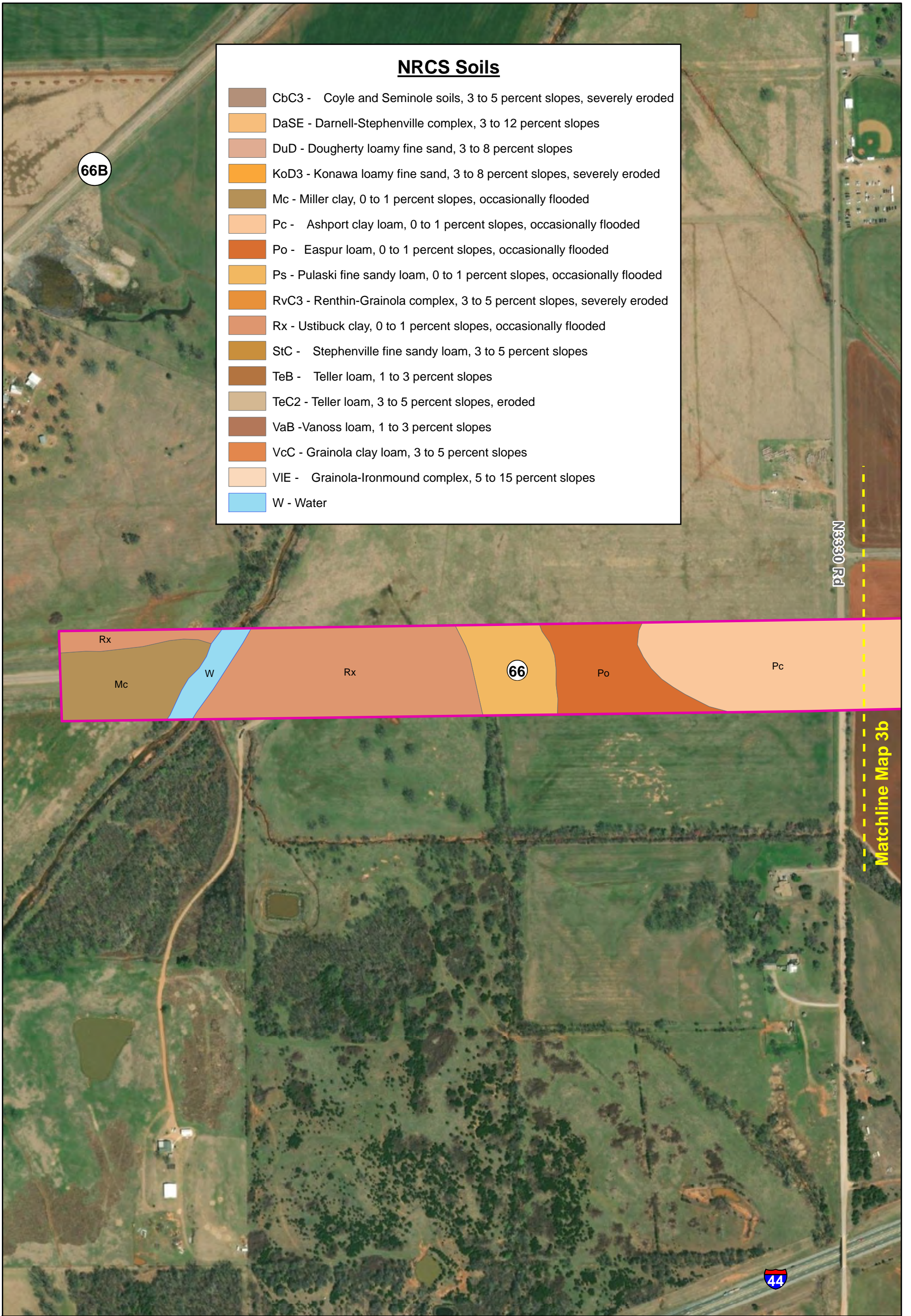
JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66
 from SH-102, east to US-177 (includes an
 approx. 1 mile turnpike project exception)
 Lincoln County, OK



DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

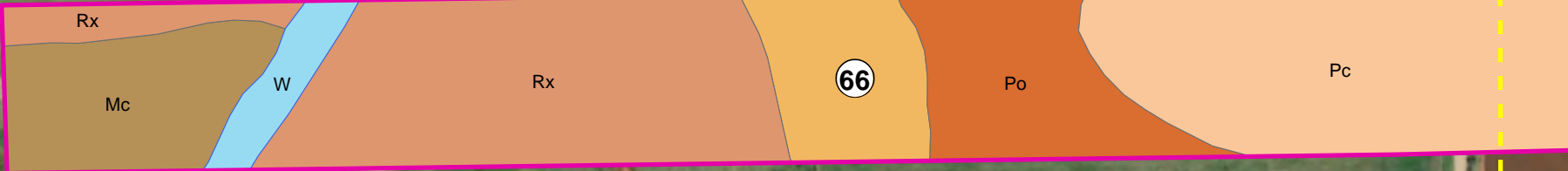
Figure 2

Date: 4/7/2022

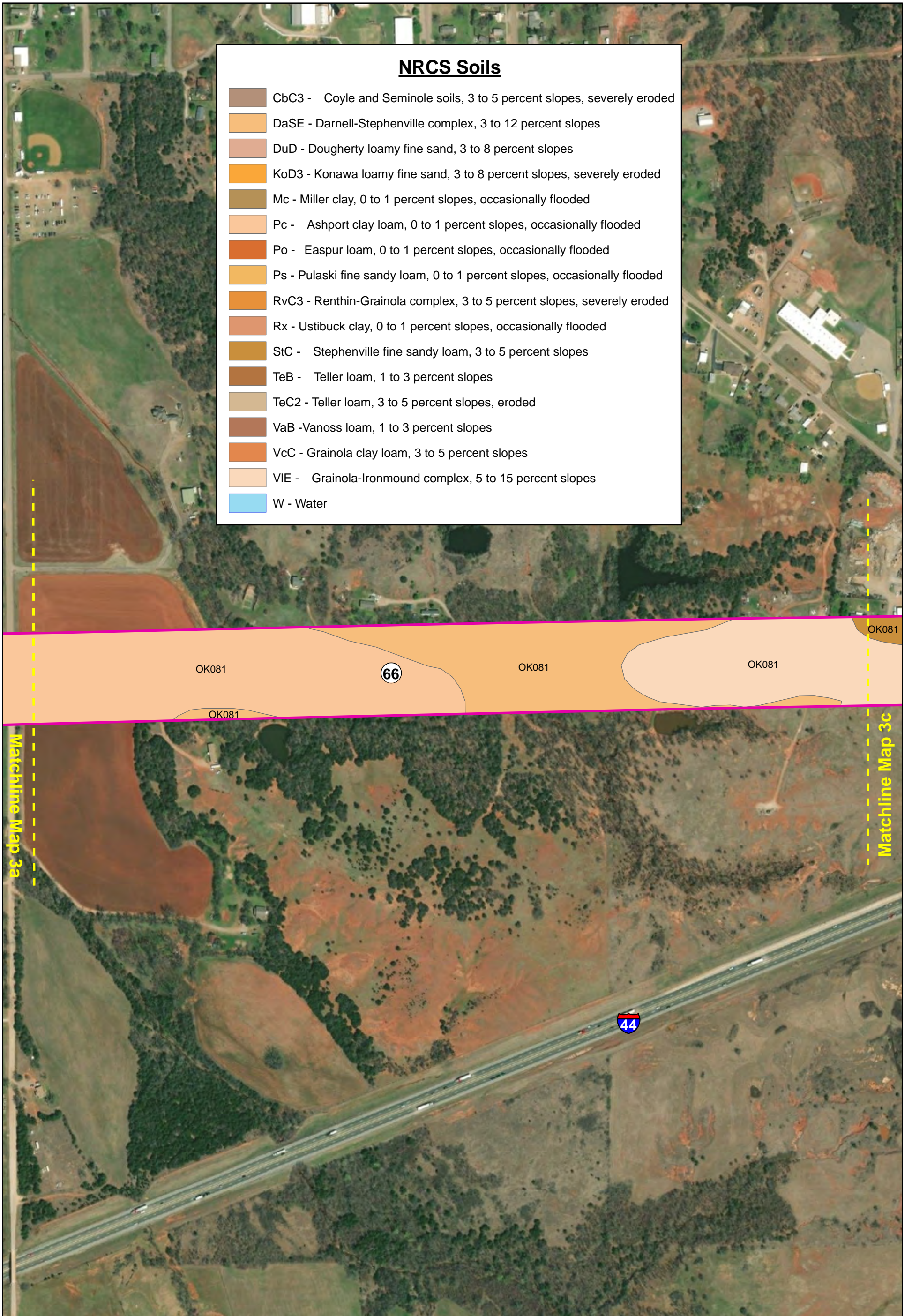


NRCS Soils

	CbC3 - Coyle and Seminole soils, 3 to 5 percent slopes, severely eroded
	DaSE - Darnell-Stephenville complex, 3 to 12 percent slopes
	DuD - Dougherty loamy fine sand, 3 to 8 percent slopes
	KoD3 - Konawa loamy fine sand, 3 to 8 percent slopes, severely eroded
	Mc - Miller clay, 0 to 1 percent slopes, occasionally flooded
	Pc - Ashport clay loam, 0 to 1 percent slopes, occasionally flooded
	Po - Easpur loam, 0 to 1 percent slopes, occasionally flooded
	Ps - Pulaski fine sandy loam, 0 to 1 percent slopes, occasionally flooded
	RvC3 - Renthin-Grainola complex, 3 to 5 percent slopes, severely eroded
	Rx - Ustibuck clay, 0 to 1 percent slopes, occasionally flooded
	StC - Stephenville fine sandy loam, 3 to 5 percent slopes
	TeB - Teller loam, 1 to 3 percent slopes
	TeC2 - Teller loam, 3 to 5 percent slopes, eroded
	VaB - Vanoss loam, 1 to 3 percent slopes
	VcC - Grainola clay loam, 3 to 5 percent slopes
	VIE - Grainola-Ironmound complex, 5 to 15 percent slopes
	W - Water



<p style="text-align: center;">COMMENTS/LEGEND</p> <p> NEPA Environmental Footprint</p> <p> Matchlines</p> <div style="text-align: center;"> <p>0 100 200 400 600 800 Feet</p> </div>	<h3>NRCS Soils</h3> <p>JP 34318(04) / J3-4318(004) Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK</p>	<p>OKLAHOMA Transportation</p> <p><small>DRAWN BY: LMP APPRV BY: SOURCE: DEQ, Tiger 2000, USGS</small></p>	<h2>Figure 3a</h2> <p>Date: 4/7/2022</p>
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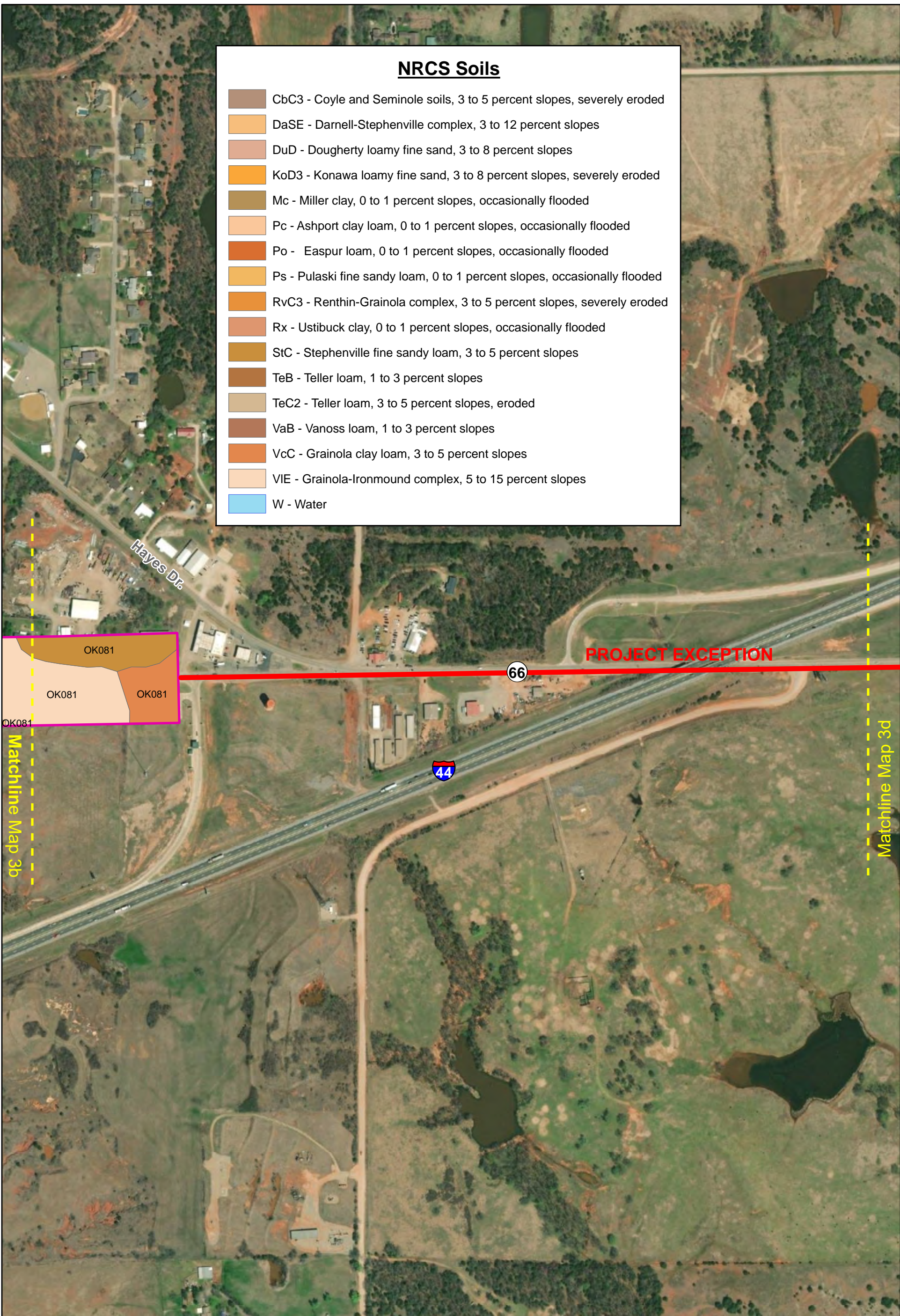


NRCS Soils	
	CbC3 - Coyle and Seminole soils, 3 to 5 percent slopes, severely eroded
	DaSE - Darnell-Stephenville complex, 3 to 12 percent slopes
	DuD - Dougherty loamy fine sand, 3 to 8 percent slopes
	KoD3 - Konawa loamy fine sand, 3 to 8 percent slopes, severely eroded
	Mc - Miller clay, 0 to 1 percent slopes, occasionally flooded
	Pc - Ashport clay loam, 0 to 1 percent slopes, occasionally flooded
	Po - Easpur loam, 0 to 1 percent slopes, occasionally flooded
	Ps - Pulaski fine sandy loam, 0 to 1 percent slopes, occasionally flooded
	RvC3 - Renthin-Grainola complex, 3 to 5 percent slopes, severely eroded
	Rx - Ustibuck clay, 0 to 1 percent slopes, occasionally flooded
	StC - Stephenville fine sandy loam, 3 to 5 percent slopes
	TeB - Teller loam, 1 to 3 percent slopes
	TeC2 - Teller loam, 3 to 5 percent slopes, eroded
	VaB - Vanoss loam, 1 to 3 percent slopes
	VcC - Grainola clay loam, 3 to 5 percent slopes
	VIE - Grainola-Ironmound complex, 5 to 15 percent slopes
	W - Water

<p>COMMENTS/LEGEND</p> <p> NEPA Environmental Footprint</p> <p> Matchlines</p> <p>0 100 200 400 600 800 Feet</p>	<p>NRCS Soils</p> <p>JP 34318(04) / J3-4318(004)</p> <p>Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK</p>	<p>DRAWN BY: LMP APPRV BY: SOURCE: DEQ, Tiger 2000, USGS</p>	<p>Figure 3b</p> <p>Date: 4/7/2022</p>
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NRCS Soils

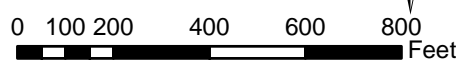
- CbC3 - Coyle and Seminole soils, 3 to 5 percent slopes, severely eroded
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- VIE - Grainola-Ironmound complex, 5 to 15 percent slopes
- W - Water



COMMENTS/LEGEND

NEPA Environmental Footprint

Matchlines



NRCS Soils

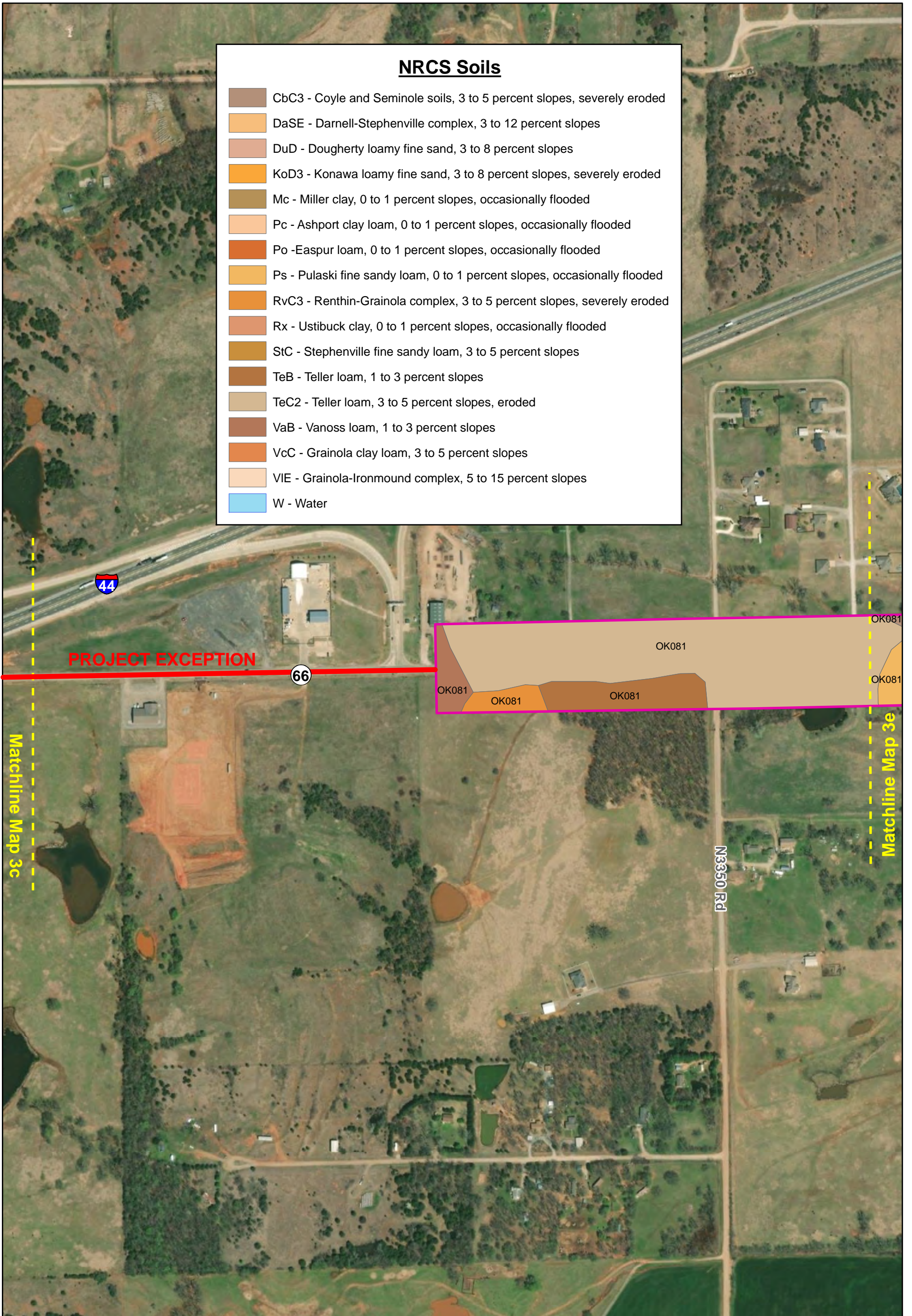
JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66
 from SH-102, east to US-177 (includes an
 approx. 1 mile turnpike project exception)
 Lincoln County, OK



DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

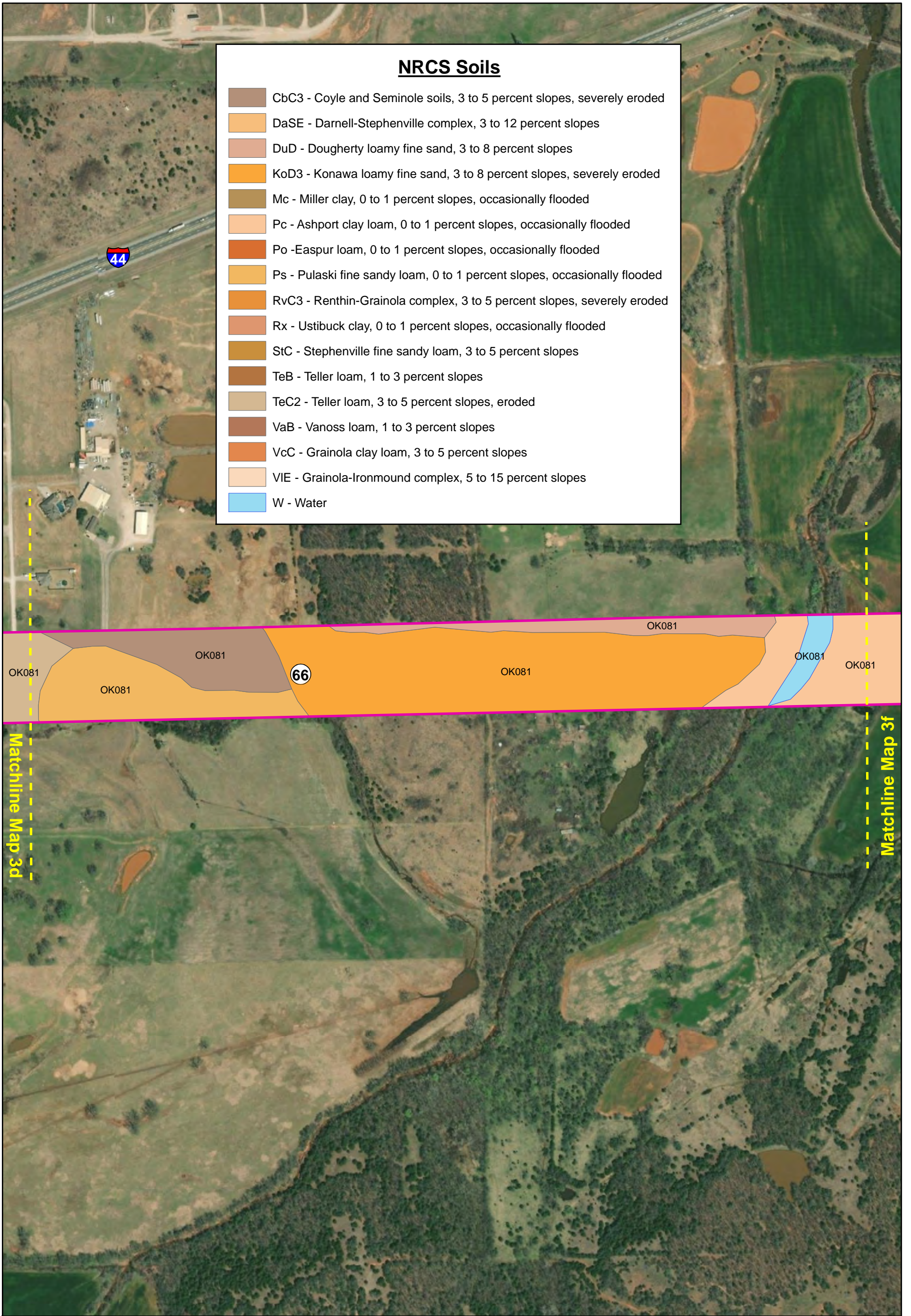
**Figure
3c**

Date: 4/7/2022



NRCS Soils	
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	W - Water

<p>COMMENTS/LEGEND</p> <p> NEPA Environmental Footprint</p> <p> Matchlines</p> <p>0 100 200 400 600 800 Feet</p>	<p>NRCS Soils</p> <p>JP 34318(04) / J3-4318(004)</p> <p>Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK</p>	<p>DRAWN BY: LMP APPRV BY: SOURCE: DEQ, Tiger 2000, USGS</p>	<p>Figure 3d</p> <p>Date: 4/7/2022</p>
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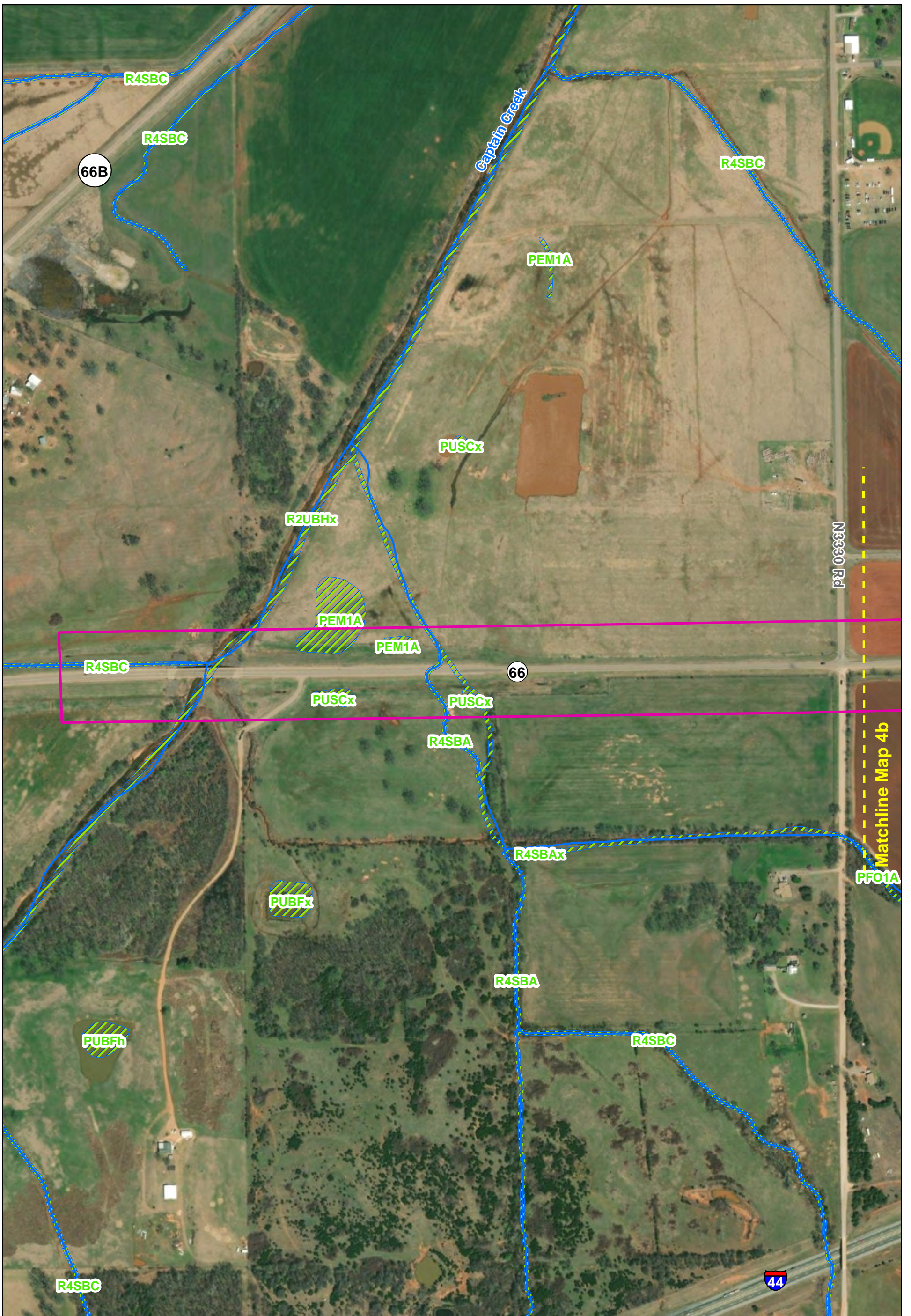
NRCS Soils

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	VIE - Grainola-Ironmound complex, 5 to 15 percent slopes
	W - Water

<p style="text-align: center;">COMMENTS/LEGEND</p> <p> NEPA Environmental Footprint</p> <p> Matchlines</p> <div style="text-align: center;"> <p>0 100 200 400 600 800 Feet</p> </div>	<h3>NRCS Soils</h3> <p>JP 34318(04) / J3-4318(004) Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK</p>	<p>OKLAHOMA Transportation</p> <p><small>DRAWN BY: LMP APPRV BY: SOURCE: DEQ, Tiger 2000, USGS</small></p>	<h2>Figure 3e</h2> <p>Date: 4/7/2022</p>
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<p>COMMENTS/LEGEND</p> <p> NEPA Environmental Footprint</p> <p> Matchlines</p> <p>0 100 200 400 600 800 Feet</p> 	<p>NRCS Soils</p> <p>JP 34318(04) / J3-4318(004)</p> <p>Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK</p>	 <p>DRAWN BY: LMP APPRV BY: SOURCE: DEQ, Tiger 2000, USGS</p>	<p>Figure 3f</p> <p>Date: 4/7/2022</p>
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COMMENTS/LEGEND

NEPA Environmental Footprint
 Matchlines

— Mapped Streams
 NWI Wetlands

0 100 200 400 600 800 Feet

NWI Wetlands

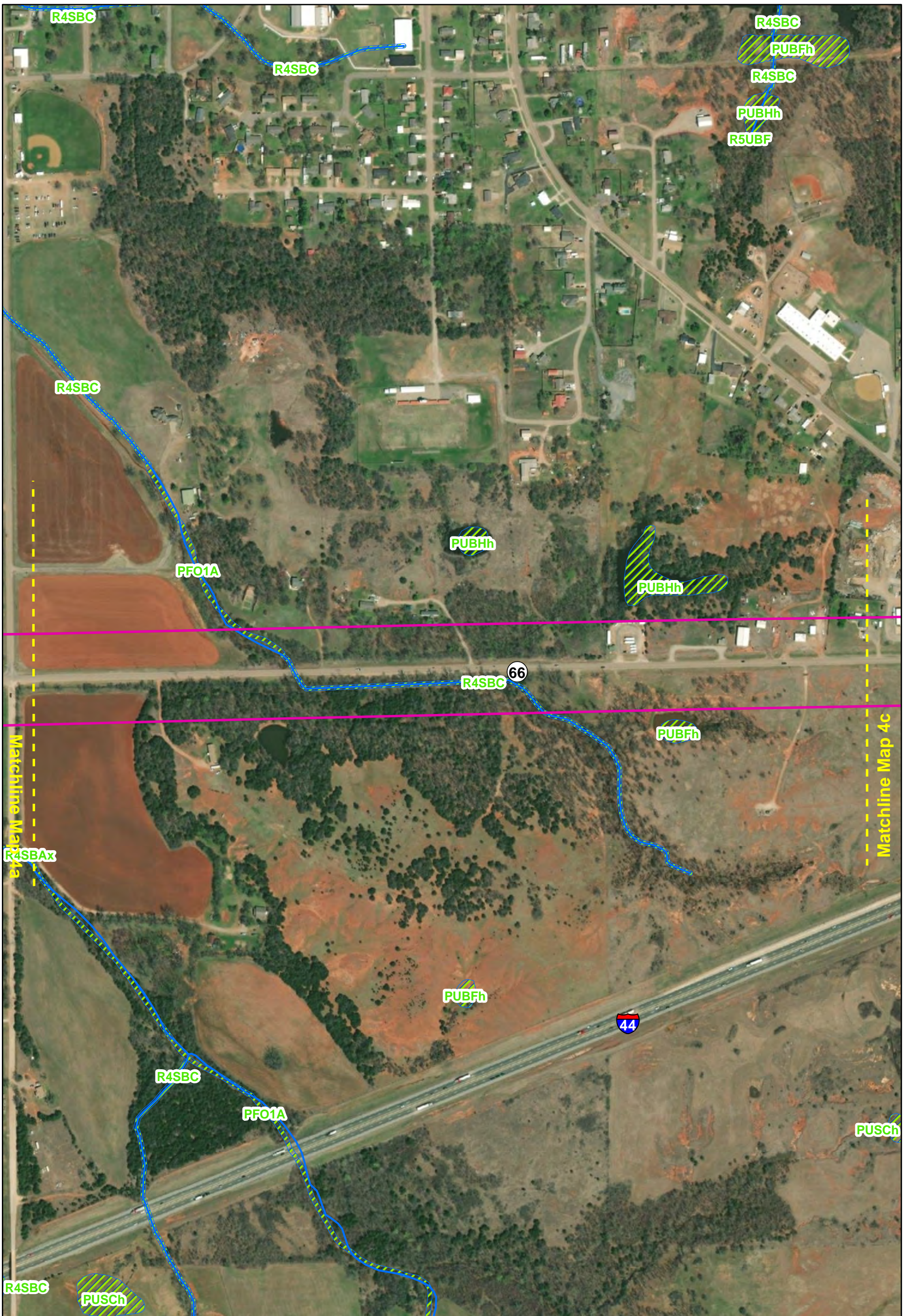
JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66
 from SH-102, east to US-177 (includes an
 approx. 1 mile turnpike project exception)
 Lincoln County, OK

OKLAHOMA
 Transportation

DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

Figure
4a

Date: 4/7/2022



COMMENTS/LEGEND	
NEPA Environmental Footprint	NWI Wetlands
Mapped Streams	Matchlines

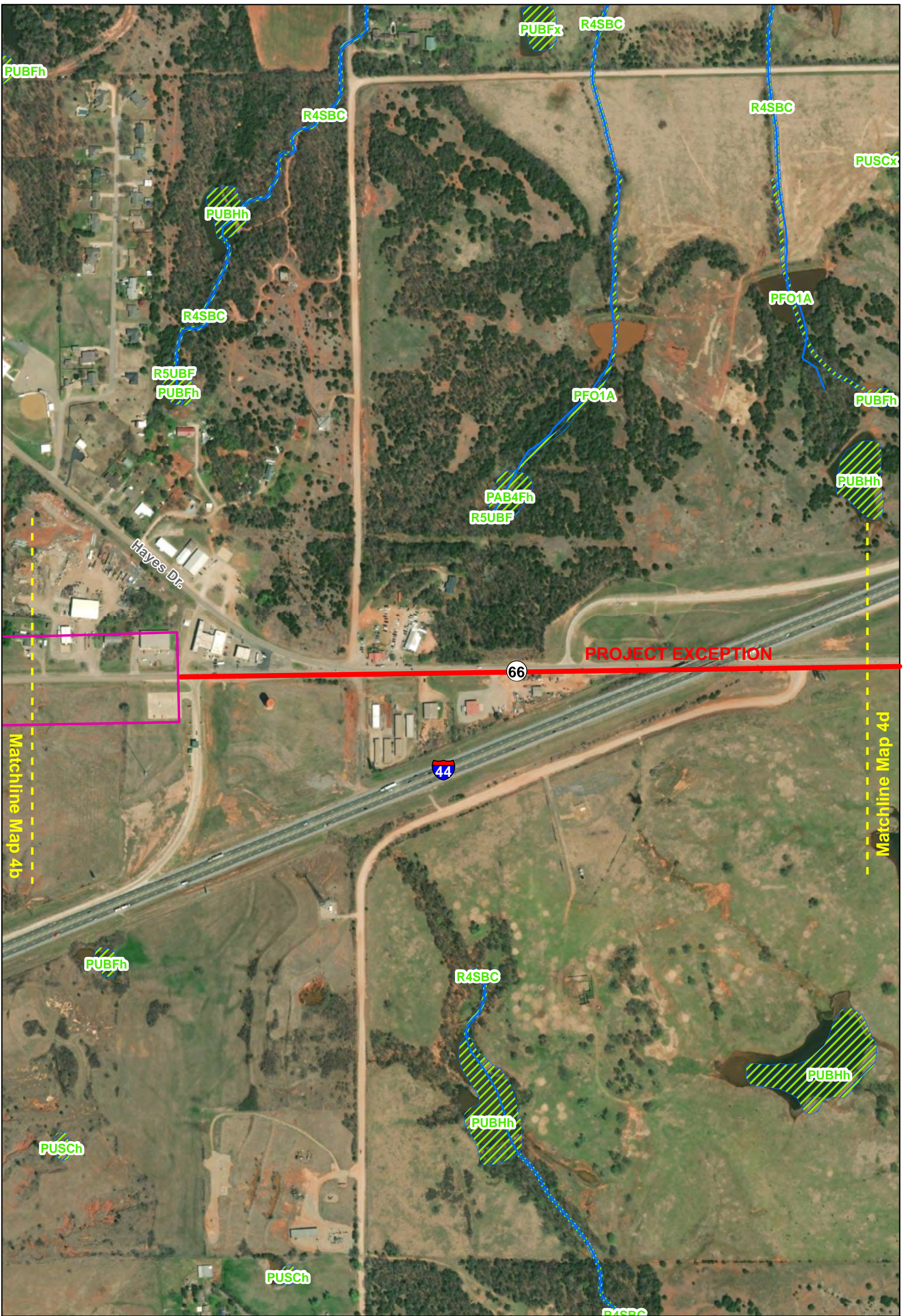
NWI Wetlands
 JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66
 from SH-102, east to US-177 (includes an
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 Lincoln County, OK







**OKLAHOMA
Transportation**

DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS


**Figure
4b**

Date: 4/7/2022

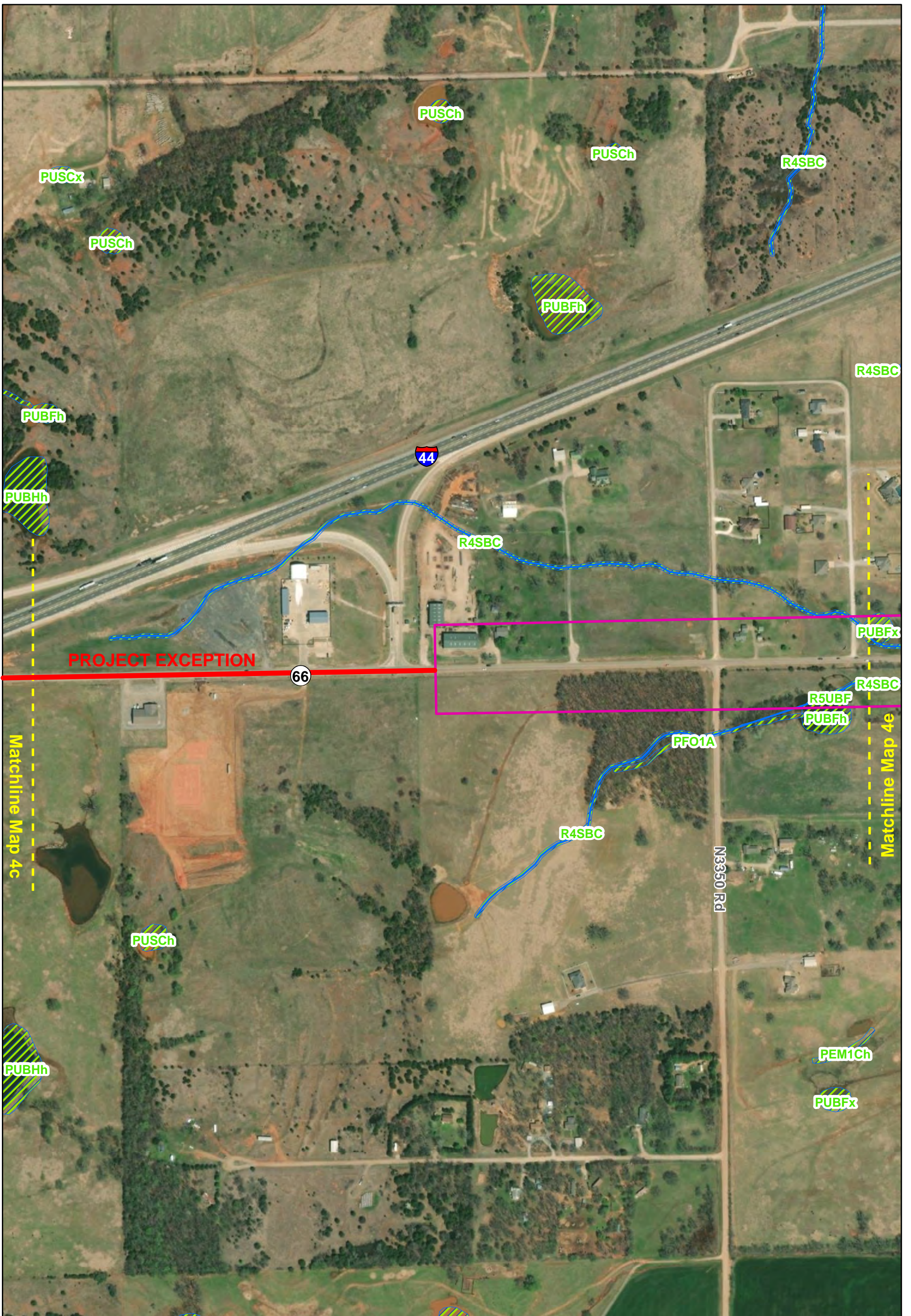


COMMENTS/LEGEND	
	NEPA Environmental Footprint
	Mapped Streams
	NWI Wetlands
	Matchlines
	
	

NWI Wetlands
 JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66
 from SH-102, east to US-177 (includes an
 approx. 1 mile turnpike project exception)
 Lincoln County, OK


 DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

**Figure
4c**
 Date: 4/7/2022



COMMENTS/LEGEND

NEPA Environmental Footprint
 NWI Wetlands

Mapped Streams
 Matchlines

0 100 200 400 600 800 Feet

NWI Wetlands

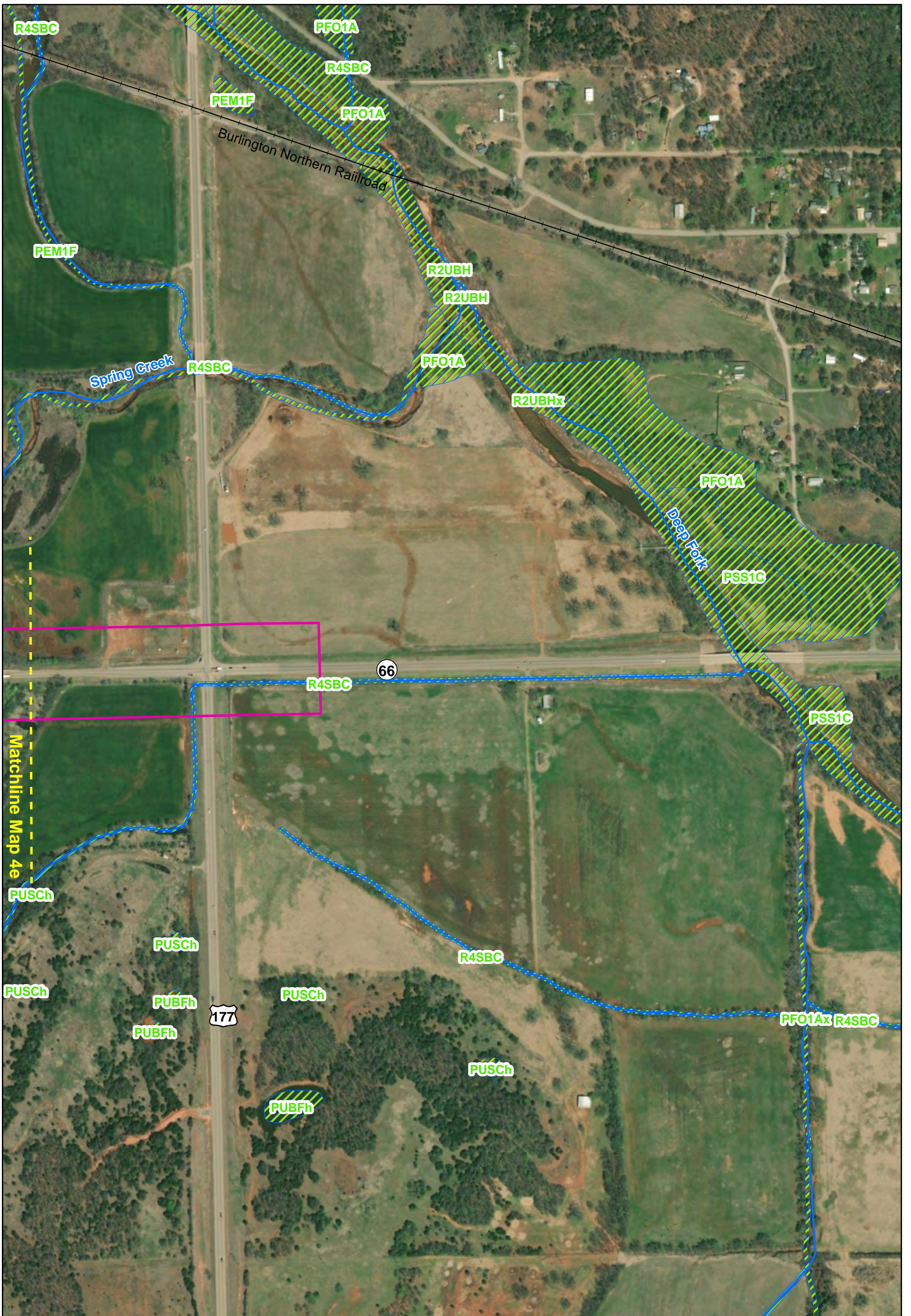
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 approx. 1 mile turnpike project exception)
 Lincoln County, OK

OKLAHOMA
 Transportation

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 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

**Figure
4d**

Date: 4/7/2022



COMMENTS/LEGEND

NEPA Environmental Footprint
 NWI Wetlands

Mapped Streams
 Matchlines

0 100 200 400 600 800 Feet

NWI Wetlands

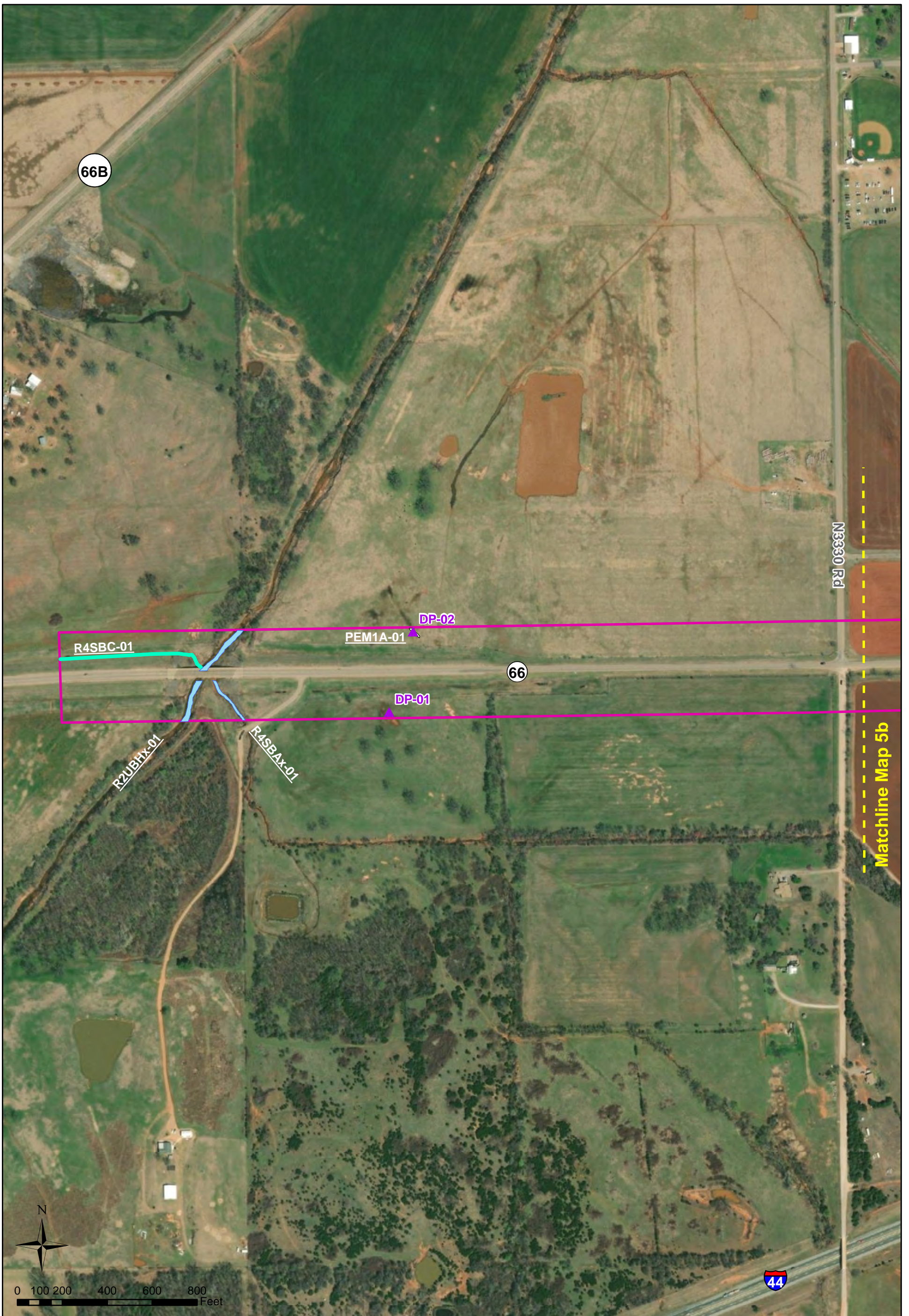
JP 34318(04) / J3-4318(004)
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 Lincoln County, OK








OKLAHOMA
 Transportation

DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

**Figure
4f**

Date: 4/7/2022



COMMENTS/LEGEND	
 NEPA Environmental Footprint	 Field Identified Wetlands
 Field Verified USGS Streams	 Wetland Data Points
 R4SBC-01 (Not Present)	 Matchlines
 NDF-1	

Delineation

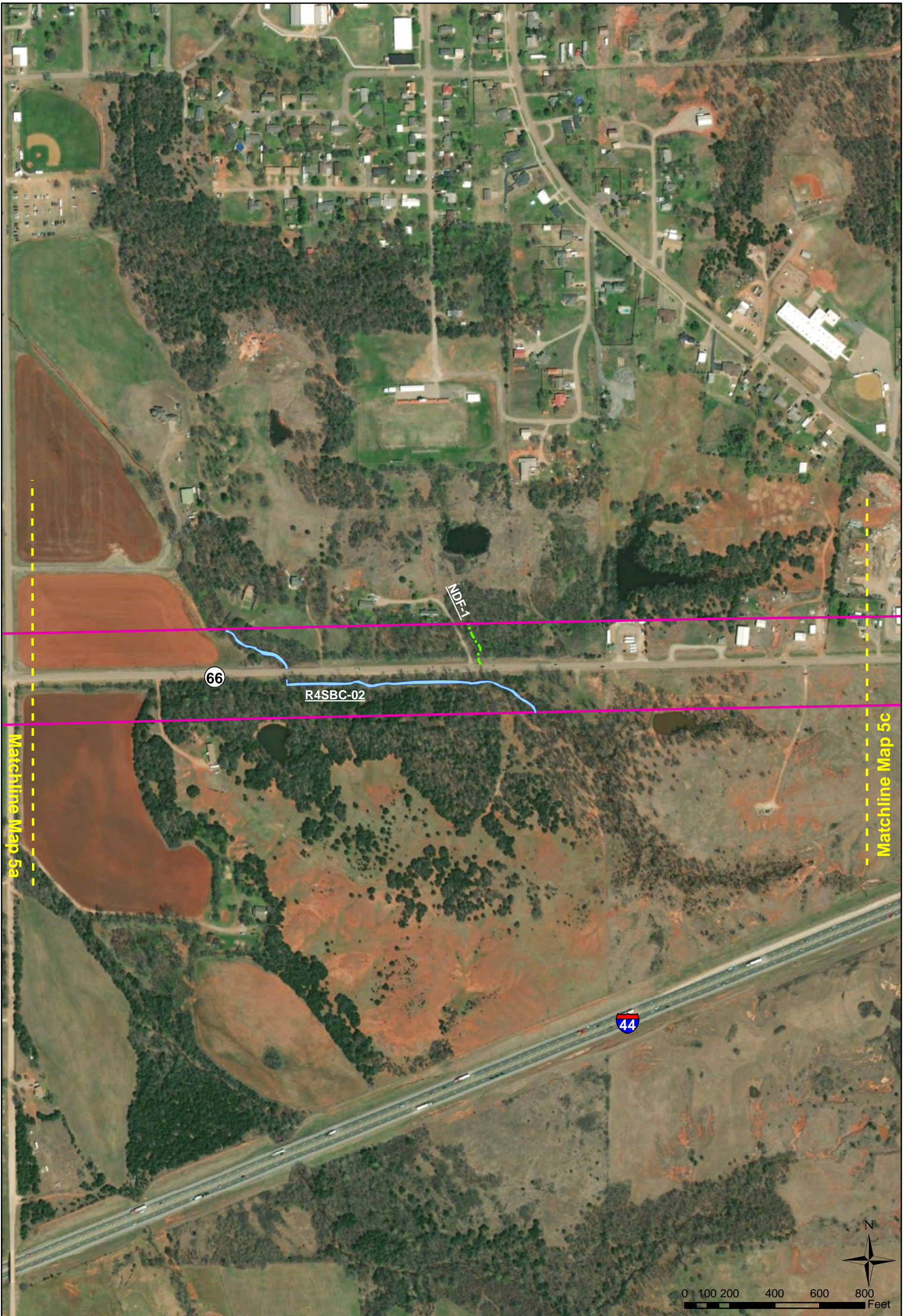
JP 34318(04) / J3-4318(004)
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 Lincoln County, OK







 OKLAHOMA
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DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

**Figure
5a**

Date: 4/7/2022



COMMENTS/LEGEND	
	NEPA Environmental Footprint
	Field Verified USGS Streams
	NDF-1
	Field Identified Wetlands
	Wetland Data Points
	Matchlines

Delineation
 JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66
 from SH-102, east to US-177 (includes an
 approx. 1 mile turnpike project exception)
 Lincoln County, OK









 DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

Figure 5b
 Date: 4/7/2022

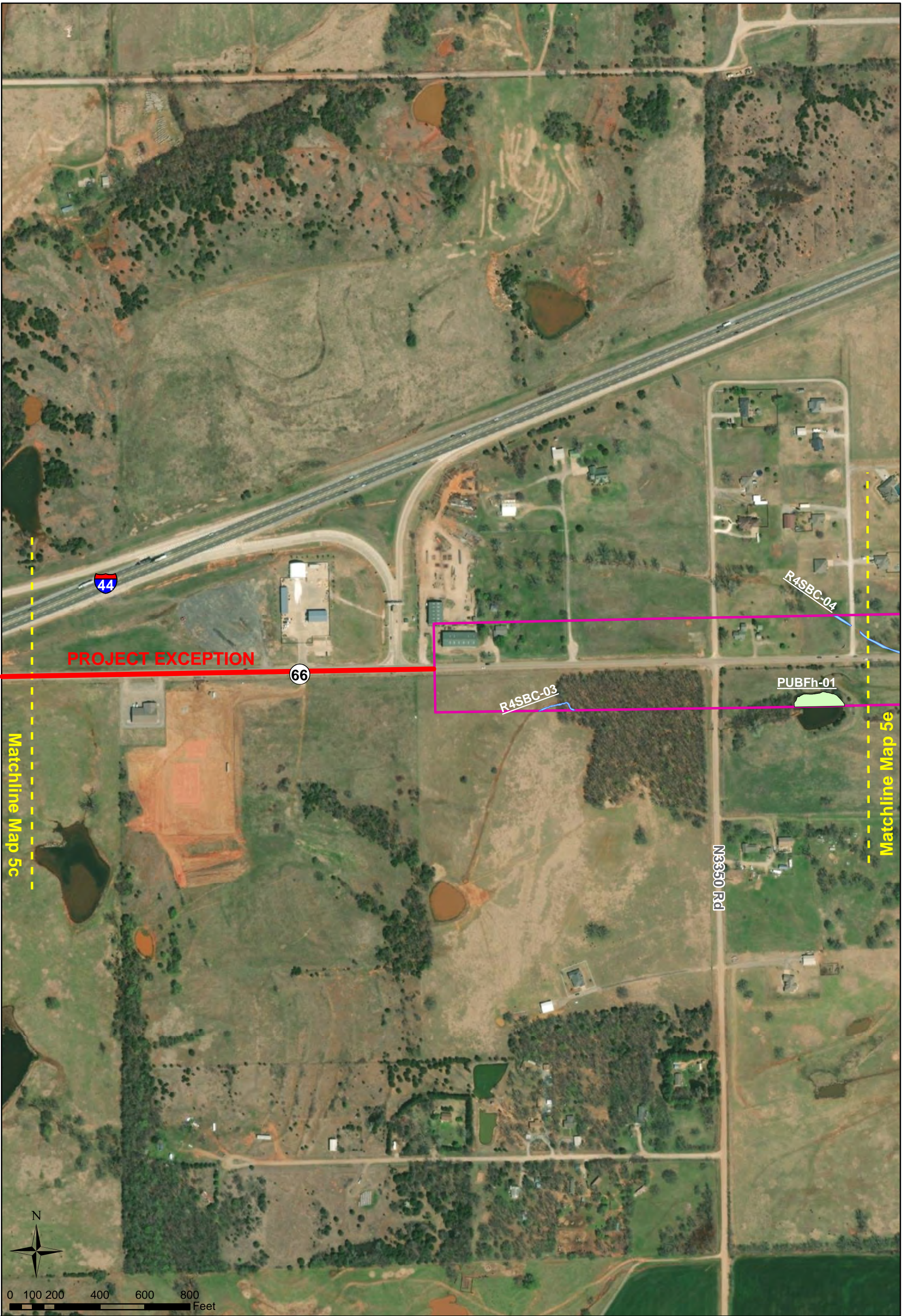


COMMENTS/LEGEND	
 NEPA Environmental Footprint	 Field Identified Wetlands
 Field Verified USGS Streams	 Wetland Data Points
 NDF-1	 Matchlines

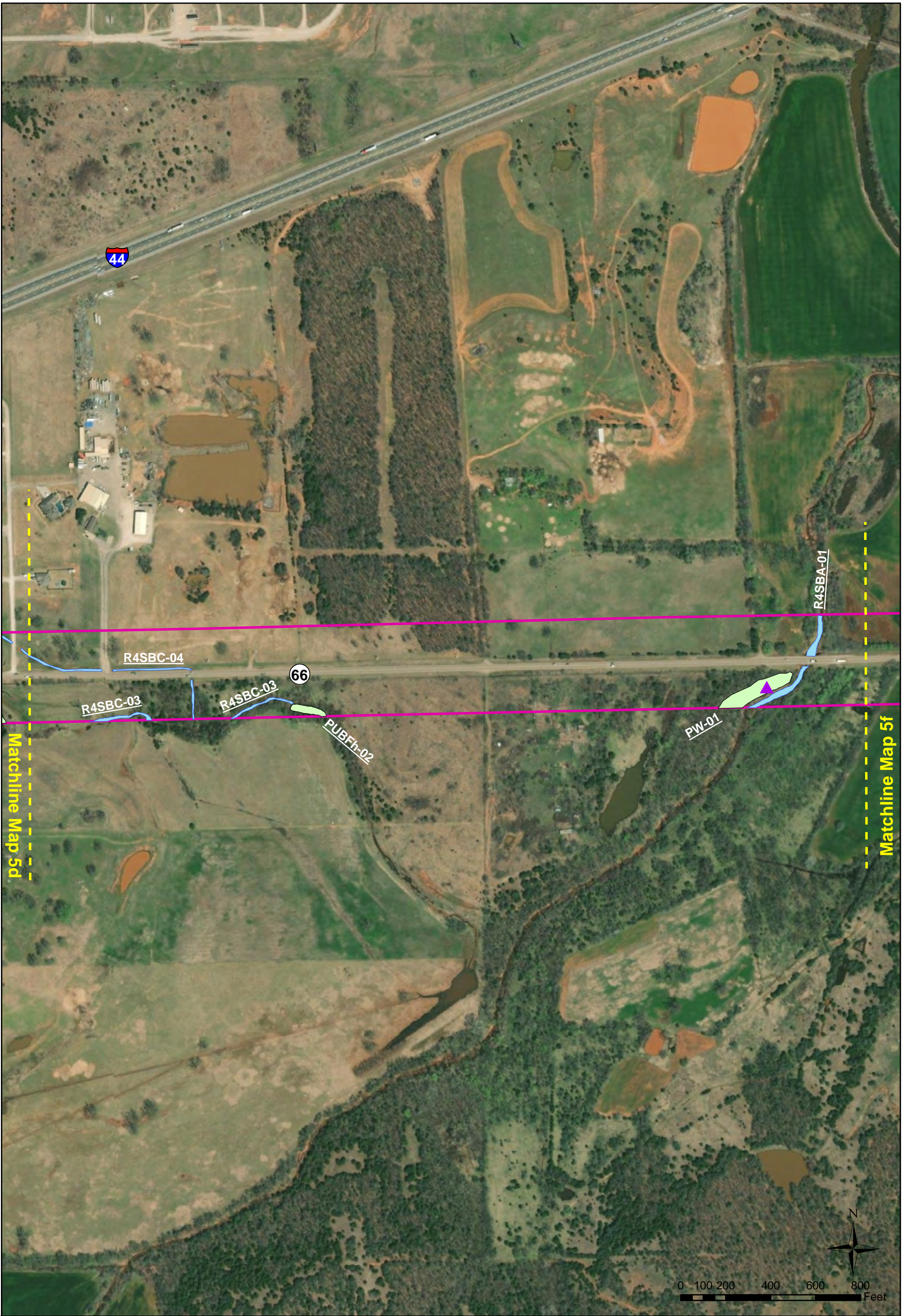
Delineation
 JP 34318(04) / J3-4318(004)
 Widen, Resurface & Bridge on SH-66
 from SH-102, east to US-177 (includes an
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 Lincoln County, OK







 OKLAHOMA
 Transportation
 DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

**Figure
 5c**
 Date: 4/7/2022




COMMENTS/LEGEND		<h3>Delineation</h3> <p>JP 34318(04) / J3-4318(004) Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1 mile turnpike project exception) Lincoln County, OK</p>		<h3>Figure 5d</h3>
<p> NEPA Environmental Footprint</p> <p> Field Verified USGS Streams</p> <p> NDF-1</p>	<p> Field Identified Wetlands</p> <p>▲ Wetland Data Points</p> <p> Matchlines</p>			









COMMENTS/LEGEND	
 NEPA Environmental Footprint	 Field Identified Wetlands
 Field Verified USGS Streams	 Wetland Data Points
 NDF-1	 Matchlines

Delineation
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 Lincoln County, OK


 DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

**Figure
5e**
 Date: 4/7/2022



COMMENTS/LEGEND	
 NEPA Environmental Footprint	 Field Identified Wetlands
 Field Verified USGS Streams	 Wetland Data Points
 NDF-1	 Matchlines

Delineation

JP 34318(04) / J3-4318(004)
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 Lincoln County, OK



DRAWN BY: LMP
 APPRV BY:
 SOURCE: DEQ, Tiger 2000,
 USGS

**Figure
5f**

Date: 4/7/2022



Photograph 1-A: Western edge of study area, facing west out of the footprint along SH-66.



Photograph 1-B: Western edge of study area, facing east into footprint along SH-66.



Photograph 2: NBI: 26639 over R2UBHx-01 (Captain Creek), migratory bird nests present.



Photograph 3: Beneath NBI: 26639.



Photograph 4: R2UBHx-01 (Captain Creek) facing south.



Photograph 5: R4SBCAx-01 (Unnamed Tributary to Captain Creek).



Photograph 6: Steeply sloped bank of R2UBHx-01 (Captain Creek).



Photograph 7: R4SBC-01 (Unnamed Tributary to Captain Creek) north of roadway.



Photograph 8: Location of PUSCx (no wetland feature present).



Photograph 9: Vegetated swale and location of a PEM1A north of roadway (no wetland feature present).



Photograph 10: Location of PEM1A-01, facing north.



Photograph 11: Structure 2 at R4SBC-02 (Unnamed trib to Captain Creek), migratory bird nests present.



Photograph 12: Beneath Structure 2, barn swallow nest.



Photograph 13: R4SBC-02 (Unnamed trib to Captain Creek) north of roadway.



Photograph 14: R4SBC-02 (Unnamed trib to Captain Creek) along south roadway ditch.



Photograph 15: Structure 3 at NDF-1, migratory bird nests present.



Photograph 16: NDF-1 north of roadway.



Photograph 17: Structure 4, no migratory bird nests or water feature present.



Photograph 18: Drainway (R4SBC-03) from PUBFh-01, facing west.



Photograph 19: Structure 5 at R4SBC-04, no migratory bird nests present.



Photograph 20: R4SBC-04 (Unnamed trib to Spring Creek) north of roadway.



Photograph 21: Structure 6 at R4SBC-04 (Unnamed trib to Spring Creek), no migratory bird nests present.



Photograph 22: R4SBC-04 south of roadway.



Photograph 23: R4SBC-03 (Unnamed Tributary) south of roadway.



Photograph 24: Structure 7, no migratory bird nests or water feature present.



Photograph 25: R4SBC-03 (Unnamed Tributary) south of roadway.



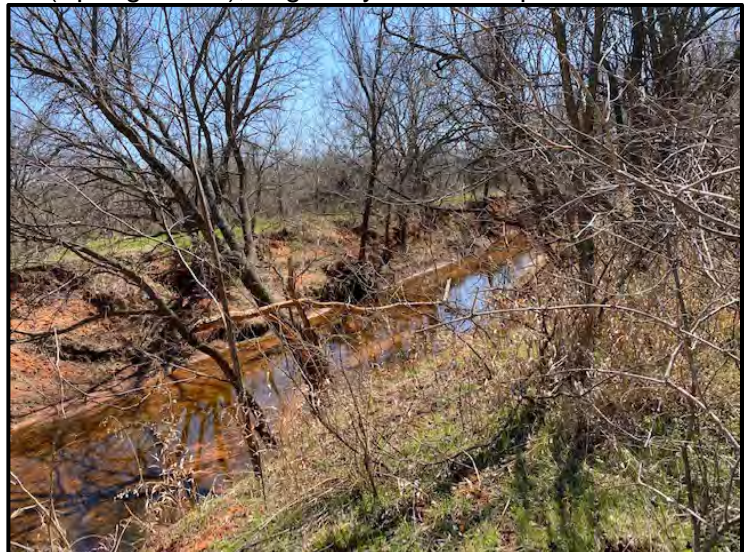
Photograph 26: PUBFh-02 south of roadway.



Photograph 27: Structure 8 (NBI: 12596) over R4SBA-01 (Spring Creek), migratory bird nests present.



Photograph 28: Cliff swallow nests beneath Structure 8 (NBI: 12596).



Photograph 29: R4SBA-01 (Spring Creek) south of roadway.



Photograph 30: Staining on trees in PW-01 south of roadway.



Photograph 31: Structure 9 at R4SBC-05, migratory bird nest present.



Photograph 32: R4SBC-05 (Unnamed Tributary to Deep Fork) south of roadway.



Photograph 33-A: East boundary study area, facing west toward study area.



Photograph 33-B: East boundary study area, facing east outside of study area.

WETLAND DETERMINATION DATA SHEET – Great Plains Region

Project/Site: Widen, Resurface & Bridge on SH-66 City/County: Lincoln Sampling Date: 3/28/22
 Applicant/Owner: Oklahoma Department of Transportation State: OK Sampling Point: DP-01
 Investigator(s): D. Daniel Section, Township, Range: S22-T14N-R2E
 Landform (hillside, terrace, etc.): Depression Local relief (concave, convex, none): Concave Slope (%): <2%
 Subregion (LRR): LRR H, MLRA 80A Lat: 35.680532 Long: -97.077076 Datum: WGS-84
 Soil Map Unit Name: Ustibuck clay, 0 to 1% slopes, occassionally flooded NWI classification: PUSCx

Are climatic / hydrologic conditions on the site typical for this time of year? Yes x No (If no, explain in Remarks.)
 Are Vegetation , Soil , or Hydrology significantly disturbed? Are "Normal Circumstances" present? Yes X No
 Are Vegetation , Soil , or Hydrology naturally problematic? (If needed, explain any answers in Remarks.)

SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.

Hydrophytic Vegetation Present? Yes <u> </u> No <u>X</u> Hydric Soil Present? Yes <u> </u> No <u>X</u> Wetland Hydrology Present? Yes <u>X</u> No <u> </u>	Is the Sampled Area within a Wetland? Yes <u> </u> No <u>X</u>
Remarks:	

VEGETATION – Use scientific names of plants.

<u>Tree Stratum</u> (Plot size: <u> </u>)	Absolute % Cover	Dominant Species?	Indicator Status	
1. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
2. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
3. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
4. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
<u> </u> =Total Cover				
<u>Sapling/Shrub Stratum</u> (Plot size: <u> </u>)	Absolute % Cover	Dominant Species?	Indicator Status	
1. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
2. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
3. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
4. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
5. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
<u> </u> =Total Cover				
<u>Herb Stratum</u> (Plot size: <u> </u>)	Absolute % Cover	Dominant Species?	Indicator Status	
1. <u>Cynodon dactylon</u>	<u>10</u>	<u>Yes</u>	<u>FACU</u>	
2. <u>Poa pratensis</u>	<u>5</u>	<u>Yes</u>	<u>FACU</u>	
3. <u>Rumex crispus</u>	<u>5</u>	<u>Yes</u>	<u>FAC</u>	
4. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
5. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
6. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
7. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
8. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
9. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
10. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
<u>20</u> =Total Cover				
<u>Woody Vine Stratum</u> (Plot size: <u> </u>)	Absolute % Cover	Dominant Species?	Indicator Status	
1. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
2. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
<u> </u> =Total Cover				
% Bare Ground in Herb Stratum <u>80</u>				

Dominance Test worksheet:
 Number of Dominant Species That Are OBL, FACW, or FAC: 1 (A)
 Total Number of Dominant Species Across All Strata: 3 (B)
 Percent of Dominant Species That Are OBL, FACW, or FAC: 33.3% (A/B)

Prevalence Index worksheet:
 Total % Cover of: Multiply by:
 OBL species 0 x 1 = 0
 FACW species 0 x 2 = 0
 FAC species 5 x 3 = 15
 FACU species 15 x 4 = 60
 UPL species 0 x 5 = 0
 Column Totals: 20 (A) 75 (B)
 Prevalence Index = B/A = 3.75

Hydrophytic Vegetation Indicators:
 1 - Rapid Test for Hydrophytic Vegetation
 2 - Dominance Test is >50%
 3 - Prevalence Index is ≤3.0¹
 4 - Morphological Adaptations¹ (Provide supporting data in Remarks or on a separate sheet)
 Problematic Hydrophytic Vegetation¹ (Explain)
¹Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic.

Hydrophytic Vegetation Present? Yes No X

Remarks:

SOIL

Sampling Point: DP-01

Profile Description: (Describe to the depth needed to document the indicator or confirm the absence of indicators.)								
Depth (inches)	Matrix		Redox Features				Texture	Remarks
	Color (moist)	%	Color (moist)	%	Type ¹	Loc ²		
0-20	7.5YR 4/4	100					Loamy/Clayey	

¹Type: C=Concentration, D=Depletion, RM=Reduced Matrix, CS=Covered or Coated Sand Grains. ²Location: PL=Pore Lining, M=Matrix.

Hydric Soil Indicators: (Applicable to all LRRs, unless otherwise noted.)		Indicators for Problematic Hydric Soils ³ :
<input type="checkbox"/> Histosol (A1)	<input type="checkbox"/> Sandy Gleyed Matrix (S4)	<input type="checkbox"/> 1 cm Muck (A9) (LRR I, J)
<input type="checkbox"/> Histic Epipedon (A2)	<input type="checkbox"/> Sandy Redox (S5)	<input type="checkbox"/> Coast Prairie Redox (A16) (LRR F, G, H)
<input type="checkbox"/> Black Histic (A3)	<input type="checkbox"/> Stripped Matrix (S6)	<input type="checkbox"/> Dark Surface (S7) (LRR G)
<input type="checkbox"/> Hydrogen Sulfide (A4)	<input type="checkbox"/> Loamy Mucky Mineral (F1)	<input type="checkbox"/> High Plains Depressions (F16)
<input type="checkbox"/> Stratified Layers (A5) (LRR F)	<input type="checkbox"/> Loamy Gleyed Matrix (F2)	(LRR H outside of MLRA 72 & 73)
<input type="checkbox"/> 1 cm Muck (A9) (LRR F, G, H)	<input type="checkbox"/> Depleted Matrix (F3)	<input type="checkbox"/> Reduced Vertic (F18)
<input type="checkbox"/> Depleted Below Dark Surface (A11)	<input type="checkbox"/> Redox Dark Surface (F6)	<input type="checkbox"/> Red Parent Material (F21)
<input type="checkbox"/> Thick Dark Surface (A12)	<input type="checkbox"/> Depleted Dark Surface (F7)	<input type="checkbox"/> Very Shallow Dark Surface (F22)
<input type="checkbox"/> Sandy Mucky Mineral (S1)	<input type="checkbox"/> Redox Depressions (F8)	<input type="checkbox"/> Other (Explain in Remarks)
<input type="checkbox"/> 2.5 cm Mucky Peat or Peat (S2) (LRR G, H)	<input type="checkbox"/> High Plains Depressions (F16)	³ Indicators of hydrophytic vegetation and wetland hydrology must be present, unless disturbed or problematic.
<input type="checkbox"/> 5 cm Mucky Peat or Peat (S3) (LRR F)	(MLRA 72 & 73 of LRR H)	

Restrictive Layer (if observed): Type: <u>High clay content</u> Depth (inches): <u>0</u>	Hydric Soil Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/>
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Remarks:
This data sheet is revised from Great Plains Regional Supplement Version 2.0 to include the NRCS Field Indicators of Hydric Soils, Version 8.0, 2016.

HYDROLOGY

Wetland Hydrology Indicators:		Secondary Indicators (minimum of two required)
Primary Indicators (minimum of one is required; check all that apply)		
<input type="checkbox"/> Surface Water (A1)	<input type="checkbox"/> Salt Crust (B11)	<input checked="" type="checkbox"/> Surface Soil Cracks (B6)
<input type="checkbox"/> High Water Table (A2)	<input type="checkbox"/> Aquatic Invertebrates (B13)	<input checked="" type="checkbox"/> Sparsely Vegetated Concave Surface (B8)
<input type="checkbox"/> Saturation (A3)	<input type="checkbox"/> Hydrogen Sulfide Odor (C1)	<input type="checkbox"/> Drainage Patterns (B10)
<input type="checkbox"/> Water Marks (B1)	<input type="checkbox"/> Dry-Season Water Table (C2)	<input type="checkbox"/> Oxidized Rhizospheres on Living Roots (C3)
<input type="checkbox"/> Sediment Deposits (B2)	<input type="checkbox"/> Oxidized Rhizospheres on Living Roots (C3)	(where tilled)
<input type="checkbox"/> Drift Deposits (B3)	(where not tilled)	<input type="checkbox"/> Crayfish Burrows (C8)
<input type="checkbox"/> Algal Mat or Crust (B4)	<input type="checkbox"/> Presence of Reduced Iron (C4)	<input type="checkbox"/> Saturation Visible on Aerial Imagery (C9)
<input type="checkbox"/> Iron Deposits (B5)	<input type="checkbox"/> Thin Muck Surface (C7)	<input type="checkbox"/> Geomorphic Position (D2)
<input type="checkbox"/> Inundation Visible on Aerial Imagery (B7)	<input type="checkbox"/> Other (Explain in Remarks)	<input type="checkbox"/> FAC-Neutral Test (D5)
<input type="checkbox"/> Water-Stained Leaves (B9)		<input type="checkbox"/> Frost-Heave Hummocks (D7) (LRR F)

Field Observations: Surface Water Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____ Water Table Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____ Saturation Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____ (includes capillary fringe)	Wetland Hydrology Present? Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>
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Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available:

Remarks:

WETLAND DETERMINATION DATA SHEET – Great Plains Region

Project/Site: Widen, Resurface & Bridge on SH-66 City/County: Lincoln Sampling Date: 3/28/22
 Applicant/Owner: Oklahoma Department of Transportation State: OK Sampling Point: DP-02
 Investigator(s): D. Daniel Section, Township, Range: S15-T14N-R2E
 Landform (hillside, terrace, etc.): Depression Local relief (concave, convex, none): Concave Slope (%): <2%
 Subregion (LRR): LRR H, MLRA 80A Lat: 35.681508 Long: -97.076694 Datum: WGS-84
 Soil Map Unit Name: Ustibuck clay, 0 to 1% slopes, occassionally flooded NWI classification: PEM1A

Are climatic / hydrologic conditions on the site typical for this time of year? Yes x No (If no, explain in Remarks.)
 Are Vegetation , Soil , or Hydrology significantly disturbed? Are "Normal Circumstances" present? Yes X No
 Are Vegetation , Soil , or Hydrology naturally problematic? (If needed, explain any answers in Remarks.)

SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.

Hydrophytic Vegetation Present? Yes <u>X</u> No <u> </u> Hydric Soil Present? Yes <u>X</u> No <u> </u> Wetland Hydrology Present? Yes <u>X</u> No <u> </u>	Is the Sampled Area within a Wetland? Yes <u>X</u> No <u> </u>
Remarks:	

VEGETATION – Use scientific names of plants.

<u>Tree Stratum</u> (Plot size: <u> </u>)	Absolute % Cover	Dominant Species?	Indicator Status	
1. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
2. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
3. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
4. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
<u> </u> =Total Cover				
<u>Sapling/Shrub Stratum</u> (Plot size: <u> </u>)	Absolute % Cover	Dominant Species?	Indicator Status	
1. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
2. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
3. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
4. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
5. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
<u> </u> =Total Cover				
<u>Herb Stratum</u> (Plot size: <u> </u>)	Absolute % Cover	Dominant Species?	Indicator Status	
1. <u><i>Eleocharis palustris</i></u>	<u>50</u>	<u>Yes</u>	<u>OBL</u>	
2. <u><i>Schoenoplectus fluviatilis</i></u>	<u>20</u>	<u>Yes</u>	<u>OBL</u>	
3. <u><i>Ambrosia artemisiifolia</i></u>	<u>5</u>	<u>No</u>	<u>FACU</u>	
4. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
5. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
6. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
7. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
8. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
9. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
10. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
<u>75</u> =Total Cover				
<u>Woody Vine Stratum</u> (Plot size: <u> </u>)	Absolute % Cover	Dominant Species?	Indicator Status	
1. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
2. <u> </u>	<u> </u>	<u> </u>	<u> </u>	
<u> </u> =Total Cover				
% Bare Ground in Herb Stratum <u>25</u>				

Dominance Test worksheet:
 Number of Dominant Species That Are OBL, FACW, or FAC: 2 (A)
 Total Number of Dominant Species Across All Strata: 2 (B)
 Percent of Dominant Species That Are OBL, FACW, or FAC: 100.0% (A/B)

Prevalence Index worksheet:
 Total % Cover of: 75 Multiply by:
 OBL species 70 x 1 = 70
 FACW species 0 x 2 = 0
 FAC species 0 x 3 = 0
 FACU species 5 x 4 = 20
 UPL species 0 x 5 = 0
 Column Totals: 75 (A) 90 (B)
 Prevalence Index = B/A = 1.20

Hydrophytic Vegetation Indicators:
 1 - Rapid Test for Hydrophytic Vegetation
X 2 - Dominance Test is >50%
X 3 - Prevalence Index is ≤3.0¹
 4 - Morphological Adaptations¹ (Provide supporting data in Remarks or on a separate sheet)
 Problematic Hydrophytic Vegetation¹ (Explain)

¹Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic.

Hydrophytic Vegetation Present? Yes <u>X</u> No <u> </u>	Remarks:
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SOIL

Sampling Point: DP-02

Profile Description: (Describe to the depth needed to document the indicator or confirm the absence of indicators.)								
Depth (inches)	Matrix		Redox Features				Texture	Remarks
	Color (moist)	%	Color (moist)	%	Type ¹	Loc ²		
0-4	7.5YR 2.5/2	100					Loamy/Clayey	
4-12	7.5YR 2.5/2	75	7.5YR 5/6	25	C	M	Loamy/Clayey	Prominent redox concentrations

¹Type: C=Concentration, D=Depletion, RM=Reduced Matrix, CS=Covered or Coated Sand Grains. ²Location: PL=Pore Lining, M=Matrix.

Hydric Soil Indicators: (Applicable to all LRRs, unless otherwise noted.)		Indicators for Problematic Hydric Soils ³ :
<input type="checkbox"/> Histosol (A1)	<input type="checkbox"/> Sandy Gleyed Matrix (S4)	<input type="checkbox"/> 1 cm Muck (A9) (LRR I, J)
<input type="checkbox"/> Histic Epipedon (A2)	<input type="checkbox"/> Sandy Redox (S5)	<input type="checkbox"/> Coast Prairie Redox (A16) (LRR F, G, H)
<input type="checkbox"/> Black Histic (A3)	<input type="checkbox"/> Stripped Matrix (S6)	<input type="checkbox"/> Dark Surface (S7) (LRR G)
<input type="checkbox"/> Hydrogen Sulfide (A4)	<input type="checkbox"/> Loamy Mucky Mineral (F1)	<input type="checkbox"/> High Plains Depressions (F16)
<input type="checkbox"/> Stratified Layers (A5) (LRR F)	<input type="checkbox"/> Loamy Gleyed Matrix (F2)	(LRR H outside of MLRA 72 & 73)
<input type="checkbox"/> 1 cm Muck (A9) (LRR F, G, H)	<input type="checkbox"/> Depleted Matrix (F3)	<input type="checkbox"/> Reduced Vertic (F18)
<input type="checkbox"/> Depleted Below Dark Surface (A11)	<input checked="" type="checkbox"/> Redox Dark Surface (F6)	<input type="checkbox"/> Red Parent Material (F21)
<input type="checkbox"/> Thick Dark Surface (A12)	<input type="checkbox"/> Depleted Dark Surface (F7)	<input type="checkbox"/> Very Shallow Dark Surface (F22)
<input type="checkbox"/> Sandy Mucky Mineral (S1)	<input checked="" type="checkbox"/> Redox Depressions (F8)	<input type="checkbox"/> Other (Explain in Remarks)
<input type="checkbox"/> 2.5 cm Mucky Peat or Peat (S2) (LRR G, H)	<input type="checkbox"/> High Plains Depressions (F16)	³ Indicators of hydrophytic vegetation and wetland hydrology must be present, unless disturbed or problematic.
<input type="checkbox"/> 5 cm Mucky Peat or Peat (S3) (LRR F)	(MLRA 72 & 73 of LRR H)	

Restrictive Layer (if observed): Type: <u>Clay pan</u> Depth (inches): <u>4</u>	Hydric Soil Present? Yes <input type="checkbox"/> No <input type="checkbox"/>
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Remarks:
This data sheet is revised from Great Plains Regional Supplement Version 2.0 to include the NRCS Field Indicators of Hydric Soils, Version 8.0, 2016.

HYDROLOGY

Wetland Hydrology Indicators:		Secondary Indicators (minimum of two required)
Primary Indicators (minimum of one is required; check all that apply)		
<input checked="" type="checkbox"/> Surface Water (A1)	<input type="checkbox"/> Salt Crust (B11)	<input checked="" type="checkbox"/> Surface Soil Cracks (B6)
<input type="checkbox"/> High Water Table (A2)	<input checked="" type="checkbox"/> Aquatic Invertebrates (B13)	<input checked="" type="checkbox"/> Sparsely Vegetated Concave Surface (B8)
<input type="checkbox"/> Saturation (A3)	<input type="checkbox"/> Hydrogen Sulfide Odor (C1)	<input type="checkbox"/> Drainage Patterns (B10)
<input type="checkbox"/> Water Marks (B1)	<input type="checkbox"/> Dry-Season Water Table (C2)	<input type="checkbox"/> Oxidized Rhizospheres on Living Roots (C3)
<input type="checkbox"/> Sediment Deposits (B2)	<input type="checkbox"/> Oxidized Rhizospheres on Living Roots (C3)	(where tilled)
<input type="checkbox"/> Drift Deposits (B3)	(where not tilled)	<input type="checkbox"/> Crayfish Burrows (C8)
<input type="checkbox"/> Algal Mat or Crust (B4)	<input type="checkbox"/> Presence of Reduced Iron (C4)	<input type="checkbox"/> Saturation Visible on Aerial Imagery (C9)
<input type="checkbox"/> Iron Deposits (B5)	<input type="checkbox"/> Thin Muck Surface (C7)	<input type="checkbox"/> Geomorphic Position (D2)
<input checked="" type="checkbox"/> Inundation Visible on Aerial Imagery (B7)	<input type="checkbox"/> Other (Explain in Remarks)	<input checked="" type="checkbox"/> FAC-Neutral Test (D5)
<input type="checkbox"/> Water-Stained Leaves (B9)		<input type="checkbox"/> Frost-Heave Hummocks (D7) (LRR F)

Field Observations: Surface Water Present? Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Depth (inches): <u>6</u> Water Table Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): <u> </u> Saturation Present? Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Depth (inches): <u>6</u> (includes capillary fringe)	Wetland Hydrology Present? Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>
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Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available:

Remarks:

WETLAND DETERMINATION DATA SHEET – Great Plains Region

Project/Site: Widen, Resurface & Bridge on SH-66 City/County: Lincoln Sampling Date: 3/28/22
 Applicant/Owner: Oklahoma Department of Transportation State: OK Sampling Point: DP-03
 Investigator(s): D. Daniel Section, Township, Range: S19-T14N-R3E
 Landform (hillside, terrace, etc.): Floodplain depression Local relief (concave, convex, none): Concave Slope (%): <2%
 Subregion (LRR): LRR H, MLRA 80A Lat: 35.680712 Long: -97.021356 Datum: WGS-84
 Soil Map Unit Name: Ashport clay loam, 0 to 1% slopes, occasionally flooded NWI classification: None

Are climatic / hydrologic conditions on the site typical for this time of year? Yes x No (If no, explain in Remarks.)
 Are Vegetation , Soil , or Hydrology significantly disturbed? Are "Normal Circumstances" present? Yes X No
 Are Vegetation , Soil , or Hydrology naturally problematic? (If needed, explain any answers in Remarks.)

SUMMARY OF FINDINGS – Attach site map showing sampling point locations, transects, important features, etc.

Hydrophytic Vegetation Present? Yes <u>X</u> No <u> </u> Hydric Soil Present? Yes <u>X</u> No <u> </u> Wetland Hydrology Present? Yes <u>X</u> No <u> </u>	Is the Sampled Area within a Wetland? Yes <u>X</u> No <u> </u>
Remarks:	

VEGETATION – Use scientific names of plants.

Tree Stratum (Plot size: <u>30</u>)	Absolute % Cover	Dominant Species?	Indicator Status	
1. <u>Ulmus americana</u>	10	Yes	FAC	Dominance Test worksheet: Number of Dominant Species That Are OBL, FACW, or FAC: <u>2</u> (A) Total Number of Dominant Species Across All Strata: <u>3</u> (B) Percent of Dominant Species That Are OBL, FACW, or FAC: <u>66.7%</u> (A/B)
2. _____				
3. _____				
4. _____				
=Total Cover	10			
Sapling/Shrub Stratum (Plot size: _____)				
1. _____				Prevalence Index worksheet: Total % Cover of: _____ Multiply by: OBL species <u>0</u> x 1 = <u>0</u> FACW species <u>30</u> x 2 = <u>60</u> FAC species <u>10</u> x 3 = <u>30</u> FACU species <u>10</u> x 4 = <u>40</u> UPL species <u>0</u> x 5 = <u>0</u> Column Totals: <u>50</u> (A) <u>130</u> (B) Prevalence Index = B/A = <u>2.60</u>
2. _____				
3. _____				
4. _____				
5. _____				
=Total Cover				
Herb Stratum (Plot size: <u>5</u>)				
1. <u>Chasmanthium latifolium</u>	10	Yes	FACU	Hydrophytic Vegetation Indicators: <u> </u> 1 - Rapid Test for Hydrophytic Vegetation <u>X</u> 2 - Dominance Test is >50% <u>X</u> 3 - Prevalence Index is ≤3.0 ¹ <u> </u> 4 - Morphological Adaptations ¹ (Provide supporting data in Remarks or on a separate sheet) <u> </u> Problematic Hydrophytic Vegetation ¹ (Explain) ¹ Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic.
2. <u>Equisetum hyemale</u>	30	Yes	FACW	
3. _____				
4. _____				
5. _____				
6. _____				
7. _____				
8. _____				
9. _____				
10. _____				
=Total Cover				
Woody Vine Stratum (Plot size: _____)				
1. _____				Hydrophytic Vegetation Present? Yes <u>X</u> No <u> </u>
2. _____				
=Total Cover				
% Bare Ground in Herb Stratum <u>50</u>				

Remarks:

SOIL

Sampling Point: DP-03

Profile Description: (Describe to the depth needed to document the indicator or confirm the absence of indicators.)								
Depth (inches)	Matrix		Redox Features				Texture	Remarks
	Color (moist)	%	Color (moist)	%	Type ¹	Loc ²		
0-10	7.5YR 2.5/2	75	7.5YR 5/6	25	C	M	Loamy/Clayey	

¹Type: C=Concentration, D=Depletion, RM=Reduced Matrix, CS=Covered or Coated Sand Grains. ²Location: PL=Pore Lining, M=Matrix.

Hydric Soil Indicators: (Applicable to all LRRs, unless otherwise noted.)		Indicators for Problematic Hydric Soils ³ :
<input type="checkbox"/> Histosol (A1)	<input type="checkbox"/> Sandy Gleyed Matrix (S4)	<input type="checkbox"/> 1 cm Muck (A9) (LRR I, J)
<input type="checkbox"/> Histic Epipedon (A2)	<input type="checkbox"/> Sandy Redox (S5)	<input type="checkbox"/> Coast Prairie Redox (A16) (LRR F, G, H)
<input type="checkbox"/> Black Histic (A3)	<input type="checkbox"/> Stripped Matrix (S6)	<input type="checkbox"/> Dark Surface (S7) (LRR G)
<input type="checkbox"/> Hydrogen Sulfide (A4)	<input type="checkbox"/> Loamy Mucky Mineral (F1)	<input type="checkbox"/> High Plains Depressions (F16)
<input type="checkbox"/> Stratified Layers (A5) (LRR F)	<input type="checkbox"/> Loamy Gleyed Matrix (F2)	(LRR H outside of MLRA 72 & 73)
<input type="checkbox"/> 1 cm Muck (A9) (LRR F, G, H)	<input type="checkbox"/> Depleted Matrix (F3)	<input type="checkbox"/> Reduced Vertic (F18)
<input type="checkbox"/> Depleted Below Dark Surface (A11)	<input type="checkbox"/> Redox Dark Surface (F6)	<input type="checkbox"/> Red Parent Material (F21)
<input type="checkbox"/> Thick Dark Surface (A12)	<input type="checkbox"/> Depleted Dark Surface (F7)	<input type="checkbox"/> Very Shallow Dark Surface (F22)
<input type="checkbox"/> Sandy Mucky Mineral (S1)	<input checked="" type="checkbox"/> Redox Depressions (F8)	<input type="checkbox"/> Other (Explain in Remarks)
<input type="checkbox"/> 2.5 cm Mucky Peat or Peat (S2) (LRR G, H)	<input type="checkbox"/> High Plains Depressions (F16)	³ Indicators of hydrophytic vegetation and wetland hydrology must be present, unless disturbed or problematic.
<input type="checkbox"/> 5 cm Mucky Peat or Peat (S3) (LRR F)	(MLRA 72 & 73 of LRR H)	

Restrictive Layer (if observed): Type: _____ Depth (inches): _____	Hydric Soil Present? Yes <input type="checkbox"/> No <input type="checkbox"/>
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Remarks:
This data sheet is revised from Great Plains Regional Supplement Version 2.0 to include the NRCS Field Indicators of Hydric Soils, Version 8.0, 2016.

HYDROLOGY

Wetland Hydrology Indicators:		Secondary Indicators (minimum of two required)
Primary Indicators (minimum of one is required; check all that apply)		
<input type="checkbox"/> Surface Water (A1)	<input type="checkbox"/> Salt Crust (B11)	<input type="checkbox"/> Surface Soil Cracks (B6)
<input type="checkbox"/> High Water Table (A2)	<input type="checkbox"/> Aquatic Invertebrates (B13)	<input type="checkbox"/> Sparsely Vegetated Concave Surface (B8)
<input type="checkbox"/> Saturation (A3)	<input type="checkbox"/> Hydrogen Sulfide Odor (C1)	<input type="checkbox"/> Drainage Patterns (B10)
<input checked="" type="checkbox"/> Water Marks (B1)	<input type="checkbox"/> Dry-Season Water Table (C2)	<input type="checkbox"/> Oxidized Rhizospheres on Living Roots (C3)
<input checked="" type="checkbox"/> Sediment Deposits (B2)	<input type="checkbox"/> Oxidized Rhizospheres on Living Roots (C3)	(where tilled)
<input type="checkbox"/> Drift Deposits (B3)	(where not tilled)	<input type="checkbox"/> Crayfish Burrows (C8)
<input type="checkbox"/> Algal Mat or Crust (B4)	<input type="checkbox"/> Presence of Reduced Iron (C4)	<input type="checkbox"/> Saturation Visible on Aerial Imagery (C9)
<input type="checkbox"/> Iron Deposits (B5)	<input type="checkbox"/> Thin Muck Surface (C7)	<input checked="" type="checkbox"/> Geomorphic Position (D2)
<input type="checkbox"/> Inundation Visible on Aerial Imagery (B7)	<input type="checkbox"/> Other (Explain in Remarks)	<input type="checkbox"/> FAC-Neutral Test (D5)
<input checked="" type="checkbox"/> Water-Stained Leaves (B9)		<input type="checkbox"/> Frost-Heave Hummocks (D7) (LRR F)

Field Observations:	
Surface Water Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____	Wetland Hydrology Present? Yes <input checked="" type="checkbox"/> No <input type="checkbox"/>
Water Table Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____	
Saturation Present? Yes <input type="checkbox"/> No <input checked="" type="checkbox"/> Depth (inches): _____ (includes capillary fringe)	

Describe Recorded Data (stream gauge, monitoring well, aerial photos, previous inspections), if available:

Remarks:

N R C S COORDINATION

No response received from NRCS.

See attached Farmland Impact Rating Form and Request Letter.

FARMLAND CONVERSION IMPACT RATING

PART I <i>(To be completed by Federal Agency)</i>	Date Of Land Evaluation Request
Name Of Project	Federal Agency Involved
Proposed Land Use	County And State

PART II <i>(To be completed by NRCS)</i>		Date Request Received By NRCS	
Does the site contain prime, unique, statewide or local important farmland? <i>(If no, the FPPA does not apply -- do not complete additional parts of this form).</i>	Yes <input type="checkbox"/>	No <input type="checkbox"/>	Average Farm Size
Major Crop(s)	Farmable Land In Govt. Jurisdiction Acres: %	Amount Of Farmland As Defined in FPPA Acres: %	
Name Of Land Evaluation System Used	Name Of Local Site Assessment System	Date Land Evaluation Returned By NRCS	

PART III <i>(To be completed by Federal Agency)</i>	Alternative Site Rating			
	Site A	Site B	Site C	Site D
A. Total Acres To Be Converted Directly				
B. Total Acres To Be Converted Indirectly				
C. Total Acres In Site				

PART IV <i>(To be completed by NRCS)</i> Land Evaluation Information				
A. Total Acres Prime And Unique Farmland				
B. Total Acres Statewide And Local Important Farmland				
C. Percentage Of Farmland In County Or Local Govt. Unit To Be Converted				
D. Percentage Of Farmland In Govt. Jurisdiction With Same Or Higher Relative Value				

PART V <i>(To be completed by NRCS)</i> Land Evaluation Criterion Relative Value Of Farmland To Be Converted <i>(Scale of 0 to 100 Points)</i>				
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PART VI <i>(To be completed by Federal Agency)</i> Site Assessment Criteria <i>(These criteria are explained in 7 CFR 658.5(b))</i>	Maximum Points				
1. Area In Nonurban Use					
2. Perimeter In Nonurban Use					
3. Percent Of Site Being Farmed					
4. Protection Provided By State And Local Government					
5. Distance From Urban Builtup Area					
6. Distance To Urban Support Services					
7. Size Of Present Farm Unit Compared To Average					
8. Creation Of Nonfarmable Farmland					
9. Availability Of Farm Support Services					
10. On-Farm Investments					
11. Effects Of Conversion On Farm Support Services					
12. Compatibility With Existing Agricultural Use					
TOTAL SITE ASSESSMENT POINTS	160				

PART VII <i>(To be completed by Federal Agency)</i>					
Relative Value Of Farmland <i>(From Part V)</i>	100				
Total Site Assessment <i>(From Part VI above or a local site assessment)</i>	160				
TOTAL POINTS <i>(Total of above 2 lines)</i>	260				

Site Selected:	Date Of Selection	Was A Local Site Assessment Used? Yes <input type="checkbox"/> No <input type="checkbox"/>
----------------	-------------------	---

Reason For Selection:



CC Environmental, LLC

December 21, 2021

Nicholas W. Jones
District Conservationist
Chandler Field Service Center
201 N. Sandy Lane
Chandler, OK 74834

RE: Site Assessment for Farmland Protection Policy Act (FPPA): Widen, Resurface & Bridge SH-66: From SH-102, east to US-177 in Lincoln County, OK, J3-4318(004), J/P 34318(04) and Identification of any NRCS Structures or Properties within the Study Area

Mr. Jones,

The Oklahoma Department of Transportation is in the early developmental stages of Widen, Resurface & Bridge Plans on SH-66 in Lincoln County.

Please find attached USDA Form AD-1006 and plans/footprint for the following federal actions in Lincoln County, Oklahoma.

In accordance with the current 7 CFR Part 658 – Farmland Protection Policy Act, Parts 1 and III of Form AD-1006 have been completed. Please complete the NRCS portions of this form within the next 45 days and return one copy to:

Geoff Canty
CC Environmental
P.O. Box 1292
Norman, OK 73069
Geoff@ccenviro.net

In addition, please let us know if the proposed project would impact any NRCS structures or properties such as flood control dams, wetlands, etc.

Your assistance is greatly appreciated. If you have any questions, please contact Geoff Canty at 405-761-1225 or Geoff@ccenviro.net.

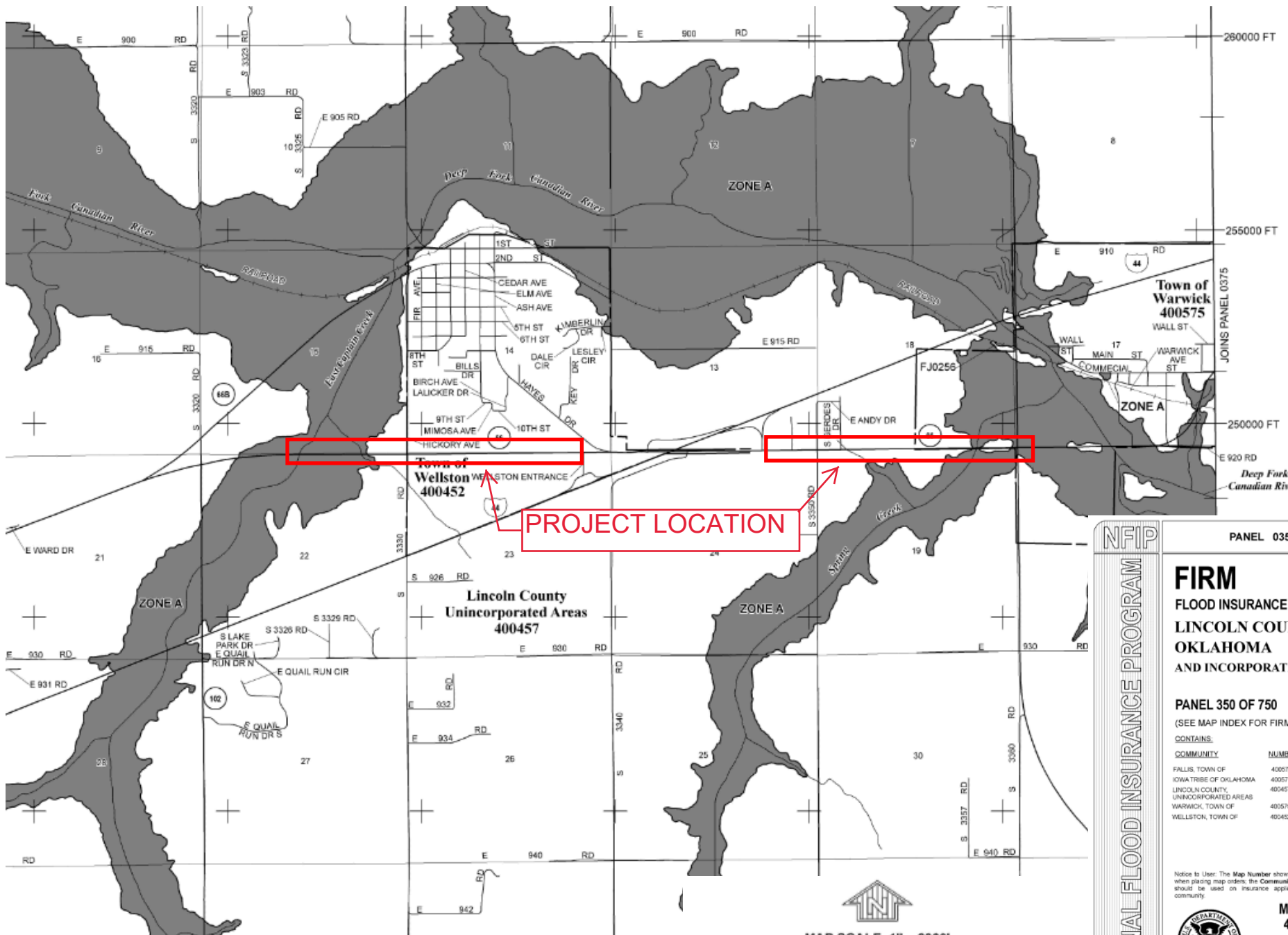
Sincerely,

A handwritten signature in black ink that reads 'GEOFF CANTY'.

Geoff Canty
CC Environmental

Enclosures: Plans and Form AD-1066
Copy to: Oklahoma Department of Transportation – Environmental Division

FLOOD PLAIN INFORMATION



PROJECT LOCATION

PANEL 0350D

FIRM
FLOOD INSURANCE RATE MAP
LINCOLN COUNTY,
OKLAHOMA
AND INCORPORATED AREAS

PANEL 350 OF 750
 (SEE MAP INDEX FOR FIRM PANEL LAYOUT)

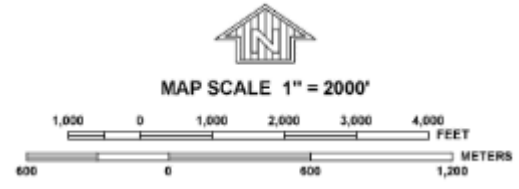
CONTAINS:

COMMUNITY	NUMBER	PANEL	SUFFIX
FALLS, TOWN OF	400572	0350	D
IOWA TRIBE OF OKLAHOMA	400577	0350	D
LINCOLN COUNTY, UNINCORPORATED AREAS	400457	0350	D
WARWICK, TOWN OF	400575	0350	D
WELLSTON, TOWN OF	400452	0350	D

Notice to User: The Map Number shown below should be used when placing map orders; the Community Number shown above should be used on insurance applications for the subject community.

MAP NUMBER
40081C0350D
EFFECTIVE DATE
AUGUST 19, 2010

Federal Emergency Management Agency



NATIONAL FLOOD INSURANCE PROGRAM



HAZARDOUS MATERIALS STUDIES

OKLAHOMA DEPARTMENT OF TRANSPORTATION CONSULTANT REPORT REVIEW – HAZARDOUS MATERIALS

Reviewed By: Evan Mace/Mike Hixon
Review Date: 5/26/2022 (updated 11/2/23)
Consultant: CC Environmental

County: Lincoln
J/P Number: 34318(04)

1. PROJECT DESCRIPTION: WIDEN RESURFACE & BRIDGE SH-66: FROM SH-102, EAST TO US-177

2. LEVEL OF INVESTIGATION: Recon Assessment Sampling

3. SUMMARY OF INVESTIGATION

- A. Relative risk of contamination in study footprint: Low Moderate High
B. Potential for contamination, if present, to affect project: Low Moderate High
C. Did Consultant recommend additional work? No Yes (describe below):

4. RECOMMENDATIONS*:

- Approval to Proceed (No Further Action)
 Approval to Proceed, Pending:
 Avoidance of described site(s)
 Plan Notes regarding described site(s) (See Section 5)
 Additional investigation by ODOT
 Approval NOT Recommended

* - If different from Consultant, explain in Section 6 General Comments

5. PLAN NOTES: None.

6. GENERAL COMMENTS: An ISA was performed by CC Environmental which identified 2 potential RECs (UST sites) on the east end of the project. 65% plans were reviewed, and per the plans, excavation is not proposed in the vicinity of the EZ-Go station. Existing drainage between the On-Cue and the road will be expanded (deeper), but the excavation does not appear to be in the vicinity of the USTs nr the pump island. Also, there have been no reported hydrocarbon leaks from the On-Cue to date. Contamination is unlikely to be encountered per review of the plans and the scope of the project. ODOT Hazardous Materials Standard Specification 107.15 is in effect in the event impacted soil/water are encountered. No further action is needed to proceed.

ATTACH EXCERPTS FROM REPORT, AS APPROPRIATE.*

*The full document is on file with ODOT's Environmental Programs Division. Please contact David Edwards at (405) 521-2673 or daedwards@odot.org, or Mike Hixon at (405) 595-8593; Michael.Hixon@odot.ok.gov for more information.

Revised 04/28/2014

INITIAL SITE ASSESSMENT

Project:

**WIDEN, RESURFACE, & BRIDGE SH-66:
FROM SH-102, EXTENDING EAST TO US-177**

LINCOLN COUNTY

JP#: 34318(04)

EC 2341A

Prepared For:



**OKLAHOMA DEPARTMENT OF TRANSPORTATION
Environmental Programs Division
Oklahoma City, OK**

Prepared By:



**CC Environmental, LLC
PO Box 1292
Norman, OK 73069
(405) 321-8181**

Report Date:

APRIL 2022

INITIAL SITE ASSESSMENT

Project:

**WIDEN, RESURFACE, & BRIDGE SH-66:
FROM SH-102, EXTENDING EAST TO US-177**

**LINCOLN COUNTY
JP#: 34318(04)**

EC 2341A

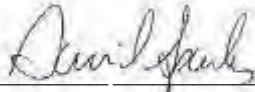
SIGNATURE CERTIFICATION:

We declare that, to the best of our professional knowledge and belief, we meet the definition of *Environmental Professional* as defined in §312.10 of 40 CFR. We have the specific qualification based on education, training, and experience to assess a property of the nature, history, and setting of the study area. We have developed and performed the all appropriate inquiries in general conformation with the standards and practices set forth in 40 CFR Part 312.

CC ENVIRONMENTAL

CCE PROJECT NO. 2340

Prepared By:



DAVID SPARKS
Environmental Specialist



DALE DANIEL
Environmental Project Manager

Reviewed By:



GEOFF CANTY
Director of Environmental Services

APRIL 9, 2022

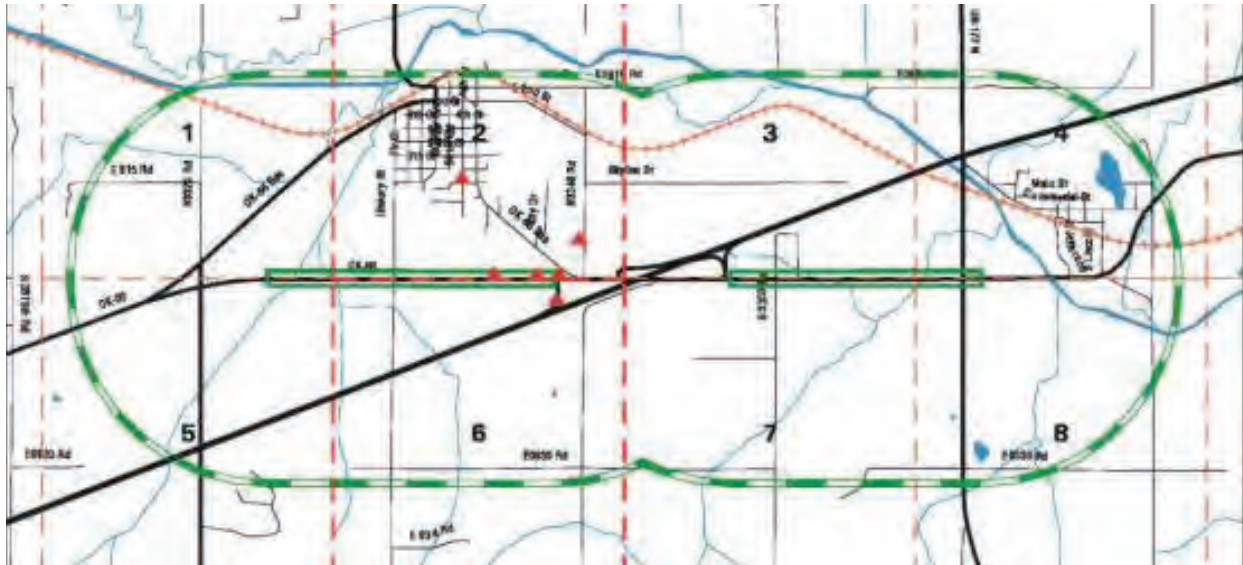
PETROLEUM STORAGE TANK WORK SCOPE CERTIFICATION SHEET

I have performed review of Petroleum Storage Tank findings within a Phase I Environmental Site Assessment in accordance with the scope and limitations of ASTM Practice E 1527-13 of the **INITIAL SITE ASSESSMENT PROJECT: WIDEN, RESURFACE, & BRIDGE SH-66: FROM SH-102, EXTENDING EAST TO US-177, LINCOLN COUNTY [JP #34318(04)], EC 2341A.**

The work scope was limited to review of Fuel Petroleum Storage Tank issues by an Oklahoma Corporation Commission (OCC) - Licensed Consultant. Pursuant to Oklahoma Administrative Code (OAC) Title 165, Chapter 26, the definition of “Regulated substances” does not include compressed natural gas, liquid natural gas, or propane. Also, oil and gas production tanks are not regulated under OAC Title 165, Chapter 26. Other aboveground petroleum storage tanks with capacity over 110 gallons must be registered, except for farm and ranch tanks, emergency generator tanks, or tanks at fleet and commercial facilities less than 2,100 gallons individual storage capacity.

This review scope focused upon Sections 1.3.4 (Site Reconnaissance), 1.3.5 (Interviews), 1.4 (Comments & Recommendations), 3.2.1 (Standard Environmental Records Review), 4.1 (Findings & Opinions), 4.2 (Recommendations), 6.2.1 (Aerial Photographs & Topographic Maps), and 6.4 (Site Photographs) of the Initial Site Assessment by CC Environmental. A summary of section 6.5.1 (EDR Database Search Results) was provided within a Memo from Geoff Canty to Sheila Baber dated 4/5/2022.

The study area is depicted below with EDR sites marked with red triangles.



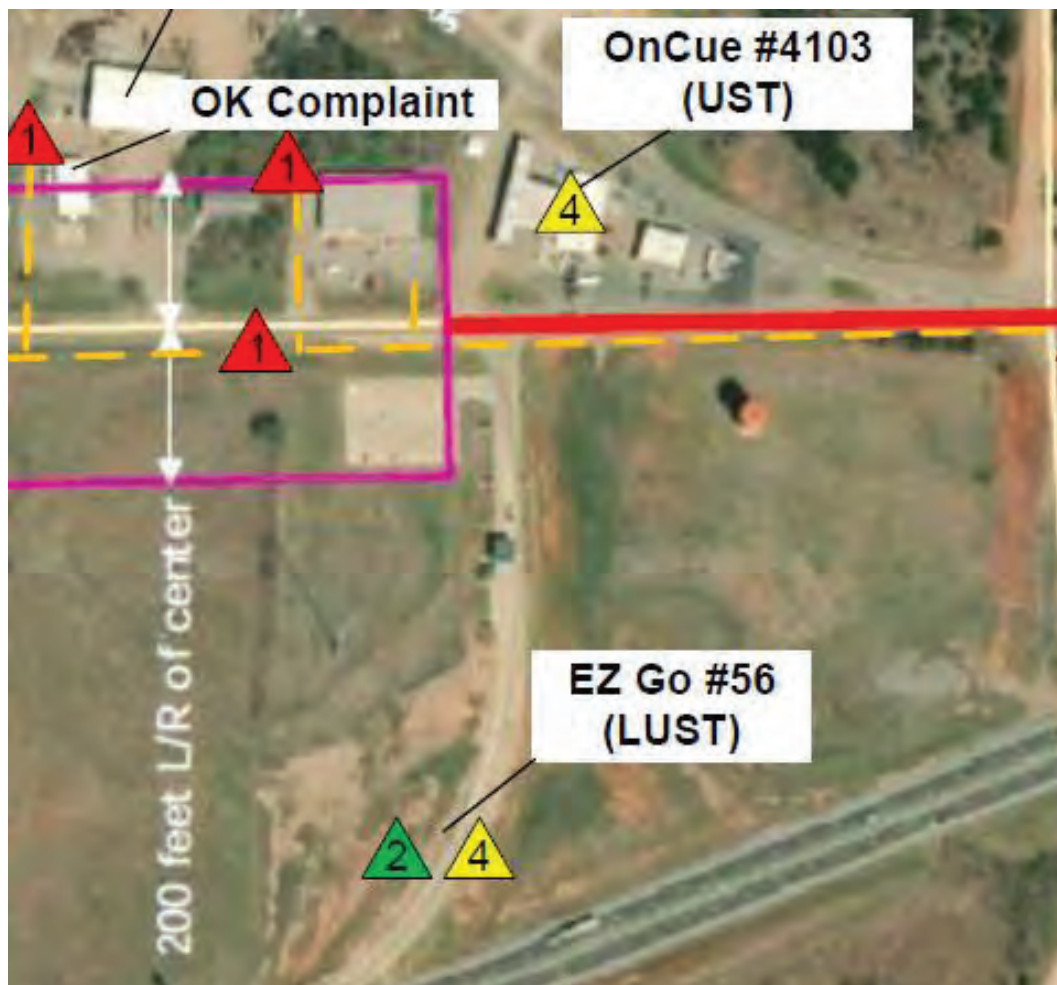
Topography for the EDR sites area falls to the west toward Captain Creek, with a ridge on the eastern boundary of the west alignment falling to the east toward Spring Creek, as follows:



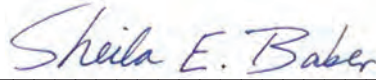
EDR map locations were transposed to maps included within the 4/5/2022 Memo. The Bus Barn site is mapped in Wellston, north and topographically separated from the study area. The Wellston OK2 AST site is reported approximately .2 mile north of the study area near the origin of a drainage swale that drains to the west. Due to distance and topography, these sites are not considered to be Recognized Environmental Conditions (REC).

The OnCue UST site is located immediately adjacent to the east boundary of the west alignment. Due to site history and proximity, it is considered a REC.

The EZ Go Leaking Underground Storage Tank (LUST) site is located south of OnCue. Groundwater is reported to flow southwest toward a drainage that crosses the highway to the west. Due to distance along the flow path this site is not considered a REC.



The petroleum storage tank facility considered a Recognized Environmental Condition (REC) is noted by **bold print** within this Certification Sheet. If portions of this property may be acquired as right-of-way or if excavation will occur in the vicinity, then additional review/testing may be warranted.

A handwritten signature in blue ink that reads "Sheila E. Baber". The signature is written in a cursive style.

Sheila E. Baber, PG
Senior Geologist, Manager, Licensed OCC Consultant #0042

4.0 FINDINGS, OPINIONS, AND RECOMMENDATIONS

Review the following points for a discussion of the items identified during this ISA evaluation.

4.1 Findings & Opinions

4.1.1 Records Review

- According to the EDR search, there were ten sites identified within the search radius of the project.
 - Review of the LUST database performed by EDR revealed two reported sites within 0.5 mile of the subject property.
 - EZ Go #56: This site (facility ID# 4110464) was located at Turner Turnpike West Box 322 approximately 500 feet south of the study area. This was listed as a closed LUST site (Case # 064-L5) in 1991 and 1998. In February and March 1989, during the UST closure, 1,800 cubic yards of impacted soil was removed from the site. Borings installed between the tank pit and restaurant did not indicate an impact, however. Four new USTs were installed. In September 1992, the product line and tank were tested due to an inventory discrepancy. The line failed the test and free product was reported in a ditch along the Turner Turnpike. Approximately 5,000 gallons of impacted water was removed. Between September 1992 and May 1993, twenty-seven borings were drilled at the site, sixteen of those were monitoring wells. The OCC closed the site in September 1998 using ORBCA standards. This site was also listed as a UST site with eight USTs permanently out of use that were removed from the ground in 1989 and 2007. Some samples taken during the closure in 2007 exceeded action levels, but under site-specific target levels established for the previous LUST. Based on the Tier 1 report, groundwater flow at the site was to the southwest down-gradient of the study area; therefore, this site was not considered to be a REC. (Note, this facility was closed and excavated circa 2010.)
 - Bus Barn: This site was located at 308 8th and Cedar approximately 0.50-mile north of the study area. This site was listed as a closed LUST site in 1997. Based on the distance of the site, this site was not considered to be a REC.
 - Review of the UST database performed by EDR revealed one additional site within 0.25 mile of the subject property.
 - OnCue #4103: This site (facility ID# 4106297) was located at 602 E Highway 66 and just outside of the study area. This site was listed as a UST site with five USTs currently in use that were installed in 1976 and 2006 that passed inspection in 2021. This site was also listed as a RCRA-VSQG site with violations that were in compliance in 2011. There have been no reported releases to the study area. However, a filling station (e.g., Bev's Convenience Store) with at least three USTs has been located at this site since the late 1970s. Due to the age of three of the tanks still in use and the site's close proximity to the study area, there is the potential for latent conditions to be present, thus this site **was considered to be a REC**.
 - Review of the AST database performed by EDR revealed one reported site within 0.25 mile of the subject property.
 - Wellston OK2 #308052: This site (facility ID# 4156975) was located 0.2 mile north on NS-334 and outside the study area. This site was listed as an AST site with one AST currently in use that was installed in 2011. There have been no reported releases to the

study area. Based on the distance of the site from the study area, this site is not considered to be a REC.

- Review of the OK COMPLAINT database performed by EDR revealed one reported site within the subject property.
 - Lincoln County: A complaint was filed for a location corresponding to the commercial business and diesel repair/farming repair facility north of the roadway and adjacent to the study area. The complaint was listed in 2012 for open burning. There have been no reported releases to the study area. Based on the status of the complaint, this site is not considered to be a REC.

Review of the TIER 2 database performed by EDR revealed one reported site within the subject property.

- Green's Propane, LLC: This site was located at 3352 West Highway 66 within the study area and immediately adjoining the highway. This site was listed as a TIER 2 site due to being a commercial propane distributor. There have been no reported releases to the study area. Based on the status of the site, this site is not considered to be a REC.
- According to the OCC Oil & Gas Division database search, there were nineteen oilfield related wells reported to be within the same quarter sections as the NEPA study area. Fifteen of the wells were listed as “*plugged and abandoned*” and were not located within the study area. No active wells were located within the study area. These well listings were not considered to be a REC.
- OWRB Search: According to the OWRB groundwater well records search, there were twenty-three monitoring wells identified within the study area. The wells appeared to be associated with the identified LUST sites and the OnCue filling station. As discussed above, the EZ Go LUST site was not considered to be a REC.

4.1.2 Site Reconnaissance

- The study area was surveyed to verify the existence of facilities identified in the environmental databases, as well as discover any additional potential environmental risks to the study area not identified in the database search. In general, the conditions observed did not change opinions regarding the environmental risk of the facilities detailed in the previous section; however, it did identify one property (auto repair facility) not listed in the database report that was elevated to a REC.
 - The study area occurred in a primarily rural residential and agricultural area. The project exception between the two study area boundaries was more of a commercial segment along an interchange for the Turner Turnpike.
 - The Green's Propane (Tier 2) site was observed within the study area. Two large commercial-sized propane tanks were noted on site along with several residential-sized tanks staged at the rear of the facility. There were no odors present or signs of stressed vegetation that may indicate a release.
 - An automotive repair facility was observed within the study area north of the roadway. Fifteen 55-gallon drums were discarded on the north side of the facility. In addition, one steel tank (approximately 250-gallon) labeled as “used antifreeze”, and what appeared to be a plastic tote of used motor oil was also noted on the north side of the building. There were no apparent large-scale releases, but the drums were in poor condition and were not in secondary containment. It is possible that past releases may have occurred based on the drum storage and housekeeping practices. **This was considered to be a REC.**

- A wrecker service with some wrecked and salvage vehicles was observed north of the roadway and in close proximity to the study area.
- An apparent diesel/farming equipment repair shop was observed north-adjacent to the study area.
- The OnCue #4103 filling station was observed as being in operation east of and in close proximity to the study area. There were no apparent releases noted.
- The J&D Metal Fabrication facility was observed north of the roadway and partially within the study area. There were no releases or discarded materials associated with the facility noted within the study area.
- Three metal ASTs (possible grain silos) were observed south of the roadway and within the study area. There were no indications of any release and the tanks did not appear to contain anything.
- An open water well was observed south of the roadway and within the study area.
- Some oilfield activity was noted in the general vicinity of the study area.

4.1.3 Interviews

- No recognized environmental conditions were identified during the interview process.

4.2 Recommendations

- The OnCue Philips 66 filling station is currently in good standing with the OCC; however, a station (e.g., Bev's Convenience Store) with three USTs has been located at this site since the late 1970s. The station is outside of the study area, but was considered to be an REC given its age, close proximity, and potential for unidentified contamination.
- During the site reconnaissance, one unnamed auto repair facility had several discarded 55-gallon drums. The drums were in poor condition, and it is possible that remaining contents could have released to the study area. This was considered to be a REC. If excavation work occurs near this site, or if this property will be acquired as part of the right-of-way/utilities process, then additional review and testing may be warranted.
- The study area occurs along a mostly rural residential and agricultural area. However, there are some commercial and light industrial activities that occur along SH-66 near the Turner Turnpike interchange. None of these facilities were considered to be RECs but may be associated with potential contaminants of concern and/or unreported releases.
- The project site is located in a historical oil and gas exploration area, (Wellston Oil Field and Wellston North Gas Field). Abandoned oil wells, gathering lines, and other pipelines may be present on or near the Property. Subsurface contamination and/or other issues may be encountered, but there were no overt indications of such.

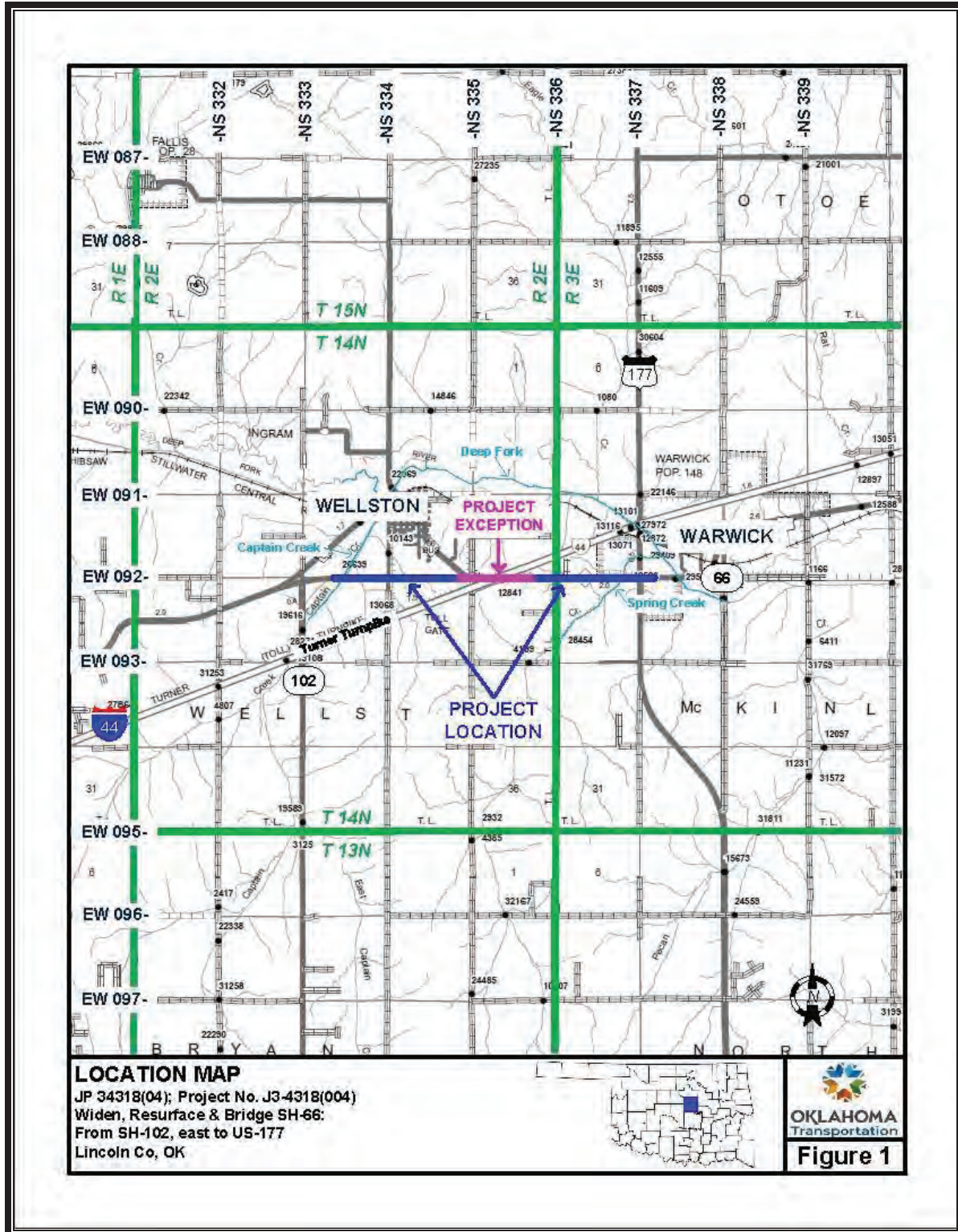


Figure 6-1: General Location Map

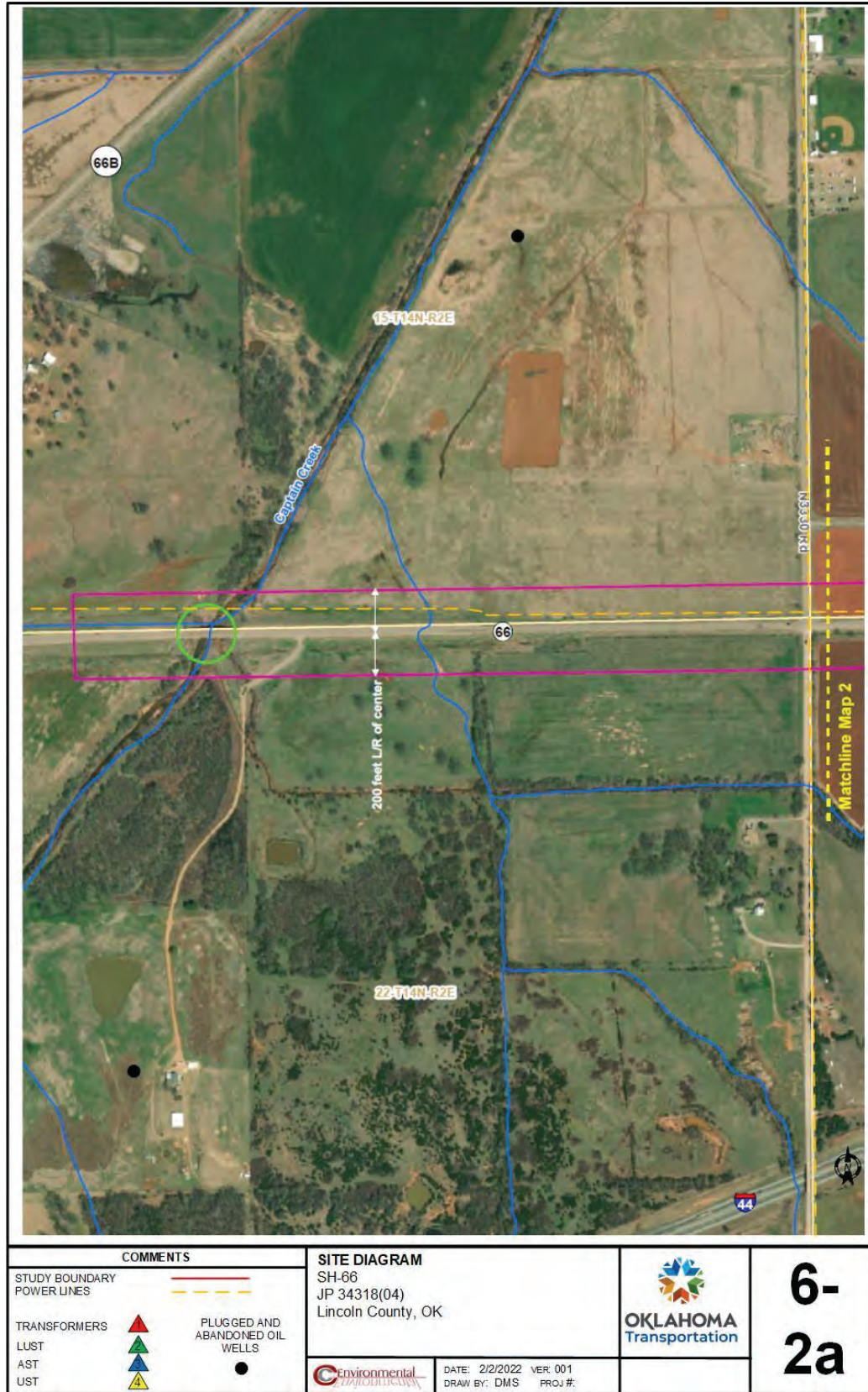


Figure 6-2a: Site Diagram

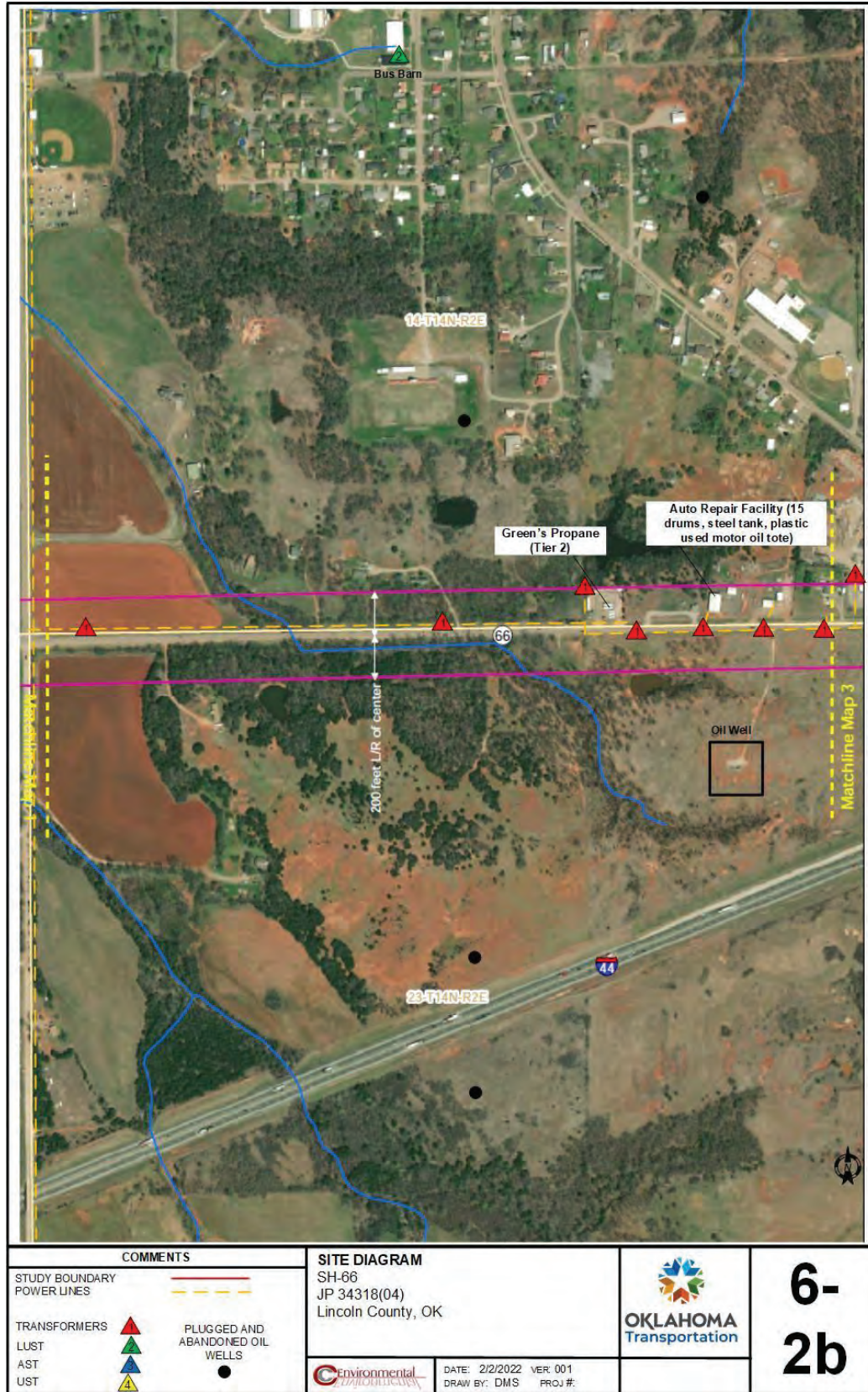


Figure 6-2b: Site Diagram

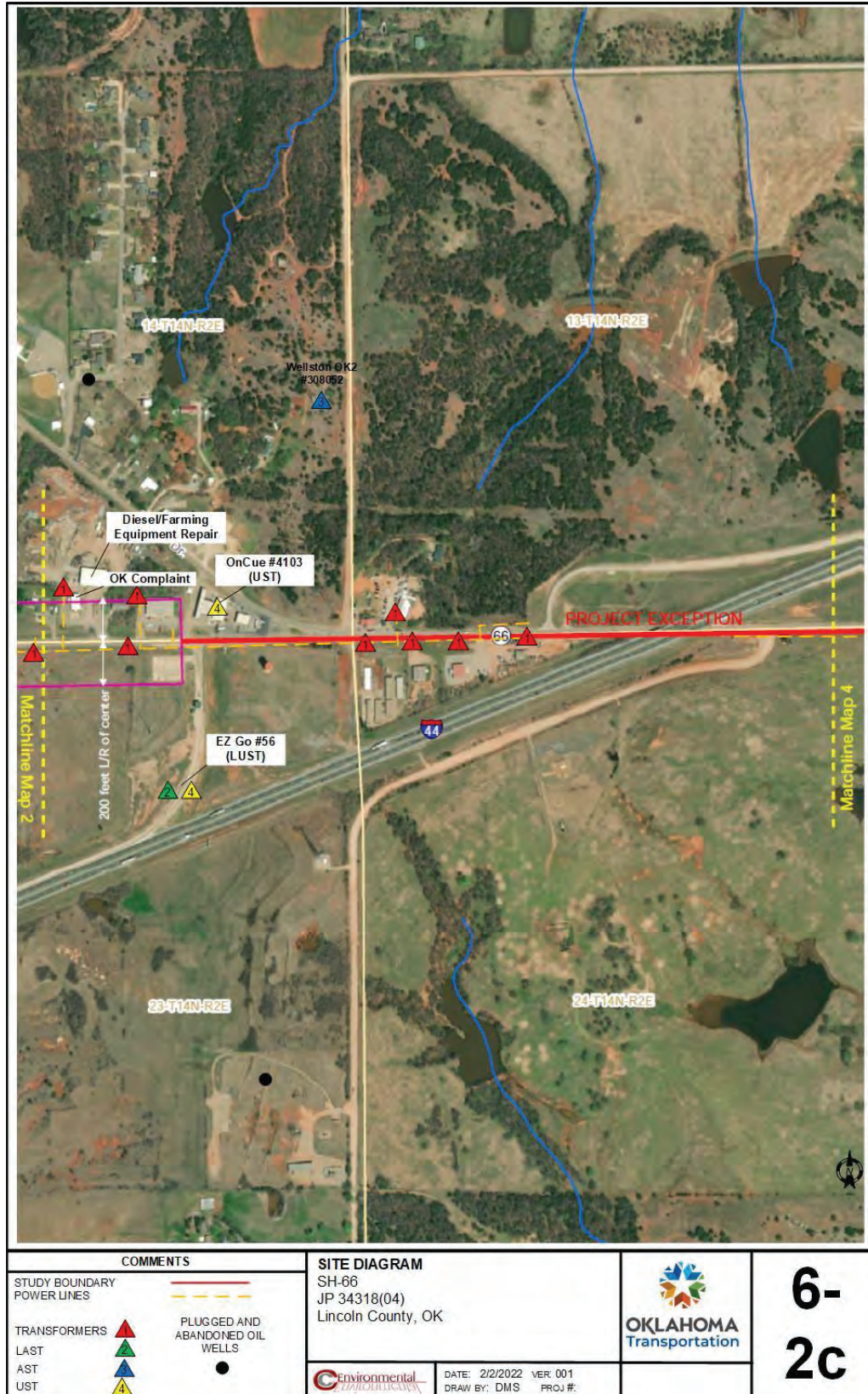


Figure 6-2c: Site Diagram



Figure 6-2d: Site Diagram

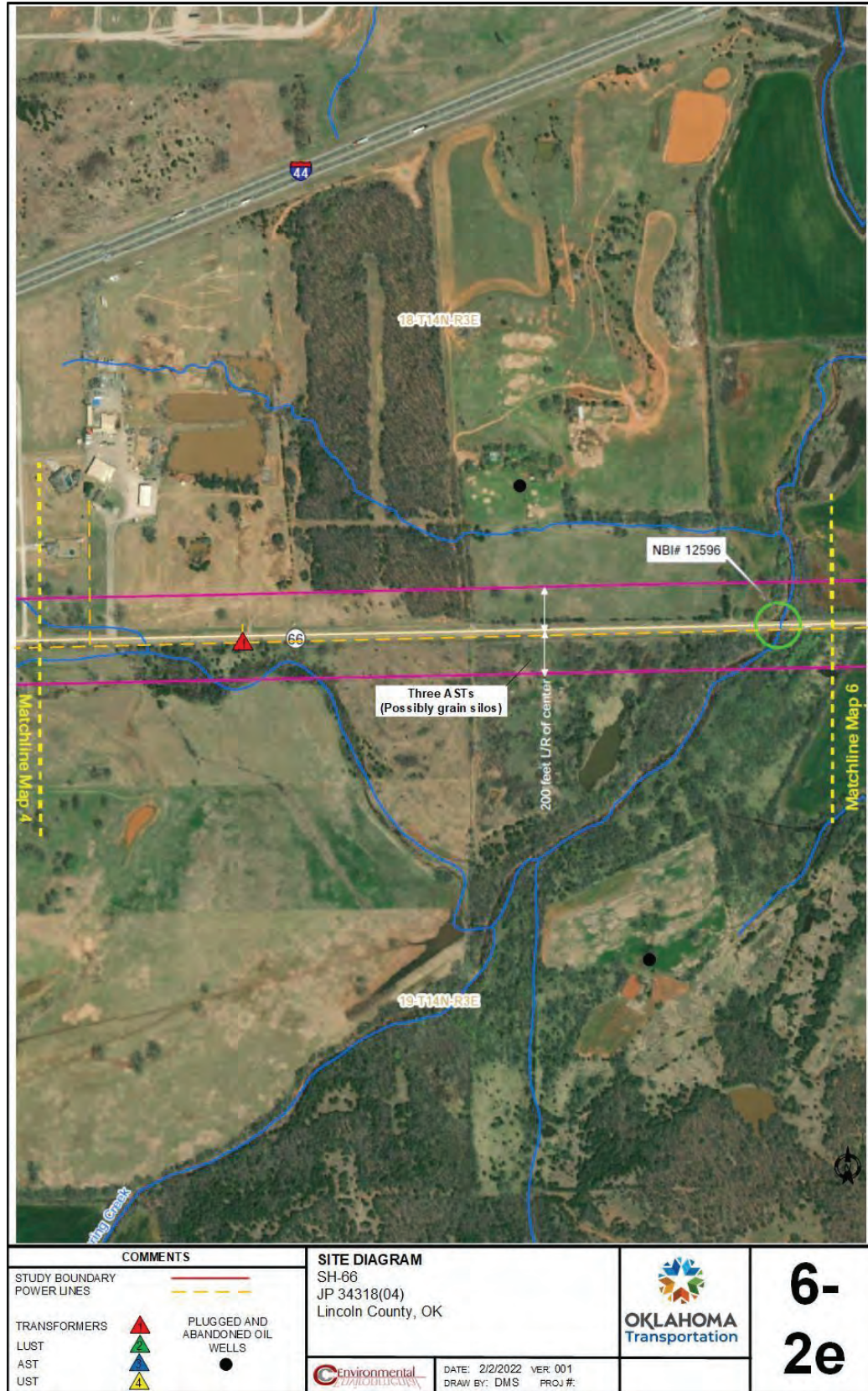


Figure 6-2e: Site Diagram

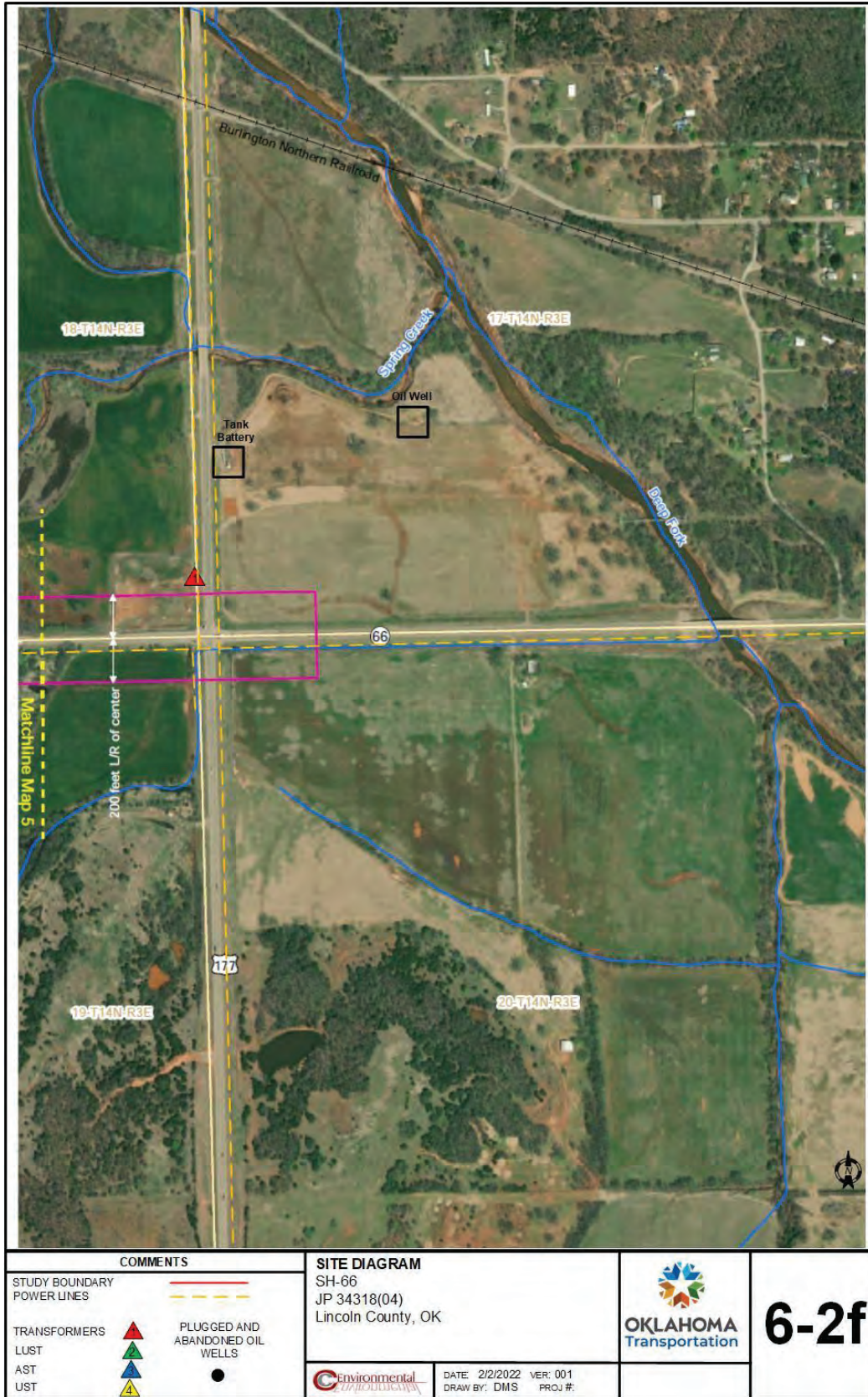


Figure 6-2f: Site Diagram

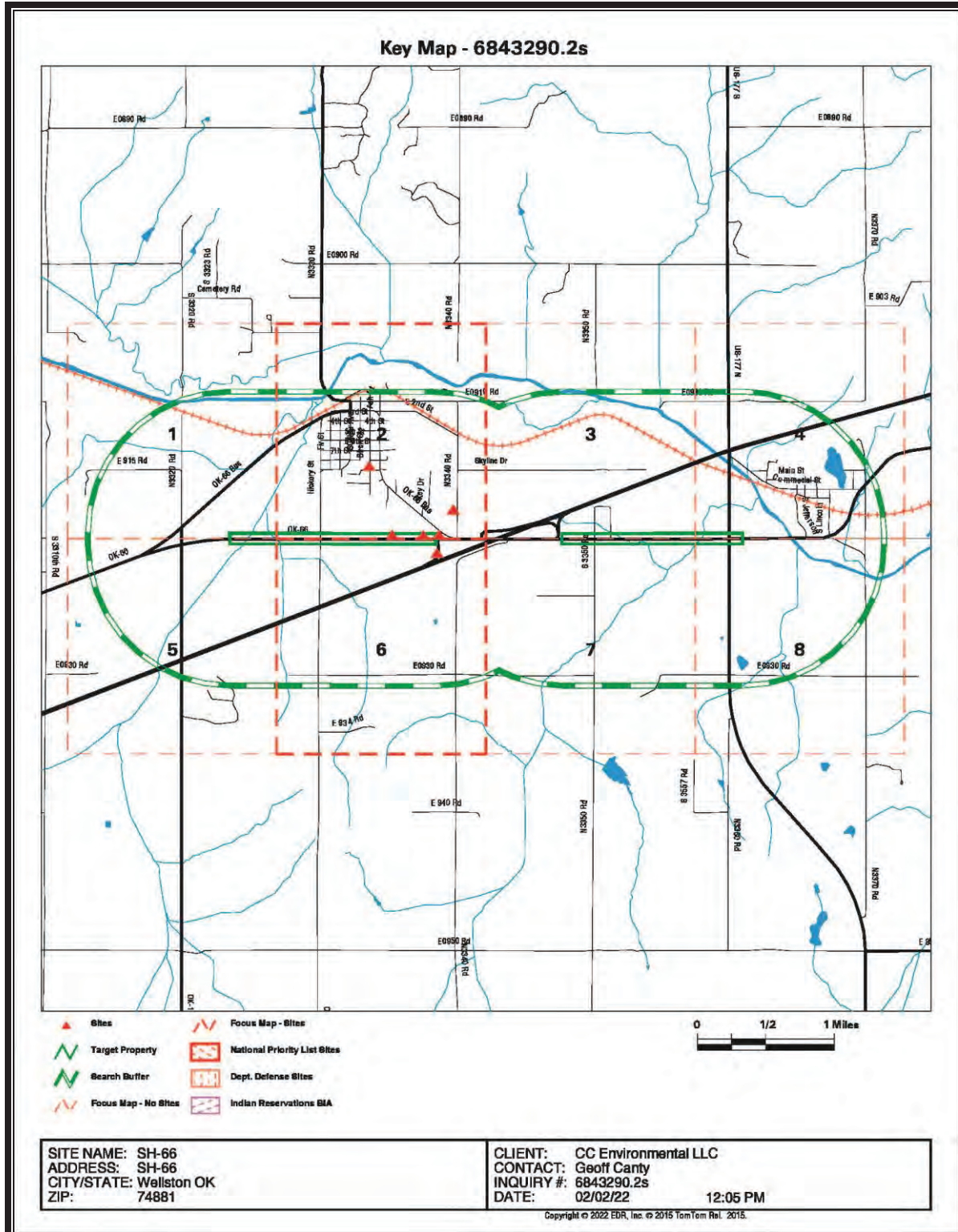


Figure 6-3: Site map depicting the extended environmental database search radii (EDR, 2022a).

OTHER



Oklahoma Department of Transportation Project Initiation Report

Report Date: 12/02/2020
Job Piece: 3431804 Div / County: 03 / 41 - LINCOLN Project ID: J3-4318(004)
Highway: SH066
Project Description: Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1.1 mile turnpike project exception)

PS&E: TBD R/W: FFY 2025 Drive-out Date: 9/30/2020 (Virtual)
Programmed Estimate: \$6,000,000

EXISTING INFORMATION

Reconnaissance Data Available Yes
Location: <http://plansrv1.agency.ok.local/osd/JP3431804/PSEDevelopment/DataReconnaissance>

Functional Classification

Area Type: Urban Suburban Rural
Terrain Type: Flat Rolling Mountainous
Access Control: Full Partial None
Highway Type: Freeway Principal Arterial Minor Arterial Collector
 NHS Non-NHS STRAHNET Scenic Hwy

Existing Condition

Current ADT: 5,300 % Trucks: 8% Number of lanes: 2 Lane Width: 12'
Outside Shoulder Width: 0-2' Inside Shoulder Width:
 Open Section Curb & Gutter Divided Median Width:
 Other (describe)
Pavement Type: Asphalt Conc. Pavement Condition: Fair
Shoulder Type: Sod Shoulder Condition: Fair
Storm Sewer No Yes Storm Sewer Condition:
Sidewalks No Yes Left Width: Right Width:

Bridges within the Project extents: SEE ATTACHED INSPECTION REPORTS

Bridge 1 NBI#: 26639 over CAPTAIN CREEK (fill/repair)
Bridge 2 NBI#: 12596 over SPRING CREEK

CONSIDERATIONS

Environmental

- Historical Properties, list**
Segments of Route 66 were identified that would specifically require documentation and review under Section 106 of the National Historic Preservation Act (NHPA).
- Archaeological Sites, list**
Archeological sites within the vicinity of the study area are primarily temporally undefined lithic scatters or early through mid-20th century farmsteads identified in the late 20th century as part of highway surveys. The likelihood of these types of archeological sites being eligible for inclusion within the NRHP is minimal
- Hazardous Waste Sites / AST's / Coal Mines / LUSTs**
 - Five (5) Hazwaste sites and three (3) tank facilities were identified within the recon area.
 - Sixteen (16) groundwater wells and no geotechnical borings were identified within the same ¼ sections as the recon area. Four (4) groundwater wells were identified within the recon area
 - The recon area is in active named oilfields: Wellston North

Report Date: 12/02/2020

Job Piece: 3431804

Div / County: 03 / 41 - LINCOLN

Project ID: J3-4318(004)

Highway: SH066

Project Description: Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1.1 mile turnpike project exception)

- The recon area is in active named oil and gas fields: Wellston West and Warwick Southeast
- The recon area is in active gas fields: Captain Creek North, Wellston, Chandler, Warwick East, Midlothian Northwest, and Happy Hill Northeast
- Twenty-six (26) oil and gas related wells were identified within the same ¼ sections as the recon area. Eight (8) of the identified oil and gas wells were observed within 660-ft. of the recon area
- Six (6) other notable facilities (one undocumented water well, three body shops, a former service station, and a fabrication business) were identified within the recon area based on data recon report. The undocumented water well was present within parcel 2. The body shops were present within parcels 5, 20, and 38. The former service station was present within parcel 45. The fabrication business was present within parcel 50.

Threatened & Endangered Species, list with seasonal restrictions

Interior Least Tern, Piping Plover, Red Knot, Whooping Crane

Section 4F or 6F Properties, list

According to the Oklahoma Tourism and Recreation Department (OTRD), no significant impacts on Land and Water Conservation Funded (LWCF) properties area expected for the proposed project

Wetlands, list

There are parallel blue line streams.

Eight mapped (aka "blue-line") streams were identified:

- Captain Creek and three of its unnamed tributaries
 - Spring Creek and two of its unnamed tributaries
 - Unnamed tributary to Deep Fork River, which is approximately 0.4-miles east of the recon area
 - Eight additional non-mapped drainage features were observed within the recon area
 - Eleven mapped wetland features were identified within the recon area. Only two of the mapped wetland features could not be verified in the field
 - Five additional non-mapped wetland features were observed within the recon area
- The recon area is located within the Garber-Wellington (GW) major bedrock aquifer.

Critical Resources / Sensitive Waters / Impaired Waters (type), list

According to the FEMA website, Captain Creek and Spring Creek cross SH-66 and are mapped in Zone A. The remaining portions of the recon area and surrounding areas were mapped in Zone X.

FEMA Flood Zone ID: A and X

Scenic Byway / Route 66

Scenic Byway/Route 66 - Proposed roadway for improvement is a section of Historic Route 66.

Alternative Impacts

- Other agencies / entities Turnpike Involvement Metropolitan Planning Organization

OTA properties are in the project exception area.

Right of Way / Utilities

Additional Right of Way anticipated, list

Approx. 20' to 30' LT & RT.

Utility Conflicts, list

OH Power LT & RT, Water line RT, switching to LT, Fiber Optic RT, Gas Line LT

Permit Information

Design Exception Anticipated: As Required by Design

Maintenance Agreements (Lighting, Signals,...), list

Report Date: 12/02/2020

Job Piece: 3431804

Div / County: 03 / 41 - LINCOLN

Project ID: J3-4318(004)

Highway: SH066

Project Description: Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1.1 mile turnpike project exception)

Permits Required: FAA USACE OWRB Railroad Other

Additional Permit Comments:

Special Considerations:

- There is an approximate 1.1 mile construction exception in the middle of the extents (On SH-66 from approx. 1.75 miles east of SH-102, through the turnpike interchange to 2.85 miles east of SH-102) This area is to be improved under OTA project # T-MC-131.
- Minor repair of fill in the backwall and around piers at NBI# 26639 Captain Creek will be required at the west end of the project in the incidental area.
- Project is on historic Route 66. May require consultation with interested parties.

PROPOSED IMPROVEMENTS

Project Intent:

To improve highway condition and safety by adding shoulders.

Description of Proposed Improvements:

- At the west end of the project (incidental construction area) complete the recommended repairs to the east pier and backwalls at Captain Creek. (flowable fill and erosion repair)
- Approx. 1,075' east of Captain Creek, (end of the improved shoulder section) begin mill (2") and overlay (5") of driving lanes and construct two full depth 8' shoulders (total of 40' clear roadway) symmetrically on existing alignment utilizing 3R criteria.
- East of Hickory Ave. (CR- 3330) begin alignment transition to the north, maintaining the southern pavement edge, in order to avoid parallel southern channel impacts at the roadway sized box approx. 1,200' east of Hickory Ave. Extend the roadway box to the north to clear zone and do not replace existing guardrail. Begin to transition back to existing alignment approx. 800' east of the roadway RCB after the parallel southern channel.
- Immediately after alignment transition, begin tapering out the typical section to include a 12' center turn lane, (52' clear roadway) and continue east to tie in to the existing 52' section from the OTA project # T-MC-13.
- At the east end of the exception area begin (approx. 950' west of CR-3350) to match the existing 52' section and begin mill (2") and overlay (5") of driving lanes and construct the additional full depth lane and 8' shoulders symmetrically on existing alignment.
- Approx. 1,875' east of CR-3350 (at the property line) begin to transition from the 52' section back down to the 40' section on existing alignment.
- At the NBI # 12596 RCB over Spring Creek, design with the recommendation of ODOT hydraulics, either extending existing RCB to 4R clear zone, or replacement with a 3-65' beam span structure on existing alignment.
- At the east end of the project tie in to the US-177 intersection turn lanes approximately 350' west of the intersection.

New Bridge Info:

ODOT Bridge Hydraulics to consider the options to extend the existing RCB, or replace with a 3-65' Type III PC beams span structure.

Design Speed:

Desired: 65

Minimum: 65

Potential to transfer Steel Beams to County: Yes No N/A

Report Date: 12/02/2020

Job Piece: 3431804

Div / County: 03 / 41 - LINCOLN

Project ID: J3-4318(004)

Highway: SH066

Project Description: Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1.1 mile turnpike project exception)

Project Termini

Beginning of Project: Along SH-66, begin incidental construction at the west end of NBI# 26639, Captain Creek. Begin roadway improvements approximately 0.7 miles east of SH-102 at the improved shoulder section, extending east.

End of Project: Along SH-66, end at the improved shoulder section and turn lanes immediately east of the US-177 intersection.

Limits of Survey: Along SH-66, begin at a the west end of the Captain Creek Bridge, extending east to the I-44 (turnpike) WB on ramp (500' inside of OTA project) Begin after exception area at the I-44 (turnpike) EB on ramp (approx. 500' inside of OTA project.), extending east to a point 500' east of US-177. Survey 200' LT & RT of existing, extending to 500' LT & RT at RCB structures. Survey 1000' up and downstream along creek channels.

Limits of NEPA: 200' LT & RT, beginning at the west end of Captain Creek, and the same extents as the project description (excluding the exception area). Cultural Resources will begin with the 30% plans.

Typical Section

Open Section Curb & Gutter Divided Median Width:
 Other

Number of lanes: 2 to 3 Lane Width: 12'

Outside Shoulder Width: 8' Inside Shoulder Width:

Storm Sewer: No (only if needed in C&G section) Yes

Sidewalks: No Yes

Overlay: No Yes Thickness: 5"

Coldmill: No Yes Thickness: 2"

Add Shoulders No Yes Width: 8'

Bridge Width: TBD

Alignment

Existing
 New, located North South East West of existing
 Parallel lanes, located North South East West of existing

Decision comments:

Spot Improvements
 Horizontal, Description: Offset alignment approx. 12' to the north (maintaining south pavement edge) to avoid parallel creek channel east of Hickory Ave.

Vertical, Description:

Detour

Shoo-fly, located North South East West of existing
 Widening, located North South East West of existing

Crossovers

Close Road

Signed Detour, Route Desc:

Anticipated Duration of Detour:

Agreement Required for Detour

Phased Construction, Desc: Phase construction by widening to the north with scab-on pavement to maintain 2 driving lanes at all times.

Report Date: 12/02/2020

Job Piece: 3431804

Div / County: 03 / 41 - LINCOLN

Project ID: J3-4318(004)

Highway: SH066

Project Description: Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1.1 mile turnpike project exception)

Aesthetics

Yes No

Traffic Items

Traffic Management Plan No Yes

Median Barrier No Yes

New Guardrail No Yes

End Treatment No Yes, Type: G.E.T. (only if span bridge is recommended)

Highway Lighting No Yes Outside Median

Traffic Signals No Yes

Miscellaneous

Channel Work No Relocation Re-alignment Cleanup

Public Involvement No Road Closure Letters

Public Meeting

Stakeholder Meeting

PROGRAMMING INFORMATION

RW Project Needed No Yes

Utility Project Needed No Yes

Initiation Estimate

Roadway:	\$7,100,000	Total Construction:	\$8,920,000
Bridge:	\$1,820,000		
Traffic Control:		Right of Way:	\$1,635,000
Signing and Striping:		Utilities:	\$3,924,857
Highway Lighting:			
Traffic Signals:			
Mobilization:			
Staking:			
E & C:		Total Estimate:	\$14,479,857

Comments:

Program Revisions

Estimate: _____ Letting Date: _____ / _____ Project Length: _____

Work Type: _____

Description: _____

Report Date: 12/02/2020

Job Piece: 3431804 Div / County: 03 / 41 - LINCOLN

Project ID: J3-4318(004)

Highway: SH066

Project Description: Widen, Resurface & Bridge on SH-66 from SH-102, east to US-177 (includes an approx. 1.1 mile turnpike project exception)

Attendees

Ron Brown, District 3
Bill Wilkinson, District 3
Matthew Helton, Wewoka Residency
Carissa Russell, Roadway Design Division
Kamber Norman, Roadway Design Division
Steven Bowen, Roadway Design Division, Geometrics
Carl Eldridge, Roadway Design Division
Eduardo Elder, Roadway Design Division
Roland Sison, Bridge Design Division
Leslie Lewis, Bridge Division - Hydraulics
Steve Jacobi, Bridge Design Division
Amanda Alexander, Environmental Programs Division
Derrick Anderson, Survey Division
Thomas Maclin, RW and Utilities Division
Joe Brutsche, Project Management Div.

Report Author: Joe Brutsche

Oklahoma Dept. of Transportation - Bridge Inspection Report

NBI No.: 26639	Structure No.: 4106 0372 X	Local ID: -1	Suff. Rating: 99.20	ND																														
Bridge Description: 80ft.X100ft.X80ft. PC BEAM		IDENTIFICATION																																
1. State: Oklahoma 2. Division: Division 3 3. County: LINCOLN 4. City: Unknown Admin Area: Unknown 5a. On/Under: Route On Structure 5b. Kind of Hwy: State Hwy 5c. Lvl of Svc: Mainline 5d. Route No.: 00066 5e. Dir. Sufx: N/A (NBI)		7. Facility Carried : S.H. 66 6. Feat. Intersect: CAPTAIN CREEK 9. Location: 3.7E OF OKLAHOMA CL 11. Mile Post: 3.719 mi 13. LRS Inv. / Sub Rte: -1 / -1 16. Latitude: 35° 40' 51.56" 17. Longitude: 097° 04' 48.30" 98. Border Brdg: Unknown (P) % Responsible: 0.00 99. Border Brdg #: Unknown																																
STRUCTURE TYPE AND MATERIALS		INSPECTION																																
43a/b. Main Span: P/S Conc. / Stringer/Girder 44a/b. Appr. Span: P/S Conc. / Stringer/Girder 45. # of Main Spans: 3 46. # of Appr. Spans: 3 107. Deck Type: Concrete-Cast-in-Place 108a. Wearing Surface: Monolithic Concrete 108b. Membrane: Unknown 108c. Deck protection: Epoxy Coated Reinforci		<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>Type</th> <th>Insp. Req.</th> <th>Insp. Done</th> <th>Freq.</th> <th>Insp. Date</th> <th>Next Insp.</th> </tr> </thead> <tbody> <tr> <td>NBI:</td> <td></td> <td>1</td> <td>24 months</td> <td>8/12/2020</td> <td>08/12/2022</td> </tr> <tr> <td>FC:</td> <td>N</td> <td>0</td> <td></td> <td>NA</td> <td>NA</td> </tr> <tr> <td>UW:</td> <td>N</td> <td>0</td> <td></td> <td>NA</td> <td>NA</td> </tr> <tr> <td>OS:</td> <td>N</td> <td>0</td> <td></td> <td>NA</td> <td>NA</td> </tr> </tbody> </table>			Type	Insp. Req.	Insp. Done	Freq.	Insp. Date	Next Insp.	NBI:		1	24 months	8/12/2020	08/12/2022	FC:	N	0		NA	NA	UW:	N	0		NA	NA	OS:	N	0		NA	NA
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UW:	N	0		NA	NA																													
OS:	N	0		NA	NA																													
AGE AND SERVICE		CLASSIFICATION																																
19. Detour Length: 3.7 mi 27. Year Built: 2001 28a/b. Lanes on/und: 2 / 0 29. ADT: 2,900 30. Year of ADT: 2018 42a/b. Type of Svc on/und: Highway / Waterway		12. Base Hwy Net.: Not on Base Network 20. Toll Facility: On free road 21. Custodian: State 22. Owner: State 26. Function Class: 07 Rural Mjr Collecto 37. Historical Sig.: Not eligible for NRHP 100. Def. Hwy: Not a STRAHNET hwy																																
GEOMETRIC DATA		CONDITION																																
10. Vert. Clearance: 99.99 ft 32. Appr Rwy Width: 40.00 ft 33. Median: No median 34. Skew: 30.00° 35. Struct. Flared: No flare 47. Horizontal Clr: 24.00 ft 48. Length Max Span: 100.00 ft 49. Struct. Length: 262.00 ft		58. Deck: 7 Good 62. Culvert: N/A (NBI) 59. Sup.: 7 Good 61. Chan./Chan. Prot.: 9 No Deficiencies Flowline Notes 30 ft. 3 in. TOP OF RAIL, N. SIDE 0 ft. 6 in. DEEP																																
OKLAHOMA ITEMS		LOAD RATING AND POSTING																																
200c. Temperature: 78 200d. Weather: Cloudy 201. Struc.Stl. ASTM Desig.: -1 / -1 202. Waterprf. Membrane: -1 Date Installed: 01/01/1901 203. Type Exp. Device: Sealed Expansion Joint 204. Type of Railing: TR-3 205. Material Quantity: -1.00 208a. Type of Abutment: Skeleton b. Type of Found.: Steel Piling 209. Type of Pier/Found.: 2 / No Drilled Shaft-No Footing 210. Foundation Elev.: <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>-1.00</td> <td>-1.00</td> <td>-1.00</td> </tr> <tr> <td>-1.00</td> <td>-1.00</td> <td>-1.00</td> </tr> </table> 211. Wear. Surf. Prot. Sys: Date Installed: 01/01/1901 211c. Silane Reapplied 211d. Date : 213. Utilities Attached:		-1.00	-1.00	-1.00	-1.00	-1.00	-1.00	31. Design Load: HL 93 41. Post. Status: A Open, no restriction 70. Posting: 5 At/Above Legal Loads 63. Op / 65. Inv. Rating Meth.: 1 LF Load Factor / 1 LF Load Factor <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th></th> <th>H</th> <th>HS</th> <th>3-3</th> <th>EV3</th> <th>SHV</th> </tr> </thead> <tbody> <tr> <td>64. Operating Rating (tons):</td> <td>56.00</td> <td>91.05</td> <td>112.99</td> <td>0.00</td> <td>0.00</td> </tr> <tr> <td>66. Inventory Rating (tons):</td> <td>39.02</td> <td>55.01</td> <td>70.99</td> <td>-1.00</td> <td></td> </tr> </tbody> </table> Date Rated: 02/28/2005				H	HS	3-3	EV3	SHV	64. Operating Rating (tons):	56.00	91.05	112.99	0.00	0.00	66. Inventory Rating (tons):	39.02	55.01	70.99	-1.00							
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GEOMETRIC DATA		APPRAISAL																																
50a. Curb/Sdwk Width L: 0.00 ft 50b. Curb/Sdwk Width R: 0.00 ft 51. Width Curb to Curb: 40.00 ft 52. Width Out to Out: 42.16 ft Deck Area: 11,043.77 sq. ft 53. Min. Vert. Cl. Ovr Brg: 99.99 ft 54a. Min. Vt. Undclr. Ref.: N Feature not hwy c 54b. Min. Vert. Undclr.: 0.00 ft 55a. Min. Lat. Undclr. Ref.: N Feature not hwy 55. Min. Lat. Underclr. R: 0.00 ft 56. Min. Lat. Underclr. L: 0.00 ft		68. Deck Geom.: 6 Equal Min Criteria 69. Vert./Horiz. Undclr: Not applicable (NB) 71. Waterway Adeq: 9 Above Desirable 72. Appr. Alignment: 9 Above Desirable Cr 67. Str Evaluation: 7 Above Min Criteri 113. Scour Critical: 8 Stable Above Footir																																
OKLAHOMA ITEMS		PROPOSED IMPROVEMENTS																																
214a. Posted Weight Limit: NR b. Posted Speed Limit: NR c. Narrow/1way Brdg Sign: No d. Vertical Clr. Sign: No Adv. Warning Sign: No e. Navigation Lights?: NA Working/Not Working: NA 215. Overpass: STATE HIGHWAY 218. Functionally Obsolete: - 220. Bridge Redecked: - 221. Substr. Cond. (U/W): 222. Fill Over RCB: 223. Appr. Slab/Rwy Cond.: 2 225. Paint Type/Ovrct: N/A 226. Date Painted: 227. Paint Color: -1 233. Deck Forming: 238. School Bus Rte.: Current & Desired route 240. Appr. Rwy Type: - 243. Grdr Spacing/No.: /		94. Bridge Cost: \$826,268 95. Roadway Cost: \$1,363,342 96. Total Cost: \$2,313,550 97. Yr. of Cost Est.: 2015 75. Type of Work: 31 Repl-Load Capacity 76. Lngth of Improvement: 340.5 ft 114. Future ADT: 4,640 115. Yr. of Future ADT: 2038																																
OKLAHOMA ITEMS		NAVIGATION DATA																																
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Oklahoma Dept. of Transportation - Bridge Inspection Report

NBI No.: 26639	Structure No.: 4106 0372 X	Local ID: -1	Suff. Rating: 99.20	ND
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Inspection Date: 8/12/20 Adam Hill
 Invoice No.: Lincoln2020 Inspected With: Erik Cox

Adam R. Hill

Digitally signed by Adam R. Hill
 DN: cn=Adam R. Hill, o=ODOT Div 3, ou=with Erik W. Cox, email=ahill@odot.org, c=US
 Date: 2020.08.20 09:16:35 -05'00'

BRIDGE NOTES:

HISTORY, UPSTREAM IS FED BY TWO CHANNELS, 2BBL CULVERT IS CARRYING PRIVATE DRIVE APPR 150' S-E OF BRIDGE. PROJ. DONE 06/2019, J/P # 33701(05)

INSPECTION NOTES: 8/12/20

#61 J. (FX) S-E/S-W DITCH IS NOT DRAINING PROPERLY A. SOME EROS TO BANKS BEYOND RIP RAP, #223(FX) LEVEL UP NEEDED @ W. END. BEAVER SLIDE @ THE S-E HAS COLLAPSED. # 215 (FX) VERTICAL CRACKS W/ STAINS & SOME LEACHING TO CORNERS OF BKWL'S @ BOTH ABUT'S.

ELEMENT CONDITION STATE DATA

Elem. / Env	Description	Unit	Total Qty	% 1	Qty. 1	% 2	Qty. 2	% 3	Qty. 3	% 4	Qty. 4
12 / 4	Re Concrete Deck	sq.ft	10,480.00	90%	9,432.00	10%	1,048.00	0%	0.00	0%	0.00
LONGITUDINAL CRACK RUNS THE LENGTH OF E. SPAN OVER S. BEAM. TRANSVERSE CRACKS, SOME ARE SHOWING THRU INTO SOFFIT.											
521 / 4	Conc Prot Coating	sq.ft	10,480.00	100%	10,480.00	0%	0.00	0%	0.00	0%	0.00
FLOODCOAT APPLIED ON CONTRACT 06/2019.											
109 / 4	Pre Opn Conc Girder/Beam	ft	930.00	100%	930.00	0%	0.00	0%	0.00	0%	0.00
FX- MINOR SPALLS TO BOTTOM FLANGE (PROBABLY RIP RAP PLACEMENT) TO OUTSIDE BEAMS E. SPAN. SOME CRACKS TO WEBS OF 3 BMs @ W. ABUT.											
202 / 4	Steel Column	each	4.00	100%	4.00	0%	0.00	0%	0.00	0%	0.00
STEEL PILES EXP. @ ABUT. # 2.											
205 / 4	Re Conc Column	each	4.00	100%	4.00	0%	0.00	0%	0.00	0%	0.00
S-E COL IS IN A SECONDARY CHANNEL^ SHAFT CAN IS EXPOSED.											
215 / 4	Re Conc Abutment	ft	97.00	91%	88.00	9%	9.00	0%	0.00	0%	0.00
FX- VERTICAL CRACKS W/ STAINS & SOME LEACHING TO CORNERS OF BKWL'S. @ BOTH ABUTS.											
234 / 4	Re Conc Pier Cap	ft	97.00	98%	95.00	2%	2.00	0%	0.00	0%	0.00
FX- FOOTBALL SIZE SPALL TO E. SIDE OF E. CAP. NO REBAR EXP.											
310 / 4	Elastomeric Bearing	each	24.00	100%	24.00	0%	0.00	0%	0.00	0%	0.00
-1											
321 / 4	Re Conc Approach Slab	sq.ft	2.00	0%	0.00	100%	2.00	0%	0.00	0%	0.00
PX- WATER STANDS @ N-E CORNER. DIAG. CRACKS EXIST @ THE E. & W. APPR. SLABS. ALSO^ THE WEST APPR. SLAB HAS LIFTED UP OUT OF VERTICAL ALIGNMENT APPROX. 1/8in. - 1/4in. . E. SLAB HAS SETTLED APPROX. 6in..											
331 / 4	Re Conc Bridge Railing	ft	525.00	87%	456.00	13%	69.00	0%	0.00	0%	0.00
RAIL HAS A FEW CLOSED VERTICAL CRACKS.											
819 / 4	PS Conc.Gird.End(5Ft)	(LF)	120.00	74%	89.00	26%	31.00	0%	0.00	0%	0.00
FX- SEE NOTE FOR #109											
859 / 4	Soffit	(EA)	1.00	0%	0.00	100%	1.00	0%	0.00	0%	0.00
TRANSVERSE CRACKS W/ LEACHING.											
870 / 4	Concrete Wingwall	(EA)	4.00	100%	4.00	0%	0.00	0%	0.00	0%	0.00
906 / 4	Sealed Exp.Jt.(SEJ-3)	(LF)	46.00	100%	46.00	0%	0.00	0%	0.00	0%	0.00
-1											
909 / 4	Pourable Fix Jt.Seal	(LF)	139.00	100%	139.00	0%	0.00	0%	0.00	0%	0.00
JT'S REPAIRED ON CONTRACT 2019.											
916 / 4	St.Bearing Assembly	(LF)	24.00	100%	24.00	0%	0.00	0%	0.00	0%	0.00
-1											
958 / 4	Concrete Cracking SF	(EA)	1.00	100%	1.00	0%	0.00	0%	0.00	0%	0.00
FX- SEE NOTE FOR #012.											

Oklahoma Dept. of Transportation - Bridge Inspection Report

NBI No.: 12596	Structure No.: 4106 0702 X	Local ID: -1	Suff. Rating: 65.60	FO																														
Bridge Description: (13ft.-17ft.-13ft.) X 15ft. X 36ft. RDY R.C. BOX SK 60 W/ RAILS & 3ft. C. WALL D.STREAM		IDENTIFICATION																																
1. State: Oklahoma 2. Division: Division 3 3. County: LINCOLN 4. City: Unknown Admin Area: Unknown 5a. On/Under: Route On Structure 5b. Kind of Hwy: State Hwy 5c. Lvl of Svc: Mainline 5d. Route No.: 00066 5e. Dir. Sufx: N/A (NBI)		7. Facility Carried : S.H. 66 6. Feat. Intersect: SPRING CREEK 9. Location: 7.0 MI E OKLA. C/L 11. Mile Post: 7.065 mi 13. LRS Inv. / Sub Rte: -1 / -1 16. Latitude: 35° 40' 51.50" 17. Longitude: 097° 01' 15.05" 98. Border Brdg: Unknown (P) % Responsible: 0.00 99. Border Brdg #: Unknown																																
STRUCTURE TYPE AND MATERIALS		INSPECTION																																
43a/b. Main Span: Concrete / Culvert 44a/b. Appr. Span: Unknown / Unknown (P) 45. # of Main Spans: 3 46. # of Appr. Spans: 0 107. Deck Type: N/A (NBI) 108a. Wearing Surface: N/A (no deck (NBI)) 108b. Membrane: N/A (no deck (NBI)) 108c. Deck protection: N/A (no deck (NBI))		<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>Type</th> <th>Insp. Req.</th> <th>Insp. Done</th> <th>Freq.</th> <th>Insp. Date</th> <th>Next Insp.</th> </tr> </thead> <tbody> <tr> <td>NBI:</td> <td></td> <td>1</td> <td>24 months</td> <td>8/15/2022</td> <td>08/15/2024</td> </tr> <tr> <td>FC:</td> <td>N</td> <td>0</td> <td></td> <td>NA</td> <td>NA</td> </tr> <tr> <td>UW:</td> <td>N</td> <td>0</td> <td></td> <td>NA</td> <td>NA</td> </tr> <tr> <td>OS:</td> <td>N</td> <td>0</td> <td></td> <td>NA</td> <td>NA</td> </tr> </tbody> </table>			Type	Insp. Req.	Insp. Done	Freq.	Insp. Date	Next Insp.	NBI:		1	24 months	8/15/2022	08/15/2024	FC:	N	0		NA	NA	UW:	N	0		NA	NA	OS:	N	0		NA	NA
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10. Vert. Clearance: 99.99 ft 32. Appr Rwy Width: 24.00 ft 33. Median: No median 34. Skew: 30.00° 35. Struct. Flared: No flare 47. Horizontal Clr: 24.00 ft 48. Length Max Span: 17.06 ft 49. Struct. Length: 54.13 ft		58. Deck: N/A (NBI) 62. Culvert: 6 Deterioration 59. Sup.: N N/A (NBI) 61. Chan./Chan. Prot.: 4 Protection Undermined 60. Sub: N N/A (NBI)																																
OKLAHOMA ITEMS		LOAD RATING AND POSTING																																
200c. Temperature: 96 200d. Weather: Ptty Cloudy 201. Struc.Stl. ASTM Desig.: -1 / -1 202. Waterprf. Membrane: -1 Date Installed: 01/01/1901 203. Type Exp. Device: Open Joint-No Device 204. Type of Railing: SFP-1 205. Material Quantity: -1.00 208a. Type of Abutment: b. Type of Found.: 209. Type of Pier/Found.: - / 210. Foundation Elev.: <table border="1" style="display: inline-table; margin-right: 10px;"> <tr><td>-1.00</td></tr> </table> <table border="1" style="display: inline-table; margin-right: 10px;"> <tr><td>8,335.00</td></tr> </table> <table border="1" style="display: inline-table; margin-right: 10px;"> <tr><td>-1.00</td></tr> </table> <table border="1" style="display: inline-table; margin-right: 10px;"> <tr><td>-1.00</td></tr> </table> <table border="1" style="display: inline-table;"> <tr><td>-1.00</td></tr> </table> 211. Wear. Surf. Prot. Sys: None Date Installed: 01/01/1901 211c. Silane Reapplied 211d. Date: 213. Utilities Attached: Communication		-1.00	8,335.00	-1.00	-1.00	-1.00	31. Design Load: MS 18 (HS 20) 41. Post. Status: A Open, no restriction 70. Posting: 5 At/Above Legal Loads 63. Op / 65. Inv. Rating Meth.: 2 AS Allowable Stress / 2 AS Allowable Stress 64. Operating Rating (tons): <table border="1" style="display: inline-table; margin-right: 10px;"> <tr><td>32.96</td></tr> </table> <table border="1" style="display: inline-table; margin-right: 10px;"> <tr><td>49.05</td></tr> </table> <table border="1" style="display: inline-table; margin-right: 10px;"> <tr><td>0.00</td></tr> </table> <table border="1" style="display: inline-table; margin-right: 10px;"> <tr><td>0.00</td></tr> </table> <table border="1" style="display: inline-table;"> <tr><td>0.00</td></tr> </table> 66. Inventory Rating (tons): <table border="1" style="display: inline-table; margin-right: 10px;"> <tr><td>19.95</td></tr> </table> <table border="1" style="display: inline-table; margin-right: 10px;"> <tr><td>36.05</td></tr> </table> <table border="1" style="display: inline-table; margin-right: 10px;"> <tr><td>-1.00</td></tr> </table> <table border="1" style="display: inline-table;"> <tr><td>-1.00</td></tr> </table> Date Rated: 01/01/1901			32.96	49.05	0.00	0.00	0.00	19.95	36.05	-1.00	-1.00																
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-1.00																																		
NAVIGATION DATA		APPRAISAL																																
38. Nav. Control: Permit Not Required 39. Vert. Clearance: 0.0 ft 40. Horiz. Clearance: 0.0 ft		64. Operating Rating (tons): 66. Inventory Rating (tons): 36a. Brdg Rail: 1 Meets Standards 36b. Transition: 1 Meets Standards 36c. Appr. Rail: 1 Meets Standards 36d. Appr. Rail Ends: 1 Meets Standard 67. Str Evaluation: 6 Equal Min Criteria																																
PROPOSED IMPROVEMENTS		NAVIGATION DATA																																
94. Bridge Cost: \$258,713 95. Roadway Cost: \$426,876 96. Total Cost: \$724,396 97. Yr. of Cost Est.: 2015		68. Deck Geom.: 2 Intolerable - Replace 69. Vert./Horiz. Undclr: Not applicable (NB) 71. Waterway Adeq: 6 Equal Minimum 72. Appr. Alignment: 8 Equal Desirable Crit 113. Scour Critical: 7 Countermeasures 75. Type of Work: 33 Widen w/o Deck Rel 76. Lngth of Improvement: 54.1 ft 114. Future ADT: 6,400 115. Yr. of Future ADT: 2040																																
NAVIGATION DATA		PROPOSED IMPROVEMENTS																																
38. Nav. Control: Permit Not Required 39. Vert. Clearance: 0.0 ft 40. Horiz. Clearance: 0.0 ft		111. Pier Protect.: 1 Not Required 116. Lift Bridge Vert. Clr.: 0.0 ft																																
214a. Posted Weight Limit: NR b. Posted Speed Limit: NR c. Narrow/1way Brdg Sign: No d. Vertical Clr. Sign: No Adv. Warning Sign: No e. Navigation Lights?: NA Working/Not Working: NA 215. Overpass: STATE HIGHWAY 218. Functionally Obsolete: FO 220. Bridge Redecked: - 221. Substr. Cond. (U/W): 222. Fill Over RCB: AG 223. Appr. Slab/Rwy Cond.: 3 225. Paint Type/Ovrct: N/A 226. Date Painted: 227. Paint Color: -1 233. Deck Forming: 238. School Bus Rte.: Current & Desired route 240. Appr. Rwy Type.: Asphalt/Bituminous 243. Grdr Spacing/No.: /		244. Span Lengths: 245. Girder Depth: 246a. Type of Overlay: NA b. Overlay Thickness: c. Overlay Date: 01/01/1901 d. Ovlv Depth Changed >1": 247. Protective Systems: <table border="1" style="width: 100%; height: 40px; margin-top: 10px;"> <tr><td> </td><td> </td></tr> </table> 248. # Field Splices w/ Corrosion: 249. Scour Crit. POA Exists?: - 250. Headwall: 34.00 258. Plans w/Found.in ODOT File: - 259. Scour Eval. in ODOT File: - 263. Interchange at Intersection: No 264. Interstate Milepoint: -1.00																																

Oklahoma Dept. of Transportation - Bridge Inspection Report

NBI No.: 12596	Structure No.: 4106 0702 X	Local ID: -1	Suff. Rating: 65.60	FO
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Inspection Date: 8/15/22 Adam Hill

Invoice No.: Lincoln2022 Inspected With: Erik Cox



BRIDGE NOTES:

INSPECTION NOTES: 8/15/22

#61(FX) BCD&E MINOR DRIFT IN S CHAN... HISTORY: 2" UTILITY CONDUIT ATTACHED N. SIDE. #361 1'-2' OLD SCOUR WAS FILLED IN WITH RIP RAP.

ELEMENT CONDITION STATE DATA

Elem. / Env	Description	Unit	Total Qty	% 1	Qty. 1	% 2	Qty. 2	% 3	Qty. 3	% 4	Qty. 4
241 / 1	Re Conc Culvert	ft	131.20	90%	118.10	10%	13.10	0%	0.00	0%	0.00
-1											
331 / 1	Re Conc Bridge Railing	ft	108.30	85%	91.90	15%	16.40	0%	0.00	0%	0.00
NEW PARAPET & APPR RAIL INSTALLED SINCE 2000. SOME VERT. CRACKS.											
870 / 1	Concrete Wingwall	(EA)	4.00	0%	0.00	100%	4.00	0%	0.00	0%	0.00
FX- OPEN CRACKS W/ LEACHING & SOME STAINING TO ALL CONN'S. 1/16" OPEN CRACKS EXTENDING INTO BBL'S & APPROX. 1/8" OPEN DIAG. CRACK NEAR CTR. OF N-E WING.											
965 / 1	Debris SF	(EA)	1.00	0%	0.00	100%	1.00	0%	0.00	0%	0.00
PX- LARGE DRIFT HUNG @ THE SOUTH END.											

Monthly Status Report

NEPA Consultant:

CEC/CCE

Eng Contract/Task Order:

CI 2341A

12/6/2023

Project: J/P 34318(04) LINCOLN SH-66: FROM SH-102, EAST TO US-177

UPDATE: 7/10/2023

Step ID		Duration in Calendar days	Target Start from Task Order	Target Completion Date from Task Order:	Actual Start Date:	Actual Completion Date:	Responsible Party	Comments
1	Notice to Proceed Date	0	12/15/2021	12/15/2021	-	12/15/2021	Contract Administrator	Complete
2.1	Provide NEPA Study Footprint	15	12/15/2021	12/30/2021	12/15/2021	12/20/2021	Designer	Complete
2.2	Approve Study Footprint and Location Map/Scope Clarification	15	12/30/2021	1/14/2022	12/20/2021	1/3/2022	EPD	Complete
3.1	Send out Property Owner Notification	10	1/14/2022	1/24/2022	1/3/2022	1/13/2022	Consultant	Complete
3.2	Tribal Property Notification	0	1/14/2022	1/14/2022	-	-	Consultant	Complete (no Tribal Property)
4.1	Cultural Resources & Tribal Coordination Initiation	10	1/14/2022	1/24/2022	1/3/2022	1/15/2022	Consultant	Complete
4.2	Tribal Coordination 30 Day Waiting Period prior to Start of Specialist Studies	45	1/24/2022	3/10/2022	1/15/2022	2/21/2022	Consultant	Complete
5.1	T&E & Wetland Studies	30	3/10/2022	4/9/2022	2/21/2022	4/9/2022	Consultant	Complete
5.2	Hazardous Waste Studies	30	3/10/2022	4/9/2022	2/21/2022	4/9/2022	Consultant	Complete
5.3	ODOT Review of Biological Studies	60	4/9/2022	6/8/2022	4/9/2022	4/21/2022	ODOT Specialists	Complete
5.4	ODOT Review of Haz Waste Studies	60	4/9/2022	6/8/2022	4/9/2022	5/26/2022 & 11/1/2023	ODOT Specialists	Complete
5.5	NRCS coordination	60	4/9/2022	6/8/2022	12/21/2021	-	Consultant	Complete (no response)
5.6	Cultural Resources Studies TO	15	1/14/2022	1/29/2022	1/3/2022	2/10/2022	Consultant	Complete
5.7	Cultural Resources Studies	60	3/10/2022	5/9/2022	2/21/2022	4/9/2022	Consultant	Complete
5.8	ODOT Review of Cultural Resources Studies	60	5/9/2022	7/8/2022	4/9/2022	11/8/2022	ODOT Specialists	Complete
5.9	SHPO Coordination	60	7/8/2022	9/6/2022	11/8/2022	8/14/2023	ODOT Specialists	Complete
6.1	Receive Preliminary Plans	0	9/1/2022	9/1/2022	-	10/31/2022	PMD	Complete
6.2	Review Plans with Footprint	15	9/1/2022	9/16/2022	10/31/2022	10/31/2022	Consultant	Complete
6.3	Attend Plan In Hand	15	9/1/2022	9/16/2022	10/31/2022	11/30/2022	Consultant	Complete
7.1	Request Relocation Studies	15	9/16/2022	10/1/2022	-	7/12/2023	EPD	Complete
7.2	Relocation Studies	30	10/1/2022	10/31/2022	7/12/2023	8/15/2023	ODOT R/W	Complete
7.3	Identify EJ Issues	15	10/31/2022	11/15/2022	8/15/2023	8/18/2023	Consultant	Complete (Not required per Leslie N.)
8	USFWS	60	9/16/2022	11/15/2022	-	4/21/2022	ODOT Specialists	Complete
9.1	Receive R/W & Utility Meeting Plans	0	6/16/2023	6/16/2023	-	6/16/2023	PMD	Complete
9.2	Review Revised Plans with Footprint	15	6/16/2023	7/1/2023	6/16/2023	6/20/2023	Consultant	Complete
9.3	Attend R/W Utility Meeting	15	7/1/2023	7/16/2023	-	7/30/2023	Consultant	Complete
10.1	Receive R/W Submittal Plans	0	6/1/2023	6/1/2023	-	9/29/2023	PMD	Complete
10.2	Review R/W Submittal Plans with Footprint	15	6/1/2023	6/16/2023	9/29/2023	10/14/2023	Consultant	Complete
11.1	Draft CE Preparation	15	6/16/2023	7/1/2023	10/14/2023	11/14/2023	Consultant	Complete
11.2	ODOT Review	15	7/1/2023	7/16/2023	11/14/2023	11/28/2023	ODOT Environmental Contract Manager	Complete
11.3	Final CE Preparation	5	7/16/2023	7/21/2023	11/28/2023	12/6/2023	Consultant	Complete
11.4	FHWA Approval of CE Document	10	7/21/2023	7/31/2023	12/6/2023	-	FHWA	Underway
11.5	Distribution of CE	1	7/31/2023	8/1/2023	-	-	ODOT Environmental Contract Manager	

updated to reflect actual plan submittal date 7/10/23 gac

Duration

594

19.8 months